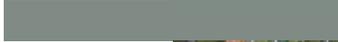
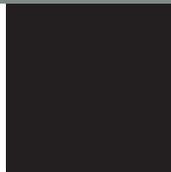




# Downtown Livability

# **BRIEFING BOOK**

APRIL 2013







# Downtown Livability

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Downtown Livability

# PROJECT BACKGROUND





# Downtown Livability

## Council Principles

Approved January 22, 2013

The over-arching purpose of this Initiative is to **advance implementation of the Downtown Subarea Plan**, in particular the Plan’s central theme of making Downtown more **Viable, Livable, and Memorable**. The project will be guided by the existing vision set forth in the Downtown Subarea Plan, and work to more effectively implement the Plan. The focus is on the specific elements of the Land Use Code and related codes as laid out in the Project Scope approved by Council in September 2012, which includes strong coordination with the companion Downtown Transportation Plan update occurring in this same timeframe. However, if other related issues arise, the Council desires to hear about these and have the opportunity to refer them to this or another venue, such as the Major Comprehensive Plan Update.

### The Project Scope includes the following:

- Amenity incentive system
- Building form and height
- Design guidelines
- NE 6th Street Pedestrian Corridor
- Light rail interface
- Downtown parking
- Vision for Downtown OLB district
- Downtown signage
- Sidewalk widths and landscaping
- Vacant sites and buildings
- Mechanical equipment screening
- Recycling and solid waste
- Vendor carts
- Range of permitted uses
- Green, energy efficient, and sustainable development forms
- The Land Use Code interface with the mobility work underway through the Downtown Transportation Plan

This is the most extensive Code update since the adoption of the original Downtown Land Use Code in 1981. In the intervening decades, Downtown Bellevue has evolved dramatically, from a bedroom suburb to a dynamic regional employment center, as well as the City’s fastest growing residential neighborhood. This project should place particular emphasis on the following changes that have led to and accompanied Downtown’s evolution.

### Change

### Principle

- After several development cycles since the original Code adoption, it has become increasingly clear what is working and not working with development incentives.
- Downtown Bellevue has experienced a massive influx of new residents. This has helped create long hoped-for urban qualities, but also led to increased frictions that occur in a dense, mixed use environment.
- Downtown has seen a significant increase in pedestrians and street-level activity.
- Through new development, Downtown has an opportunity to create more memorable places, as well as a distinctive skyline.

- 1. Refine the incentive system to develop the appropriate balance between private return on investment and public benefit.**
- 2. Promote elements that make Downtown a great urban environment while also softening undesirable side effects on Downtown residents.**
- 3. Increase Downtown’s liveliness, street presence, and the overall quality of the pedestrian environment.**
- 4. Promote a distinctive and memorable skyline that sets Downtown apart from other cities, and likewise create more memorable streets, public spaces, and opportunities for activities and events.**

## Change

- Environmental rules and strategies have evolved over the past decades since the Downtown Code was adopted.
- Downtown is attracting a younger and more diverse demographic mix, of workers, visitors, and residents.
- As Downtown has become a more mature urban center, it is experiencing an increase in visitors and more interest in tourism.
- We live in an increasingly global economy, with flows of goods and services, capital and people transcending state and national boundaries.
- Downtown's relationship with adjacent residential neighborhoods has evolved. It remains important to achieve a transition in building form and intensity between Downtown and adjacent residents, but nearby neighborhoods are also seeking the attractions that the city center brings.
- The development arena is becoming increasingly competitive, as Downtown continues to seek quality investments that implement the Subarea Plan vision.
- As Downtown has matured and filled in, opportunities for quality development are becoming limited, and expectations have grown as to how each development contributes to the greater whole.
- Bellevue's park and open space system has dramatically evolved, for example with acquisition and planning for Meydenbauer Bay Park, development of the Downtown Park, and the nearby Botanical Garden on Wilburton Hill.

## Principle

- 5. Encourage sustainability and green building innovation in Downtown development. Enable design that promotes water, resource, and energy conservation, and that advances ecological function and integrity.**
- 6. Respond to Downtown's changing demographics by meeting the needs of a wide range of ages and backgrounds for an enlivening, safe and supportive environment.**
- 7. Promote elements that will create a great visitor experience and a more vital tourism sector for Downtown.**
- 8. Strengthen Downtown's competitive position in the global and regional economy, while reinforcing local roots and local approaches.**
- 9. Maintain graceful transitions with adjoining residential neighborhoods, while integrating these neighborhoods through linkages to Downtown attractions.**
- 10. Refine the Code to provide a good balance between predictability and flexibility, in the continuing effort to attract high quality development that is economically feasible and enhances value for all users.**
- 11. Promote through each development an environment that is aesthetically beautiful and of high quality in design, form and materials; and that reinforces the identity and sense of place for Downtown and for distinct districts.**
- 12. Advance the theme of "City in a Park" for Downtown, creating more green features, public open space, trees and landscaping; and promoting connections to the rest of the park and open space system.**



# Downtown Livability

## **PUBLIC ENGAGEMENT PLAN**

The Downtown Livability Initiative will necessitate extensive public outreach to a broad spectrum of stakeholder communities. The project scope includes a targeted review of specific regulations that guide Downtown development and land use activity. Public outreach began in fall 2012 and will be ongoing throughout 2013 and into 2014. Feedback and public comment will be sought from Downtown residents, perimeter area neighborhoods, property owners and management, workforce, corporations, visitors, business leaders, civic institutions and non-profits.

### **Objectives for Public Engagement:**

- Seek ideas and perspectives on an on-going basis from the Downtown community on the full range of topics covered in the Downtown Livability Initiative.
- Keep stakeholders informed on the current status of the Livability Initiative, how they can impact change, and help them discover and adapt to the future Downtown environment.
- Establish on-going long term relationships with individual residents, organizations and businesses with Downtown interests.
- Coordinate with organizations that have a continued interest in the Downtown, such as the Bellevue Downtown Association, Bellevue Chamber, major corporations such as Microsoft and Expedia, retail associations, Sound Transit, Metro Transit and others.
- Ensure that information is well documented and distributed through a variety of means, including traditional outreach, interactive experiences and technology based methods.

### **Project Advisory Body:**

An advisory body comprised of the following members will help guide this effort. The full Planning Commission will ultimately review and hold a hearing on the entire code amendment package prior to Council for adoption. This approach is intended to instill a wide range of perspectives into the Downtown Livability process, while turning the work over to the Planning Commission as early as possible in order to meet the project timeline.

- 2 members from the Planning Commission
- 1 member from the Transportation Commission
- 1 member from the Environmental Services Commission
- 1 member from the Parks & Community Services Board
- 1 member from the Arts Commission
- 1 member from the Human Services Commission
- 1 member from the Bellevue Downtown Association

- 1 member from the Bellevue Chamber
- 1 small business representative
- 1 resident of Downtown
- 1 resident from neighborhoods near Downtown
- 1 architect
- 1 City-wide representative
- 1 Downtown employer

**Stakeholder Groups:**

To accomplish the above objectives, public outreach will incorporate a multi-faceted approach to reach stakeholders. Outreach tactics will include interactive experiences, traditional media and web use, and creative use of new technology. The following groups are the primary targets for outreach.

**Residential:** The Downtown Livability Initiative provides an opportunity for Neighborhood Outreach staff to launch a parallel process to engage with and establish relationships with Downtown residents both by residential building and in broader neighborhood segments. Personal interaction will help determine geographic and demographic definitions of Downtown neighborhoods seen from the lens of the residents and will aid with planning for long-term neighborhood outreach and identity.

**Workforce:** The Downtown workforce is currently near 42,000 jobs and constitutes a large voice in what is desirable for the Downtown mobility and cultural/entertainment experience. Taking the information to this demographic through interaction in their workplace and use of technology will be most effective in reaching the largest number of this target audience.

**Business Owners and Developers:** These stakeholders are more limited in number, but will require extensive outreach and interaction because of the scope elements being analyzed. It is anticipated that outreach in small groups or individual settings will be most effective.

**Organized Business Community, Civic Organizations and Non-Profits:** These stakeholders are best reached through interaction with organized groups such as BDA, Chamber, Rotary and various civic institutions and non-profits.

**Perimeter Area Neighborhoods:** The neighborhoods surrounding the perimeter of Downtown are all long established, active neighborhoods with engaged leadership (Surrey Downs, Enatai, Vuecrest, Meydenbauer). These residents consider Downtown part of their daily living environment and consider it their expanded neighborhood. Not only should we seek their input, we should leverage their years of experience as we explore the identity of the Downtown residential community.

**Visitors:** Because visitors are not part of the permanent fabric of our community, we

must seek input from the businesses and institutions that cater to tourism such as Meydenbauer Center and the hotel community.

### **Outreach Strategies:**

Some or all of the identified outreach strategies below will be implemented to achieve the outreach objectives.

**Open Houses:** An initial open house and scoping meeting kicked off the Downtown Livability Initiative on November 29, 2012. Outreach was primarily through email, traditional media, COB website, BDA and posters in Downtown residential and public buildings. Attendance of 150 was greater than anticipated. An animated and interested crowd confirmed that stakeholders are interested in active participation in the Downtown Livability Initiative feedback process. Display stations categorized the scoping topics around Enjoying Downtown, Getting Around Downtown, and Designing Downtown. Future open houses will be scheduled around project milestones, tentatively set for June 2013 and November 2013.

**Briefings:** Downtown Livability team will create a travelling display and presentation that can be taken to companies such as Microsoft and Expedia, and to functions at BDA, Chamber and other interested boards, civic organizations and non-profits, to provide an overview of Downtown Livability. By reaching out to these groups, we will include a much larger segment of the workforce and business population.

**Neighborhood Focused Meetings:** Downtown residential outreach team will work in parallel with Downtown Livability Initiative to reinforce neighborhood identity in defined Downtown neighborhoods. A variety of events and interactions will be used to begin to create Downtown engagement for residents.

**Focus Groups:** To help articulate values of the community, a variety of targeted focus groups will take place over the course of the project. Examples of stakeholder groups include: residents, workers, visitors, architects & planners, property owners & developers, brokers, large companies and retail, visionaries, former Downtown advisory committee members (from 2002-2004), institutions, and cultural/arts groups. These focus groups would allow staff to dig deeper on specific topics.

**Coffee and Conversations:** Coffee events in various Downtown locations offer a chance for informal conversation on issues facing Downtown. Provides a venue for local residents and business owners be updated on project and provide feedback. These would be facilitated by staff or by advisory body members.

**Walking Tours:** Guided tours led by staff would explore the history, arts, culture, hidden treasures, and little known connections in the Downtown area. Highlights would include Downtown history and architectural background as well as a look at public plazas and art, pedestrian through-streets, mid-block crossings, and sites for future development.

These would be followed by coffee house discussions and gathering of feedback.

City of Bellevue Downtown Livability Project Website: Site will be continually updated with project information. Interactive features could include on-line visual simulations and/or survey tools. Use of City Facebook page and social media for two way interaction.

Media: News releases and story pitches to local media surrounding major project milestones. Use of It's Your City, Neighborhood News internally, and external media including Seattle Times, Bellevue Reporter, Puget Sound Business Journal, Transit Blog, etc.

Flyers/Posters: Downtown public bulletin boards have been identified and postings for events will be done in public venues, as well as residential and office buildings.



## SCOPE OF WORK

August 2011

### TASK 1: BEST PRACTICES

#### APPROACH

The initial task of the DKS team to analyze best practices is fundamental for setting the outcomes of the study. The preferred measures of effectiveness (MOEs) chosen here for use in Bellevue's complex planning environment will require careful consideration of what has been successfully applied elsewhere. To accomplish this task, the DKS team will draw upon experience from several states when developing and evaluating transportation systems and plans applicable to Downtown Bellevue. The DKS team will use planners and engineers who are recognized leaders in traffic operations, parking, transit operations, bicycle and pedestrian facilities to advise on this task. Each MOE will be included for its ease of measurement, predictability in forecasts, understandability to policy makers and the general public, and applicability to Downtown Bellevue.

#### SPECIFIC SUBTASKS

- i. The DKS team will prepare a decision-making framework that includes qualitative and quantitative MOEs to help identify and compare the performance of specific transportation system recommendations relative to desired viability, livability, accessibility, and memorability outcomes – as described in the Downtown Subarea Plan. In addition to the current adopted level of service (LOS) standard that measures traffic capacity at intersections, MOEs may be comprised of such components as the benefit to mobility (vehicular level of service/travel time/mode split/pedestrian and bicycle circulation), the effect on the Downtown environment (urban character/aesthetics/pedestrian and bicycle environment), benefit to community sustainability (such as walkability, bikeability, vehicle miles traveled and greenhouse gas emissions), and other factors considered best practice or important for a multi-modal downtown environment.

***Deliverable:*** Prepare for Transportation Commission consideration and discussion recommended measures of effectiveness -both qualitative and quantitative, and an explanation of why these are important for this project -that can be used to evaluate individual proposed projects as well as the overall package of transportation system recommendations for the Downtown Transportation Plan Update.

- ii. The DKS team will research and correlate transportation and growth trends in Downtown Bellevue, and compare to similar emerging mixed-use urban centers. The DKS team will

summarize the correlation between growth and certain transportation measures such as Average Weekday Trips, PM Peak hour trips, Vehicle Miles Traveled, Greenhouse Gas Emissions (GHG) travel time, and others. The team will analyze how this research correlates with the Bellevue-Kirkland-Redmond (BKR) travel demand forecasts and how it could be applied within Downtown Bellevue to affect transportation system decisions that are made based upon trip generation and travel demand.

***Deliverable:*** *A brief technical paper that describes findings from Downtown Bellevue and other similar emerging urban centers related to land use and specific transportation system metrics, especially metrics related to non-motorized modes.*

## **TASK 2: EXISTING CONDITIONS**

### **APPROACH**

Using the MOEs defined in Task 1, the DKS team will prepare an Existing Conditions report that documents the physical infrastructure, and the operations and performance for all key travel modes. DKS will coordinate with City staff to obtain data from a variety of sources including the City of Bellevue, King County Metro, Sound Transit, Puget Sound Regional Council, Washington State Department of Transportation, Microsoft (Connector bus service), and potentially other private sources such as Inrix.

The DKS team will include future baseline conditions in addition to existing conditions information where available. The 2030 baseline condition will include a description of the anticipated transportation system performance, given growth assumptions and regional and local transportation assumptions (determined to be reasonably foreseeable by 2030). The report will present the 2030 baseline condition assuming that a newer plan has not been adopted, except where prior projects are deemed not reasonably foreseeable and have been excluded from consideration for this project. The DKS team will include a description of GHGs in the report for both the existing and 2030 baseline conditions.

### **SPECIFIC SUBTASKS**

- i. The DKS team will document the existing (2011 or most recent) conditions of the transportation system in Downtown Bellevue, including the physical infrastructure, operations and performance for all modes. The task will incorporate the existing conditions and 2030 baseline findings as provided by City of Bellevue staff.

***Deliverable:*** *An “Existing Conditions Report” that includes an inventory of the physical components of the motorized and non-motorized transportation system in Downtown Bellevue (streets, alleys, mid-block pedestrian connections and crossings, on-street parking, traffic control, etc.) and the operational metrics such as traffic counts, pedestrian and bicycle counts, transit ridership, level of service, accident type and location, and other components that are available. No original data collection is expected in this task. The Existing Conditions Report will include a status report, provided by City staff, on the projects in the current Downtown Subarea Plan. The report*

will also contain 2030 baseline condition land use and transportation information where provided by City staff.

## **TASK 3: ROADWAY CAPACITY AND OPERATIONS**

### **APPROACH**

The DKS team will summarize roadway operations and discuss variability for AM and PM peak periods, weekday mid-day, AM and PM off-peak, and weekends. The DKS team will analyze transit priority, pedestrian crossing times and changes in activity, and signal system improvements. DKS will work with staff to include recognition of the Sydney Coordinated Adaptive Traffic Systems (SCATS) investment in traffic operations analyses and travel model forecasts. DKS will identify and evaluate potential traffic movement strategies that may vary by time of day and day of week and in real-time. DKS will present travel delay to the movement of people to supplement the more typical vehicle delay analysis. DKS will identify locations where off-peak street use for parking and loading/unloading during non-peak hours would support adjacent land use and could occur without significantly degrading traffic operations at those times. Finally, DKS will discuss with staff ways to modify travel demand model forecasts that may have oversaturated the traffic volumes, because this cannot be effectively simulated in models; to remedy the technical problem, DKS will recommend several appropriate technical adjustments that the City staff may implement.

### **SPECIFIC SUBTASKS**

- i. DKS will work with City staff to assess the performance of several roadway and intersection projects identified in the Downtown Subarea Plan that have not been built or programmed. This effort may include assessing these projects using the MOEs developed earlier. DKS will identify possible project modifications or alternate approaches to general capacity expansion that may provide a better solution for the City's viability, livability, accessibility, and memorability outcomes in the Subarea Plan.

***Deliverable:*** Memorandum that summarizes findings of Downtown roadway/intersection projects that are described in the Downtown Subarea Plan (and assumed as part of the roadway network in the 2030 BKR travel demand model) but not built or programmed, to determine their performance relative to the MOEs developed in Task 1. The memorandum will make initial recommendations for each planned project as to whether it should be retained, modified, removed or replaced with an alternative mobility improvement.

- ii. DKS will identify potential new capacity and operational improvements that would address 2030 travel demand needs on the local roadway system (within Downtown and along arterials that serve Downtown from adjacent neighborhoods and the regional freeway system). DKS will evaluate each project as stand-alone or through packaging with a set of related projects.

***Deliverable:*** A memorandum that presents potential new local roadway capacity projects or operational enhancements for the 2030 transportation system in Downtown and the vicinity and uses MOEs developed in Task 1 to evaluate these projects.

**Deliverable:** *A technical memorandum summarizing the City’s efforts in Traffic Signal Adaptive System technologies and the implementation/operation of the Sydney Coordinated Adaptive Traffic Systems SCATS to more efficiently move traffic in and through Downtown. Coordinate with staff to compile information that describes the City’s progress and outcomes of the system as well as any new technology advancements and operational strategies for the signal system network in Downtown.*

- iii. DKS will identify potential improvements to the regional transportation network, beyond those assumed in the 2030 “baseline” travel demand model that would improve the capacity to move people and goods to the benefit of Downtown Bellevue residents and businesses.

**Deliverable:** *A technical memorandum that includes a list of potential new regional transportation system projects and include quantitative and/or qualitative rationale to describe the benefit of each project or group of projects to mobility for Downtown Bellevue residents and businesses.*

- iv. DKS will explore ideas and options to implement a more extensive system of short-term on-street parking and loading zones to support the needs for Downtown residents and businesses. This includes a generalized assessment of the concept of implementing off-peak on-street parking and permanent on-street parking on targeted arterials and the potential impacts on roadway capacity, transit operations and bicycle facilities, as well as an assessment of the use of existing and new alleys for parking and loading.

**Deliverable:** *A report that includes a mapped inventory of existing short-term on-street parking, loading zones and alleys in Downtown Bellevue, and an analysis of the benefits and opportunities to increase the on-street parking supply and/or loading zones. The report will include a qualitative and/or quantitative review of the value and trade-offs to the Downtown community – businesses and residents – of on-street parking and loading zones, and the potential of increasing the supply of each. It will also include a brief evaluation of the potential benefits/impacts of implementing a paid on-street parking program in Downtown Bellevue.*

- v. DKS will work together with City staff to apply traffic operational modeling software (VISSIM or similar) to simulate 2030 travel modes (car, bus, light rail, bicycle, and pedestrian) in select corridors Downtown and link the traffic simulation with 3D visual representations of future Downtown buildings and streetscapes. DKS will work with staff to obtain BKR travel demand data and document/code the roadway details within the desired study areas and corridors.

**Deliverable:** *A set of VISSIM and AVI files that document the 2030 “baseline” condition, and the recommended 2030 multi-modal transportation condition for the select areas and corridors of Downtown Bellevue and for select arterial corridors that serve the Downtown in adjacent areas..*

## TASK 4: TRANSIT SERVICE AND OPERATIONS

### APPROACH

The DKS team will present an assessment of the Downtown Bellevue Transit Center capacity, and how different types of transit service (short-distance local, long-distance commute, overall Downtown circulation, planned light rail service and associated light rail feeder service) are each able to serve both the Transit Center as well as the remainder of Downtown. The DKS team will identify strategies that recognize the relationship between transit speed and reliability, and ridership, and will identify operational projects that would maximize access within and through Downtown, potentially increasing transit mode shares of all types of trips.

The DKS team will include an analysis of underserved transit markets by location and time of day; generalized approaches on each category of transit service differently; which facilities are needed to provide access, safety and comfort in different conditions; and ways to maximize productivity for transit operators, while improving speed and reliability for transit riders.

### SPECIFIC SUBTASKS

- i. The DKS team will review the projected Downtown 2030 baseline transit ridership (bus and light rail) in the BKR travel demand model which assumes East Link light rail and the current transit integration plan, and identify ways to potentially improve transit mode share.

***Deliverable:** A technical memorandum that documents the projected 2030 transit mode share relative to other motorized trips. This memorandum will also provide recommendations for programmatic, project, operational and service changes that may improve transit ridership and mode share over the model projections.*

- ii. The DKS team will review the existing conditions information as well as analyze transit trips projected to/from Downtown Bellevue in 2030 to identify the underserved transit markets in the region. This will provide a basis to recommend specific improvements to planned transit service (considering primarily routing and headways) could help meet market demand and improve transit mode share to Downtown Bellevue. Recommendations should provide support for advocacy to transit service providers for improved service to these underserved markets.

***Deliverable:** A technical memorandum that documents a quantitative analysis and mapping of transit markets for Downtown Bellevue commuters and identify those markets that are currently underserved. The memorandum will include specific recommendations for modifications to transit service to better serve these markets.*

- iii. While Bellevue does not directly provide transit service, Bellevue does control the rights-of-way on which transit operates. The City, in partnership with transit service providers, may be able to enhance transit speed and reliability in Downtown Bellevue by implementing measures such as transit signal prioritization, bus-only lanes in peak hours, improving sidewalks and bus stops, and other tools and strategies.

**Deliverable:** *A technical memorandum that identifies specific recommendations for the application of available tools, technology and the public right-of-way to improve transit speed and reliability, and to improve passenger comfort and transit access in Downtown Bellevue. The memorandum will describe potential benefits to productivity, ridership and transit mode share that may be realized through each concept.*

## **TASK 5: NON-MOTORIZED TRANSPORTATION**

### **APPROACH**

The City of Bellevue developed a comprehensive Pedestrian and Bicycle Transportation Plan in 2009. Work on the Downtown Transportation Plan Update will complement that effort. To do this, the DKS team will examine non-motorized transportation solutions that provide both access and comfort. This will include methods to provide a quantitative justification for ways to increase pedestrian and bicyclist safety, security and access, and connect to larger non-motorized systems that link neighborhood, citywide and regional trip origins and destinations.

### **SPECIFIC SUBTASKS**

- i. The DKS team will prepare an evaluation of the current and planned bicycle system. Recommendations for improvements will be based on, but not limited to, the Bellevue Pedestrian and Bicycle Transportation Plan (2009). The team will identify improvements to existing or planned bicycle routes and facilities that will provide superior bicycle access within Downtown and connectivity to neighborhoods and regional facilities and destinations.

**Deliverable:** *A technical memorandum that includes a bicycle route map, descriptions and/or illustrations of bicycle facilities recommended to serve Downtown and provide connections to neighborhoods and regional facilities and destinations, and ways to prioritize bicycle system recommendations in the context of the MOEs identified in Task 1. Recommendations will include innovative and industry-accepted treatments that provide for bicycle travel through congested and constrained urban corridors like those in Downtown Bellevue.*

- ii. The DKS team will prepare an evaluation of the current and planned pedestrian system. Recommendations for improvements will be based on, but not limited to, the Bellevue Pedestrian and Bicycle Transportation Plan (2009) and the current Downtown Land Use Code. The team will identify improvements to existing or planned sidewalks, mid-block connections, intersection and mid-block crosswalks/signals, and other facilities that will provide a more safe, comfortable and accessible (including accessibility in the ADA context) pedestrian environment within Downtown Bellevue and to adjacent neighborhoods.

**Deliverable:** *A technical memorandum that describes new or enhanced pedestrian facilities that enable Downtown to be a more accessible and more attractive place to walk. The memorandum will include ways to prioritize pedestrian system recommendations in the context of the MOEs.*

## TASK 6: IMPLEMENTATION PLAN

### APPROACH

The DKS team will organize elements in a manner agreed with City staff. Specifically, this will include packaging projects that work together; developing project diagrams and sketches that convey both the strategies and their benefits to the public; and creating a phased and prioritized implementation plan that will provide a useful guide to pursue funding through regional programs and developer incentives.

### SPECIFIC SUBTASKS

- i. The DKS team will recommend specific transportation system improvements – after review of BKR travel demand modeling results and evaluating potential projects developed in Tasks 3, 4 and 5 -that will meet the intended mobility standard in 2030 (which is assumed to be a Level of Service E+ for Mobility Management Area 3 that encompasses all of Downtown Bellevue). This will include infrastructure for all modes as well as operations improvements.

***Deliverable:*** A comprehensive list of recommended transportation system projects and technology applications for all modes. Includes a MOE evaluation for each project or group of projects with respect to their role in providing long-term multi-modal mobility in Downtown Bellevue. The list will include a vicinity map of all projects and a larger-scale site map to highlight individual projects or group of projects.

***Deliverable:*** A set of planning-level sketches for select projects or group of projects (up to 10) to be jointly determined by the Consultant and the City.

***Deliverable:*** A set of planning-level cost estimates for operational improvements and for the above recommended capital projects.

- ii. DKS will prepare an implementation plan for recommended improvements to Downtown roadways and operations, transit service and facilities, and non-motorized transportation facilities.

***Deliverable:*** A report that includes a phased implementation plan, a prioritization strategy and potential funding resources for recommended improvements to the transportation system. The report will include logical pairings/groupings of infrastructure and/or operational and programmatic improvements. As part of the phasing and prioritization considerations, the report will include threshold metrics or conditions that would need to occur to trigger the implementation of a project – such as degraded level of service, increased traffic volume, transit capacity, or increased transit or auto travel time. A section of the report will discuss applicable funding resources and strategies that the City could use to fund implementation of recommended projects. The report will include a documentation of additional projects that arise as “good ideas” through the planning effort, but may have to be implemented beyond the defined 2030 horizon – or as policy or code amendments.

## TASK 7: PUBLIC INVOLVEMENT

### APPROACH

The City of Bellevue and the DKS team will work together to effectively engage the community using well-thought through public involvement processes and concise presentation materials.

### SPECIFIC SUBTASKS

- i. Bellevue staff will lead the public involvement component of the Downtown Transportation Plan Update. Consultant will support staff in the undertaking of public and stakeholder involvement. The DKS team will provide support, generally consisting of preparing electronic and print materials and attending and participating in public meetings as directed by City staff. DKS staff will be available to attend meetings with stakeholder groups (such as the Bellevue Downtown Association), interested business owners and property owners, Downtown residents, transportation advocacy organizations such as Feet First and the Cascade Bicycle Club, and community open houses. DKS will also assist City staff with outreach through electronic and social media strategies.

***Deliverable:** Assistance to staff in public information and outreach activities such as preparing newsletters, creating materials and presentations for community meetings and stakeholder group discussions, developing visual simulations and displays, creating and managing a project web site, crafting a web-survey, producing a video, and other related tasks. Consultant attendance at public and stakeholder meetings will be kept to a minimum, with the appropriate level of support for each meeting to be determined jointly by the City and Consultant.*

- ii. The DKS team will provide support for staff presentations to the Bellevue City Council, and City boards and commissions.

***Deliverable:** Prepare presentation graphics or other materials; and attend and participate in meetings with City Council, the Transportation Commission (who is the advisory group for this project), and other City boards and commissions as scheduled by staff. Consultant attendance at meetings will be kept to a minimum, with the appropriate level of support for each meeting to be determined jointly in advance by the City and Consultant.*

## TASK 8: FINAL REPORT

### APPROACH

The content and format of the final report will require both the consultant team and City staff to work together to ensure that its content is appropriate. DKS anticipates at least two rounds of internal review before releasing it for wider distribution. This review will help ensure that the final deliverables are well written without typographical errors, contain understandable tables and figures, and convey concise messages for readers.

### SPECIFIC SUBTASKS

- i. The DKS team will coordinate with City staff to product a final report that documents the

process and recommendations of the Downtown Transportation Plan Update. This report will include an executive summary (that may be used as a stand-alone product) and will be organized in chapters and appendices roughly corresponding to the Tasks identified in this Scope of Work. The final report will be the repository for the all of the text, graphics, plans and figures developed as recommendations. It will also document all of the public involvement activities in summary form, with appendices as needed.

***Deliverable:*** Staff and the DKS team will work together to develop the content and format of the final report. DKS will prepare an electronic copy of the final report in PDF format as well as deliver to the City the electronic source document.

## **TASK 9: PROJECT MANAGEMENT**

### **APPROACH**

The management of the DKS consultant team and deliverables on this project is best attained when project procedures are established at the beginning. DKS will work with City staff on a style guide and review protocol at the outset of the project to assure that products have a consistent feel and appearance. DKS will also work with the staff to provide appropriate documentation on project progress so that project invoices can be easily understood and approved by the City. Perhaps, most importantly, DKS will carefully guide the progress of its staff, as well as closely monitor work assignments by the sub consultants. DKS has a history and a close working relationship with its sub consultants, which will foster close coordination with their key staff.

### **SPECIFIC SUBTASKS**

- i. The Consultant will manage the contract tasks and budget in general accordance with the contract. Proposed reallocation of budget resources among budgeted tasks, or changes to the preliminary timeline may be initiated by the City or consultant and may be approved with concurrence of the each party.

***Deliverable:*** Consultant project manager and appropriate consultant team members will develop an agenda for and participate in a project kick-off meeting at a mutually agreed-upon time subsequent to receiving a notice to proceed.

***Deliverable:*** Consultant project manager will prepare invoices and progress reports on a monthly basis, and submit these to the city of Bellevue project manager. Progress reports will document the specific work accomplished and the completion status for each task identified in this scope or work, plus the budget status for each task. Identify and document any emerging issues related to task, budget or timeline.

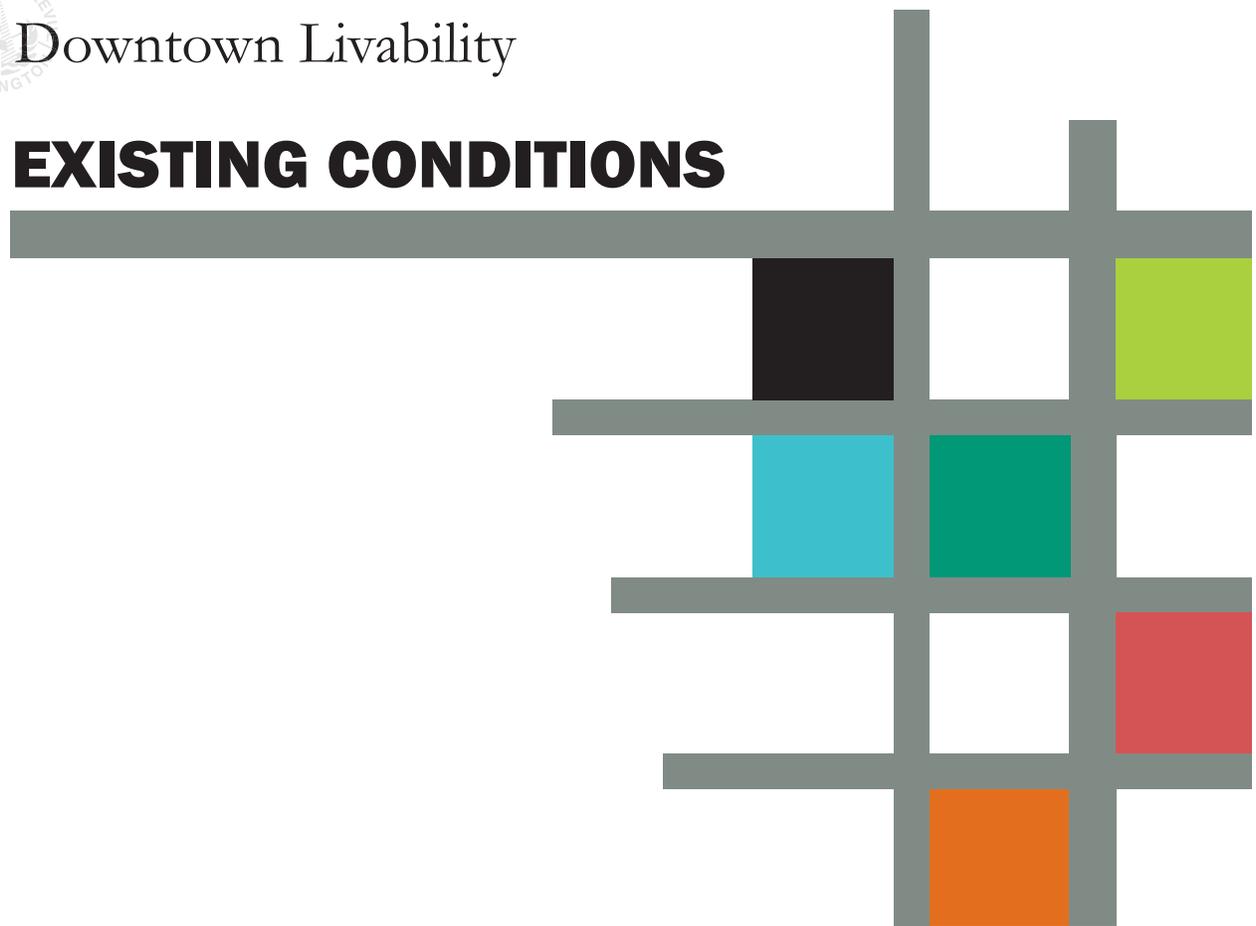
***Deliverable:*** Consultant project manager and city of Bellevue project manager will meet on a mutually agreeable monthly recurrence schedule to review progress and issues related to the scope, timeline and budget. Additional project management meetings will be held as needed in response to emerging issue and in preparation for major events.





# Downtown Livability

## **EXISTING CONDITIONS**

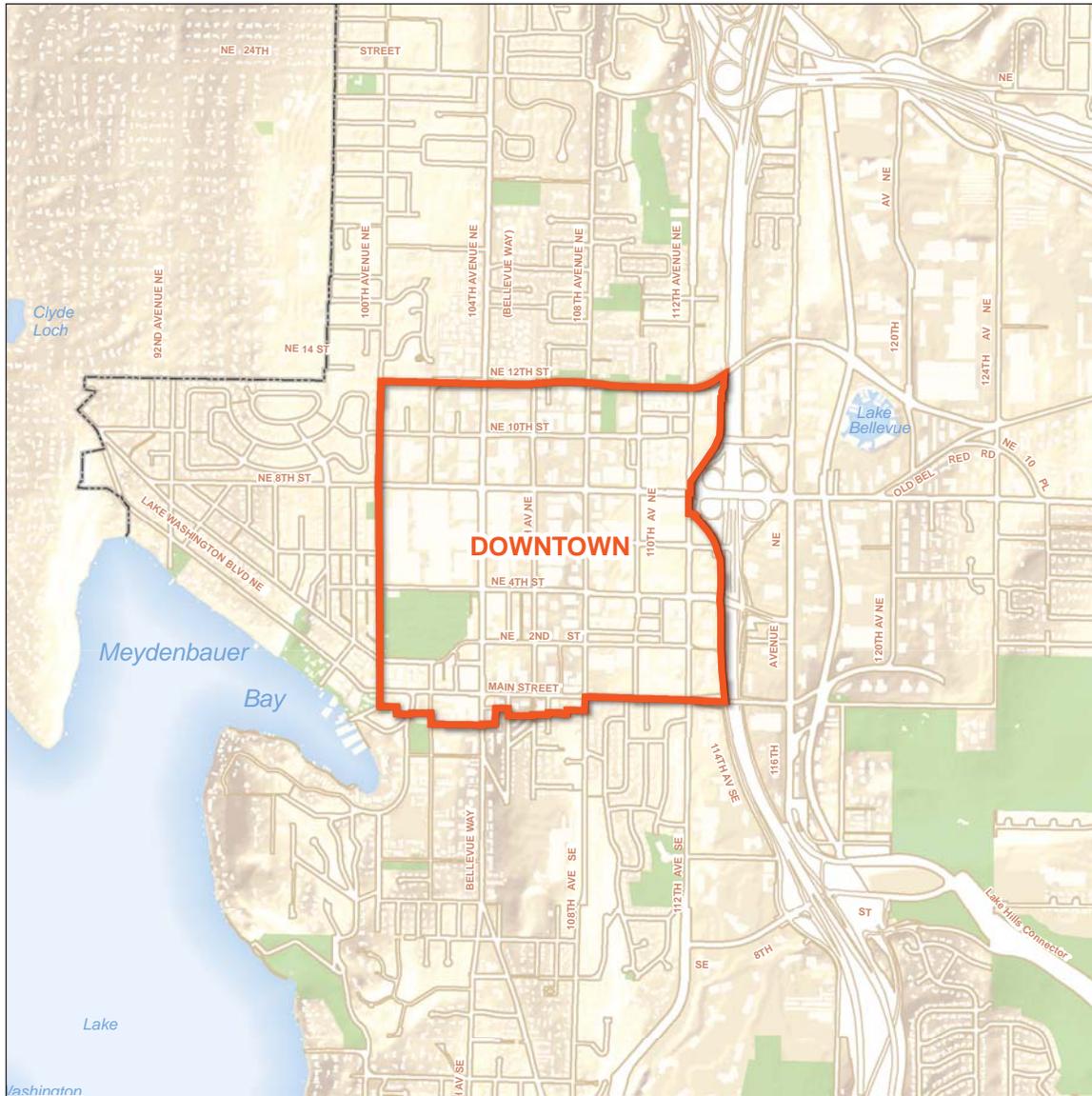






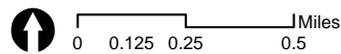
# Downtown Livability

## VICINITY MAP



 Downtown Boundary

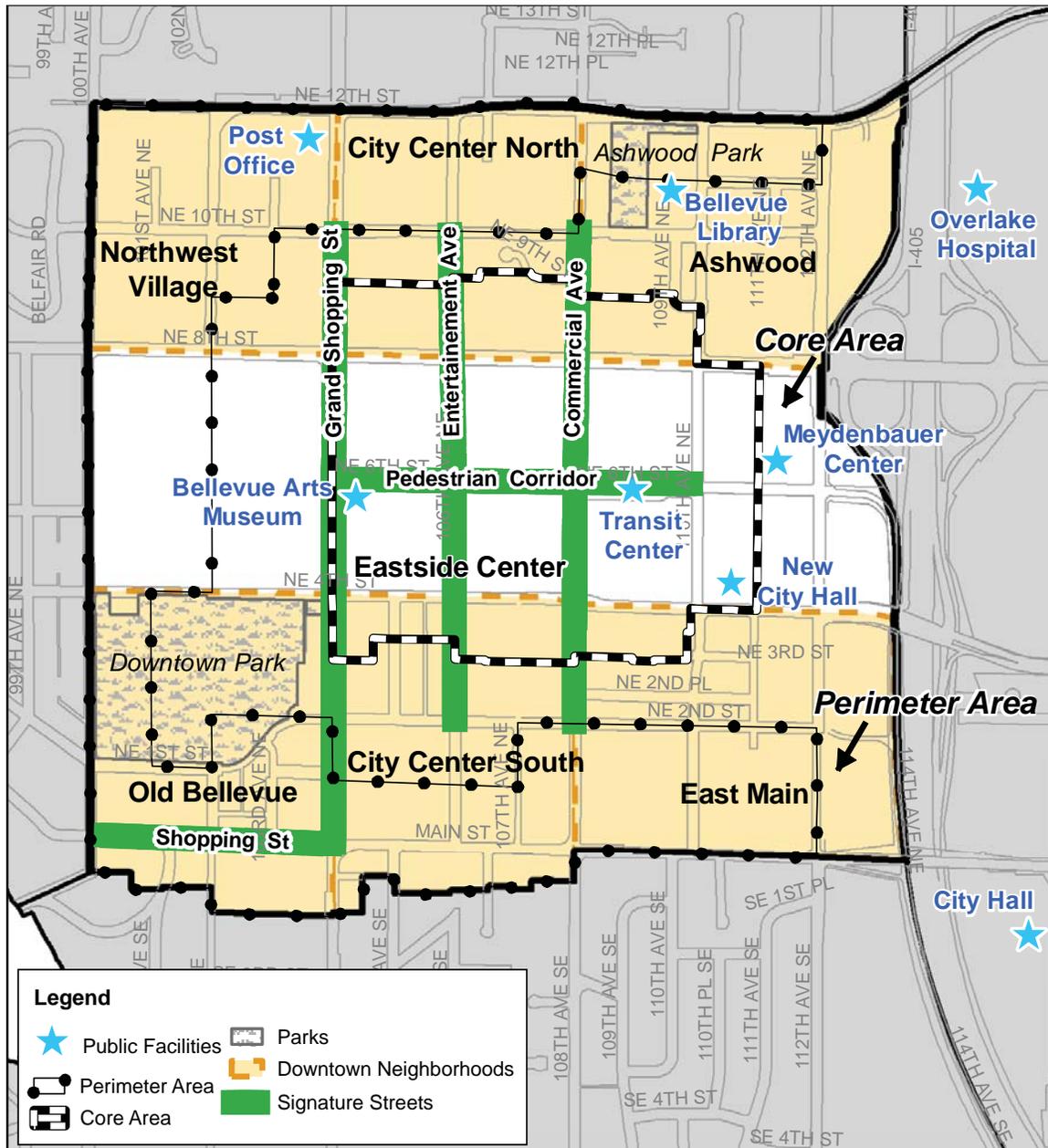
 City Limits





# Downtown Livability

## SUBAREA PLAN



**Legend**

- ★ Public Facilities
- Parks
- ⬜ Perimeter Area
- Downtown Neighborhoods
- ▬ Core Area
- ▬ Signature Streets



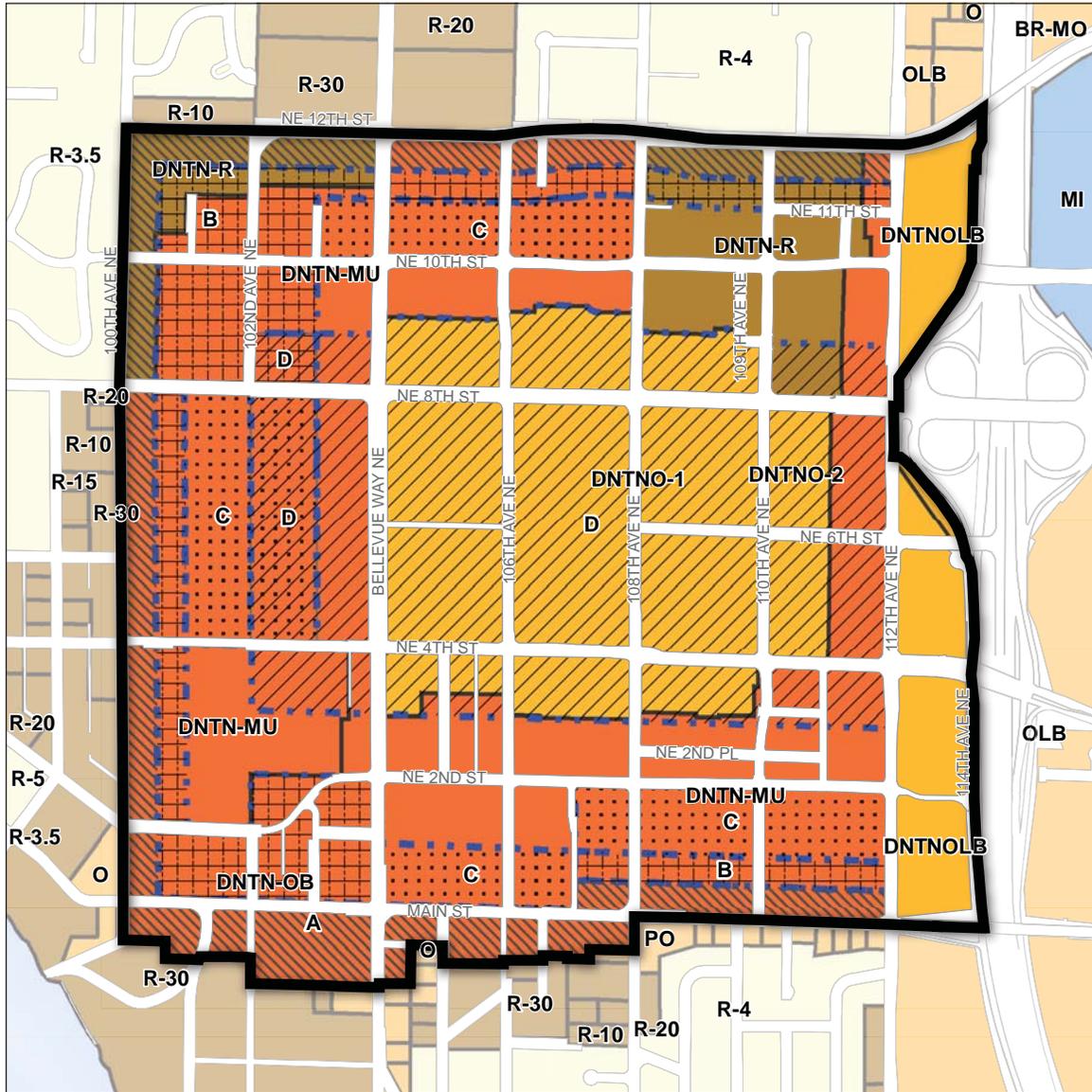
FIGURE A  
Downtown Future Land Use Plan



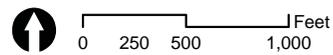


# Downtown Livability

## DOWNTOWN ZONING AND DESIGN DISTRICTS



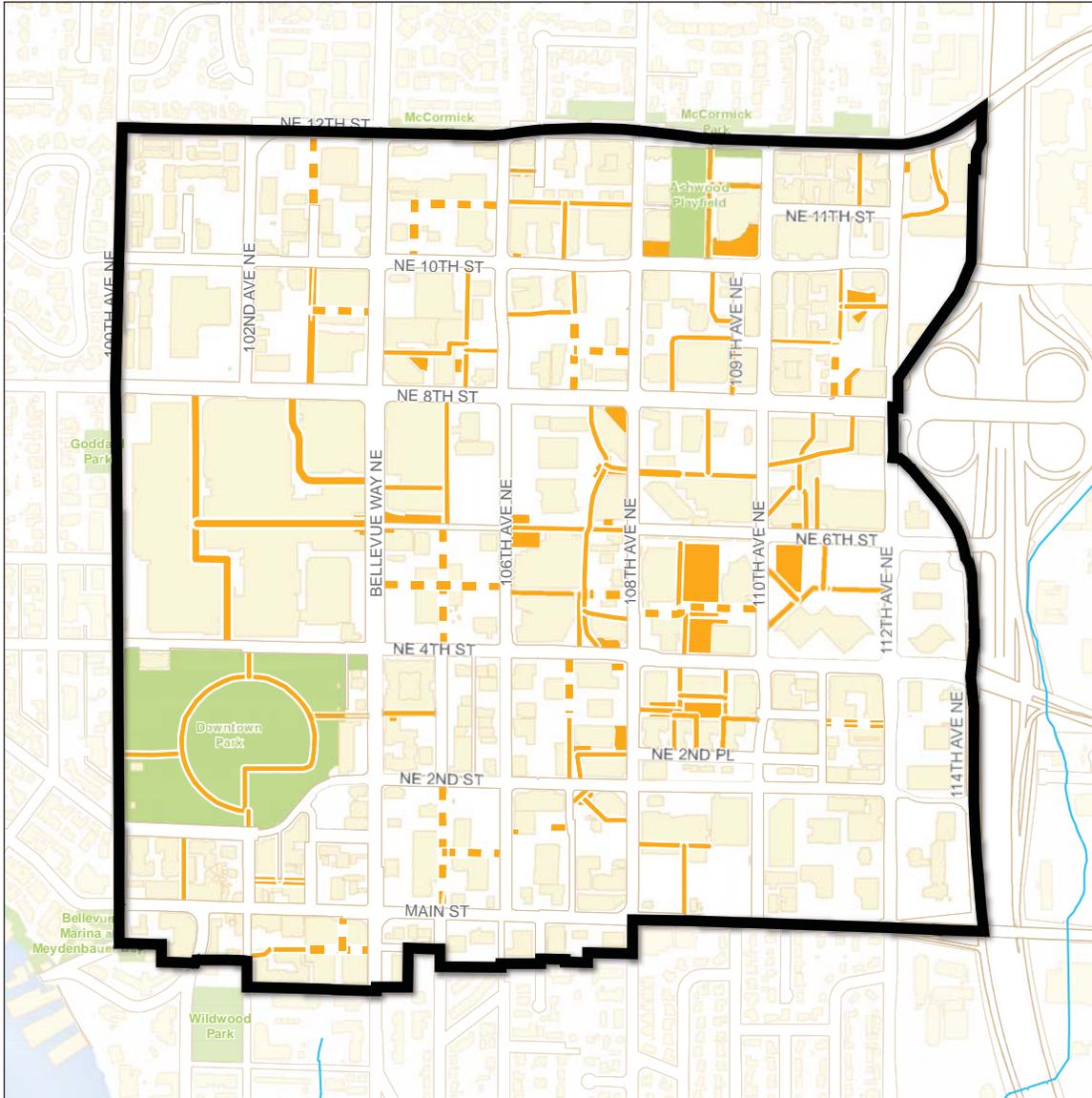
- |                     |                           |
|---------------------|---------------------------|
| Generalized Zoning  | Downtown Boundary         |
| Single Family       | Downtown Design Districts |
| Multi-Family        | A Subdistrict             |
| Office              | B Subdistrict             |
| Commercial          | C Subdistrict             |
| Medical Institution | Downtown Core             |



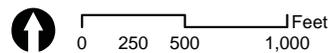


# Downtown Livability

## PUBLIC OPEN SPACE AND EASEMENTS



- Public Plazas
- Mid-block Connections
- Proposed public access\*
- Existing de facto link no formal agreement
- Existing public access
- Downtown Boundary
- City Limits
- City Parks



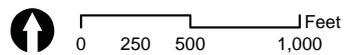


# Downtown Livability

## TOPOGRAPHY



-  Downtown Boundary
-  2ft Contours
-  10ft Contours



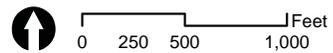


# Downtown Livability

## FIGURE GROUND



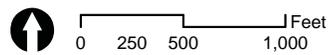
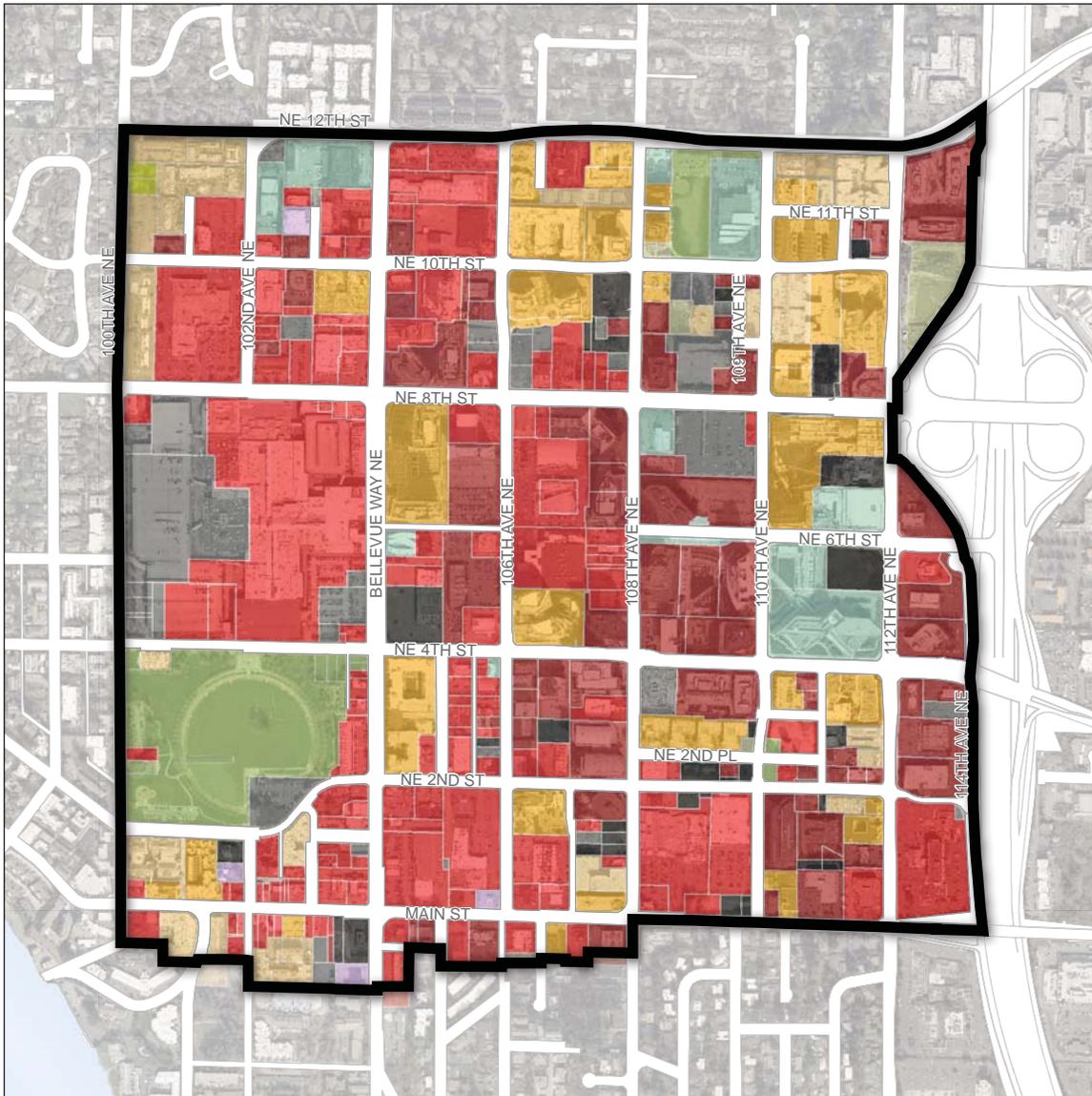
- Downtown Boundary
- Parcels
- City Limits





# Downtown Livability

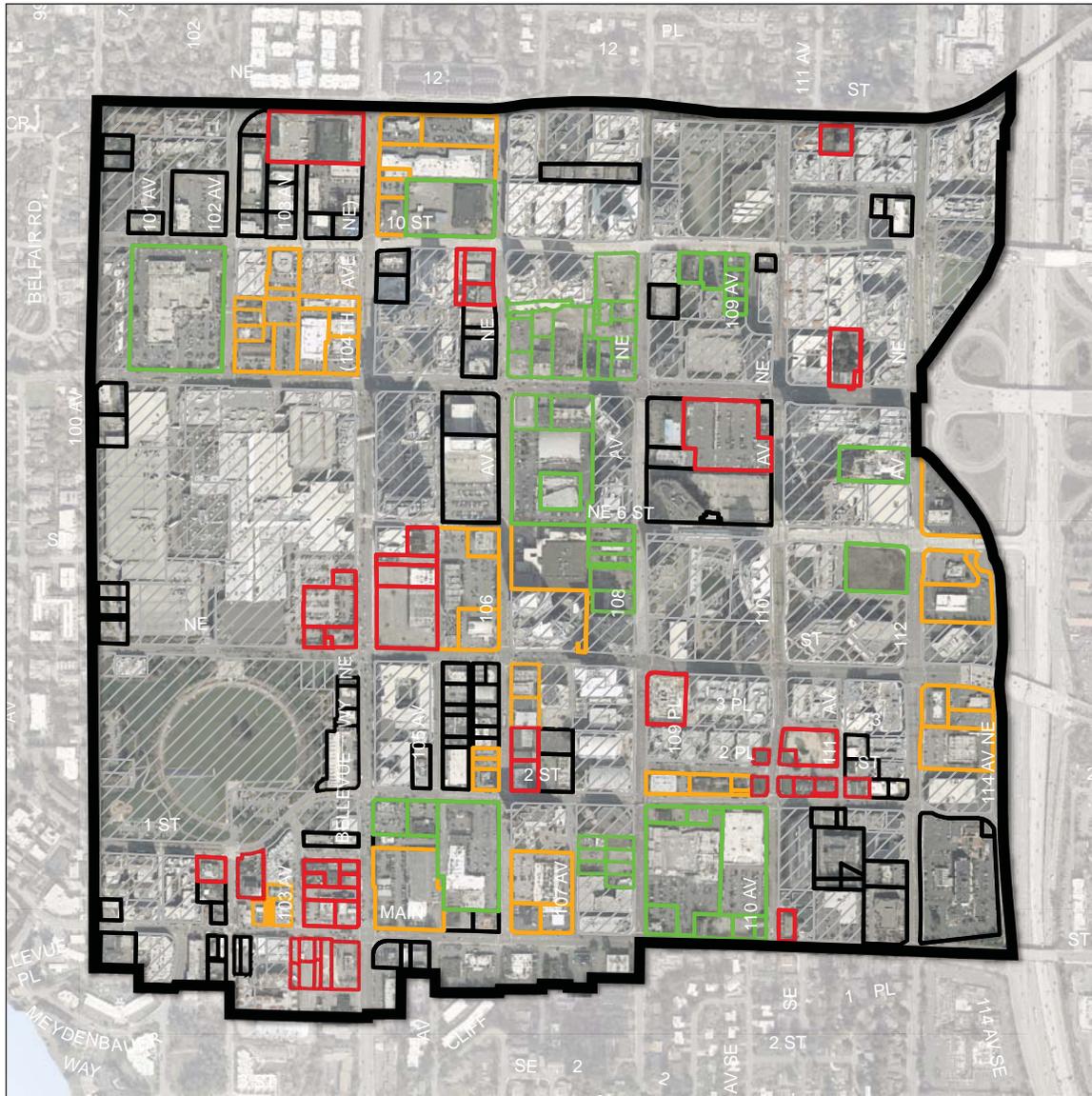
## EXISTING LAND USE



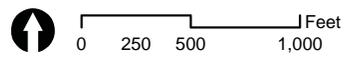


# Downtown Livability

## REDEVELOPMENT POTENTIAL



- Built out in 2012
- Redevelop by 2020
- Redevelop by 2030
- Redevelop by 2040
- Other soft sites
- Downtown Boundary





# Downtown Livability

## OBLIQUE IMAGES





# Downtown Livability

## OBLIQUE IMAGES





# Downtown Livability

## OBLIQUE IMAGES





# Downtown Livability

## OBLIQUE IMAGES







# Downtown Livability

## OBLIQUE IMAGES





# Downtown Livability

## OBLIQUE IMAGES





# Downtown Livability

## OBLIQUE IMAGES



# DOWNTOWN BY THE NUMBERS



## City of Bellevue

- 1953** Year the City of Bellevue was incorporated
- 5,950** Bellevue's population at time of incorporation
- 1960s** Period of rapid growth in Bellevue – with floating bridges across Lake Washington, Bellevue becomes bedroom community to Seattle
- 1979** City develops Downtown Subarea Plan with vision of a pedestrian-friendly, mixed-use urban center
- 130,200** Current population – fifth largest city in the state
- 130,900** Current jobs in the city – major employers include Puget Sound Energy, Symetra Financial, Microsoft, Boeing, T-Mobile USA, Verizon, Expedia, Nordstrom, Overlake Hospital, Group Health Medical Center and Bellevue College
- 2** Bellevue ZIP codes in the top 25 wealthiest in the Puget Sound area (2012 Puget Sound Business Journal Book of Lists)
- 4** Bellevue's rank on Money Magazine's national list of "Best Places to Live" (July 2010)
- 4** Bellevue high schools in Newsweek's top 200 nationally (June 2012)
- 15,512** Fall 2011 enrollment at Bellevue College
- \$2.4b** 2011 taxable retail sales (City of Seattle: \$5.4 billion)

## Downtown Bellevue

- 410** Downtown in acres (1.87% of city's total land area)
- 20** Downtown Park in acres
- 43,339** Current downtown workforce
- 70,300** Forecast workforce by 2030
- 10.6%** 4Q 2012 downtown office vacancy rate (CBRE)
- 10,000** Current downtown residents
- 19,000** Forecast residents by 2030
- 61,400** Downtown's average "daytime population"
- 3,000** Visitors to the Bellevue Regional Library each day
- 1,150** Buses coming into Bellevue Transit Center each weekday

BELLEVUE'S DEMOGRAPHICS	1990 Census	2000 Census	2010 Census*
Median age (years)	35.4	38.2	38.5
Percent of population age 65 or older	10.4%	13.4%	13.9%
Average persons per household	2.41	2.37	2.41
Percent households of one person	26.0%	28.4%	28.1%
Percent of adults (age 25+) with at least a bachelor's degree*	45.7%	54.1%	61%
Percent minority race or ethnicity	13.3%	24.5%	40.8%
Percent of population (age 5+) that speak a language other than English*	13.6%	26.9%	36%

DOWNTOWN DEMOGRAPHICS	2000 Census	2010 Census*
Median age (years)	57.3	34.1
Percent of population age 65 or older	43.5%	15.6%
Average persons per household	1.43	1.53
Percent households of one person	65.8%	59.6%
Percent of adults (age 25+) with at least a bachelor's degree*	45.3%	63%
Percent minority race or ethnicity	19.8%	43.7%
Percent of population (age 5+) that speak a language other than English*	22.7%	41%

\*Source: 2007-2011 American Community Survey 5-year estimates.  
 Note: ACS data are not always directly comparable to 1990 and 2000 census figures.





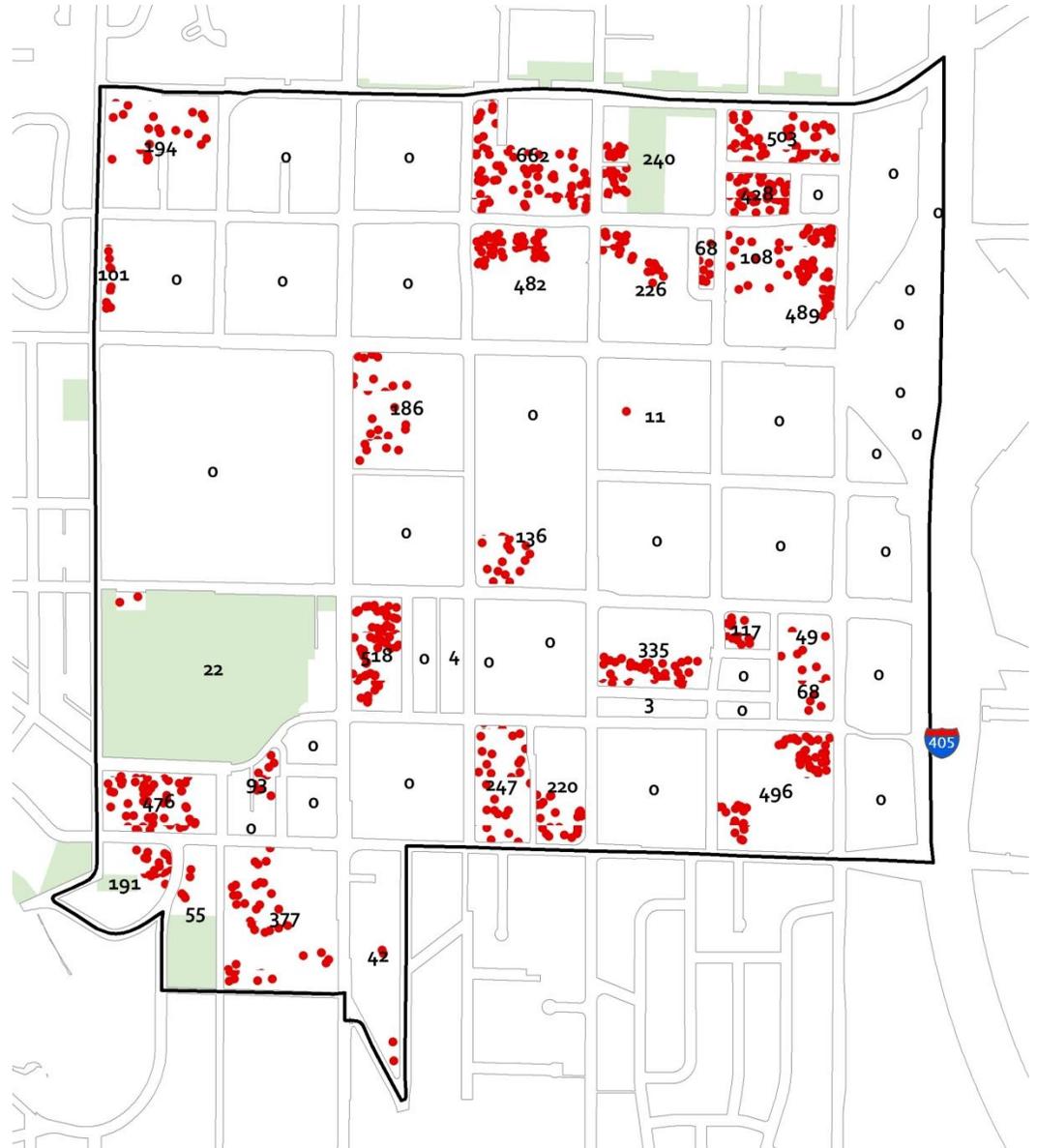
# THE CHANGING FACE OF DOWNTOWN

Demographic Trends 2000 to 2010

# Population Growth and Where People Live

Downtown Bellevue has been one of the fastest growing neighborhoods in Bellevue over the past couple of decades with the number of housing units increasing tenfold and population following suit. In 1990, Downtown Bellevue had 703 units and 1,182 people. Currently in 2012, Downtown has over 7,500 units and over 10,500 in population.

If past population and housing growth is an indication of future trends, the City is tracking its projected forecast well. It is anticipated that Downtown and the Bel-Red subarea will accommodate the majority of Bellevue's projected population increase over the next 20 to 30 years. Population within Downtown Bellevue is projected to nearly double over the next 20 years reaching 19,000 by 2030.



Notes: Census tracts covering Downtown do not align perfectly with Bellevue's Downtown boundary. Population figures given are slightly higher than what exists within the Downtown boundary alone.

# Who Lives in Downtown?

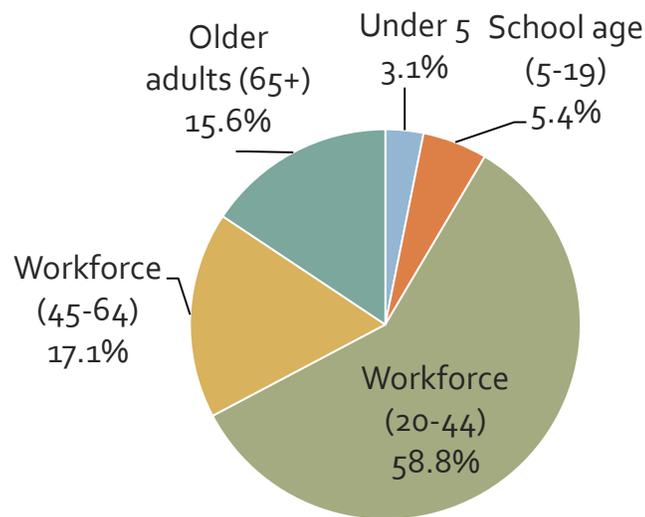
## Age and Gender Distribution

One of the most dramatic demographic shifts in downtown has been the decrease in the median age of residents, which went from 57.3 in 2000 to 34.1 in 2010.

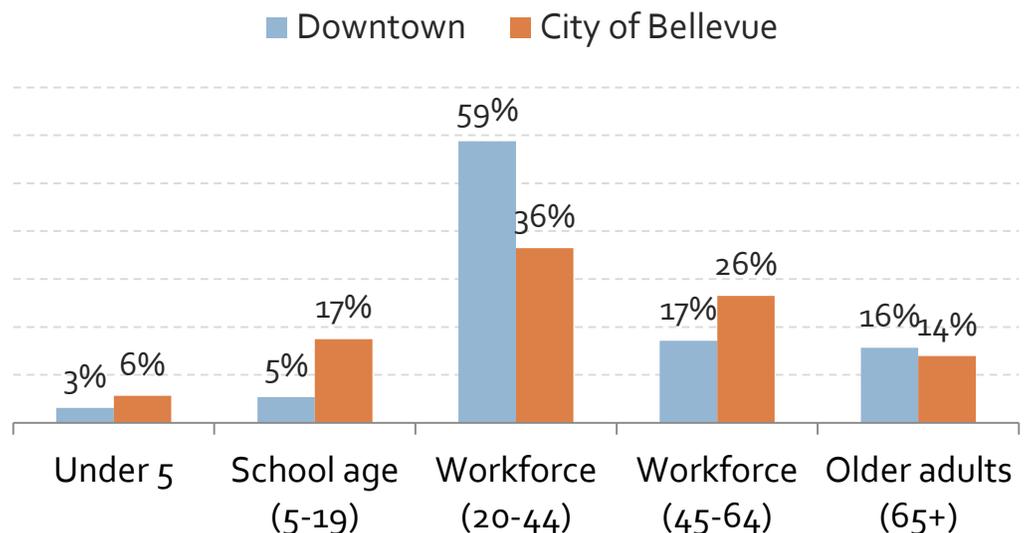
Compared to the City as a whole, Downtown Bellevue has a smaller proportion of children under 18 years of age as well as a smaller proportion of the older workforce population, 45 to 64 years of age. Consequently, it has higher proportions of the young workforce population, 19-44 years of age as well as a higher proportion of older adults, 65 years and older.

Fifty-one percent of Downtown residents were male in 2010 and 49 percent were female, compared to a 50/50 split citywide.

### Downtown Age Distribution



### Downtown vs. City as a Whole



# Who Lives in Downtown?

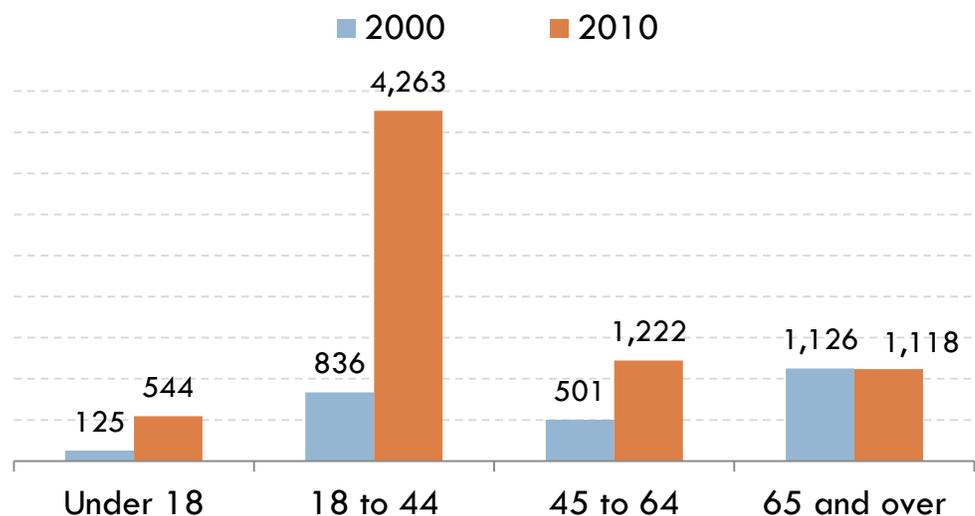
## Age Trends

As shown in the chart on the top, 18 to 44 year olds were the largest and fastest growing age cohort in Downtown between 2000 and 2010, significantly altering the age profile of Downtown, which previously was comprised largely of older adults. By 2010, the number and percentage of 18 to 44 year olds had shot up to comprise nearly 60% of Downtown's population.

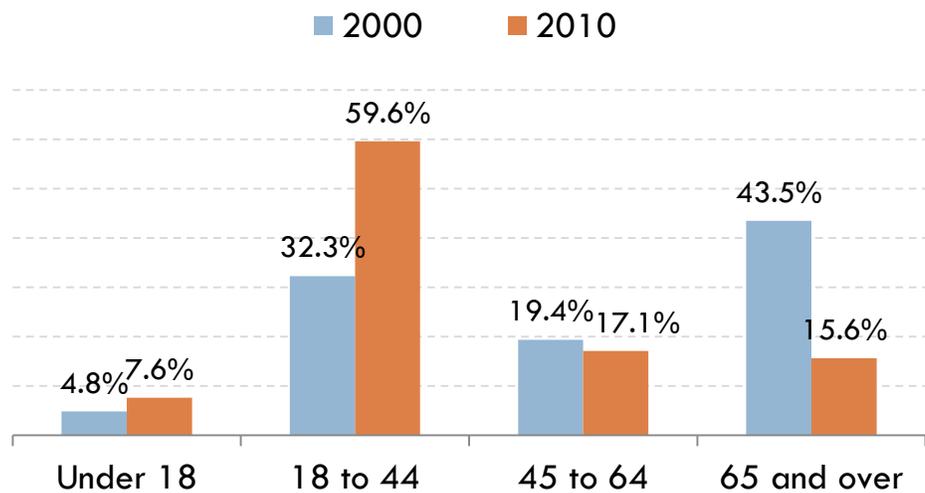
The second fastest growing age cohort in Downtown was children. Downtown witnessed the largest percent increase in the number of children of all of Bellevue's neighborhoods, increasing from 125 in 2000 to 544 in 2010, an increase of over 335 percent.

Median age dropped considerably going from 57.3 in 2000 to 34.1 in 2010, lower than the median age in the City as a whole of 38.1.

### Numeric Distribution



### Percent Distribution



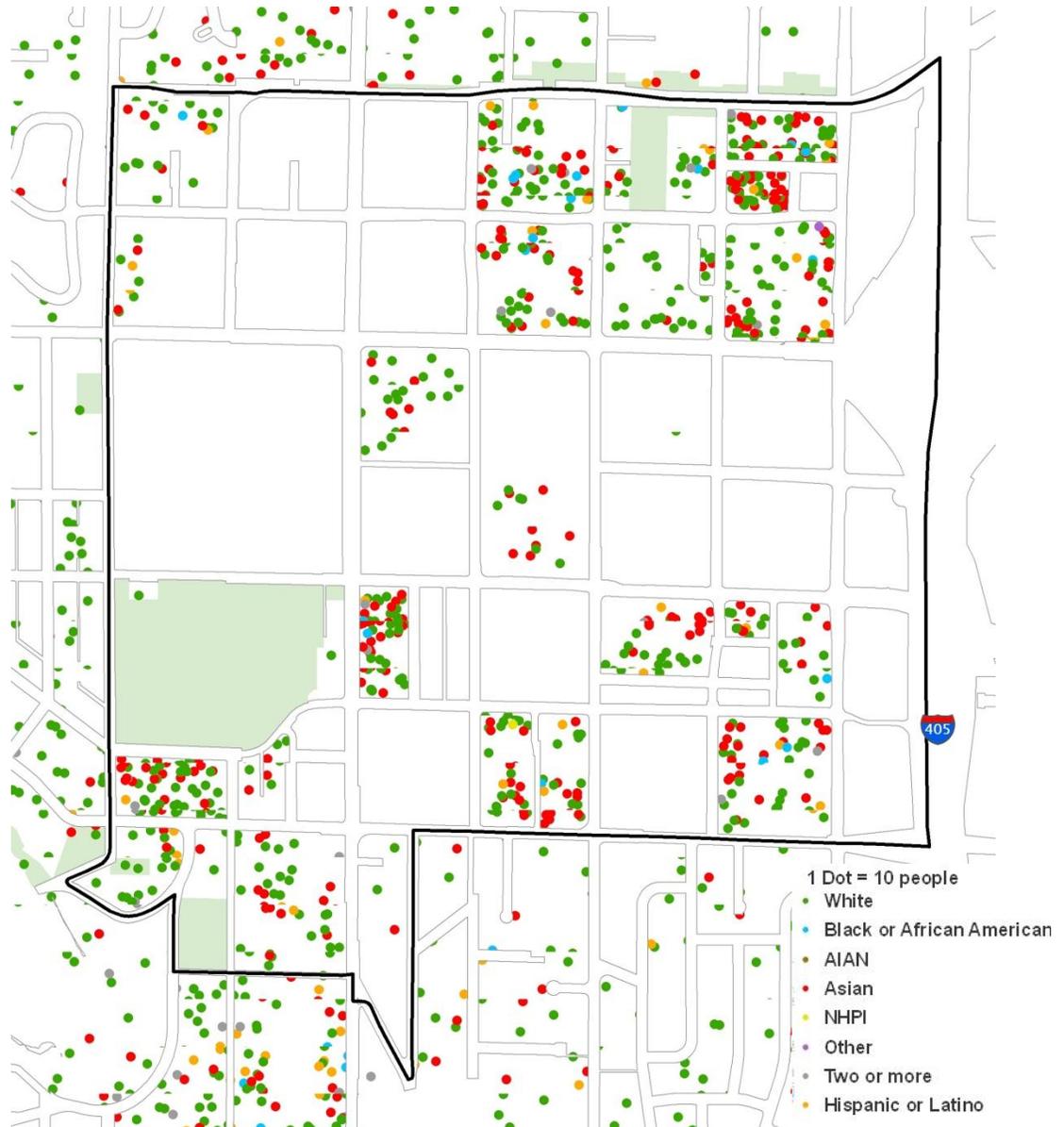
# Who Lives in Downtown?

## Race and Ethnicity

Another dramatic shift that mimicked a citywide trend was the increase in minority populations.

In 2000, less than 20 percent of downtown's residents were of a minority race or ethnicity, whereas in 2010, nearly 44 percent of downtown's residents were of a minority race or ethnicity.

Asian residents comprised the majority of minority populations within downtown representing just over a third of all downtown residents.



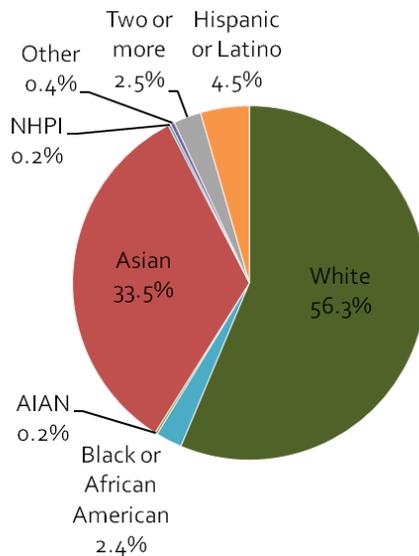
# Who Lives in Downtown?

## Race and Ethnicity

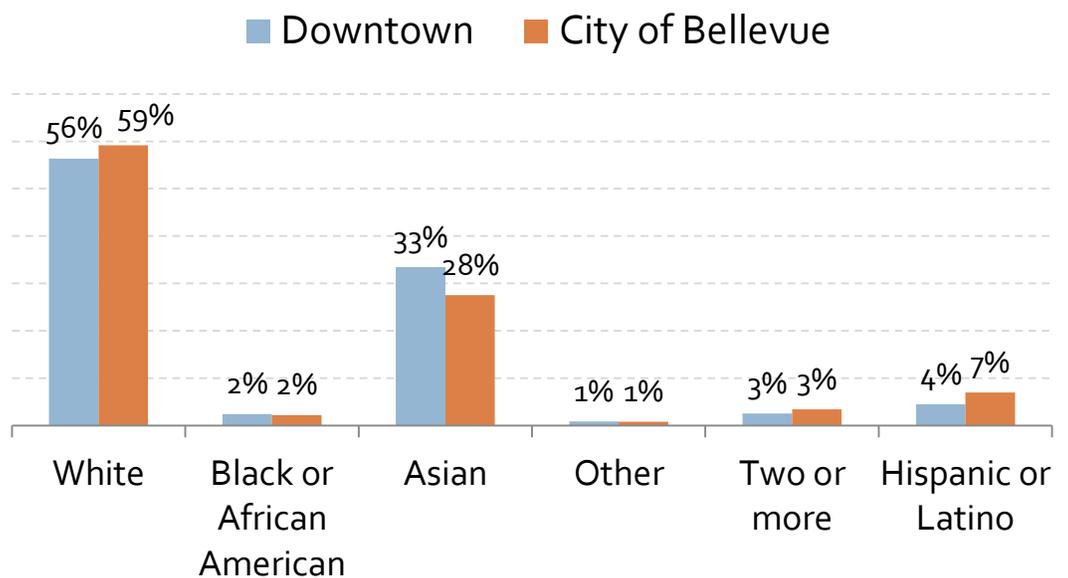
Downtown has a slightly higher proportion of minorities than the City as a whole, with Asians comprising over a third of downtown's residents versus 28 percent in the City as a whole.

Downtown also has a slightly larger proportion of blacks or African Americans than the city as a whole and smaller proportions of Hispanics and whites.

### Downtown Distribution



### Downtown vs. City as a Whole

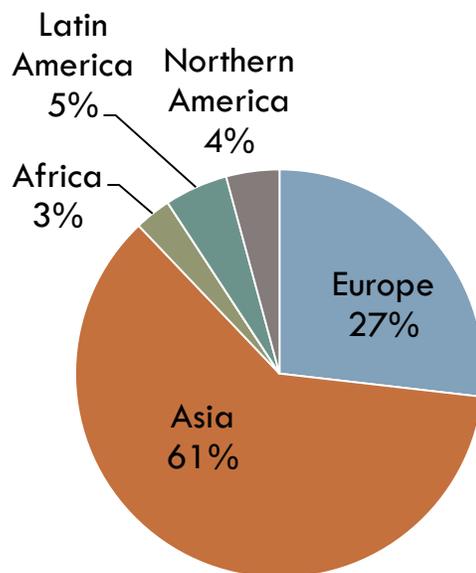


# Who Lives in Downtown?

## Place of Birth and Languages Spoken

About 44 percent of Downtown's residents were born in a foreign country, and about 41 percent speak a language other than English at home.

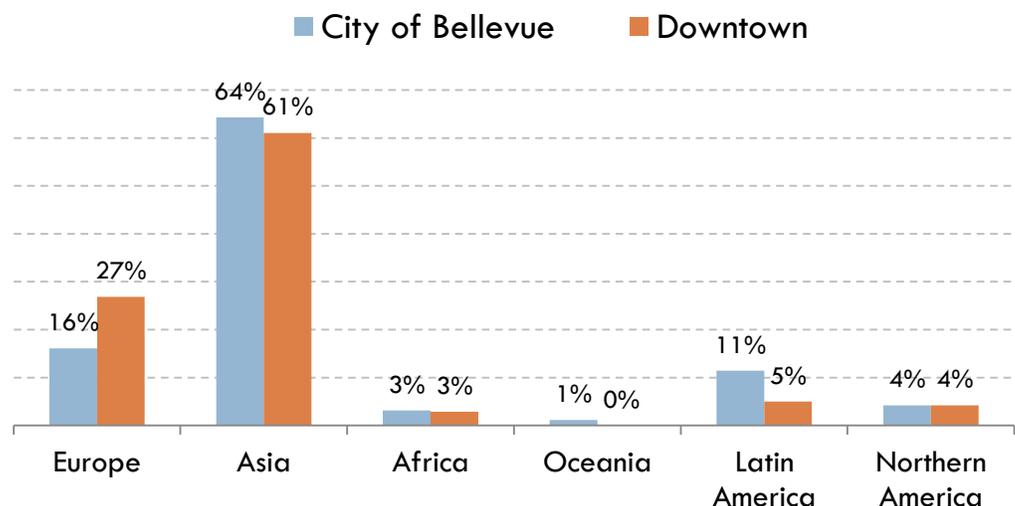
Place of Birth for the foreign born



Top Ten Languages Spoken at Home Other than English

1. Chinese
2. Russian
3. Hindi
4. Spanish
5. Vietnamese
6. Urdu
7. German
8. Japanese
9. Persian
10. Korean

Downtown vs. City as a Whole



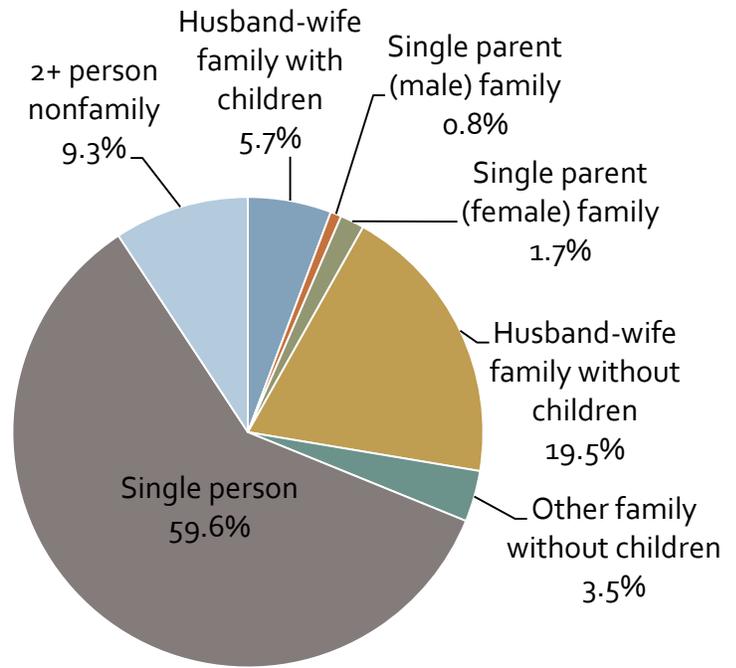
# Who Lives in Downtown?

## Household Types and Sizes

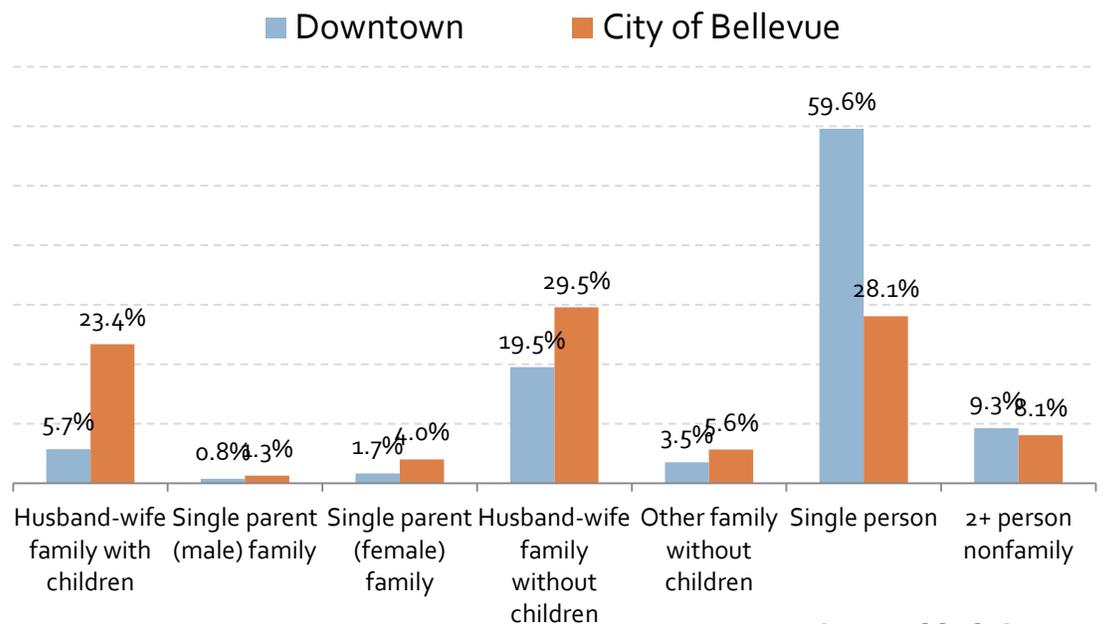
Although downtown remains the neighborhood with the highest percentage of single person households at nearly 60 percent, that figured dropped between 2000 and 2010 with an influx of more married couples both with and without children as well as 2+ person non-family households.

As a result, average household size increased slightly.

Downtown Households



Downtown vs. City as a Whole



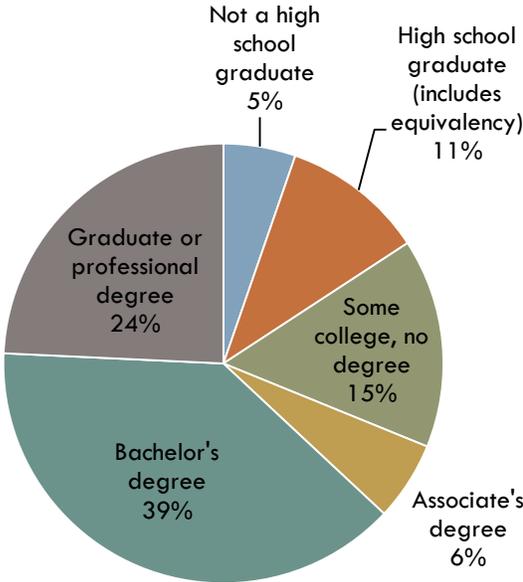
Source: 2010 Census

# Who Lives in Downtown?

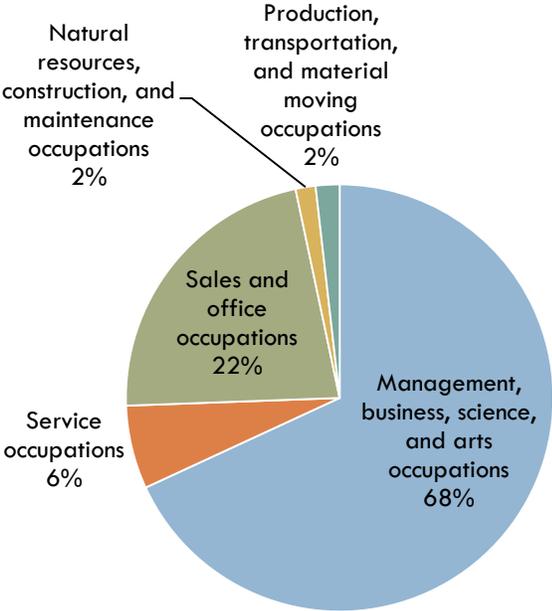
## Educational Attainment and Occupations

Downtown's residents are highly educated, with about 63 percent having a bachelor's degree or higher and over 70 percent working in management, professional and related occupations.

### Educational Attainment



### Occupations



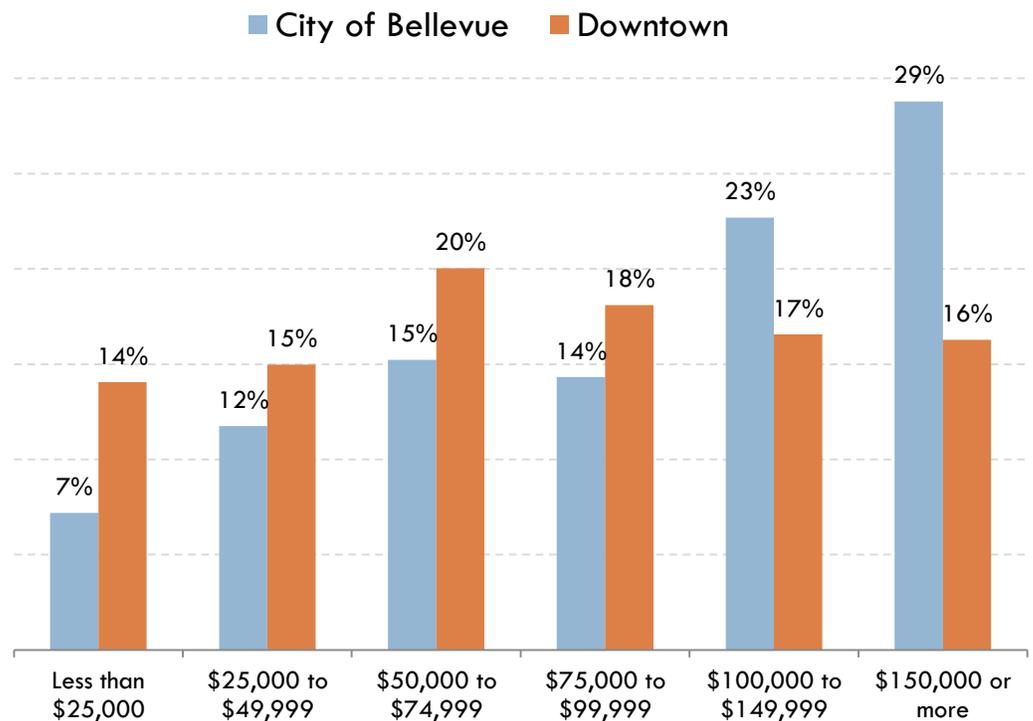
Source: U.S. Census Bureau, 2007-2011 American Community Survey

# Who Lives in Downtown?

## Income and Poverty rate

While Downtown residents had a higher per capita income of \$61,469 compared to \$46,943 citywide, a more even distribution of household income existed in Downtown than citywide.

### Income distribution



### Percent in poverty

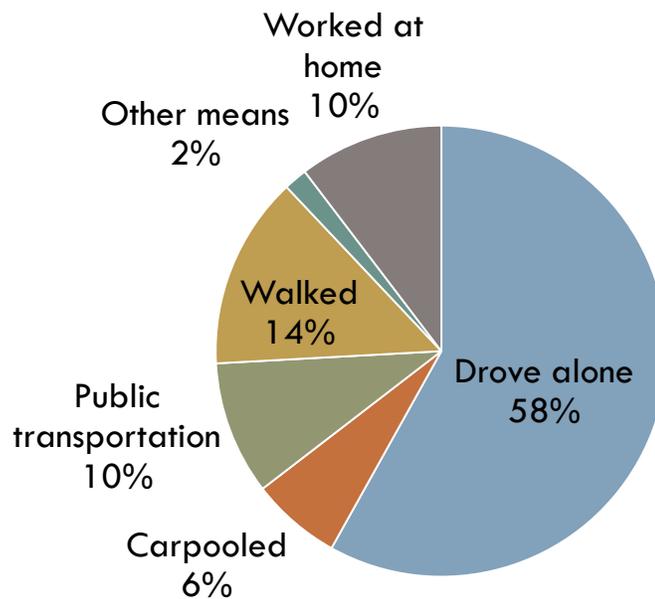
*About 9.3 percent of downtown residents had incomes below the poverty level in 2007-2011 compared to about 6.6 percent citywide.*

# Who Lives in Downtown?

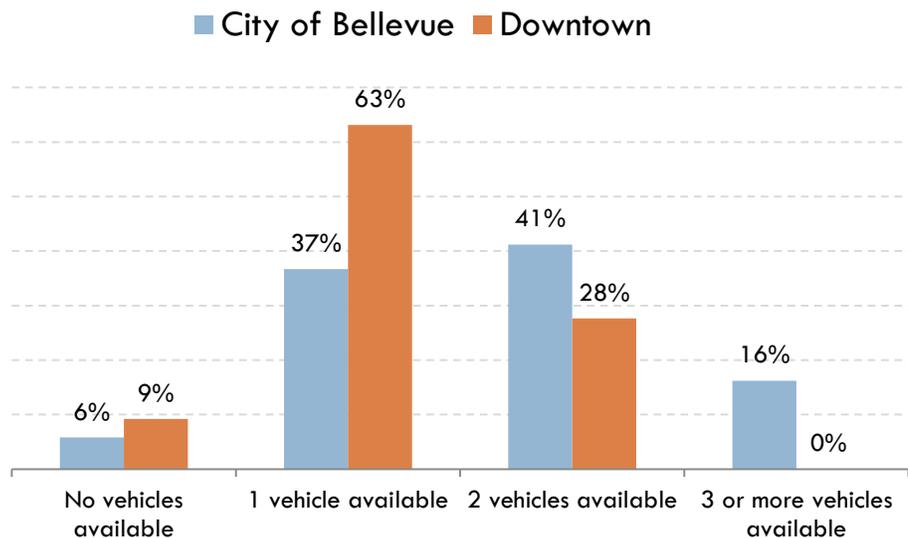
## Travel to work and vehicle ownership

Downtown had the highest percentage of residents who walked to work in Bellevue at 14 percent, and had a higher percentage than average of households who were car-free at nine percent.

### Travel to work



### Vehicle ownership

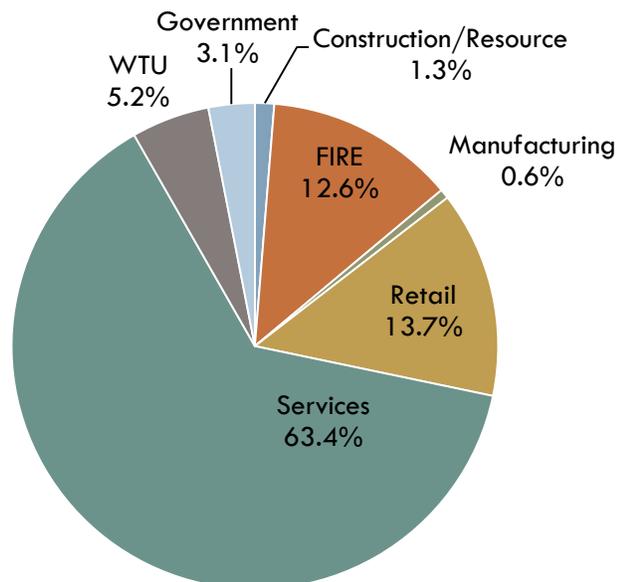


# Downtown Employment

## Downtown Bellevue Total Employment Estimates: 2000-2011

Sector	2000	2001	2002	2003	2004	2005	2006	2007	2008	2009	2010	2011
Construction/Resource	789	835	932	889	713	745	1,008	969	924	788	625	553
FIRE	5,595	6,042	5,380	5,644	5,757	6,229	7,554	7,313	6,884	5,976	5,422	5,475
Manufacturing	583	379	227	224	261	239	264	252	244	235	260	274
Retail	5,628	5,575	5,439	5,363	5,126	5,645	6,322	6,010	5,924	5,293	5,675	5,957
Services	19,482	19,407	17,029	16,625	16,227	17,840	20,262	21,636	22,142	21,807	27,094	27,498
WTU	1,460	1,331	1,349	1,437	1,350	1,347	1,406	1,525	1,736	1,596	1,641	2,250
Government	505	564	545	522	504	505	1,434	1,479	1,375	1,414	1,420	1,332
<b>Total</b>	<b>34,042</b>	<b>34,133</b>	<b>30,901</b>	<b>30,704</b>	<b>29,938</b>	<b>32,550</b>	<b>38,250</b>	<b>39,184</b>	<b>39,229</b>	<b>37,109</b>	<b>42,136</b>	<b>43,339</b>

## Downtown Bellevue 2011 Employment by major sector



Source: Puget Sound Regional Council and COB Planning and Community Development

# Major Downtown Employers

## Headquarters

Eddie Bauer

Paccar

## Technology/Online Companies

Ascentium

Bungie

Drugstore.com /Walgreens

Expedia

Microsoft

Oracle

## Energy and Engineering Firms

CH2M Hill

HDR Engineering

Puget Sound Energy

## Finance and Marketing

Symetra Financial

Waggener Edstrom Worldwide

## Retail, Restaurants and Hotels

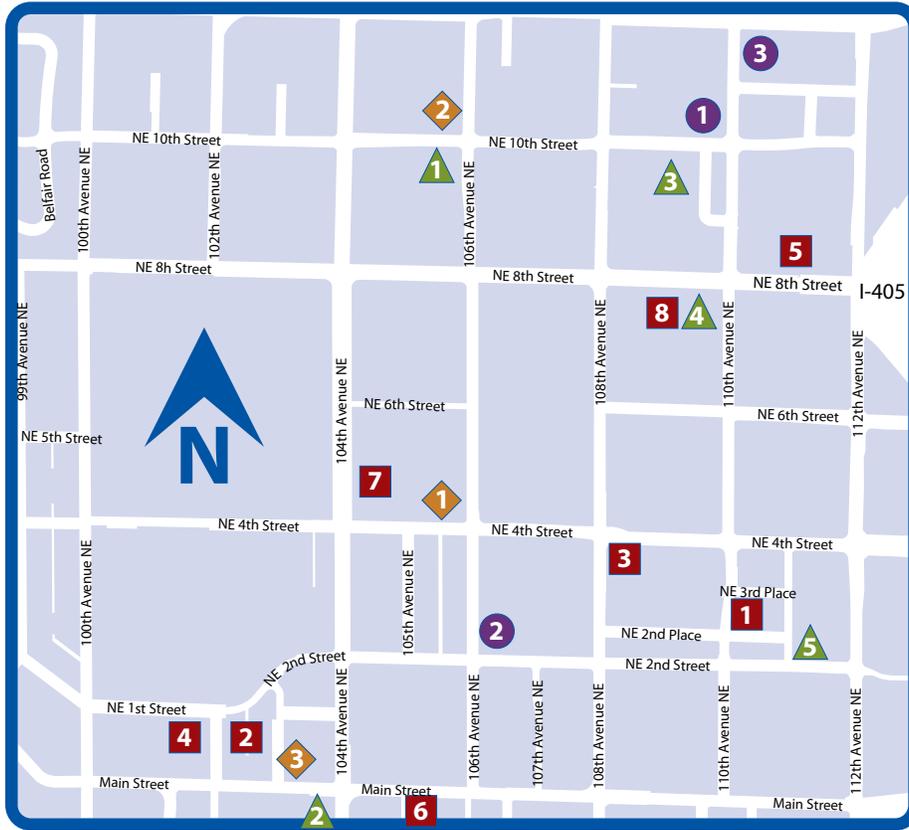
Cheesecake Factory

Hyatt Regency

Macy's

Nordstrom





**In Review Land Use & Building**

- 1** Marriott Hotel (Building Permit)  
200 110th Avenue NE
- 2** Bellevue Park II Apartments (Building Permit)  
10203 NE 1st St.
- 3** The Summit Bldg C / Bentall (Building Permit)  
320 108th Avenue NE
- 4** Bellevue Park Hotel (Building Permit)  
25 102nd Avenue NE
- 5** Alley 111 (Building Permit)  
11011 NE 9th Street
- 6** Alamo Manhattan Main Street (Land Use Approval)  
10505 Main Street
- 7** Lincoln Square Expansion (Building Permit)  
410 Bellevue Way NE
- 8** Bellevue Center (Land Use Approval)  
10833 NE 8th Street

**Under Construction**

- 1** Bellevue Regional Library Parking Garage  
1111 110th Avenue NE
- 2** SOMA Towers Phase I  
288 106th Avenue NE
- 3** Park Metro / Evergreen Pt. Development  
11017 NE 12th Street

**Issued Land Use & Building**

- 1** Tateuchi Center (Land Use and Building Approval)  
855 106th Avenue NE
- 2** Bellevue & Main / SRM (Land Use Approval)  
15 Bellevue Way SE
- 3** Pacific Regent Phase II (Land Use Approval)  
919 109th Avenue NE
- 4** 8th Street Office Bldg/Beacon Capital  
10833 NE 8th Street
- 5** Bellevue Apartments / LIHI (Land Use Approval)  
204 111th Avenue NE

**In the Pipeline**

- 1** 415 Office Building/Schnitzer NW  
415 106th Avenue NE
- 2** Bellevue Cadillac/1001 Properties, LLC  
1001 106th Avenue NE
- 3** 10350 Main Street/VanderHoek Corporation  
10350 Main Street

**In Review**



**1 Marriott Hotel**  
200 110th Avenue NE

- 376 Rooms
- 7.5 K SF Restaurant
- 440 Parking Stalls

Contact: Arne Hall at **425.269.9897**



**3 The Summit Bldg C / Bentall**  
320 108th Avenue NE

- 300 K SF Office
- 652 Parking Stalls
- Target Finish: Unknown

Contact: John Jackson, Bentall at **206.315.3807**



**2 Bellevue Park II Apartments**  
10203 NE 1st Street

- 10,400 SF Retail / Commercial
- 160 Apartment Units
- 4 levels of below-grade parking with 213 stalls.

Contact: Mark Modawell at **425.990.1616**



**4 Bellevue Park Hotel**  
25 102nd Avenue NE

- 9 Stories /107 Residential Units
- 75 Parking Stalls

Contact: Jon Szczesniak at **206.659.7040**

**In Review**



**5 Alley 111**  
11011 NE 9th Street

- 259 residential units
- 76,200 sf retail/restaurant
- Underground parking with 247 spaces

Contact: Brandon Kelly at **206.902.5476**



**7 Lincoln Square Expansion**  
410 Bellevue Way NE

- 120 Room Hotel
- 200 Condos
- 545,000 SF Office
- 392,000 SF Retail

Contact: Dan Meyers at **425.460.5810**



**6 Alamo Manhattan Main Street**  
10505 Main Street

- 260 residential units
- 7,000 sf retail at the ground level
- Underground parking with 319 spaces

Contact: Robert Lamkin at **972.726.9400**



**8 Bellevue Center**  
10833 NE 8th Stret

- 24 stories
- 526,132 SF office
- 1,258 parking spaces in underground parking
- 8,500 SF restaurant and 2,500 retail
- Nearly 2 acres landscaped plaza

# *Downtown Bellevue*

## Under Construction



### 1 **Bellevue Regional Library Parking Garage**

1111 110th Avenue NE

- Library Parking Structure
- 3 levels (one level below grade)
- 250 parking stalls
- 108,000 SF
- Target Start Fall 2010

Contact: Sally Roth at 206.523.6150



### 3 **Park Metro / Evergreen Point Development**

11017 NE 12th Street

- 75 Units
- 111 Parking Stalls
- 35,000 SF Pedestrian Oriented Frontage
- Target Start: Unknown

Contact: Austin Kovach at 206.617.2899



### 2 **SOMA Towers Phase I**

200 106th Avenue NE

- 2 Towers, 21 and 18 Stories
- 266 Residential Units, including 34 affordable units
- 26,000 SF Retail
- 5 Levels of Parking for 395 vehicles

Contact: Linda Abe, Su Development at 425.453.8886 x313

**Issued Design Review**



**1 Tateuchi Center**  
855 106th Avenue NE

- 2,047 Seat Performance Hall
- 3 Levels of Underground Parking
- Target Start: Summer 2012

Contact: Scott Douglas / MG2 Architects at 425.463.1291



**3 Pacific Regent Phase II**  
919 109th Avenue NE

- 22 Story Senior Housing/Congregate Care Community
- 168 Units
- 206 Parking Stalls
- Target Start: Unknown

Contact: Sunrise Dvmt at 650.938.2249



**2 Bellevue & Main / SRM**  
15 Bellevue Way SE

- 33,000 SF Retail/Commercial
- 262 Residential Units
- 475 Parking Stalls

Contact: Andy Loos, SRM at 206.352.7873



**4 8th Street Office Bldg/Beacon Capitol**  
10833 NE 8th Street

- 32 story office building
- 680,000 SF Office and 10,800 SF retail/commercial at the ground level
- 6 levels of below-grade parking with approx. 1,700 stalls

Contact: Arne Hall, ALH LLC at 425.269.9899

**Issued Design Review**



**5 Bellevue Apartments / LIHI**  
204 111th Avenue NE

- 5 Stories
- 57 Apartment Units
- 23 Parking Stalls

**Contact: Karen Anderson-Bittenbender, LIHI, at 206.383.2983**



**In the Pipeline**

**1 415 Office Building**

415 106th Avenue NE

Contact: Greg Kletzly, Schnitzer NW at **206.626.3700**

**2 Bellevue Cadillac**

Retail/Residential

1001 106th Avenue NE

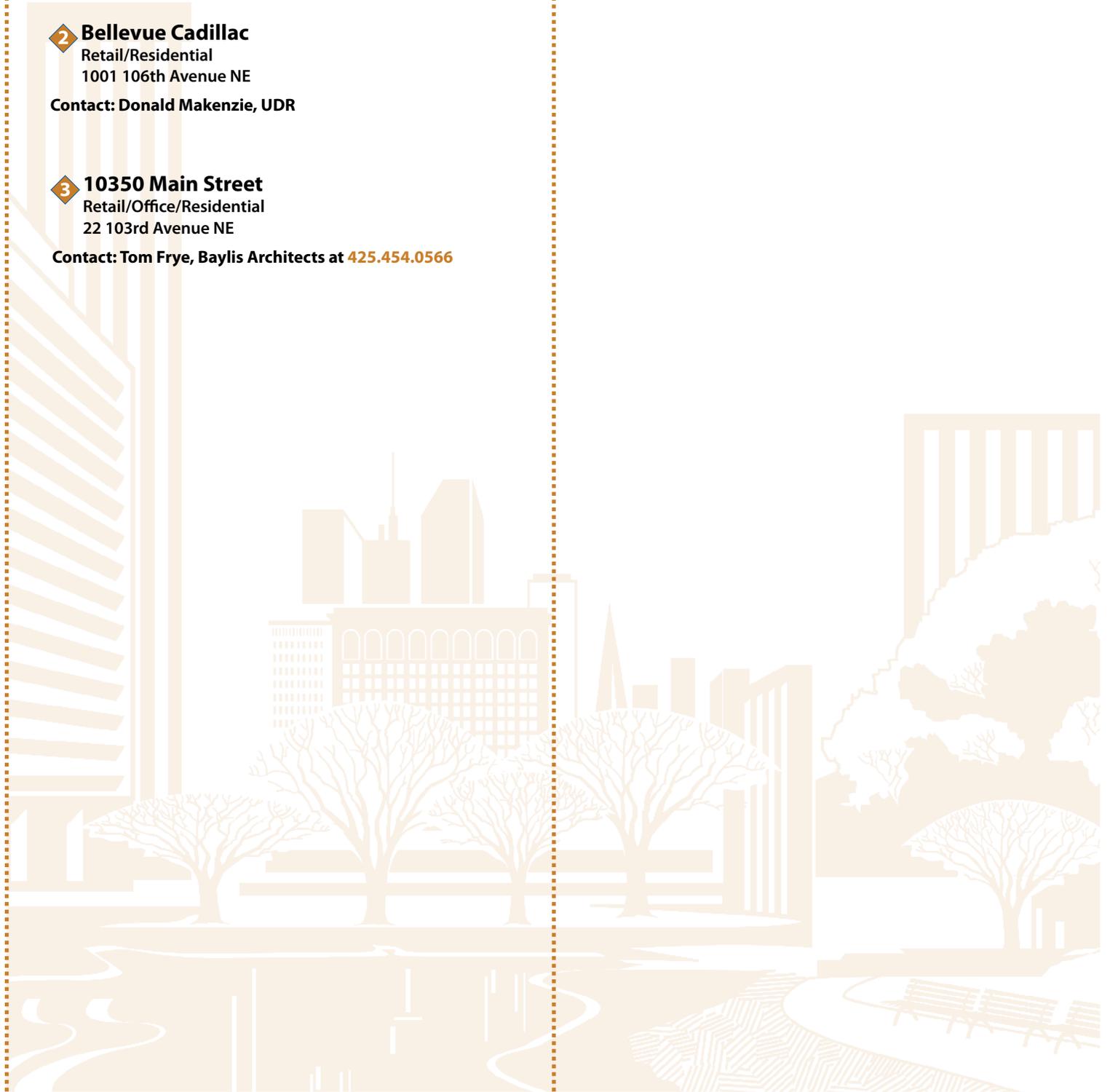
Contact: Donald Makenzie, UDR

**3 10350 Main Street**

Retail/Office/Residential

22 103rd Avenue NE

Contact: Tom Frye, Baylis Architects at **425.454.0566**

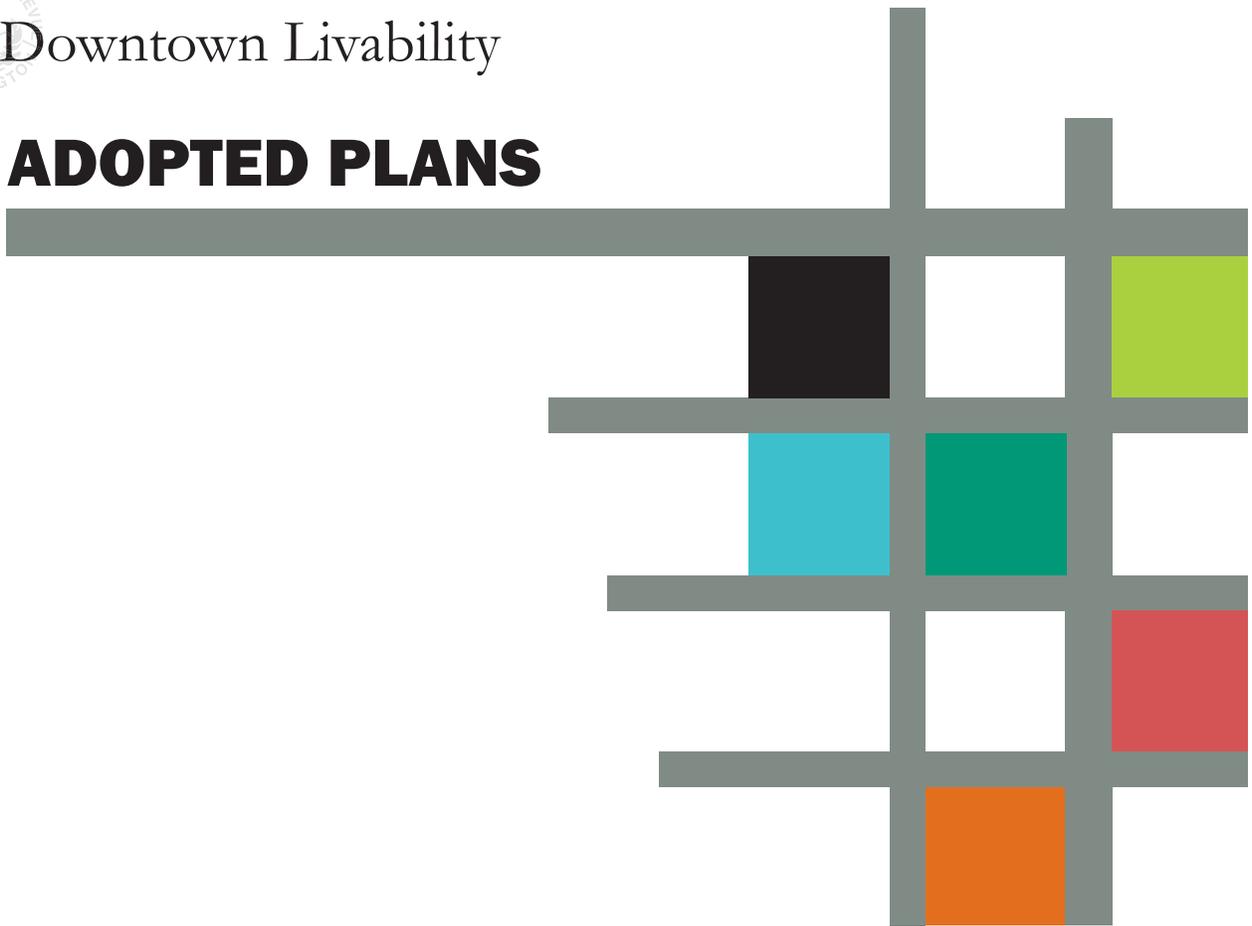






# Downtown Livability

## **ADOPTED PLANS**





# Downtown Subarea Plan

## GOAL:

### The Great Place Strategy

To remain competitive in the next generation, Downtown Bellevue must be viable, livable, memorable, and accessible. It must become the symbolic as well as functional heart of the Eastside Region through the continued location of cultural, entertainment, residential, and regional uses located in distinct, mixed-use neighborhoods connected by a variety of unique public places and great public infrastructure.

## OVERVIEW

### A Community's Vision

Between 2001 and 2003, the city worked with Bellevue residents and business interests to strengthen the community's vision for Downtown while focusing on planning issues that face a maturing urban center with many of the basic elements already in place. This second generation of planning addressed solutions for increased transportation demand as well as the character of future Downtown development over the next twenty years.



*New dining, business and gathering places are helping to create a lively, vibrant Downtown.*

This document provides the policy framework and list of associated projects to support development of Downtown Bellevue as the primary urban center of the Eastside, consistent with countywide and regional plans. This Subarea Plan is implemented through regulations that guide the scale and character of new development, targeted public investments such as roadway, transit and pedestrian improvements, new parks and public buildings, as well as private-sector investments such as entertainment and cultural attractions that continue to further the vision for Downtown.

## Evolution of Downtown Bellevue

Downtown Bellevue has been dramatically transformed over the past century. A ferry landing at Meydenbauer Bay just west of the present Downtown boundary was the early impetus for commercial development of the area. By the early 1900s, a small amount of retail and other services had sprouted on Main Street near the Bay, in the area known today as Old Bellevue.

Significant development in Downtown Bellevue awaited completion of the first bridge across Lake Washington in 1940. This growth was stimulated by the removal of the bridge toll in 1949. When Bellevue was incorporated in 1953, Downtown was a cluster of structures along Main Street and Bellevue Square was a modest strip mall. The city's first Planning Commission embraced the idea of planned Downtown growth done in an orderly and efficient manner.

Downtown experienced rapid growth during the 1960s. By the mid-1970s, the area had emerged as a major business center, though much of the development was suburban in nature with acres of surface parking. During this period, the city, jointly with Downtown business interests, launched a series of planning studies focusing on the future of Downtown Bellevue. This was partially in response to a major threat of a competing regional mall planned for the Redmond area. These studies resulted in a major new vision for the area, adopted by the city as the Central Business District Subarea Plan in 1979. It called for Downtown to be the financial and business hub of the community, and the place to concentrate regional retail, major office, residential, hotel and institutional uses.



*Photograph of downtown area just prior to incorporation in 1953.*

The 1980s saw an unprecedented level of office construction in King County, and Downtown Bellevue was in a position to receive a major amount of the Eastside's growth. Numerous high-rise office towers were built in Downtown Bellevue, shaping the skyline as well as nearly doubling employment. This period also saw the loss of some of the commercial establishments that functioned as neighborhood retail to the surrounding residences as Downtown became a much more urban place.

During the 1990s, Downtown added signature public open spaces, including the 20-acre Downtown Park and centrally located Compass Plaza. A high-rise office



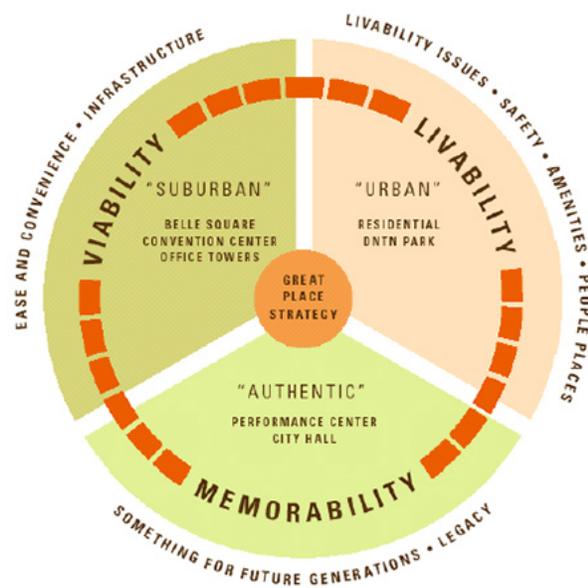
*Downtown Park is a regional attraction.*

presence emerged around 108th Avenue NE. Major civic projects were constructed, including King County’s flagship regional library, the Meydenbauer Convention Center & Theater, and a new building for the Bellevue Arts Museum. Private and public investment has

helped to shape the NE 6th Street pedestrian corridor. Downtown housing began to really blossom into a major Downtown land use beginning in the late 1990s, with new multifamily developments springing up throughout the subarea. Regional retail continued to expand with new and exciting uses that are continuing to enliven the Downtown streetscape. This Subarea Plan now continues with the evolution of the 1979 Plan, as the vision for Downtown Bellevue is strengthened for the next century based on lessons learned and the many successes that have already taken place.

## Viability, Livability, Memorability

Downtowns evolve through a dynamic process as shown by the graphic below. This is a non-linear progression in which cities are relatively more viable, livable, or memorable during different stages of their growth. It is a constantly changing response to an array of influences. As Downtown Bellevue enters the 21st century, it sits on the threshold between viable and livable in its evolution.



Viability is about quantity; about creating critical mass. Viability is achieved through large-scale, single-action projects and factors such as freeway interchanges, regional shopping, high-rise zoning, and the addition of jobs.

Livability is about quality; about weaving an urban fabric rich in resources and quality of life. Livable cities provide welcoming places to eat and sources of entertainment. Livable cities develop parks and open space. Truly great cities are

*Downtowns evolve through a non-linear process.*

also memorable. Memorable cities impart an unforgettable experience from having visited there. Memorable cities have strong, clear identities.

Downtown Bellevue should work to make progress on all three of these dimensions. But at today's point in Downtown Bellevue's evolution, it is important to focus extra attention on graduating to a higher level of livability.

## **Regional Role**

Downtown Bellevue is the hub of activity for the City of Bellevue as well as the greater Eastside, providing office and residential concentrations as well as retail and cultural attractions. Extending from NE 12th Street south to the Main Street area and from 100th Avenue NE to Interstate 405, Downtown covers nearly 410 acres, or two percent of the city's land area.

As of 2004, there were 35,000 workers and over 4,000 residents housed in Downtown Bellevue. The 2020 forecast is for an additional 28,000 jobs and 10,000 residents, or roughly three-quarters of the city's future employment and residential growth. This focus of future development within Downtown takes pressure off existing residential areas within Bellevue.

Planned growth in Downtown Bellevue is an important part of the Central Puget Sound's growth management strategy. The Puget Sound Regional Council's *Vision 2020* and King County's Countywide Planning Policies identify Downtown Bellevue as an urban center. Downtown Bellevue is a place where growth should be focused if the region is to further growth management goals, such as reducing sprawl and retaining open space.

The 2004 update of this Plan coincides with several major regional transportation enhancement projects, such as the I-405 and I-90 Corridor Programs, the Trans-Lake Washington Project, and Sound Transit's Sound Move Plan, all of which will have major benefits for and impacts on Downtown Bellevue. The relationship between these 20 to 30 year transportation planning efforts have been considered in the development of this Plan due to the important role of regional accessibility in a major commercial employment center like Downtown Bellevue.

## **Major Focal Points of the Subarea Plan**

The future success of Downtown Bellevue rests on first being a livable place and slowly evolving into a truly memorable place. This will be accomplished through a series of coordinated urban design and transportation initiatives.

Downtown Bellevue is beginning to take on the features of a true city center; an exciting place to work, shop, visit, or call home. The focus of urban design within

this Plan is to create a series of distinct, mixed-use neighborhoods tied together by a series of “signature streets” and great public infrastructure. Each district will be unique and have the urban amenities to support an active, fulfilling lifestyle and make it a great urban place.

Transportation planning in Bellevue requires multiple approaches to be undertaken simultaneously. The transportation vision is to provide regional access to Downtown via regional roadway and transit systems; mobility between Downtown and other parts of Bellevue; and safe circulation within Downtown for motorized and non-motorized modes as population and employment increase over time.



*Downtown Bellevue will continue to be a place to focus job and housing growth.*

## General

### GOAL:

**To become the symbolic and functional heart of the Eastside Region through the continued location of cultural, entertainment, residential, and regional uses.**

The vision for Downtown Bellevue is a dense, mixed-use urban center that has a high pedestrian orientation and range of complementary land uses. These policies generally reinforce that vision while providing direction covering the entire Downtown Subarea.

### POLICIES

#### Land Use

**POLICY S-DT-1.** Emphasis shall be placed on Downtown livability, with provisions made for the needs, activities, and interests of Downtown residents, employees, shoppers, and visitors.



*The city encourages combining residential and retail in the same building to achieve housing, urban design, and transportation goals.*

**POLICY S-DT-2.** Encourage a variety of land uses to occur in mixed-use buildings or complexes where appropriate.

**POLICY S-DT-3.** Develop Downtown as an aesthetically attractive area.

**POLICY S-DT-4.** The highest intensity development shall be located in the core of Downtown, with diminishing intensities towards the edges of Downtown (see Figure A for delineation of Core Area and Perimeter Area).

**POLICY S-DT-5.** Organize Downtown to provide complementary functional relationships between various land uses.

**POLICY S-DT-6.** Develop Downtown as the Eastside's most concentrated and diverse regional retail district.

**POLICY S-DT-7.** Encourage Downtown to continue to serve surrounding residential areas as a neighborhood retail district.

**POLICY S-DT-8.** Locate major office development in the Downtown core in order to complement retail activities and facilitate public transportation (see Figure A).

**POLICY S-DT-9.** Provide bonus incentives (related to permitted intensity, height, etc.) for private developments to accomplish the public objectives outlined in this Plan.

**POLICY S-DT-10.** Require design review to ensure high quality, aesthetically pleasing Downtown development.

**POLICY S-DT-11.**  
Encourage the development of major civic, convention, and cultural uses within Downtown.

**POLICY S-DT-12.** Expand the convention center as a resource for convention and community uses, and explore opportunities for complementary uses.



*The annual Bellevue Arts and Crafts Fair draws more than 300,000 people to Downtown each summer.*

**POLICY S-DT-13.** Encourage private participation in development of Downtown community facilities.

**POLICY S-DT-14.** Encourage visual and performing arts organizations to locate Downtown.

**POLICY S-DT-15.** Encourage the assembly of land or coordination of development as appropriate to facilitate a quality built environment.

**POLICY S-DT-16.** Restrict the location of drive-in and drive-through activities within the Downtown Subarea.

### **Economics**

**POLICY S-DT-17.** Promote economic development strategies that further Downtown Bellevue as an Urban Center, consistent with regional plans.

**POLICY S-DT-18.** Strengthen Downtown's role as the Eastside's major business and commercial center and as an important revenue source for the City of Bellevue.

**POLICY S-DT-19.** Maintain an attractive economic environment to encourage private investment through stable tax rates and a predictable regulatory framework.

### **Historic Resources**

**POLICY S-DT-20.** Recognize the importance of Downtown's historic resources as identified in the Bellevue Historic and Cultural Resources Survey.

**POLICY S-DT-21.** Work with local heritage groups to:

1. Collect, preserve, interpret, and exhibit items that document the history of Downtown Bellevue;
2. Use plaques and interpretive markers to identify existing and past sites of historic and cultural importance;
3. Develop a contingency plan and prioritization for Downtown's historic resources, which may include voluntary relocation of significant historic structures to Bellevue parks property.

**POLICY S-DT-22.** Provide voluntary incentives for the replication or protection of historic façades or other significant design features when redevelopment occurs.

**POLICY S-DT-23.** Develop a voluntary mechanism to allow air rights to be transferred from historic properties to other Downtown property.

## Residential Development

**POLICY S-DT-24.** Provide density incentives to encourage urban residential development throughout Downtown.



*Density incentives encourage residential development throughout Downtown.*

**POLICY S-DT-25.** Provide for a range of Downtown urban residential types and densities.

**POLICY S-DT-26.**

Encourage residential uses to occur in mixed-use structures or complexes.

**POLICY S-DT-27.** Explore the use of tax incentives to encourage additional work-force housing within the Downtown Subarea.



*Residential uses are encouraged as part of mixed-use structures.*

**POLICY S-DT-28.** Work with regional housing organizations such as A Regional Coalition of Housing (ARCH) and the Downtown Action to Save Housing (DASH) to develop additional Downtown residential projects.

## Public Safety

As Downtown densities and uses increase over time, it is important to maintain adequate response times for public safety functions. This may be delivered in a number of ways and will be further explored by the city when the need arises.

**POLICY S-DT-29.** Provide adequate fire and life safety services for the Downtown Subarea as population and employment increase over time.

## Utilities

**POLICY S-DT-30.** Require undergrounding of all utility distribution lines.

**POLICY S-DT-31.** Where possible, combine utility and transportation rights-of-way into common corridors.

**POLICY S-DT-32.** Require developer funding for extensions of collection and distribution lines.

**POLICY S-DT-33.** Minimize potential impacts to pedestrians caused by utility equipment, such as cabinets, within the sidewalk where possible.

**POLICY S-DT-34.** Utility installations visible in the public right-of-way should be consistent with Downtown design guidelines.

## Urban Design

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### **GOAL:**

**To develop a functional and aesthetically pleasing Downtown which creates a livable and highly pedestrian-oriented urban environment that is compatible with adjacent neighborhoods.**

Downtown Bellevue has been evolving from the commercial center of a suburban bedroom community into the multi-faceted heart of the Eastside region. To continue this evolution as a great urban place and to remain economically healthy over the coming growth cycles, a number of strategies are needed to take the next step in becoming a livable and memorable place. These strategies will nurture a sense of place in a series of Downtown neighborhoods. They will recognize the importance of the pedestrian, and establish a high level of significance on the design of buildings and public spaces.

## **POLICIES**

### **General Design and Function**

Design and function in Downtown Bellevue is guided by policy direction in concert with development standards and design guidelines. These all seek to ensure an aesthetically pleasing urban environment with a high level of pedestrian orientation.

**POLICY S-DT-35.** Create a pedestrian environment with a sense of activity, enclosure, and protection.

**POLICY S-DT-36.** Utilize development standards for building bulk, heights, setbacks, landscaping requirements, stepbacks, floor area ratios, open space requirements, and development incentives.

**POLICY S-DT-37.** Link building intensity to design guidelines relating to building appearance, amenities, pedestrian orientation and connections, impact on adjacent properties, and maintenance of view corridors. These guidelines will seek to enhance the appearance, image, and design character of the Downtown.



*The pedestrian corridor flows into inviting open public spaces.*

**POLICY S-DT-38.** Minimize the adverse impact of Downtown development on residential neighborhoods with consideration of through-traffic, views, scale, and land use relationships.

### Hierarchy of Streets

The streets in Downtown Bellevue may be placed in a hierarchy based on their connectivity, cross-section, and current and future volume. As the graphic below shows, there are a range of street types in Downtown Bellevue. The pedestrian-bias streets of NE 6th and the portion of Main Street in Old Bellevue are unique in Downtown Bellevue. The NE 6th Street



*Downtown Bellevue has a clear hierarchy of streets.*

Pedestrian Corridor shifts from west to east from a limited auto-access street, to no auto access, to a transit mall. Old Bellevue has a two-lane Main Street with on-street parking, small retail shops, and high levels of pedestrian activity that provide a

signature look and feel. At the other end of the spectrum are auto-bias streets. They will provide pleasant pedestrian environments, but are intended for current high vehicle volumes, and will be required to serve similar and increasing volumes in the future. Bellevue Way, NE 4th, NE 8th, and 112th Ave NE are examples. The streets in between auto-bias and pedestrian-bias are said to be neutral. They will evolve over time to serve both pedestrians and automobiles in a manner that reinforces the adjacent land uses and travel demands of future development.

**POLICY S-DT-39.** Utilize a hierarchy of streets to guide right-of-way use in a manner that will promote a safe, attractive environment for both motorized and non-motorized users.



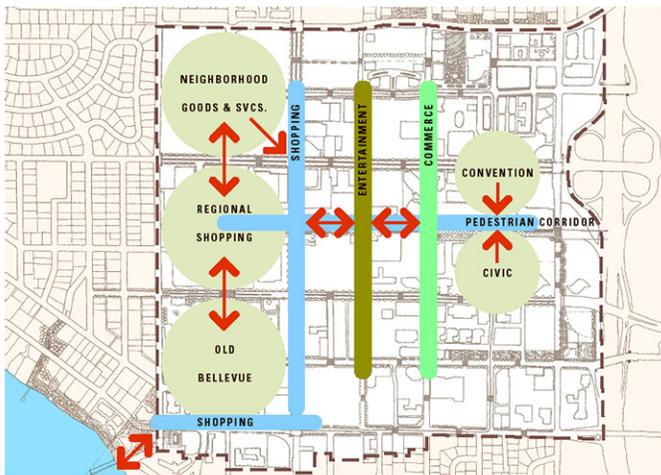
All roadways should promote a safe and attractive environment for motorized and non-motorized users.

**POLICY S-DT-40.** Enhance the appearance of all types of streets and adjoining sidewalks with street trees, landscaping, water features, pedestrian-scaled lighting, street furniture, paving treatments, medians, or other softening treatments as appropriate.

**POLICY S-DT-41.** Minimize disruption of vehicular flow on auto-bias streets.

**Signature Streets**

The functional aspect of Downtown Bellevue’s streets can be refined around a set of signature themes. The graphic below shows three types of signature streets. Bellevue Way, Main Street in Old Bellevue, and the NE 6th Pedestrian Corridor are identified as *Shopping Streets*. The others are 106th Avenue NE as *Entertainment Avenue*, and 108th Avenue NE as



Signature streets will help tie the Downtown districts together.

Downtown's *Commerce Avenue*. These streets will help tie Downtown together with complementary uses and design elements. All these streets will continue to support multiple uses, with the unique identities evolving over time.

**POLICY S-DT-42.** Reinforce the emerging identity of 108th Avenue NE as the Eastside's business address. Provide incentives for private development and utilize public funds to create a dense office environment with supporting transit service and retail uses.



*Main Street in Old Bellevue is comprised of small-scale, pedestrian-friendly uses along the street edge.*

**POLICY S-DT-43.** Encourage new development on Main Street in Old Bellevue to embrace the character of the small-scale, pedestrian-friendly street frontage that has developed there over time.

**POLICY S-DT-44.** Provide incentives for 106th Avenue NE to develop as Downtown's *Entertainment Avenue*. This area will include a concentration of shops, cafés, restaurants, and clubs that provide for an active pedestrian environment during the day and after-hours venues for residents and workers by night.



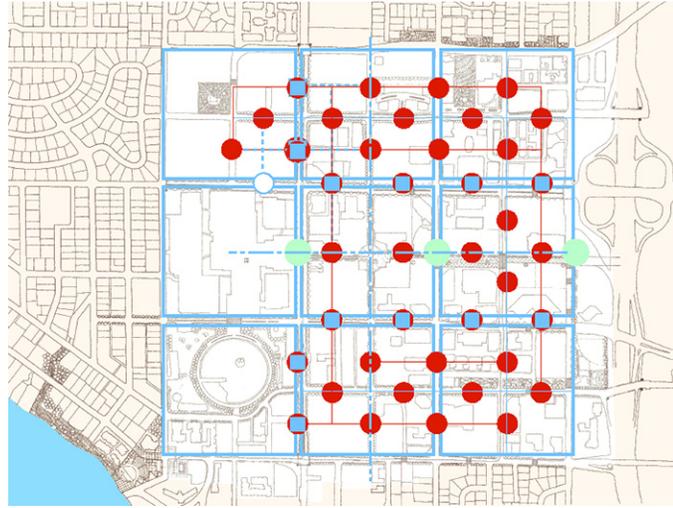
*The vision for Bellevue Way is a Grand Shopping Street.*

**POLICY S-DT-45.** Continue to encourage the NE 6th Street Pedestrian Corridor as a major unifying feature for Downtown Bellevue.

**POLICY S-DT-46.** Provide incentives for Bellevue Way to realize its vision as a *Grand Shopping Street*, with an exciting mix of retail shops, restaurants, hotels, offices and residential units.

## Mid-Block Pedestrian Crossings

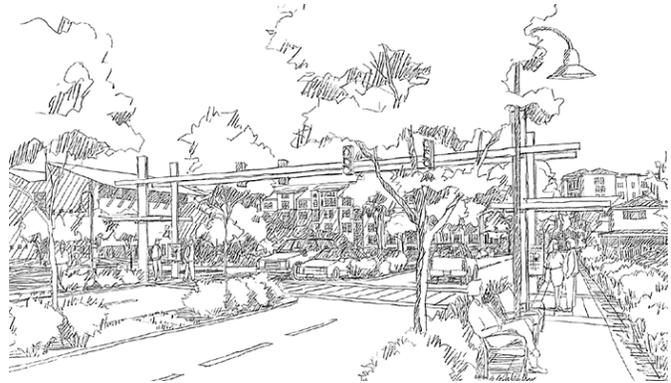
The scale of Downtown's 600-foot long superblocks provides a challenge in creating a fine-grained pedestrian environment. In select locations, there may be opportunities to improve pedestrian mobility across arterial streets with signalized mid-block pedestrian crossings. The graphic below shows the concept for a series of these connections and the impact they could have as a system.



*Concept for a series of signalized, mid-block pedestrian crossings.*

The precise location and number of these crossings will be determined by the design of adjacent superblocks, consideration of traffic flow, and the quality of the pedestrian environment. Mid-block crossings would not be appropriate on auto-biased streets, but may be possible on auto-neutral streets and pedestrian-biased streets.

**POLICY S-DT-47.** Reinforce the importance of the pedestrian in Downtown Bellevue with the use of a series of signalized mid-block crossings. Consideration should be given to the design of adjacent superblocks, consideration of traffic flow, and the quality of the pedestrian environment when implementing mid-block crossings.

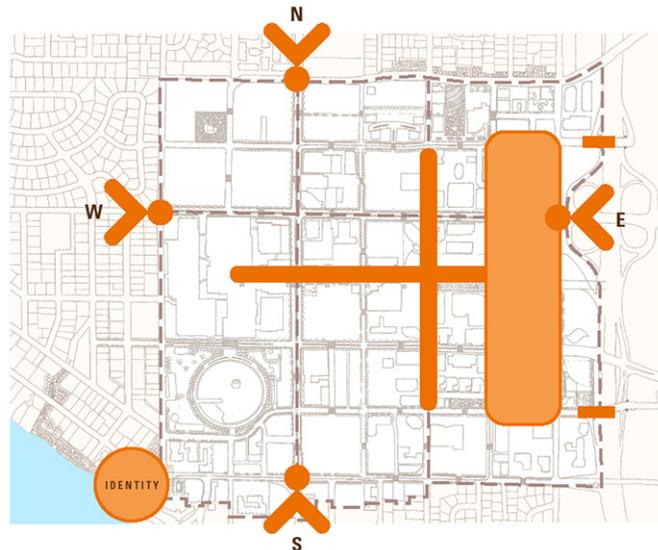


*Artist's sketch of possible design treatment of mid-block crossing.*

## Gateways and Wayfinding

There are a number of ways to express a gateway into Downtown Bellevue. They could incorporate architectural elements, a variety of vegetation, water features, decorative paving, and interpretive or directional signage. Wayfinding is a key element in a maturing, complex Downtown. Wayfinding not only helps people navigate from point A to point B on foot, bicycle or car, but also contributes to the design character of the public realm.

The graphic below shows gateway and identity opportunities for the Downtown Subarea. Major gateway locations for Downtown are on Bellevue Way from the north and south, and on NE 8th from the east and west. Major identity opportunities are shown at Meydenbauer Bay, the Pedestrian Corridor, the ridge along 108th Avenue (the highest point in Downtown, with some of the tallest buildings), the properties directly visible from I-405, and new bridges to be constructed across I-405 at NE 10th Street and NE 2nd Street.



*Gateway and identity opportunities.*

**POLICY S-DT-48.** Provide for a sense of approach to Downtown at key entry points through the use of gateways and identity treatments that convey a sense of quality and permanence.

**POLICY S-DT-49.** Enhance the attractiveness of the I-405 right-of-way in accordance with its role as a gateway to the City of Bellevue and the Downtown Subarea.

**POLICY S-DT-50.** Develop a comprehensive wayfinding system geared for a range of users (i.e. pedestrians, bicyclists, and automobiles). The system should be built around a set of common design elements, but also includes unique components that vary by Downtown neighborhood as appropriate.

## Unifying Urban Design Feature

In the coming years, Bellevue will have a number of opportunities to develop a unifying urban design feature or features that will add to Downtown livability and memorability. The evolution of Downtown neighborhoods will present an opportunity to gracefully link a literal and symbolic expression throughout the entire Downtown. An example may be the use of water. Water can be expressed in many ways. There are a number of instances of water already in Downtown. Downtown Park has the canal and waterfall. Many fountains are within the public realm, and many more are within private developments just off the sidewalk.



*The expression of water is evident in downtown's many fountains and water features.*

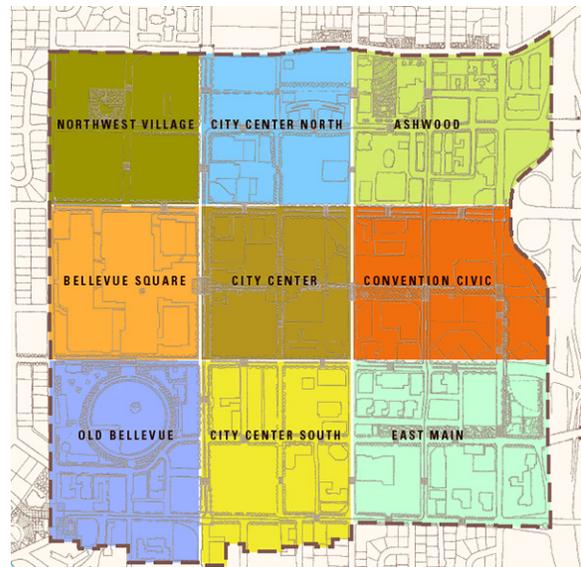
**POLICY S-DT-51.** Develop a strategy on how to link Downtown together through the use of literal and/or symbolic major design features that vary by district.

**POLICY S-DT-52.** Provide incentives to assist developers in implementing a major unifying design feature.

**POLICY S-DT-53.** Incorporate the unifying design feature in public projects whenever possible.

## Downtown Districts

A key piece of the Great Place Strategy that guides this Subarea Plan is development of a series of distinct, mixed-use neighborhoods (or districts) within Downtown that each capitalize on their locations and unique identities. Downtown was originally laid out in a manner that is defined by its street grid and system of 600-foot superblocks. Rather



*Downtown divided into a nine-square grid.*

than one homogenous Downtown, the superblocks may be grouped together to form nine districts. Each district has clear boundaries formed by major arterials that extend outside of Downtown. Each district is pedestrian-friendly in size, and easily traversed in a ten-minute walk.

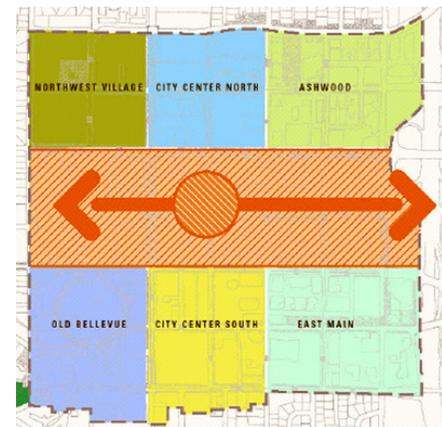
By connecting the center row into a common district as shown in the graphic below, the symbolic and functional center of the Eastside is created. This will be known as Bellevue's City Center District. To the north and south of the City Center are three districts. Some of the districts such as Ashwood and Old Bellevue already have clear identities. The identity of others is not as clear and will evolve over time.

**POLICY S-DT-54.** Provide incentives to reinforce unique characteristics of Downtown Districts to create pedestrian-scaled, diverse, and unique urban lifestyle experiences and options.

**POLICY S-DT-55.** Utilize design guidelines to help differentiate development within each of the Downtown Districts as they evolve over time.

**POLICY S-DT-56.** Differentiate Downtown Districts through streetscape improvements such as wayfinding elements, gateways, mid-block pedestrian crossings, public art, landscaping and street trees, lighting, and street furniture.

**POLICY S-DT-57.** Create pedestrian linkages within and between the Downtown Districts as well as to surrounding residential areas outside Downtown.



*Three center squares are connected to form a single district.*

### ***Northwest Village***

The Northwest Village District currently provides a wide array of primarily neighborhood-oriented retail and service uses. It is a neighborhood shopping area for both Bellevue and the Points communities of Medina, Clyde Hill, Yarrow Point, and Hunts Point. This district is somewhat isolated from the rest of Downtown. No large-scale development has occurred in this district during the growth cycles of the 1980s and 1990s. The district will evolve over time as it has a significant amount of growth potential. There will need to be public investments for parks and open space. Additional housing will add to the village feel that currently exists in and around the pedestrian-scaled 102nd Avenue NE area. The development of “alleys with addresses” will add to the village feel of the area. With both infill development and large-scale redevelopment this area will evolve into a vibrant urban neighborhood.

**POLICY S-DT-58.** Create intimacy for the pedestrian through the development of “alleys with addresses.” These are small-scale pedestrian frontages accessed off of mid-block connections.

**POLICY S-DT-59.** Continue to provide neighborhood-oriented retail and service uses for the Northwest Village District as well as for the surrounding neighborhoods both within and outside Downtown.

**POLICY S-DT-60.** Enhance the connection and interface for the pedestrian from the Northwest Village District to Bellevue Square.

**POLICY S-DT-61.** Examine additional opportunities for on-street parking in the district.

**POLICY S-DT-62.** Explore opportunities for shared parking, or a park-once district concept for short term parking.

**POLICY S-DT-63.** Develop a neighborhood park in the Northwest Village District.

**POLICY S-DT-64.** Emphasize the intersection of 102nd Avenue NE and NE 10th Street as a central gathering place for the district.

### ***City Center North***

The City Center North District is home to the Bellevue Place mixed-use development. It currently provides the defining character for the district. As it expands with additional uses, such as a large performing arts center, more activity will be centered around the intersection of 106th Avenue NE and NE 10th Street. North of NE 10th Street, multifamily development has recently occurred, and more is planned. There is a great opportunity to develop a high-rise housing row in this district.

**POLICY S-DT-65.** Encourage the development of high-rise housing along NE 10th Street within this district.

**POLICY S-DT-66.** Improve pedestrian connectivity from City Center North to the Ashwood District to the east, Northwest Village to the west, and across NE 8th Street to the south.

**POLICY S-DT-67.** Develop a soft or hard open space amenity in the vicinity of 106th Avenue NE and NE 10th Street.



*Bellevue Place, a major mixed-use development, helps define the character of City Center North.*

## ***Ashwood***

The Ashwood District is defined by the King County Regional Library, Ashwood Park, and the concentration of dense urban housing. The area currently lacks some of the neighborhood-serving uses that are desirable for a Downtown neighborhood such as small grocery and drug stores, but they are likely to emerge over time. Future improvements to Ashwood Park will also play a large role in the maturation process for this district.

**POLICY S-DT-68.** Explore opportunities to unite the district by bridging a perceived gap formed by NE 10th Street.

**POLICY S-DT-69.** Encourage other civic uses to locate in this district, using the King County Library as an anchor.

**POLICY S-DT-70.** Encourage uses that will bring additional pedestrian activity to the area.

**POLICY S-DT-71.** Examine additional opportunities for on-street parking in the district.

**POLICY S-DT-72.**

Encourage expansion of the King County Library to the north. Explore a potential partnership to develop appropriate community center facilities as part of the expansion.



*The King County Regional Library is an anchor for the Ashwood District.*

**POLICY S-DT-73.** Provide pedestrian and bicycle connectivity across I-405 at NE 10th Street.

**POLICY S-DT-74.**

Encourage ground-level residential units on 109th Avenue NE, 111th Avenue NE, and NE 11th Street.

**POLICY S-DT-75.** Encourage a unified, high density urban residential community with supporting neighborhood retail and service uses.

**POLICY S-DT-76.** Limit the amount of office and retail development in the area to take into account the predominantly residential character of the area.

**POLICY S-DT-77.** Use of Ashwood Park site as an urban park or community facility should work in conjunction with residential uses in the area.

### ***Eastside Center District***

The Eastside Center District is comprised of three smaller districts: Bellevue Square, City Center, and the Civic/Convention District. The Eastside Center is within walking distance to all of Downtown's key features. The key to the Eastside Center District is tying it together from east to west along the NE 6th Pedestrian Corridor, and having it become the symbolic and functional heart of the Eastside Region.

**POLICY S-DT-78.**

Capitalize on the relocation of City Hall to Downtown to help nurture a strong civic and convention center presence on the eastern portion of the Eastside Center District.



*Meydenbauer Center helps form the eastern portal of the Eastside Center District.*

**POLICY S-DT-79.** Provide incentives to develop the intersection of 106th Avenue NE and NE 6th Street as a central location for public gatherings.

**POLICY S-DT-80.** Pedestrian Crossings may be appropriate over the public right-of-way on Bellevue Way between NE 4th Street and NE 8th Street, and over NE 4th and NE 8th Streets between Bellevue Way and 110th Avenue NE, provided that there is a clear demonstration of public benefit, and design criteria are fully met.

**POLICY S-DT-81.** Develop the NE 6th Pedestrian Corridor as a unifying feature for Downtown Bellevue by siting buildings and encouraging uses that add to pedestrian movement and activity.

**POLICY S-DT-82.** A range of activities shall be permitted, including office, urban residential, hotel, retail, civic, and entertainment uses.

**POLICY S-DT-83.** Day-time and night-time activities should be encouraged.

**POLICY S-DT-84.** Encourage pedestrian-oriented post office facilities to be located in this area.

**POLICY S-DT-85.** Allow uses and development intensity that is supportive of transit and day/night activity.

**POLICY S-DT-86.** Discourage use of the eastern portion of this District for large-scale, stand-alone transit parking. Transit parking may be appropriate if combined with other uses.

### ***Old Bellevue***

The Old Bellevue District sits above Meydenbauer Bay and proudly displays the roots of Downtown. This area is home to many small shops and Downtown’s oldest buildings. This district is also home to the 20-acre Downtown Park. Main Street functions like the traditional “Main Street USA”, with low traffic speeds, comfortable sidewalks, and on-street parking — elements that together make this a very safe and enjoyable place to walk.

**POLICY S-DT-87.** Provide a graceful pedestrian connection from Downtown Park through Old Bellevue to Meydenbauer Bay.

**POLICY S-DT-88.** Encourage redevelopment to maintain some of the historic façade treatments of older buildings in this district.

**POLICY S-DT-89.** Explore opportunities for shared parking, or a park-once district concept, to improve the availability of the short-term parking supply for retail and service users.



**POLICY S-DT-90.** Establish a heightened presence of Downtown Park as seen from Bellevue Way.

*Downtown Park should be a visible presence on Bellevue Way.*

**POLICY S-DT-91.** Reinforce the unique character of the Old Bellevue area by encouraging residential development, specialty retail, and other development with an emphasis on pedestrian activity.

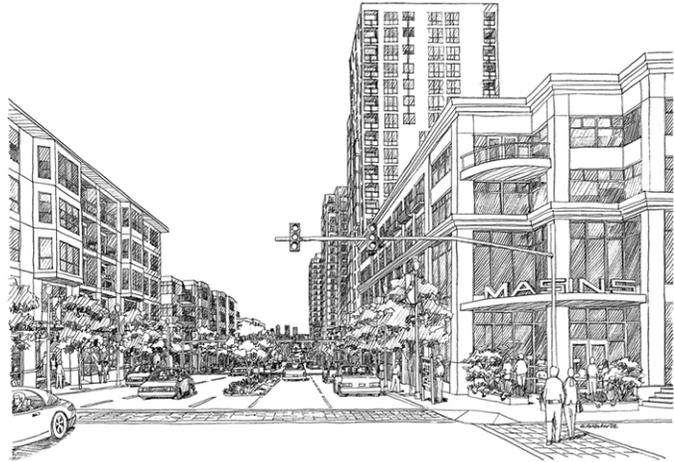
## ***City Center South***

The City Center South District is emerging as a true mixed-use neighborhood. New and exciting restaurant, retail, and residential uses are adding a greater level of activity in this area. The proximity to the Surrey Downs and 108th Avenue Neighborhoods provides an opportunity to have appropriately-scaled transitional uses along the edge of Downtown.

**POLICY S-DT-92.** Encourage development of neighborhood-serving retail uses.

**POLICY S-DT-93.** Explore opportunities for “live/work” spaces in this district.

**POLICY S-DT-94.** Encourage new development to provide open space amenities that are accessible to the public and function as neighborhood gathering places on the south side of Main Street, between 108th Avenue SE and Bellevue Way SE.



*Artist's sketch of the view looking west on Main Street compatible with the vision for the district.*

## ***East Main***

The East Main District experienced a significant amount of office and residential development in the 1990s, but still lacks a single defining feature. The district has a great deal of potential. Development of a needed open space amenity could provide a focus point and function as a catalyst for additional high-quality development.

**POLICY S-DT-95.** Develop a linear neighborhood park in the vicinity of NE 2nd Place that acts as a defining feature for the district.

**POLICY S-DT-96.** Take advantage of the topography of the area for views as well as for visibility from I-405.



*Concept for a new neighborhood park in the vicinity of NE 2nd Place.*

**POLICY S-DT-97.** Enhance the transition from this district [South Main] to the adjacent neighborhoods by providing a lineal green open space buffer in the vicinity of the southeast corner of Downtown.

**POLICY S-DT-98.** Explore opportunities to showcase the historic Sacred Heart Catholic Church on Main Street.

## Parks, Recreation & Open Space

Open space provided by both the public and the private sectors is a key component of being a livable city. Throughout Bellevue, open space punctuates, accents, and highlights the fabric of a city. Parks, recreation, and open space amenities within Downtown will function as a system that reinforces the notion of Bellevue as a “city in a park.” Some pieces, like the 20-acre Downtown Park, serve a regional need, while others are oriented to a particular neighborhood or district. The character of Downtown’s amenities will vary from quiet, contemplative and green, to crowded, high energy and more architectural. The system will be tied together in a planned and deliberate way by creating connections along Downtown’s sidewalks and mid-block pedestrian connections.

Downtown Park will continue to be one of Bellevue’s finest public assets. It is an important gathering place for people, a venue for special events and a key factor in the developing identity of Downtown. The Park plays a pivotal role in making Downtown an appealing place to live, work and play.

Major new features of the system will be neighborhood parks in the northwest and southeast quadrants of Downtown as well as a visual and physical connection from Downtown Park to Meydenbauer Bay. People naturally gravitate to areas with water to enjoy the aesthetics and unique recreational opportunities. This connection is imperative if Bellevue intends to identify itself as a waterfront city and provides an opportunity to recognize the Meydenbauer Bay’s historical significance in the region’s development.



*Concept for Downtown parks and open space network.*

## GOAL:

To provide urban parks, recreation opportunities, and open space within Downtown.

## POLICIES

### General

**POLICY S-DT-99.** Emphasize the street environment as a key component of the Downtown open space network.

**POLICY S-DT-100.** Encourage active and passive recreational activities to locate throughout Downtown.

**POLICY S-DT-101.**  
Provide appropriately scaled parks and open spaces throughout Downtown.

**POLICY S-DT-102.**  
Analyze alternative locations and explore potential partnerships to provide community center space and functions for the Downtown Subarea and surrounding neighborhoods.



*Both public and private open spaces help make Downtown Bellevue a livable place.*

**POLICY S-DT-103.** Encourage developers to provide open space amenities accessible to the public such as mini-parks, plazas, rooftop gardens, and courtyards in private developments. Such amenities must be clearly identified and maintained for public use.

**POLICY S-DT-104.** Require developer contributions for a coordinated system of major and minor public open spaces along the pedestrian corridor and at designated intersections. These could include areas for seating, fountains, courtyards, gardens, places to eat, and public art.



*The proposed open space network will provide both visual and physical connections between the Downtown and beautiful Meydenbauer Bay.*

**POLICY S-DT-105.** Provide a visual and physical connection from Downtown to Meydenbauer Bay that terminates in a significant waterfront presence. The connection will provide unique recreation, retail, and tourism opportunities.

**POLICY S-DT-106.** Encourage new residential development to include open space and recreation amenities targeted to growing Downtown population.

**POLICY S-DT-107.** Create connections along public sidewalks and mid-block connections that link key parks and open spaces and include dispersed recreation opportunities and urban plazas where appropriate.



**POLICY S-DT-108.** Provide a lineal green open space buffer in the vicinity of the southeast corner of Downtown to transition from single family residential uses to higher density residential and commercial uses north of Main Street within Downtown.

*The proposed open space network will provide both visual and physical connections between downtown and beautiful Meydenbauer Bay.*

**POLICY S-DT-109.** Provide an east-west connection through the Downtown Subarea for the Lake-to-Lake Trail system.

**POLICY S-DT-110.** Continue to preserve significant older trees within the Downtown Subarea.

## **Downtown Park**

**POLICY S-DT-111.** Develop and operate Downtown Park within the framework of the Council approved Downtown Park master plan.

**POLICY S-DT-112.** Complete the phased development of Downtown Park, and enhance its visual and functional presence on Bellevue Way.

**POLICY S-DT-113.** Utilize Downtown Park as the prime location for special events and gatherings throughout the year.

**POLICY S-DT-114.** Strengthen pedestrian connections between Downtown Park and other Downtown features, such as Bellevue Square, the NE 6th Street pedestrian corridor, Bellevue Way, Main Street, and Meydenbauer Bay. This will enhance the role of the Park as a major pedestrian destination and as a pedestrian linkage with other areas of Downtown.

**POLICY S-DT-115.** Within density and height limits permitted in the Land Use Code, work with adjoining property owners through the Design Review process to ensure a graceful transition between the Park and future development. The ground level of buildings facing Downtown Park should include pedestrian oriented uses, be visually accessible, and where appropriate, provide pedestrian connections for the general public and for commercial uses that complement the Park. A continuous north-south pathway on private properties separating adjoining development from the Park shall not be required.

### **Neighborhood Parks**

**POLICY S-DT-116.** Provide an equitable distribution of neighborhood-serving parks and open spaces throughout Downtown. These amenities should include a mix of passive, green open space along with more formalized hardscape areas.

**POLICY S-DT-117.** Update the Ashwood Park master plan to reflect a contemporary vision for the site that considers some community center functions while retaining significant passive open space use. Explore partnerships for development consistent with the vision.

## **Edges & Transitions**

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Downtown Bellevue is unlike many other urban centers in that it is directly adjacent to vibrant single family neighborhoods on three of its four edges (north, west, and south; I-405 lies to the east). The city is committed to protecting these neighborhoods by utilizing traffic and parking management outside Downtown, and defining Perimeter Areas through zoning within Downtown to reduce potential spillover impacts.

## **POLICIES**

### **Neighborhood Traffic & Parking Management**

Existing programs to protect neighborhoods adjacent to Downtown Bellevue from significant adverse transportation impacts will be enhanced and expanded as the need arises. Such programs will include a traffic management program to discourage cut-through traffic and a residential parking zone program to discourage commercial parking in residential areas. Traffic calming measures may be implemented to remedy specific situations.

**POLICY S-DT-118.**

Protect the residential neighborhoods surrounding Downtown from traffic impacts by monitoring traffic volume levels on residential streets and establishing appropriate traffic control measures with residents' concurrence.



*Traffic control measures may be used to reduce spillover impacts.*

**POLICY S-DT-119.** Establish residential parking permit programs wherever appropriate in the residential communities surrounding Downtown and enforce parking violations to eliminate parking spillover from Downtown.

**Perimeter Areas**

**POLICY S-DT-120.** Provide an equitable distribution of Perimeter Areas along the north, west, and south boundaries of Downtown, based on their potential for protecting surrounding residential neighborhoods.

**POLICY S-DT-121.** Provide incentives for multifamily residential uses and neighborhood-serving retail and service uses within Perimeter Areas to provide stability both within the Downtown Subarea and within surrounding residential neighborhoods.

**POLICY S-DT-122.** Require development occurring within Perimeter Areas to participate in traffic mitigation measures to reduce impacts on surrounding residential neighborhoods.

**POLICY S-DT-123.** Establish development standards and design guidelines for Perimeter Areas that will break down the scale of new development and add activities and physical features that will be compatible both with the Downtown Subarea and surrounding residential areas.



*Linear buffers, complete with art and landscaping, help define and protect surrounding neighborhoods.*

**POLICY S-DT-124.** Utilize sidewalk, landscaping, and green space treatments within Perimeter Areas to provide a transition from Downtown to surrounding residential neighborhoods.

### **Linear Buffers**

**POLICY S-DT-125.** Utilize lineal green open space buffers directly outside Downtown (north of NE 12th Street between 106th Place NE and 112th Avenue NE, and in the vicinity of the southeast corner of Downtown) to provide a graceful transition from Downtown to surrounding residential neighborhoods.

## **Transportation & Circulation**

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### **GOALS:**

**To provide an accessible transportation network for motor vehicle circulation, public transportation, high occupancy vehicles, pedestrian circulation, bicycle circulation, and integrated parking.**

**To identify the road and transit improvements needed to implement the city's vision for Downtown Bellevue as a dense, mixed-use urban center.**

### **POLICIES**

#### **Regional Roadway Access**

Downtown Bellevue relies on regional access to prosper from both an economic and cultural standpoint. This requires a significant amount of coordination with other local, state, and federal partners. Maintaining adequate regional accessibility is also essential in minimizing impacts on Bellevue's arterial and local streets.

**POLICY S-DT-126.** Aggressively pursue local, state, and federal action to implement improved automobile and high occupancy vehicle (HOV) access to and from the Downtown Subarea from I-405 at NE 6th Street.

**POLICY S-DT-127.** Actively participate in the SR-520 bridge replacement and HOV project. Evaluate access needs in the SR-520 corridor including the recommended new on-ramp at Bellevue Way NE.

**POLICY S-DT-128.** Minimize growth of traffic on arterial streets in residential areas north, west and south of Downtown by encouraging the use of freeway facilities. Arterial streets should not function as alternative routes to freeways. Traffic flow should be managed in accordance with the relevant Subarea Plan policies and should be distributed among arterial streets.

**POLICY S-DT-129.** Emphasize the use of 114th Avenue SE as the primary arterial street between SE 8th and Main Street. Provide direct access from 114th Avenue SE to I-405 through the SE 8th interchange modification so as to minimize traffic impacts on the residential neighborhood south of Downtown.

## **Regional and Local Transit**

The 2020 growth forecast for Downtown Bellevue shows a significant increase in transit demand. To meet this demand, a doubling of overall transit frequency will be required to ensure sufficient local and regional service for workers, residents, and visitors. This increase in transit service will result in a quadrupling of transit ridership. High capacity transit is a key component of the long-range vision for Downtown. Achieving high levels of transit ridership to Downtown Bellevue will also depend on a significant expansion of service for local and regional routes and Park and Ride capacity for trips that originate outside the city. These improvements will seek to provide a competitive trip frequency and travel time advantage, as well as locate parking in areas where a significant increase in ridership is expected to originate.



*Bellevue works with transit providers to improve connections between Downtown, urban centers and neighborhoods.*

Dedicated transit lanes on 108th Avenue NE and the 106th/108th one-way couplet would improve transit service and schedule reliability. Revisions to simplify and speed service within Downtown are recommended to achieve the large increase in transit trips internal to Downtown – 30 percent of the total ridership increase. To maintain Downtown mobility, transit should be targeted to connect the Bellevue Transit Center, major retail and office areas, and activity areas adjacent to Downtown such as Overlake Hospital.

**POLICY S-DT-130.** Encourage transit service providers to improve transit connections between Downtown and the city’s neighborhoods.

**POLICY S-DT-131.** Work with transit providers to significantly expand transit service, including express bus transit, to Downtown Bellevue to accommodate anticipated increases in ridership.

**POLICY S-DT-132.** Explore ways of providing the most effective transportation services and marketing programs for trips between major retail, office, and transit facilities Downtown, as well as activity areas on the edge of Downtown such as Overlake Hospital.

**POLICY S-DT-133.** Encourage transit service providers to improve transit connections between Downtown Bellevue and other designated urban centers.

**POLICY S-DT-134.** Support transit ridership to Downtown Bellevue by encouraging the regional transit providers to expand Park-and-Ride capacity outside of Bellevue.

**POLICY S-DT-135.** Provide space within or near Downtown for bus layovers and other transit facilities needed to support projected levels of transit service and ridership. Layover space and other facilities, whether developed within the right-of-way or off-street, must be located and developed in a manner that minimizes impacts on residential areas, provides an active pedestrian environment and is consistent with the district character direction in this Plan.

**POLICY S-DT-136.** Encourage convenient and frequent transit services and provide incentives for attractive waiting areas in Downtown in recognition that transit extends the range of the pedestrian.

**POLICY S-DT-137.** Coordinate with transit providers to enhance information and incentives available to transit riders and potential transit riders to encourage and facilitate transit use.

**POLICY S-DT-138.** Work with Sound Transit and other regional partners to develop a High Capacity Transit system that connects Downtown Bellevue to other key activity centers.

## **Downtown Roadways**

Maximizing the efficiency of our Downtown roadway network will require some significant changes over the next 20 years. These include operational changes, including a one-way couplet on 106th and 108th Ave NE, and extensions of NE 2nd and NE 10th Street across I-405 to 116th Ave NE.

These changes will help relieve pressure on NE 4th and NE 8th Street in providing east-west access by more equally distributing volumes over the full network.



*Odd-numbered streets offer opportunities for pleasant pedestrian and vehicular movement.*

**POLICY S-DT-139.** Retain the existing odd-numbered streets for vehicular and pedestrian circulation in Downtown. Consider vacating those streets only if such vacation would improve overall circulation in Downtown.

**POLICY S-DT-140.** Improve Downtown circulation and arterial continuity to points beyond Downtown by extending NE 2nd and NE 10th Streets across I-405.

**POLICY S-DT-141.** Improve traffic flow within Downtown by creating a one-way couplet consisting of 106th Avenue NE (northbound) and 108th Avenue NE (southbound) between Main Street and NE 12th Street. Provide contraflow transit operations on 108th Ave NE between NE 4th and NE 8th Street.

**POLICY S-DT-142.** Restrict left turns at mid-block locations and at major intersections where needed to improve traffic operations, safety, and/or capacity.

**POLICY S-DT-143.** Enhance the city’s computerized traffic control system to maximize the operation of the traffic signals in Downtown, and encourage use of transit through improved speed and reliability for transit coaches.

### Mid-Block Connections

Mid-block connections are necessary to provide parking garage and loading/delivery access without disrupting traffic flow on the major arterial streets. These mid-block connections on private property will be part of the overall design, viability, and pedestrian friendliness of the superblock development, and could create attractive physical environments for the pedestrian while still providing vehicular access.

Mid-block connections should be developed under flexible design standards. Traffic flow and capacity constraints on adjacent streets will be important factors. Mid-block connections must be shown to serve a reasonable transportation or planning purpose for serving the developments that contain them; they should not be used as a city regulation to create through-grid streets on private property. The exact alignment and location of mid-block connections is subject to the design process on private property.



*Mid-block connections create attractive pedestrian environments and improve traffic flow.*

Mid-block connections are recommended for portions of the 103rd, 105th, and 107th Avenue, and NE 5th and NE 7th Street alignments (see Figure B). Development projects will incorporate mid-block connections for vehicles and/or pedestrians as determined through the development review process.

**POLICY S-DT-144.** Provide mid-block access corridors within a Downtown superblock which accommodates vehicle access to parking areas, loading/delivery access, and pedestrian circulation. Develop specific design concepts and implement them as development occurs in each superblock.

## **Demand Management**

Transportation demand management (TDM) strategies require coordination between the city, transit agencies and the private sector, and focus on reducing peak hour, single occupant vehicle (SOV) commute trips. TDM strategies to provide information and incentives will encourage commuters and other travelers to try an SOV alternative for trips to Downtown.

**POLICY S-DT-145.** Promote provision of high occupancy vehicle (HOV) transportation services including transit, carpools, and vanpools to, from, and within the Downtown Subarea.

**POLICY S-DT-146.** Support the Bellevue Downtown Transportation Management Association.

**POLICY S-DT-147.** Support the Downtown Transportation Management Program.

**POLICY S-DT-148.** Minimize Downtown SOV commute trips by coordinating with the Bellevue TMA and transit agencies to provide transit and rideshare incentives, subsidies, and promotional materials to Downtown employers and employees.

## **Parking Demand and Utilization**

The parking situation in Downtown Bellevue is characterized by an adequate overall supply of parking, with limited short-term parking in a few areas. This situation is dynamic and will change over time with Downtown land use. Parking industry standards suggest that when a local area's parking supply (within a 700-foot radius) exceeds 85 percent occupancy in the peak parking demand hour, the supply is constrained and does not provide convenient access to visitors who require space for short time periods. These are the most important users for ensuring the economic vitality of the area. When surveys show that the peak hour parking occupancy routinely exceeds 85 percent, a variety of strategies may be implemented to bring peak hour occupancies below the 85 percent criteria. More effective management of the parking supply is the first priority, and if

management steps do not lower the utilization rate to under 85 percent, then strategic additions to the parking supply may be warranted.

The first management approach should be to shift as many commuters as possible to transit and other alternative modes through enforcement, pricing, and/or incentives, so they do not compete with visitors for the most convenient parking spaces. Strategies to supplement the parking supply for short term users, where warranted, may include creating more on-street parking, cooperating with private property owners to develop more shared use of existing spaces, or as a last resort, constructing public parking structures at critical locations. Another needed management action is to improve signage to direct visiting motorists to the available public parking supply.

**POLICY S-DT-149.** Establish parking requirements specific to the range of uses intended for the Downtown Subarea.

**POLICY S-DT-150.** Develop Downtown parking facilities and systems that are coordinated with a public transportation system and an improved vehicular circulation system.

**POLICY S-DT-151.** Encourage the joint use of parking and permit the limitation of parking supply.

**POLICY S-DT-152.** Evaluate the parking requirements in the Land Use Code and regularly monitor the transportation management program, employee population, parking utilization, parking costs paid by commuters and the percentage of those who directly pay for parking. If monitoring indicates that the use of transit and carpool is not approaching the forecast level assumed for this Plan, revise existing parking and transportation management requirements as needed to achieve forecast mode split targets found in the Transportation Element of the Comprehensive Plan.

**POLICY S-DT-153.** Permit short-term on-street parking on Downtown streets if such action does not create significant traffic problems.

**POLICY S-DT-154.** Initiate a public/private comprehensive examination of short-term parking problems Downtown, and develop a work plan to implement solutions.



*Appropriately placed on-street parking can benefit adjacent residential and commercial uses.*

**POLICY S-DT-155.** Utilize quantitative measures to analyze the short-term parking supply for neighborhood-scale retail and services, and implement parking management strategies or increase the parking supply as appropriate, and as resources allow.

**POLICY S-DT-156.** Investigate allowing Downtown developers to pay a fee into a “pool” in lieu of providing parking on-site. Pooled funds would be used to provide short-term public parking where it is in shortest supply. Land Use Code amendments would be required to provide for the collection and administration of a fee in lieu of parking program.

**POLICY S-DT-157.** Explore opportunities to implement a parking guidance system to more efficiently utilize the Downtown parking supply.

## **Pedestrian and Bicycle**

Downtown Bellevue’s 600-foot long superblocks present both challenges and opportunities for safe and cohesive pedestrian and bicycle movement. These transportation modes are addressed in detail in the Pedestrian and Bicycle Transportation Plan. In accordance with the Plan, private development and public capital investments will enhance the environment for pedestrians and bicyclists.

**POLICY S-DT-158.** Provide for the needs of bicycles and pedestrians in the design and construction of new facilities in Downtown, especially in the vicinity of the Transit Center, along the NE 6th Street pedestrian corridor, and on 106th Avenue NE where on-street parking and/or wider sidewalks may be appropriate.



*The Pedestrian Corridor provides a safe, convenient connection through Downtown Bellevue.*

**POLICY S-DT-159.** Enhance the mobility of pedestrians and bicyclists Downtown by improving signals and crosswalks at intersections and mid-block locations.

**POLICY S-DT-160.** Improve the pedestrian experience by providing street trees and other landscaping in sidewalk construction, especially along the edges of Downtown.

**POLICY S-DT-161.** Provide safe and convenient pedestrian linkages to adjacent neighborhoods to the north, south and west of Downtown, as well as across I-405 to the east.

**POLICY S-DT-162.** Provide pedestrian linkages through superblocks that help create a finer-grained pedestrian network.

**POLICY S-DT-163.** Designate and enhance bicycle routes through Downtown to create a more pleasant and safe environment for bicycling.



*Street trees and landscaping, together with active sidewalk-oriented uses, enhance the pedestrian experience.*

**POLICY S-DT-164.** Encourage the developers and owners of Downtown buildings to provide long-term bicycle parking and storage for employees and short-term bicycle parking for visitors.

## **Transportation Implementation**

**POLICY S-DT-165.** Implement the transportation facility improvements listed in Table 1 and shown on Figures B and C.

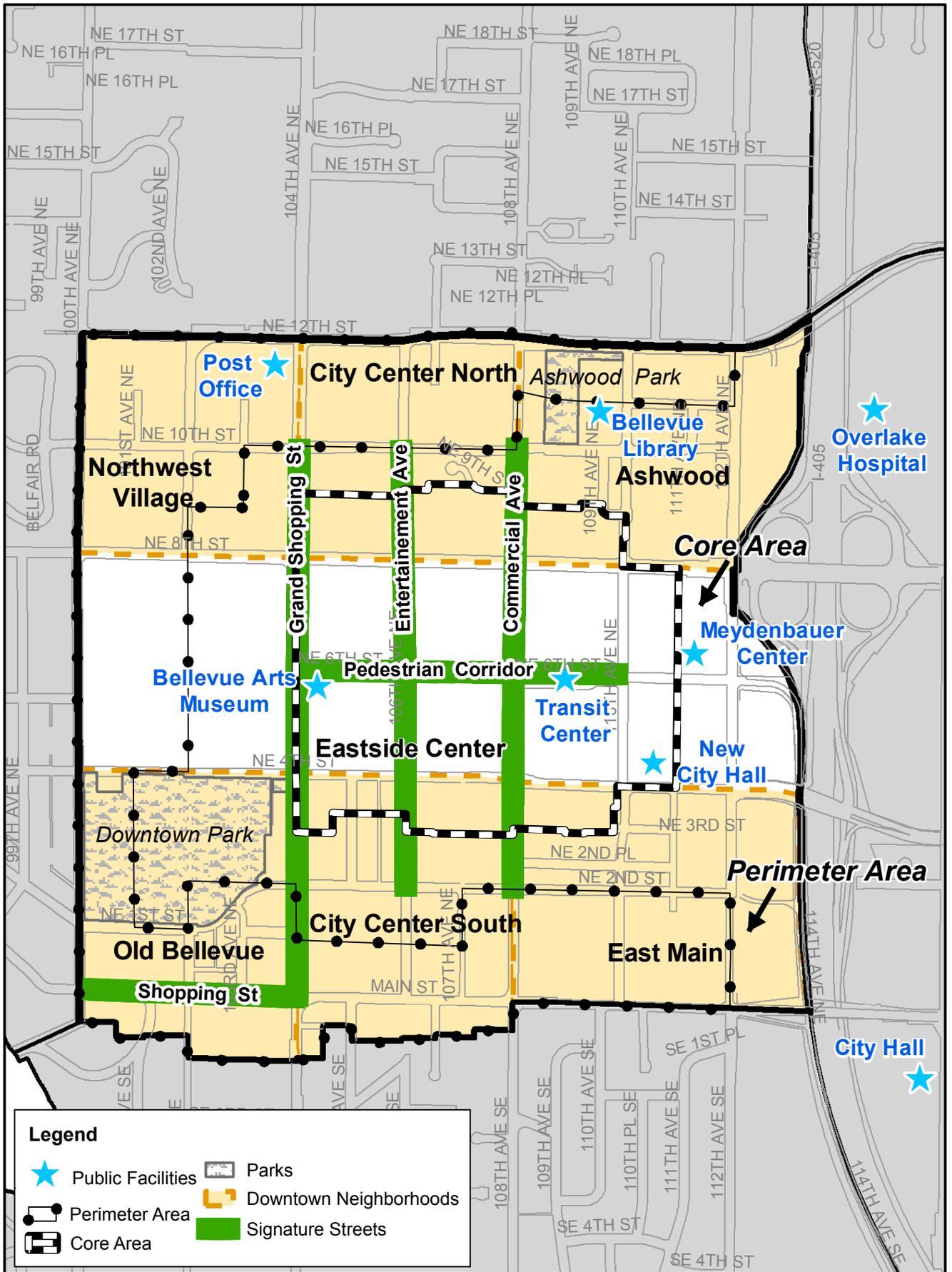
**POLICY S-DT-166.** Aggressively work with King County-Metro, Sound Transit, the Washington State Department of Transportation, and the Federal Highway Administration to implement the adopted capital facility component in this Plan where they have jurisdiction. The highest priority items in the Plan are state projects on I-405, including modifications to the NE 4th and NE 8th Street interchanges, construction of the NE 6th Street interchange, construction of new I-405/SR-520 access at NE 2nd and NE 10th Streets via collector/distributor lanes, and the widening of I-405 with general purpose and HOV lanes. The city will work to maintain design flexibility and to minimize inconveniences, economic disruption and other construction-related impacts.

**POLICY S-DT-167.** Annually review the progress of improvement projects and phasing.

**POLICY S-DT-168.** Support programs to meet air quality standards including the continuation and expansion of the state vehicle emission inspection and maintenance program.

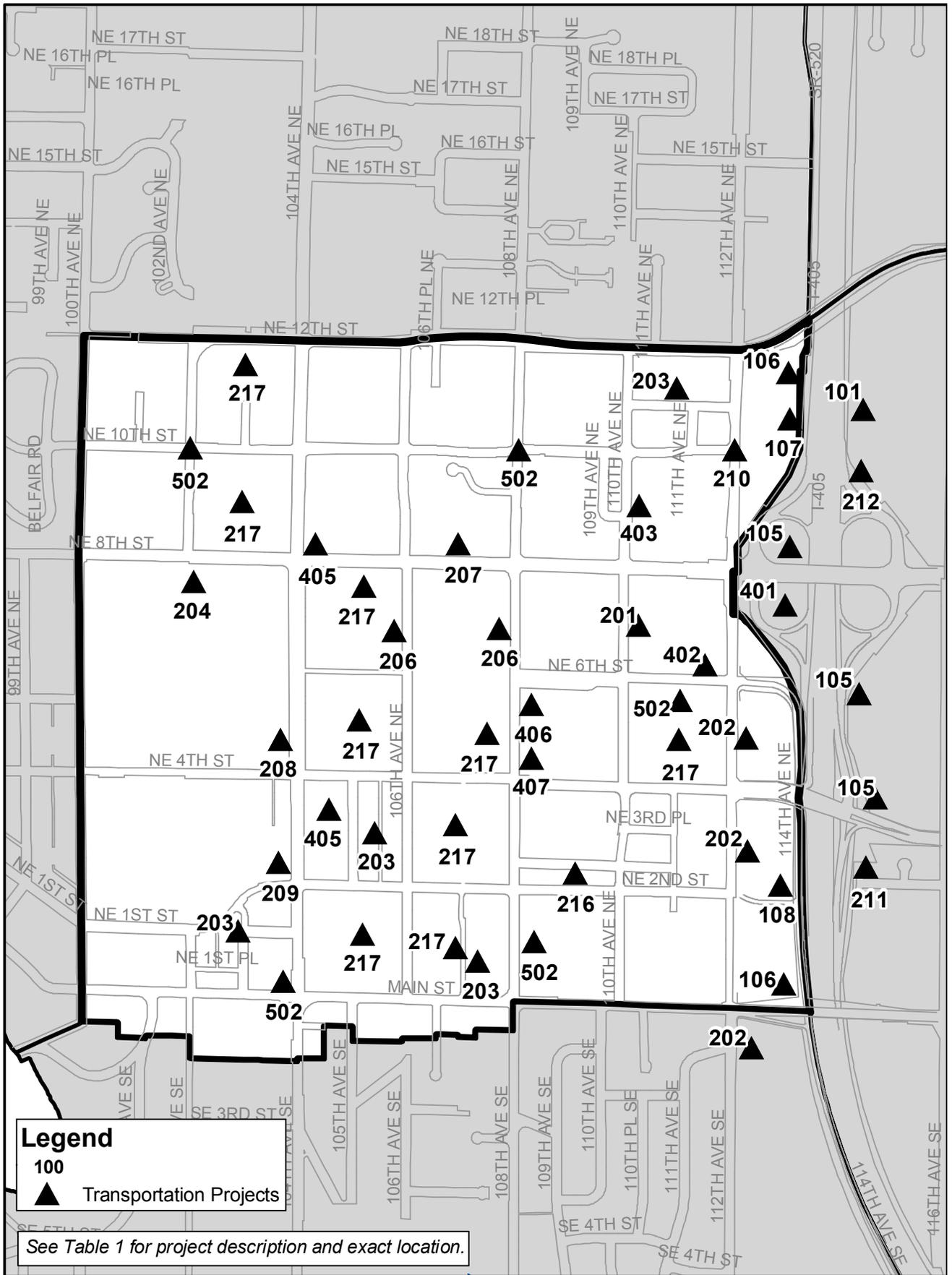
**POLICY S-DT-169.** Consider physical design treatments to reduce noise in residential neighborhoods before a major street construction program is implemented.

**POLICY S-DT-170.** The Downtown Future Land Use Plan Map (Figure A) is intended to show the major land use and character elements outlined by the goals and policies contained in the Downtown Subarea Plan. It is not intended to show specific densities or dimensions of future development. The Bellevue Land Use Code should be referenced for specific development standards.



**FIGURE A**  
**Downtown Future Land Use Plan**



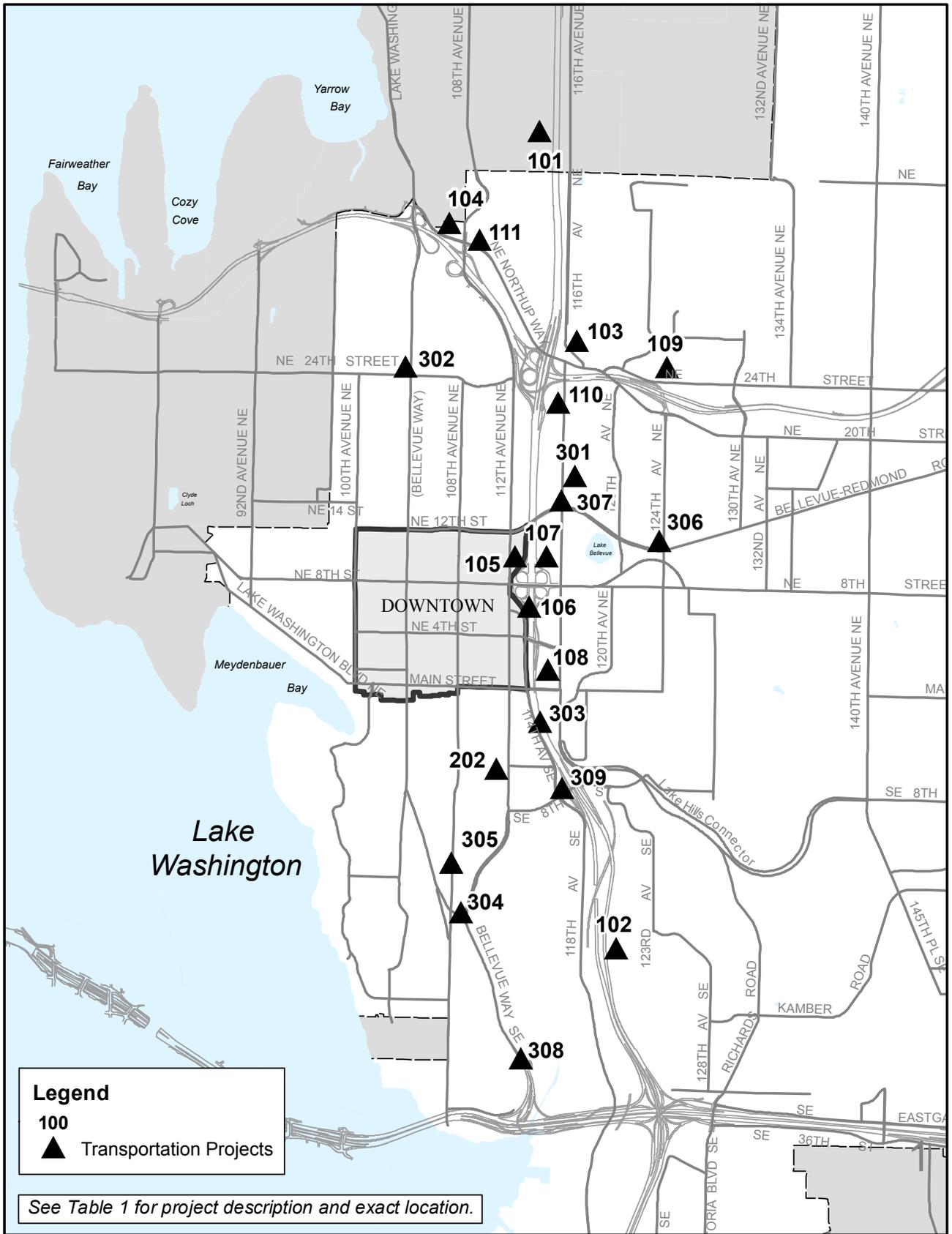


**Legend**  
 100  
 ▲ Transportation Projects

See Table 1 for project description and exact location.

**FIGURE B**  
**Downtown Transportation Map**





**FIGURE C**  
**Outside Downtown Transportation Map**



*Table 1 Downtown Plan Project List*

<b>Project Number</b>	<b>Project Location/Description</b>
<b>Freeways</b>	
<b>101</b>	<b>I-405 from NE 8th Street to NE 70th Street</b> Construct an HOV lane and one or more additional lanes (general purpose, HOV, and/or HCT) in each direction to improve the person carrying capacity of the corridor with final designation of usage and number of lanes dependent on the results of the Growth Management Act, implementation of Transportation Demand Management legislation and analysis performed in the High Capacity Transit study.
<b>102</b>	<b>I-405 from SE 8th Street to I-90</b> Construct an HOV lane and one or more additional lanes (general purpose, HOV and/or HCT) in each direction to improve the person carrying capacity of the corridor with final designation of usage and number of lanes dependent on the results of the Growth Management Act, implementation of Transportation Demand Management legislation and analysis performed in the High Capacity Transit study.
<b>103</b>	<b>I-405 at Northup Way and 116th Avenue NE</b> Construct a northbound on-ramp and a southbound off-ramp connecting the intersection of 116th NE and Northup Way with I-405.
<b>104</b>	<b>SR-520 at Bellevue Way NE</b> Construct an eastbound on-ramp to SR-520 from Bellevue Way NE.
<b>105</b>	<b>I-405 at NE 4th, NE 6th and NE 8th Street</b> Construct freeway interchange and access improvements between I-405 and Downtown Bellevue in the vicinity of NE 4th, 6th and 8th. Principal features of the improvements are: <ul style="list-style-type: none"> <li>a. A new 4-lane HOV ramp on NE 6th between 112th NE and I-405, connecting to a new HOV interchange at NE 6th and I-405 to serve transit, carpools and vanpools. Cul-de-sac 114th NE at NE 6th.</li> <li>b. Widen NE 8th from 110th NE to just east of 116th NE; with intersection improvements at NE 8th/112th NE and NE 8th/116th NE.</li> <li>c. Widen NE 4th overpass for an additional eastbound lane; remove HOV restrictions.</li> <li>d. Widen NE 6th to five lanes between 110th and 112th NE.</li> <li>e. Widen I-405 from Main Street to NE 12th to accommodate the HOV/ Transit interchange at NE 6th and design the interchange modification to allow for an additional travel lane in each direction.</li> </ul>
<b>106</b>	<b>I-405</b> Add two general purpose lanes on I-405 in each direction north of NE 10th and south of NE 2nd, with one additional lane in each direction between these streets.
<b>107</b>	<b>I-405</b> Add new I-405/SR-520 access at NE 10th via collector-distributor lanes from and to the north.
<b>108</b>	<b>I-405</b> Add new I-405 access at NE 2nd via collector-distributor lanes from and to the south.
<b>109</b>	<b>SR-520/124th Avenue NE</b> Create a new full interchange at SR-520 near 124th NE.

- 110**                    **I-405/116th Avenue NE**  
Provide new ramps to/from 116th NE/Northup Way.
- 111**                    **SR-520/Bellevue Way**  
Add eastbound off-ramp from SR-520 to 108th NE/112th NE at Bellevue Way.

**Roadways(Downtown)**

- 201**                    **110th Avenue NE from NE 4th Street to NE 9th Street**  
Widen this section from three lanes to four or five lanes, including traffic signal at NE 6th.
- 202**                    **112th Avenue SE/NE from SE 8th Street to NE 12th Street**  
Widen this section to provide additional turn lanes at all intersections. This widening would provide the following:  
a.    Left-turn lanes on 112th at SE 4th, SE 6th, NE 2nd, NE 6th and NE 10th.  
b.    Dual left-turn lanes on 112th NE at NE 8th.  
c.    Northbound to eastbound right-turn lanes on 112th NE at NE 4th.
- 203**                    **103rd Avenue NE/105th Avenue NE/107th Avenue NE/NE 11th Street**  
Reconstruct the following sections within Downtown to meet city design standards (i.e., sidewalks, lane width, etc.):  
a.    103rd NE between Main and NE 1st.  
b.    105th NE between NE 2nd and NE 4th.  
c.    107th NE between Main and NE 2nd.  
d.    NE 11th between 110th NE and 112th NE.
- 204**                    **Downtown**  
Restrict left turns on major arterials to signalized intersections.
- 205**                    **Downtown**  
Ongoing optimization of traffic signals on major arterials.
- 206**                    **106th & 108th Avenue NE**  
Implement a one-way couplet (106th NE northbound and 108th NE southbound) between Main and NE 12th.
- 207**                    **NE 8th Street**  
Add one westbound lane on NE 8th St between 105th NE and 108th NE (while preserving the large sequoia).
- 207.5**                **NE 8th Street / Bellevue Way**  
Add new southbound to westbound right turn lane.
- 208**                    **NE 4th Street / Bellevue Way**  
Provide dedicated southbound to westbound and westbound to northbound right turn lanes. Add new dual northbound to westbound left turn lanes on Bellevue Way.
- 209**                    **NE 2nd Street / Bellevue Way**  
Add new southbound left-turn lane on westbound NE 2nd. Add new northbound to eastbound right turn lane on Bellevue Way. Add new southbound to eastbound dual left turn lanes on Bellevue Way.
- 210**                    **NE 10th Street / 112th Avenue NE**  
Configure the intersection for one left-turn only, one thru-lane and one thru/right-turn for each approach with the extension of NE 10th.
- 211**                    **NE 2nd Street**  
Extend NE 2nd Street from 112th NE across I-405 to 116th NE, including intersection improvements at 112th NE and 116th NE. Add new dual southbound to eastbound left turn lanes on 112th at NE 2nd.

<b>212</b>	<b>NE 10th Street</b> Extend NE 10th Street from 112th NE across I-405 to 116th NE. Development related road & access improvements
<b>213</b>	<b>106th Avenue NE from Main Street to NE 12th Street</b> Widen this section from 4 to 5 lanes.
<b>214</b>	<b>108th Avenue NE from NE 8th Street to NE 12th Street</b> Widen this section from 4 to 5 lanes.
<b>215</b>	<b>108th Avenue NE from Main Street to NE 4th Street</b> Widen this section from 4 to 5 lanes.
<b>216</b>	<b>NE 2nd Street from Bellevue Way to 112th Avenue NE</b> Widen this section from 3 or 4 to 5 lanes.
<b>217</b>	<b>Portions of the 103rd, 105th, and 107th Avenue, and NE 5th and NE 7th Street alignments</b> Provide mid-block access corridors within a Downtown superblock which accommodates vehicle access to parking areas, loading/delivery access, and pedestrian circulation. Develop specific design concepts and implement them as development occurs in each superblock.
<b>Roadways (outside of Downtown)</b>	
<b>301</b>	<b>116th Avenue NE from NE 12th Street to Northup Way</b> Widen this section from 3 lanes to 5 lanes.
<b>302</b>	<b>NE 24th Street at Bellevue Way</b> Provide a westbound to northbound right-turn lane and prohibit north to west and south to east turning movements during the p.m. peak period on Bellevue Way NE.
<b>303</b>	<b>114th Avenue SE from Main Street to SE 8th Street</b> Widen this section from 2 to 3 lanes and provide an additional southbound lane on 114th Avenue SE at the SE 8th Street intersection, in coordination with I-405 widening.
<b>304</b>	<b>112th Avenue SE/Bellevue Way Intersection</b> Extend the northbound right-turn lane and rechannelize the intersection to favor traffic flow to 112th Avenue SE.
<b>305</b>	<b>108th Avenue SE between Main Street and Bellevue Way</b> Provide traffic control measures on 108th SE to discourage through traffic on this street. Specific measures should be developed through a neighborhood traffic control program involving residents of that area.
<b>306</b>	<b>124th Avenue NE at Bel-Red Road</b> Provide northbound to westbound and southbound to eastbound left-turn lanes (as proposed in the Bel-Red Subarea Plan).
<b>307</b>	<b>NE 12th Street/116th Avenue NE Intersection</b> Provide a northbound to eastbound right-turn lane and eastbound to northbound dual left-turn lanes.
<b>308</b>	<b>Bellevue Way SE 30th Street to I-90</b> Provide additional southbound and northbound thru-lanes when a traffic signal is installed at the SE 30th Street/Bellevue Way intersection.
<b>309</b>	<b>SE 8th Street at 114th Avenue SE/118th Avenue SE</b> Provide HOV and other traffic improvements.

## Transit

- |            |  |
|------------|--|
| <b>401</b> | <b>Downtown</b><br>Construct the transit facilities within Downtown needed to support the projected level of transit service and ridership consistent with the Downtown Implementation Plan. |
| <b>402</b> | <b>NE 6th Street between 112th Avenue NE and I-405</b><br>Construct a new transit center.  |
| <b>403</b> | <b>Downtown</b><br>Provide a transit circulator for access to restaurants, shopping, parking and places of employment throughout Downtown and possibly to adjacent activity areas.           |
| <b>404</b> | <b>Region</b><br>Construct new park-and-ride lots specifically aimed at providing transit service to Downtown Bellevue.  |
| <b>405</b> | <b>Downtown</b><br>Improve transit stop facilities and amenities for transit riders.   |
| <b>406</b> | <b>108th Avenue NE</b><br>Add a northbound contraflow curb lane for buses only between NE 4th and NE 8th.  |
| <b>407</b> | <b>108th Avenue NE</b><br>Add a southbound curb lane for buses only between NE 10th and Main.  |

## Parking

- |            |  |
|------------|--|
| <b>501</b> | <b>Downtown</b><br>Allow on-site parking requirements for new buildings to be met by off-site parking facilities. Such facilities should be strategically located to reduce traffic congestion.                    |
| <b>502</b> | <b>Downtown</b><br>Facilitate the construction of garages for short-term parking (if, following study, such facilities are determined to be the most appropriate option to address to short-term parking problems. |
| <b>503</b> | <b>Downtown</b><br>Implement a parking guidance system to more efficiently utilize the Downtown parking supply.  |

## Pedestrian & Bicycle Facilities

- |            |  |
|------------|--|
| <b>601</b> | <b>Downtown</b><br>Improve pedestrian facilities by completing a network of sidewalks in Downtown where they are missing, providing connections from surrounding neighborhoods, enhancing pedestrian signals and crosswalks, and removing obstacles on sidewalks. The interim sidewalk width where they are currently missing should be at least 8 feet.                                 |
| <b>602</b> | <b>Downtown</b><br>Develop policies and standards which can be used to identify and evaluate appropriate locations for mid-block pedestrian crossings. Provide mid-block pedestrian crossings with a signal as a need arises.  |
| <b>603</b> | <b>Downtown</b><br>Designate bicycle routes through Downtown, as shown on the Pedestrian and Bicycle Transportation Plan, and including routes on Main (as part of the Lake-to-Lake Trail), on or adjacent to NE 6th from Bellevue Way to 114th NE, 100th NE, 106th NE from Main to NE 10th, 108th NE, 112th NE, 114th NE south of NE 6th, and Bellevue Way north and south of Downtown. |

**604**                    **Downtown**  
Implement provisions to encourage the developers and owners of Downtown buildings to provide long-term bicycle parking and storage and showers/lockers for employees and short-term bicycle parking for visitors.

**Parks and Open Space**

**701**                    **Downtown**  
Complete development of Downtown Park in accordance with adopted Master Plan.

**702**                    **Downtown**  
Improve Ashwood Park with guidance from an updated master planning process when funding becomes available.

**703**                    **Downtown**  
Acquire land and develop a neighborhood park in the Northwest Village District.

**704**                    **Downtown**  
Acquire land and develop a neighborhood park in the East Main District.

**705**                    **Downtown**  
Develop a graceful connection from Downtown to Meydenbauer Bay.

**706**                    **Downtown**  
Acquire land and development a linear green buffer on the south side of Main Street between 112th SE and 110th SE.

**Gateways & Wayfinding**

**801**                    **Downtown**  
Implement a phased Downtown wayfinding system for pedestrians, bicycles, and automobiles that reinforces city identity as well as unique characteristics of Downtown neighborhoods as appropriate.

**802**                    **Downtown**  
Develop gateways into Downtown at identified locations through private development and public investment.

**Municipal Buildings**

**901**                    **Downtown**  
Relocate civic functions to a Downtown campus.



# **BELLEVUE PARKS & OPEN SPACE SYSTEM PLAN**

**2010**



**City of Bellevue  
Parks & Community Services**

**425-452-6885**

**[www.bellevuewa.gov/parks-community-services.htm](http://www.bellevuewa.gov/parks-community-services.htm)**

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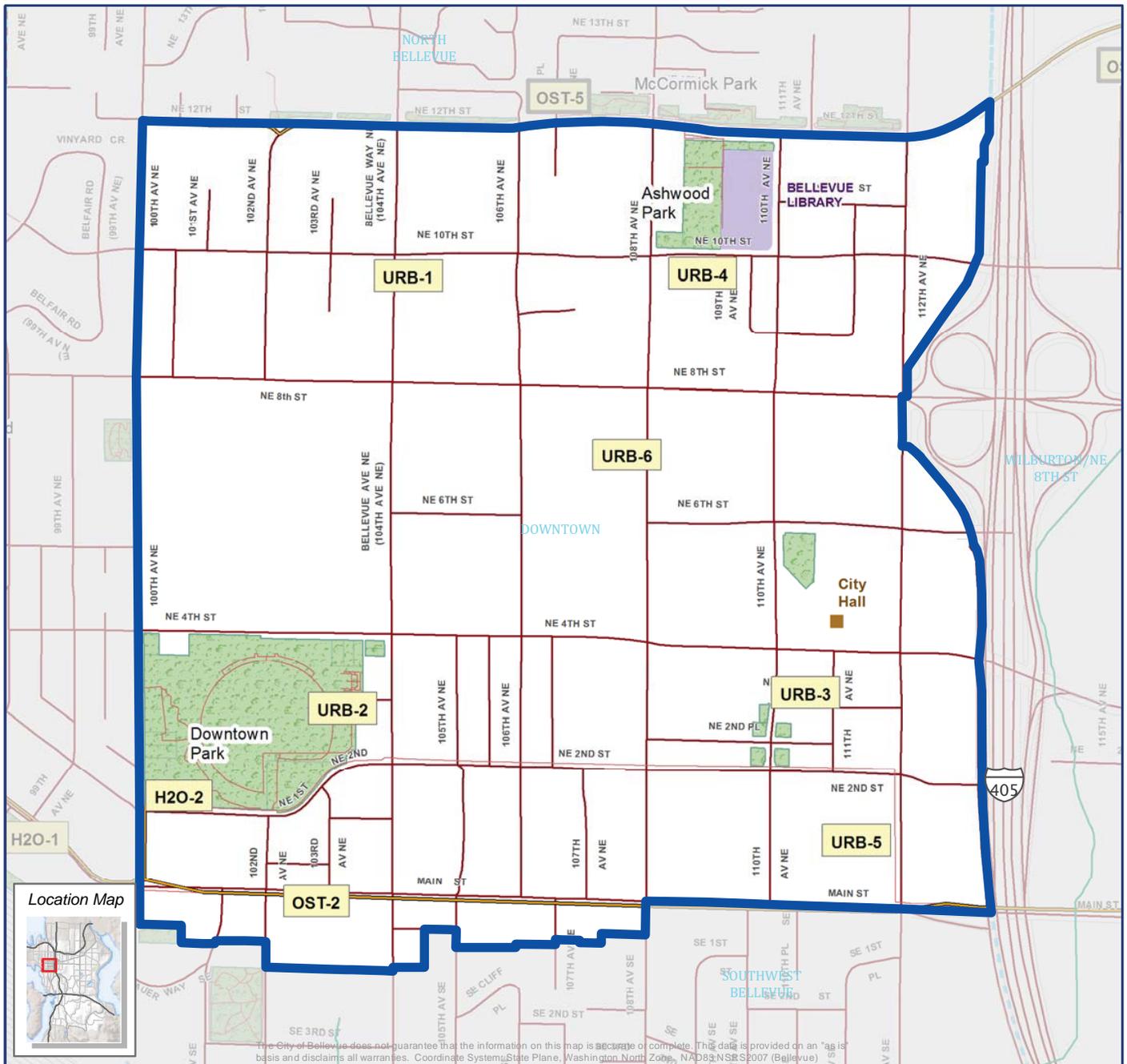
*For print version users,  
Appendix III includes  
full addresses for  
all hyperlinks found  
throughout the plan.*

<b>Downtown</b>		Total Acres	Waterfront (ft)	Soccer / Football type Sportfields	Multi-use (overlay) Sportfields	Baseball / Softball Fields	Play Areas	Tennis Courts	Basketball Courts	Picnic Shelters	Commy. Recreation Center / School	Community Building(s)
<b>Public Park and Open Space System</b>		<b>26.59</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>
Community Parks												
City Hall (Plaza)		0.89										1
Downtown Park		21.36					1					
Neighborhood Parks												
Ashwood Park <i>(including 2 - plaza easements)</i>		3.69		1								
Open Space / Undeveloped / Maintenance												
Corner 5 Parcels (NE 2nd)		0.65										
City Properties		0.00										
Trail Systems												
Lake to Lake Greenway Trails*												
Bel-Red Greenway Trails*												
<b>Public School Sites / Library</b>		<b>2.54</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>
Bellevue Regional Library (KC)		2.54										1
<b>Private Schools / Parks</b>		<b>0.00</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

\* In multiple Subareas

‡ Half of total property contributes to Neighborhood and Community Park level of service considerations

# Downtown Subarea



## Current Statistics

Subarea Size	411 Acres
Population for 2010	5,050
Projected Population for 2030	13,625
Public Parks and Open Space	26.59 Acres
Public Schools/Libraries	2.54 Acres
Trails	3.03 Miles

## Households within 1/3 mile walking distance to the entrance of:

A Park or Trail	74 %
Public School Grounds only	0 %

## Recommendations

- OST-2: Lake to Lake Greenway Trails: Multi-use Connections
- URB-1: Downtown Subarea: Add Neighborhood Park in NW Quadrant
- URB-2: Downtown Subarea: Downtown Park
- URB-3: Downtown Subarea: Add Neighborhood Park in SE Quadrant
- URB-4: Downtown Subarea: Ashwood Park
- URB-5: Downtown Subarea: Main Street Linear Buffer Greenway
- URB-6: Downtown Subarea: Add Community Center
- H2O-2: Meydenbauer Bay Park to Downtown Park Connection

**ABC-01 Park Capital Recommendations**

- Greenway Trails System
- Parks & Open Space (City Owned)
- Trails
- Downtown Subarea
- Public School Properties/Libraries
- Private Parks/Private Schools
- City Limits
- Unincorporated

1 in = 0.14 miles



# Meydenbauer Bay

## PARK AND LAND USE PLAN

**EDAW** | AECOM  
November 2009  
**DRAFT**

# thank you!



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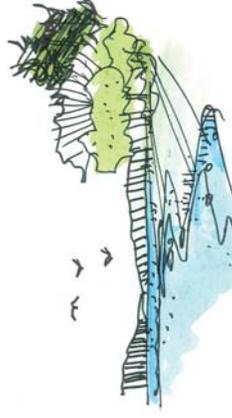
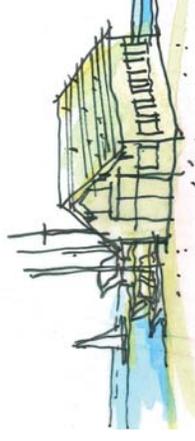
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## Executive Summary



DRAFT

## CHAPTER 1: EXECUTIVE SUMMARY

The City of Bellevue has long had a vision of connecting the Meydenbauer Bay waterfront to Downtown Park to create a signature park and waterfront destination. In 1953 King County conveyed a small parcel to the City to use for park purposes. With acquisition of a small adjacent parcel soon after, the City improved the existing Beach Park. In 1987 the City's Park, Recreation, and Open Space Plan identified acquisition of the Meydenbauer Bay waterfront as a major focus to provide unequaled waterfront amenities and connect the waterfront to Downtown Park and the downtown. Since the late 1990s, Bellevue has acquired land along Meydenbauer Bay as it became available, to expand Meydenbauer Beach Park and provide an important recreational opportunity for its citizens. The City Council recognized the need to plan for the ultimate goal of achieving a connection of this key waterfront area to the downtown area and enhancing the surrounding area.

In 2007, the City Council appointed the Meydenbauer Bay Park and Land Use Plan Steering Committee (Steering Committee) to serve in an advisory capacity to the Council, Planning Commission and Parks Board. Representing various neighborhoods and citywide interests, the Steering Committee was charged with guiding staff in public outreach and development of draft alternatives for both the park master plan and the neighboring upland area, evaluating those alternatives, and ultimately recommending final land use and



**A** Figure 1.0-1: Historic Ice House and Whaling Building at Bellevue Marina

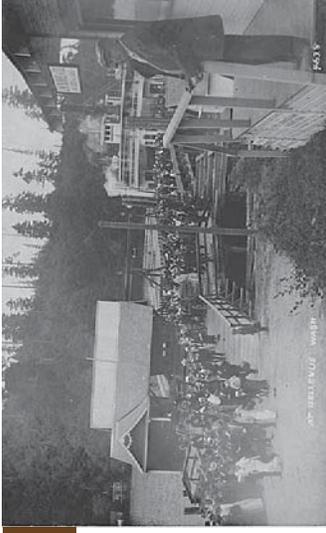
park master plan alternatives and actions to implement the vision. The Steering Committee's work was guided by broad planning principles approved by the Council for the project.

The City initiated its planning process for the Meydenbauer Bay Park and Land Use Plan (the Plan) in early 2007, which resulted in a Preliminary Preferred Land Use Plan (PPLUP) for land uses and development intensity in the upper block and south of Main Street portion of the study area (Sasaki 2008). When the City continued its planning process with a focus on the new waterfront park in May 2008, it also focused on reintegrating the new park and uplands, with greater attention to the edge condition and relationship of these two important components of the Plan.

The Plan illustrates potential building masses, siting, relationships, and concepts for the upland area that provide pedestrian connections between the new waterfront park and upland areas, as well as physical and interactive spaces and amenities that reinforce the pedestrian experience and the connection of the waterfront to nearby upland areas. The Plan envisions the closure of 100th Avenue SE, and coordinated redevelopment of several parcels of land under different ownerships (including one City-owned parcel). The Plan proposes modest policy and regulatory changes to provide some degree of incentive (other than increasing building height or allowing new uses) that could improve the pedestrian environment along the edges of the upper block.

The Plan calls for the development of a memorable waterfront park that will attract people year round with a variety of uses and thoughtfully designed places that respect the setting, express community environmental values, and sup-

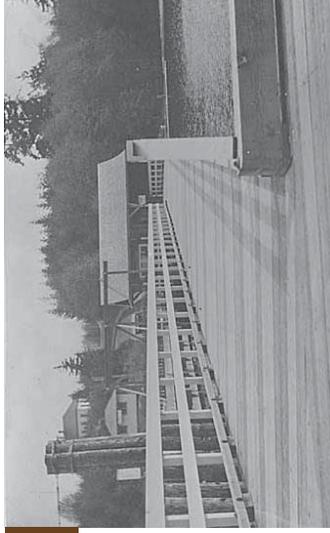
port a range of active and passive activities. The waterfront and park are planned to be both a respite within the city and a connection between the city and the lake. The park will be a pedestrian place that encourages contemplation, socializing, and recreation; welcoming visitors who arrive by boat, car, bus, and bicycle or on foot. Streetscape improvements are recommended for several nearby streets with an emphasis on improving pedestrian amenities, safety, and connections to surrounding neighborhoods, Downtown Park, Old Bellevue, and downtown Bellevue.



**B**



**A**



**C**

**A** Figure 1.0-2: Whaling Boats Docked in Meydenbauer Bay, Bellevue, ca. 1937 (Image Courtesy Eastside Heritage Center, L85.39.1)

**B** Figure 1.0-3: Passengers Disembarking from Ferry, Bellevue, 1914 (Image Courtesy Eastside Heritage Center, 1995.123.02)

**C** Figure 1.0-4: Bellevue Ferry Dock, Bellevue, ca. 1917 (Image Courtesy Eastside Heritage Center, 2002.125.03)

**D** Figure 1.0-5: Meydenbauer Park in Bellevue in 1936 (Image Courtesy Eastside Heritage Center, L84.13)



**D**

## 2.5 Steering Committee Recommendations

The Steering Committee was appointed by the Mayor, and approved by the City Council to serve in an advisory capacity to the Council, the Parks Board, and the Planning Commission. Their work over a two and one half year period consisted of guiding the development of draft alternatives for both the Park Master Plan and the neighboring upland area, evaluating those alternatives, and ultimately selecting final land use and park master plan alternatives and identifying actions to implement the vision. Their work was to culminate with final reports summarizing the recommendations of the committee on both the land use and park master plan project components.

The Steering Committee's recommendations follow in a letter to the Council that records and embodies concepts, concerns, and caveats related to their vision and to specific Plan elements. Their recommendation also summarizes the more challenging issues addressed by the committee and their conclusions on those issues, and acknowledges public concerns regarding several existing conditions in the vicinity of the proposal. Finally, the Steering Committee Recommendations section summarizes the concepts embedded in the Plan. Some of these concepts originate in the Planning Principles, while others evolved throughout the process. Together with the Plan-specific recommendations, the summary of Plan concepts is intended to memorialize the features and nuances of the Plan that were important to the committee.



## Meydenbauer Bay PARK AND LAND USE PLAN

November 19, 2009

The Honorable Mayor Grant Degginger  
Members of the Bellevue City Council  
450 110th Avenue NE  
Bellevue, WA 98004

### RE: Steering Committee Recommendation – Meydenbauer Bay Park and Land Use Plan

Dear Mayor Degginger and City Council members:

On behalf of the Meydenbauer Bay Steering Committee, consisting of co-chairs Doug Leigh and Iris Tocher, and members Stefanie Beighle, Hal Ferris, Betina Finley, Merle Keeney, Marcelle Lynde, Bob MacMillan, David Schooler, Tom Tanaka, Stu Vander Hoek, and Rich Wagner, we are pleased to transmit to the City Council the committee's recommendation for the Meydenbauer Bay Park and Land Use Plan (Plan). The Council is presented with an extraordinary opportunity to create a unique and memorable experience for future generations of Bellevue residents. We believe the Plan recommended by the committee fulfills the vision embodied in the Bellevue Comprehensive Plan and Parks & Open Space System Plan, and provides a strategy for the delivery of a spectacular public asset that capitalizes on the community's patient and steadfast pursuit of this vision. The committee thanks the Council for the clear and comprehensive guidance provided in our Steering Committee Charge and the project's Planning Principles, which enabled us to develop a Plan that is both visionary and respectful of its unique setting.

The steering committee's recommendation for the proposed Plan, and for specific aspects of the Plan, is presented below. In addition, the committee identifies several concerns for you to consider regarding existing conditions that were raised repeatedly in public comment. While it was not the committee's charge to address these concerns which exist with or without the proposed Plan, the concerns provide context for the Plan. Recognizing and addressing these concerns may help to narrow focus to the Plan itself. The committee discusses these concerns in the "Broader Issues" section that follows the steering committee recommendation.



A

## Recommendation

**A. The steering committee recommends adoption of the Meydenbauer Bay Park and Land Use Plan, as identified and evaluated by the Final EIS as the “preferred alternative”, as summarized below, and as presented in the Draft Meydenbauer Bay Park and Land Use Plan dated November, 2009.**

The Plan envisions an extraordinary community-wide public asset. The new waterfront park will be a pedestrian place that encourages contemplation, socializing, and recreation; welcoming visitors who arrive by boats, car, bus and bicycle or on foot. People will be attracted year around by the variety of uses and thoughtfully designed places that respect the setting, express the community’s environmental values, and support a range of both active and passive activities. Public access to the waterfront will be greatly increased for viewing, swimming, wading, boating, and walking on fixed and floating piers. The Plan provides a variety of opportunities upland of the shoreline as well, including picnicking, informal play, walking, and viewing. Two public gathering spaces are included to ensure that the park enjoys year-round use. The park moves from its most natural at the west end, maintaining and improving the ecology of the ravine in the existing Meydenbauer Beach Park, and culminates in an urban connection in the entry plaza at 100th Ave SE and Main Street.



B

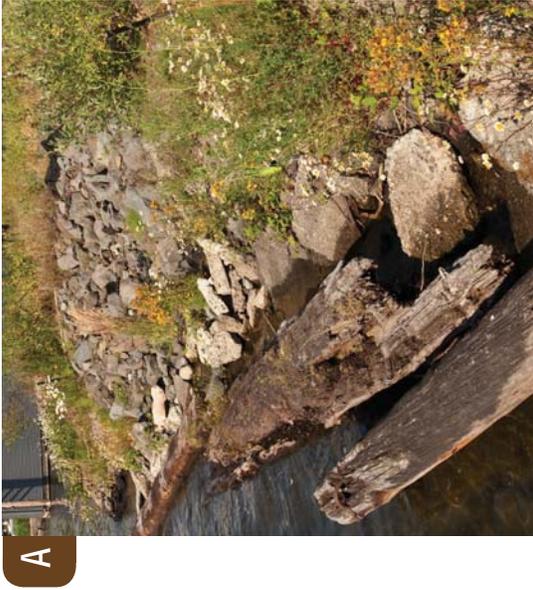
ing the recreation pier will maximize tranquil enjoyment and interpretive opportunities in the ravine, the most natural zone of the park. Much of the existing shoreline, 800 linear feet of the park’s 1250-foot shoreline, will be softened and restored. Along with shoreline restoration, natural and engineered storm water treatment facilities, and significant reduction in impervious surface will improve water quality. Reduction of over-water coverage by nearly 35%, and use of current standards and best management practices for new over water piers and floats, will improve fish habitat.

The Plan concentrates active recreation such as swimming, play areas, picnicking, and the activity building in the central portion of the new park. The activity building will provide year-round opportunities for recreation and viewing regardless of weather. The marina is adjacent to the central recreation area, but is across 99th Ave NE. Short term parking for marina users and accessible parking stalls are located at the marina. The promenade, which parallels the shoreline, provides a pedestrian connection at the foot of the slope that avoids conflicts with cars.

Through reconfiguration of the piers, the Plan balances the desire to continue long term moorage with the goals of providing public access and restoring the shoreline, as well as with the funding requirement to provide at least 14 slips of transient moorage. The marina maintains opportunities for long term moorage on an expanded Pier 1 and removes Piers 2 and 3. The boating community is broadened with the addition of the transient moorage slips and canoe and kayak rental, launch, and tie up. Additionally, flexible pro-

**A** *Figure 2.5-1: Example of Park Activities*

**B** *Figure 2.5-2: Example of Park Activities*



gramming proposed for the historic Whaling Building on Pier 1 will include maritime, historic, and cultural activities that augment boating activities. Over-water structures connect the marina with the adjacent uses on both sides. On the west, the curved pedestrian pier separates the swim beach from boats and on its east side, accommodates canoe and kayak tie ups. A floating boardwalk connects Pier 1 near the Whaling Building to a pedestrian pier at the foot of the entry plaza where Pier 3 is currently located. The south side of the floating boardwalk provides moorage for transient boaters, and the east side of the pedestrian pier is kept free of boaters in consideration of the Meydenbauer Bay Yacht Club's youth sailing program which operates from their westernmost pier. The area between the promenade and the floating boardwalk will provide an excellent opportunity for contiguous shoreline restoration and shallow water habitat.

At the new park's east end, the Plan calls for an entry plaza that connects the park and Meydenbauer Bay to Wildwood Park, Old Bellevue, Downtown Park, and adjacent neighborhoods. The Plan calls for closing 100th Ave SE/SE Bellevue Place to vehicle traffic and using the steep grade of the right-of-way to develop a series of plazas, walkways, grand stairways, and public spaces with water features and public art. The street level plaza at 100th Ave and Main St provides pedestrians and visitors with wide open views of the Bay, the boats, Pickle and Groat Points, Lake Washington, and Seattle. Below the plaza is space for structured parking. Midway down the slope a gathering space which will invite viewing and visiting during inclement weather. An elevator



from the garage will facilitate ADA access to the gathering space, and an elevated pier will stretch from the gathering space to the shoreline, ultimately ending with access to the pedestrian pier and floating boardwalk.

The Plan suggests modest changes to land use in the adjacent upland areas, providing the opportunity for corridors that visually open up the waterfront from Downtown Park and facilitate pedestrian movement to the waterfront, Old Bellevue, and Wildwood Park. The land use concepts pertain to two geographic subareas within the study area, lying upland of the waterfront area. The two subareas are identified as "Upper Block" and "South of Main".

In the Upper Block, redevelopment of aging property incentivized by increasing allowable density is intended to provide improved streetscapes and pedestrian amenities, and recognize existing densities that commonly exceed current zoning allowances. In the area South of Main, coordinated redevelopment of several parcels is intended to result in public spaces and pedestrian connections through the block, helping connect the new waterfront park with Old Bellevue and also with Wildwood Park. It also envisions shared underground parking accessed both from Main Street and 101st Avenue SE.

**A** Figure 2.5-3: Example of Restored Shoreline (EDAW AECOM)

**B** Figure 2.5-4: Improved Streetscape along Main Street/Lake Washington Boulevard Connects to New Entry Plaza.

**B. The committee recommends the closure of 100th Avenue SE/SE Bellevue Place to vehicles, to provide a gateway to the new park and connections to Old Bellevue, Downtown Park, Wildwood Park, and adjacent neighborhoods. Recognizing that the EIS transportation analysis and conclusions were based in part on the assumption that it would be completed within the analysis time horizon (year 2020), and that it could have positive effects in terms of relieving traffic congestion on Main Street, the committee also recommends that the NE 2nd Street improvement project (CIP Plan No. PW-R-150) be completed prior to the closure.**

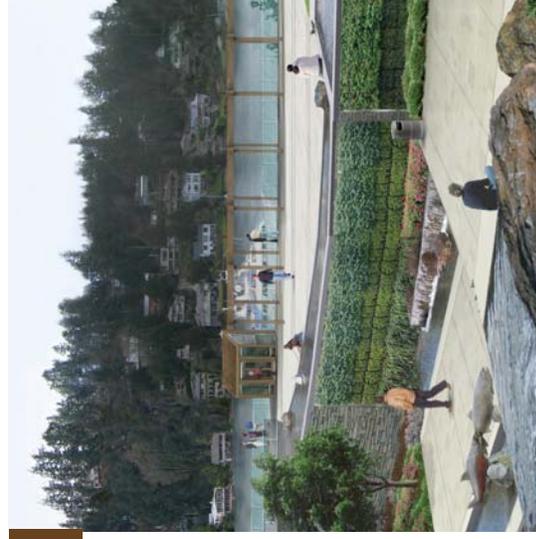
The Plan recommends 100th Avenue SE/SE Bellevue Place be closed to vehicle traffic and the right-of-way incorporated into the primary pedestrian access which begins at the entry plaza, and through a series of terraces, walkways, stairways, and public spaces with water features and public art connects the waterfront and the new park to Main Street and Downtown Park. This aspect of the plan has been very controversial among area residents. The committee evaluated options that would keep the road open for general vehicle use, but concluded that the closure greatly benefits the park user's experience and the park's connectivity, and advances the pedestrian priority promoted by Planning Principle No. 5:

*"The park and its connections should be places that can be enjoyed by pedestrians without fear of conflicts with automobiles. Where vehicle drives or parking areas are necessary, they should be designed and located to promote a "pedestrian first" message."*

Before reaching consensus on this aspect of the plan, the committee insisted on and received detailed transportation analysis. A preliminary analysis of the effects of road closure was provided to the committee by the project consultants in October 2008. That analysis indicated that the road closure would not lead to any unmitigatable adverse impacts. More detailed quantitative analysis was provided in the Environmental Impact Statement which confirmed that no significant adverse impacts would result from road closure. Even so, the committee feels that addressing overall traffic management in the area is important. Specifically, the committee believes that completion of the NE 2nd Street improvement project is vital to accommodate the closure of 100th Ave SE/SE Bellevue Place, as that project will help relieve congestion on Main Street. The committee made sure that the Plan includes options for all properties in the immediate vicinity, particularly the Ten Thousand Meydenbauer Condominium, to have continued adequate access for residents, guests, and services (including emergency services).



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**B**

**A** Figure 2.5-5: View of 100th Avenue SE

**B** Figure 2.5-6: Simulation of Improved Pedestrian Access to Waterfront along 100th Avenue Corridor



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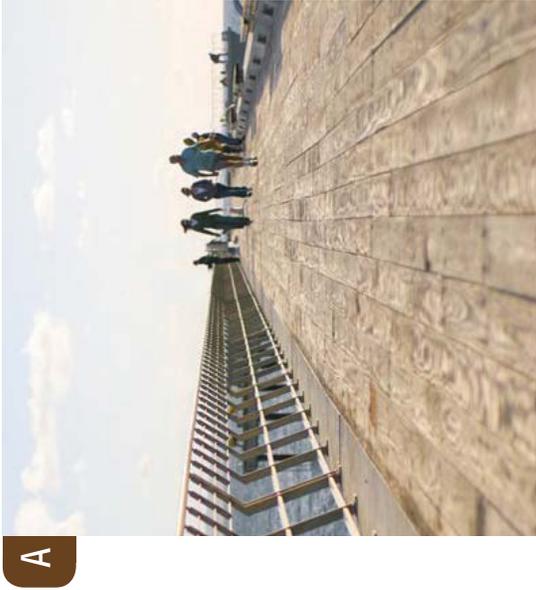
**C. The committee recommends reconfiguration of the marina, removing Piers 2 and 3, and expanding Pier 1 (alternatively Pier 2 could be shortened to remove its connection to the shoreline and connected to Pier 1 as a finger pier).**

The committee recognizes that this will reduce long term moorage from 87 slips to a range of 38 to 48 slips, but it will allow a balance of several interests, including:

- Provide at least 14 transient moorage slips required by a funding source obligation (in addition to the 38 to 48 leasable slips);
- Increase the spectrum of Bellevue boaters served by the park, by providing opportunities for transient moorage and people powered vessels such as canoes and kayaks;
- Allow for removal of the hardened shoreline and restoration to improve fish habitat and water quality;
- Improve overwater experiences for non-boaters; and
- New overwater structures to be designed and constructed with be more compatible with the bay and fish populations.

Several concerns were raised by the public with respect to transient moorage. While there was some objection to the provision of any transient moorage, most people understood that provision of at least 14 moorage slips is a requirement of a funding source used to purchase the marina. Also, some public comments suggested locating the transient moorage northwest of Pier 1. However, the transient moorage requirement must be met on the properties which the funds helped purchase, i.e., the Yacht Basin and/or Meydenbauer Marina, now collectively known as the Bellevue Marina at Meydenbauer Bay. Therefore, suggestions to locate the transient moorage northwest of Pier 1 would not meet this requirement. The floating boardwalk provides a logical location for transient moorage, with convenient access to both park facilities and downtown amenities, while providing separation of transient moorage from long-term moorage.

**A** Figure 2.5-7: Reconfigured Marina - Expanded Pier 1



**A** *Figure 2.5-8: Example of Public Pedestrian Pier (EDAW/AECOM)*

**D. The committee recommends multiple and varied opportunities for the public to access the shoreline and provide connectivity within the project area.**

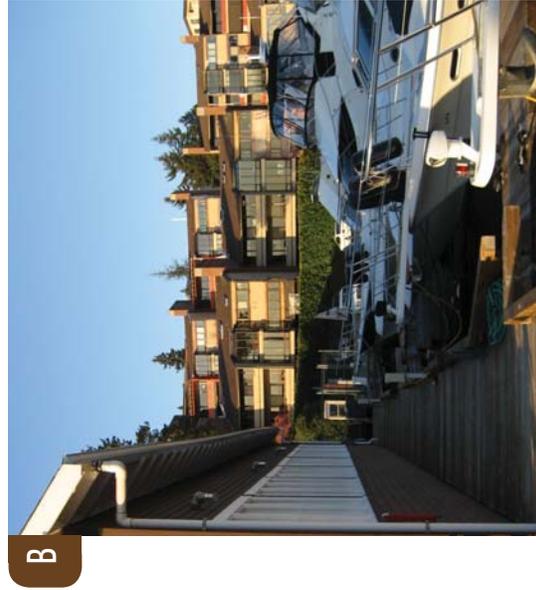
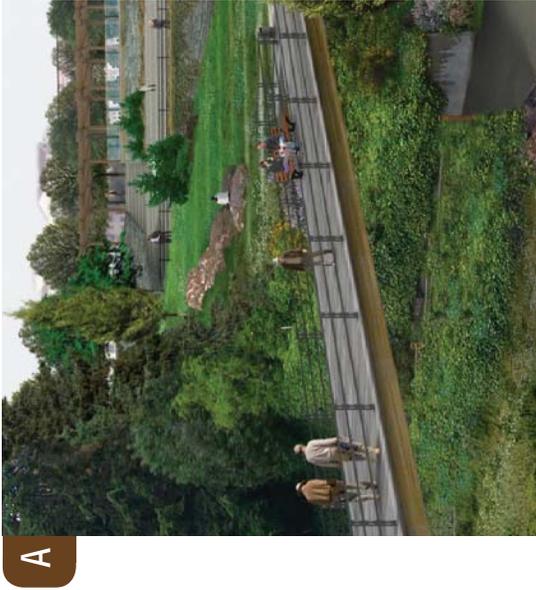
Several means of providing public access to and through the park, and to and over the water were evaluated by the EIS and the steering committee. In the Upper Block, redevelopment of aging property incentivized by increasing allowed density is intended to provide improved streetscapes and pedestrian amenities. This can help improve not only the immediate pedestrian environment, but also strengthen the connection from adjacent neighborhoods to the waterfront. A water feature extending from Downtown Park to the waterfront will provide a powerful visual connection and invitation to explore both parks. In the area South of Main, coordinated redevelopment of several parcels is intended to result in public spaces and pedestrian connections through the block, helping connect the new waterfront park with Wildwood Park.

In the new park itself, the committee’s recommendation provides a range of opportunities for pedestrian movement. Key public access elements include:

- An elevated viewing platform, extending from the entry plaza at Main St and 100th Ave to the shoreline edge, provides visitors with spectacular views of the bay. It passes over both the Vue Condominium driveway entrance and a new shoreline promenade, at an elevation that ensures proper clearance for emergency service vehicles below. Visitors could use an elevator to de-

scend from the platform to the shoreline. This provides an accessible route that responds to the site’s steep topography.

- A public pedestrian pier located at the base of the elevated viewing platform connects with a meandering floating boardwalk that runs roughly parallel to the shoreline, connecting at its western terminus to Pier 1. The floating boardwalk serves several purposes: it provides required moorage for transient boaters, offers the non-boating public an opportunity to get out over the water, expands the “real estate” of this narrow part of the park, and moves some of the pedestrian and boating activity farther from adjacent condominium residents.
- A curved pedestrian pier located west of the marina arcs out toward the lake and provides added opportunity for pedestrians to walkout over the water and enjoy striking views of the bay and the mountains beyond, the Bellevue skyline, and the new park itself. This pier also provides tie-up space for canoes and kayaks, and physically separates the swim beach from boating areas.
- A hand launch path is provided for canoes and kayaks, located between the curved pedestrian pier and Pier 1.
- A shoreline promenade provides a continuous walkway along most of the shoreline. It will be designed to accommodate emergency service vehicles for access to and through the park, and for providing emergency services to adjacent residences.



**F.** The committee recommends that the elevated walkway beginning mid-slope below the entry plaza and terminating in an elevated viewing platform be designed to maximize and enhance views of the water and distant vistas; be of an appropriate scale that is compatible with its surroundings; extend to the shoreline edge and connect to the floating boardwalk; and be as light and transparent as possible.

The committee feels this platform is an important park feature, and one which will contribute to a remarkable and memorable shoreline experience capitalizing on the spectacular view opportunities offered by the bay. The committee recognizes that members of the public have concerns regarding the visual mass of the elevated viewing platform. The success of this feature will depend in part on its final design which will be determined at the project level. It is important that this feature be of a very high quality of design, consistent with Planning Principle 7, Superior Design, which states:

*“The park should be reinforced, communicated, and celebrated through high quality urban design, landscape architecture, building design, and streetscape treatment, not only within the park itself but also throughout nearby public spaces and park connections. The plan should reflect a high standard of excellence.”*

Design elements to consider for this element might include:

- A “step-down” prior to reaching the shoreline edge (while still providing necessary clearance for emergency vehicles and driveway access to the Vue Condominiums);
- A staircase in place of an elevator;
- Relocating the elevator structure northward, further from the shoreline edge.

**F.** The committee recommends that the expanded Pier 1 maintain as many long-term moorage slips as possible, after providing for pedestrian access to the water, shoreline restoration, and at least 14 transient moorage slips.

The committee recognizes that the floating boardwalk between the pedestrian pier and Pier 1 will be designed and located as necessary to comply with regulations in effect at the time of its construction, including the City’s updated Shoreline Master Program. Should those regulations require locating that boardwalk farther away from the shoreline, the design of Pier 1 and its moorage capacity could be affected. Therefore, the committee recommends that, if adjustments to the boardwalk are necessary, they should occur in a manner that maintains as many long term moorage slips as possible, and still provides pedestrian access to and over the water, allows shoreline restoration, and at least 14 transient boat slips as required by funding sources.

**A** Figure 2.5-9: Elevated Viewing Platform Provides Unobstructed Views of the Bay.

**B** Figure 2.5-10: Bellevue Marina Pier 1

**G. The Committee recommends that Comprehensive Plan and Land Use Code amendments be drafted to implement the modest land use changes included in the Plan and described below.**

The purpose of the land use study was to explore market-based land use incentives and zoning standards that would support redevelopment and encourage through-block public corridors for views of, or pedestrian access toward, the water. The committee held six meetings focusing on land use in the upland area to understand how much incentive would be necessary to encourage redevelopment and public corridors and spaces, and to evaluate what level of incentive would be acceptable.

Planning Principle 3, Complementary Land Use, provides that:

“Urban design and land uses in the upland area adjacent to the park should be pedestrian-oriented and serve the broader community to make the transition from the upland to the shoreline seamless, enjoyable, inviting and compelling. They should draw the pedestrian toward the water, convey a sense of excitement, and provide an interactive experience between the waterfront and upland area”

Ultimately, in the Upper Block the Committee reached two critical conclusions. First, due to topography and the location of nearby buildings, opportunities for ground-level views of the bay from NE 1st Street were limited at best. Second, the development intensity necessary to persuade

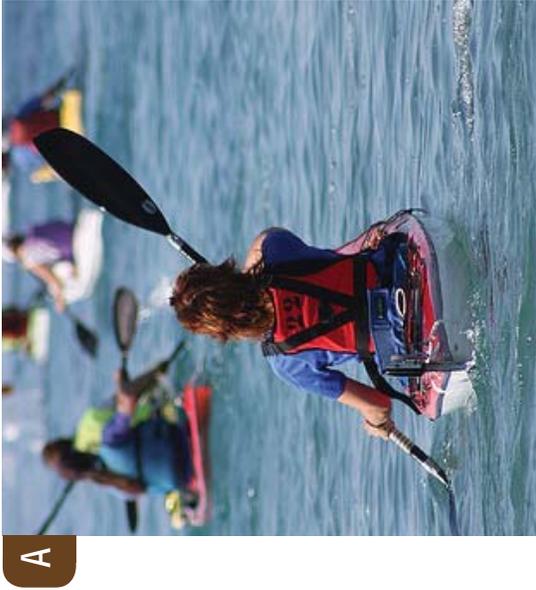
property owners to redevelop properties and create a significant view and pedestrian corridor was so great in the context of the current market that it would result in buildings having much greater bulk and height than those on surrounding properties. The Steering Committee determined that such buildings would be unacceptable in this location and would provide too little public benefit.

Additionally, aging apartment structures in the Upper Block exceed current density limits, reducing the potential for incentives to encourage redevelopment. Recognizing this as well as the committee’s resolve to maintain existing height limitations, the recommended plan incorporates incentives not for the purpose of creating through block corridors, but to help improve the edge treatment along public walkways through implementing the following concepts:

- Incentivize redevelopment of existing structures to provide public benefits.
- Incentives should maintain existing height limits and existing allowable uses.
- Redevelopment should achieve improved architecture and pedestrian amenities (consistent sidewalk system, landscaping, surface treatments, benches, signage, public art).
- Incentives to consider include increased density (to approximately 60 dwelling units per acre – control through FAR), modest relaxation of coverage and setback development standards to accommodate the increased density, and reduced parking requirements.

There are three parcels in the subarea South of Main that will likely redevelop in the foreseeable future. The Chevron Station and Meydenbauer Apartments are privately owned, and the east Bayvue Village Apartments parcel is owned by the City of Bellevue. To encourage coordinated redevelopment among all three property owners, the recommended plan incorporates the following concepts:

- Facilitate coordinated redevelopment to provide public benefits.
- Incorporate 100th Avenue SE into the pedestrian connection from Main Street; close road to vehicles but ensure emergency and service vehicle access to adjacent properties.
- Change the character of Meydenbauer Way SE to be more “pedestrian friendly”, by emphasizing pedestrians but still providing access for vehicle use, access, and parking.
- Redevelopment should promote shared underground parking with access from Main Street and 101st Avenue SE, provide pedestrian connection to Wildwood Park, and use water as unifying theme and to strengthen connections.
- Incentives to consider include increased density (to approximately 60 dwelling units per acre – control through FAR) on multi-family parcels, southerly expansion of retail uses east of 100th Avenue SE, modify development standards to accommodate the increased density, and parking controls (maximum limits).
- Incentives should maintain existing height limits on all parcels.



**A** *Figure 2.5-11: Rental of People Powered Vessels (PPVs)*

**H. The committee recommends that commercial uses west of 100th Ave SE and within the park be limited to vendors, leased moorage, and rental of people powered vessels (PPVs) such as canoes and kayaks. The committee also recommends the following commercial uses for the Upper Block and the South of Main subareas:**

- In both upland areas, continue to allow commercial uses where allowed by existing zoning (i.e., in the DNTN and O districts)
- In the South of Main area, allow limited retail expansion south of the Chevron site (east of 100th Ave SE).

The committee considered various alternatives which evaluated commercial uses within the proposed park boundaries west of 100th Ave SE, including vendor kiosks, café, leased moorage, and rental of people powered vessels (PPVs) such as canoes and kayaks. After considerable discussion, the committee concluded that west of 100th Ave SE, only limited commercial activities are appropriate within the park as similar opportunities are found in close proximity along Main Street. The recommended Plan includes up to six temporary vendors to provide food, non-alcoholic beverages, and/or items for use in the park, but cautions that vendors should use portable rather than fixed carts or kiosks, be located where impacts on neighboring residential properties are minimized, and be restricted to certain hours of operation. The recommended Plan reflects the committee's decision not to include a café. These recommendations are intended to minimize commercial activity within the park to help maintain the tranquility of surrounding residents while still providing water-oriented activities and basic conveniences for park users.

**I. The committee recommends that sufficient on-site parking be provided to meet the demands of a typical day's use at the park.**

Meeting the parking demand of a typical day's use will help avoid spillover parking in surrounding neighborhoods. The peak parking demand on a typical day is estimated to be 149 stalls, based on a review of the Institute of Transportation Engineers (ITE) Parking Generation manual, the City of Bellevue Land Use Code, and estimates prepared by Per-teet, Inc. The recommended plan provides parking for approximately 156 vehicles in a combination of surface parking areas and underground structures, exceeding the peak demand by a small amount. The existing parking lot in the west ravine that now serves the existing Meydenbauer Beach Park would be retained as part of the on-site supply. In addition, 10 short-term parking spaces are included in a vehicle pullout along Lake Washington Boulevard, and 8 short-term and accessible spaces are provided near Pier 1. An additional 8 surface stalls, not included in the on-site 156 stall total, would be located along the west side of 99th Ave NE (in the right-of-way) in close proximity to the swim beach and Pier 1.

The majority of the parking is provided by two underground structures. One is located west of 99th Ave NE, south of Lake Washington Boulevard, and would be integrated with the activity building proposed at that location. Access would be from 99th Ave NE. The other is located west of 100th Ave SE, south of Lake Washington Boulevard, and would be located below the entry plaza. Access to this structure could be provided by both Lake Washington Boulevard from above and Meydenbauer Way SE from below.

Placing much of the parking below grade helps reinforce the pedestrian character of the park, minimize view impacts, and maximize above-grade areas for park purposes. The plan includes construction of a primary pedestrian entry and waterfront connection consisting of plazas, walkways, and stairs that create below grade space that can be captured for parking development southwest of 100th Ave SE and Lake Washington Boulevard.

**J. The Committee recommends that buildings and structures in the park be limited to park uses and be designed to take advantage of the project area steep slope to avoid interrupting views, provide accessibility, and maximize available open space for park use.**

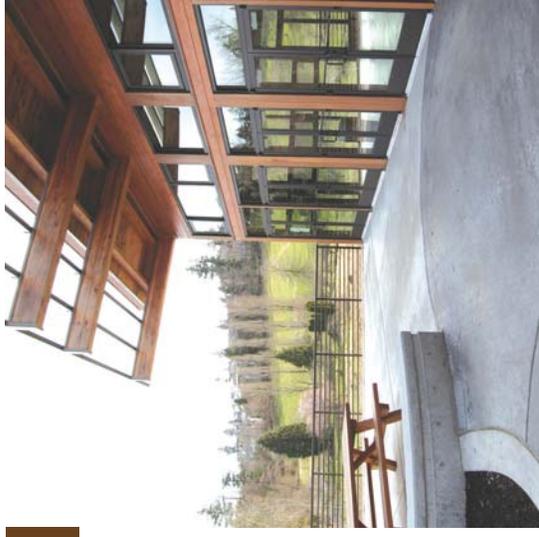
The committee considered and discussed a variety of buildings or structures for the park, including a 4,000 to 8,000 sf activity building; 3,000 sf environmental education center; 3,000 sf café; enclosed (or enclosable) space on the parcel at the west of 100th Ave SE/south of Lake Washington Blvd; retained Whaling building for use as a historic/cultural maritime center; retained ice house for harbor master residence and support, storage or marina office space; and restrooms in appropriate locations.

- The committee supports an activity building of up to 8,000 sf in size, but recommends that the building footprint be limited to 4,000 sf. The activity building provides opportunities for enjoying programmed and unprogrammed activities during all kinds of weather, has the flexibility to accommodate a variety of activities, and increases viewing opportunities.
- The recommended Plan retains the Whaling Building and Ice House, recognizing the heritage and importance of Meydenbauer Bay in Bellevue's history, and providing opportunities for maritime, cultural and historical activities, public art and interpretive programming. Support for this concept was evidenced by both public comment and non-profit organizations who might offer such activities.

- The Plan calls for removal of the existing restroom building at Meydenbauer Beach Park, and the provision of a new restroom building in convenient proximity to the relocated swim beach, given that the existing restroom would be separated from the park activity area and swim beach by the daylighted stream. Other restrooms would be provided in the activity building, and at the marina.

- The Plan also includes some enclosed or enclosable gathering space in the entry plaza at Main Street and 100th Avenue SE to allow enjoyment of the views during inclement weather.

The committee first favored an environmental education center, in response to Planning Principle No. 8 Environmental Stewardship, but eventually chose to incorporate environmental activities in a multi-purpose activity building and the park features themselves. Contributing to that decision was our conclusion that a single purpose environmental education center in the park would replicate the new Mercer Slough Environmental Education Center. Nor does the Plan include a café, because of the park's proximity to Main Street restaurants and cafes.



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**B**

**A** Figure 2.5-12: Lewis Creek Community Center (Miller Hull)

**B** Figure 2.5-13: Whaling Boats Docked in Meydenbauer Bay, Bellevue, ca. 1925 (Image Courtesy Eastside Heritage Center, L 85.39.2)

**K. The Committee recommends that the culverted stream in the existing park's northwest ravine be daylighted from Lake Washington Boulevard to the shoreline.**

The committee proposes that only the lower portion (from Lake Washington Boulevard to the shoreline) be daylighted. The EIS evaluated both an alternative that daylighted the entire stream from the park entry to the shoreline, and the alternative that evaluated a partial daylighting, and concluded that little added environmental benefit would be gained by daylighting the upper portion. The committee feels that the limited additional benefit would not justify displacing the existing parking lot in the ravine.

Daylighting the stream will require the removal and relocation of the existing swim beach, swim beach pier, restroom, and play area. These uses are accommodated in the committee's recommended plan, in a more centralized location near other active recreation uses. The removal/relocation of these features and the daylighting of the lower portion of the stream contribute to the park's transition from a more natural character at its northwest end to a more urban character at its southeast end, and present an opportunity to introduce a stormwater feature with potential environmental and/or educational benefits.

**Broader Issues**

The committee grappled with many challenging issues throughout the course of our work. Some of these issues are specific to the recommended plan or to alternatives that were discussed as part of the process, and were summarized earlier in this Recommendation.

However, several concerns were raised repeatedly throughout the planning process addressing existing issues that extend beyond the scope of the committee's work. While it was not the committee's charge to address these larger concerns, they do provide some context for the Meydenbauer Bay Park and Land Use Plan. These concerns exist with or without the proposed project. The committee offers some actions for the City to consider in response to the concerns listed below. Recognizing and addressing these existing issues may help ease concerns related to the narrower focus of the Meydenbauer Bay Park and Land Use Plan.

**A. Traffic.** There was a great deal of concern expressed about traffic congestion in and near the Downtown, particularly along Main Street. Turning movements to and from Main Street, especially left-turn movements, are reported as often difficult due to vehicle volumes and queues. Limited sight distance at some intersections compounds the difficulty and increases the chance of accidents. Pedestrian crossings and bicycle travel must compete with vehicles. The committee was urged to address these existing issues and ensure that they will not worsen as a result of the Meydenbauer Bay Park and Land Use Plan. Although the EIS concludes that the plan creates no significant unavoidable adverse traffic impacts, the committee recognizes the concerns raised by the public regarding existing traffic conditions and encourages the City to continue working toward solutions for existing and future area congestion.

**B. Parking.** Limited parking exists in and near Old Bellevue. This reinforces the importance of the new park providing enough parking to meet its typical demand. It also suggests

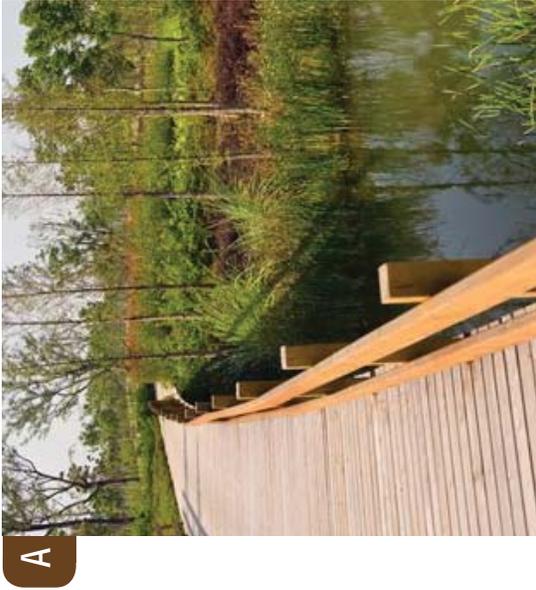
that opportunities be explored for shared or centralized structured parking to help meet growing Downtown parking demand. An underground parking structure in Downtown Park was often suggested as a location for such a structure. Other public facilities in the area might offer similar opportunities.

**C. Water quality.** Many comments were received with respect to aquatic vegetation (e.g., milfoil) and sedimentation in Meydenbauer Bay.

1. **Aquatic Vegetation.** The City currently controls aquatic vegetation at the Bellevue Marina, and coordinates the timing and type of treatment to the extent possible with the adjacent Meydenbauer Bay Yacht Club. This is expected to continue when the new park is completed. However, it is possible that a more comprehensive approach involving most or all shoreline property owners on Meydenbauer Bay, including the City, would improve coordination and effectiveness of vegetation removal measures.

2. **Sedimentation.** Concern was heard about sedimentation in the bay, particularly near storm water outfalls and the mouth of Meydenbauer Creek. The EIS concluded that the proposal would not cause long-term sedimentation impacts, but that does not address concerns about the present conditions. The City could work with the shoreline property owners to evaluate this situation and help develop an appropriate course of action.

D. **Bay enforcement.** The committee heard several complaints about undesirable activity in the bay itself and its negative effects in terms of noise, rude or disrespectful behavior, and lack of compliance with the "No Wake" zone. There was a desire voiced for increased patrolling and enforcement of on-water and in-water activities in the bay, as well as requests to extend the No Wake zone out as far as possible toward the mouth of the bay.



**A** *Figure 2.5-14: Example of a Footbridge over Restored Wetlands (EDAW AECOM)*

### Summary of Recommended Plan Concepts

The recommended Plan incorporates the following concepts that are important to the committee. Some of these Plan concepts originate in and respond directly to the planning principles, others were in response to public comment, and some emerged through committee discussion and review of data, studies, and information requested of city consultants and staff.

- The Plan serves the broad interests of the community at large, and recognizes that the park will be a community-wide asset.
- The Plan is guided by, and is consistent with, the Council-approved Planning Principles.
- The Plan is designed to coordinate with other plans, including the Parks and Open Space System Plan, the Lake-to-Lake Trail, Art Walk, downtown circulator, and Pedestrian & Bicycle Plan.
- The Plan emphasizes environmental stewardship, including stream, wetland and shoreline restoration and enhancement, incorporation of stormwater treatment in project design, minimizing impervious surfaces, and pursuing LEED or similar certification for new structures. Implementing the plan will result in visible ecological benefit.
- The Plan seeks to improve streetscapes throughout the study area, including connections to adjacent neighborhoods, Downtown Park, Wildwood Park, and Old Bellevue, to enhance character, continuity, and wayfinding.
- The Plan ensures appropriate vehicle, pedestrian, and emergency and service access to adjacent residential properties.
- The Plan incentivizes redevelopment of aging structures and coordinated redevelopment to provide public benefits, such as improved streetscapes, public spaces,

- pedestrian connections, and shared parking. The Park transitions from a more natural character (at its northwest end) to a more urban character (at its southeast end).
- The Plan improves the ecology of the site.
- The Plan increases public access to and over the water through careful redevelopment and restoration.
- The park provides a wide variety of user experiences, including on-water, in-water, and near-water recreation, viewing opportunities, and both indoor and outdoor activities.
- The Plan maintains flexibility of on-site programs and structures.
- The Plan fulfills requirements of funding sources used for the purchase of park parcels, including transient moorage quantity and location requirements, impervious surface limitations, and public access to the shoreline.
- The Plan maintains and expands boating opportunities to include a different mix of boat users – long term and transient moorage as well as people powered vessels (PPVs) such as canoes and kayaks.
- The Plan limits commercial uses to the east side of 100th Ave SE, except for temporary or seasonal vendors with moveable or portable carts, PPV rental, and leased moorage.
- The Plan takes advantage of steep topography to minimize the visual impact of structures and provide public views of the water.
- The Plan uses water features to create a strong elemental connection between Downtown Park and Meydenbauer Bay.
- The Plan provides sufficient parking on-site to meet typical park demand.

## Conclusion

As the Council is aware, the steering committee developed this Plan over a period of two and one-half years. We held twenty-one public meetings and six public open houses and workshops, attended two public hearings related to the Draft Environmental Impact Statement, and received and considered a significant volume of oral and written comment from the public. Guided by our Steering Committee Charge and Planning Principles, the committee worked through many issues and ultimately identified a preferred alternative. The preferred alternative was evaluated in the Final EIS and serves as the basis for the Draft Meydenbauer Bay Park and Land Use Plan. The result is a Plan that weaves a unique and memorable waterfront park for all Bellevue residents with the surrounding neighborhoods through pedestrian improvements and modest land use changes, providing connectivity between Downtown Park, Wildwood Park, Meydenbauer Bay, Old Bellevue and downtown.

It has been a privilege to serve as co-chairs of this committee and to work with our dedicated and knowledgeable committee members. The combination of expertise and perspectives represented on the committee helped ensure a Plan that is responsive to our Charge and the Planning Principles. We look forward to seeing the Plan become reality in the years to come.

Sincerely,

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Doug Leigh  
Committee Co-Chair

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Iris Tocher  
Committee Co-Chair



# Vision

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*Visual Simulation of Meydenbauer Bay Park's Entry Plaza Looking West from the Intersection of 100th Avenue and Main Street*

## CHAPTER 3: VISION

Implementation of the Plan will reestablish Bellevue as a waterfront city by strengthening the connections between the City Center, Old Bellevue with its historic main street, and the Lake Washington shoreline. A visit to the waterfront, with its new signature public park, will be memorable. People will be attracted year round by the variety of uses and thoughtfully designed places that respect the setting, express community environmental values, and support a range of both active and passive activities. The waterfront and park are planned to be both a respite within the city and a connection between the city and the lake. The park will be a pedestrian place that encourages contemplation, socializing, and recreation. It will welcome visitors who arrive by boat, car, bus, and bicycle or on foot. When surrounding properties within the project study area are redeveloped in accordance with proposed policy and regulatory changes, they will provide new and inviting pedestrian connections and outdoor spaces that complement park uses. New development within the park and study area will address water quality issues and high-flow impacts from creeks and shoreline development through the promotion of Low-Impact Development (LID) (for example, the use of rain gardens), on-site stormwater detention for new and redeveloped projects, and control of point sources that discharge directly into the lake.



**A** Figure 3.0-1: Meydenbauer Bay Park and Land Use Plan

## 3.1 Connections

### Waterfront Connections

The Bellevue Waterfront once supported a ferry, and whaling, fishing, and ship building industry. As the City urbanized and expanded, the City Center shifted north, and the historic water-oriented uses were replaced by residential and recreational boating uses that don't relate to Bellevue's downtown. The Plan proposes to reconnect the lake to the urban center and Old Bellevue and thereby reestablish Bellevue as a waterfront city.

Throughout the planning process, a range of design strategies for strengthening the physical, visual, and land use connections was discussed. Strategies included pulling the more urban character of downtown toward the shoreline, or the character of the shoreline, park, or historic buildings toward the town. There were many discussions about the appropriate form, character, and scale of the public spaces, streets, and structures. Ultimately, the Plan proposes park zones and a variety of connection strategies informed by the context, scale, and character of the immediately surrounding uses.

### Neighborhood Connections

The neighborhoods surrounding the proposed park are comprised of traditional medium- to large-lot single-family subdivisions to the west and north, transitioning to more diverse and urban multi-family midrise and mixed-use neighborhoods closer to Main Street. Connections to the west and

north/central residential neighborhoods will be enhanced through the improvement of trail and pathway connections. A new green space and community activity building located adjacent to Lake Washington Boulevard will serve as a new gathering space and grand viewing terrace, creating a stronger connection for neighborhoods to the north. Relocation of active uses including the beach and playground will ensure that the west end of the park remains more passive and quiet. Streetscape improvements including landscape pathways and bicycle accommodations along Meydenbauer Way SE will improve connections to Wildwood Park and the Old Bellevue neighborhood. The Plan envisions the redevelopment of the upland parcels to include higher density residential infill west of 100th Avenue NE and mixed-use redevelopment south of Main Street, with a series of interconnecting pathways and public spaces capturing views and providing stronger connections.

### Downtown Park & Old Bellevue Connections

The 100th Avenue SE street right-of-way in combination with the publicly owned kite parcel (the current site of the Bayvue Village Apartments) is planned to provide a dramatic connection from the intersection of Main Street to the waterfront, including a new street-level entry plaza with art and fountains, terraced overlooks, and an elevated pier that extends to the shoreline and provides expansive views of the lake. A covered open-air structure will encourage year-round visitation and house an elevator to access parking below the plaza. The pier rests upon and extends beyond an enclosed building space that could provide storage for canoes or kayaks.



Streetscape improvements for the 100th Avenue NE street right-of-way begin adjacent to Downtown Park and are proposed to include enhanced planting, paving, lighting, and water features. South of Main Street, the Plan proposes to convert the 100th Avenue SE street right-of-way to a pedestrian corridor with a grand staircase punctuated by intermediate terraces with art, lush landscaping, and continuation of the iconic linear water feature.

**A** Figure 3.1-1: Early Park Connection Concept Diagram



### 3.2 Public Park Zones

#### Ravine

The west end of the park, located in a forested ravine setting, is currently developed as an intimate neighborhood park with restrooms, a play area, a public pier, and beach located near the outfall of a storm drain. The Plan proposes to relocate these recreational uses in order to daylight and restore the native stream that currently flows through a drainage pipe with an outfall that discharges directly into the lake near the swim area. Native vegetation will be retained and managed to remove invasive plants and promote the succession of native species. Ecological functions will be restored and stormwater cleansed before it reaches the bay. Existing parking, located upstream, will be retained to support community access. In this zone, the shoreline promenade connects to a slightly elevated bridge from which visitors overlook, but are restricted from entering, the restored creek and landscape. The existing wetland located along the shoreline at the north end of Meydenbauer Beach Park will be relocated and modified to a more natural state near the mouth of the daylighted stream.



**A** Figure 3.2-1: Ravine Enlargement Plan

- 1 Partially daylighted stream with adjacent trails
- 2 Ravine enhancements (retain and enhance forested areas; remove invasive plants)
- 3 Retain existing parking (28 spaces) and entry road
- 4 Footbridge over daylighted stream

A

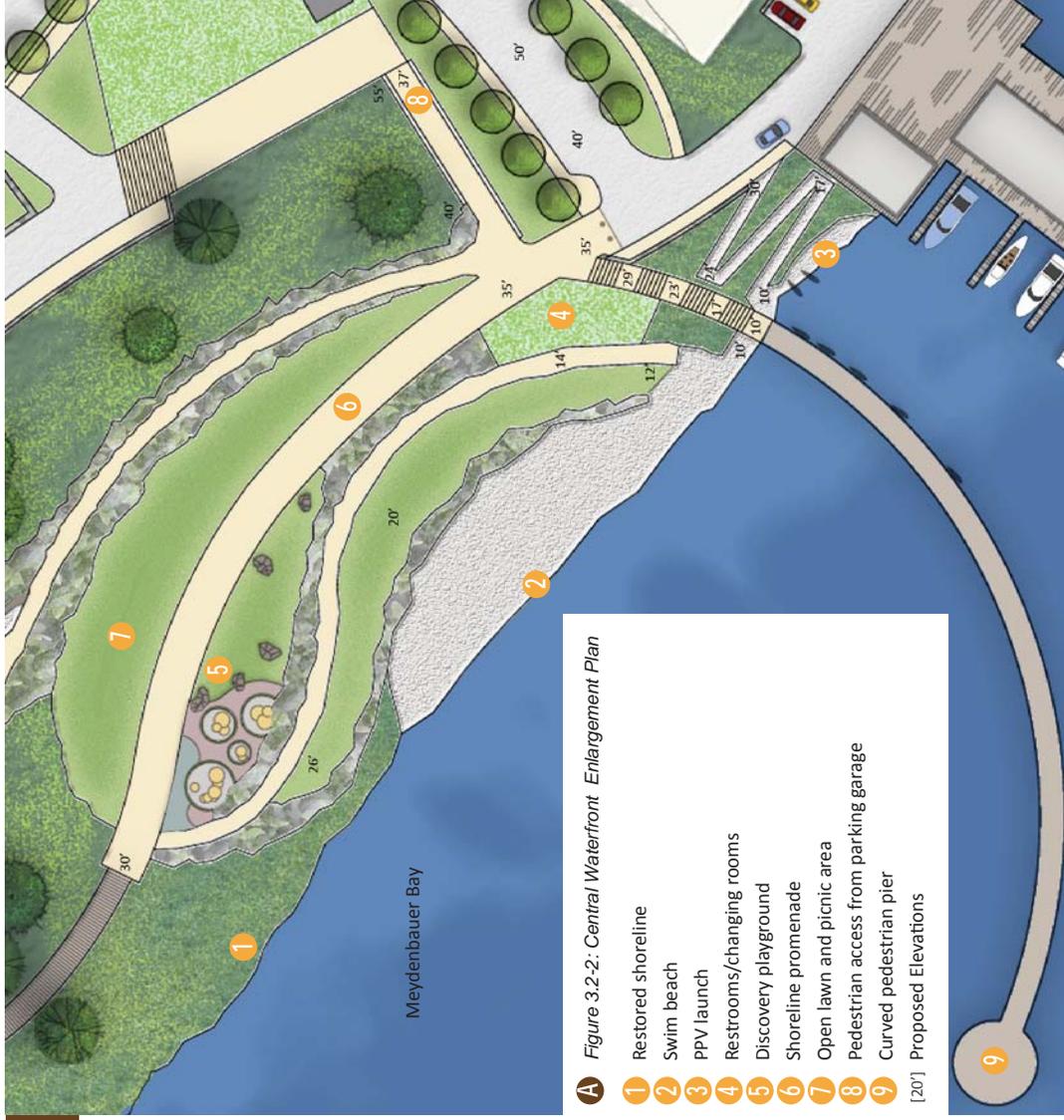


Figure 3.2-2: Central Waterfront Enlargement Plan

- 1 Restored shoreline
  - 2 Swim beach
  - 3 PPV launch
  - 4 Restrooms/changing rooms
  - 5 Discovery playground
  - 6 Shoreline promenade
  - 7 Open lawn and picnic area
  - 8 Pedestrian access from parking garage
  - 9 Curved pedestrian pier
- [20'] Proposed Elevations

### Central Waterfront

Land in the central waterfront zone has been acquired for park expansion. Homes remain occupied by previous owners or tenants prior to park development. The Plan proposes a new and larger beach, a new restroom and changing facility, a custom designed playground with elements that encourage creative and unstructured play, and a shoreline promenade connecting the ravine to the marina. The distinguishing and memorable feature of this zone is the curved overwater boardwalk that provides new connections over the water, moorage for people-propelled vessels (PPVs) such as canoes and kayaks, and definition of the swim area and new views of the lake, shoreline, and beach. The proposed waterfront will balance shoreline and water-dependent uses with ecological concerns. All of the small public and private accessory piers and shoreline armoring within this zone will be removed to promote improved near-shore habitat contiguous with the mouth of the daylighted stream. The shoreline area north of the new beach will be restored to a more natural state with gradual slopes and native vegetation.

### Lake

Both land and water contribute to a memorable waterfront park experience. The lake is a point of arrival and host to a range of activities including swimming, water play, and moorage. Meydenbauer Bay is small, and the Plan carefully locates new facilities to provide recreational and visual access, promote safety, protect the shoreline, encourage restoration of fish habitat, and improve water quality. Resource management strategies along the water's edge vary depending on the use. Overwater walks and docks are located and designed to improve access and yet control and limit access points to encourage reestablishment of riparian vegetation and shoreline habitat.



## Marina

Meydenbauer Bay has a unique existing and historical waterfront that could provide a number of opportunities for the community to celebrate the rich heritage of the bay. Originally, the shoreline of Meydenbauer Bay was undeveloped. Throughout the early 1910s, and prior to the construction of the I-90 and 520 bridges, transportation between Seattle and Bellevue was by ferry. The Leschi ferry began operating in 1913 and ran through 1950 (HistoryLink.org 2008). Following the opening of the Lake Washington Ship Canal in 1917, the American Pacific Whaling Company wintered their whaling boats at Meydenbauer Bay to keep them out of the salt water (HistoryLink.org 2008). Two original buildings remain at the marina today: the Whaling Building and the Ice House (Eastside Heritage Center 2007). Meydenbauer Bay has been the location of pristine shoreline habitat, public access, ferry transportation, and public and private vessel moorage. To date, the City has worked to maintain a number of these elements in Meydenbauer Bay, and the Plan envisions opportunities to enhance many of them.

The park will continue to provide long-term and transient moorage with fewer ecological impacts. Modifications to the marina will follow the Clean Marina Certification program guidelines. The waterfront and the marina will provide facilities to support canoeing, kayaking, and sailing. The amount of long-term moorage will be reduced and reconfigured to open up views of the bay, reduce shading of the shoreline, and allow for shoreline restoration and habitat improvement. Pier 1 will be expanded to the south to increase capacity of Pier 1 to 38-48 long-term moorage slips. Fourteen designated transient moorage slips will be provided either on Pier 1 or next to the floating boardwalk. The Whaling Building will be preserved and adapted for community use, such as education or interpretive uses.

A sewage pump-out facility will be incorporated on Pier 1. The exact location of the facility, to be determined during detailed design phases, will need to consider adequate water depth and navigable channel width for boat access.

## Hillside

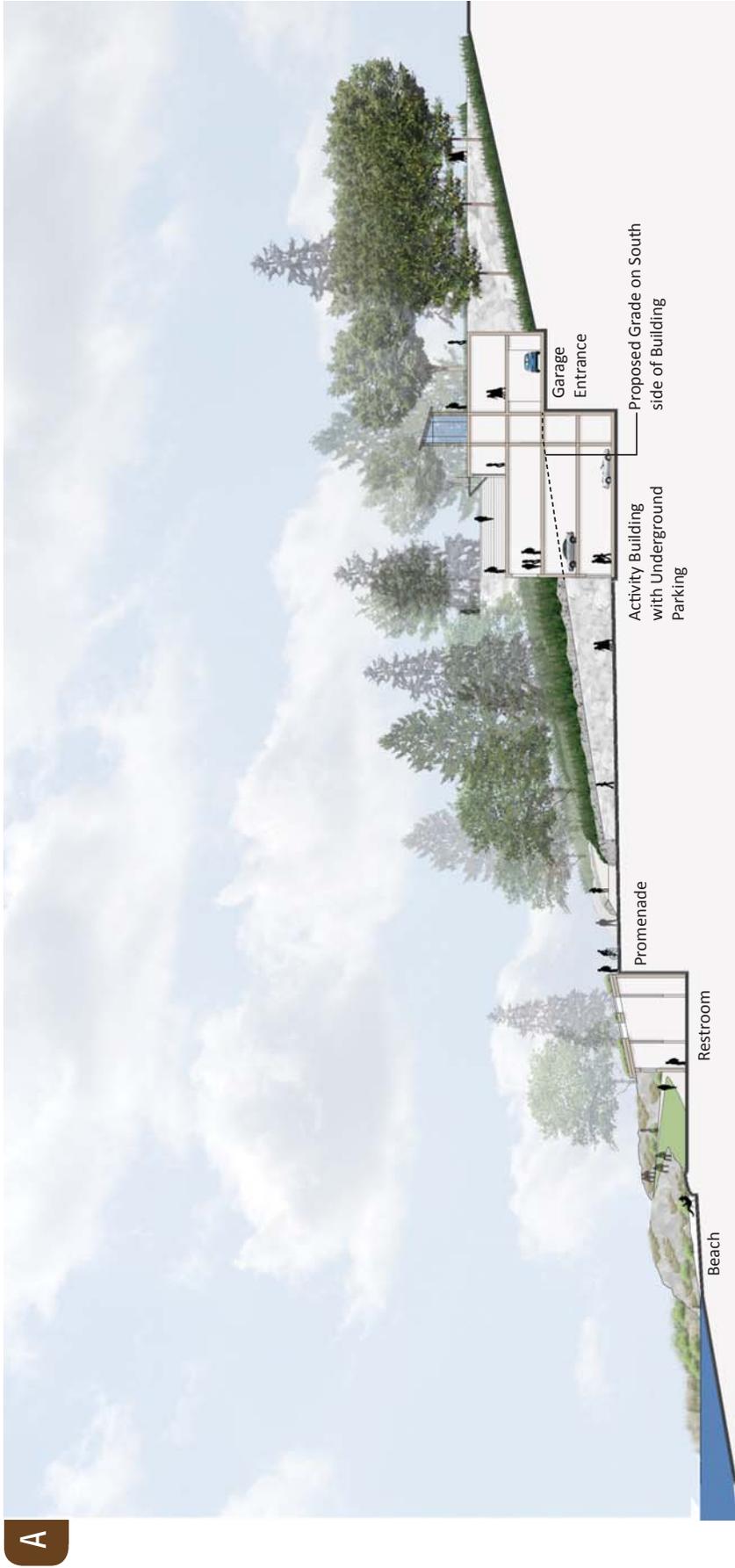
The park hillside zone is located adjacent to and above the central waterfront. Currently the site of nine City-owned single-family homes, this zone offers expansive views and the opportunity to develop trail connections and unique sites for sunbathing, picnicking, interpretation, and education. A play area will be integrated into an attractive rock outcrop that serves as an informal classroom and extension of the sunning lawn area, which will be a distinguishing landscape feature. New parkland adjacent to Lake Washington Boulevard provides an opportunity to continue the Olmstead legacy of regional waterfront parkways. The Plan proposes an attractive streetscape with a vehicle pull-off, short-term parking, scenic overlooks, an upland green space, and a new approximately 8,000 sf community activity building with structured parking below. New trails and an elevator associated with the new community activity building will enhance connections and access between the upland terrace and the water. Pedestrians exiting the parking garage will find themselves close to the elevation of the proposed shoreline promenade and public beach. Storage space is envisioned in the lower level of the activity building to support park and marina programs. The proposed activity building and adjacent site development are envisioned to include best practices for sustainable sites and buildings.

The activity building program uses will relate to the adjacent family-oriented outdoor spaces. Educational activities will cover topics such as boating, swimming, and bay and lake ecology. This space will be used to support a series of organized programs throughout the park, such as summer day-camps, and will provide opportunities to enjoy the park in inclement weather.

- A** Figure 3.2-4: Hillside Enlargement Plan
- 1** Outdoor classroom
  - 2** Enhanced hillside woodland
  - 3** Grand viewing terrace with vehicle pull-off
  - 4** Activity building with parking below
  - 5** Parallel parking
  - 6** Parking garage vehicular entrance
- [201] Proposed Elevations



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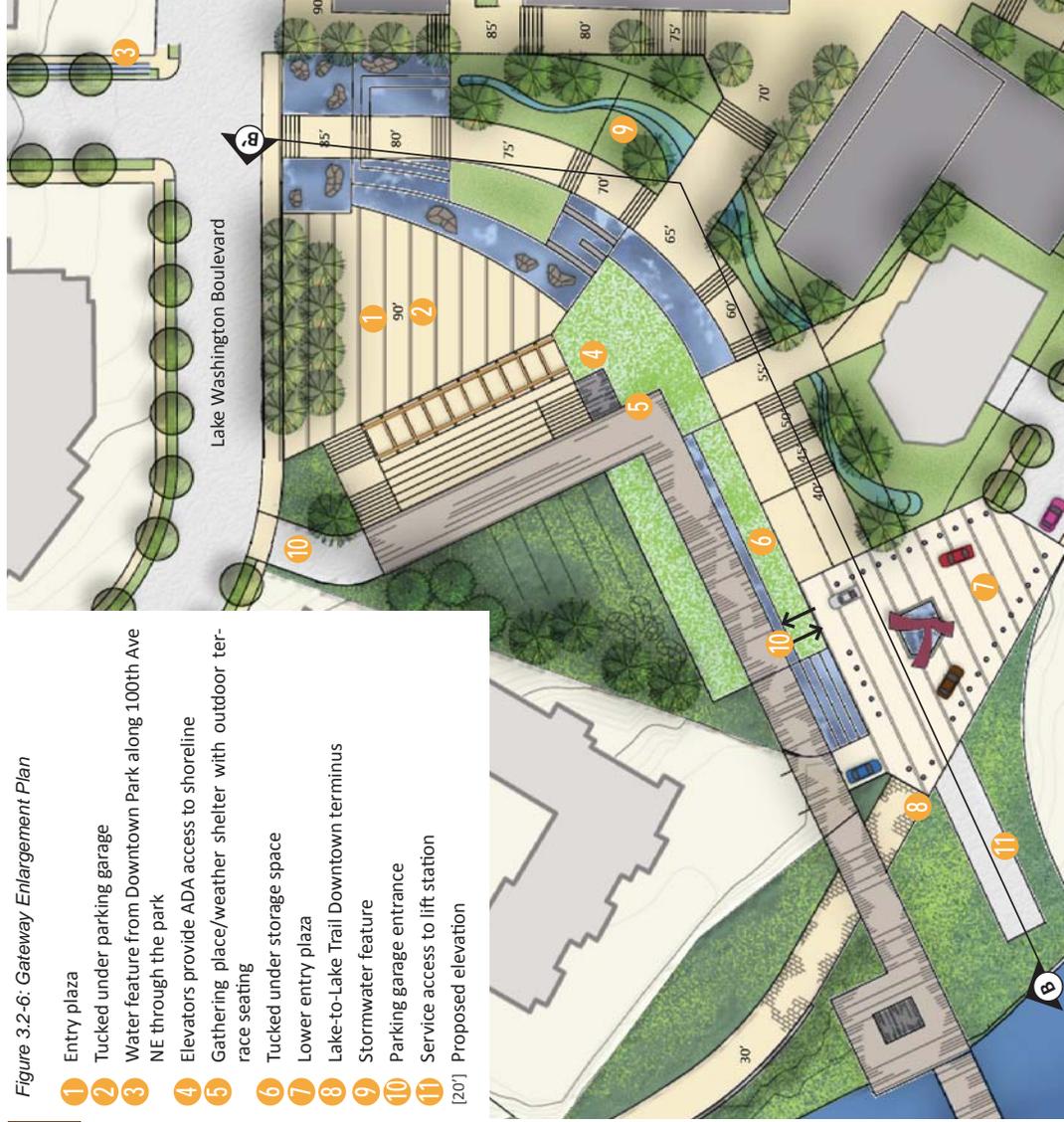


A

**A** Figure 3.2-5. Section/Elevation A-A' Hillside and Central Waterfront Zones

Figure 3.2-6: Gateway Enlargement Plan

- 1 Entry plaza
  - 2 Tucked under parking garage
  - 3 Water feature from Downtown Park along 100th Ave NE through the park
  - 4 Elevators provide ADA access to shoreline
  - 5 Gathering place/weather shelter with outdoor terrace seating
  - 6 Tucked under storage space
  - 7 Lower entry plaza
  - 8 Lake-to-Lake Trail Downtown terminus
  - 9 Stormwater feature
  - 10 Parking garage entrance
  - 11 Service access to lift station
- [20'] Proposed elevation



### Gateway

In 2007, the City of Bellevue acquired a kite-shaped parcel of land located at the southwest corner of the intersection of Main Street and 100th Avenue. With this purchase the City gained a tremendous opportunity to develop a dramatic public space and strong connection to the City's waterfront marina to Main Street and Downtown Park; a connection celebrating views, improving accessibility, and introducing engaging uses and activities. Treatment of this kite-shaped, steeply sloping parcel was the most discussed park zone during the master planning process. Several alternative designs were explored and evaluated. Ultimately, the Plan recommends the development of a series of dynamic public spaces with layers of activities and uses. A street level entry plaza with engaging art, landscaping, and water features serves as a gateway to the park as well as a terminus of Old Bellevue's Main Street. Structured parking will be located underground beneath the entry plaza. 100th Avenue SE will be transformed into a pedestrian place with a wide stairway and generous viewing terraces framed by memorable water features and terminating in a lower entry plaza at the water's edge. The lower entry plaza will be designed to be safely shared by people and cars. A new structure tucked into the hillside will provide small boat storage and provide a rooftop public gathering space. This linear park will connect to an elevated pier that extends to the water's edge, providing new views of Lake Washington, Seattle, and the bay. The grade transitions between the street level entry plaza and the shoreline include elevators as well as an exterior route with stairs, terraced seating, and a sheltered overlook.



**A** Figure 3.2-7: Section/Elevation B-B' Gateway Zone



A

**A** Figure 3.2-8a: Visual Simulation of Meydenbauer Bay Park's Entry Plaza at the Gateway Zone

**B** Figure 3.2-8b: Photo Looking West from the Intersection of 100th Avenue and Main Street

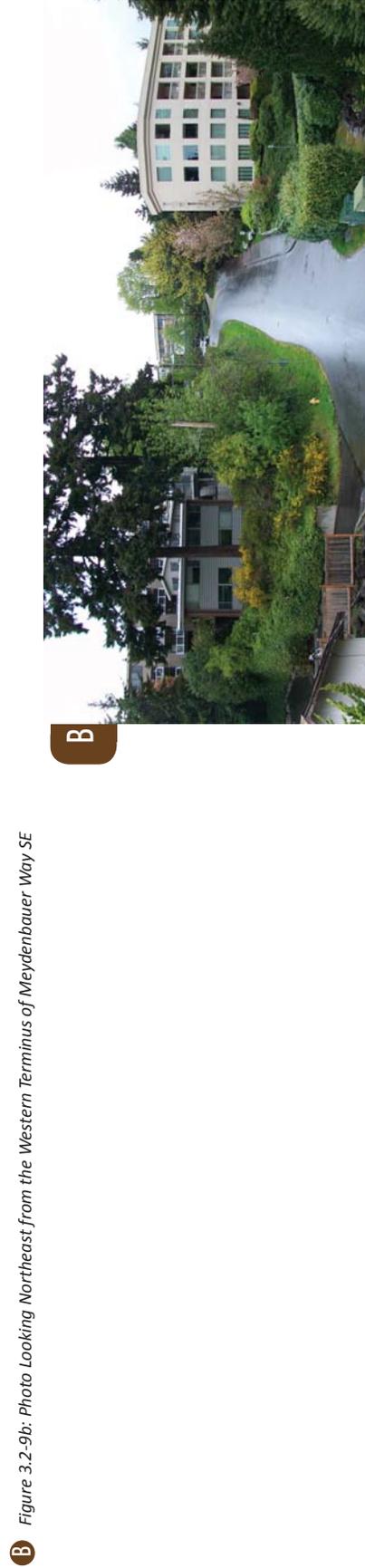


B



A

Figure 3.2-9a: Visual Simulation of Meydenbauer Bay Park's Lower Entry Plaza at the Gateway Zone



B

Figure 3.2-9b: Photo Looking Northeast from the Western Terminus of Meydenbauer Way SE

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### 3.3 Properties Adjacent to Public Park Lands

#### Uplands

The Plan addresses two distinct upland areas: the “Upper Block,” which is surrounded by NE 1st Street, 100th Avenue NE, Lake Washington Boulevard NE, and 99th Avenue NE; and “South of Main,” which lies along 100th Avenue SE/Bellevue Place SE, generally between Main Street and Meydenbauer Way SE. Both areas include medium density multi-family residential. South of Main includes a mix of commercial, retail, and residential uses. Properties in the Upper Block contain two-to five-story apartments, condominiums, and office buildings. These areas have been identified as prime for infill and redevelopment with inherent opportunities to improve the pedestrian environment, views, and pedestrian connections between Main Street and Meydenbauer Bay. The Plan illustrates a conceptual land use plan developed by Sasaki Associates in collaboration with the Steering Committee in 2007.

The Plan recommends redevelopment of portions of the uplands to provide an improved pedestrian environment, a transition between the new waterfront park and the downtown, and other public amenities. Redevelopment will also facilitate more graceful pedestrian connections from Old Bellevue and other nearby neighborhoods to the shoreline. The Plan envisions land use to be largely residential with small-scale commercial and pedestrian-oriented retail south of Main Street to provide neighborhood services and activate the pedestrian connection to the waterfront.

**A** Figure 3.3-1: South of Main Enlargement Plan

- 1** Parking garage entrance
  - 2** Informal seating
  - 3** Water feature
  - 4** Street level retail
  - 5** Pedestrian plaza
  - 6** Terraced seating
  - 7** Wildwood Park connection
- [20'] Proposed elevation





A

**A** Figure 3.3-2a: Visual Simulation of Meydenbauer Bay Park from Meydenbauer Bay

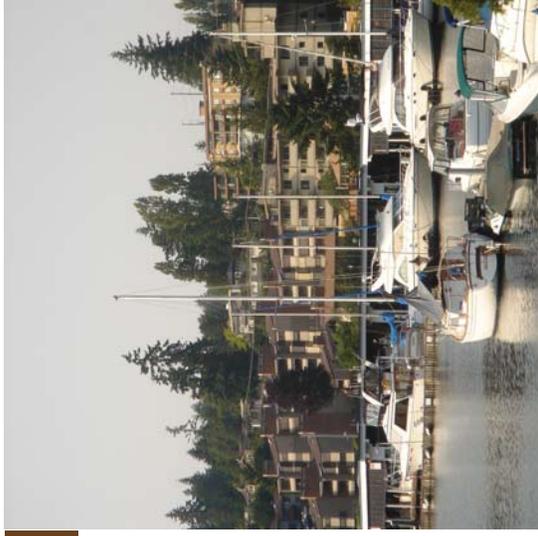
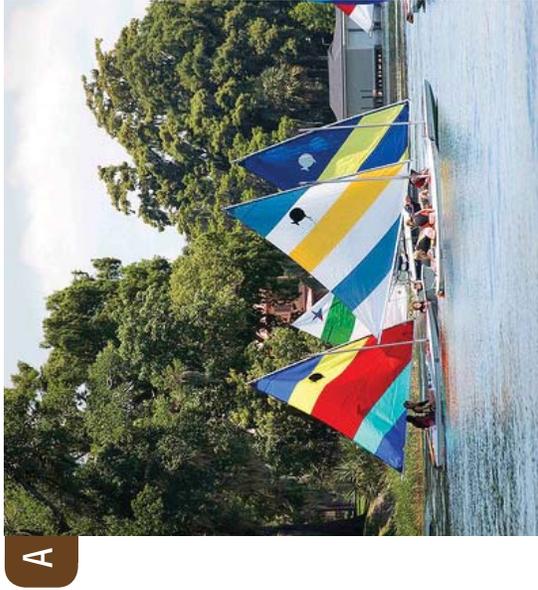
**B** Figure 3.3-2b: Photo Looking Northeast from Meydenbauer Bay

*(Note: These photo simulations provide representative views of the alternatives from properties across the bay and are not intended to depict the view from any specific property.)*



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## Yacht Club

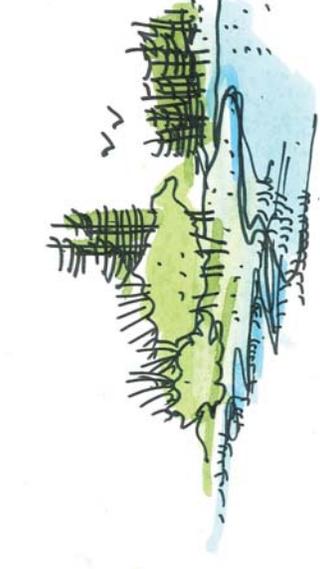
The Meydenbauer Bay Yacht Club borders the South of Main sub area to the southwest, and the City's marina abuts the Club's marina at the shoreline. This privately owned property includes facilities and operates programs that are compatible with and complement the Plan vision, such as the Youth Sailing program. No changes are proposed.

## Neighborhoods

Preservation of the existing residential neighborhood character of areas located along both sides of Lake Washington Boulevard, east of the existing Meydenbauer Beach Park, and west of 101st Avenue NE is a priority. Connections between these neighborhoods, Wildwood Park, and the new waterfront park will be enhanced through the improvement of streetscapes, pathways, and upland green spaces.

**A** Figure 3.3-3: Youth Sailing Programs

**B** Figure 3.3-4: Meydenbauer Bay Yacht Club





# CULTURAL COMPASS



A Strategic Vision for the Arts & Culture

# Adopted

by City Council  
**September 20, 2004**

Bellevue Arts Commission  
City of Bellevue Department of Planning and Community Services

# Executive Summary

## ***Introduction***

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Responding to Bellevue City Council's vision to establish Bellevue as the cultural hub of the Eastside, the Arts Commission requested and Council agreed to funding a city-wide strategic visioning process entitled "Cultural Compass: A Strategic Vision for Arts and Culture." In May, 2003, the City contracted with Wolf, Keens and Company to facilitate the process, provide their expertise, and write the plan. The mission statement for the process was: *"To develop an action-oriented, inclusive, consensus strategic vision for Bellevue's cultural development that realistically addresses the needs, interests, and priorities of all of Bellevue's residents."*

Among the tasks completed as part of the project were:

- Conducting a comprehensive **community cultural assessment**
- Facilitating a series of meetings with a 31-member **Steering Committee**
- Designing and conducting a "**Cultural Compass Forum**" attended by over 75 people
- Performing a **financial analysis** of Bellevue-based cultural organizations
- Compiling a **cultural organization and program inventory**.

Over the course of the past year, Cultural Compass has conducted numerous meetings and engaged hundreds of residents. Ideas, concerns, and priorities have been cycled through several review processes, focusing on the Steering Committee but engaging residents at many moments in the process.

## ***Rationale for the Plan's Structure***

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In this plan, by culture we refer to the entire range of fine and traditional art forms, the humanities, as well as the history and heritage of this community and this region. We include the built environment, as well as the folkways and traditions of all of Bellevue's residents, long-time and newly arrived.

Bellevue is just fifty years old. It is still inventing itself. While much of the ground work is laid – a transportation grid and a role as a commercial center for the Eastside for just two examples – there is still much to be worked out. The role of arts and culture in Bellevue is one such area and it is a priority, according to Bellevue's City Council and the many residents contacted during this process.

The City has a strong foundation of cultural activities. Yet there is a lack of consensus about what the City should aspire to in the area of arts and culture. The Cultural Compass is designed to address that by describing what a more mature cultural sector would look like in ten years. This plan proposes, in short, what Bellevue's cultural sector should be when it grows up.

## ***A Vision for Arts and Culture***

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By 2015, Bellevue's cultural sector will have reached adulthood. Some of its significant attributes are listed below.

- Residents of Bellevue take advantage of compelling arts and cultural opportunities on multiple levels of engagement as part of the fabric of their lives.
- Families and friends take advantage of cultural festivals and informal cultural activities on a regular basis.
- The business community works hand in hand with the cultural sector to enhance the quality of life experience for both Bellevue residents and employees.
- Arts and cultural organizations located in Bellevue are part of an active matrix of communication and coordination.
- The City of Bellevue is known nationally as a model of urban cultural cultivation for cities of its scale.
- Residents of the Eastside come to Bellevue for leisure and educational arts opportunities (festivals, major professional offerings, and classes)

often in conjunction with shopping or dining in Bellevue establishments.

## ***Core Initiatives***

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The following four initiatives represent the core of the “big picture” synthesis of the consultants’ findings. They highlight the qualities and opportunities on which Bellevue can build a dynamic and unique approach to arts and culture. The four initiatives are:

1. **Bellevue offers extraordinary cultural participation and learning opportunities for people of all ages.** Bellevue prides itself on being an excellent place for families and children, in large part because of the range of educational activities available. Arts and culture are already a strong component in the appeal to families. Bellevue’s reputation can be enhanced further by building dynamic cultural opportunities for residents of all ages.

Examples of ways to achieve this that are included in this plan are:

- Provide “arts and cultural encounters” in cultural districts to encourage new experiences. (Strategies 2.2 and 4.2)
  - Link Parks Department and other introductory arts programs to more advanced classes and opportunities to view professional cultural presentations. (Strategy 4.3)
  - Build new alliances and communication between the Bellevue School District and cultural organizations that provide programs in schools. (Strategy 4.1)
  - Expand audiences of residents and employees through new programs that bring arts and cultural experiences, discounted tickets, and exhibitions into the workplace. (Strategies 2.3 and 4.4)
2. **Distinctive cultural districts are linked to Bellevue’s bustling commercial districts.** Given Bellevue’s geographic location and its urban infrastructure, the City has emerged as a commercial and transportation hub for the Eastside. It is particularly well suited to serve as a center for arts and culture as well. Such a center can be built in synergetic relationship to the commercial retail sectors in the City by defining specific cultural districts that overlap with existing commercial

areas of the City. In addition, Bellevue's ethnic diversity, stronger than other Eastside communities, can be used as a distinct component in creating lively and interdependent cultural and commercial sectors.

Examples of ways to achieve this that are included in this plan are:

- Designate cultural districts in Bellevue's existing commercial areas to strengthen cultural amenities and social activities. (Strategies 2.1, 2.2)
- Develop year-round festival-type events and other programs to capitalize on the rich ethnic traditions of Bellevue's residents, focusing these programs in cultural districts (Strategy 3.1)
- Use public art and graphics to enhance the energy and visibility of cultural/commercial districts. (Strategies 6.3, 6.4)
- Introduce "cultural encounters" to make shopping or running errands in Bellevue's commercial areas a unique experience. (Strategy 4.2)

3. **Bellevue's maturing cultural sector builds its visibility, identity, and capacity.** As a City, Bellevue has begun to articulate a consistent and strong image for itself as a major urban center on the Eastside. The cultural sector is not as far along in that process. To develop and carry the banner as an Eastside cultural hub, Bellevue must ensure that Bellevue-based cultural organizations have the capacity to sustain their operations. Building cultural audiences is, in part, a function of developing and maintaining the cultural sector's visibility and identity.

Examples of ways to achieve this that are included in this plan are:

- Provide enhanced information about arts and culture to Eastside residents and visitors. (Strategy 1.2)
- Use arts and culture as incentives for locating and doing business in the City. (Strategies 2.1 and 1.3)
- Promote the cultural expressions of various ethnic groups in Bellevue. (Strategies 2.2, 3.2, and 3.3)

- Foster alliances, collaboration, and training among Bellevue cultural entities to build their capacity. (Strategies 1.1 and 7.5)
4. **Bellevue develops and attracts strong leadership in support of its cultural sector's infrastructure and funding.** Bellevue is a young city. While its residents have interest in and commitment to arts and culture, the necessary underpinnings of a vibrant cultural sector are not yet fully in place. This plan outlines what is needed in great detail. Few of the strategies in this plan are likely to be implemented without the active cultivation of three key ingredients: **leadership** in support of cultural priorities, City of Bellevue **staff capacity** to coordinate the implementation of planning initiatives, and adequate **funding** from both the public and private sectors. The plan addresses these concerns forcefully.

Examples of ways to achieve this that are included in this plan are:

- Build the City's leadership role in the implementation of this plan through coordination, alliance development, and policy direction. (Strategies 1.4 and 7.1)
- Employ appropriate community mechanisms and guidelines for setting public priorities for arts and cultural development. (Strategies 2.1, 5.2, 6.4, and 7.2)
- Provide an appropriate city administrative structure, adequate staff, and appropriate funding levels to implement plan priorities. (Strategies 7.3, 7.4, and 8.1)

## ***Key Findings***

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Using individual interviews, small group meetings, and working with the Steering Committee, the consultants were able to make and subsequently test a set of observations on various aspects of Bellevue's cultural landscape. Many of these observations came from a variety of sources as well as the consultants' own research. The vision and core initiatives described above are a synthesis of those processes. In the sections below, important findings are summarized under eight headings – cultural organizations and artists; cultural economic and business development; cultural diversity; cultural education; cultural facilities; public art; cultural resources; and administrative infrastructure.

## **Cultural Organizations and Artists**

Bellevue's cultural sector has a preponderance of small-budget organizations and that translates to a heavier reliance on volunteers and less well-developed management systems. There is a strong component of organizations with an educational orientation, which is not surprising considering the value placed on cultural education and participation in Bellevue.

There is room for growth in the number of cultural organizations that Bellevue can support, as long as that growth is grounded in a growing audience and focuses on programs that fill specific niches. Bellevue's cultural groups fit within a complex ecology on the Eastside and in Seattle and they cannot be evaluated in isolation from these other organizations. Indeed, regional organizations such as the Eastside Arts Coalition and Standing Ovation represent important assets that Bellevue can support and enhance.

Available evidence suggests that the artist population of Bellevue is dominated by avocational artists with relatively few professional artists. Given the cost of studio or rehearsal space in Bellevue, this is not surprising. Issues of concern for artists – whether professional or avocational – include finding performance and exhibition opportunities, sharing information, creating congenial meeting places and, for professional artists, making a living.

## **Cultural Economic and Business Development**

For many non-residents, Bellevue is stereotyped as a fast-growth edge city with a predominance of malls and office towers. Those who know Bellevue understand that those are elements of the City but that there is much more than that: Bellevue has dynamic diversity in its population and in the types of businesses within its City boundaries, as well as enviably high standards of civic amenities including education and parks. The task is to define Bellevue's acknowledged strengths - including its commercial hubs - as components in building a unique mix of cultural amenities for the City.

Indeed, one of the key themes that has emerged in this process is the importance of linking cultural events and activities to the various commercial areas of Bellevue. Key among these are Downtown, Crossroads, and the Factoria-Eastgate corridor. This approach can

accommodate a broad range of artistic and cultural programs, from sophisticated offerings featuring regionally and nationally recognized artists to presentations such as school performances celebrating the accomplishments of local and non-professional groups.

As the City has emerged as the commercial center of the Eastside, many in City government envision Bellevue as the “cultural hub of the Eastside.” This points to a key role imagined for the arts and culture in invigorating and enlivening the City. Yet the task is broader than just Downtown – it must address all three commercial areas because they form a geographically significant framework in which development needs to be focused.

Many people have mentioned an interest in developing an active nightlife in Downtown to extend the area’s appeal beyond working, shopping, and increased dining. By establishing the Downtown as Bellevue’s primary destination for a range of night time entertainment, the City’s ability to attract audiences will be enhanced. This will require careful thought as to the types of programs, facilities, and incentives that may be required to attract and retain such entertainment businesses since currently Bellevue is seen as more of a “happy hour” rather than “after hours” community.

## **Cultural Diversity**

The increased ethnic diversity of the Eastside and Bellevue is one of the most significant and defining changes in Bellevue in the past ten years. Indeed, data show that Bellevue has a higher percentage of foreign-born residents than King County as a whole, the majority of whom are from Asian countries.

The question of how to address Bellevue’s cultural diversity is a complex one. Some residents believe that understanding of this new diversity is weak in Bellevue, especially in the corporate sector. Others commented on an emerging dichotomy relative to Bellevue: most multi-cultural or ethnically specific cultural activities are seen as happening in the Crossroads area, or in faith-based locations, while “high end” and more conventional cultural fare is centered in the Downtown.

While there are potentially divisive issues implicit in these comments, many people perceive cultural diversity as an important asset for Bellevue in the 21<sup>st</sup> century. They see the ability to celebrate these differences, especially through the arts and heritage, as an important way to ensure

the successful future of Bellevue. Diversity is also seen by many as an important link between economic and cultural vitality. Ethnic and cultural retail enterprises are thriving at Crossroads Shopping Center, for example, and represent a way in which diversity serves to foster economic development.

It is important in any cultural development planning process to address how new audiences will be identified and engaged. Understanding the programming interests and priorities of Bellevue's newer immigrant and relocated populations will be critical to the success of these efforts.

## **Cultural Education**

One of the key attributes of communities that focus on participatory cultural experiences, as Bellevue does, is the high value placed on arts and cultural education. There is broad interest in encouraging children and youth to experience arts and culture in hands-on settings. Family and intergenerational cultural participation is also highly valued, whether in a workshop, festival, or performance setting.

The City's public schools offer a wide range of arts classes, compared to many other communities. Many classes and other opportunities for young people are also available outside the school setting and are seen as important community assets. Community-based programs such as choirs and Parks Department programs are augmented by Art-Zones, the Bellevue Community College's Continuing Education program.

While there is some collaboration and partnering between organizations that currently offer cultural activities, interviewees noted that there remain untapped opportunities for increased communication or relationships. There may be roles for the civic, business and education sectors to work together more effectively. The question of affordability is often a concern and must be kept in mind when encouraging children or adults to explore new cultural experiences.

Both formal classes and informal encounters or activities are seen as being important components of cultural education and participation for all ages, and may occur in a variety of settings including classrooms, places of worship, homes, commercial settings or the parks. Festivals and fairs are considered important gateways to cultural participation with hands-on activities and low-risk opportunities to explore cultural activities in a social setting.

Arts and culture are also widely seen as important for preserving and sharing cultural traditions that reflect the growing ethnic diversity of Bellevue. Cultural education is one way to assure that individuals retain connection to their heritage, and also to expand understanding and celebration throughout the community.

## **Cultural Facilities**

Just as artists and cultural organizations are the cornerstone of the cultural sector, adequate and appropriate facilities are the keystone: without a mix of performance, rehearsal, exhibition, and other spaces, Bellevue's cultural sector will be hard-pressed to reach its full potential. The mix of cultural facilities in Bellevue – and indeed on the Eastside more generally – has developed based on cultural facility planning work conducted in 1989 that indicated that most Eastside communities could individually sustain 100-150-seat “black box” spaces and somewhat larger 400-500-seat facilities. That process also proposed a larger, approximately 2,000-seat facility in the Bellevue area. In 2003, based on the findings of a Citizens Advisory Committee, City Council affirmed the desirability of having a large performing arts center in Bellevue and endorsed the Performing Arts Center, Eastside (PACE) project as the means of developing one

Since the 1970s, performing and visual arts organizations in Bellevue and the Eastside have matured and developed stronger audiences from a greatly increased population base. Thus, in the performing arts, the need for fully-equipped mid-sized (from 750- to 1,200-seat capacity) venues is greater than ever. And there are very few of the smaller scale black box venues that would provide some relief from the heavily scheduled 400-seat spaces.

The situation is quite similar for the visual arts. With the exception of the Bellevue Art Museum's professional caliber exhibition spaces, there is only a limited amount of gallery space or non-traditional exhibition space available in various malls, businesses, and public buildings. The need among visual artists and visual arts organizations for studio, exhibition, storage, and office space parallels that of performing artists and organizations.

In other, older cities many facility needs might be satisfied in low-cost, vacant space in old warehouses or lower quality office facilities. But

because Bellevue is a relatively new city, it does not have the inventory of older buildings that might be inexpensively used by cultural organizations for some or all of their activities. Looking at the building stock available in the City, it makes sense to explore options in various malls and other commercial venues, looking at spaces that are not purpose-built for arts and cultural usage but might be retrofitted for such activities.

Yet the capital costs of facilities are high and the risks of miscalculation of regional audience demand are grave. For that reason, several keystone principles should underlie the City's approach to cultural facility development in Bellevue over the next ten years:

- Coordinate cultural facility development regionally so that audience potential is carefully calibrated and overlapping or competing initiatives are avoided.
- Develop a set of consistent guidelines to evaluate potential cultural facility projects so that support is provided for initiatives that move forward the goals of this planning process.
- Provide incentives to private sector developers that add cultural amenities to their projects or use the City's bonding authority to underwrite the costs of capital construction.

## **Public Art**

Public art projects in Bellevue to date range from permanently installed individual works such as *Salmon Woman and Raven* by Tom Jay, to integrated landscape or architectural elements such as *Double Inquiry* by Larry Kirkland and the Newport Retaining Wall by Vicki Scuri. The biennial Bellevue Sculpture Exhibition is also a project of the City's Public Art Program.

Bellevue's Public Art Program has not yet attained high visibility among Bellevue residents. Indeed, few people interviewed for this planning process made reference to the program, either positively or negatively. So while the program has been on-going for some years, it has not had the impact anticipated by the Arts Commission.

There are some clear opportunities for strengthening the existing foundation of the Public Art Program. Chief among these is the need to align and focus the vision for the program with the larger vision and direction of the cultural plan itself. This suggests developing processes that effectively engage more elements of the community in various stages

of the program, from involvement in art selection to the development of related community programs. Involving artists – both local and of regional and national renown – in planning, selection, and programs is likely to strengthen outcomes. Public art projects should be located primarily in geographic areas identified as zones of cultural activity where they can reinforce the density of cultural engagement.

## **Cultural Resources**

Bellevue's aspiration to become the cultural hub of the Eastside is an ambitious one. To realize it will require significant investments from a wide range of sources. The necessary support can be likened to a "three-legged stool," which includes:

1. Increased earned income as well as partnerships and collaborations to enhance efficient operations by cultural organizations
2. Stronger financial and administrative support from the City of Bellevue
3. Increased private sector support and engagement.

It is clear that additional public sector resources are central to the growth and development of Bellevue's cultural sector. In many communities, a municipality plays the central role in "priming the pump" for cultural development by providing capital funds for facilities, or covering on-going facilities costs, or by granting operating support to established cultural organizations. The City has already acknowledged the value of a role for arts and culture in its vision for Downtown (as articulated in the 2003 Downtown Plan Report) and in the City as a whole. The issue today is less "whether" to invest in the arts and culture but "how."

Tax incentives to cultural businesses, special cultural or entertainment districts, trade-offs for developers that include cultural amenities, user fees – all of these mechanisms provide opportunities. But this plan goes beyond advocacy for more dollars to focusing on specific, and innovative, ways in which public sector resources can most effectively be mobilized in support of arts and culture.

Engaging the private sector – individuals and businesses – may prove more challenging than the public sector. There appears to be a less complete understanding of the resources and level of effort that will be required to implement the vision for arts and culture in Bellevue within the private sector. The case must be made that supporting Bellevue's cultural

development is necessary for keeping the Seattle and the region's "cultural ecology" strong

It will also be important to work with the charitable giving programs of ArtsFund, and Standing Ovation on the Eastside, to maximize and build a "culture of giving for culture" in Bellevue and the Eastside.

## **Administrative Infrastructure**

The prior section emphasizes the need for the cultural sector to capture additional resources, primarily dollars and leadership. Another critical need for growing Bellevue's cultural sector is improving the structures and systems that provide a range of "umbrella" services to the cultural sector as a whole. This plan discusses building stronger ties to the community, marketing Bellevue's cultural sector, providing technical assistance to organizations, working with developers and schools, and coordinating the efforts of cultural groups, among many other tasks. How will this get done? What entity will oversee the implementation of this cultural plan?

Experience has shown that responsibility for these and other tasks must be vested in some specific entity or else they will get lost. City government has a history of providing some of these services in Bellevue, and such a role is common in communities throughout the country that have a strong and vibrant cultural life. The City of Bellevue is willing to take on a larger role. Bellevue is also fortunate to have a private sector with a growing interest in supporting arts and culture. These combined interests signal an opportunity to consider a range of models for moving the plan forward. Whatever option City Council selects, it is critical that the entity provide a robust framework for implementing the plan if the cultural sector is to thrive and mature.

Bellevue's current level of arts and cultural organizational infrastructure and staffing resources are inadequate to meet existing commitments and will not permit significant efforts to implement this cultural plan. Indeed, the aspirations of Bellevue to strengthen its cultural sector require supplementing the single staff person devoted to this area in its Department of Planning & Community Development with additional staff and other resources. This suggests establishing a formal "local arts agency" structure to coordinate and facilitate cultural development, a common structure in cities of Bellevue's size. It also suggests a restructuring of the make-up and/or the mandate of the Bellevue Arts

Commission so that the broader mission embodied in this plan becomes its purview.

## ***Goals and Strategies***

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### **GOAL 1. Strengthen Bellevue's arts and cultural organizations and opportunities for artists, both professional and avocational.**

- **Strategy 1.1.** Evaluate, inventory, and strengthen technical assistance and capacity building programs to support Bellevue-based cultural organizations and artists.
- **Strategy 1.2.** Raise the region's awareness of Bellevue's arts and culture, using pooled services for joint event calendars, both print and web-based, and other mechanisms.
- **Strategy 1.3.** Develop a program of rental subsidies and other incentives for cultural organizations and artists' workspace based in Bellevue.
- **Strategy 1.4.** Expand and solidify the City's role as a convenor and facilitator of cultural organizations and artists and other sectors of the community.

### **GOAL 2. Employ Bellevue's arts and cultural assets to further the City's economic development priorities.**

- **Strategy 2.1.** Strengthen and expand the City of Bellevue's land use and development incentives and other mechanisms in support of cultural uses and amenities as articulated in this cultural plan.
- **Strategy 2.2.** Develop distinct and differentiated "arts, culture, and entertainment (ACE) areas" in Bellevue's Downtown, Crossroads, the Factoria-Eastgate corridor, and other areas of the City.
- **Strategy 2.3.** Identify and strengthen mutually beneficial activities between Bellevue businesses and cultural groups.
- **Strategy 2.4.** Establish and promote workplace benefits that offer employees of Bellevue-based businesses opportunities for discounts or access to classes and other programs offered by Bellevue cultural organizations.

### **GOAL 3. Promote the arts and cultural traditions of Bellevue's increasingly diverse population to distinguish and enrich civic life.**

- **Strategy 3.1.** Build a strong and vibrant series of ethnic and cultural heritage festivals and celebrations.

- **Strategy 3.2.** Use the cultural expressions of various ethnic groups to develop presentations for public schools and other venues.
- **Strategy 3.3.** Devise more effective ways to build culturally diverse audiences and enhance communication with ethnically specific populations.

**GOAL 4. Foster a unique, learning-oriented cultural environment in Bellevue for people of all ages.**

- **Strategy 4.1.** Develop mechanisms to improve communication, cooperation, and planning among arts and cultural groups, the Bellevue School District, Bellevue Community College, and others as a way to improve service delivery.
- **Strategy 4.2.** Develop programs of “arts and cultural learning encounters” in shopping centers, workplaces, and other non-traditional venues to foster cultural participation and allow for observation of the creative process.
- **Strategy 4.3.** Strengthen the range of affordable, publicly accessible cultural education programs in Bellevue in a wide range of disciplines.
- **Strategy 4.4.** Design a City-wide exhibition and performance program that highlights formal and informal arts and cultural activities of Bellevue residents and employees of Eastside businesses.

**GOAL 5. Develop a mix of flexibly designed, quality visual and performing arts spaces to serve Bellevue and Eastside organizations, artists, and residents.**

- **Strategy 5.1.** Update the inventory of existing cultural facilities in Bellevue to include spaces suitable for cultural purposes and provide better coordination of their usage.
- **Strategy 5.2.** Establish guidelines for cultural facility development in Bellevue, based on the priorities of this cultural plan, by which proposals can be reviewed and chosen for City support and/or incentives.
- **Strategy 5.3.** Evaluate workshop, storage, rehearsal, and other spaces for working artists and cultural organizations in the City’s light industrial district.
- **Strategy 5.4.** Continue to work with the Bellevue School District to establish joint use arrangements between schools and community users for specific school spaces.

## **GOAL 6. Strengthen and expand Bellevue's Public Art Program.**

- **Strategy 6.1.** Address key structural and policy issues that relate to the existing Public Art Program.
- **Strategy 6.2.** Expand efforts to engage and educate Bellevue's residents more effectively about public art.
- **Strategy 6.3.** Develop approaches and plans for Bellevue's Public Art Program that will guide implementation of its mission in coordination with this cultural plan.
- **Strategy 6.4.** Establish an initiative that encourages the integration of public art into private developments.

## **GOAL 7. Develop a mix of stable and sustainable public and private sector funding and support mechanisms to strengthen Bellevue's arts and cultural sector.**

- **Strategy 7.1.** Confirm and build the City's leadership role in supporting arts and culture.
- **Strategy 7.2.** Convene a task force of business and community leaders to encourage private sector investment (through cash and in-kind contributions as well as participation on boards, etc.) in Bellevue cultural activities and organizations.
- **Strategy 7.3.** Enhance City funding for arts and cultural organizations by exploring a range of municipal funding mechanisms.
- **Strategy 7.4.** Explore a "round-up" funding program that allows residents to round up their tax and/or utility bills to support a special fund for arts and culture.
- **Strategy 7.5.** Improve the fund-raising skills, strengthen earned income capacity, and explore options for cost reductions through shared services among cultural organizations.

## **GOAL 8. Establish or refine City of Bellevue and community structures and systems to assist in the delivery of the range of cultural initiatives outlined in this Plan.**

- **Strategy 8.1.** Establish a discrete administrative entity that has primary responsibility for coordinating the implementation of this cultural plan and other culturally-oriented tasks.

The chart below provides additional information about the plan's strategies, including their priority and potential implementation partners.

	Description	General Priority	Implementation Partners
<b>GOAL 1: Organizations and Artists</b> <b>Strengthen Bellevue's arts and cultural organizations and opportunities for artists, both professional and avocational.</b>			
1.1	Evaluate and strengthen technical assistance and capacity building	Very high	All technical assistance service providers, City of Bellevue arts and cultural staff as well as appropriate Parks & Community Services Departmental staff, Bellevue Library, Eastside Arts Coalition, Eastside Heritage Center, regional businesses.
1.2	Raise the region's awareness of Bellevue's arts and culture	High	City, Chamber, representatives of shopping centers, cultural organizations, local merchants
1.3	Develop a program of rental subsidies and other benefits for cultural organizations and artists	High	City, cultural organizations, developers, facility managers, regional cultural organizations.
1.4	Expand and solidify the City's role as a convenor and facilitator	High	City, cultural organizations, artists, volunteers
<b>GOAL 2: Cultural Economic and Business Development</b> <b>Employ Bellevue's arts and cultural assets to further the City's economic development priorities.</b>			
2.1	Strengthen and expand the City of Bellevue's land use and development incentives and other mechanisms	Very high	City planning and other officials, representatives of developers and other commercial entities, cultural organizations.
2.2	Develop distinct and differentiated "arts, culture, and entertainment (ACE) areas"	High	City departments including Planning & Community Development and others
2.3	Identify and strengthen mutually beneficial activities between Bellevue businesses and cultural groups	High	BDA, Chamber of Commerce, representatives of mall management, cultural organizations, minority and ethnic business associations.
2.4	Establish and promote workplace benefits	Moderate	City, BCC, BDA, business and civic leaders, Eastside Arts Coalition, 4Culture, KCLS, Eastside Heritage Center, cultural organizations.
<b>GOAL 3: Cultural Diversity</b> <b>Promote the arts and cultural traditions of Bellevue's increasingly diverse population to distinguish and enrich civic life.</b>			
3.1	Build a strong and vibrant series of ethnic and cultural heritage festivals and celebrations	High	City, shopping center operators, BDA, sponsors of existing ethnic and other festivals, Chinese opera programs at Meydenbauer, language and culture programs, church festival groups, Eastside Latino Leadership Forum's Committee for Cultural Planning, Institute for Community Involvement, Eastside Asian Pacific Islander Association

	<b>Description</b>	<b>General Priority</b>	<b>Implementation Partners</b>
3.2	Use the cultural expressions of various ethnic groups to develop presentations for public schools and other venues.	Moderate	City, Eastside Heritage Center, Bellevue School District, other ethnic organizations, cultural organizations
3.3	Devise more effective ways to build culturally diverse audiences and enhance communication with ethnically specific populations.	High	City, representatives of local and regional media, representatives of ethnic communities, public schools, cultural organizations
<b>GOAL 4: Cultural Education</b> <b>Foster a unique, learning-oriented cultural environment in Bellevue for people of all ages.</b>			
4.1	Develop mechanisms to improve communication, cooperation, and planning among arts and cultural groups, the Bellevue School District, Bellevue Community College, and others	Very high	City, BSD, cultural organizations, educators, heritage and ethnic organizations.
4.2	Develop programs of "arts and cultural learning encounters" in shopping centers, workplaces, and other non-traditional venues	High	City, cultural organizations, artists, shopping center management, corporations.
4.3	Strengthen the range of affordable, publicly accessible cultural education programs	Moderate	City Department of Parks & Community Services, cultural organizations, BSD
4.4	Highlight formal and informal arts and cultural activities of residents and employees of Eastside businesses	Moderate	City, BDA, corporate leaders, cultural organizations, artists
<b>GOAL 5: Cultural Facilities</b> <b>Develop a mix of flexibly designed, quality visual and performing arts spaces to serve Bellevue and Eastside organizations, artists, and residents.</b>			
5.1	Update the inventory of existing cultural facilities in Bellevue	High	City, developers, real estate brokers, cultural organizations
5.2	Establish guidelines for cultural facility development in Bellevue	High	City, cultural organizations, cultural facility owners and managers
5.3	Evaluate workshop, storage, rehearsal, and other facilities for working artists and cultural organizations	High	City, cultural organizations, artists, property owners and managers
5.4	Continue to work with the Bellevue School District to establish joint use arrangements	Moderate	BSD, cultural organizations, ethnic organizations, artists

	Description	General Priority	Implementation Partners
<b>GOAL 6</b>			
<b>Strengthen and expand Bellevue's Public Art Program.</b>			
6.1	Address key structural and policy issues that relate to the existing Public Art Program.	Very high	City, interested community members, cultural professionals, artists
6.2	Expand efforts to engage educate residents more effectively about public art	High	City, Arts Commission, artists, BSD, Library, docents, targeted communities
6.3	Develop approaches and plans for Bellevue's Public Art Program that will guide implementation	High	City, 4Culture, other Public Art Programs, arts commissions, artists
6.4	Establish an initiative that encourages the integration of public art into private developments	Very high	City, developers and facility managers
<b>GOAL 7</b>			
<b>Develop a mix of stable and sustainable public and private sector funding and support mechanisms to strengthen Bellevue's arts and cultural sector.</b>			
7.1	Confirm and build the City's leadership role in supporting arts and culture.	High	City departments and elected officials, cultural organizations
7.2	Convene leadership committee of business civic leaders	High	Cultural organizations, civic and business leadership
7.3	Enhance City funding for arts and cultural organizations by exploring municipal funding mechanisms.	Very high	City leadership, corporate leadership, cultural leadership, community leadership
7.4	Explore a "round-up" funding program	High	City, appropriate corporate sponsors (including utility, cable, and others that consistently bill for services)
7.5	Improve cultural organizations' fund-raising, earned income capacity, explore options for cost reductions	High	City, cultural organizations, technical service providers
<b>GOAL 8</b>			
<b>Establish or refine City of Bellevue and community structures and systems to assist in the delivery of the range of cultural initiatives outlined in this Plan..</b>			
8.1	Establish a discrete administrative entity with primary responsibility for implementation	Very high	Appropriate City staff, elected officials, private sector

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# Introduction

## ***Background and History of the Process***

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Responding to Bellevue City Council's vision to establish Bellevue as the cultural hub of the Eastside, the Arts Commission requested and Council agreed to fund a city-wide strategic visioning process entitled "Cultural Compass: A Strategic Vision for Arts and Culture." In May, 2003, the City of Bellevue contracted with Wolf, Keens & Company to conduct the planning process and write the plan. The firm coordinated the year-long project, working with the Bellevue Arts Commission and City staff, under the leadership of a volunteer community-based Steering Committee. The mission of this planning initiative was summarized as follows:

*To develop an action-oriented, inclusive, consensus strategic vision for Bellevue's cultural development that realistically addresses the needs, interests, and priorities of all of Bellevue's residents.*

Among the tasks completed as part of this project were the following:

- Conducting a **comprehensive cultural assessment** through a series of individual and group meetings and interviews that engaged over two hundred individuals from all sectors of the City (as well as representatives from outside Bellevue), including artists, representatives of arts and cultural organizations, business and civic leaders, educators, youth, and many others.
- Facilitating a series of meetings with the **Steering Committee**, a 31-member body invited by the Mayor specifically to oversee the development of the Cultural Master Plan.
- Assisted in developing a **Cultural Compass Forum** that brought regional and national leaders in cultural planning to speak in Bellevue, at which over 75 residents offered their views on priorities for arts and culture.
- Performing a **financial analysis** of Bellevue-based cultural organizations and a review of education programs in the City.

- 
- Compiling a **cultural organization and program inventory**, providing baseline information on about 23 local organizations.

As part of the planning process, the consultants worked with the Steering Committee and submitted a series of papers for community review and comment. The process has been designed to learn from itself by gathering and assessing new information as an ongoing part of the study process and then bringing those findings back for review. This iterative sequence was quite effective in informing the cultural assessment.

1. After the initial round of interviews and a material review, the consultants summarized what they had learned in a document called “**Emerging Themes**.” This formed the information base for a meeting of the Steering Committee and was revised based on feedback generated there.
2. Using the revised “**Themes**” document and with information from additional interviews and early research findings, the consultants developed a series of seven “**Compass Papers**” in key areas – facilities, diversity, culture and business relations, education, organizations and artists, public art, and resources. The papers were reviewed by the Steering Committee and made available to all participants.
3. Each **Compass Paper** was then reviewed in depth by a “**working group**” made up of individuals with particular interest or expertise in the topic area and revisions were made to the Papers.
4. With the revised **Compass Papers** and the findings of the various **research components**, the consultants generated a **preliminary set of goals and strategies**, which were reviewed with the Steering Committee. Feedback from that meeting was used to refine those goals and strategies for this **cultural master plan** report.

The consultants will discuss the sequence of events to follow the distribution of this report in a later section of the report.

## ***The Rationale for the Plan’s Structure***

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First, a definition is in order: what do we mean by “culture?” It is not easy to find a single word that describes the range of activities and events that this plan encompasses. The best that has been identified is “culture.” In this plan, by culture we refer to the entire range of fine and traditional art forms, the humanities, as well as the history and heritage of this community and this region. We include the built environment, as well as the folkways and traditions of all of Bellevue’s residents, long-time and newly arrived.

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Bellevue is just fifty years old. It is still inventing itself. While much of the ground work is laid – a transportation grid, a role as a commercial center for the Eastside for just two examples – there is still much to be worked out. The role of arts and culture in Bellevue is one such area and it is a priority. In 2000 and 2003, the City Council established four “cultural infrastructure” priorities which serve to guide this planning document. They are:

- Create community and neighborhood festivals and events
- Support cultural enhancements/performing arts center
- Define City role in promoting and sponsoring the cultural arts
- Define what actions would be necessary for Bellevue to become the cultural center of the Eastside.

The City is blessed with a strong mix of cultural organizations and many people with an interest in and a commitment to arts and culture. There is a substantial range of cultural offerings. Yet at the same time, there is a lack of consensus about what the City should aspire to relative to arts and culture and what a uniquely Bellevue mix of cultural organizations, events, and activities would look like. Indeed, Bellevue’s cultural sector is not much older than its City. The cultural sector faces some challenging decisions about what, in short, it should be when it grows up.

The cultural plan has been designed to address that very question. It provides a “strategic vision for arts and culture” that describes what a more mature cultural sector might look like ten years down the road. That vision is grounded in the consultants’ research and tempered by the comments of Steering Committee members, the Bellevue Arts Commission, and over 200 other planning participants. Four “core initiatives” in Part I of this report paint a fuller picture of what implementation of the vision might look like.

## ***Contents of the Report***

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This report is divided into ten sections. Part I describes the strategic vision for arts and culture in Bellevue and outlines four over-arching initiatives that grow out of that vision.

In Parts II through IX, the plan offers key findings in eight topic areas that emerged in the planning process, as well as specific goals and the necessary strategies to reach the vision. The sections are:

- Part II: Cultural Organizations and Artists
- Part III: Cultural Economic and Business Development
- Part IV: Cultural Diversity
- Part V: Cultural Education
- Part VI: Cultural Facilities

- 
- Part VII: Public Art
  - Part VIII: Cultural Resources
  - Part IX: Administrative Infrastructure

The goals and strategies are presented in the plan in eight distinct sections, although they are not always exclusive to one topic area and issues and assessments often overlap and may apply to multiple areas.

After each strategy, the consultants provide two pieces of information:

- A general assessment of the priority of the strategy in the form of moderate, high, or very high (nothing of low priority made it into the document)
- A list of potential partners that might logically be considered for involvement in the initiative.

These are not meant to be either definitive or proscriptive. For instance, other organizations than those listed might have strong interest in a particular strategy; others that are listed may decide not to be involved. This information is provided as a starting place for a more involved community process of implementation.

The final section of the report, Part X, provides details on the necessary next steps to move from planning to implementation.

## ***Acknowledgements***

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This planning process has required a great deal of assistance from a wide range of people. We would like to express our appreciation for the cooperation of the many people who gave their time and energy to participate in the meetings that have comprised this process. The City Council of Bellevue appropriated the necessary funds. In addition, the Arts Commission, the Steering Committee members (listed in an appendix at the end of this report) and Steering Committee co-chairs met on numerous occasions to work on aspects of this plan. The City's Planning and Community Development Department and its Arts Specialist, as well as other City staff, have all contributed to the success of the effort. We are especially grateful to those who provided space for our meetings: Bellevue Art Museum, Bellevue High School, Crossroads Community Center, Music Works Northwest, and North Bellevue Community Center. Thank you all.



# EAST LINK LIGHT RAIL



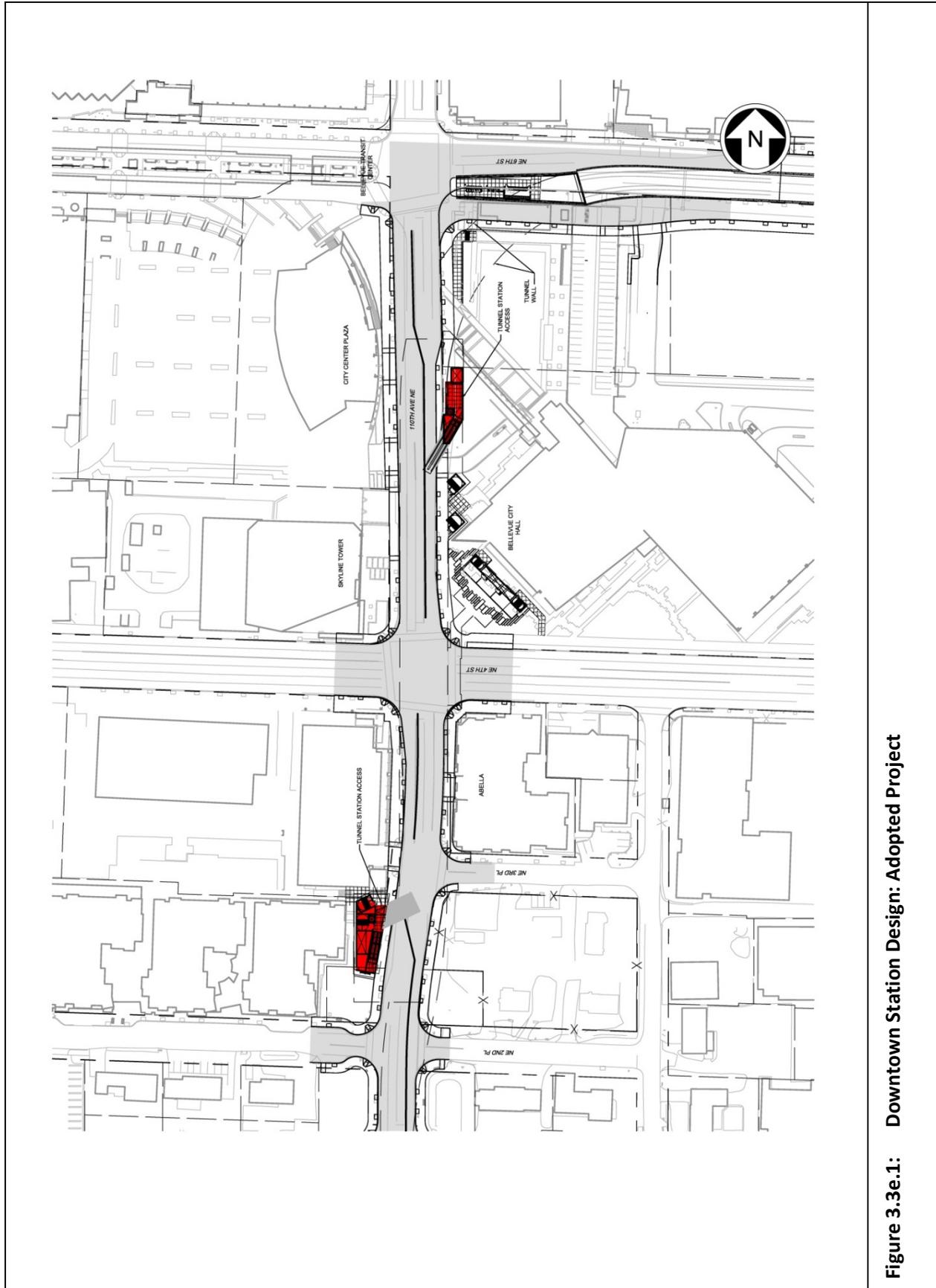


Figure 3.3e.1: Downtown Station Design: Adopted Project

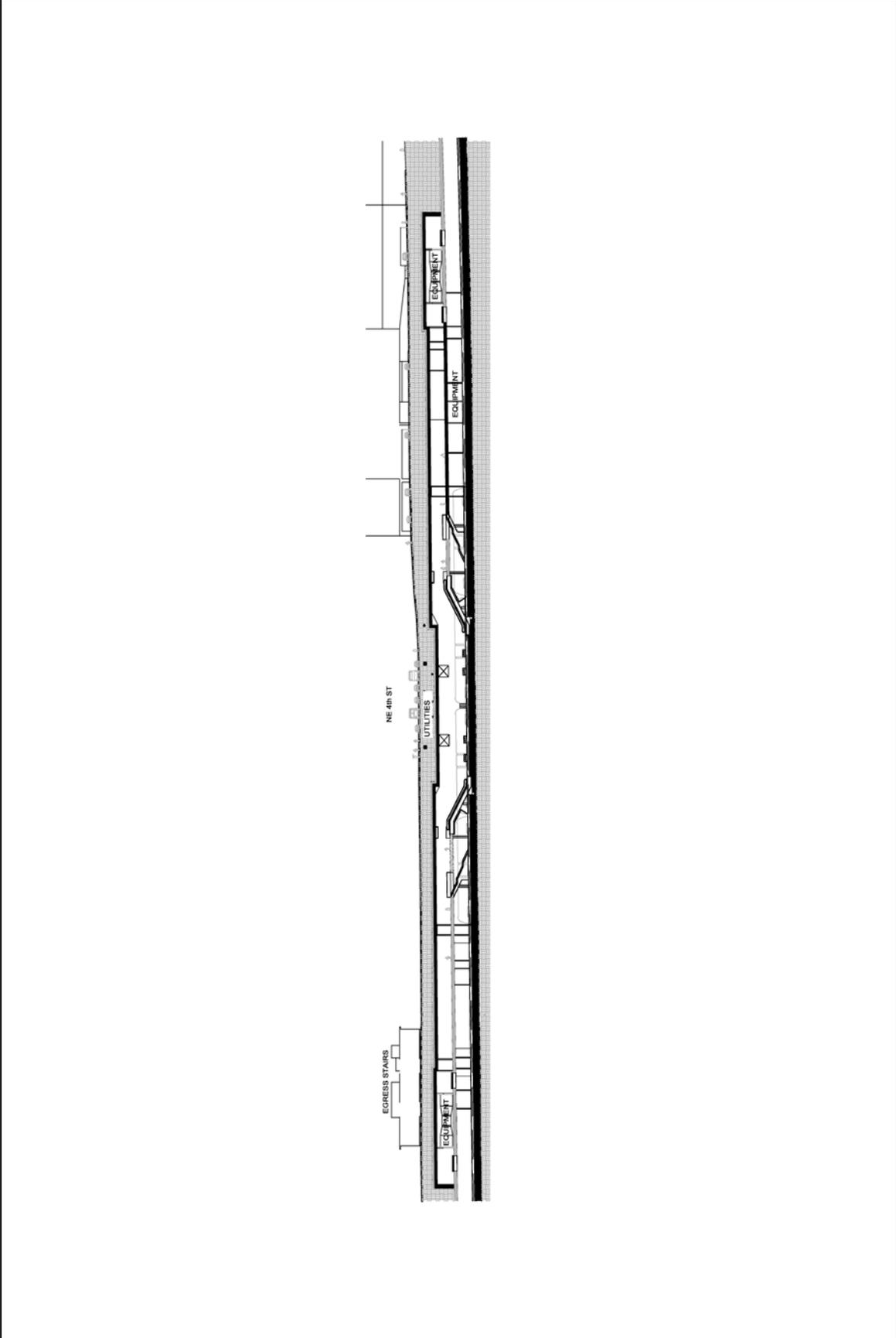


Figure 3.3e.2: Downtown Station Design: Adopted Project



Figure 3.3e.3: Downtown Station Design: Adopted Project

### 3.3 Downtown Station Design

#### 3.3.1 Cost Savings Idea 3e –Optimize Adopted Project (PE)

Table 3-4

**Cost Savings Evaluation: Downtown Station Design – Optimize Adopted Project (PE) - 3e**

Description: Downtown Station Design - Optimize Adopted Project	Proposal: 3e
<p><b>MOU Project:</b> Provides a cut-and-cover tunnel and station with tracks side-by-side, with track spacing widening at the station to provide for a center platform and mezzanine above to transition passengers from center to side(s) of 110th Ave. NE.</p>	
<p><b>Cost Savings Idea (3e): Optimize Adopted Project (Preliminary Engineering - PE)</b> - The first phase of advancing the PE included analysis of design refinements to the station structure only, and included the removal of 50 feet of mezzanine at each end of the station. In addition, the PE design included a pair of up and down escalators plus public stairs at each end of the platform; these were changed to an up escalator and down stair at each end, reducing the number of escalators by two. It also reduced the station, platform and tunnel width by 2 feet from the current PE design. This Cost Savings Idea evaluates the relocation of the North Station Entrance from its current location in front of City Hall to the West Side of 110<sup>th</sup> Ave in front of the City Center Plaza building. This option would also locate the stair, escalator and elevator outside of the foundation wall of City Center Plaza. The current phase includes further analysis of the track alignment between the East Main Station and the north tunnel portal (“portal to portal”) and a further analysis of ventilation requirements.</p> <p><b>Why Consider this Configuration:</b></p> <ul style="list-style-type: none"> <li>• It would provide a west side entrance closer to the Bellevue Transit Center to facilitate bus transfers and access into downtown Bellevue.</li> <li>• This Cost Savings Idea reduced the station, platform, and tunnel width from current adopted project (PE design) and raised the tunnel alignment.</li> <li>• This Cost Savings Idea optimizes LRT operations through the tunnel. It maintains operational speed and trip time at both NE 6<sup>th</sup> St. and crossing I-405.</li> <li>• This option maintains four travel lanes on 110<sup>th</sup> Ave NE between NE 4<sup>th</sup> and NE 6<sup>th</sup> St. Although the option removes the dedicated northbound left-turn into the Bellevue Transit Center, a left turn only movement for buses into the Bellevue Transit Center may be considered.</li> </ul> <p><b>Design Considerations Addressed (From Sound Transit and City of Bellevue Cost Savings Work Plan - Motion M2012 -41 dated June 28, 2012):</b></p> <ul style="list-style-type: none"> <li>• <u>Optimize configuration to minimize impacts to surface traffic while retaining entrances north and south of NE 4<sup>th</sup>.</u> The PE design was optimized to look for cost savings. Option maintains four lanes on NE 110<sup>th</sup> Ave. and four lanes on NE 6<sup>th</sup> St. This Cost Savings Idea removes the dedicated northbound left-turn into the Bellevue Transit Center. However, a left turn only movement for buses, only, into the Bellevue Transit Center may be considered.</li> </ul>	

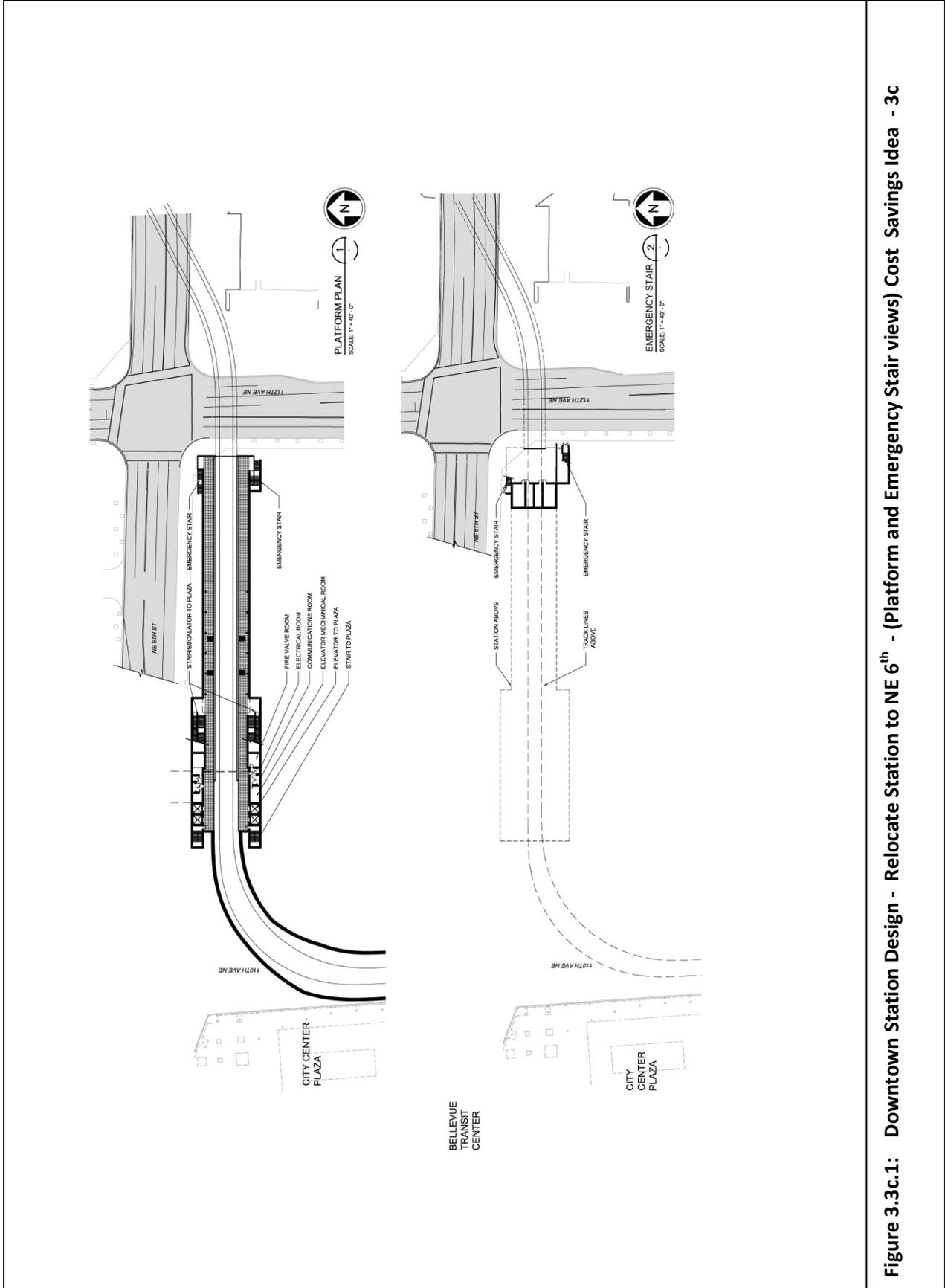


Figure 3.3c.1: Downtown Station Design - Relocate Station to NE 6<sup>th</sup> - (Platform and Emergency Stair views) Cost Savings Idea - 3c

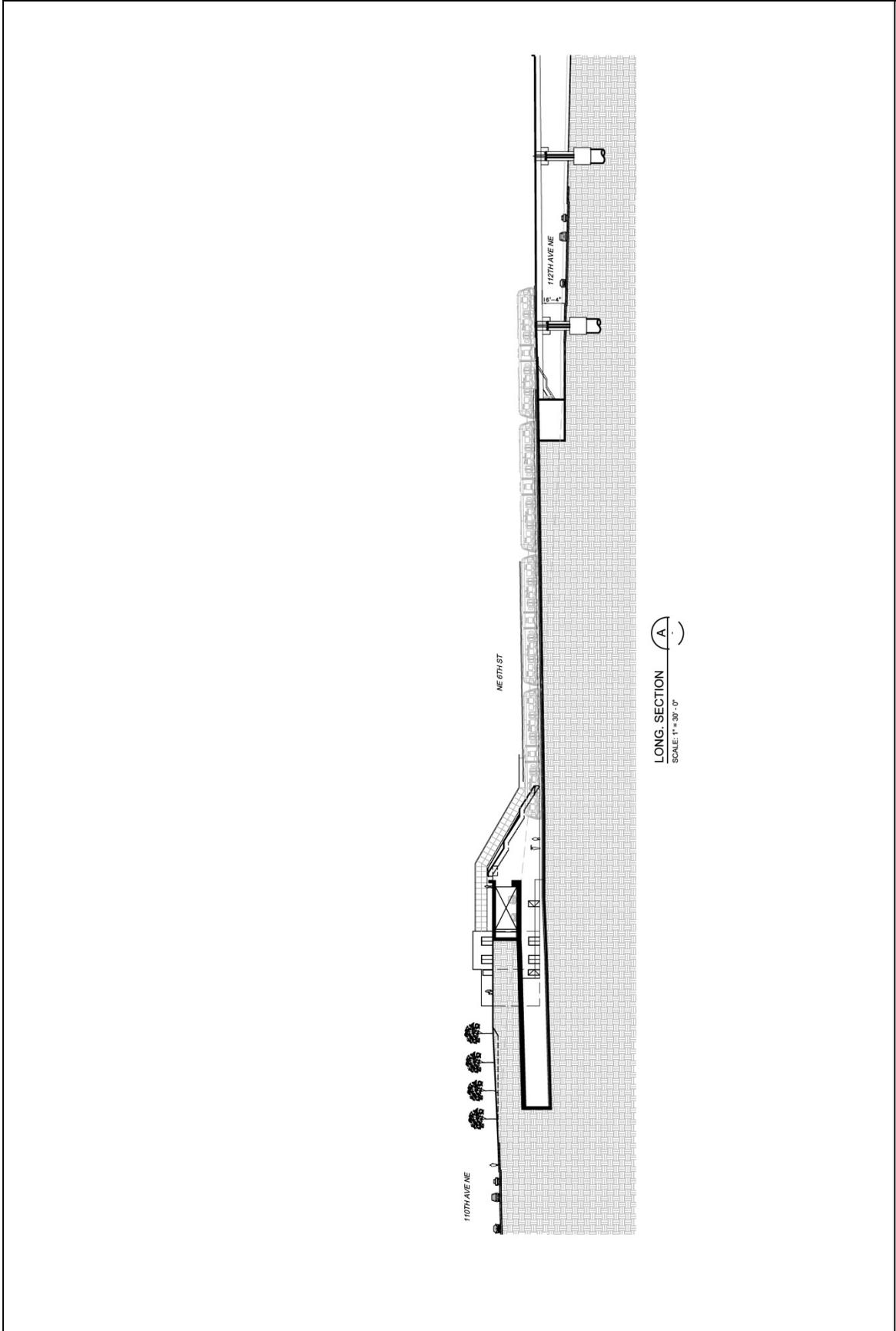


Figure 3.3c.2: Downtown Station Design - Relocate Station to NE 6<sup>th</sup> – Cost Savings Idea - 3c

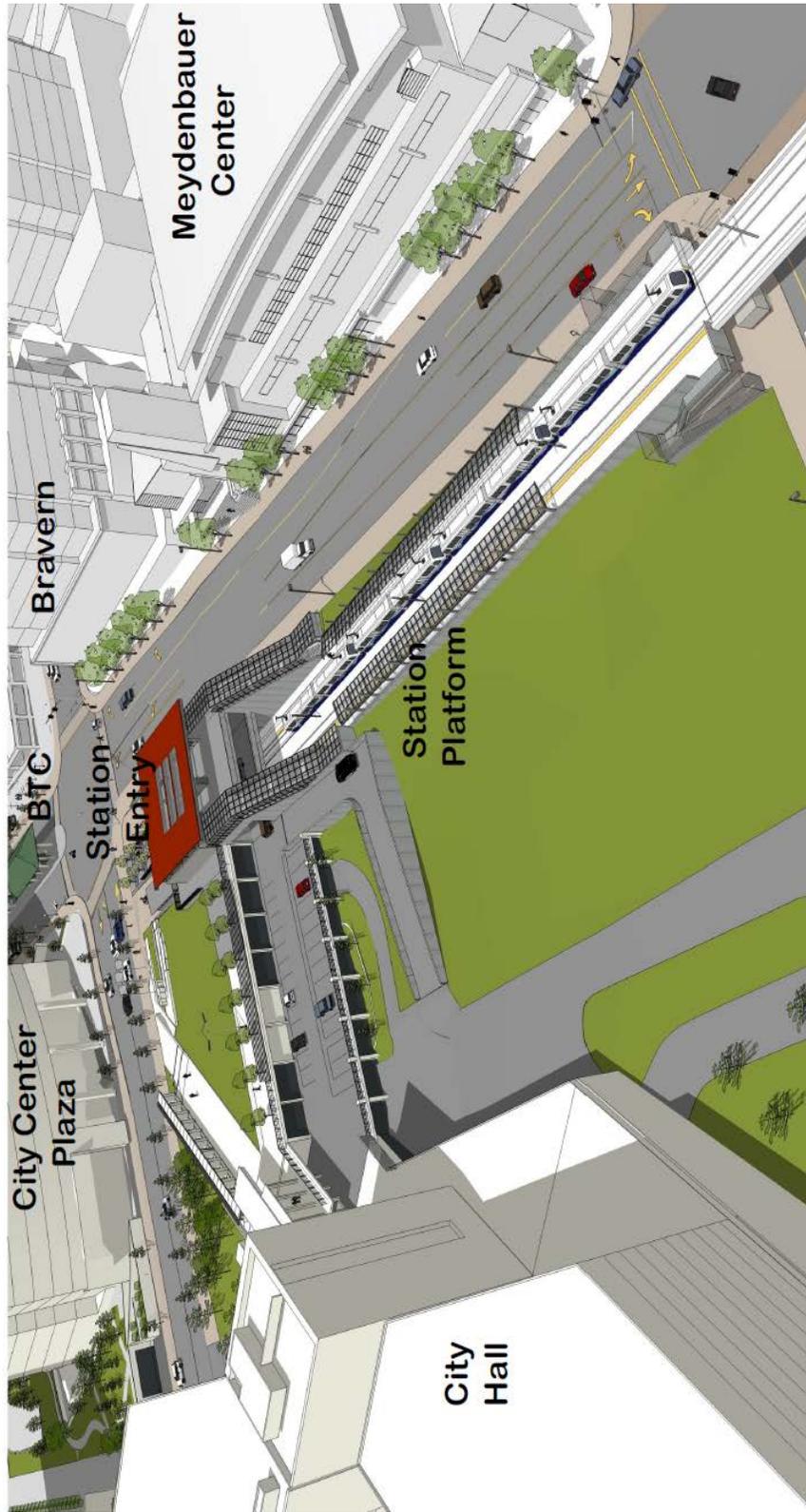


Figure 3.3c.3: Downtown Station Design - Relocate Station to NE 6<sup>th</sup> - Cost Savings Idea - 3c

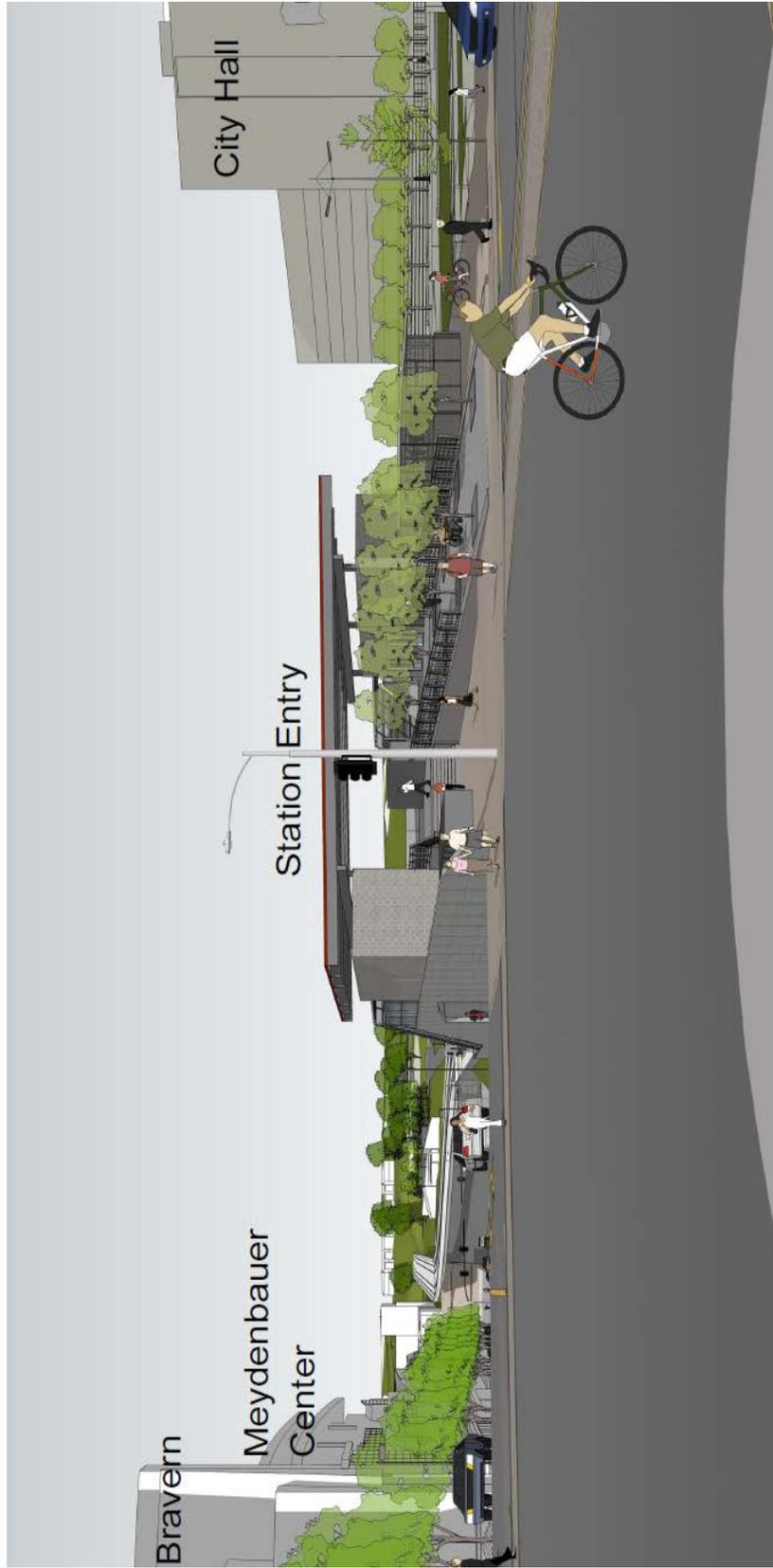


Figure 3.3c.4: Downtown Station Design - Relocate Station to NE 6<sup>th</sup> - Cost Savings Idea -Street view - 3c

### 3.3.5 Cost Savings Idea – Relocate Station to NE 6<sup>th</sup> St. – 3c

Table 3-6

Cost Savings Evaluation: Downtown Station Design – Relocate Station to NE 6<sup>th</sup> St - 3c

Description: Downtown Station Design	Proposal: 3c
<p><b>MOU Project:</b> Provides a cut-and-cover tunnel and station with tracks side-by-side, with track spacing widening at the station to provide for a center platform and mezzanine above to transition passengers from center to side(s) of 110th Ave. NE.</p>	
<p><b>Cost Savings Idea (3c): Relocate Station to NE 6<sup>th</sup> St</b> – This idea would move the station to the south edge of the NE 6<sup>th</sup> St. corridor, the station is “daylighted”, and the side platforms become partly on-grade and partly elevated as it approaches 112<sup>th</sup> Ave. NE. This configuration features surface access from the City Hall plaza. The platform has public access only from the west end.</p> <p>Vertical circulation from the west end of the side platform is by means of elevators, escalators and stairs down from the City Hall Plaza. The east end of the side platform is served by emergency egress stairs only.</p> <p>By moving the station from its current PE location in 110<sup>th</sup> Ave, a vertical realignment of the tunnel is possible, resulting in a shallower tunnel.</p> <p>Why Consider this Configuration:</p> <ul style="list-style-type: none"> <li>• Eliminates underground station construction costs.</li> <li>• Maintains current configuration of 110<sup>th</sup> Ave. NE and NE 6<sup>th</sup> St.</li> <li>• Maintains an entrance near City Hall and the Bellevue Transit Center.</li> </ul> <p>Design Considerations Addressed (From Sound Transit and City of Bellevue Cost Savings Work Plan - Motion M2012-41 dated June 28, 2012):</p> <ul style="list-style-type: none"> <li>• <u>Reach agreement on impacts to City Hall and damages payment prior to further design</u> – The Collaborative Design Process Team will reach agreement on the extent of impacts to City Hall and compensation for damages prior to a decision to select this Cost Savings Idea.</li> <li>• <u>Determine acceptability of design deviation (curve at 110<sup>th</sup>/NE 6<sup>th</sup>)</u> – Current conceptual design results in a design deviation (curve at 110<sup>th</sup>/NE 6<sup>th</sup> and curve from station to I-405) resulting in slower LRT operational speeds through the station area. Speeds are reduced from 20 mph to 10 mph west of the station and from 35 mph to 20 mph east of the station.</li> </ul>	



## Executive Summary

### Plan Vision

Bellevue's Comprehensive Plan acknowledges that anticipated growth in travel necessitates a multi-modal transportation approach that offers the public choices about how they travel within, to, and through Bellevue. The new Pedestrian and Bicycle Transportation Plan – adopted by City Council on 2/17/2009 - establishes the City's overall vision.

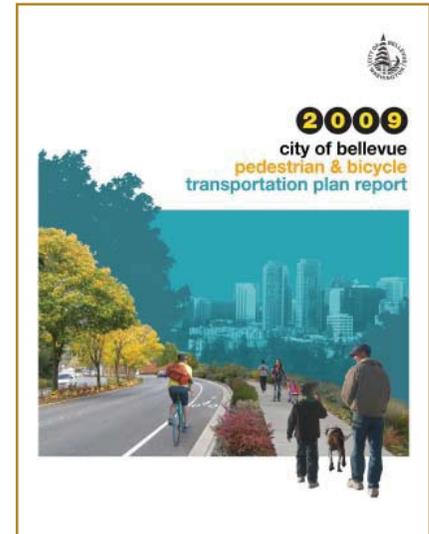
*To plan, design, build, and maintain an integrated, comprehensive network of pedestrian and bicycle facilities in collaboration with community stakeholders. In doing so, the City will advance the following objectives:*

- *Provide transportation choices for those who can or wish to travel by foot or bicycle to destinations within their neighborhood, city, and the greater Eastside and region*
- *Improve health and fitness, and enhance recreational benefits*
- *Ensure that those in the community who cannot drive due to age, income or disability have mobility options*
- *Provide a safe and accessible street environment for all users*
- *Improve overall neighborhood livability*
- *Support and enhance public transit use*
- *Reduce air and noise pollution, energy use, and oil consumption*
- *Support economic development*

### Plan Strategies

The Plan is the product of extensive public outreach, research, inter-agency coordination, and field work, and review by the Transportation Commission. The outreach effort included on-line surveys, focus groups, and conversations with citizens at public events and over the Internet. In response to public feedback, the Plan aims to achieve the following:

- **Implementation Targets.** Complete a connected network of citywide and downtown bicycle routes; make substantial progress on the sidewalk network within 10 years; decrease collisions; and, increase the amount of biking and walking.
- **Improvement Priorities.** Give special consideration to projects that improve network connectivity, enhance accessibility to major community facilities, and address safety issues.
- **Context Sensitive Design.** Work with the public in designing transportation facilities that are safe, attractive, and compatible with surrounding land uses.
- **Coordination.** Implement public education and encouragement programs, enabling policies, and land use patterns that support bicycle and pedestrian movement.
- **Best Practices.** Look to other cities for examples of innovative pedestrian and bicycle initiatives and assess how these strategies might be incorporated into Bellevue's programs.
- **Accommodation.** Consider the needs of pedestrians and bicyclists in planning and designing road projects.



**The complete Plan Report is available at: [http://www.bellevuewa.gov/walking\\_biking.htm](http://www.bellevuewa.gov/walking_biking.htm)**



## Project List

*One of the most important outcomes of the Plan is a detailed assessment of Bellevue’s transportation network resulting in specific recommendations for new pedestrian and bicycle facilities. The Plan includes 435 projects that when built will yield 90 miles of sidewalk, 144 miles of bikeway, and 20 miles of trail facility improvements.*

The Plan is composed of a variety of different facility types aimed at pedestrians and bicyclists of all levels of experience. From shared bicycle facilities and 5 foot-wide sidewalks on quiet streets to bicycle lanes with 6 foot-wide sidewalks and 4 foot-wide planter strips on arterials, the pedestrian and bicycle network can address the needs of a range of users as well as be customized to the constraints and opportunities in a wide range of contexts and locations.



***The plan vision is for a pedestrian network that will connect important destinations with sidewalks, trails, and other routes so that walking becomes a more viable transportation option.***



***Priority bicycle corridors will create a continuous network that promotes connections to surrounding jurisdictions and creates links among communities within the City.***

The implementation of this Plan requires a long-term commitment. As a long-range transportation plan, the Pedestrian and Bicycle Plan represents the first step in the project development process. There are numerous steps in moving a project from a long-range transportation plan through to construction.



## Implementation

Given that the plan represents a long range vision, all of the project descriptions in the plan are framed as “conceptual.” The images on page 4 of this flyer provide a snapshot into the potential future of some of the near-term (funded) projects in the Plan. The visual concepts should be regarded as illustrative only. Much additional engineering and design will be needed on all of these projects when determining the most optimal pedestrian and bicycle facilities.



***The vision for the Bel-Red Corridor emphasizes pedestrian and bicycle mobility (Ped-Bike Plan # O-110-N, S-100-N, and S-100-S).***

## New Tools

Creating good conditions for walking and cycling often requires adopting new and innovative designs. City staff are actively involved in evaluating different tools for improving walking and cycling conditions in Bellevue. The following are several recent examples:



***Bellevue’s initial “sharrow” experiment on 161st Ave SE is shown to improve both bicycle positioning and motorist behavior.***



***Rubber sidewalk panels at NE 10 St west of 102 Ave NE allow tree roots to grow under the sidewalk without creating a trip hazard.***

The new Plan Report is available at: [http://www.bellevuewa.gov/walking\\_biking.htm](http://www.bellevuewa.gov/walking_biking.htm) For more information, contact Franz Loewenherz, at 425-452-4077 or [FLoewenherz@bellevuewa.gov](mailto:FLoewenherz@bellevuewa.gov)



## Representative 2009-2015 Capital Investment Program (Funded Projects)

The photographs below on the left show streets as they look now. The photos on the right show the same streets with proposed improvements added.

**SE 26th St/SE 24th St to West Lake Sammamish Parkway (PW-W/B-76):** Construct curb, gutter and six foot sidewalk on south side; accommodate future bike lanes. Completion 2009.



**145th PI SE/SE 16th St to SE 24th St (PW-R-151):** Construct five foot bike lanes, curb, gutter and six foot sidewalk along both sides, and other landscaping where feasible. Completion 2010.



**108th Ave SE/Bellevue Way to I-90 (PW-W/B-71):** Construct five foot bike lanes on both sides and curb, gutter and six foot sidewalk on one side where missing. Completion 2012.





# Downtown Livability

## **EXISTING LAND USE CODE**





**Part 20.25A Downtown****20.25A.010 General.**

A. This Part 20.25A, Downtown, contains information which applies to development and activity within a Downtown Land Use District. Specific sections apply to limited areas within the Downtown Land Use Districts as follows:

1. Downtown Old Bellevue Design District, LUC 20.25A.070;
2. Civic Center Design District, LUC 20.25A.065;
3. Perimeter Design District, LUC 20.25A.090;
4. Downtown Core Design District, LUC 20.25A.100.

**B. Procedural Merger.**

Within the Downtown Land Use District, any administrative decision required by this Part 20.25A or by the Land Use Code, including but not limited to the following, may be applied for and processed through a single Design Review, Part 20.30F LUC:

1. Administrative Conditional Use Permit, Part 20.30E LUC;
2. Design Review, Part 20.30F LUC;
3. Variance, Part 20.30G LUC.

**C. Design Review Required.**

All development within a Downtown Land Use District must be reviewed by the Director of the Department of Planning and Community Development through Design Review, Part 20.30F LUC. (Ord. 4973, 3-3-97, § 101; Ord. 4816, 12-4-95, § 201; Ord. 4255, 6-3-91, § 4; Ord. 3653, 5-19-86, § 1)

**20.25A.020 Dimensional requirements – General.****A. Dimensional Requirements in Downtown Districts.**

1. General. Paragraph A.2 of this section (Chart: Dimensional Requirements in Downtown Districts) sets forth the dimensional requirements for each land use district in the Downtown. Each structure, development, or activity in a Downtown Land Use District shall comply with these requirements except as otherwise provided in this section. These dimensions are different for property located in the Perimeter Design District. If your property is within 1,200 feet of the north, south, or west boundary of the Downtown, you may be in this district. See LUC 20.25A.090.

Chart 20.25A.020.A.2

Dimensional Requirements in Downtown Districts

Downtown Land Use District	Building Type (7)(14)	Minimum Setback (3)(9)			Maximum Building Floor Area per Floor Above 40' (5)(9)(18)(24)	Maximum Building Floor Area per Floor Above 80' (5)(18)(24)	Maximum Lot Coverage	Building Height (6)		Floor Area Ratio	
		Front (1)(15)	Rear (25)	Side (25)				Basic	Max.	Basic (10)	Max. (8)
DNTN-O-1	Nonresidential	0 (2)	0/20'	0/20'	24,000 gsff	24,000 gsff	100%	200'	300'	5.0	8.0
	-Diminishing				30,000 gsff	24,000 gsff					
	Residential	0 (2)	0/20'	0/20'	20,000 gsff	12,000 gsff	100%	200'	450'	5.0	Unlimited
	Parking	0 (2)	0/20'	0/20'	20,000 gsff	20,000 gsff	100%	100'	100'	N/A	N/A
DNTN-O-2	Nonresidential	0 (2)	0/20'	0/20'	24,000 gsff	24,000 gsff	100%	150'	250'	4.0	6.0
	-Diminishing				30,000 gsff	24,000 gsff					
	Residential	0 (2)	0/20'	0/20'	20,000 gsff	12,000 gsff	100%	150'	250'	4.0	6.0
	Parking	0 (2)	0/20'	0/20'	20,000 gsff	20,000 gsff	100%	100'	100'	N/A	N/A
DNTN-MU (20)	Nonresidential	0	0/20'	0/20'	22,000 gsff	20,000 gsff	100%	60'	100'	0.5	3.0
	-Diminishing				30,000 gsff (23)	22,000 gsff					
	Residential	0	0/20'	0/20'	20,000 gsff	12,000 gsff (21)	100%	150'	200'	2.0	5.0
	Parking	0	0	0	20,000 gsff	N/A	75%	60'	60'	N/A	N/A
DNTN-R (20)	Nonresidential	0	0	0	20,000 gsff	12,000 gsff	75%	60'	65'	0.5	0.5
	Residential	0	0/20'	0/20'	20,000 gsff	12,000 gsff	100%	150'	200'	2.0	5.0
	Parking	0	0	0	N/A	N/A	N/A	40'	40'	N/A	N/A
DNTN-OB (20)	Nonresidential	0 (4)	0/20'	0/20'	20,000 gsff	12,000 gsff	100%	60'	100'	0.5	1.0
	Residential	0 (4)	0/20'	0/20'	20,000 gsff	12,000 gsff	100%	150'	200'	2.0	5.0
	Parking	0	0	0	N/A	N/A	75%	40'	40'	N/A	N/A
DNTN-OLB	Nonresidential	20' (19)	20' (19)	0/20'	22,000 gsff	N/A	60%	75'	75'	0.5	3.0
	-Diminishing				30,000 gsff (23)	N/A					
	Residential	20' (19)	20' (19)	0/20'	20,000 gsff	12,000 gsff	75%	75'	90'	2.0	3.0
	Parking	20' (19)	20' (19)	0	20,000 gsff	N/A	75%	45'	45'	N/A	N/A

gsff = Gross Square Feet Per Floor (24)

**Notes: Dimensional requirements in Downtown Districts**

- (1) Measured from inside edge of the required perimeter sidewalk. If existing utilities which cannot reasonably be relocated require the planting of street trees on the property side of a sidewalk as provided for in LUC 20.25A.060, four feet is added to the required setback.
- (2) No parking or vehicle access lane is permitted between the required perimeter sidewalk and the main pedestrian entrance to the building.
- (3) Minimum setbacks are subject to required landscape development. See LUC 20.25A.040.
- (4) The maximum setback from Main Street in the Downtown-OB District is 0 feet.
- (5) For floors above 40 feet, gross square feet per floor may be averaged unless an applicant takes advantage of the diminishing floor plates alternative described in subsection B of this section.
- (6) The maximum building height may only be achieved by participation in the FAR Amenity Incentive System, LUC 20.25A.030.
- (7) A single building is considered residential if more than 50 percent of the gross floor area is devoted to residential uses. See LUC 20.50.020 for the definition of "gross floor area."
- (8) The maximum permitted FAR may only be achieved by participation in the FAR Amenity Incentive System, LUC 20.25A.030. Where residential and nonresidential uses occur in the same building, the FAR is limited to the maximum FAR for the building type as determined in accordance with Note (7).
- (9) See subsection B of this section for exceptions to the minimum setback and maximum building floor area per floor above 40 feet requirements.
- (10) All new development and all substantial remodels must participate in the FAR Amenity Incentive System. See subsection C of this section for amenity requirements regarding the provision of basic floor area.
- (11) Intentionally deleted.
- (12) Intentionally deleted.
- (13) Intentionally deleted.
- (14) Except as set forth in Note (18), hotels and motels shall be considered as residential structures.
- (15) If the subject property abuts a street classified pursuant to LUC 20.25A.115, Design Guidelines – Building/Sidewalk Relationships, the maximum setback is 0 feet unless otherwise approved by the Director.
- (16) Intentionally deleted.
- (17) Intentionally deleted.
- (18) For the purposes of determining Maximum Building Floor Area per Floor, including the averaging and diminishing floor plate methods described in Note (5) and subsection B of this section, hotels and motels shall be considered as nonresidential structures.

- (19) No parking or vehicle access lane is permitted between the sidewalk on 112th Avenue NE and the main pedestrian entrance to the building, except that a drop-off lane may be permitted for a hotel or motel entrance. The maximum setback from 112th Avenue NE is 30 feet, unless a greater setback is approved by the Director of the Development Services Department to permit a drop-off lane. See paragraph B.2 of this section for exceptions to setback requirements.
- (20) Some dimensions are different for properties located in the Perimeter Design District. For property within 1,200 feet of the north, south or west boundary of the Downtown, see LUC 20.25A.090 for the Perimeter Design District requirements.
- (21) Applicable only to building floors above 100 feet in height.
- (22) See LUC 20.25A.020.B.1.d for diminishing floor plate provisions.
- (23) Not applicable to Perimeter Design Subdistricts A and B.
- (24) Gross Square Feet Per Floor (gsf/f) refers to the floor area in square feet within the surrounding exterior walls measured from the interior wall surface and including openings in the floor plate such as vent shafts, stairwells, and interior atriums.
- (25) Where building height exceeds 75 feet, there shall be a minimum side setback of 20 feet and a minimum rear setback of 20 feet from any interior property line applicable to all floors above 40 feet. See subsection B of this section for exceptions to this minimum setback requirement.

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## B. Exceptions to Dimensional Requirements.

### 1. Floor Plate Exceptions.

- a. For residential buildings, the "Maximum Floor Area per Floor above 40 Feet" may be increased by not more than 10 percent through Design Review, Part 20.30F LUC, if the applicant demonstrates that the increase is necessary for reasonable development of the building, and will not have a significant adverse effect on other properties. Each square foot of floor area above the maximum requires a proportionate square footage of amenity in conformance with LUC 20.25A.030.C; however, the amenity area provided under this requirement may not be used to exceed the basic floor area ratio.
- b. For buildings on streets designated "A" or "B" in LUC 20.25A.115, Design Guidelines – Building/Sidewalk Relationships, the limitation of "Maximum Building Floor Area per Floor Above 40 Feet" may be modified through Design Review to apply to floor area above 45 feet. In this case, the applicant must demonstrate that the modification is necessary for creating higher, more prominent and distinctive ground-level pedestrian-oriented frontage; and the minimum retail floor height shall be 16 feet.
- c. For structures that do not exceed 70 feet in height (as defined by the International Building Code, as adopted and amended by the City of Bellevue), the Director may allow the connection of floor plates above 40 feet such that those floor plates exceed the "Maximum Building Floor Area per Floor Above 40 Feet"; provided, that:
  - i. The connection is to allow for safe and efficient building exiting patterns. The connecting floor area shall include required exiting corridor area and may include the floor area of units or other building uses;
  - ii. The connection occurs on no more than three floor levels above 40 feet; and
  - iii. The alternative design results in a building mass that features separate and distinct building elements.

- d. **Diminishing Floor Plate.** A nonresidential building in the Downtown O-1, Downtown O-2, Downtown MU, and Downtown OLB Land Use Districts can exceed the "Maximum Building Floor Area per Floor Above 40 Feet," to a maximum of 30,000 square feet, if the building incorporates two floor plates which are each at least 20 percent smaller than the floor plate of the floor below it. If only one floor of a nonresidential building exceeds the "Maximum Building Floor Area per Floor Above 40 Feet," the building must incorporate one floor plate which is at least 20 percent smaller than the floor plate of the floor below it. Exposed roof area at the level of the diminished floor must have some amount of landscaping and be physically accessible for use; or, if not a flat surface, must provide a visually interesting roof form.
- e. **Performing Arts Centers** may have unlimited floor area per floor up to 100 feet in height, measured from average finished grade, provided that:
  - i. The proposal site abuts 106th Ave NE, between the south side of NE 10th Street and the north side of NE 2nd Street, and where feasible, an entrance is provided on 106th Avenue NE; and
  - ii. The floor plate exception applies only to that portion of the building which contains the performing arts use and subordinate uses do not exceed 25 percent of the total area; and
  - iii. The ground floor design is consistent with the Building/Sidewalk Design Guidelines for "A" rights-of-way, excluding the arcade provision; and
  - iv. The exception from the floor plate limitation is the minimum necessary to accommodate the performing arts center use, or equipment functionality related to that use.

## 2. Setback/Stepback Exceptions.

- a. Marquees or awnings which comply with the requirements of LUC 20.25A.030.C are permitted to extend over the public right-of-way upon approval of the Director of the Transportation Department and the Director of the Development Services Department notwithstanding the provisions of the Sign Code, Chapter 22B.10 BCC, or any other City Code.
- b. The Director of the Development Services Department may approve an intrusion into the 20-foot front yard setback from the east side of 112th Avenue NE in the Downtown-OLB District to permit the location of pedestrian-oriented frontage retail uses within a portion of the required setback area. The intrusion shall be limited to a maximum of 30 percent of the required front yard setback area. All building areas within the setback areas shall be devoted to pedestrian-oriented uses and meet the design criteria of LUC 20.25A.030 for pedestrian-oriented frontage. Amenity floor area earned may be used to exceed the permitted basic floor area ratio.
- c. The Director may allow modifications to the minimum side and rear setback required above 40 feet for buildings with a building height in excess of 75 feet if:
  - i. The applicant can demonstrate that the resulting design will be more consistent with the Design Review criteria of LUC 20.25A.110; and
  - ii. The building design, with the modification, will create sufficient spacing between towers to encourage a feeling of an open and airy downtown.
- d. Minor building elements may intrude into any required setback of this section, subject to all the terms and conditions of LUC 20.20.025.C.

- e. The Director may allow modifications to the setback requirements above 40 feet for performing arts centers meeting the locational requirements of subsection B.1.e.i of this section if:
    - i. The applicant can demonstrate that the resulting design will be more consistent with the Design Review criteria of LUC 20.25A.110; and
    - ii. Interesting roof forms, significant floor plate modulation, significant façade modulation, or other such unique architectural features are provided to minimize impacts to abutting structures.
3. Floor Area Ratio Exceptions.
- a. Up to a maximum of 1.0 FAR of the floor area in a project limit that is devoted to retail activities will not be counted for the purpose of calculating FAR in the proportions set forth in LUC 20.25A.115, so long as the retail activities are designed and located in compliance with:
    - i. LUC 20.25A.115;
    - ii. Guidelines 2 and 3 for "A" Rights-of-Way in Section IV.F of the Design Guidelines – Building/Sidewalk Relationships; and
    - iii. If applicable, Guidelines and Standards for Upper Level Retail in Section IV.F of the Design Guidelines – Building/Sidewalk Relationships.

- b. In the area of the Downtown bounded on the west by Bellevue Way, on the east by 112th Avenue NE, on the south by NE 4th Street and on the north by NE 8th Street, the maximum Floor Area Ratio may be exceeded if the major pedestrian corridor is constructed as required by LUC 20.25A.100.E.1, or the major public open space is constructed as required by LUC 20.25A.100.E.4. The maximum Floor Area Ratio may only be exceeded by the amount provided for under the major pedestrian corridor amenity bonus, LUC 20.25A.030.C.16 or by the amount provided for under the major public open space amenity bonus, LUC 20.25A.100.E.4.d.vi, or by a combination thereof.

#### 4. Height Exceptions.

- a. The maximum height identified in subsection A of this section may be increased by no more than 15 percent or 15 feet, whichever is greater, and only if the applicant can demonstrate that the additional height accommodates architecturally integrated mechanical equipment, interesting roof forms, significant floor plate modulation, significant facade modulation, or other such unique architectural features, and that the resulting design exceeds the quality and design requirements of LUC 20.25A.110.
- b. In the area of the Downtown bounded on the west by Bellevue Way, on the east by 112th Avenue NE, on the south by NE 4th Street and on the north by NE 8th Street, the maximum height identified in LUC 20.25A.020.A may be exceeded if the major pedestrian corridor is constructed as required by LUC 20.25A.100.E.1, or if the major public open space is constructed as required by LUC 20.25A.100.E.4. The maximum height may only be exceeded by the amount required to accommodate the additional Floor Area Ratio provided under LUC 20.25A.030.C.16 or to accommodate the additional Floor Area Ratio provided under LUC 20.25A.100.E.4.d, or a combination thereof. Heights may be exceeded under this provision only to the extent depicted on the map entitled "Heights in Core Design District" in LUC 20.25A.030.E.
- c. Except within the Perimeter Design District, the maximum building height can be exceeded if right-of-way is dedicated as provided by subsection D of this section but only to the extent of the floor area earned as a result of the dedication. This provision does not preclude the applicability of subsection B.4.a of this section but in no event may the combined effect of applying that subsection and this subsection be to increase the building height over the maximum building height set forth in subsection A.2 of this section by more than 10 percent or 15 feet, whichever is greater.

#### 5. Lot Coverage Exceptions.

- a. Underground buildings as defined in LUC 20.50.050 are not structures for the purpose of calculating lot coverage.
- b. Buildings constructed partially below grade and not higher than 30 inches above average finished grade are not structures for the purpose of calculating lot coverage; provided, that the rooftop of the building shall be landscaped consistent with the planting requirements for the specific use that is proposed for the building roof area and for the land use district in which the use is located, or shall be developed with pedestrian improvements, such as plazas and walkways, as approved by the Director. All landscaping shall comply with standards set forth in LUC 20.20.520. The provisions of LUC 20.20.520.J (Alternative Landscaping Option) are applicable.

### C. Basic Floor Area Ratio Requirements.

1. General. Each new residential, nonresidential, and mixed use building and each remodel which expands the floor area of an existing residential, nonresidential, or mixed use building

by more than 50 percent within any three-year period shall provide one or more amenities from the following list:

- a. Pedestrian-oriented frontage;
- b. Landscape feature;
- c. Arcade;
- d. Marquee;
- e. Awning;
- f. Sculpture;
- g. Water feature;
- h. Active recreation area (Downtown-R only);
- i. Retail food (Downtown-R only);
- j. Child care services (Downtown-R only);
- k. Plaza (only applicable to: 1) nonresidential buildings on rights-of-way designated as "D/R" Streets in Design Guidelines – Building/Sidewalk Relationships adopted by LUC 20.25A.115; or 2) Downtown-OLB if located between the sidewalk on the east side of 112th Avenue NE and the building);
- l. Residential entry courtyard (only applicable to residential buildings on rights-of-way that are either not designated or are designated as "D/R" Streets in Design Guidelines – Building/Sidewalk Relationships adopted by reference in LUC 20.25A.115).

Except for active recreation and child care services, the above amenities shall be located on ground level and oriented to a public right-of-way.

In multibuilding developments within a single project limit, amenities may be allocated among all buildings within the project limit; provided, that such allocation must be approved by the Director. If construction of the multi-building development is to be phased, no phase may depend on the future construction of amenities.

2. Amount of Amenity Required. The amount of bonus floor area in square feet generated by the amenity(s) provided to meet the Basic Floor Area Ratio (FAR) requirement must at a minimum be equal to 20 percent of the Project Limit area in square feet times the Basic FAR permitted for a nonresidential building in the land use district:

$$\text{Basic FAR Requirement in square feet} = 0.20 \times \text{Project Limit area in square feet} \times \text{Basic Nonresidential FAR}$$

For purposes of this paragraph, project limit, as defined in LUC 20.50.040, shall be the land area used in the computation of the Basic Floor Area Ratio requirement, and the Basic Floor Area Ratio calculation is made regardless of any transferred floor area.

3. Required Review. The Director of the Department of Planning and Community Development may approve an amenity proposed by the applicant only if:
  - a. The design criteria established in LUC 20.25A.030.C for the amenity have been met; and

- b. Public benefit will be derived from the development of the proposed amenity in the proposed location.
4. Amenity Incentive System Credit. Amenities provided as required by this subsection C may also be used to exceed the basic floor area ratio through the Amenity Incentive System, LUC 20.25A.030.

#### **D. Floor Area Ratio Computation – Right-of-Way Designation.**

1. General. Land which is dedicated to the City of Bellevue for right-of-way without compensation to the owner in conformance with paragraph D.2 of this section is included in land area for the purpose of computing maximum FAR notwithstanding LUC 20.50.020, floor area ratio (FAR).
2. Special Dedications.
  - a. A property owner may make a special dedication by conveying land identified for right-of-way acquisition in a Transportation Facilities Plan of the Comprehensive Plan, the Transportation Facilities Plan adopted by the City Council or the Capital Investment Program Plan to the City of Bellevue by an instrument approved by the City Attorney.
  - b. A property owner may also make a special dedication by conveying land identified by the Director of Transportation as necessary for safety or operational improvement projects.
3. Recording Requirements. The Director of the Development Services Department must record the amount (square footage) of floor area earned by area dedicated in conformance with paragraph D.2 of this section and the increase in maximum building height acquired in conformance with subsection B.4.c of this section with the King County Division of Records and Elections and with the Bellevue City Clerk. (Ord. 5990, 2-7-11, §§ 1, 2; Ord. 5717, 2-20-07, §§ 5, 6; Ord. 5582, 2-7-05, §§ 1, 2; Ord. 5571, 12-6-04, § 7; Ord. 5497, 11-17-03, §§ 1, 2, 3; Ord. 5480, 10-20-03, § 12; Ord. 5372, 6-3-02, §§ 1, 2; Ord. 5351, 1-7-02, § 1; Ord. 5100, 10-19-98, § 6; Ord. 5091, 8-3-98, §§ 2 – 5; Ord. 5050, 1-20-98, §§ 1 – 3; Ord. 4979, 3-17-97, § 9; Ord. 4973, 3-3-97, § 102; Ord. 4884, 6-17-96, § 1; Ord. 4816, 12-4-95, § 202; Ord. 4268, 7-1-91, § 1; Ord. 4235, 4-1-91, § 1; Ord. 4117, 1-16-90, § 2; Ord. 3901B, 5-16-88, § 1; Ord. 3813, 7-20-87, § 4; Ord. 3766, 3-23-87, §§ 1, 2; Ord. 3747, 1-20-87, § 6; Ord. 3653, 5-

19-86, §§ 2, 3; Ord. 3553, 10-7-85, § 3; Ord. 3530, 8-12-85, §§ 37, 38; Ord. 3498, 5-27-85, §§ 44, 45)

## **20.25A.025 Nonconforming uses, structures and sites.**

### **A. Nonconforming Uses.**

1. A nonconforming use may be continued by successive owners or tenants, except where the use has been abandoned. No change to a different use classification shall be made unless that change conforms to the regulations of this Code.
2. If a nonconforming use of a structure or land is discontinued for a period of 12 months with the intention of abandoning that use, any subsequent use shall thereafter conform to the regulations of the district in which it is located. Discontinuance of a nonconforming use for a period of 12 months or greater constitutes prima facie evidence of an intention to abandon.
3. A nonconforming use may be expanded only pursuant to an Administrative Conditional Use Permit if the expansion is not more than 20 percent or 20,000 square feet, whichever is less, or by a Conditional Use Permit if the expansion is over 20 percent or 20,000 square feet.

### **B. Nonconforming Structures.**

1. A nonconforming structure may be repaired or remodeled, provided there is no expansion of the building, and provided further, that the remodel or repair will not increase the existing nonconforming condition of the structure.
2. A nonconforming structure may be expanded; provided, that the expansion conforms to the provision of the Land Use Code, except that the requirements of LUC 20.25A.115, Design Guidelines – Building/Sidewalk Relationships, shall be applied as described in paragraphs B.3 and B.4 of this section.
3. For expansions made within any three-year period which together do not exceed 50 percent of the floor area of the previously existing structure, the following shall apply:
  - a. Where the property abuts a street classified as a 'D' or 'E' right-of-way, the expansion is not required to comply with LUC 20.25A.115; and
  - b. Where the property abuts a street classified as an 'A', 'B' or 'C' right-of-way the expansion shall be in the direction of the classified street so as to reduce the nonconformity of the structure, except that an expansion which is no greater than 300 square feet in area and which is for the purpose of loading or storage is exempted from this requirement.
4. For expansions made within any three-year period which together exceed 50 percent of the floor area of the previously existing structure, the structure shall be brought into conformance with LUC 20.25A.115.
5. If a nonconforming structure is destroyed by fire, explosion, or other unforeseen circumstances to the extent of 75 percent or less of its replacement value as determined by the Director for the year of its destruction, it may be reconstructed consistent with its previous nonconformity. If such a structure is destroyed to the extent of greater than 75 percent of its replacement value, then any structure erected and any related site development shall conform to the regulations of this Code.

**C. Nonconforming Sites.**

1. A nonconforming site may not be changed unless the change conforms to the requirements of this Code, except that parking lots may be reconfigured within the existing paved surface. This paragraph shall not be construed to allow any parking lot reconfiguration that would result in a parking supply that does not conform to the minimum/maximum parking requirements for the Downtown, LUC 20.25A.050.
2. A structure located on a nonconforming site may be repaired or remodeled, provided there is no expansion of the building, and provided further, that the remodel or repair will not increase the existing nonconforming condition of the site.
3. Expansions of a structure located on a nonconforming site, made within any three-year period which together do not exceed 50 percent of the previously existing floor area, do not require any increase in conformance with the site development provisions of this Code, except as otherwise provided in paragraph B.3. of this section.
4. Expansion of a structure located on a nonconforming site made within any three-year period which together exceed 50 percent of the floor area of the previously existing structure shall require compliance with the site development provisions of this Code.
5. For expansions of a structure on a nonconforming site made within any three-year period which together exceed 20 percent of the replacement value of the previously existing structure:
  - a. Easements for public sidewalks shall be provided, unless the Director of the Department of Transportation determines such easements are not needed; and
  - b. A six-foot-wide walkway shall be provided from the public sidewalk or street right-of-way to the main building entrance, unless the Director of Planning and Community Development determines the walkway is not needed to provide safe pedestrian access to the building. (Ord. 5480, 10-20-03, § 13; Ord. 5091, 8-3-98, §§ 6, 7; Ord. 4973, 3-3-97, § 103; Ord. 4816, 12-4-95, § 203)

**20.25A.030 FAR amenity incentive system.****A. General.**

A building may exceed the basic floor area ratio permitted for development within a Downtown Land Use District only if it complies with the requirements of this section. In no case may the building exceed the maximum floor area ratio permitted for the district, and each unit of measurement (square feet, linear feet, etc.) may only be used to gain one floor area ratio bonus, except where specifically provided otherwise.

**B. Required Review.**

The Director of the Department of Planning and Community Development may approve an amenity which complies with subsection C of this section if:

1. The design criteria established for the amenity have been met; and
2. A public benefit will be derived from the development of the proposed amenity in the proposed location.

**C. Specific Requirements.**

For purposes of applying the FAR Amenity Incentive System, the “City Center District” shall mean that area of the Downtown bounded by 100th Avenue NE, 112th Avenue NE, NE 4th Street and NE 8th Street.

For purposes of applying the Amenity Incentive System, a level shall be considered the ground level so long as less than half of that ground level story height is located above or below the average finished grade of the adjacent public right-of-way or pedestrian connection. The two stories immediately above the ground level story and intended to activate the ground level pedestrian environment through demonstrated compliance with LUC 20.25A.115, Design Guidelines – Building/Sidewalk Relationships, shall be considered an upper level.

Participation in the FAR Amenity Incentive System must comply with the following chart:

FAR AMENITY STANDARDS	DOWNTOWN LAND USE DISTRICT						DESIGN CRITERIA
	DNTN-O-1	DNTN-O-2	DNTN-MU	DNTN-R	DNTN-OB	DNTN-OLB	
<b>1. PEDESTRIAN-ORIENTED FRONTAGE*</b> Building frontage devoted to uses which stimulate pedestrian activity. Uses must abut a perimeter sidewalk, a plaza, an arcade, a mid-block retail connection, or be located on an upper level. Uses which compose pedestrian-oriented frontage include, but are not limited to, specialty retail stores, groceries, drug stores, shoe repair shops, cleaning establishments, floral shops, beauty shops, barber shops, department stores, hardware stores, apparel shops, art galleries, travel agencies, restaurants and theaters. Banks and financial institutions are not pedestrian-oriented uses.	When located on the ground level or at the upper level within the City Center District:						1. Pedestrian-oriented frontage must comply with: a. LUC 20.25A.115; b. Guidelines 2 and 3 for “A” Rights-of-Way in Section IV.F of the Design Guidelines – Building/Sidewalk Relationships; and c. If applicable, Guidelines and Standards for Upper Level Retail in Section IV.F of the Design Guidelines – Building/Sidewalk Relationships.
	200:1	150:1	100:1	100:1	200:1	100:1	
	When located at the upper level outside of the City Center District:						
	100:1	75:1	50:1	50:1	100:1	50:1	

\*Measured as square feet of permitted development for each qualifying linear foot of frontage.

<sup>1</sup> Measured as square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

FAR AMENITY STANDARDS	DOWNTOWN LAND USE DISTRICT						DESIGN CRITERIA
AMENITY <sup>1</sup>	DNTN-O-1	DNTN-O-2	DNTN-MU	DNTN-R	DNTN-OB	DNTN-OLB	
<p><b>2. PLAZA</b> A continuous open space, which is readily accessible to the public at all times, predominantly open above, and designed specifically for use by people as opposed to serving as a setting for a building.</p>	When located on the ground level or at the upper level within the City Center District:						<ol style="list-style-type: none"> <li>1. Must abut and be within 3' in elevation of a perimeter sidewalk or pedestrian connection so as to be visually and physically accessible.</li> <li>2. Must provide protection from adverse wind, wherever practical.</li> <li>3. At least 10% of the plaza surface area must be landscaped.</li> <li>4. Must provide at least one sitting space for each 100 sq. ft. of plaza.</li> <li>5. Must be enclosed on at least two sides by a structure or by landscaping which creates a wall effect.</li> <li>6. Minimum size is 500 sq. ft. in OB; 1,000 sq. ft. in other land use districts.</li> <li>7. Maximum size of bonusable plaza square footage is 1,500 sq. ft. in OB; 5,000 sq. ft. in other land use districts.</li> <li>8. Minimum horizontal dimension is 20 ft.</li> <li>9. Must provide opportunities for penetration of sunlight.</li> <li>10. May not be used for parking, loading or vehicular access.</li> </ol>
	8:1	8:1	6:1	4:1	6:1	6:1	
	When located at the upper level outside of the City Center District:						
	4:1	4:1	3:1	2:1	3:1	3:1	

<sup>1</sup> Measured as square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

FAR AMENITY STANDARDS	DOWNTOWN LAND USE DISTRICT						DESIGN CRITERIA
AMENITY <sup>1</sup>	DNTN-O-1	DNTN-O-2	DNTN-MU	DNTN-R	DNTN-OB	DNTN-OLB	
<b>3. LANDSCAPE FEATURE</b> A continuous open space located at or near grade whose principal feature is an unusual and pleasing landscape form. The purpose is to serve as a focal point and a visual landmark, rather than as a specific location for pedestrian activity.	When located on the ground level or at the upper level within the City Center District:						1. Must abut the intersection of and be visually accessible from two public rights-of-way or perimeter sidewalks in order to receive the full bonus available. 2. One-half of the available bonus will be awarded if the landscape feature abuts and is visually accessible from a right-of-way, perimeter sidewalk or pedestrian connection but is not located at an intersection. 3. Maximum area is 1,000 sq. ft. in DNTN-O-1, DNTN-O-2, DNTN-MU and DNTN-OLB and 500 sq. ft. in DNTN-OB. No bonus is awarded if the landscape feature exceeds the maximum size.
	8:1 But see Design Criterion #1 & 2	8:1 But see Design Criterion #1 & 2	8:1 But see Design Criterion #1 & 2	8:1 But see Design Criterion #1 & 2	8:1 But see Design Criterion #1 & 2	8:1 But see Design Criterion #1 & 2	
	When located at the upper level outside of the City Center District:						
	4:1 But see Design Criterion #1 & 2	4:1 But see Design Criterion #1 & 2	4:1 But see Design Criterion #1 & 2	4:1 But see Design Criterion #1 & 2	4:1 But see Design Criterion #1 & 2	4:1 But see Design Criterion #1 & 2	
<b>4. ENCLOSED PLAZA</b> A publicly accessible, continuous open space located within a building and covered to provide overhead weather protection while admitting substantial amounts of natural daylight (atrium or galleria).	When located on the ground level or at the upper level within the City Center District:						1. Must be accessible to the public at least during normal business hours. 2. Must be readily accessible from a perimeter sidewalk or pedestrian connection. 3. Must be signed to identify the enclosed plaza as available for public use. 4. At least 5% of the area must be landscaped. 5. Must provide at least one sitting space for each 100 sq. ft. of area. 6. Must be coordinated with pedestrian-oriented frontage to the maximum extent possible. 7. Minimum horizontal dimension is 20'. 8. Minimum area is 750 sq. ft.
	10:1	10:1	4:1	2:1	8:1	2:1	
	When located at the upper level outside of the City Center District:						
	5:1	5:1	2:1	1:1	4:1	1:1	

<sup>1</sup> Measured as square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

FAR AMENITY STANDARDS	DOWNTOWN LAND USE DISTRICT						DESIGN CRITERIA
AMENITY <sup>1</sup>	DNTN-O-1	DNTN-O-2	DNTN-MU	DNTN-R	DNTN-OB	DNTN-OLB	
5. ARCADE A continuously covered area which functions as a weather-protected extension of the publicly accessible space which it abuts.	When located on the ground level or at the upper level within the City Center District:						1. At least 50% of the linear frontage must be developed as pedestrian-oriented frontage which complies with the design criteria of this section. This pedestrian-oriented frontage may be counted separately to gain floor area ratio exceeding the basic FAR through the Amenity Incentive System. 2. Pavement below must be constructed to provide for drainage. 3. When adjacent to a perimeter sidewalk, design must provide opportunity for connection to adjacent development across property lines. 4. Must have a horizontal rather than sloping orientation across the building facade. 5. Must present a coordinated design along its entire route. 6. Must be accessible to pedestrians at all times. 7. Minimum height is 8' above finished grade, or 8' above the upper level walk. No bonus is awarded if the maximum height is exceeded.
	8:1	6:1	4:1	6:1	8:1	4:1	
	When located at the upper level outside of the City Center District:						
	4:1	3:1	2:1	3:1	4:1	2:1	

<sup>1</sup> Measured as square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

FAR AMENITY STANDARDS	DOWNTOWN LAND USE DISTRICT						DESIGN CRITERIA
	DNTN-O-1	DNTN-O-2	DNTN-MU	DNTN-R	DNTN-OB	DNTN-OLB	
<b>6. MARQUEE*</b> A permanent overhead canopy projecting from the elevation of a building, and designed to provide continuous overhead weather protection to the area underneath.	When located on the ground level or at the upper level within the City Center District:						1. Must be developed over a perimeter sidewalk or pedestrian connection. 2. Pavement below must be constructed to provide for drainage. 3. Must have a horizontal rather than sloping orientation along the building elevation. 4. Design must be coordinated with building design. 5. Minimum height is 8' above finished grade, or 8' above the upper level walk except as otherwise required in the International Building Code, as adopted and amended by the City of Bellevue. 6. Maximum height is 12' above finished grade or 12' above the upper level walk. No bonus is awarded if the marquee exceeds the maximum height. 7. To ensure daylight penetration the ratio of the marquee's projection from the building to its height above finished grade may not exceed 3:4.
	4:1	3:1	2:1	2:1	2:1	2:1	
	When located at the upper level outside of the City Center District:						
	2:1	1.5:1	1:1	1:1	1:1	1:1	

\*See LUC 20.25A.020.B for setback exception.

<sup>1</sup> Measured as square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

FAR AMENITY STANDARDS	DOWNTOWN LAND USE DISTRICT						DESIGN CRITERIA
	DNTN-O-1	DNTN-O-2	DNTN-MU	DNTN-R	DNTN-OB	DNTN-OLB	
<b>7. AWNING*</b> A rooflike structure of fabric stretched over a rigid frame projecting from the elevation of a building designed to provide continuous overhead weather protection.	When located on the ground level or at the upper level within the City Center District:						1. Must be developed over a perimeter sidewalk or pedestrian connection. 2. Pavement below must be constructed to provide for drainage. 3. Must have a horizontal rather than sloping orientation along the building elevation. 4. Design must be coordinated with building design. 5. Minimum height is 8' above finished grade or 8' above the upper level walk. 6. Maximum height is 12' above finished grade or 12' above the upper level walk. No bonus is awarded if the awning exceeds the maximum height. 7. To ensure daylight penetration, the ratio of the awning's projection from the building to its height above finished grade may not exceed 3:4.
	1:1	0.75:1	0.5:1	1:1	0.5:1	0.5:1	
	When located at the upper level outside of the City Center District:						
	0.5:1	0.375:1	0.25:1	0.5:1	0.25:1	0.25:1	
*See LUC 20.25A.020.B for setback exception.							
<b>8. LANDSCAPE AREA</b> An outdoor landscaped area providing visually or physically accessible space for tenants of the development of which it is a part.	2:1	2:1	1:1	1:1	1:1	1:1	1. This area must be in addition to any landscape development required by the Land Use Code. 2. May not be used for parking or storage. 3. May be located at grade or on top of a structure. 4. At least 30% of the area must be planted with evergreen plant materials.
<b>9. ACTIVE RECREATION AREA</b> An area which provides active recreational facilities for tenants of the development of which it is a part.	3:1	3:1	1:1	1:1	1:1	1:1	1. May not be used for parking or storage. 2. May be located out of doors, on top of, or within a structure. 3. Recreational facilities include, but are not limited to, racquet ball or handball courts or health clubs.

<sup>1</sup> Measured as square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

FAR AMENITY STANDARDS	DOWNTOWN LAND USE DISTRICT						DESIGN CRITERIA
	DNTN-O-1	DNTN-O-2	DNTN-MU	DNTN-R	DNTN-OB	DNTN-OLB	
10. RESIDENTIAL USES*	4:1	4:1	2:1**	N/A	2:1	N/A	Area devoted to service cores and community facilities may be used to obtain bonus floor area. No area devoted to parking or circulation may be used for this purpose.
*Excludes Hotels and Motels. **See LUC 20.25A.090.E.7 for special bonus provisions for Perimeter Design District Subdistrict C.							
11. UNDERGROUND PARKING	0.5:1	0.5:1	0.5:1	3:1	2:1	1:1	1. The amenity bonus applies only to that structure or portion of a structure located below the average finished grade around a building. 2. Must be covered by a structure or developed open space.
12. ABOVE GRADE PARKING LOCATED UNDER PRINCIPAL USE RESIDENTIAL STRUCTURE*	1:1	1:1	4:1	4:1	3:1	N/A	1. Parking must be enclosed. 2. Exterior surface must be the same material as used on the principal use building.
*Parking qualifying for this bonus must serve a residential use. It must be located under a structure which contains a residential use, and all bonus floor area must be devoted to residential use.							
13. PUBLIC MEETING ROOMS	2:1	2:1	2:1	2:1	N/A	2:1	1. May include fixed seat auditorium or multipurpose meeting rooms. 2. Must be available for public use, but may operate under a reservation or nominal fee system. 3. Must provide seating for at least 50 persons.
14. SCULPTURE* Any form of sculpture or other artwork located outside of the building.	5:1	5:1	5:1	5:1	5:1	5:1	Must be displayed near the main pedestrian entrance to a building.
*Measured in units of \$100.00 of appraised value.							

<sup>1</sup> Measured as square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

FAR AMENITY STANDARDS	DOWNTOWN LAND USE DISTRICT						DESIGN CRITERIA
	DNTN-O-1	DNTN-O-2	DNTN-MU	DNTN-R	DNTN-OB	DNTN-OLB	
<b>15. WATER FEATURE*</b> A fountain, cascade, stream water, sculpture or reflection pond. The purpose is to serve as a focal point for pedestrian activity.	When located on the ground level or at the upper level within the City Center District:						1. Must be located outside of the building, and be publicly visible and accessible at the main pedestrian entrance to a building, or along a perimeter sidewalk or pedestrian connection. 2. Water must be maintained in a clean and noncontaminated condition. 3. Water must be in motion during daylight hours.
	8:1	8:1	8:1	8:1	8:1	8:1	
	When located at the upper level outside of the City Center District:						
	4:1	4:1	4:1	4:1	4:1	4:1	
*Measured in units of \$100.00 of appraised value, or actual construction cost, whichever is greater.							
<b>16. MAJOR PEDESTRIAN CORRIDOR</b> The major pedestrian corridor located on or in the immediate vicinity of NE 6th Street between 102nd Avenue and 110th Avenue NE.	16:1*	N/A	16:1*	N/A	N/A	N/A	Must comply with the requirements of LUC 20.25A.100.E.1.
*Bonus floor area may be achieved through the provision of this amenity only in conjunction with a permit to construct the Major Pedestrian Corridor in accordance with LUC 20.25A.100.E.1.							
<b>17. CHILD CARE SERVICES*</b> A use providing regular care and training for children, generally for less than 24 hours outside of the immediate family or kindergarten through 12th grade education system.	8:1	8:1	8:1**	8:1	8:1	8:1	1. Must comply with the requirements of LUC 20.20.170. 2. Floor area for this amenity may also be counted as pedestrian-oriented frontage if the criteria of LUC 20.25A.030.C.1 are met.
*Floor area may be excluded from calculation of maximum floor area ratio. **See LUC 20.25A.090.E.7 for special bonus provisions for Perimeter Design District Subdistrict C.							
<b>18. RETAIL FOOD*</b> A self-service retail enterprise which sells food, beverages and household goods for consumption off the premises.	N/A	N/A	2:1	2:1	N/A	N/A	1. Maximum bonusable area is 30,000 sq. ft., except in Perimeter Design District Subdistrict C, when no limit applies. 2. Floor area for this amenity may also be counted as pedestrian-oriented frontage if the criteria of LUC 20.25A.030.C.1 are met.
*Floor area may be excluded from calculation of maximum floor area ratio.							

<sup>1</sup> Measured as square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

FAR AMENITY STANDARDS	DOWNTOWN LAND USE DISTRICT						DESIGN CRITERIA
	AMENITY <sup>1</sup>	DNTN-O-1	DNTN-O-2	DNTN-MU	DNTN-R	DNTN-OB	
<b>19. PUBLIC RESTROOMS*</b> A room or rooms containing toilets and lavatories for the use of the general public, with only limited control for purposes of personal safety.	When located on the ground level or at the upper level within the City Center District:						1. Shall be located on the ground level of the building, or on the upper level if accessible from retail activities meeting the requirements of LUC 20.25A.030.C.1. 2. Shall be open for use by the public during normal business hours, although access may be monitored by a person located at the restroom facility. 3. Shall be handicapped accessible. 4. Shall be signed to identify its location.
	8:1	8:1	4:1	4:1	4:1	4:1	
When located at the upper level outside of the City Center District:							
4:1	4:1	2:1	2:1	2:1	2:1		
*Floor area may be excluded from calculation of maximum floor area ratio.							
<b>20. PERFORMING ARTS SPACE*</b> Space containing fixed seating for public assembly for the purpose of entertainment or cultural events (live performances only).	10:1	10:1	10:1	N/A	N/A	N/A	This bonus shall apply only to performing arts spaces that are less than 10,000 sq. ft.
*Floor area may be excluded from calculation of maximum floor area ratio.							
<b>21. SPACE FOR NON-PROFIT SOCIAL SERVICES*</b> Space which is made available, rent free, to charitable and social service organizations which provide emergency assistance, health services, referral services, or other specialized social services directly to the public.	4:1	4:1	4:1	4:1	4:1	4:1	1. Such space shall principally provide outreach functions, rather than administrative functions. 2. Maximum bonusable area is 5,000 sq. ft. 3. Bonus floor area for this amenity may also be counted as pedestrian-oriented frontage if the criteria of LUC 20.25A.030.C.1 are met.
*Floor area may be excluded from calculation of maximum floor area ratio.							

<sup>1</sup> Measured as square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

FAR AMENITY STANDARDS	DOWNTOWN LAND USE DISTRICT						DESIGN CRITERIA
	DNTN-O-1	DNTN-O-2	DNTN-MU	DNTN-R	DNTN-OB	DNTN-OLB	
<p><b>22. DONATION OF PARK PROPERTY</b>                      Property which is donated to the City, with no restriction, for park purposes.</p>	8:1	6:1	4:1	5:1	5:1	3:1	<ol style="list-style-type: none"> <li>1. The need for such property in the location proposed must be consistent with City-adopted policies and plans.</li> <li>2. The minimum size of a donated park parcel is 10,000 sq. ft.</li> <li>3. Donated park parcels need not be contiguous with the site for which development is proposed.</li> </ol>
<p><b>23. RESIDENTIAL ENTRY COURTYARD</b>                      A continuous open space enclosed on at least two sides, predominantly open above, for use by residents of a residential building and visually accessible to passersby on the adjoining right-of-way.</p>	N/A	N/A	4:1	4:1	N/A	N/A	<ol style="list-style-type: none"> <li>1. Must abut and be within 3' in elevation of a public street or perimeter sidewalk so as to be visually accessible. All bonusable surface area must be visible from the perimeter sidewalk along adjacent public streets.</li> <li>2. Minimum size is 750 sq. ft., with a minimum horizontal dimension of 20' adjacent to the sidewalk along the public street frontage.</li> <li>3. Must provide protection from adverse wind, wherever practical.</li> <li>4. At least 20% of the bonusable surface area must be landscaped.</li> <li>5. Must be enclosed on at least two sides by a structure or by landscaping which creates a wall effect.</li> <li>6. Must provide opportunities for penetration of sunlight.</li> <li>7. May not be used for parking, loading, or vehicular access.</li> <li>8. The maximum bonusable area is 1,500 sq. ft. per courtyard.</li> </ol>

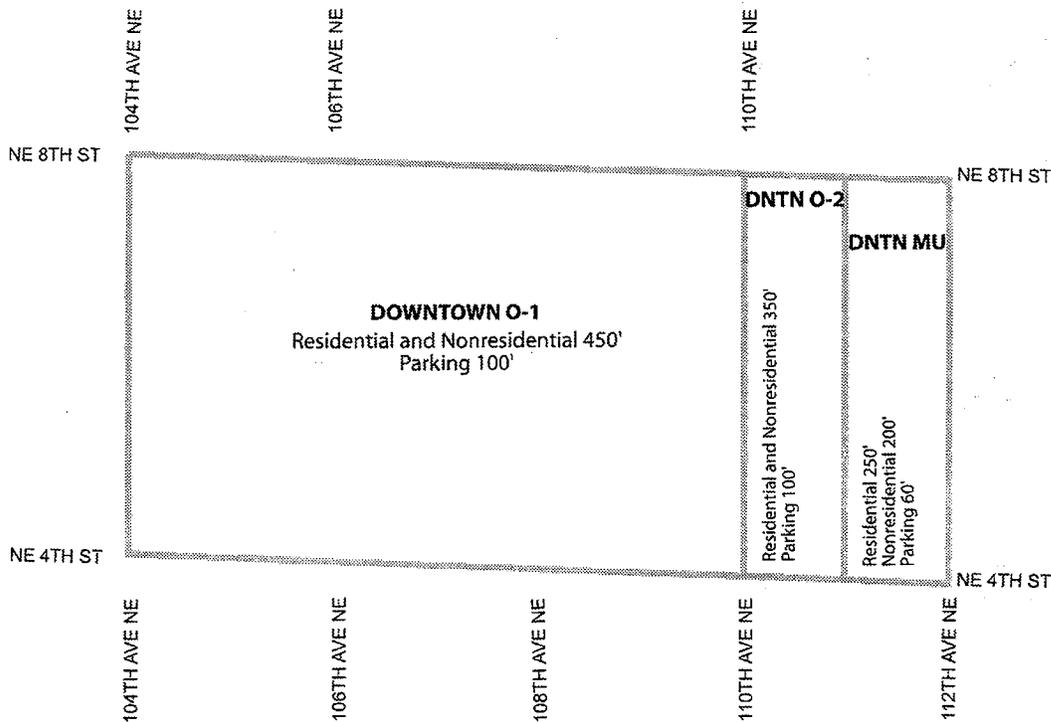
<sup>1</sup> Measured as square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

**D. Recording.**

The total amount of bonus floor area earned through the Amenity Incentive System for a project, and the amount of bonus floor area to be utilized on-site for that project must be recorded with the King County Division of Records and Elections, and with the Bellevue City Clerk.

**E. Transfer of Bonus Floor Area.**

1. When Floor Area May Be Transferred.
  - a. Bonus floor area earned through the amenity incentive system for a specific parcel of land may be transferred to an abutting parcel of land in the same ownership.
  - b. Bonus floor area earned for actual construction of the major pedestrian corridor may be transferred to any other property within the area of the Downtown bounded on the west by Bellevue Way, on the east by 112th Avenue NE, on the south by NE 4th Street and on the north by NE 8th Street. Properties may utilize this transferred floor area only to the extent that the building height does not exceed limits depicted on the map entitled "Heights in Core Design District."
2. Amount of Floor Area Transfer. No more than 25 percent of the gross floor area of a proposed project may be transferred floor area. This limitation does not include floor area generated by construction of the major pedestrian corridor or major public open spaces or for the floor area generated by the neighborhood service or residential use special bonus provisions in Perimeter Design District Subdistrict C.
3. Recording Required. The property owner must record each transfer of floor area with the King County Division of Records and Elections and with the Bellevue City Clerk.
4. Notwithstanding any provision of this Code, no transfer of floor area occurs when all property is included in one project limit.



**Heights in Core Design District\*  
Corridor/Open Space Bonuses  
Used on-site and/or transferred**

\*These building heights may be achieved, but not exceeded, as a result of either transferring from another site, using on-site, or a combination thereof, the bonus floor area received as a result of constructing the Major Pedestrian Corridor or Major Public Open Spaces. These building height limits may not be exceeded by using any other Code provision. The 450-foot height limit shown for Downtown O-1 shall be calculated by including any mechanical or other similar equipment or nonhabitable structural elements. (Ord. 5582, 2-7-05, § 3; Ord. 5571, 12-6-04, § 8; Ord. 5497, 11-17-03, § 4; Ord. 5480, 10-20-03, §§ 14, 15; Ord. 5050, 1-20-98, § 4; Ord. 4979, 3-17-97, § 10; Ord. 4973, 3-3-97, § 104; Ord. 4816, 12-4-95, § 204; Ord. 4268, 7-1-91, § 3; Ord. 3901B, 5-16-88, § 2; Ord. 3653, 5-19-86, §§ 4, 5; Ord. 3530, 8-12-85, § 40)

**20.25A.040 Landscape development and fences.**

- A. The provisions of LUC 20.20.520, except as they conflict with this section, apply to development in the Downtown Land Use Districts.
- B. Site perimeter and parking structure landscaping shall be provided in Downtown Land Use Districts according to the following chart, Landscape Development Requirements. In addition, street trees may be required by LUC 20.25A.060.B.

**Landscape Development Requirements**

Land Use District	Location On-Site		
	Street Frontage	Rear Yard	Side Yard
Downtown-O-1 Downtown-O-2 Downtown OB (2)	If buffering a surface vehicular access or parking area – 8’ Type III (1)	None Required	None Required
Downtown-MU Downtown-R Downtown OLB (2)  See LUC 20.25A.090 for Perimeter Design District Standards	If buffering a surface vehicular access or parking area – 8’ Type III (1)	If buffering a surface vehicular access or parking area – 5’ Type III	If buffering a surface vehicular access or parking area – 5’ Type III

- (1) An alternative design may be approved through Alternative Landscape Option, LUC 20.20.520.J.
- (2) On the east frontage of 112th Avenue NE, a landscape buffer (minimum width 20 feet) meeting the requirements of the Perimeter Design District landscape buffer, LUC 20.25A.090.D.4.b.ii, shall be provided, except where a plaza or pedestrian-oriented frontage meeting the requirements of LUC 20.25A.030 abuts the sidewalk.

**C. Fences.**

- 1. No fence may violate the sight obstruction restrictions at street intersections. (See BCC 14.60.240.)
- 2. Any fence which exceeds eight feet in height requires a Building Permit and shall conform to the International Building Code, as adopted and amended by the City of Bellevue.
- 3. Height shall be measured from finished grade at the exterior side of the fence. No person may construct a berm upon which to build a fence unless the total height of the berm plus the fence does not exceed the maximum height allowable for the fence if the berm was not present.
- 4. Prohibited Fences. Barbed wire may not be used in fencing in any Downtown land use district. Electric fences are not permitted in any Downtown land use district. Chain link fences are not permitted in any Downtown land use district, except:
  - i. To secure a construction site or area during the period of construction, site alteration or other modification; and
  - ii. In connection with any approved temporary or special event use. (Ord. 5582, 2-7-05, § 4; Ord. 5050, 1-20-98, § 5; Ord. 4973, 3-3-97, § 105; Ord. 4816, 12-4-95, § 205; Ord. 3690, 8-4-86, § 9)

**20.25A.050 Downtown parking, circulation and walkway requirements.****A. General.**

The provisions of LUC 20.20.590, except as they conflict with this section, apply to development in the Downtown Land Use Districts.

**B. Minimum/Maximum Parking Requirement by Use – Specified Uses.**

This subsection supersedes LUC 20.20.590.F.1. Subject to LUC 20.20.590.G and 20.20.590.H, the property owner shall provide at least the minimum and may provide no more than the maximum number of parking stalls as indicated below:

**Downtown Parking Requirements**

Land Use		Downtown Zones				
		-O-1,-O-2		-R,-MU,-OB, -OLB		
		Min.	Max.	Min.	Max.	
a.	Auditorium/Assembly Room/Exhibition Hall/Theater/Commercial Recreation (1)	per 8 fixed seats or per 1,000 nsf (if there are no fixed seats)	1.0 (10.0)	2.0 (10.0)	1.5 (10.0)	2.0 (10.0)
b.	Financial Institution	per 1,000 nsf	3.0	4.0	4.0	5.0
c.	Funeral Home/Mortuary (1)	per 5 seats	1.0	1.0	1.0	no max.
d.	High Technology/Light Industry	per 1,000 nsf	2.0	3.5	2.0	3.5
e.	Home Furnishing/Retail/Major Appliances – Retail	per 1,000 nsf	1.5	3.0	1.5	3.0
f.	Hospital/In-Patient Treatment Facility/Outpatient Surgical Facility	per 1.5 patient beds	1.0	2.0	1.0	2.0
g.	(Deleted by Ord. 5790)					
h.	Manufacturing/Assembly (Other than High Technology/Light Industrial)	per 1,000 nsf	0.7	1.0	1.0	1.5
i.	Office (Business Services/Professional Services/General Office)(3)	per 1,000 nsf	2.0	2.7	2.5	3.0
j.	Office (Medical Dental/Health Related Services)	per 1,000 nsf	3.0	4.0	4.0	5.0
k.	Personal Services:					
	Without Fixed Stations	per 1,000 nsf	2.0	2.0	2.0	3.0
	With Fixed Stations	per station	0.7	2.0	1.0	1.5
l.	Residential	per unit	0	2.0	1.0(5)	2.0

nsf = net square feet (see LUC 20.50.036)

Land Use		Unit of Measure	Downtown Zones			
			-O-1,-O-2		-R,-MU,-OB,-OLB	
			Min.	Max.	Min.	Max.
m.	Restaurant	per 1,000 nsf	0	15.0	10.0(4)	20.0
n.	Retail	per 1,000 nsf	3.3	5.0	4.0(4)	5.0
o.	Retail in a Mixed Development (except Hotel)(2)	per 1,000 nsf	0	3.3	2.0(4)	4.0
p.	Senior Housing: Nursing Home	per patient bed	0.4	0.8	0.4	0.8
	Senior Citizen Dwelling or Congregate Care	per living unit	0	1.0	0.33	1.0

nsf = net square feet (see LUC 20.50.036)

Notes to Parking Requirements:

- (1) Room or seating capacity as specified in the International Building Code, as adopted and amended by the City of Bellevue, at the time of the application is used to establish the parking requirement.
- (2) If retail space in a mixed development exceeds 20 percent of the gross floor area of the development, the retail use parking requirements of paragraph B of this section apply to the entire retail space.
- (3) Special Requirement in Perimeter Design District: The Director of Planning and Community Development may require the provision of up to 3.5 parking stalls per 1,000 net square feet for office uses within the Perimeter Design District to avoid potential parking overflow into adjacent land use districts outside Downtown.
- (4) Restaurant and retail uses located in existing buildings with 1,500 nsf or less floor area in Downtown-OB have a minimum parking ratio of 0. Restaurant and retail uses located in existing buildings with more than 1,500 nsf floor area in Downtown-OB shall provide parking according to the above table for any floor area over 1,500 nsf.
- (5) The minimum requirement for studio apartment units available to persons earning 60 percent or less than the median income as determined by the United States Department of Housing and Urban Development for the Seattle Metropolitan Statistical Area is 0.25 stalls per unit. An agreement to restrict the rental or sale of any such units to an individual earning 60 percent or less of the median income shall be recorded with the King County Division of Records and Elections.

**C. Shared Parking.**

1. General. In the Downtown, this subsection supersedes LUC 20.20.590.1.1 – 2. Subject to compliance with other applicable requirements of this Code, the Director of Planning and Community Development may approve shared development or use of parking facilities

located on adjoining separate properties or for mixed use or mixed retail use development on a single site if:

- a. A convenient pedestrian connection between the properties or uses exists; and
- b. The availability of parking for all affected properties or uses is indicated by directional signs, as permitted by Chapter 22B.10 BCC (Sign Code).

2. Number of Spaces Required.

- a. Where the uses to be served by shared parking have overlapping hours of operation, the property owner or owners shall provide parking stalls equal to the total of the individual parking requirements for the uses served reduced by 20 percent of that total number; provided, that the Director may approve a further reduction of that total number if the property owner or owners demonstrate to the satisfaction of the Director that the resulting provision of parking will be adequate for the proposed uses.
- b. Where the uses to be served by shared parking do not overlap their hours of operation, the property owner or owners shall provide parking stalls equal to the greater of the applicable individual parking requirements.

3. Documentation Required. Prior to establishing shared parking or any use to be served thereby, the property owner or owners shall file with the King County Division of Records and Elections and with the Bellevue City Clerk a written agreement approved by the Director of Planning and Community Development providing for the shared parking use. The agreement shall be recorded on the title records of each affected property.

**D. Off-Site Parking Location.**

1. General. In the Downtown, this subsection supersedes LUC 20.20.590.J. Except as provided in paragraph D.2 of this section, the Director of Planning and Community Development may authorize a portion of the approved parking for a use to be located on a site other than the subject property if:
  - a. Adequate visitor parking exists on the subject property; and
  - b. Adequate pedestrian, van or shuttle connection between the sites exists; and
  - c. Adequate directional signs in conformance with Chapter 22B.10 BCC (Sign Code) are provided.
2. District Limitations – Downtown-R Limitations. Parking located in the Downtown-R District may only serve uses located in that district unless otherwise permitted through Design Review, Part 20.30F LUC, and then, only if such parking is physically contiguous and functionally connected to the use which it serves in an adjacent land use district.
3. Short-Term Retail Parking Facilities. The Director may approve the development of short-term retail parking facilities (see definition at LUC 20.50.040) not associated with a specific use. Upon the separate approval of the Director, a property owner or owners may satisfy all or a portion of the parking requirement for a specified retail use through an agreement providing parking for the use at a designated short-term retail parking facility; provided, that:
  - a. Adequate pedestrian, van or shuttle connection exists between the sites; and

- b. Adequate directional signs in conformance with Chapter 22B.10 BCC (Sign Code) are provided.
4. Documentation Required. Prior to establishing off-site parking or any use to be served thereby, the property owner or owners shall file with the King County Division of Records and Elections and with the Bellevue City Clerk a written agreement approved by the Director of Planning and Community Development providing for the shared parking use. The agreement shall be recorded on the title records of each affected property.

#### **E. Commercial Use Parking.**

1. Any parking facilities or parking stalls located in the Downtown and developed to meet the requirements of the Land Use Code for a particular use may be converted to commercial use parking (see definition at LUC 20.50.040); provided, that the property owner shall:
  - a. Comply with all parking and dimensional requirements and with the performance standards for parking structures of this Code.
  - b. If the parking facility or parking stalls proposed for commercial use were approved for construction subsequent to the effective date of Ordinance 2964 (enacted on March 23, 1981), the commercial use parking facility or parking stalls shall comply with all landscaping requirements set forth at LUC 20.25A.040.
  - c. If the parking facility or parking stalls proposed for commercial use were approved for construction prior to the effective date of Ordinance 2964 (enacted on March 23, 1981), and the commercial use parking facility occupies more than 30 spaces, the minimum landscaping requirements of this Code shall be deemed met where the property owner installs landscaping in compliance with an approved landscaping plan which achieves the following objectives:
    - i. Surface parking areas shall be screened from street level views to a minimum height of four feet by a wall, hedge, berm or combination thereof.
    - ii. The minimum width of any hedge planting area shall be three feet.
    - iii. Visual relief and shade shall be provided in the parking area by at least one deciduous shade tree (12 feet high at planting) for every 20 parking stalls, provided such trees shall not be required in covered or underground parking. Each tree planting area shall be at least 100 square feet in area and four feet in width, and shall be protected from vehicles by curbing or other physical separation. If irrigation is provided, the planting area may be reduced to 40 square feet.
    - iv. The proposed landscaping plan shall be reviewed by the Director for compliance with these objectives and shall be approved by the Director prior to initiation of the commercial use parking.
  - d. If the parking facility or parking stalls proposed for commercial use were approved for construction prior to the effective date of Ordinance 2964 (enacted March 23, 1981) and the commercial use parking facility occupies 30 spaces or less, the commercial use parking facility shall be exempt from the landscaping requirements of this Code.
2. Assurance Device. The Director of Planning and Community Development may require an assurance device pursuant to LUC 20.40.490 to ensure conformance with the requirements and intent of this subsection.

**F. Parking Area and Circulation Improvements and Design.**

1. Landscaping. Paragraph F.1 of this section supersedes LUC 20.20.590.K.7. The property owner shall provide landscaping as required by LUC 20.25A.040.
2. Compact Parking. Paragraph F.2 of this section supersedes LUC 20.20.590.K.9. The Director of Planning and Community Development may approve the design and designation of up to 65 percent of the spaces for use by compact cars.
3. Vanpool/Carpool Facilities. The property owner must provide a vanpool/carpool loading facility that is outside of required driveway or parking aisle widths and that is contained within the required parking and circulation areas. The facility must be adjacent to an entrance door to the structure served by the parking or as nearly so as possible and must be consistent with all applicable design guidelines.
4. Performance Standards for Parking Structures. The Director of Planning and Community Development may approve a proposal for a parking structure through Design Review, Part 20.30F LUC. The Director of Planning and Community Development may approve the parking structure only if:
  - a. Driveway openings are limited and the number of access lanes in each opening is minimized.
  - b. The structure exhibits a horizontal, rather than sloping, building line.
  - c. The dimension of the parking structure abutting pedestrian areas is minimized, except where retail, service or commercial activities are provided.
  - d. The parking structure complies with the requirements of LUC 20.25A.115.
  - e. A wall or other screening of sufficient height to screen parked vehicles and which exhibits a visually pleasing character is provided at all above-ground levels of the structure.
  - f. Safe pedestrian connection between the parking structure and the principal use exists.
  - g. Loading areas are provided for vanpools/carpools as required by paragraph F.3 of this section.
  - h. Vehicle height clearances for structured parking must be at least seven and one-half feet for the entry level to accommodate vanpool parking.

**G. Interim and Phased Parking.**

1. Interim Parking.
  - a. When Allowed. The Director of Planning and Community Development may approve the installation of interim parking up to the maximum parking allowed if determined to be necessary to mitigate spillover parking impacts. Such interim parking may exist for a period not to exceed five years from the date of Temporary or Final Certificate of Occupancy, whichever comes first. The Director of Planning and Community Development may upon written request grant no more than two one-year extensions to the five-year interim parking time limit.
  - b. Approval Required. The Director of Planning and Community Development must review and approve a plan indicating current parking demand, how much interim parking is pro-

posed, when the parking will be removed, and how the interim parking area will be restored.

- c. Design. The property owner must provide perimeter and interior parking lot landscaping as required by LUC 20.25A.040 and must comply with all dimensional standards of this Code.
- d. Removal of Interim Parking. The Director of Planning and Community Development may require the removal of interim parking prior to the expiration of the approval period when parking supply exceeds demand. The property owner proposing interim parking shall file a written agreement containing this limitation with the Bellevue City Clerk.
- e. Assurance Device. The Director of Planning and Community Development may require an assurance device pursuant to LUC 20.40.490 to insure conformance with the requirements and intent of paragraph G.1 of this section.

## 2. Phased Parking.

- a. Schedule Required. The property owner may install the required parking spaces in phases if the schedule has been approved by the Director of Planning and Community Development. Each phased parking installation must include enough parking to meet the parking requirements for the completed phases of the development for which the parking is provided. This phasing schedule must specifically indicate when all parking approved pursuant to this section will be provided.
- b. Assurance Device. The Director of Planning and Community Development may require an assurance device pursuant to LUC 20.40.490 to insure compliance with the requirements and intent of paragraph G.2 of this section.

## H. Director's Authority to Require Parking Exceeding Maximum.

In Downtown Districts, the Director of Planning and Community Development may require the installation of more than the maximum number of parking stalls, for other than office uses, if the Director determines that:

1. Such additional parking is necessary to meet the parking demand for a specified use; and
2. Shared or off-site parking is not available or adequate to meet demand; and
3. Any required Transportation Management Program will remain effective. (Ord. 5790, 12-3-07, § 4; Ord. 5717, 2-20-07, § 7; Ord. 5571, 12-6-04, § 9; Ord. 5091, 8-3-98, §§ 8, 9; Ord. 5050, 1-20-98, § 6; Ord. 4979, 3-17-97, § 11; Ord. 4973, 3-3-97, § 106; Ord. 4816, 12-4-95, § 206; Ord. 4654, 6-6-94, § 40; Ord. 3813, 7-20-87, § 5; Ord. 3747, 1-20-87, § 8)

## 20.25A.060 Walkways and sidewalks.

### A. Minimum Width – Perimeter.

1. The minimum width of perimeter walkway or sidewalk is 12 feet plus four feet in which street trees are to be planted plus six inches curb along:
  - a. NE 6th between 110th Avenue NE and 112th Avenue NE; and
  - b. 106th Avenue NE between NE 4th and NE 8th; and

- c. 108th Avenue NE between NE 4th and NE 8th; and
  - d. 110th Avenue NE between NE 4th and NE 8th.
2. The minimum width of a perimeter walkway or sidewalk is 12 feet plus four feet of planter strip plus six inches curb along:
    - a. Bellevue Way between Main and NE 12th; and
    - b. NE 4th between 100th Avenue NE and 112th Avenue NE; and
    - c. NE 8th between 100th Avenue NE and 112th Avenue NE.
  3. The minimum width of a perimeter walkway or sidewalk, excluding the area required for street trees in paragraph B.2 of this section, is eight feet plus four feet in which street trees are to be planted plus six inches curb along any other street.

#### **B. Street Trees Required – Perimeter.**

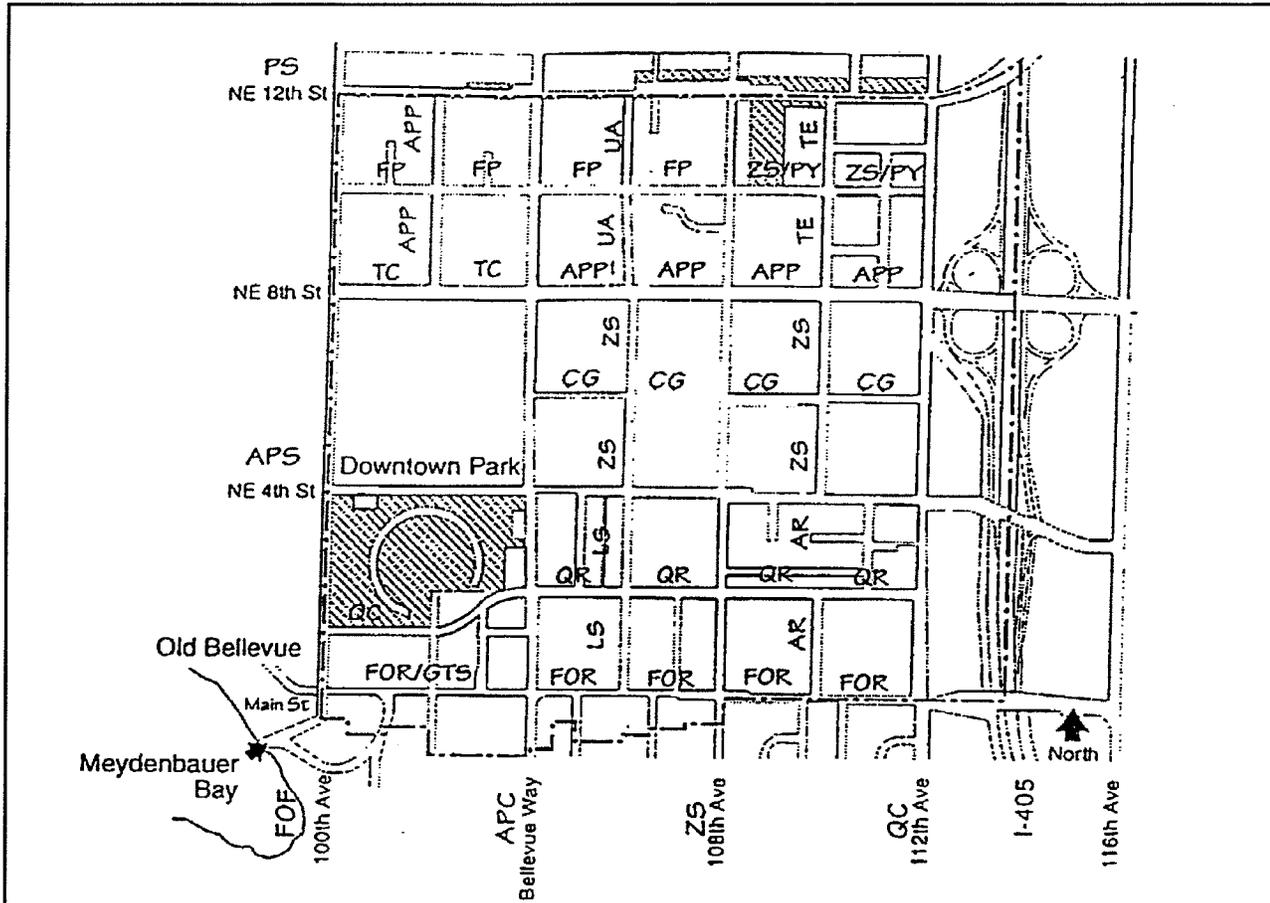
1. The property owner shall install street trees and other required vegetation, in addition to any landscaping required by LUC 20.25A.040, according to the requirements of Plate B and this section or as approved by the Director unless, upon the request of the applicant, minor modification is approved by the Director.
2. Except for the streets listed in paragraph A.2 of this section, the area in which street trees are planted must be at least four feet wide, and located between the street improvement and the walkway or sidewalk unless precluded by existing utilities which cannot reasonably be relocated. Any street tree planting area must be at least four feet by six feet or five feet by five feet or smaller area as approved by the Director, unless upon request of the applicant minor modification of this requirement is approved by the Director, and protected by an approved decorative grate. This grate may intrude into the sidewalk.
3. Street trees, at least three inches in caliper or as approved by the Director, must be planted at least three feet from the street curb, and a maximum of 25 feet on center, unless upon request of the applicant minor modification of this requirement is approved by the Director, and conforms to the sight distance requirements of BCC 14.60.240. A street tree planting area may also include decorative paving and other plant materials except turf.
4. On the streets listed in paragraph A.2 of this section, street trees shall be planted and placed as required in paragraph B.3 of this section. These required street trees together with shrubbery, groundcover and other approved plantings are required in a planter strip along the length of the frontage. The planter strip must be at least four feet wide, unless a smaller width is approved by the Director. Vegetation included in the planter strip shall be urban in charac-

ter, shall be compatible with other plantings along the same street, and shall reflect the character of the area within which they are planted, as approved by the Director.

### C. Walkways – Mid-Block.

1. Purpose. A pedestrian connection provides an opportunity for increased pedestrian movement through superblocks in the Downtown.
2. Location. Except as set forth in paragraph C.4 of this section, pedestrian connections are required in each superblock. These pedestrian connections must be provided to permit movement through the superblock from a perimeter walkway or sidewalk to publicly accessible spaces, adjoining structures or development, or parking areas. The Director may modify or eliminate the requirement for these pedestrian connections for projects with expanded lot coverage and floorplates as allowed pursuant to LUC 20.25A.065.C where providing such connections is not feasible due to structure and site size.
3. Design Guidelines. The following criteria apply to review of a proposed pedestrian connection.
  - a. Pedestrian connections must be developed as an internal walkway or sidewalk, an arcade, a pedestrian skybridge, and may meander.
  - b. Pedestrian connections must comply with the applicable definitions in Chapter 20.50 LUC.
  - c. Pedestrian connections must be designed to form logical routes from origins to destinations.
  - d. Pedestrian connections must offer diversity in terms of activity and pedestrian amenity along pedestrian routes.
  - e. Pedestrian connections must meet construction code handicapped requirements.
  - f. Pedestrian connections should use trees and landscaping to provide definition and enclosure.
  - g. Pedestrian connections should provide for weather protection from rain through use of sheltered walkways or sidewalks, canopies, multiple building entrances, lobbies, and entries of sufficient size and accessibility.
4. Old Bellevue.
  - a. Development abutting the streets identified below shall provide mid-block connections between:
    - i. Main Street and NE 1st Street at approximately 101st Avenue NE; and
    - ii. 102nd and 103rd Avenues approximately mid-block between Main Street and NE 1st Street; and
    - iii. Main Street at approximately 103rd Avenue and 102nd Avenue SE at approximately SE 1st Street.
  - b. A mid-block connection must be developed as a walkway or a combination walkway and vehicular lane. The connection shall incorporate decorative lighting and seating areas.
  - c. If a combination walkway and vehicular lane does not have a separate raised walkway, the walkway surface must be paved with unit paver blocks or other unique paving surface to denote that it is a pedestrian area. (Ord. 5582, 2-7-05, § 5; Ord. 5372, 6-3-02, § 3; Ord.

5091, 8-3-98, § 10; Ord. 4973, 3-3-97, § 108; Ord. 4816, 12-4-95, § 208; Ord. 4654, 6-6-94, § 41; Ord. 3766, 3-23-87, § 3; Ord. 3747, 1-20-86, § 10; Ord. 3498, 5-27-85, § 53)



Existing parkland      - - - Downtown Boundary

Symbol	Latin Name/Common Name	Symbol	Latin Name/Common Name
APC	Acer platanoides 'Cleveland'/Cleveland Maple	LS	Liquidambar styraciflua/Sweet Gum
APP	Acer platanoides 'Parkway'/Parkway Maple	PS	Prunus serrulata 'Kwanzan'/Kwanzan Cherry
APS	Acer platanoides 'Superform'/Superform Maple	PY	Prunus yedoensis 'Akebono'/Akebono Cherry
AR	Acer rubrum/Red Maple	PC	Pyrus calleryana 'Bradford'/Bradford Pear
CJ	Cercidephyllum japonicum/Katsura Tree	QC	Quercus coccinea/Scarlet Oak
FOR	Fraxinus ornus 'Raywood'/Raywood Ash	QR	Quercus robur/English Oak
FOF	Fraxinus oxycarpa 'Flame'/Flame Ash	TC	Tilia cordata/Little Leaf Linden
FP	Fraxinus pennsylvanica 'Marshall'/Marshall Seedless Ash	TE	Tilia euchlora 'Redmond'/Redmond Linden
GTI	Gleditsia triacanthos inermis/Thornless Honey Locust	UA	Ulmus americana 'Homestead'/Homestead Elm
GTS	Gleditsia triacanthos 'Skyline'/Skyline Honey Locust	ZS	Zelkova serrata/Zelkova
CG	See street tree requirements in the NE 6th Street Pedestrian Corridor Guidelines		

City of  
Bellevue
PLATE B
No Scale


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DOWNTOWN BELLEVUE STREET  
TREE SPECIES PLAN

(Ord. 5100, 10-19-98, § 1; Ord. 5091, 8-3-98, § 11; Ord. 4979, 3-17-97, § 12; Ord. 4973, 3-3-97, § 108; Ord. 4816, 12-4-95, § 208; Ord. 3690, 8-04-86, § 10)

**20.25A.065 Civic Center Design District.****A. Definition of District.**

The Civic Center Design District encompasses the area bounded by the centerlines of 110th Avenue NE on the west, NE 8th Street on the north, I-405 on the east, and NE 4th Street on the south.

**B. Purpose.**

The purpose of the Civic Center Design District is to implement the Downtown Subarea policies concerning the Special Opportunity Area, by providing specific standards. These standards will permit the development of cultural, conference and exhibition facilities and other uses as envisioned by the policies.

**C. Development Standards.**

All provisions of this Part 20.25A LUC shall apply to this district, with the following exceptions:

1. Within the Civic Center Design District, maximum lot coverage may be up to 100 percent for buildings in which more than 50 percent of the gross floor area, excluding parking, is comprised of one or more of the following uses: city government facilities, cultural facilities, conference facilities and exhibition facilities.
2. Within the Civic Center Design District, the building floor area per floor above 40 feet high may be unlimited for buildings and floors in which more than 50 percent of the gross floor area, excluding parking, is comprised of one or more of the following uses: city government facilities, cultural facilities, conference facilities and exhibition facilities.
3. Building types listed in paragraphs C.1 and C.2 of this section should incorporate special design features as described below:
  - a. Building facades should be divided into increments through the use of offsets, facets, recesses or other architectural features which serve to break down the scale. Roof forms should incorporate terraces, planting areas, decorative features, or other elements to soften the rectilinear profile.
  - b. Special attention should be given to the provision of elements at or near the ground level such as awnings, recessed entries, water features, address signs, seasonal flower beds, seating, pedestrian-oriented uses and display kiosks.
4. Nothing in these provisions shall affect the maximum floor area ratios permitted for the underlying land use districts.
5. Within the Civic Center Design District, the minimum side and rear setback required above 40 feet for all buildings with a building height in excess of 75 feet may be eliminated for buildings and floors in which more than 50 percent of the gross floor area, excluding parking, is comprised of one or more of the following uses: city government facilities, cultural facilities, conference facilities and exhibition facilities. (Ord. 5582, 2-7-05, § 6; Ord. 5372, 6-3-02, § 4; Ord. 4973, 3-3-97, § 109; Ord. 4816, 12-4-95, § 209; Ord. 4038, 7-31-89, § 1)

**20.25A.070 Downtown-Old Bellevue District.****A. Design Review Required.**

All development within the Downtown-Old Bellevue Land Use District must be reviewed by the Director of Planning and Community Development using the Design Review process, Part 20.30F LUC, and applying the Downtown Design Review Criteria, LUC 20.25A.110, in reviewing an application for development in the Downtown-Old Bellevue Land Use District.

**B. Development Requirements.**

Development within the Old Bellevue Land Use District must comply with the following if the property abuts the named streets:

1. **Street Improvements.** The applicant shall provide half-street and sidewalk improvements including paving, street trees, lighting and other street furniture comparable to the existing Main Street streetscape between 102nd Avenue and Bellevue Way on:
  - a. Both sides of Main Street between 100th Avenue and Bellevue Way; and
  - b. 102nd and 103rd Avenues between SE 1st Street and NE 1st Street; and
  - c. The west side of Bellevue Way between SE 1st Street and NE 2nd Street; and
  - d. The east side of 100th Avenue between SE Bellevue Place and NE 1st Street; and
  - e. Both sides of NE 1st and NE 2nd between 100th Avenue and Bellevue Way.
2. **Mid-Block Connections.** See LUC 20.25A.060.
3. **Parking.** An off-street parking area may not be located between the buildings and streets along:
  - a. 102nd Avenue NE between Main Street and NE 1st Street; and
  - b. Main Street between 100th Avenue and Bellevue Way.
4. **Minor Publicly Accessible Spaces.**
  - a. The applicant shall provide minor publicly accessible spaces on Main Street at the intersections of 100th Avenue and Bellevue Way.
  - b. A minor publicly accessible space may be outdoors or may be enclosed as long as adequate access is provided, and its existence is readily identifiable.
  - c. A minor publicly accessible space must be developed as a plaza, an enclosed plaza or landscape feature. The design criteria of LUC 20.25A.030.C must be met, and the FAR amenity bonus may be utilized.
  - d. Structures must directly abut the minor publicly accessible space.
5. **Pedestrian-Oriented Frontage and Marquees or Awnings.**
  - a. The applicant shall provide pedestrian-oriented frontage and marquees or awnings on:

- i. Both sides of Main Street from 100th Avenue to Bellevue Way; and
  - ii. 102nd Avenue between Main Street and NE 1st Street.
- b. Pedestrian-oriented frontage, marquees, and awnings must comply with the design criteria of LUC 20.25A.030.C, and the FAR amenity bonus may be utilized.
  - c. Pedestrian-oriented frontage must include display windows having mullions that are spaced two to six feet apart.
  - d. The use of exposed concrete, metal or plastic for storefront facades is not permitted. This does not preclude the use of metal and acrylic glazing on marquees or vinyl fabric on awnings, nor the use of brass, copper or aluminum for decorative trim. (Ord. 5582, 2-7-05, § 7; Ord. 4973, 3-3-97, § 110; Ord. 4816, 12-4-95, § 210; Ord. 4654, 6-6-94, § 42; Ord. 3813, 7-20-87, §§ 6, 7; Ord. 3530, 8-12-85, § 43; Ord. 3498, 5-27-85, § 55)

### **20.25A.090 Perimeter Design District.**

#### **A. Definition of District.**

The Perimeter Design District is an area composed of three subdistricts (A through C) as depicted on the following map. The Perimeter Design District includes area within the boundary of the Downtown as shown, whether or not said areas are within a Downtown Land Use District.

The Perimeter Design District shall include the following property west of the centerline of 112th Avenue NE within the Downtown as defined in LUC 20.50.016:

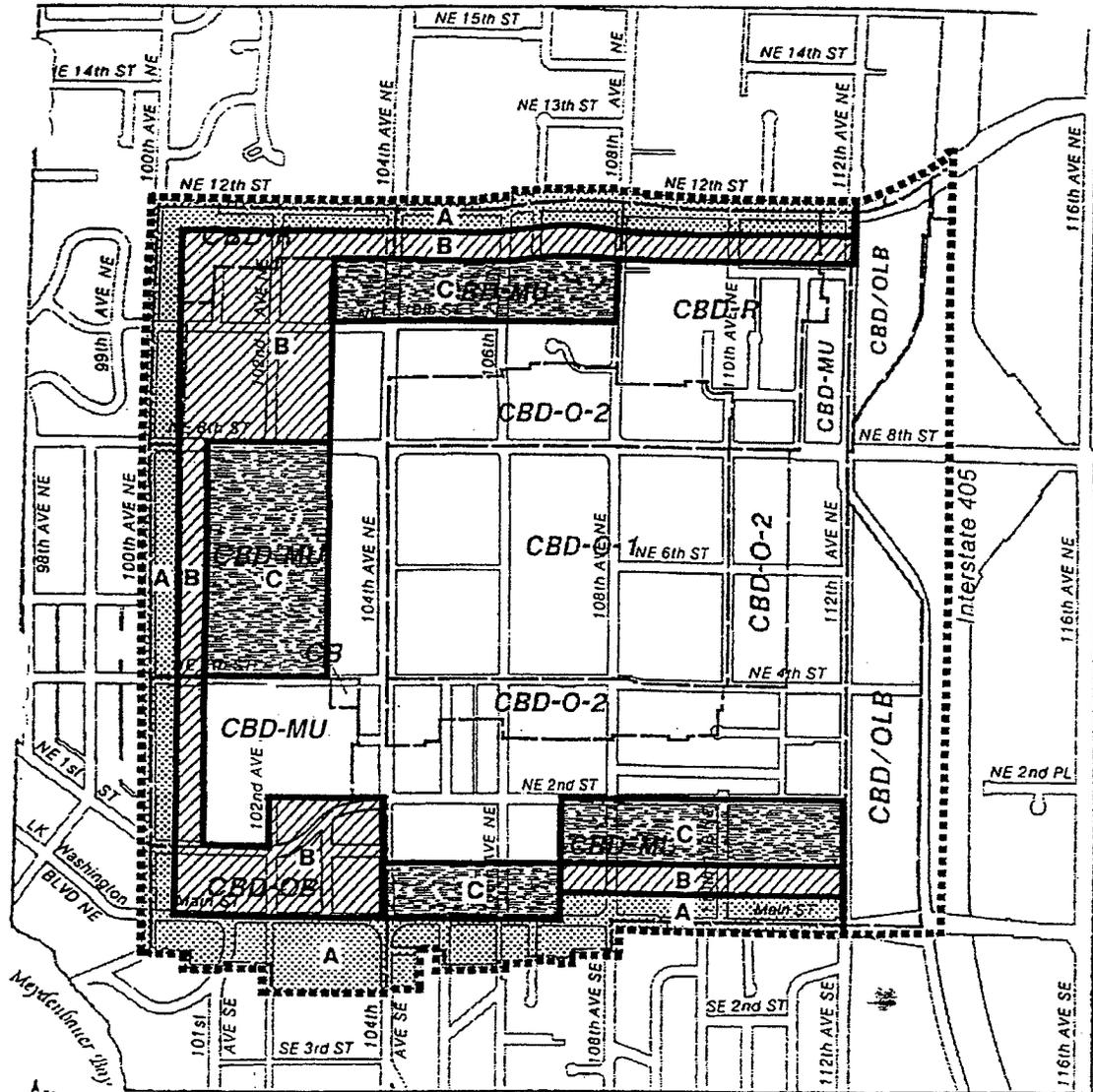
##### **Subdistrict A:**

1. All property south of the Main Street right-of-way; and
2. All property in the Main Street, 100th Avenue NE and NE 12th Street rights-of-way; and
3. All property measured 150 feet into the Downtown from the internal edge of the Main Street, 100th Avenue NE and NE 12th Street rights-of-way, respectively, except that property south of NE 1st Street and lying between a line parallel to and 150 feet east of the east edge of the 100th Avenue NE right-of-way and the centerline of 107th Avenue NE.

##### **Subdistrict B:**

1. All property measured 150 feet into the Downtown from the internal edge of Subdistrict A, except that property south of NE 1st Street, if extended, lying between the centerline of 104th Avenue NE and the centerline of 107th Avenue NE; and

2. All property north of the north edge of the Main Street right-of-way that is within the area that is east of the internal edge of Subdistrict A, south of the centerline of NE 2nd Street and west of the centerline of 104th Avenue NE except that property north of the centerline of NE 1st Street, west of the centerline of 102nd Avenue NE, south of the centerline of NE 2nd Street, if extended, and east of a line parallel to and 300 feet east of the east edge of the 100th Avenue NE right-of-way; and




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### Perimeter Design Districts

-  Subdistrict A
-  Subdistrict B
-  Subdistrict C

Note: The above figure shall be read to replace "CBD" wherever it appears with "DNTN."

3. All property within the area bounded by the northerly edge of the NE 8th Street right-of-way on the south, the southerly boundary of Subdistrict A on the north, the easterly boundary of Subdistrict A on the west, and a line parallel to and 300 feet west of the westerly edge of the 104th Avenue NE right-of-way on the east.

Subdistrict C:

1. All property within the area bounded by a line parallel to and 300 feet north of the northerly edge of the Main Street right-of-way on the north, the northerly edge of the Main Street right-of-way on the south, and the centerlines of 104th Avenue NE on the west and 107th Avenue NE on the east; and
2. All property within the area bounded by the northerly boundary of Subdistrict B on the south, the southerly edge of the NE 2nd Street right-of-way on the north, and the centerlines of 107th Avenue NE on the west and 112th Avenue NE on the east; and
3. All property within the area bounded by the easterly boundary of Subdistrict B on the west, the centerlines of NE 4th Street on the south and NE 8th Street on the north, and a line parallel to and 300 feet west of the westerly edge of the 104th Avenue NE right-of-way on the east; and
4. All property within the area bounded by the southerly boundary of Subdistrict B on the north, the northerly edge of the NE 10th Street right-of-way, if extended, on the south, and the centerlines of 103rd Avenue on the west and 108th Avenue NE on the east.

**B. Purpose.**

The purpose of the Perimeter Design District is to establish a stable development program for the perimeter between the Downtown and adjacent residential neighborhoods. The program helps secure the future of both areas.

**C. Review Criteria.**

The Director of the Department of Planning and Community Development shall use the Downtown Design Review Criteria, LUC 20.25A.110; the Design Guidelines – Building/Sidewalk Relationships, LUC 20.25A.115; and the provisions of this section in reviewing an application for development in the Perimeter Design District.

**D. Development Standards.**

1. Limitation on Modification. Notwithstanding any other provision of the Land Use Code, the development standards contained in this subsection may not be modified.

2. Perimeter Design District Dimensional Requirements. Dimensional requirements for properties within the Perimeter Design District shall be regulated by the chart set forth in this Section; provided, that dimensional requirements from the "Dimensional Requirements in Downtown Districts" chart set forth in LUC 20.25A.020.A.2 shall apply for dimensions not regulated in the following chart:

Perimeter Subdistrict	Building Type <sup>5</sup>	Minimum Setback from the DNTN Boundary <sup>1,4,10</sup>	Max. Lot Coverage	Building Height <sup>2</sup>		Floor Area Ratio <sup>3</sup>	
				Basic	Max.	Basic	Max.
Subdistrict A	Nonresidential	20'	75% <sup>6</sup>	30'	40'	.5	1.0 in DNTN-MU 1.0 in DNTN-OB .5 in DNTN-R
	Residential	20'	75% <sup>6</sup>	30'	55'	2.0	3.5
	Parking	20'	75%	30'	40' <sup>9</sup>	N/A	N/A
Subdistrict B	Nonresidential	N/A	75% <sup>6</sup>	30'	65'	.5	1.5 in DNTN-MU 1.0 in DNTN-OB .5 in DNTN-R
	Residential	N/A	75% <sup>6</sup>	45'	90'	2.0	5.0
	Parking	N/A	75%	40'	40'	N/A	N/A
Subdistrict C	Nonresidential	N/A	75%	30'	100' <sup>7</sup>	.5	3.0 <sup>7</sup>
	Residential	N/A	100%	150'	200'	2.0	5.0
	Mixed Use <sup>8</sup>	N/A	75%	30'	100' <sup>7</sup>	.5	3.0 <sup>7</sup>
	Parking	N/A	75%	40'	40'	N/A	N/A

Notes: Perimeter Design District Dimensional Requirements:

- (1) Measured from inside edge of required perimeter sidewalk, if applicable. If existing utilities which cannot reasonably be relocated result in the planting of street trees on the property side of a sidewalk as provided for in LUC 20.25A.060, four feet are added to the required setback.
- (2) The maximum building height may only be achieved by participation in the FAR Amenity Incentive System, LUC 20.25A.030. In Subdistrict B, the maximum height may be exceeded by no more than 10 percent, and only if the applicant can demonstrate that the additional height accommodates architecturally integrated mechanical equipment, interesting roof forms, significant floor plate modulation, significant facade modulation, or other such unique architectural features, and that the resulting design exceeds the quality and design requirements of LUC 20.25A.110. In Subdistrict C, the maximum building height may be increased by no more than 15 percent and only if the applicant can demonstrate that the additional height accommodates architecturally integrated mechanical equipment, interesting roof forms, significant floor plate

modulation, significant facade modulation, or other such unique architectural features; that the resulting design exceeds the quality and design requirements of LUC 20.25A.110; and that added height is necessary for provision of the neighborhood services listed in paragraph D.3 of this section.

- (3) The maximum permitted FAR may only be achieved by participation in the FAR Amenity Incentive System, LUC 20.25A.030. Where residential and nonresidential uses occur in the same building, the FAR is limited to the maximum FAR for the building type as determined in accordance with Note (8) and LUC 20.25A.020.A.2, Note (7). See LUC 20.25A.070.B for FAR requirements in Old Bellevue.
- (4) See LUC 20.25A.020.B for exceptions to minimum setback requirements.
- (5) Notwithstanding any other provisions of this Code, in Downtown-OB and all Downtown districts within Perimeter Design Subdistrict C, hotels and motels shall be considered as residential structures. In all other Downtown districts within Perimeter Design Subdistricts A and B, hotels and motels shall be considered as nonresidential structures.
- (6) The maximum lot coverage in Downtown-OB is 100 percent.
- (7) In Subdistrict C, the FAR amenity system in LUC 20.25A.030 may be used to achieve building height not to exceed 65 feet and FAR not to exceed 1.5. The maximum nonresidential FAR of 3.0 and maximum nonresidential 100-foot height may only be achieved through participation in the special bonus provisions in paragraph D.6 of this section.
- (8) Mixed use building type is applicable only in Perimeter District Subdistrict C. See paragraph D.6.d of this section for special provisions of the mixed use building type.
- (9) A parking structure may achieve the maximum permitted height if the development of the project limit area which contains the parking structure provides a bonusable amenity through participation in the FAR Amenity Incentive System, LUC 20.25A.030.
- (10) On lots that are bisected by the Downtown boundary, the Director may allow the minimum setback from the Downtown boundary to be measured from the perimeter property lines abutting other lots located outside the Downtown boundary. The modification must be consistent with the Perimeter District purpose statement contained in subsection B of this section. This provision may be used to modify only the setback location and not the minimum setback size.

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3. FAR Exemption. In Subdistricts A and B, the following uses are excluded from the gross building area for the purpose of calculating floor area ratio (FAR):

- a. Hardware (Retail)
- b. Food (Retail)
- c. Drugstores
- d. Personal Services
- e. Education
- f. Universities and Colleges
- g. Charitable and Social Service Organizations
- h. Religious Activities
- i. Library/Museum

- j. Art Gallery
- k. Child Care Services
- l. Gasoline Service Stations
- m. Garden Supplies

Nothing in this paragraph amends the uses permitted in a land use district as displayed in Chart 20.10.440 – Uses in Land Use Districts.

#### 4. Landscape Development.

a. General. The standards of this paragraph supplement other landscape requirements of this Part 20.25A and LUC 20.20.520 for development in the Perimeter Design District.

#### b. Linear Buffers.

- i. General. Any development situated within Perimeter Design District – Subdistrict A shall provide a “linear buffer” within the minimum setback adjacent to the Downtown boundary required by paragraph D.2 of this section. The purpose of this feature is to produce a green buffer that will soften the visual impact of the relatively larger buildings. These design standards are minimum requirements for the size and quantity of trees, shrubs and other “linear buffer” elements. The specific design of the “linear buffer” for each project site will be determined through the Design Review Process. Design considerations include but are not limited to the placement of elements and their relationship to adjacent property as well as to the proposed development. Different sets of design standards apply to each of the locational conditions.
- ii. Where the Downtown boundary falls within the Main Street, 100th Avenue NE or NE 12th Street right-of-way, the minimum setback from the Downtown boundary shall be landscaped according to the basic requirements and either Alternative A or B of the supplemental requirement.

#### (1) Basic Requirements (applicable in all cases):

- (a) Must have a minimum width of 20 feet;
- (b) Must abut and be within three feet in elevation of a sidewalk, so as to be visually and physically accessible;
- (c) Must provide at least one sitting space for each 200 square feet of the perimeter setback area;
- (d) May not be used for parking; vehicular access drives shall be kept to a minimum;
- (e) Must be readily accessible to the public at all times;
- (f) Must include seasonal color in an amount of at least 10 percent of the perimeter setback area.

#### (2) Supplemental Requirements:

##### (a) Alternative A:

- (i) Three deciduous trees, with a minimum caliper of three inches, per each 1,000 square feet of the perimeter setback area; and
- (ii) Two flowering trees, with a minimum caliper of two inches, per each 1,000 square feet of perimeter setback area; and
- (iii) Ten evergreen shrubs, minimum five-gallon size, per 1,000 square feet of the perimeter setback area; and
- (iv) Any paved surfaces shall be no more than 10 percent of the perimeter setback area; and

- (v) Planting area must either be raised or sloped. If raised, the planting area shall be surrounded by a wall with a minimum height of 18 inches and a maximum height of 24 inches to allow for sitting.
  - (b) Alternative B:
    - (i) Three deciduous trees, with a minimum caliper of three inches, per each 1,000 square feet of the perimeter setback area; and
    - (ii) Lawn greater than five feet in width or ground cover on at least 25 percent of the perimeter setback area; and
    - (iii) Any paved surfaces shall be no more than 75 percent of the perimeter setback area; and
    - (iv) Paved areas shall use brick, stone or tile in a pattern and texture that is level and slip-resistant; and
    - (v) Opportunities for pedestrian flow from the sidewalk shall be frequent and direct. Changes in grade between the linear buffer and sidewalk shall be accommodated by steps or terraces, rather than walls.
  - iii. Where the Downtown boundary abuts property outside the Downtown other than right-of-way described in paragraph D.4.b.ii of this section, the minimum setback from the Downtown boundary (or perimeter property lines when the setback has been relocated pursuant to Note 10 of subsection 20.25A.090.D.2) shall be landscaped as follows:
    - (1) The entire setback (20 feet) shall be planted. No portion may be paved except for vehicular entrance drives and required mid-block pedestrian connections.
    - (2) The setback must incorporate a berm having a minimum height of three and one-half feet.
    - (3) The setback must be planted with:
      - (a) Evergreen and deciduous trees, with no more than 30 percent deciduous, a minimum of 10 feet in height, at intervals no greater than 20 feet on center; and
      - (b) Evergreen shrubs, a minimum of two-gallon in size, at a spacing of three feet on center; and
      - (c) Living ground cover so that the entire remaining area will be covered in three years.
  - c. Street Trees. Street trees required by LUC 20.25A.060.C along Main Street, 100th Avenue NE or NE 12th Street must be at least four inches in caliper.
- 5. Special Design Standards. The following design standards apply to all development within the Perimeter Design District:
  - a. Upper Level Stepback. A building facade facing any of the following streets must incorporate a 15-foot deep stepback in that facade at a height no more than 40 feet above average finished grade:
    - i. The south side of NE 12th Street between 102nd Avenue NE and 112th Avenue NE; and
    - ii. Both sides of Main Street between 100th Avenue NE and Bellevue Way NE; and
    - iii. The north side of Main Street between 108th Avenue NE and 112th Avenue NE; and
    - iv. The east side of 100th Avenue NE between the southwest corner and northwest corner of the Downtown boundary; and
    - v. Both sides of 102nd Avenue NE between Main Street and NE 1st Street.

Encroachments into this stepback are permitted for minor building elements subject to all the terms and conditions of LUC 20.20.025.C.
  - b. Lighting. Lighting fixtures shall incorporate cutoff shields to minimize off-site impacts.

- c. Signs. All signs shall be an integral part of the architectural design and shall be consistent with the scale and architecture of the building.

6. Special Bonus Provisions for Subdistrict C.

- a. The nonresidential 1.5 FAR may be exceeded in Subdistrict C only through the provision of neighborhood services and residential uses as provided in this paragraph.
- b. The nonresidential 1.5 FAR may be exceeded in Subdistrict C only if at least 25 percent of the bonus FAR square footage is comprised of neighborhood services; provided, that the Director of the Department of Planning and Community Development may reduce the requirement to no less than 15 percent when at least 20 percent of all development on the site will consist of housing. Neighborhood services include the following uses:
  - i. Hardware (retail)
  - ii. Food (retail)
  - iii. Drugstores
  - iv. Personal services (as identified in LUC 20.10.440 Services Chart)
  - v. Child care services
  - vi. Gasoline service stations
  - vii. Garden supplies (retail)
- c. In Subdistrict C, the FAR amenity standards in LUC 20.25A.030.C apply except that the amenity bonuses which may be earned to exceed the nonresidential FAR of 1.5 are as follows:
  - i. Neighborhood services, as defined in paragraph D.4 of this section, are bonused at a ratio of 2:1\*; and
  - ii. Residential uses, as described in LUC 20.25A.030.C.10 are bonused at a ratio of 1:1\*.

\*Measured as square feet of permitted development for each qualifying developed square foot of amenity.

- d. Mixed Use. In a mixed use building type, the maximum FAR of 3.0 is the limit of all non-residential uses in the building. Additional floor area may be permitted only as necessary to provide the special bonus residential uses required by this section in order to achieve the nonresidential floor area above an FAR of 1.5. In no case may the total FAR exceed 5.0. For the purposes of these provisions, the residential uses may be located in a separate building or buildings within the same project limit as the nonresidential uses; however, the mixed use building height of the Perimeter Design District Dimension Chart shall apply.

**E. Design Guidelines.**

1. Development projects should include a mid-block street, where feasible, to provide more convenient circulation within the perimeter of the Downtown and to promote development with a human scale.
2. Buildings should incorporate interior arcades, open courtyards, enclosed plazas or combinations thereof which offer mid-block pedestrian connections between perpendicular and/or parallel streets.
3. Buildings should be clad with materials which minimize reflected light. Overhangs, awnings, sunscreens and other devices should be considered in order to minimize conditions of glare.

4. Building facades should be divided into increments through the use of bay windows, offsets, angled facets, recesses and other architectural features which serve to break down the scale.
5. Rooftops should incorporate features such as pitched or sloped forms, terraces, perimeter planting to soften an otherwise rectilinear profile.
6. Surface parking should be concealed from street level views by berms, hedges, walls or combinations thereof.
7. Special attention should be given to the provision of elements at or near the ground level such as awnings, recessed entries, water features, address signs, seasonal flower beds, seating, pedestrian-oriented uses and display kiosks. (Ord. 5717, 2-20-07, § 8; Ord. 5582, 2-7-05, §§ 8, 9; Ord. 5480, 10-20-03, § 16; Ord. 5372, 6-3-02, §§ 5, 6; Ord. 5147, 6-21-99, § 1; Ord. 5091, 8-3-98, §§ 12, 13; Ord. 4973, 3-3-97, § 112; Ord. 4816, 12-4-95, § 212; Ord. 4268, 7-1-91, §§ 4 – 7; Ord. 4235, 4-1-91, §§ 2, 3; Ord. 3747, 1-20-87, §§ 12, 13; Ord. 3553, 10-7-85, § 6)

### **20.25A.100 Downtown Core Design District.**

#### **A. Definition of District.**

The Downtown Core Design District encompasses the area bounded by the extension of the centerlines of 102nd Avenue NE on the west, NE 9th Street on the north, 112th Avenue NE on the east and NE 3rd Street on the south plus any area within the Downtown-O-2 Land Use District not described above.

#### **B. Purpose.**

The purpose of the Downtown Core Design District is to implement the Downtown Subarea Comprehensive Plan Policies by providing more specific development guidelines, and by assuring high levels of attractiveness, urbanity, design quality, and coordination of development within the most intensive, visible portion of Downtown.

#### **C. All Development Subject to Design Review.**

All development within the Downtown Core Design District must be reviewed by the Director of Planning and Community Development through Design Review, Part 20.30F LUC.

#### **D. Review Criteria.**

The Director of Planning and Community Development shall use the Downtown Design Review Criteria, LUC 20.25A.110, and the provisions of this section in deciding upon an application for development in the Downtown Core Design District.

#### **E. Downtown Core Design District Guidelines.**

1. Major Pedestrian Corridor.
  - a. Purpose. The major pedestrian corridor is to serve as a focus for pedestrian use.
  - b. Location. The alignment of the major pedestrian corridor is defined as the area within 30 feet of the extension of the north line of Lots 3 and 4, Block 2 of Cheriton Fruit Gardens Plat No. 1 recorded in the King County Department of Records and Elections in Volume 7 of Plats at page 47, extending from the eastern edge of the enclosed portion of Bellevue Square to 108th Avenue NE and the area within 30 feet north of the north curb and 30 feet

south of the south curb of the Bellevue Transit Center traffic lanes as hereafter approved by the City, extending across the 108th Avenue NE right-of-way and to 110th Avenue NE. This alignment may be modified by the Bellevue Pedestrian Corridor Guidelines or by a Corridor Development Design Plan for a specific property.

- c. Bellevue Pedestrian Corridor Guidelines. Each development abutting the pedestrian corridor as described in paragraph E.1.c.v of this section must comply with the provisions of this paragraph and the Bellevue Pedestrian Corridor Guidelines and Major Open Space Design Guidelines as adopted by the City Council, or as the same may hereafter be amended. The Bellevue Pedestrian Corridor and Major Public Open Space Design Guidelines consist of general design guidelines consistent with provisions of this paragraph.
  - i. The corridor must present a coordinated design. The City will consider coordinated design features such as uniform treatment of signing, landscaping and lighting over the entire length of the corridor. Variety in design will be allowed and in some cases encouraged in order to provide visual interest and harmony with adjacent development. The corridor must incorporate numerous pedestrian amenities such as seating areas, landscaping, art features, weather protection and pedestrian scale lighting.
  - ii. The major pedestrian corridor must provide predominantly continuous pedestrian-oriented frontage, plazas, pedestrian ways, street arcades, landscape features, or enclosed plazas along its entire length.
  - iii. The entire corridor must be open to the public 24 hours per day. Segments of the corridor may be bridged or covered for weather protection, but not enclosed. Temporary closures will be allowed as necessary for maintenance purposes.
  - iv. Pedestrian movement across 104th Avenue NE, 106th Avenue NE or 108th Avenue NE shall be at grade.
  - v. The major pedestrian corridor width is established as part of the Bellevue Pedestrian Corridor Guidelines. The corridor width shall average 60 feet and in no case be less than 40 feet over each superblock west of 108th Avenue NE, and shall average 30 feet and in no case be less than 20 feet on each side over the superblock extending from the western edge of the 108th Avenue NE right-of-way to 110th Avenue NE. All subdivisions or short subdivisions hereafter approved or permits for any structure or permanent parking or circulation area shall be reviewed for compatibility with the alignment of the major pedestrian corridor and major public open space as specified in paragraph E.1.b of this section or in the Bellevue Pedestrian Corridor and Major Public Open Space Design Guidelines if any lot line, structure or permanent parking or circulation area is within:
    - (1) 330 feet of the centerline of the major pedestrian corridor if west of 108th Avenue NE; or
    - (2) The area between the exterior edge of the curblines of the Transit Center and the eastward extension of the trigger lines as defined in paragraph E.1.c.v(1) of this section to 110th Avenue NE.
- d. Preservation of the Major Pedestrian Corridor.
  - i. Prior to the issuance of a Building Permit for any structure other than surface parking; and other than any interior remodel or exterior remodel which enlarges exterior dimensions such that new floor area not exceeding a total of 20 percent of the gross floor area of the existing building is added; and provided, that all new floor area is devoted to pedestrian-oriented uses; located within the major pedestrian corridor as defined in paragraph E.1.b of this section, the following conditions must be met:
    - (1) The alignment of the major pedestrian corridor related to the proposed structure or permanent parking or circulation area must be established by the execution and recording of a legal agreement in accordance with paragraph E.1.e.i or ii of this section.

- (2) A Design Development Plan for the section of the corridor required to be constructed under paragraph E.1.c.iii of this section. Corridor must be approved by the Director of Planning and Community Development as required by paragraph E.1.e.ii of this section. Construction must begin prior to the issuance of certificate

- of occupancy or a temporary certificate of occupancy for the structure other than surface parking as required by paragraph E.1.e.iii(2) of this section.
- ii. Building Permits for surface parking areas to be located in this corridor as defined in paragraph E.1.b of this section may be granted for up to a five-year period, subject to the landscape requirement for surface parking areas in the Downtown-MU Land Use District, as specified in LUC 20.25A.040. Building Permits for parking areas may be renewed only if the Director of Planning and Community Development finds that an extension is necessary to meet the maximum Code requirements for parking and the extension is necessary for the construction of a building requiring utilization of the surface parking area.
- e. Provision of the Corridor.
- i. If the property owner wishes to at any time obtain bonus FAR for construction of the major pedestrian corridor, the City may approve the subdivision or short subdivision of property resulting in any interior lot line which is within the distances specified in paragraph E.1.c.v of this section only if:
    - (1) The owner of the property to be subdivided or short subdivided executes a legal agreement providing that all property that he/she owns within the superblock in which any of property to be subdivided or short subdivided is located and which is within the alignment of the major pedestrian corridor established under paragraph E.1.b, E.1.c or E.1.e.iii of this section (hereafter the "Corridor Property") shall be subject to a nonexclusive right of pedestrian use and access by the public. The agreement shall legally describe and shall apply to only that property of the owner located within the distances specified in paragraph E.1.c.v of this section. Such an agreement shall further provide that:
      - (a) The public right of pedestrian use established thereunder shall be enforceable by the City of Bellevue, and the City shall have full rights of pedestrian access to and use of the corridor property for purposes of enforcing the rights of the public under this agreement.
      - (b) The obligations under the agreement shall run with the corridor property. The agreement shall be reviewed at the end of 50 years from the date the agreement is signed and shall continue or change in accordance with the then existing public need for pedestrian use and access of the corridor for subsequent 50-year terms.
      - (c) The owner will design and construct the corridor within such corridor property in accordance with the requirements of paragraph E.1 of this section.
      - (d) The agreement shall be recorded with the King County Department of Records and Elections.
      - (e) The owner will maintain the portion of the corridor located on the corridor property and keep the same in good repair.
      - (f) The City will provide adequate police protection.
      - (g) No modifications may be made to the corridor without approval of the City in accordance with paragraph E.1.e.ii of this section.
      - (h) The alignment of any such portion of the pedestrian corridor established by a legal agreement may be modified or terminated by the property owner and the City if the alignment of any section of the major pedestrian corridor changes pursuant to paragraph E.1.e.ii of this section.
      - (i) The owner may adopt reasonable rules and regulations for use of his/her portion of the corridor; provided, that the same may not be inconsistent with the requirements or intentions of this section.
      - (j) Any other terms and conditions that the owner(s) and the City agree to.
  - ii. Corridor Design Development Plan. Prior to the issuance of a Building Permit for the construction of any structure other than surface parking; and other than any interior remodel or exterior remodel which enlarges exterior dimensions such that new floor

area not exceeding a total of 20 percent of the gross floor area of the existing building is added; and provided, that all new floor area is devoted to pedestrian-oriented uses; on the property, any portion of which abuts the major pedestrian corridor and is within the distances specified in paragraph E.1.c.v of this section, a Design Development Plan for the section of the corridor required to be constructed under paragraph E.1.e.iii of this section must be submitted to and approved by the Director of Planning and Community Development, through Design Review, Part 20.30F LUC. If the owner constructs a temporary pedestrian linkage under paragraph E.1.e.iii of this section, preparation of the Corridor Design Development Plan will not be required until the property to be developed is located within:

- (1) 130 feet of the centerline of the major pedestrian corridor, west of 108th Avenue NE; or
  - (2) The area between the exterior edge of the curblines of the Transit Center and the eastward extension of the trigger lines as defined in paragraph E.1.e.ii(1) of this section to 110th Avenue NE. The proposed plan must specify the following elements:
    - (a) Landscaping,
    - (b) Lighting,
    - (c) Street furniture,
    - (d) Color and materials,
    - (e) Relationship to building frontage,
    - (f) Specific alignment for property on which the corridor will have to be constructed by the applicant proposing development,
    - (g) Any other physical element which the Director of Planning and Community Development and the City Council, in their review, determine is necessary for and consistent with the Design Development Plan for a specific section of the major pedestrian corridor, not including specific requirements to construct structures containing retail uses abutting the corridor.
- iii. The City may issue a permit for the construction of a structure other than surface parking and other than any interior remodel or exterior remodel which enlarges exterior dimensions such that new floor area not exceeding a total of 20 percent of the gross floor area of the existing building is added; and provided, that all new floor area is devoted to pedestrian-oriented uses; on property any part of which abuts the major pedestrian corridor and is within the distances specified in paragraph E.1.c.v of this section at the time of the adoption of Ordinance No. 2945 only if:
- (1) The owner complies with paragraph E.1.e.i(1)(a) through (j) of this section if that owner wishes to earn bonus FAR for construction of the major pedestrian corridor; and
  - (2) The owner files a Building Permit application to construct his/her section of the corridor on (a) land he/she owns within the corridor and within the superblock of the subject construction permit for a structure, and (b) on one-half the width of any abutting City-owned land in the corridor (except for intersections listed below). The City shall initiate or abutting property owners may initiate a street vacation for right-of-way the City owns between 104th Avenue NE and 106th Avenue NE at NE 6th Street in conjunction with or prior to an owner application to construct the major pedestrian corridor. Actual construction of the corridor must begin prior to the issuance of a certificate of occupancy or temporary certificate of occupancy for the structure other than surface parking. The City shall construct the corridor at the street intersections of the corridor and 104th Avenue NE, 106th Avenue NE, and 108th Avenue NE. The width of the corridor that would have to be constructed under the requirements of paragraph E.1.e.iii of this section may be modified when the final alignment of the corridor is established as part of Corridor Design Development Plan (paragraph E.1.e.ii of this section). Notwithstanding this potential change in the width of the corridor that would have to be constructed under paragraph E.1.e.iii of this section, property owners shall at a minimum be required to

construct the section of the corridor as specified in paragraph E.1.e.iii(2)(a) of this section. Building Permits for surface parking areas to be located on property any part of which abuts the major pedestrian corridor and is within the distances specified in paragraph E.1.c.v of this section at the time of the adoption of the ordinance codified in this chapter may be issued subject to the conditions specified in paragraph E.1.d.ii of this section. Notwithstanding any other requirement of this section, if a temporary pedestrian linkage is constructed as specified in paragraph E.1.f of this section, construction of the corridor will not be required unless the property to be developed is located within the distances specified in paragraph E.1.e.ii of this section.

- f. Temporary Pedestrian Linkage.
  - i. Any temporary pedestrian linkage developed under paragraph E.1.c.iii of this section shall at a minimum include a combination of paving, landscaping and lighting to permit safe pedestrian movement at night.
  - ii. The City Council must approve a plan for any temporary pedestrian linkage to be prepared as part of a Corridor Design Planning process following the procedures of Process IV, LUC 20.35.400 et seq. The Planning Commission shall be the advisory body.
  - iii. Any owner constructing a temporary pedestrian linkage under paragraph E.1.e.iii of this section must construct the linkage across all lands that he/she owns within the superblock where development is proposed that abut or are within the alignment of the corridor.
- g. Maintenance. Each segment of the major pedestrian corridor shall be maintained by the property owners abutting it. The City shall maintain the intersections of all public streets with the corridor.
- h. Bonus Floor Area for Major Pedestrian Corridor Construction. Bonus floor area associated with the major pedestrian corridor, LUC 20.25A.030, shall be awarded to owners of property within the distances specified in paragraph E.1.c.v of this section through Design Review, Part 20.30F LUC, and according to the provisions of paragraph E.1.e.iii(2) of this section, in conjunction with an application for a permit to construct a structure, permanent parking, or circulation area within the major pedestrian corridor and the provision of a legal agreement establishing the public right of pedestrian use pursuant to paragraph E.1.e.i(1)(a) through (j) of this section.
- i. Exempt Activity/Use. Notwithstanding the provisions of paragraph E.1 of this section, the following activities and uses may occur on property within the distances specified in paragraph E.1.c.v of this section without concurrent construction of the major pedestrian corridor, the temporary pedestrian linkage or the intermediate pedestrian corridor:
  - i. Surface parking approved pursuant to paragraph E.1.d.ii of this section;
  - ii. Landscape development;
  - iii. Street, access and sidewalk improvements, including the Transit Center as provided for in paragraph E.2 of this section;
  - iv. Any interior remodel;
  - v. Any exterior remodel; provided, that if exterior dimensions are enlarged new floor area may not exceed a total of 20 percent of the gross floor area of the structure as it existed on the effective date of this provision; and provided, that all new pedestrian level floor area is devoted to pedestrian-oriented uses;
  - vi. Development of the temporary pedestrian linkage or the intermediate pedestrian corridor.

- j. Intermediate Pedestrian Corridor.
  - i. Notwithstanding any provision of this Code which requires construction of the major pedestrian corridor, a property owner may phase construction of that section of the major pedestrian corridor otherwise required to be built by delaying any portion not directly abutting or adjacent to the project limit which triggered the construction requirement if the owner provides an intermediate pedestrian corridor for that delayed portion of the corridor property which:
    - (1) Is at least 16 feet in width from the centerline of the major pedestrian corridor west of 108th Avenue NE, or extending outward from the exterior edge of the north or south curblines of the Bellevue Transit Center traffic lanes. This space shall be designed to include a minimum four feet edge separating and defining the space, a minimum eight feet pedestrian movement area and a minimum four feet recreation/activity area.
    - (2) Incorporates lighting, planting, seating, and scored or decorative paving.
    - (3) Provides a sense of enclosure along the exterior edge of the space by the use of a design element which both physically and visually separates the intermediate corridor from abutting property. Nonexclusive examples of such an element include a sculptural wall, dense planting, or berm.
    - (4) Is consistent with the applicable provisions of the Bellevue Pedestrian Corridor Guidelines, as determined by the Director of Planning and Community Development.
  - ii. Design for any intermediate pedestrian corridor must be approved through Design Review, Part 20.30F LUC, in conjunction with the Design Development Plan for the major pedestrian corridor required to be constructed.
  - iii. An intermediate pedestrian corridor satisfies any requirement of this Code to construct the temporary pedestrian linkage.
  - iv. Space developed as an intermediate pedestrian corridor must be replaced by the major pedestrian corridor at the time of development on any project limit abutting or adjacent to the major pedestrian corridor. Construction of the major pedestrian corridor must be in conformance with all requirements of paragraph E.1.e of this section.

## 2. Transit Center.

- a. Purpose. The Transit Center is to provide the opportunity for intra-Eastside and regional bus routes to be timed and coordinated in a manner to maximize bus service for Bellevue Downtown employees, shoppers, and Bellevue residents.
- b. Location. The Transit Center is located within the Core Design District on NE 6th Street between 108th Avenue NE and 110th Avenue NE. Any location of the Transit Center serving the Downtown must:
  - i. Be coordinated with feasible transit routes;
  - ii. Be coordinated with efficient traffic operation;
  - iii. Be compatible with private development in the immediate vicinity;
  - iv. Permit the opportunity for phased construction; and
  - v. Be compatible with areas of pedestrian focus.
- c. Design.
  - i. The design of the Transit Center must maximize accessibility for passengers and buses while providing a high level of adjacent pedestrian-oriented activity and employment density. The Center must include bus access lanes, bus stalls, and passenger shelters. It may be integrated into another privately or publicly developed facility.



- ii. Ownership. The owners of property to be devoted to a major public open space will retain fee ownership of that property.
- iii. Public Access – Legal Agreement.
  - (1) Each owner of property to be devoted to a major public open space who chooses to participate in the FAR bonus system for a major public open space shall execute a legal agreement providing that such property is subject to a nonexclusive right of pedestrian use and access by the public.
  - (2) The agreement shall further provide that the public right of pedestrian use established thereunder shall be enforceable by the City of Bellevue, and the City shall have full rights of pedestrian access to and use of the major public open space for purposes of enforcing the rights of the public under the agreement.
  - (3) The agreement shall be recorded with the King County Division of Records and Elections and Bellevue City Clerk.
  - (4) The obligations under the agreement shall run with the land devoted to a major public open space. The agreement shall be reviewed at the end of 50 years from the date the agreement is signed and shall continue or change in accordance with the then-existing public need for pedestrian use and access of a major public open space for subsequent 50-year terms.
  - (5) The owner of property to be devoted to a major public open space will maintain that portion of the major public open space and keep the same in good repair.
  - (6) The owners of property to be devoted to a major public open space may adopt reasonable rules and regulations for the use of that space; provided, that the rules and regulations are not in conflict with the right of pedestrian use and access and the intention of paragraph E.4.d.iii of this section.
- iv. Arrangement of Space. The general apportionment, location, and major design features of at least the minimum area of a major public open space shall be established as part of the Bellevue Pedestrian Corridor and Major Public Open Space Design Guidelines. The specific apportionment and specific design of a major public open space on each affected parcel shall be established through the Design Development Plan described in paragraph E.4.d.x of this section.
- v. Development Rights. Space above and beneath the area to be devoted to a major public open space may be developed by the property owner so long as that development is not in conflict with any established pedestrian use of and access to the major public open space, the intentions of paragraph E.4.d.iii of this section, if applicable, and the Bellevue Pedestrian Corridor and Major Public Open Space Design Guidelines.
- vi. Floor Area Ratio.
  - (1) Basic. Area to be devoted to a major public open space may at any time be used to calculate the basic floor area ratio of development for any project limit which incorporates that major public open space, or of development on property in the same ownership which abuts property which incorporates the major public open space. For purposes of this provision, abutting property includes all property in the same ownership separated from the major public open space by only a single public right-of-way. Any transfer of basic floor area to an abutting property must be recorded with the King County Division of Records and Elections and Bellevue City Clerk.
  - (2) Bonus.
    - (a) Bonus floor area at the ratio of 16 square feet of floor area to one square foot of major public open space will be awarded to owners of property to be devoted to the major public open space who provide a recorded legal agreement pursuant to paragraph E.4.d.iii of this section upon approval of an application to construct that major public open space.
    - (b) Bonus floor area earned for construction of a major public open space may be:
      - (i) Transferred to any other property within the area of the Downtown bounded on the west by Bellevue Way, on the east by 112th Avenue NE,

- on the south by NE 4th Street and on the north by NE 8th Street. Properties may utilize transferred floor area only to the extent that the building height does not exceed limits depicted on the map entitled "Height Limits in the Core Design District" in LUC 20.25A.030.E. Each transfer must be recorded with the King County Division of Records and Elections and Bellevue City Clerk; and
- (ii) Utilized to exceed the maximum building height of structures on the project limit incorporating the major public open space, or of structures to which the bonus floor area is transferred, subject to the limitations in paragraph E.4.d.vi(2)(b)(i) of this section.
- vii. Construction Required. Subject to paragraph E.4.d.viii of this section, construction by the property owner of all or part of a major public open space on property in that ownership at the location identified in the Bellevue Pedestrian Corridor and Major Public Open Space Design Guidelines is required in conjunction with any development on property in that ownership within:
- (1) 175 feet of the intersection of the eastern edge of the 106th Avenue NE right-of-way and the centerline of the major pedestrian corridor, but including only that area east of the 106th Avenue NE right-of-way; or
  - (2) 175 feet of the intersection of the centerline of the 110th Avenue NE right-of-way and the centerline of the major pedestrian corridor, or the extension thereof; or
  - (3) 175 feet of the intersection of the centerline of the Bellevue Way right-of-way and the centerline of the major pedestrian corridor.
- viii. Exempt Activity/Use. Notwithstanding the provisions of paragraph E.4.d.vii of this section, the following activities and uses may occur on property described therein without concurrent construction of the major public open space:
- (1) Surface parking, subject to the landscape development provisions of this Code, for a period of not more than five years;
  - (2) Temporary major pedestrian corridor improvements in conformance with the Interim Corridor Design Plan;
  - (3) Landscape development;
  - (4) Street improvements including the Transit Center as provided for in paragraph E.2 of this section;
  - (5) Any interior remodel;
  - (6) Any exterior remodel which enlarges exterior dimensions such that new floor area not exceeding a total of 20 percent of the gross floor area of the existing building is added, and all new floor area is devoted to pedestrian-oriented uses.
- ix. Major Public Open Space Design.
- (1) Prior to issuance of a Building Permit for any structure which requires construction of all or part of a major public open space, or prior to actual construction of all or part of a major public open space, whichever comes first, the Bellevue Pedestrian Corridor and Major Public Open Space Design Guidelines shall contain an illustrative design generally apportioning the minimum required amount of major public open space for that entire open space. Each major public open space may have a separate illustrative design.
  - (2) The property owners shall record the approved illustrative design with the King County Division of Records and Elections and Bellevue City Clerk.
- x. Design Development Plan.
- (1) Prior to issuance of a Building Permit for any structure which requires construction of all or part of a major public open space, or prior to actual construction of all or part of a major public open space, whichever comes first, a Design Development Plan for that portion to be constructed must be submitted to and approved by the Director of Planning and Community Development.
  - (2) The Director of Planning and Community Development shall review the plan, or amend any approved plan through Design Review, Part 20.30F LUC. Alternatively, the City Council may review and adopt the plan as part of an action on any

project subject to the City Council Design Review process, LUC 20.30F.116. A plan approved by the Council through the City Council Design Review process may be amended by the Director through Part 20.30F LUC.

- (3) The proposed plan must specify the following elements:
  - (a) Landscaping;
  - (b) Lighting;
  - (c) Street furniture;
  - (d) Color and materials;
  - (e) Relationship to building frontage;
  - (f) Specific location of the major public open space;
  - (g) All design features required pursuant to paragraph E.4.c of this section;
  - (h) Relationship to and coordination with other portions of the major public open space, and with the major pedestrian corridor;
  - (i) Any other physical element which the Director of Planning and Community Development determines is necessary for and consistent with the Major Public Open Space Design Plan.

#### 5. Minor Publicly Accessible Spaces.

- a. Purpose. Minor publicly accessible spaces provide relief from high intensity urban development, serve as visual gateways to the intensive Downtown Core, and provide opportunities for active or passive recreation.
- b. Location. Minor publicly accessible spaces shall be located along Bellevue Way and 108th Avenue NE approximately at their intersections with NE 8th Street, NE 6th Street and NE 4th Street. Additionally, at least two spaces shall be located in each superblock based on coordination of design and proximity to other publicly accessible spaces, or pedestrian connections.
- c. Design.
  - i. Minor publicly accessible spaces may be outdoors or enclosed as long as adequate access is provided and their existence is easily identifiable.
  - ii. A minor publicly accessible space must be open at least during normal business hours.
  - iii. A minor publicly accessible open space must be developed as a plaza, enclosed plaza, or art or landscape feature. The design criteria of LUC 20.25A.030.C must be met, and the FAR amenity bonus may be utilized.

#### 6. View Preservation Corridors.

- a. Purpose. View preservation corridors retain the opportunity for viewing Lake Washington, the Seattle skyline, the Olympic Mountains and the Cascade Mountains from the major public open spaces and the major pedestrian corridor.
- b. Design Criteria. Consideration must be given to the identification and preservation of these views in the siting, orientation and bulk of structures in the Core Design District.

7. Upper Level Stepback. Each building facade within the Core Design District facing NE 4th, NE 8th or Bellevue Way must incorporate a minimum 20-foot-deep stepback at a height between 25 feet and the level of the first floorplate above 40 feet. This required stepback may be modified or eliminated if the applicant demonstrates through Design Review (Part 20.30F LUC) that: (a) such stepback is not feasible due to site constraints, such as a small or irregularly shaped lot, or (b) the modification is necessary to achieve design elements or features encouraged in this Section 20.25A.100 or the provisions of LUC 20.25A.110 and the modification does not interfere with preserving view corridors. Where a modification has been

granted under LUC 20.25A.020.B.4, the upper level stepback may be incorporated between 25 feet and the level of the first floorplate above 45 feet. (Ord. 5582, 2-7-05, §§ 10, 11, 12; Ord. 5497, 11-17-03, § 5; Ord. 5480, 10-20-03, § 17; Ord. 5372, 6-3-02, § 7; Ord. 5242, 10-2-00, § 1; Ord. 5100, 10-19-98, §§ 2, 3, 4; Ord. 5050, 1-20-98, § 7; Ord. 4973, 3-3-97, § 113; Ord. 4816, 12-4-95, § 213; Ord. 4654, 6-6-94, § 43; Ord. 4268, 7-1-91, §§ 8, 9; Ord. 3901B, 5-16-88, § 3; Ord. 3530, 8-12-85, §§ 46 – 53; Ord. 3498, 5-27-85, §§ 57 – 59, 61, 62; Ord. 3272, 11-21-83, §§ 1 – 5, 7 – 9)

### **20.25A.110 Design Review criteria.**

The Director of the Department of Planning and Community Development will consider the extent to which a proposal complies with the following criteria whenever a decision using Design Review, Part 20.30F LUC, is required by this Part 20.25A. Additional design criteria from other sections of this Part 20.25A may also apply to specific projects.

#### **A. Site Design Criteria.**

1. Vehicular Circulation and Parking.
  - a. Provide efficient vehicular access to parking and service areas which is coordinated on a superblock basis.
  - b. Coordinate the location of vehicular and pedestrian mid-block connections, considering opportunities for mid-block crossings.
  - c. Provide for safe and pleasant movement within the Downtown for the pedestrian and bicyclist.
  - d. Maximize the separation of vehicular traffic from pedestrian areas by means of level changes, space and distance, or landscaping.
  - e. Incorporate retail shopping space at ground level into parking structures whenever practical and appropriate.
  - f. Minimize the location of parking adjacent to pedestrian connections.
  - g. Limit the number of driveway openings and the number of access lanes in each opening.
  - h. Where appropriate, fit garages into the topography and make use of garage roofs for public open space, recreation or landscaped areas.
  - i. Maximize the use of underground parking.
2. Pedestrian Circulation and Amenities. See LUC 20.25A.060.
3. Wind and Sun.
  - a. Ensure that the form and placement of buildings consider desirable year-round conditions of sun and shade in surrounding open spaces and public areas.
  - b. Design new buildings with wind conditions in mind so as to shelter pedestrians from undesirable winds, particularly on the ground, and in publicly accessible areas.
  - c. Consider how new buildings might incorporate outdoor spaces of calm, especially for winter, and places of suitable breeziness in summer at levels of pedestrian activity.

- d. Consider wind and sun in design of landscaping: e.g., evergreen tree-planting as wind blocks, or deciduous trees on south and west sides of open spaces to maximize winter sun penetration.

#### 4. Open Space.

- a. Design and locate open spaces, such as plazas, squares and large landscaped areas, to work as part of a comprehensive system of spaces in the Downtown.
- b. Design open spaces to provide for maximum use by a wide range of people.
- c. In designing open spaces, especially plazas, consider the following:
  - i. Orientation. Orient to sunlight and provide good physical and visual access to the sidewalk, so that the space is perceived as an extension of the sidewalk.
  - ii. Dimensions. Design as adequate for seating, planting, etc., but not so large as to appear barren and uninviting.
  - iii. Seating. Provide comfortable height and depth, and appropriate arrangement.
  - iv. Pavement. Use nonglare, nonslip, and safe surface materials.
  - v. Trees and Planting. Consider provision for shade and sun. Use to create space and define human scale. Provide protection from wind.
  - vi. Amenities. Use pedestrian-scaled lighting, fountains, litter receptacles, bicycle racks, and sheltered waiting areas.
  - vii. Provision of Space for Attractions. Design to permit vendors, outdoor cafes, rotating art displays, or abutting retail activity.
  - viii. Physical Access. Ensure ready physical as well as visual access with special attention to elevational difference.
  - ix. Enclosure. Use landscaping or structure to provide a sense of enclosure.

#### 5. Light and Glare.

- a. Consider and mitigate light and glare impacts upon major public facilities, streets and major public open spaces.
- b. Mitigating measures may include use of low-reflecting building materials, landscaping, tilting of reflective panels, reorientation of the building or the addition of screening devices such as louvered screens and marquees.

### **B. Downtown Patterns and Context.**

#### 1. Natural Setting and Topography.

- a. Make creative use of any existing topographic variations in site design and location of buildings, circulation patterns, parking area design and public spaces to enhance the setting and provide variety.
- b. Make maximum use of views to mountains, Lake Washington and the Seattle skyline.
- c. Seek high quality of design for all buildings constructed at prominent locations. These locations may include areas on ridge crests and hilltops, fronting and public open spaces, those closing a vista and those affording a silhouette against the sky.

2. Landscape Design.
  - a. Make effective use of significant landscape features to complement and contrast with building forms. This includes massing of plant materials to constitute a recognizable visual unit which contrasts effectively with built forms.
  - b. Encourage retention of significant existing vegetation, where it can be incorporated into efficient site design and maintained in a safe and healthful condition.
  - c. Consider the location or relocation of traffic control boxes, power vaults, utility boxes and similar features in the design of the pedestrian areas to minimize the impact on the visual and physical quality of the pedestrian environment.
3. Views.
  - a. Consider the negative impact of a building on views, both from existing buildings and future developable or redevelopable sites.
  - b. Consider the availability of public views from public spaces such as streets, street intersections, parks, plazas and areas of pedestrian concentration.
4. Building Height and Bulk.
  - a. Buildings near public open spaces should permit visual access and, where feasible, physical access to the public open space.
  - b. Wherever practicable, buildings should be oriented to minimize the shadows they cast on publicly accessible open spaces.
  - c. Encourage slender towers, particularly at upper levels.
  - d. Discourage buildings of extreme rectangular shape which tend to be out of proportion for their floor area.
  - e. Encourage spacing between towers to retain the feeling of an open, airy Downtown.
  - f. For buildings outside the Core Design District, encourage building massing which minimizes visual impacts to surrounding residential neighborhoods.
  - g. Stepbacks required for diminished floorplate buildings, LUC 20.25A.020.A.2(22), should be oriented to the public street or streets adjacent to the building site to maximize the availability of light and air at the street level and to preserve view corridors. Where the site abuts more than one public street, preference for the orientation of the stepback should be given to the street intended to have the highest orientation to pedestrians as provided by LUC 20.25A.115, Design Guidelines: Building/Sidewalk Relationships.
  - h. Encourage rooftop features, appropriate to the overall height and scale of the building, to modify an otherwise unmodulated profile.
5. Transitions. In transitions between districts in the Downtown and between properties, the lower portions of buildings should be designed to promote easy circulation, good relationships among open spaces, visual connection in scale, and maximum penetration of sunlight to the ground level.

6. Patterns of Activity.

- a. Maximize opportunities for vital, pedestrian-level activity in all areas of the Downtown.
- b. Provide space for a variety of appropriate activities accessible to the public at large in the Downtown, especially at ground level and at main pedestrian levels.
- c. Uses and activities that are nonpublic or nonpedestrian in nature should not be located adjacent to pedestrian areas.

7. Signage.

- a. Ensure that signage is an integral part of the architectural design.
- b. Encourage signage which is scaled to the pedestrian and enhances the pedestrian environment.
- c. Signage must comply with the applicable requirements of Chapter 22B.10 BCC, now or as hereafter amended. (Ord. 5717, 2-20-07, § 9; Ord. 5582, 2-7-05, §§ 13, 14, 15, 16; Ord. 5497, 11-7-03, § 6; Ord. 5050, 1-20-98, § 8; Ord. 4973, 3-3-97, § 114; Ord. 4816, 12-4-95, § 214; Ord. 4268, 7-1-91, § 10; Ord. 3530, 8-12-85, § 54; Ord. 3498, 5-27-85, § 63; Ord. 3262, 6-6-83, § 2)

**20.25A.115 Design Guidelines – Building/Sidewalk Relationships.**

**A. General.**

1. Each development within a Downtown Land Use District must comply with the applicable provisions of that document entitled "Design Guidelines – Building/Sidewalk Relationships," now or as hereafter amended pursuant to the provisions of that document.

2. For purposes of applying the FAR Amenity Incentive System, the “City Center District” shall mean that area of the Downtown bounded by 100th Avenue NE, 112th Avenue NE, NE 4th Street and NE 8th Street.
3. For purposes of applying the Amenity Incentive System, a level shall be considered the ground level so long as less than half of that ground level story height is located above or below the average finished grade of the adjacent public right-of-way or pedestrian connection. The two stories immediately above the ground level story and intended to activate the ground level pedestrian environment through demonstrated compliance with this section shall be considered an upper level.

**B. Development Standards.**

Each development located on a street designated pursuant to Section IV.E. of the Design Guidelines – Building/Sidewalk Relationships shall comply with the following standards within the project limit:

Right-of-Way Designation	Required Street Wall Conditions <sup>1</sup>
“A” Rights-of-Way	100% of street wall within project limit shall incorporate retail activities.
“B” Rights-of-Way	100% of street wall within project limit shall incorporate retail activities and service activities, at least 50% of which shall be retail activities.
“C” Rights-of-Way	Each street wall within the project limit shall incorporate some amount of service activities and commercial activities.
“D” Rights-of-Way	Each street wall within the project limit shall incorporate some amount of service activities and commercial activities.
“D/R” Rights-of-Way	At least 50% of the street wall within the project limit shall incorporate one or more of: service activities, commercial activities, landscape features, terraced planters, residential entry courtyards, or plazas.

<sup>1</sup>A project shall be considered to comply with the proportional requirements set forth above even though some portion of the street wall incorporates required access points supporting allowed uses in the project.

**C. Retail Activities Exempt from FAR.**

Each square foot of floor area of retail activities that satisfies the requirements of LUC 20.25A.020.B.7 shall be eligible for an exemption from calculating FAR for the proposal in the following proportion:

Location of Retail Activity	Amount Exempt from FAR: Amount of Retail Activity Provided (sq. ft.)
Ground level	1:1
Mid-Block Retail Connection at Ground Level	1:1
Upper Level Within the City Center District	1:1
Upper Level Outside the City Center District	0.5:1

**D. Mid-Block Retail Connection.**

The exemption allowed pursuant to LUC 20.25A.020.B.7. and this section may be earned for retail activities in compliance with subsection C above when located on a mid-block retail connection designed in accordance with the Guidelines and Standards for Mid-Block Retail Connections in Section IV.F of the Design Guidelines – Building/Sidewalk Relationships.

The pedestrian connections required in other sections of this Part 20.25A LUC are not required to be Mid-Block Retail Connections. However, any pedestrian connection meeting the criteria of this section shall be deemed to satisfy other applicable criteria for required pedestrian connections. (Ord. 5497, 11-17-03, § 7; Ord. 5091, 8-3-98, § 14; Ord. 4979, 3-17-97, § 13; Ord. 4973, 3-3-97, § 115; Ord. 4816, 12-4-95, § 215; Ord. 3309, 11-7-83, § 1)

**20.25A.120 Project phasing plan required.**

An applicant for a project with multiple buildings within a single project limit shall submit a project phasing plan for approval by the Director with the application for land use review. A phasing plan may also be submitted for a project approved prior to the effective date of this section at any time prior to expiration of the original approval pursuant to LUC 20.40.500. If submitted at the time of application for land use review, the decision on the phasing plan shall be merged with the decision on the underlying land use permit. If submitted after the land use permit or approval has been issued, the request for modification shall be processed pursuant to LUC 20.30F.175. The land use review will include review of all phases of the project. The phasing plan shall identify the discrete phases of intended project development, including delineation of the individual site areas for each phase and the sequence of development of each phase. The phasing plan may be approved by the Director if the following criteria are met:

- A. Each phase of the project independently complies with this Code within the limits of that phase; and
- B. The site areas of future phases within the single project limit will remain in current use or, if altered, be improved in the interim to create a site that complies with the requirements of this Code for permanent development with the completion of each phase.

The Director may require an assurance device pursuant to LUC 20.40.490 to ensure compliance with the conditions of land use approval and the phasing plan. (Ord. 5481, 10-20-03, § 1)

**20.25A.125 Vesting and expiration of vested status of land use permits and approvals – Downtown projects.****A. General.**

The vested status of a land use permit or approval granted pursuant to this Part 20.25A shall expire as provided pursuant to LUC 20.40.500, unless otherwise provided in this section.

**B. Extended Vesting Period.**

An applicant for a project with multiple buildings within a single project limit may request a modification to the vesting and expiration provisions of LUC 20.40.500, allowing for vesting of the land use permits and approvals for a period of up to 10 years from the date of issuance of the Design Review pursuant to LUC 20.30F.145.

1. Timing of Request for Modification. The request for modification may be submitted with the application for land use review, or at any time prior to expiration of a previously approved land use permit or approval pursuant to LUC 20.40.500. If submitted at the time of application for

land use review, the decision on the request for modification shall be merged with the decision on the underlying land use permit. If submitted after the land use permit or approval has been issued, the request for modification shall be processed pursuant to LUC 20.30F.175. In no event shall the vesting period exceed 10 years from the date of issuance of the Design Review pursuant to LUC 20.30F.145.

2. The Director may approve an increased vesting period; provided, that the project complies with an approved phasing plan pursuant to LUC 20.25A.120. The Director shall consider: (a) the site and size, (b) the size, scope and complexity of the project, and (c) construction and permitting activity in the vicinity of the project in determining the appropriate vesting period. The vested status of other required reviews, including but not limited to SEPA, Traffic Standards Code, Transportation Development Code, and building or other technical code review, is not controlled by this section.
3. The Director may require an assurance device pursuant to LUC 20.40.490 to ensure compliance with the conditions of land use approval and the phasing plan throughout the vested period. (Ord. 5481, 10-20-03, § 2)

### **20.25A.130 Downtown pedestrian bridges.**

#### **A. Where Permitted.**

Pedestrian bridges over the public right-of-way may be allowed at or near the mid-block in the following locations; provided, that no more than one bridge may be allowed on any side of a 600-foot superblock:

1. On NE 4th Street between Bellevue Way and 110th Avenue NE;
2. On NE 8th Street between Bellevue Way and 110th Avenue NE; and
3. On Bellevue Way between NE 4th Street and NE 8th Street.

Above-grade pedestrian crossings over the public right-of-way in existence at the time of adoption of the ordinance codified in this section shall not be considered nonconforming, and may be repaired or replaced in their current locations without compliance with this section.

#### **B. Location and Design Plan.**

The City Council shall review any Downtown Pedestrian Bridge Location and Design Plan, and may amend any approved Downtown Pedestrian Bridge Location and Design Plan, using the City Council Design Review Process, LUC 20.30F.116.

1. Prior to issuance of any permits for a proposed downtown pedestrian bridge, a Downtown Pedestrian Bridge Location and Design Plan must be submitted to and approved by the City Council.
2. A Downtown Pedestrian Bridge Location and Design Plan shall identify the location of the downtown pedestrian bridge, include a finding by Council that the proposal satisfies the public benefit test set forth in paragraph C of this section, be consistent with the development standards of paragraph D of this section, and be consistent with the Comprehensive Plan.
3. The Director shall ensure that the approved downtown pedestrian bridge is constructed consistent with the Design Plan. Modification to the location of the downtown pedestrian bridge, or to the articulated public benefits requires approval by the City Council pursuant to this section. Modifications to the design of the crossing that do not modify the location or public ben-

efits, and that are consistent with the intent of the Design Plan may be approved by the Director through the process set forth in Part 20.30F LUC.

4. The property owners shall record the approved Design Plan with the King County Division of Records and Elections and Bellevue City Clerk.

### **C. Public Benefit Required.**

The Council may approve, or approve with modifications, a proposed downtown pedestrian bridge if it finds that the bridge provides a public benefit. For the purposes of this section, a downtown pedestrian bridge shall be determined to provide a public benefit when it meets all of the following criteria:

1. The bridge improves pedestrian mobility; and
2. The bridge does not detract from street level activity; and
3. The bridge functions as part of the public realm.

### **D. Development Standards.**

Each proposed downtown pedestrian bridge must be developed in compliance with the following standards:

1. The bridge must be open from at least 6:00 a.m. to midnight, or during the hours of operation of adjacent uses, whichever is greater. Signs shall be posted in clear view stating that the pedestrian bridge is open to the public during these hours;
2. The bridge connects upper-level publicly accessible space to upper-level publicly accessible space and provides a graceful and proximate connection between the sidewalk and bridge level that is visible and accessible from the sidewalk. The vertical connection should occur within 50 feet of the sidewalk;
3. Vertical circulation elements must be designed to indicate the bridge is a clear path for crossing the public right-of-way;
4. Directional signage shall identify circulation routes for all users;
5. Structures connected by the bridge shall draw pedestrians back to the sidewalk with retail activities, as defined by the Building/Sidewalk Design Guidelines, at the ground level immediately adjacent to both sides of the pedestrian bridge;
6. It is preferred that the bridge remain unenclosed on the sides, but allow enclosure or partial enclosure if the applicant demonstrates it is necessary for weather protection;
7. Visual access shall be provided from the sidewalk and street into the bridge;
8. Bridge may not diverge from a perpendicular angle to the right-of-way by more than 30 degrees;
9. The interior width of the bridge, measured from inside face to inside face shall be no less than 10 feet and no more than 14 feet;
10. Bridge shall be located at the second building level, with a minimum clearance of 16 feet above the grade of the public right-of-way;

11. Impacts on view corridors, as described in LUC 20.25A.100 and 20.25A.110, shall be minimized;
12. Impacts on the function of City infrastructure, including but not limited to utilities, lighting, traffic signals, etc., shall be avoided or mitigated;
13. Lighting shall be consistent with public safety standards;
14. Signage on the exterior of the bridge, or on the interior of the bridge that is visible from a public sidewalk or street is not permitted;
15. Bridge must be architecturally distinct from the structures that it connects; and
16. Bridge must exhibit exemplary artistic or architectural qualities.

**E. Public Access – Legal Agreement.**

1. Owners of property that is used for pedestrian bridge circulation and access between the bridge and public sidewalk shall execute a legal agreement providing that such property is subject to a nonexclusive right of pedestrian use and access by the public during hours of bridge operation.
2. The agreement shall provide that the public right for pedestrian use shall be enforceable by the City of Bellevue, and the City shall have full rights of access for the pedestrian bridge and associated circulation routes for purposes of enforcing the rights of the public under this agreement.
3. Owners of property subject to this legal agreement will maintain the pedestrian access route and may adopt reasonable rules and regulations for the use of this space; provided, that the rules and regulations are not in conflict with the right of pedestrian use and access and consistent with this section.
4. The agreement shall be recorded with the King County Division of Records and Elections and Bellevue City Clerk. (Ord. 5496, 11-17-03, § 2)





# Design Guidelines Building/Sidewalk Relationships

CENTRAL BUSINESS DISTRICT



City of  
Bellevue

(Revised 12/03)

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The text of this document was adopted by the City Council through Ordinance Number 3309 on November 7, 1983. These guidelines have been incorporated by reference into the Land Use Code as Section 20.25A.115. The ordinance cited above has included a number of other specific changes to the code.

# I. Purpose

The Land Use Code adopted in 1981 established an entirely new set of regulations for guiding the growth and development of Bellevue's Central Business District. New districts having varying levels of intensity were devised. Parking requirements were reduced. Particular emphasis was placed upon the development and improvement of public transportation. Major attention was focused upon pedestrian movement, with associated facilities and amenities. Code provisions reflect a conscious effort to intensify new development by responding to the marketplace. The Code also ensures a high level of design quality, liveliness and convenience for people who work, shop and live in the downtown.

This document is intended to supplement and expand upon the general design criteria already found in the Land Use Code. This document sets forth more specific standards and guidance according to which individual projects may be evaluated, with respect to relationships between buildings and sidewalks. These guidelines will serve three groups of people. First, the Planning Department will use these guidelines as a part of the review for all proposed projects in the CBD. They will be referenced in the section of the Land Use Code governing downtown development. Second, the guidelines should assist developers and their architects in the design of projects by providing explicit directions. Finally, these guidelines will benefit the public by providing a pedestrian-oriented environment in downtown Bellevue.

These design guidelines intentionally emphasize qualitative rather than quantitative measures. They describe, in clear terms, those features that are to be addressed in the design of projects. It is evident that each guideline could generate numerous solutions. Varied and imaginative designs are certainly encouraged. Photographic examples have been included in this document to provide illustrations of the intent of various terms.

There are several other design guidelines that address particular areas or facilities within the CBD. Provisions governing these subjects are as follows:

1. Pedestrian Connections Through Superblocks  
Land Use Code 20.25A.100.E.4.
2. Pedestrian Corridor Design Guidelines  
Resolution No. 3946.
3. Old Bellevue District  
Land Use Code 20.25A.070.

Applicants should consult all portions of the Land Use Code, particularly 20.25A, to identify other applicable requirements.

## II. Urban Design Goals

The Central Business District Subarea Plan contains numerous policies relating to Urban Design, some general in nature and some associated with particular areas. Several of the policies, combined, provide the framework for this document. These policies are summarized into five objectives, which are highlighted below, along with a brief discussion of each.

### **The Physical Character of the Entire Downtown is to be Enhanced.**

Downtown Bellevue, for most of the thirty years of its existence, has been a sprawling collection of low rise buildings separated by vast parking lots. Early planning concepts led to the establishment of large superblocks separated by arterial streets. Accommodating the automobile was obviously of paramount concern. The result clearly was a setting that provided very little in the way of protection, convenience or interest for people on foot. Development and redevelopment occurring under the new Land Use Code will be radically different: surface parking will be greatly diminished, high intensity uses will be concentrated into an area suitable for walking, and large numbers of employees will be located in the downtown without having arrived by car. In short, public policies and the marketplace will shape an entirely new downtown. It is imperative to ensure that the evolving physical character is enhanced through deliberate and specific design decisions. (See Policy 21.V.3.255)

### **A Pedestrian Environment is to be Created, with Activity, Enclosure and Protection.**

Downtown should be a rich, exciting, and human setting for a wide variety of activities. A concentration of people in a relatively compact area results in greater demands for goods and services, entertainment and culture. The network of streets and sidewalks, of course, serves to link together different uses: offices, shops, restaurants, residences and so forth. But, at the same time, the sidewalks themselves must be appealing and lively places. To be lively and interesting, sidewalks must be lined with active



View of Downtown Bellevue from  
Meydenbauer Bay



Old Bellevue

commercial and retail uses and must incorporate features such as weather protection, vegetation, street furniture and special paving treatment. The necessary attributes of activity, enclosure and protection are achieved only when buildings 'reach out' and 'embrace' the sidewalk both through overall physical form as well as through small scale details. (See Policy 21.V.3.260)

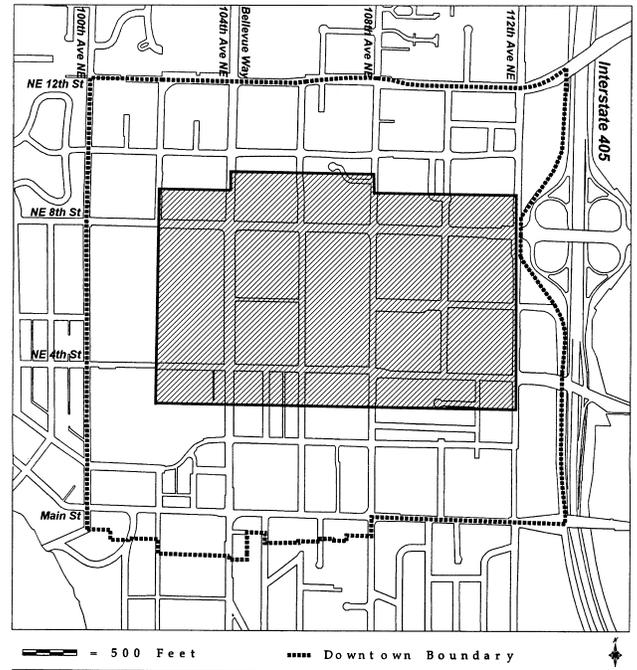
**Buildings Within and Around the Core\* are to Contribute to Pedestrian Orientation.**

If the pedestrian experience is to be interesting and diverse, it is necessary to ensure that activities occurring within the street level of a building are connected to adjacent sidewalks. Areas that appear to be empty of human activities will not attract people.

The design of a building can contribute to the sense of activity if it provides for visual and physical access directly from the public sidewalks. As development occurs over time, street corridors will assume a steadily increasing degree of continuity and cohesiveness with respect to pedestrian orientation. (See Policies 21.V.3.280, 21.V.3.300, 21.V.3.315 and 21.V.3.320)

**Streets Within the Core\* are to Receive Particular Pedestrian Emphasis.**

The core area is designated as that area of the downtown which is to be most intensely developed. Automobile oriented uses are generally discouraged but high rise office uses, with associated service and retail activity are encouraged through both height and floor area incentive provisions. Special design guidelines have previously been adopted to guide the development of the Major Pedestrian Corridor along the alignment of N.E. 6th Street and a 'primary path' of pedestrian movement. Other streets within the core will need to function as important connections between the corridor and surrounding blocks. Together with other 'secondary paths', such as mid-block locations, all core area streets must have pedestrian attracting features. Those nearest the Pedestrian Corridor are especially important. (See Policies 21.V.3.495 and 21.V.3.500)



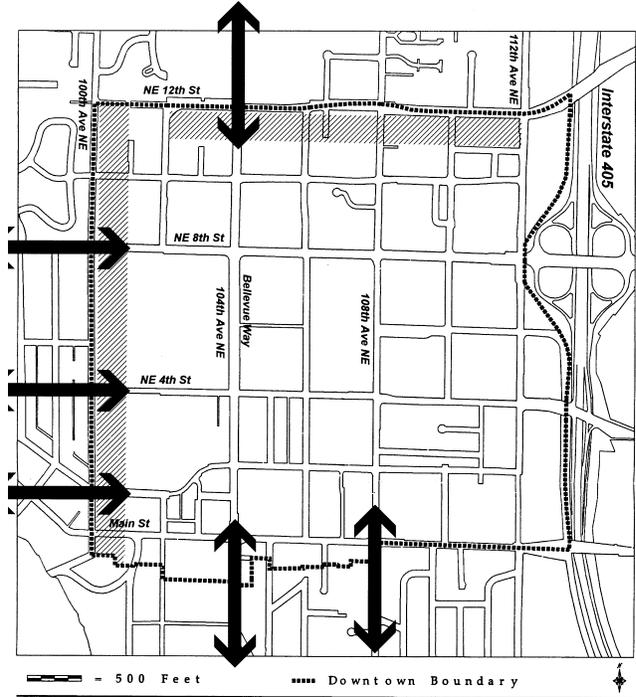
Core Design District  
(See LUC 20.25A.100A for detailed boundary).



108th Avenue N.E. south of N.E. 8th Street

## Pedestrian Connections are to be Provided to Adjacent Neighborhoods

The Central Business District cannot be isolated from its surroundings. Safe, convenient and attractive connections to nearby residential areas are important. Certain streets, by virtue of their extension into these areas, can provide this necessary connective role. As such, they need to also have special pedestrian emphasis in order to serve as 'entrances'. By the same token, streets which separate the downtown from surrounding neighborhoods should receive attention with respect to ways of blending commercial and residential development. Commercial development on the perimeter of the downtown should incorporate design elements that will provide a sensitive transition to surrounding areas. This reflects policies and criteria already established in both the CBD Subarea Plan and the Land Use Code. (See Policy 21.V.3.225)



Neighborhood Relationship Map

### III. Major Issues

Observation of other downtown areas and a comparison with the Central Business District of Bellevue reveals a number of major design issues. The following list of issues provides a general framework for the development of specific guidelines.

#### Concentration versus Openness

An extremely intense, unrelieved concentration of buildings can be forbidding, unpleasant and devoid of desirable daylight. Obviously, some amount of open space is valuable. However, open space per se is not necessarily conducive to a lively downtown setting. Used excessively in front of every building or at every corner, open space can actually detract from the sense of activity along the street. If used selectively and only at special points, and if designed to encourage use (seating, planing, sunny spots, etc.), open space can serve an important role in the downtown setting.

#### Enclosure

Until the adoption of the new Code, downtown buildings had to be located behind certain specified setback lines. This resulted in the development of a major 'gap' between the street/sidewalk corridor and building facades. It has also contributed to the 'illusion' of overly wide streets when, in fact, most streets have widths typically found in many downtowns. Although the present Code does not require setbacks, a sort of 'setback attitude' persists. In order for sidewalks and streets to appear urbane, active and lively, buildings must enclose and contain space within the street corridor.

#### Distinguishing the Ground Level

The treatment of the ground level of buildings is most crucial with respect to pedestrian experience. The ground level is, after all, closest to view and touch and provides for entrances. Many buildings recognize this already, with special details, materials, finishes, signage or display windows. Other buildings, unfortunately, ignore the relationship to the sidewalk with no demarcation at all. By providing special distinguishing characteristics at the ground level, such as retail space and arcade, artwork or architectural



Bellevue Square



102nd Avenue N.E. at N.E. 10th Street



Main Street in Old Bellevue

recesses, the continuity of pedestrian experience is reinforced. Ground floor activities and treatment can serve as a means of tying together buildings of different size, shape and use.

### Internal/External Relationships

The downtown is a place of human activity and interaction concentrated in a relatively small area. When such activities are invisible or inaccessible, diversity is lost. What makes any downtown interesting is the wide variety of activities that it offers. If the downtown is to appear lively at all times, buildings must be oriented to the outside.

### A Pathway System which Protects the Pedestrian

A downtown area is not conducive to movement on foot. When sidewalks are narrow or nonexistent, the absence of parked cars along the curb possibly makes pedestrians feel exposed and unsafe. The Land Use Code now requires 12- to 16-foot sidewalks with street trees which should improve this situation. The emerging downtown must be perceived as safe, convenient and attractive by people who will inevitably be on foot in increasing numbers. Building designs which contribute to the sense of protection, with overhangs, canopies, arcades, recessed entrances, and other devices can contribute to a protective pathway system. Street furniture along the curb line, special paving textures and graphics can also assist in enhancing the pedestrian experience.

### Streetscape and Landscape

Plants certainly can enliven an otherwise harsh and hard-surfaced setting. Plants can also conceal or diminish the view of utilitarian structures. However, planting should generally not be used to separate commercial and retail activities from the sidewalk, as this simply diminishes the degree of liveliness and connection. Dense concentrations of planting are suitable only for very limited purposes such as major open spaces or reducing the visual impacts of parking facilities.



Restaurant, City Center



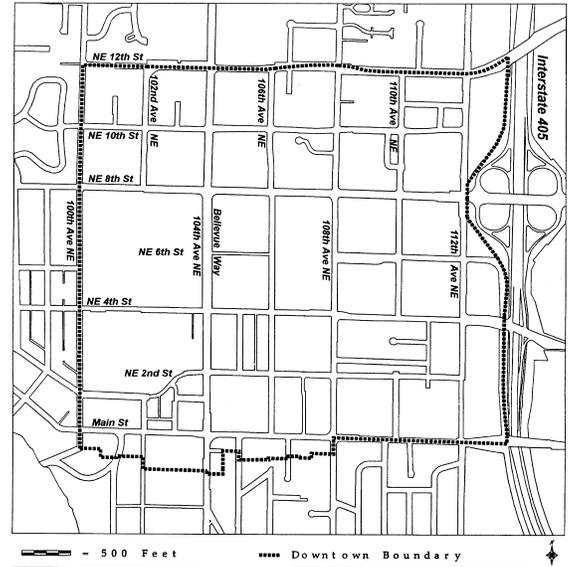
One Bellevue Center, 108th Avenue N.E. at N.E. 4th Street



Segment of the Major Pedestrian Corridor, N.E. 6th Street near 108th Avenue N.E.

## Superblock/Ownership Pattern

The CBD is composed, generally, of square blocks, 600 feet on each side. In addition, many parcels are relatively large, and often several adjoining large parcels are in common ownership. This condition presents some unusual development opportunities but also poses problems such as perceived walking distances, emergency access to interior areas, and the potential for internally-oriented 'fortress-like' development.



## Unifying Elements within Variety

Every new development project will combine a unique set of elements in an effort to meet a particular need in the marketplace at any given time. In addition, preferences of the developer and architect regarding a desired image will be evident. These factors tend to produce physical forms which may or may not be compatible with respect to color, materials, massing or height. For the most part, this is acceptable, even desirable, as a totally uniform environment could be monotonous and boring. However, the use of compatible and similar techniques of treating the street level edges of projects can serve as a strong 'glue' which binds together otherwise distinct buildings. Special treatment of the sidewalk, together with the ground level, can serve an effective unifying function within an overall setting of variety.



Transit Center and Major Pedestrian Corridor

# IV. Guidelines

## A. Applicability

The guidelines contained within this document apply to any development project which abuts a public right-of-way designated pursuant to Paragraph E.

## B. Master Plan Required:

If the applicant proposed to phase development abutting a public right-of-way designated pursuant to Paragraph E, the applicant must submit a conceptual master plan of the entire street frontage indicating generally how these guidelines will be met, and including:

1. Conceptual drawings of structures,
2. General types and location of uses,
3. Conceptual drawings of sidewalk level of building faces, and
4. Estimated development phasing schedule.

Any space which is indicated for a proposed future phase shall have a minimum depth of 100 feet.

## C. Development Review Submittals:

The applicant must submit the following with an application for development abutting a right-of-way designated pursuant to Paragraph E.

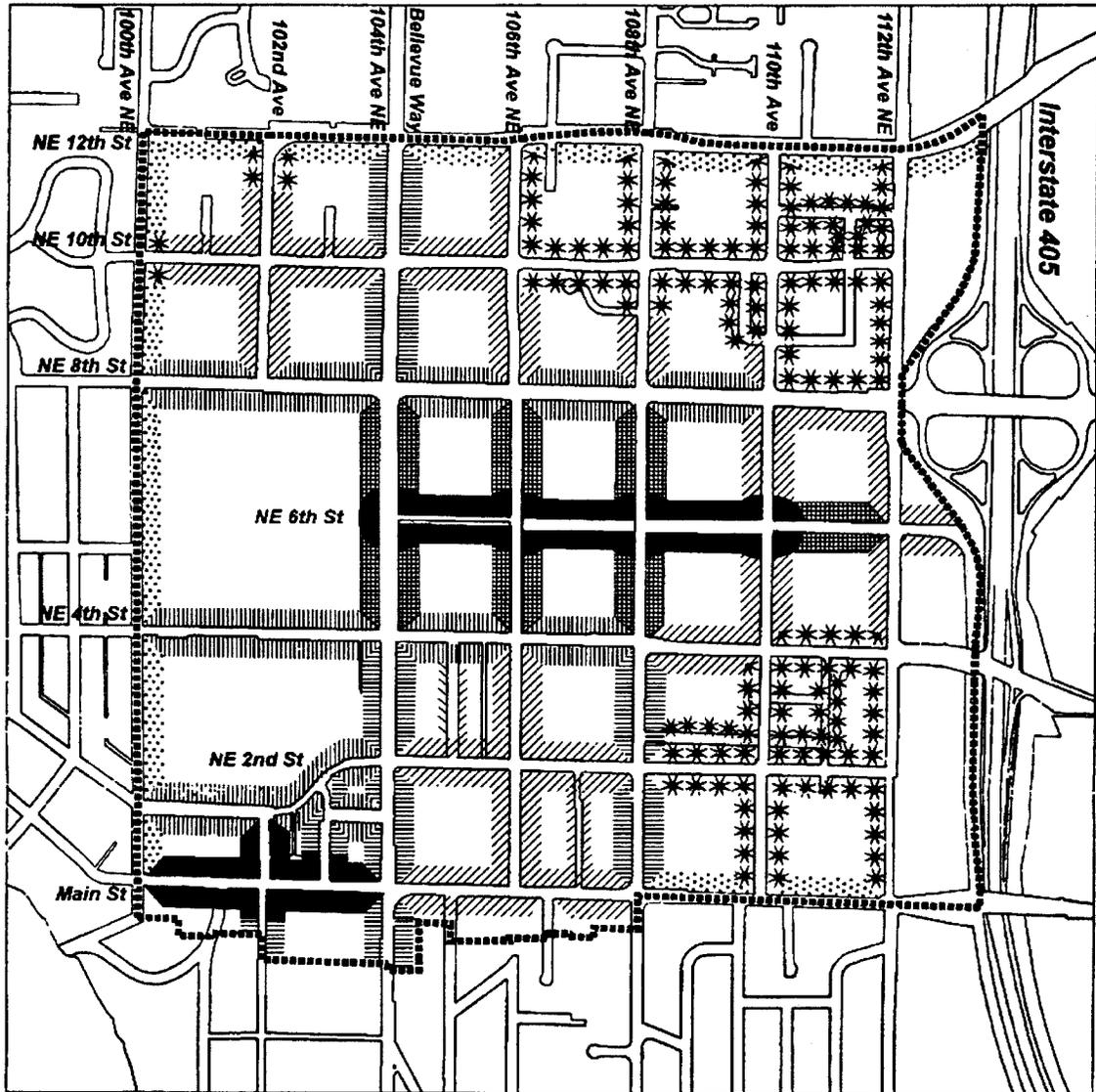
1. A site plan including:
  - a. Location and dimensions of the subject property, and if applicable, the project limit,
  - b. Designation of all rights-of-way pursuant to Paragraph E which abut the subject property.
2. A verbal description of how each applicable design guideline will be met,
3. A graphic depiction of how each applicable design criteria will be met, including elevations of all right-of-way frontage development, and
4. Any information requested by the Planning Director in order to evaluate the development according to the applicable design guidelines.

## D. Modification of Guidelines

The Planning Commission will review these Guidelines periodically to assure that they are producing the desired objectives in a reasonable manner and will report to the City Council. (Ordinance 3380, 1-25-88, Section 1)

## E. Designation of Rights-of-Way

This document establishes a system of designating rights-of-way for the purpose of applying varying design guidelines. The intent is to create a hierarchy of rights-of-way, with the greatest attention being given to those within and around the Core Design District. The hierarchy reflects different intensities of pedestrian activity in recognition of policies in the CBD subarea plan. The accompanying map denotes the designation of each right-of-way. (Ordinance 5582, 2-7-05, Section 17; Ordinance 5100, 10-19-98, Section 7)



— 500 Feet

----- Downtown Boundary



### Right-of-Way Designations

-  A
-  B
-  C
-  D
-  D/R
-  E

Note: In the Downtown-Old Bellevue District, the "A" right-of-way designation extends south of Main Street on both sides of 102nd Avenue SE for a distance of 100 feet and extends north of Main Street on both sides of 103rd Avenue NE for a distance of 100 feet.

## **F. Edge\* Conditions for Each Right-of-Way**

The following guidelines describe edge conditions to be provided along each type of right-of-way. In each case, the conditions listed must be provided, except where vehicular access points are located.

\* 'Edge': The street level portion of a development which fronts upon a public right-of-way. For purposes of applying these guidelines, the Major Pedestrian Corridor is to be considered a right-of-way.

## 'A' Rights-of-Way

### Intent:

Rights-of-way designated 'A' shall have the highest orientation to pedestrians. This shall be achieved by emphasizing the designer relationship between the first level of the structure and the horizontal space between the structure and the curb line. This relationship should emphasize to the greatest extent possible, both the physical and visual access into and from the structure, as well as the amenities and features of the outside pedestrian space. In order to achieve the intended level of vitality, design diversity, and people activity on an 'A' right-of-way, retailing or marketing activities shall be provided for in the design.

### Guidelines:

1. Street level edges of the entire project limit shall incorporate retail activities.
2. The following characteristics shall be incorporated into the design of the structure:

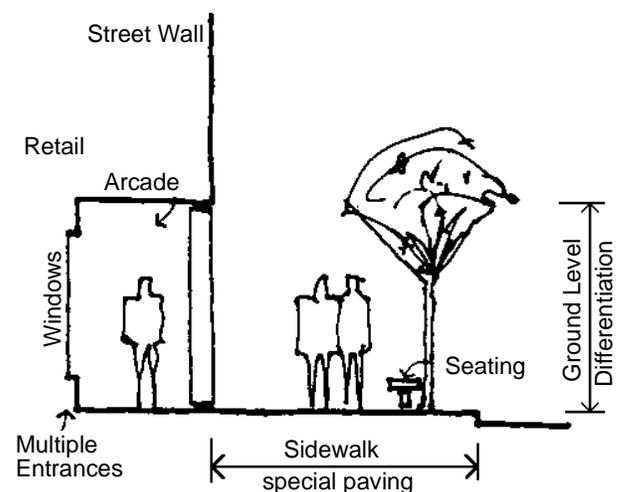
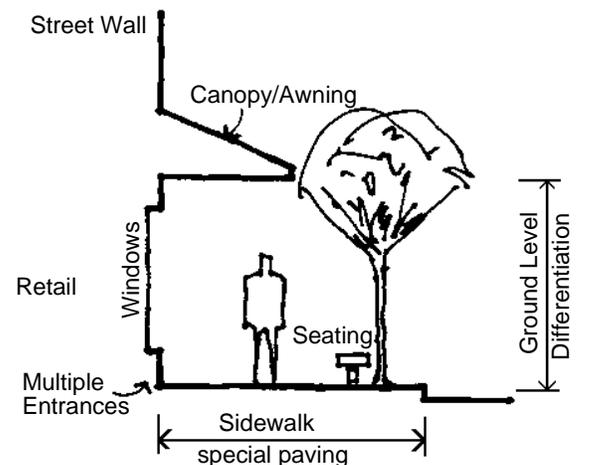
- Windows providing visual access
- Street walls
- Multiple entrances
- Differentiation of ground level
- Canopies, awnings or arcades

3. The following characteristics should be incorporated into the design of the sidewalk:

- Special paving treatment
- Seating

4. These Guidelines for 'building/sidewalk relationships' are to be used in conjunction with other guidelines adopted by the City. In the event the guidelines conflict with more specific guidelines adopted for Old Bellevue, the Major Pedestrian Corridor, or the Major Public Open Spaces, the more specific guidelines, now or as amended in the future, take precedence.

### Examples:



## 'B' Rights-of-Way

### Intent:

Rights-of-way designated 'B' shall have moderate to heavy orientation to pedestrians. This shall be achieved by developing the design so that there is a close relationship between exterior and interior activities with respect to both physical and visual access. Design attention should be given to sidewalk related activities and amenities.

'B' rights-of-way are to provide a diverse and active connection between the 'retailing' 'A' rights-of-way and other CBD rights-of-way.

### Guidelines:

1. Street level edges of the entire project limit shall incorporate retail activities and service activities, with at least half of the frontage being in the retail category.

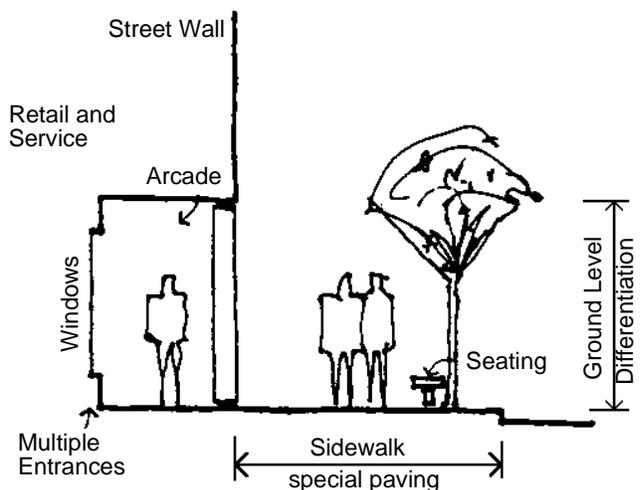
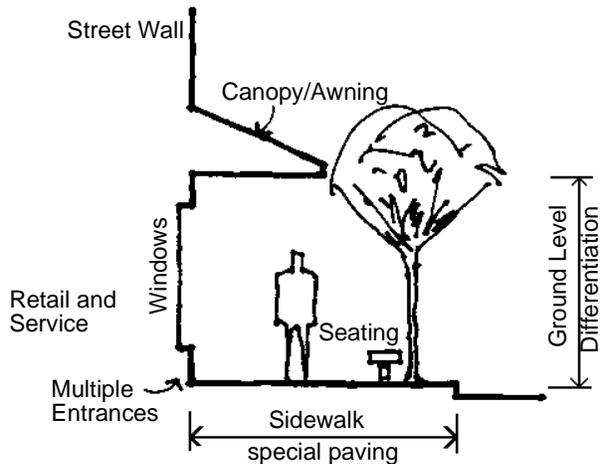
2. The following characteristics shall be incorporated into the design of the structure:

- Windows providing visual access
- Street walls
- Multiple entrances
- Differentiation of ground level
- Canopies, awnings or arcades

3. The following characteristics should be incorporated into the design of the sidewalk:

- Special paving treatment
- Seating

### Examples:



## 'C' Rights-of-Way

### Intent:

Rights-of-way designated 'C' shall have moderate orientation to pedestrians. This shall be achieved by designing some relationship between exterior and interior activities with respect to visual access. Design attention should be given to sidewalk related activities and amenities.

'C' rights-of-way are to provide a major pedestrian connection between the core area and residential areas surrounding the CBD.

### Guidelines:

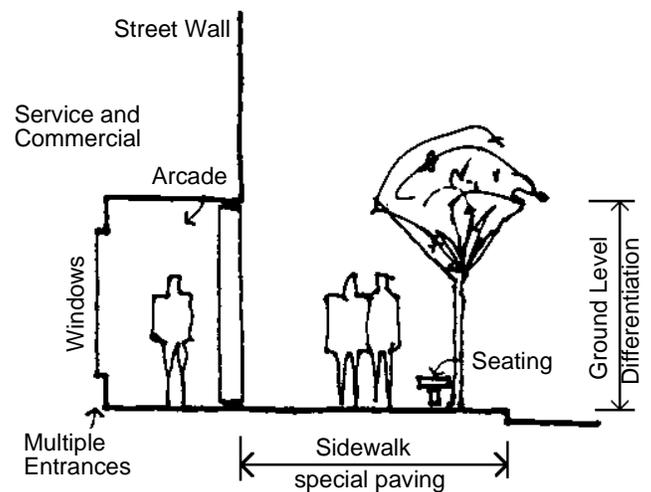
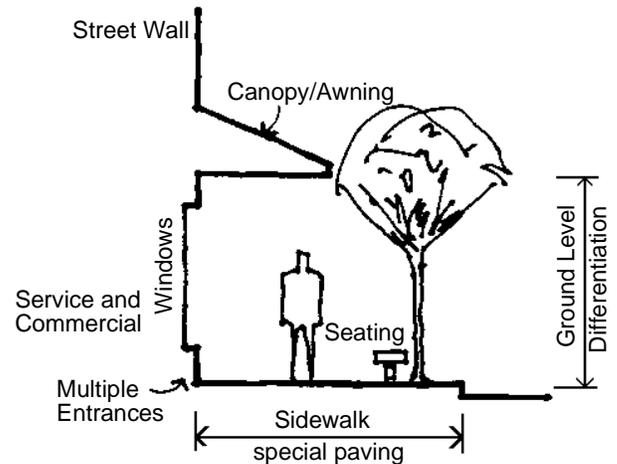
1. Street level edges of the entire project limit shall incorporate some amount of service and commercial activities.
2. The following characteristics shall be incorporated into the design of the structure:

Windows providing visual access  
Street walls  
Multiple entrances  
Differentiation of ground level  
Canopies, awnings or arcades

3. The following characteristics should be incorporated into the design of the sidewalk:

Special paving treatment  
Seating

### Examples:



## 'D' Rights-of-Way

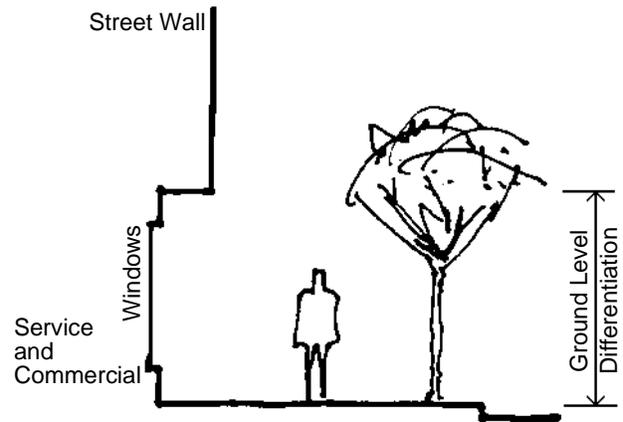
### Intent:

Rights-of-way designated 'D' shall have low to moderate orientation to pedestrians. This shall be achieved by designing some relationship between exterior and interior activities with respect to visual access. Design attention should be given to sidewalk related activities and amenities.

### Guidelines:

1. Street level edges of the entire project limit shall incorporate some amount of service and commercial activities.
  - Windows providing visual access
  - Street walls
  - Differentiation of ground level
2. The following characteristics shall be incorporated into the design of the structure:

### Examples:



## 'D/R' Rights-of-Way

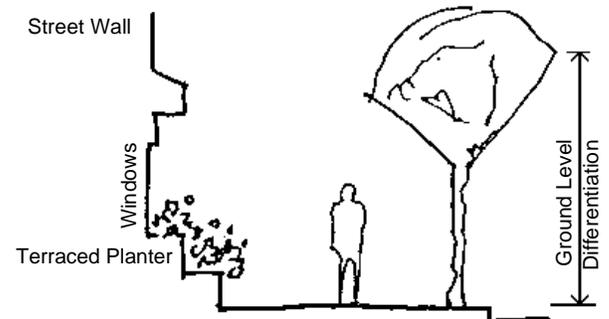
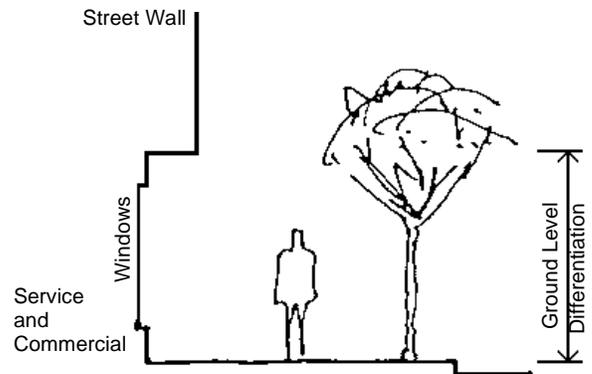
### Intent:

Rights-of-way designated 'D/R' shall have low to moderate orientation to pedestrians and shall complement residential uses. This shall be achieved by designing some relationship between exterior and interior activities with respect to visual access, and by incorporating landscape features that soften the urban edge. Design attention should be given to amenities that complement these areas' residential character and moderate the urban environment, while providing attractive visual access for pedestrians and other passersby.

### Guidelines:

1. At least 50 percent of the street level edges of the entire project limit shall incorporate service and commercial activities, landscape features, terraced planters, residential entry courtyards, plazas, or a combination of these features.
2. The following characteristics shall be incorporated into the design of the structure:
  - Windows at ground level
  - Street walls
  - Differentiation of ground level

### Examples:



## 'E' Rights-of-Way

### Intent:

Rights-of-way designated 'E' shall have low orientation to pedestrians. Such rights-of-way are intended to provide a visual buffer between the downtown and surrounding residential neighborhoods.

### Guidelines:

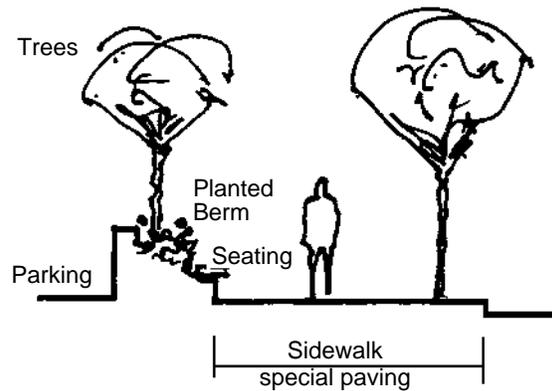
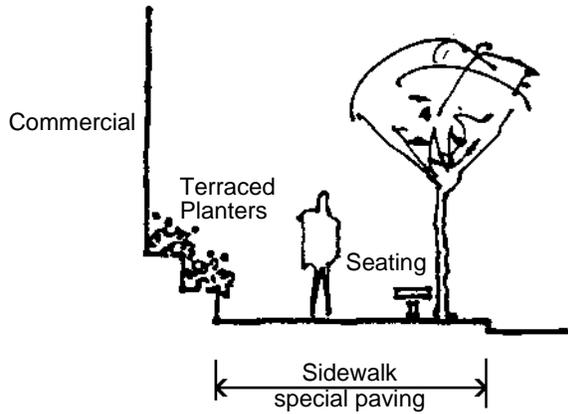
1. Street level edges of the entire project limit may incorporate commercial activities but shall also incorporate the following characteristics:

Trees, hedges or terraced planters  
or  
Planted berms

2. The following characteristics shall be incorporated into the design of the structure:

Special paving treatment  
Seating

### Examples:



## **‘MBRC’ Mid-Block Retail Connections**

### **Intent:**

A mid-block retail connection (MBRC) faced with retail activity exempt from FAR calculation shall have a high orientation to pedestrians. This is achieved by emphasizing the relationship between the vertical street wall and the ground plane devoted to mid-block access and the public right-of-way. This relationship should emphasize to the greatest extent possible, both physical and visual access into and from the structure at frequent intervals, as well as the amenities and features of the outside pedestrian space. In order to achieve the intended level of vitality, design diversity, and pedestrian activity on a mid-block retail connection, retail activities as defined herein shall be provided for in the design.

### **Guidelines:**

1. Materials and design elements such as paving, lighting, landscaping, and signage should incorporate design elements of the adjacent right-of-way to identify it as part of the public realm.
2. The MBRC may be covered but may not be enclosed.
3. Access from the public right-of-way should be encouraged and enhanced by multiple clear points of entry that identify the MBRC as a public space. Access through the site should form a clear circulation logic with the street grid.
4. Wayfinding, signage, symbols and lighting should identify the MBRC as a public space.
5. Design of the ground level and upper level retail should relate to the MBRC and be distinct from the rest of the building. This can be achieved through the use of common architectural style, building materials, articulation, and color.
6. Variation shall be incorporated into the design by including dimensional and level changes at both the ground plain and building walls.
7. Pedestrian-oriented lighting should be provided that is compatible with the landscape design, improves safety and minimizes glare.

Design should be high quality, and materials should be durable and convey a sense of permanence.

8. Landscaping shall be used to animate and soften the space. The use of art and water is also encouraged.
9. MBRC design should not incorporate loading, refuse handling, parking, and other building and site service uses at the ground level facade, though such activities may be conducted in an MBRC when reasonable alternatives are not available. Operational procedures should encourage the above-referenced activities after normal business hours.
10. Provide complete project design for all phases within a project limit to ensure coordinated design and construction across multiple phases.

### **Standards:**

1. At least one entire side of the MBRC shall comply with guidelines 2 and 3 for ‘A’ rights-of-way.
2. Minimum dimension for an MBRC double loaded with retail should be 25 feet wide exclusive of drive lane widths. An MBRC with single loaded retail should be a minimum of 12 feet wide exclusive of drive lane widths.
3. The MBRC should be open from 6:00 a.m. to midnight or during normal business hours, whichever is longer. Signs should be posted in clear view stating the MBRC is open to the public during these hours.
4. Each tenant space should have an exterior entrance.

## **“ULR” Upper Level Retail**

### **Intent:**

Upper level retail is intended to activate the ground level pedestrian environment. This is accomplished through extensive visual access to the upper level from the exterior, convenient and frequent access from the street or MBRC, clear line of sight from grade and visibility of ongoing activity within the upper level retail. Upper level retail should be designed and managed so as to draw the attention and interest of the pedestrian to the upper level and to increase opportunities for interaction and movement between the ground and upper levels. In order to achieve the intended level of vitality, design diversity, and people activity at upper level retail the following characteristics should be provided in the design.

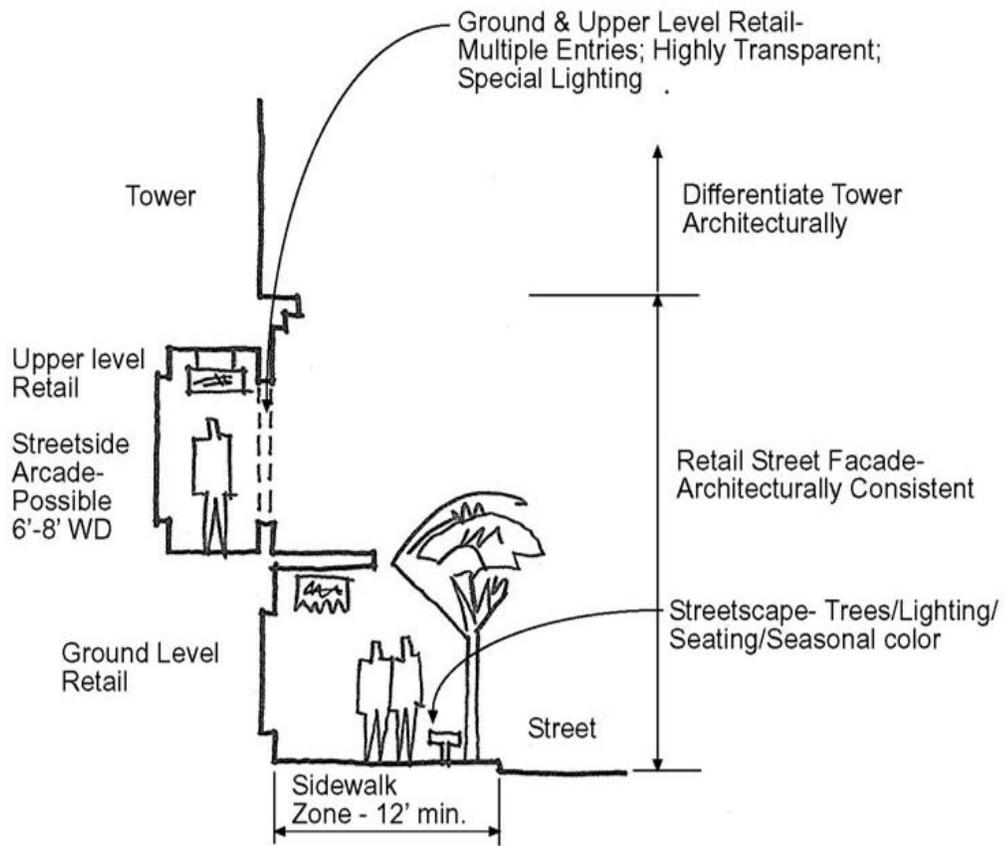
### **Guidelines:**

1. Architectural treatment of the upper level retail space should read as part of the ground level and be distinct from the architectural treatment of the building above.
2. Extensive visual access into the upper level retail space should be available from the sidewalk or the MBRC with frequent clear lines of sight from grade.
3. Lighting and signage should be used to enliven and draw attention to upper level retail.
4. Access to upper level retail is permitted via an upper level arcade or balcony, or directly through ground level retail for a multilevel single tenant.

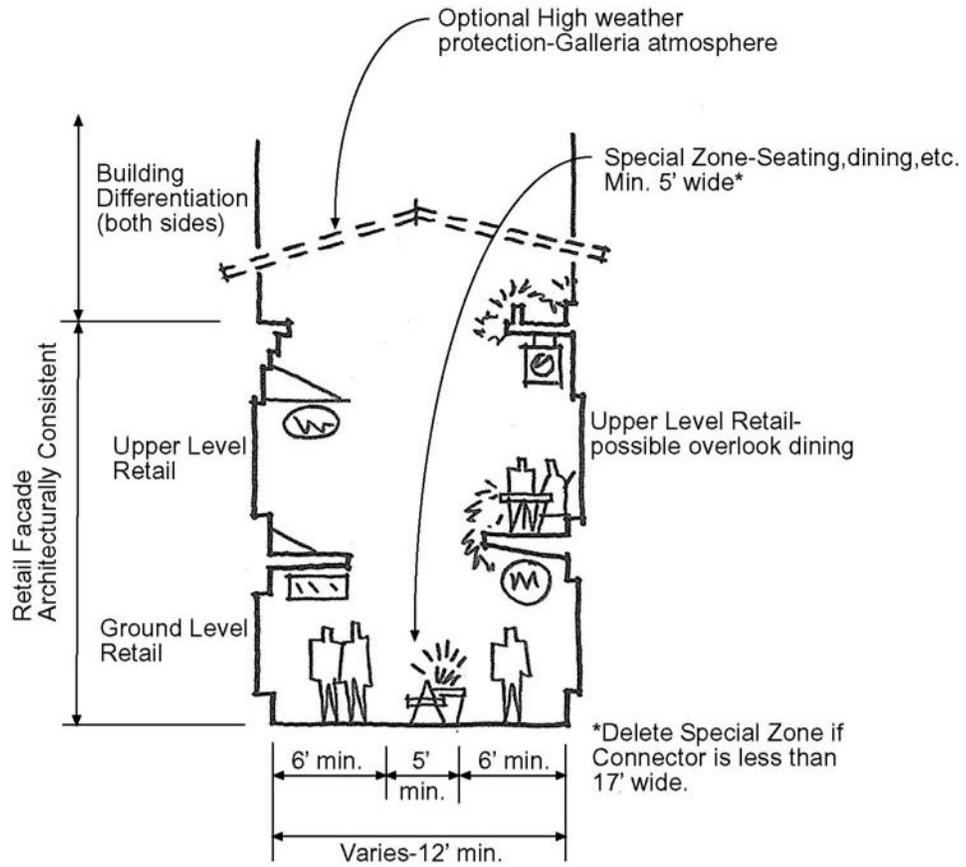
### **Standards:**

1. Points of physical vertical access between the ground level and upper levels should be located no more than 150 feet apart to facilitate frequent pedestrian access to upper level retail.
2. Each tenant space should have an exterior entrance.
3. Floor area and building facade directly below upper level retail must comply with guidelines for “A” rights-of-way.

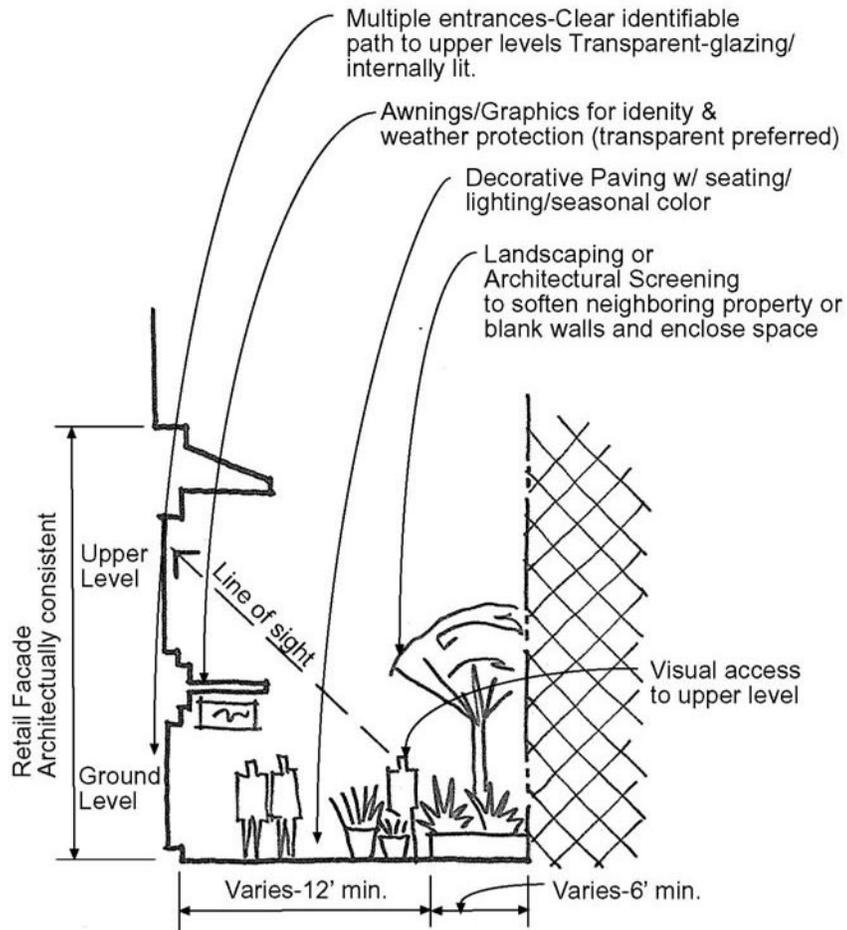
4. Visual access should not be impaired by small, enclosed display windows, window coverings and tinted or reflective glazing.



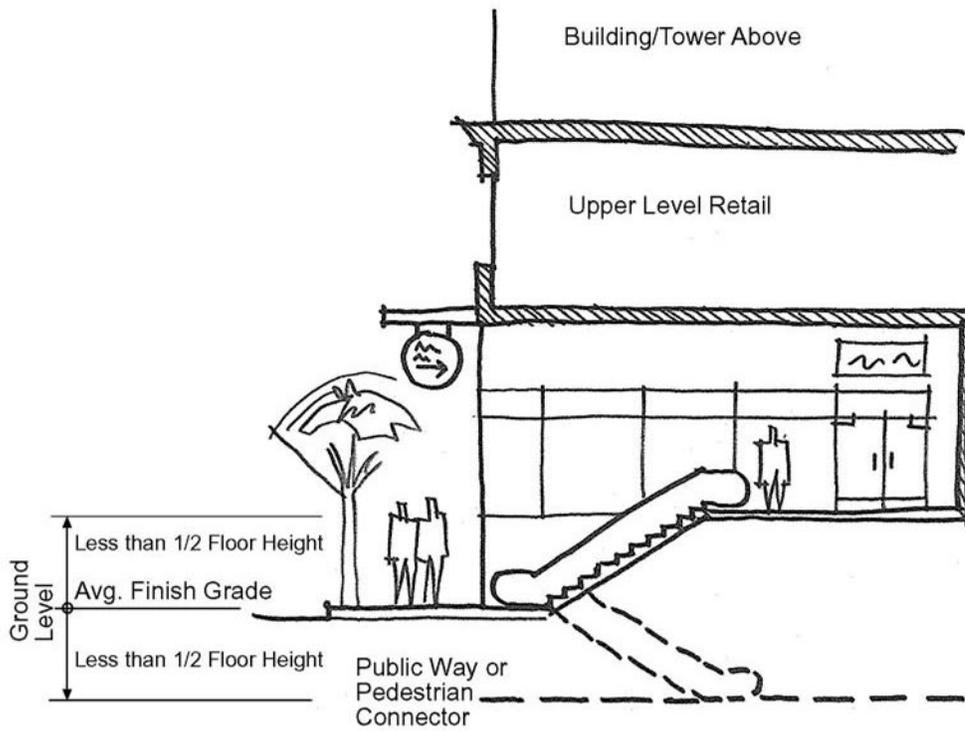
R.O.W. Condition



2 Sided Midblock  
Retail Connection



**One-sided Mid-block  
Retail Connection**



Ground Level Defined

## **G. Definitions of Edge Conditions**

### **Retail Activities**

For purposes of these guidelines, 'Retail Activities' include any 'pedestrian-oriented use' such as retail stores, groceries, drug stores, shoe repair shops, cleaning establishments, floral shops, beauty shops, barber shops, department stores, apparel shops, art galleries, travel agencies, restaurants and theaters.

### **Service Activities**

For purposes of these guidelines, 'Service Activities' include all retail activities; finance, insurance and real estate services and business services. This type of activity is intended to be pedestrian-attracting in nature.

### **Commercial Activities**

For purposes of these guidelines, 'Commercial Uses' include all retail activities; service activities, professional services, general office, library, museum, recreation activities and hotels.

### **Windows, Providing Visual Access**

Such windows are transparent enough to permit the view of activities within a building from nearby streets and sidewalks. Reflective or very dark tinted glass does not meet this purpose. In order to provide visual access at the street level, the amount of glass must be extensive in both vertical and horizontal dimensions.

### **Street Walls**

A 'street wall' is a building wall which generally abuts the sidewalk, although there may be occasional setbacks and recesses for the purpose of plazas and open space. The street wall helps define and enclose the street corridor, creating a sense of activity, intensity and spatial containment. Street walls can incorporate arcades at the sidewalk level, with habitable space above.

### **Multiple Entrances**

Pedestrian entrances spaced at frequent intervals along the sidewalk contribute to variety and intensity. Providing numerous points of physical access reduces the possibility of long, monotonous facades. Combined with display windows, entrance points can greatly enhance pedestrian environment.

### **Differentiation of Ground Level**

A traditional method of breaking down the mass of a large building is to provide for a distinctly different architectural treatment at the ground or lower levels. This special treatment can take many forms such as a recess, arcade, change in material, color or texture, higher structural bays, overhangs, projecting display windows, and intermediate cornice details.

## **Canopies, Awnings or Arcades**

In the Pacific Northwest, protection from rainfall is a very important consideration. Canopies, awnings, and arcades can be used on pedestrian-oriented streets to add a certain measure of comfort. Such devices can also help define an enclosed sidewalk environment by providing a sort of 'ceiling' to the linear space.

## **Special Paving Treatment**

The quality of the streetscape 'floor' can enhance the experience of people on foot. Special scoring patterns, colors, pavers, bollards, and bands at regular intervals can provide interest. Areas near intersections should receive the greatest attention with respect to special treatment, since pedestrian activity is concentrated near crosswalks.

## **Seating**

People on foot enjoy opportunities for pausing and resting, watching other people and activities, or eating lunch. Sitting spaces are particularly sought after in one core area even where there is traffic, because the core is where there is the greatest concentration of people on foot. Seating can consist of a variety of forms, including individual units, benches and ledges.

## **Trees, Hedges or Terrace Planting**

Certain types of dense planting can help soften or screen particular conditions such as parking facilities, blank walls, vehicular service areas. The principal value of such planting is the provision of visual relief; little contribution is made to pedestrian activity and intensity. Therefore, application should be limited and should be reserved chiefly for streets which are peripheral to the downtown or which have a minor role in the overall pedestrian system.

## **Planted Berms**

As with other planting, berms are chiefly valuable for their ability to screen or soften views of otherwise unappealing areas. They contribute very little to actual pedestrian activity. Therefore, application should be primarily reserved for peripheral streets where the major concern is providing the visual transition to surrounding residential neighborhoods.

# PEDESTRIAN CORRIDOR AND MAJOR PUBLIC OPEN SPACE



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## DESIGN GUIDELINES

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*CITY OF BELLEVUE, WASHINGTON*



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The text of this document was adopted by the City Council, City of Bellevue, through Resolution No. 3946, on December 14, 1981, Resolution No. 4285 on December 12, 1983, and Ordinance No. 5100 on October 19, 1998. Other reference documents are: Ordinance No. 2945, Ordinance No. 3259, and Resolution No. 3948.

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## I. INTRODUCTION AND PURPOSE

Pedestrian amenities are a major focus of the Land Use Code adopted in 1981 to guide the growth and development of Bellevue's Central Business District. A network of public spaces is provided by the Code to tie together intense new development and to ensure a high quality pedestrian environment. In addition to improvements to existing sidewalks, several new types of features are called for: a Major Pedestrian Corridor, Major Public Open Spaces, and mid-block pedestrian connections.

The Pedestrian Corridor is to be a pedestrian street extending from the Bellevue Square regional shopping center to 110th Avenue NE, following the alignment of NE 6th Street. Major Public Open Spaces are to be located at the midpoint of the Pedestrian Corridor at 106th Avenue NE and at each end at Bellevue Way NE and 110th Avenue NE.

Pedestrian connections through development abutting the Corridor are to provide for pedestrian circulation through each block. The easterly third of the Pedestrian Corridor will wrap around a transit center for buses.

The Land Use Code requires the property owners abutting the Pedestrian Corridor and Major Public Open Spaces to prepare design guidelines for these features. This document contains the design guidelines for the Major Pedestrian Corridor prepared for the property owners by TRA and Don Miles Associates/PPS, and for the Major Public Open Spaces prepared by Don Miles Associates/PPS. The design guidelines for the Major Public Open Space at 110th Avenue NE will be prepared in the future and are not included in this document. These guidelines have been reviewed and approved by the Pedestrian Corridor Committee, City staff, and Planning Commission, and have been adopted by the City Council by resolution.

These guidelines will serve three groups of people. First, the Planning and Community Development Department will use these guidelines as a part of the review of proposed projects in the CBD. Second, the guidelines should assist developers and their architects and landscape architects in the design of projects by providing explicit criteria. Finally, these guidelines will benefit the public by ensuring a premier quality downtown walking environment.

Applicants should consult all portions of the Land Use Code, particularly LUC 20.25A, to identify other applicable requirements.

In 1998 the City of Bellevue hired Hewitt Isley to assist them in revising the Design Guidelines for the Corridor to reflect and relate to current thinking and development. The primary purpose of the guidelines remains the same, to benefit the public by ensuring a premier quality downtown-walking environment. However, some of the criteria with regard to the character of the street have changed slightly. The two major aspects that will influence the character of the street are:

- 1.) The Corridor will not be strictly limited to pedestrians. Vehicular travel will be allowed on two of the three blocks within the Corridor. Although vehicles are being introduced to the corridor the primary emphasis will be to develop it with the pedestrian as the focus, providing scale, open space, amenities, activities, and a streetscape conducive and inviting to the pedestrian.
- 2.) The Corridor is still seen as a unit, however, each block is envisioned to have its own distinguishable and unique character.

The revisions to the Design Guidelines were developed together with the Pedestrian Corridor Committee comprised of property owners and developers, and the City staff.

## II. PEDESTRIAN CORRIDOR DESIGN GUIDELINES

### BACKGROUND

Bellevue was originally planned in the 1950s to provide convenient, efficient automobile access and adequate space for parking and auto-oriented land uses. As Bellevue has grown, demands on available land have increased land values and vehicular congestion. With increased density and urbanization, policies have been adopted to encourage pedestrian and transit trips.

In 1981 the City Council of the City of Bellevue passed an ordinance increasing allowable density in the CBD and requiring the creation of a Pedestrian Corridor linking the Bellevue Square regional shopping center with high-rise office and mixed use development to the east. The guidelines provide property owners, developers, architects, and the City with a framework for development of architectural plans fostering successful pedestrian-related development.

In 1998 the City of Bellevue retained Hewitt Isley to address current issues influencing the Corridor's development and to assist the City in revising the design guidelines accordingly. The revisions to the guidelines were developed together with the Pedestrian Corridor Committee, consisting of property owners and developers, and city staff. The study process also included a series of public workshops and meetings.

### OBJECTIVES

The objectives of the Corridor as defined by the Committee are as follows:

Provide a safe, comfortable, lively, and attractive place for pedestrians.

Achieve an identity and an image as a special place.

Allow for modification and expansion over time as surrounding conditions change.

Accommodate access to other major public facilities such as transit center, civic/convention center, and other public spaces.

Reflect the qualities of a truly "urban" environment with its intensity, sophistication, and diversity.

Reinforce and stimulate high quality future adjacent development.

Reflect characteristics of this locale: climate, vegetation, and topography.

Encourage evening and weekend use, as well as weekday use.

Provide a focal point for downtown Bellevue.

### IDENTIFICATION

The Corridor should be united by common graphics and visual elements, with differing character established for the three blocks described below.

### IMAGE

The Corridor, when completed, will offer a variety of pedestrian experiences and amenities. The physical appearance of the Corridor, based upon design guidelines, is described below:

Segments of the Corridor may be partially or entirely covered for weather protection but not enclosed and will be open to the public 24 hours a day, excluding temporary closures for Corridor maintenance purposes and special events.

As the Corridor is encouraged to follow the existing Bellevue topography, it may be divided into a series of levels stepping down with the topography and connected by ramps and steps with the high point at 108th Avenue NE. North/south street crossings will be at grade.

Major public open spaces will be located along the Corridor and will provide a sense of gateway and focal point at regular intervals. An open space of approximately 10,000 square feet will be located at Bellevue Way NE, an open space of approximately 30,000 square feet will be divided equally on either side of the center of the Corridor alignment on the east side of 106th Avenue NE, and another approximately 10,000 square feet of open space will be located in the vicinity of 110th Avenue NE. These spaces will be landscaped and may contain other pedestrian amenities such as: activity areas, event areas, seating, water features, art features, and pedestrian-scaled lighting. The spaces will have abutting pedestrian-oriented frontages and may contain vendors, kiosks, and other activity generating features.

Both base structures or towers may front on the Corridor to allow direct pedestrian connections. In order to relate the building walls fronting the Corridor to the pedestrian, a visual break must be provided. Building walls fronting the Corridor may have bay windows, awnings, balconies, plantings, and other treatments which relate to pedestrians.

Building location, massing, and variation of structure shall be considered to create sunny areas at reasonable intervals.

Building walls on the Corridor may vary in distance from the center of the Corridor alignment, but overall character should emphasize connectivity and variation within a unified, linear whole. Through pedestrian movement along the Corridor will be concentrated along the edges where window shopping, cafe seating, flower tubs, etc., will often occur.

Secondary pedestrian movement paths will provide additional pedestrian access and commercial frontages through Corridor super blocks, reinforcing the role of the Corridor as a focus for pedestrian activity. The secondary paths may lead to enclosed atriums and landscaped spaces.

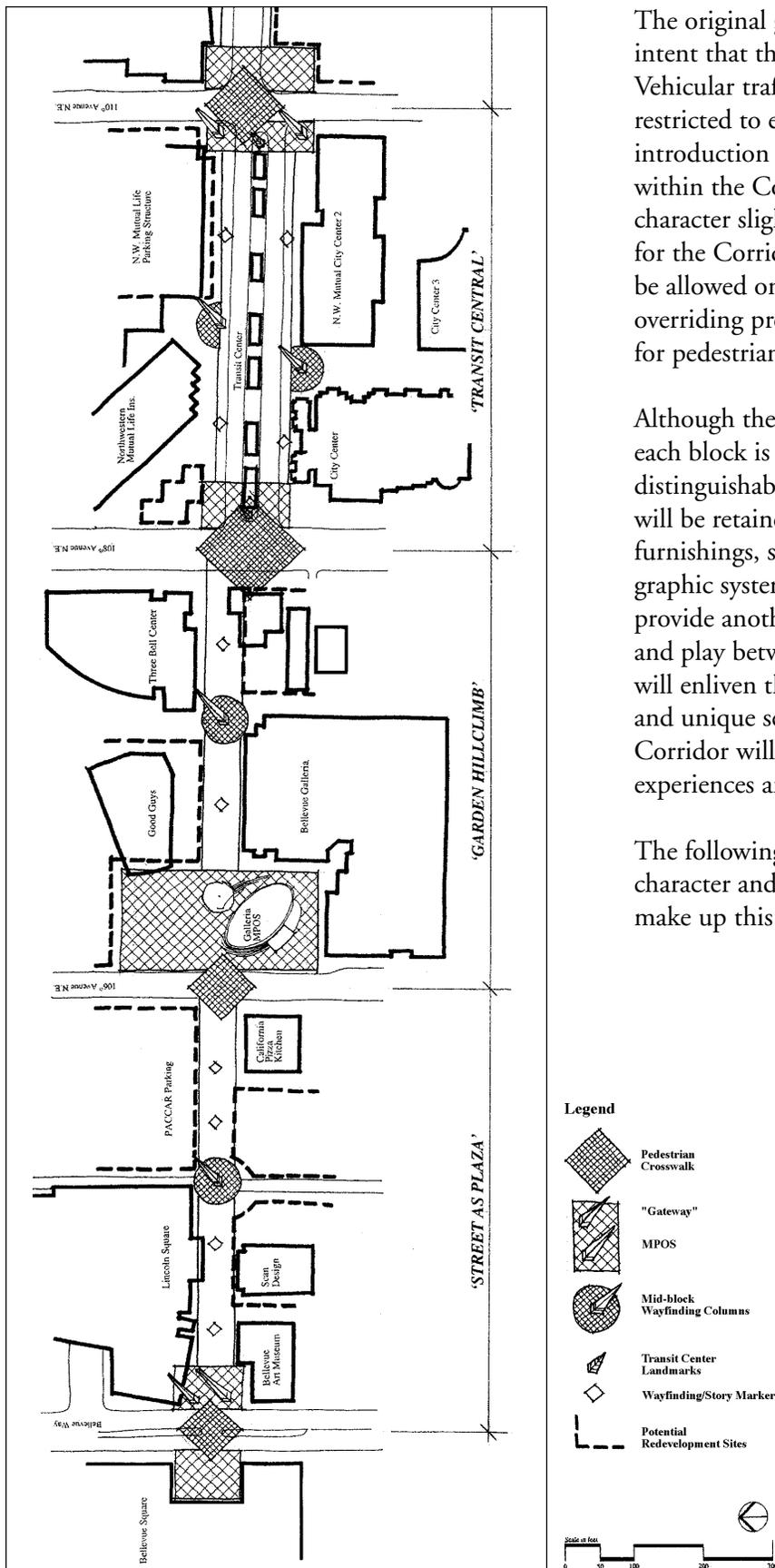
The Corridor may provide a canopy of shade trees, seating, water features, kiosks, directional graphics, seasonal plantings, lighting, artwork, and other pedestrian amenities.

Weather protection such as awnings, balconies, or arcades may be provided at various points along the Corridor. The topography and differing architectural treatments will result in a variety of types, heights, and alignments for weather protection. The variety of weather protective treatments may bring color, rich detail, ornamentation, and distinctive pedestrian-scaled signage and lighting into the Corridor design.

A coordinated system of lighting, paving, street furniture, and informational graphics will provide an element of continuity.

Sky bridges over, concourses under (excluding parking areas), and enclosure of the Corridor are not permitted, in order to concentrate pedestrians in the Corridor and to encourage public access.

The Corridor will be designed, constructed, and managed to provide a safe and comfortable pedestrian space for the public and will include special access provisions for the handicapped, as required by code.



The original guidelines were prepared with the intent that the Corridor be strictly pedestrian. Vehicular traffic was not permitted or was restricted to emergency vehicles only. With the introduction of vehicles on two of the blocks within the Corridor, this has changed the character slightly and is reflected in today's vision for the Corridor. Although vehicular traffic will be allowed on two of the three blocks the overriding premise that the Corridor is primarily for pedestrians is still true.

Although the Corridor as a whole is seen as a unit, each block is envisioned to have its own distinguishable and unique character. Continuity will be retained through the use of common site furnishings, such as lights, litter receptacles and graphic system. The street tree canopy will provide another layer of continuity. This contrast and play between similar and unique site elements will enliven the street overall, giving it an exciting and unique sense of place. When completed, the Corridor will offer a rich variety of pedestrian experiences and amenities.

The following is a description of the unique character and quality of the three blocks that will make up this special Corridor.

Figure II-1: Conceptual Diagram

**“Street as Plaza” — Bellevue Way to 106th Avenue N.E.**

The intent of this block is to give it the character of an urban plaza. Cars are allowed through the block but the design intent is to make it pedestrian-friendly. The street section is to be asymmetrical in configuration. The sidewalk along the north edge will be wider creating a larger pedestrian area, which will take advantage of more opportunity for sun exposure. The additional width along the north side will also permit the installation of the double row of street trees that are to characterize the whole Corridor. The double row of trees will provide a unifying element giving the street a sense of continuity. The sidewalk along the south edge will be a minimum of 16 feet wide. The south sidewalk is to be of sufficient width to accommodate a single row of street trees. The vehicle travel lane, or lanes, are to be kept at a maximum width of 10 feet.

Curbs will be installed to differentiate between vehicular travel lanes and the pedestrian sidewalks. To help enhance and emphasize the notion of “street as plaza”, a uniform and continuous patterned surface treatment of unit pavers is to be used within this part of the Corridor. Stamped concrete pavers to match unit pavers may be considered in vehicle driving areas for durability.

The primary pedestrian movement will be along the edges where window shopping, café seating, vendors, flower tubs, etc. will often occur. Wider sidewalks and smaller scale paving will encourage activities to reach out into the street. Pedestrian drop-off would be permitted in the drive lane but no special accommodations are to be provided, such as drop-off lane or a lay-by. On occasion the block can be closed to vehicular through-traffic and become strictly a pedestrian space providing venues for special events, festivals, and street fairs.

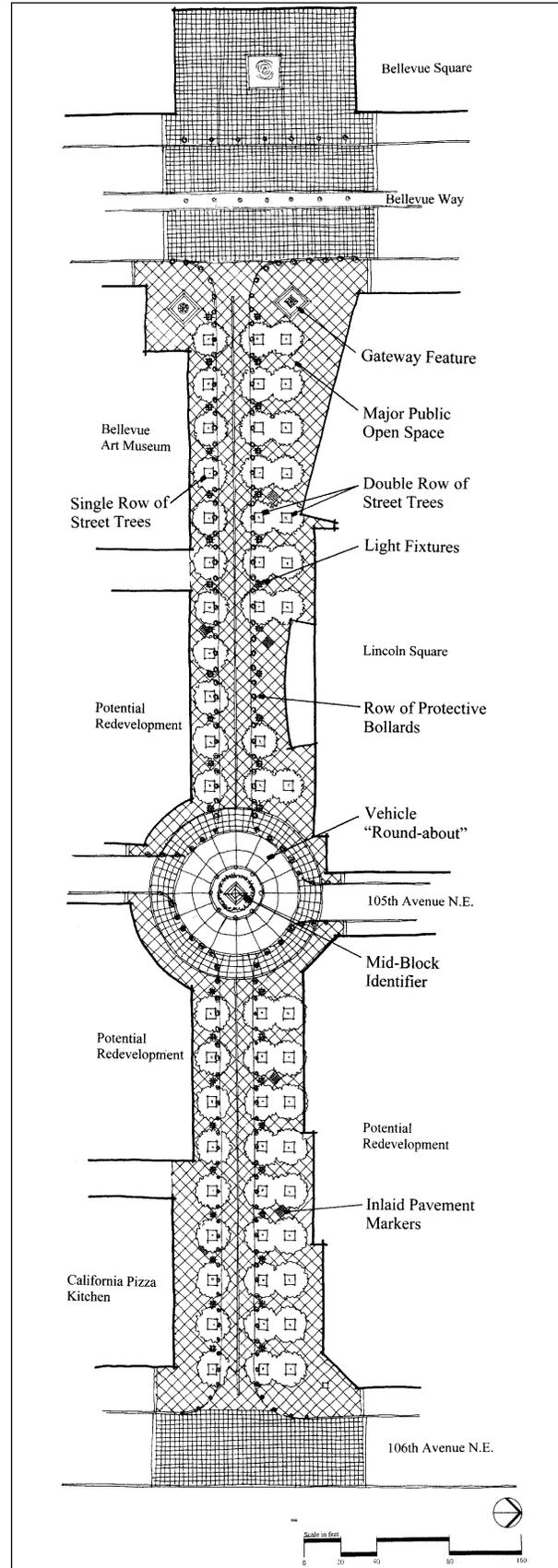


Figure II-2: “Street as Plaza” Illustrative Plan

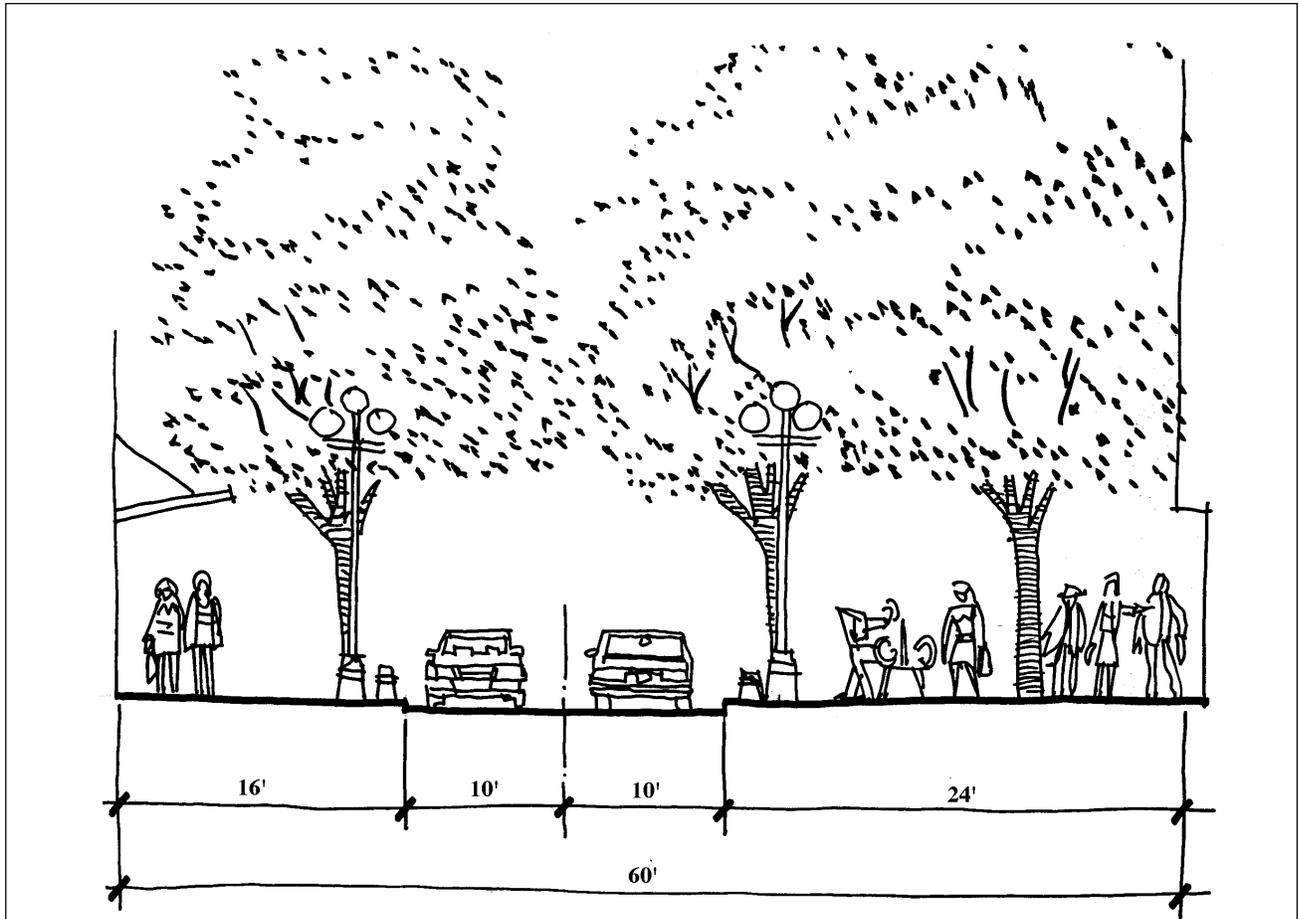


Figure II-3: "Street as Plaza" Section



Figure II-4: "Street as Plaza" Sketch

### “Garden Hillclimb” — 106th Avenue N.E. to 108th Avenue N.E

Vehicular traffic is not allowed through this section of the Corridor. The design intent within this section of the Corridor is to give it a garden-like character in contrast to the more hardscape of the “Street as Plaza” block. A boulevard approach is envisioned. The major flow of people is concentrated along the edges adjacent to the buildings. The center portion would be more green and garden-like providing opportunities for intimate spaces and rest spots along the Corridor. (Each outdoor room could take on its own unique character, perhaps derived from a particular garden theme.) A more diverse and unique plant palette may be used. A variety of annuals and perennials are appropriate to plant in keeping with the garden quality of this block.

This section of the Corridor has the greatest change in grade. ADA or Washington State Barrier Free requirements, whichever is applicable, should be used. The coordinated system of lighting, trash receptacle, graphic system, and street trees would continue throughout the block providing continuity with the other blocks along the Corridor. Site furnishings, such as benches, planting pots, drinking fountains, and paving patterns could be unique to this section emphasizing its individual character.

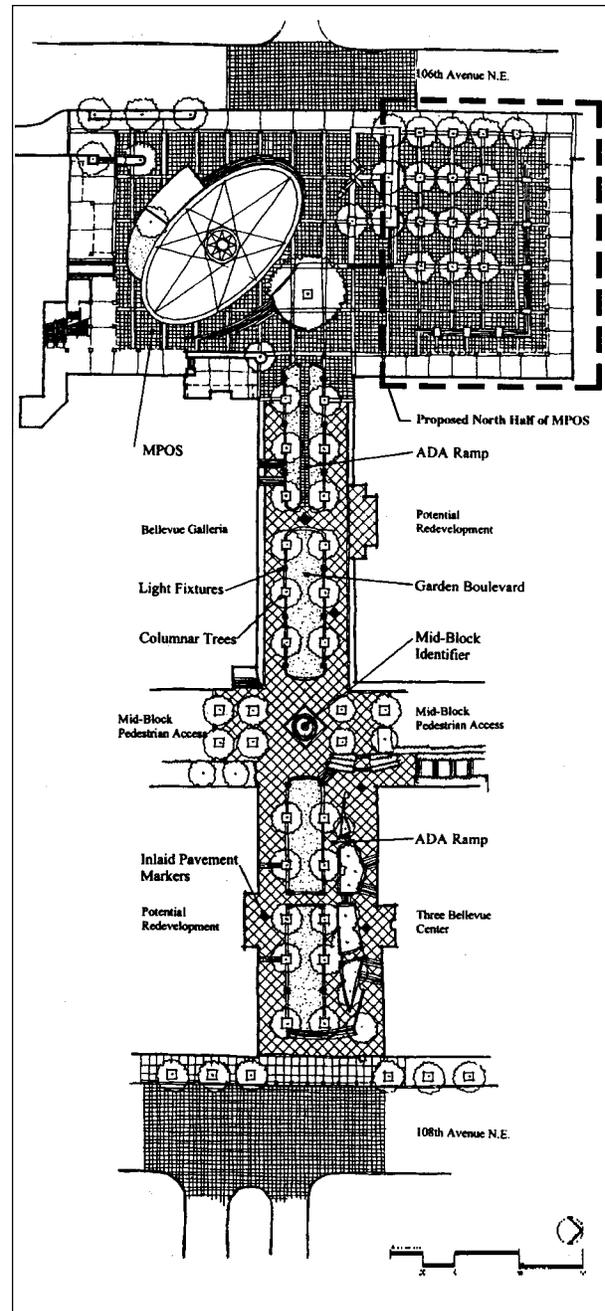


Figure II-5: “Garden Hillclimb” Illustrative Plan

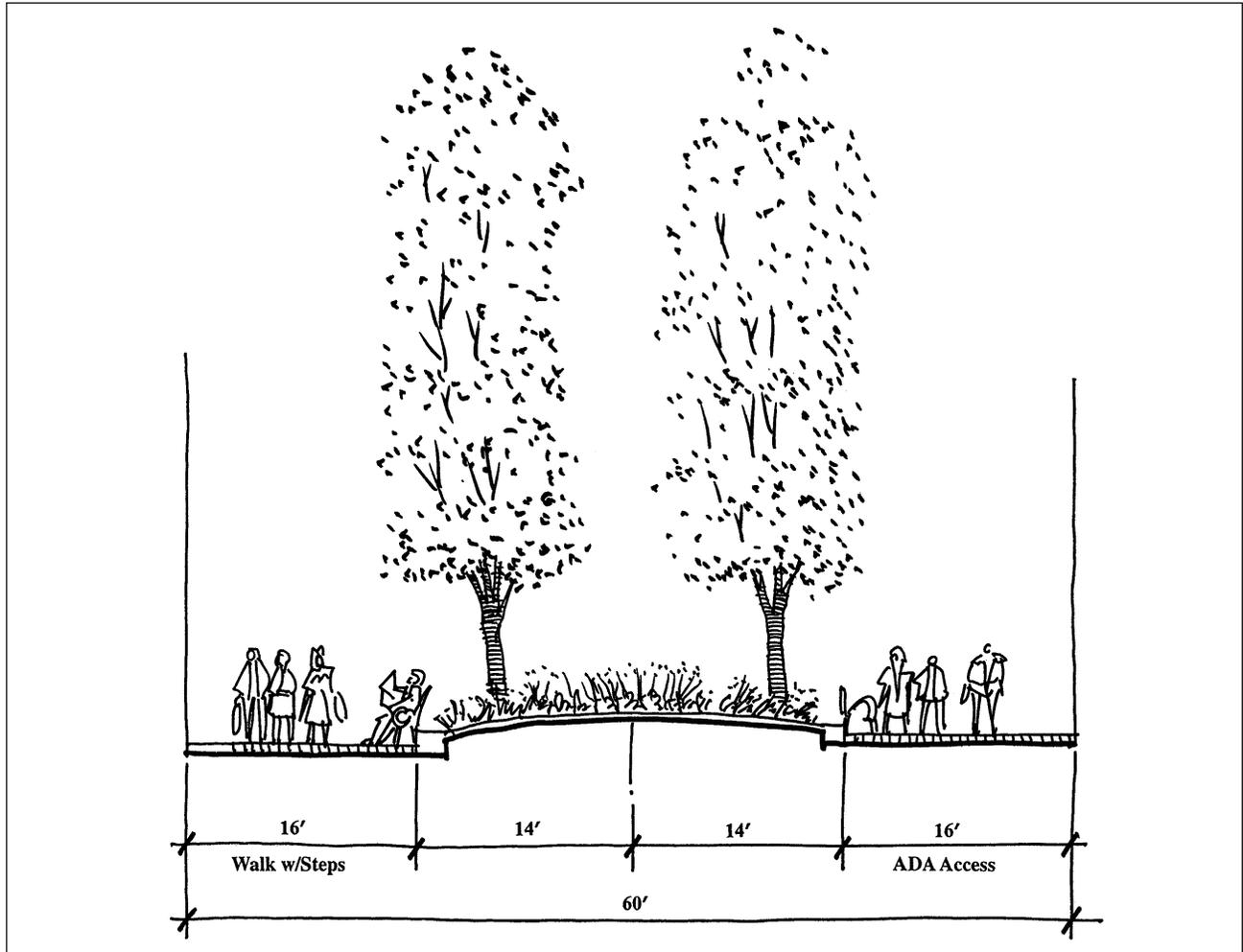


Figure II-6: "Garden Hillclimb" Section; Full Boulevard Development – Flowers with Columnar Trees

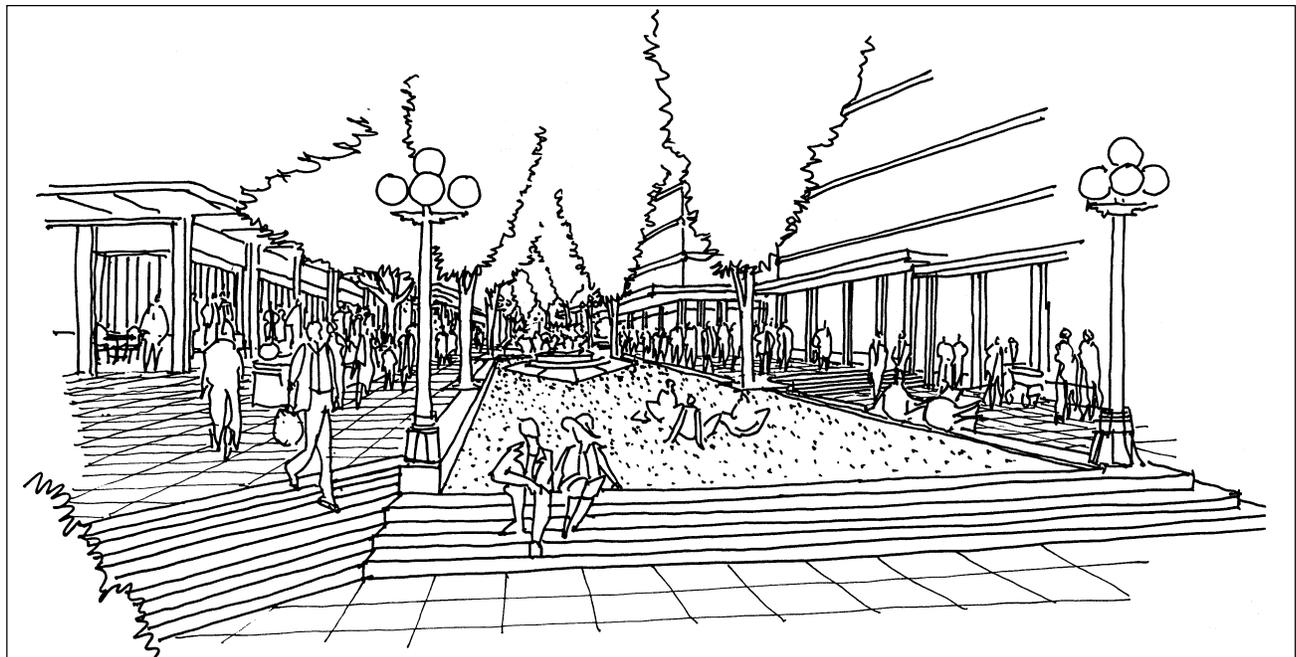


Figure II-7: "Garden Hillclimb" Sketch

### “Transit Central” — 108th Avenue N.E. to 110th Avenue N.E.

The intent within this section of the Corridor is to increase the area dedicated to the pedestrian and provide better pedestrian continuity and passage. To accomplish this it is suggested that the sidewalk along the north edge be widened. This will provide adequate space to plant a double row of street trees in keeping with the overall theme and pattern of street tree plantings. Widening the north sidewalk will also provide more space for seating, kiosks, vendors, and artwork, therefore activating the Street. It offers a more pleasant space to wait for a bus and gives it relief from the steady stream of busses queuing up. A single row of street trees is to be maintained along the south side strengthening the concept of an asymmetrical street section. Paving and site furnishings should be coordinated and similar in character with the furnishings already used in the existing Transit Center.

A portion of the east end of the block is to be incorporated into the Major Public Open Space planned at the intersection of 6th and 110th. This is an important aspect of the MPOS since from this corner there are excellent views of the Cascade Range.

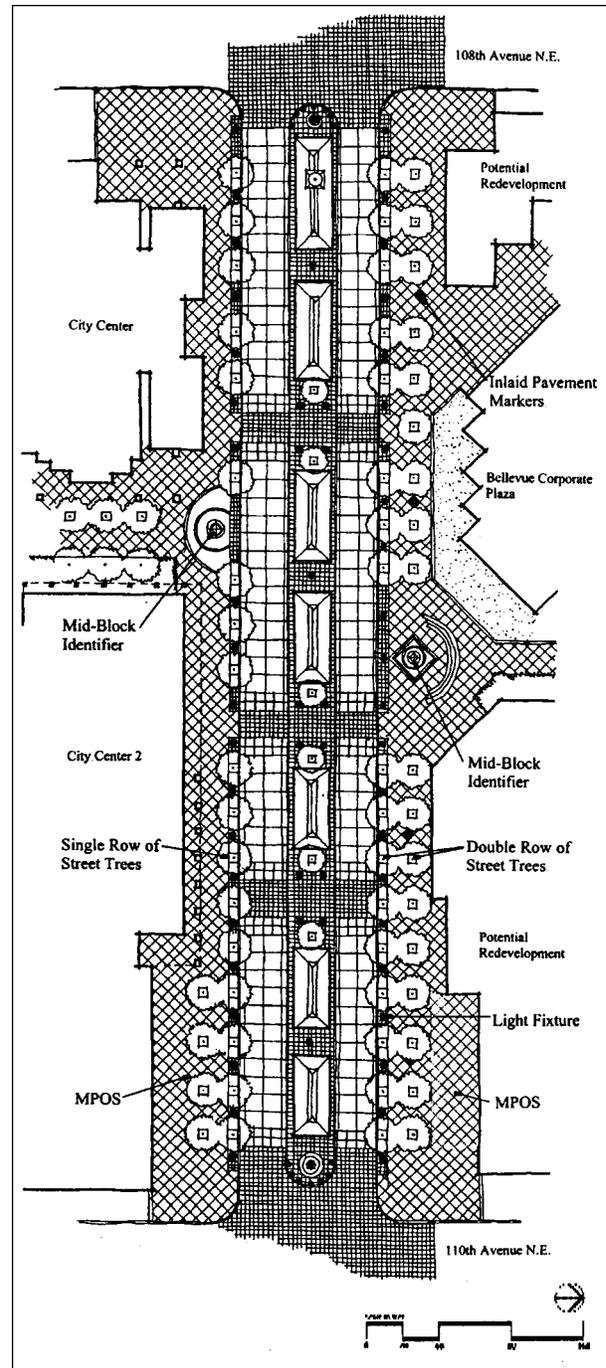


Figure II-8: “Transit Central Illustrative” Plan

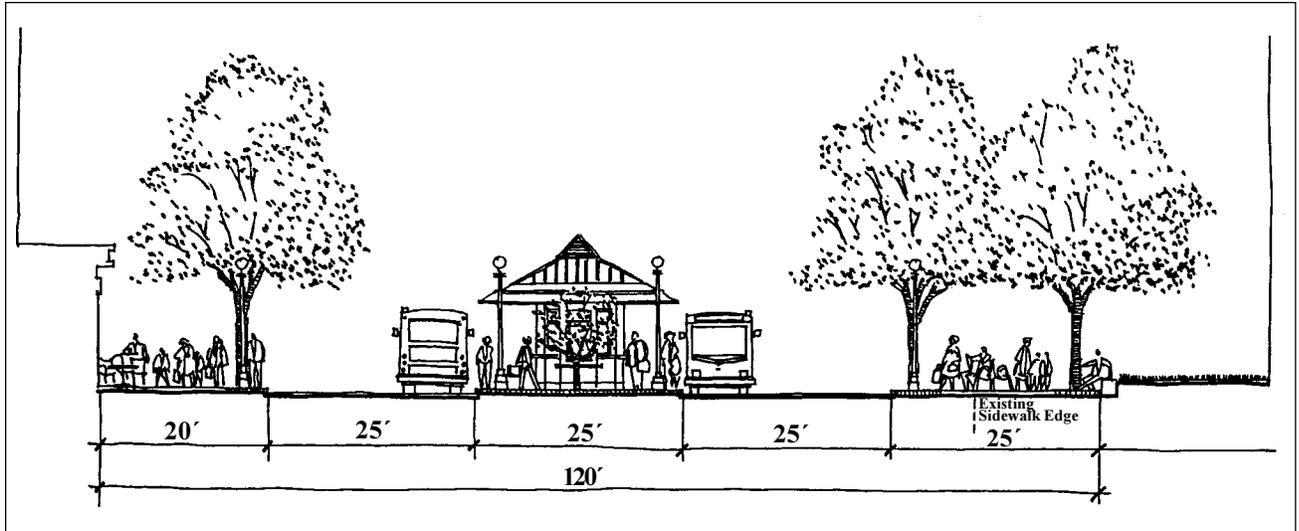


Figure II-9: "Transit Central" West End Section – West End – Widened Sidewalk Along North Side



Figure II-10: "Transit Central" Sketch

## 1. PRIMARY PATHS OF MOVEMENT

### Intention:

To support desired pedestrian pattern of keeping near edges.

### Accomplished by:

Establishing two “streams” of movement which can meander, but should generally follow outer edges of the Corridor.

Encouraging features such as showcase windows, cafes, recessed entrances, canopies, and arcades.

### Principles addressed:

Shifts in alignment  
Multiple use of edges  
Part of a system

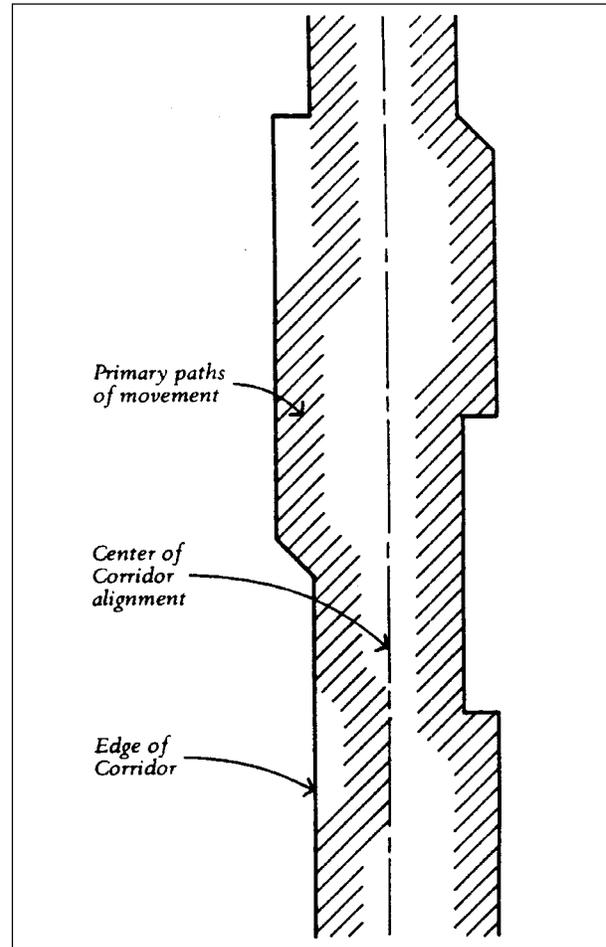


Figure II-11: Primary Paths of Movement

## 2. SECONDARY PATHS

### Intention:

To reinforce and complement the primary Corridor with other paths of movement.

### Accomplished by:

Additional paths of movement located within and around adjacent buildings.

Maintaining existing north-south sidewalks as paths of movement.

Intersecting paths of movement between and within adjacent buildings.

### Principles addressed:

Part of a system  
Adjacent uses

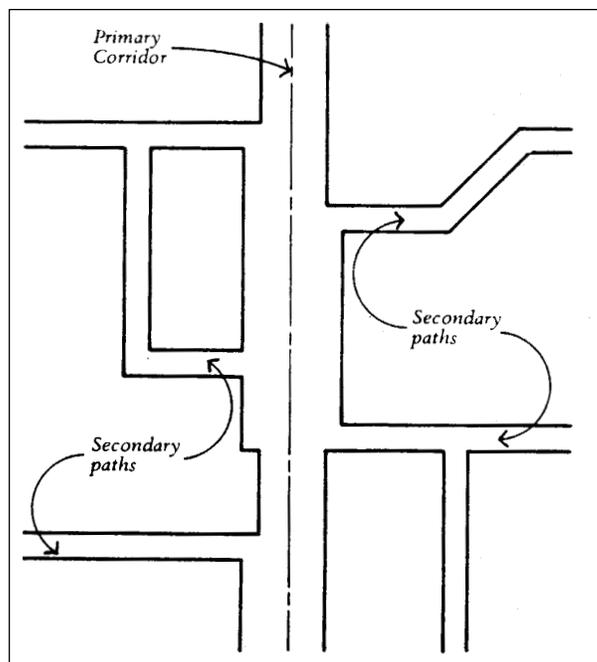


Figure II-12: Secondary Paths

### 3. MID-BLOCK INTERSECTIONS

#### Intention:

To promote more of a pedestrian scale to each super-block by sub-dividing the block approximately at the mid-point. The mid-block intersection for each block will take on its own character and relate to a logical connection or link, which intersects the Corridor. In conjunction with mid-block identifier, (to be described in a following section), the mid-block intersections become points of reference along the Corridor, while reinforcing the unity and continuity of the Corridor. The width of these connections may vary from as little as six feet for a pedestrian only connection, to 26 feet where a two lane connection with a single sidewalk is needed, to as much as 42 feet at points where turning lanes and double sidewalks may be needed where this lane connects to major arterials.

#### Accomplished by:

“Street as Plaza” – Bellevue Way to 106th N.E.:  
The mid-block intersection on this block is to accommodate vehicular traffic. A circular element is to be implemented which will slow traffic down and relate to the pedestrian character of the corridor. Paving treatment is to be pedestrian in scale. The location of the mid-block intersection is at approximately the 105th alignment. The street that intersects the Corridor is to take on the character of a narrow lane. It’s primary purpose is to permit access to service and parking, and to permit interconnected ingress and discharge of traffic to major arterials. In the event that the lane is necessary for access to private development, it should be designed with at least a sidewalk on one side of the lane, and at least two lanes of vehicle travel. It should be well-lit and designed for safe and comfortable pedestrian use. These lanes are not intended to compete with the Pedestrian Corridor as pedestrian spaces, and are not required to have pedestrian-oriented uses or

the street design requirements associated with the Pedestrian Corridor. However, the corner where the lanes connect with the Pedestrian Corridor should be designed so that the Pedestrian Corridor improvements wrap the corner, providing a graceful transition from the Corridor to the lane.

“Garden Hillclimb” – 106th N.E. Avenue to 108th N.E. Avenue:

The mid-block intersection on this block is strictly pedestrian. The intersection is located at approximately the 107th alignment. Special paving, a mid-block identifier and other furnishing will signify this as a special point of reference along the Corridor.

“Transit Central” – 108th Avenue N.E. to 110th Avenue N.E. and Civic Center – 110th Avenue N.E. to 112th Avenue N.E.:

The mid-block intersections within these two blocks are pedestrian in character. However, they are not centered on the Corridor since vehicular traffic runs down the center. Therefore, they are offset and become an integral part of the sidewalks on either side of the travel lanes. They are to be located relative to mid-block secondary paths. Special paving, a mid-block identifier, and other site furnishing will identify this as a special point of reference along the Corridor.

#### Principles addressed:

- Part of a system
- Adjacent uses
- Focal points
- Scale
- Wayfinding/directional information
- Artwork

#### 4. WAYFINDING

**Intention:**

To enhance and strengthen the concept of continuity of the Corridor.

**Accomplished by:**

Providing and implementing a system of elements based on a common theme or story line, providing a visual method of path finding and directional orientation along the corridor. The elements are to be easily recognizable, offer a strong visual impact, and in some instances visible from a distance, such as the mid-block identifiers.

The design of the components for the wayfinding system are to be done at the same time to ensure continuity.

Encourage the hiring of artists to design elements of the system based on the story line.

The major elements of the wayfinding system are entry symbols, mid-block identifiers, and inlaid paving markers which are described in the design details section of these guidelines.

**Principles addressed:**

Part of a system  
Directional information  
Graphic/signing  
Artwork  
Discovery

## 5. CORRIDOR WALLS

### Intention:

To ensure that the design and use of the “walls” abutting the Corridor support the pedestrian-oriented character of the Corridor.

### Accomplished by:

Ensuring that the walls of structures abutting the Corridor incorporate major “visual breaks” at some point above the level of the Corridor. The breaks should be positioned so that the ratio of the width of the Corridor to the height of the breaks falls between 1:2 and 2:1. Therefore, since the width of the Corridor can vary from 40 to 60 feet, breaks should occur at some point between 20 feet and 120 feet above the Corridor. Examples of visual breaks include setbacks, large recesses, corners, and changes in materials.

Ensuring that uses fronting on the Corridor are pedestrian-oriented. Pedestrian-oriented frontage is defined as being devoted to uses which stimulate pedestrian activity, provide visual interest, or use sidewalk space (for cafe seating, displays, etc.). Access for pedestrians from the Corridor to buildings shall be at convenient intervals.

Ensuring that first floor facades are predominantly glass to maintain a high degree of transparency. Highly reflective and dark-tinted glass is prohibited.

Ensuring that facades of other floors abutting the Corridor also include some amount of glass.

Allowing certain features, such as outdoor cafes, showcase windows, awnings, canopies, and bay windows to extend into the Corridor.

Discouraging long, uninterrupted facades.

### Principles addressed:

- Spatial containment
- Multiple use of edges
- Fine-grain use
- Weather protection

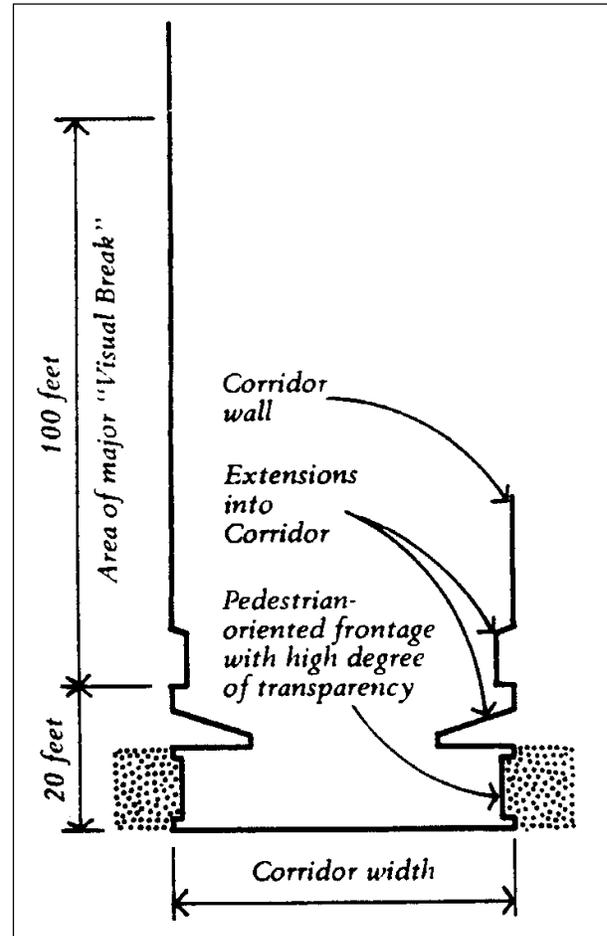


Figure II-13: Corridor Walls

## 6. MASSING OF ABUTTING STRUCTURES

### Intention:

To create an intense urban place, giving consideration to open spaces and public areas, and giving special consideration to sun, shade, and air.

### Accomplished by:

Ensuring that the form and placement of buildings consider year-round conditions of sun and shade within the Corridor.

Encouraging lower portions of buildings to be built to the Corridor edge, but not necessarily in the same manner. Variations in use, design, and configuration should be encouraged.

### Principles addressed:

- Spatial containment
- Solar exposure

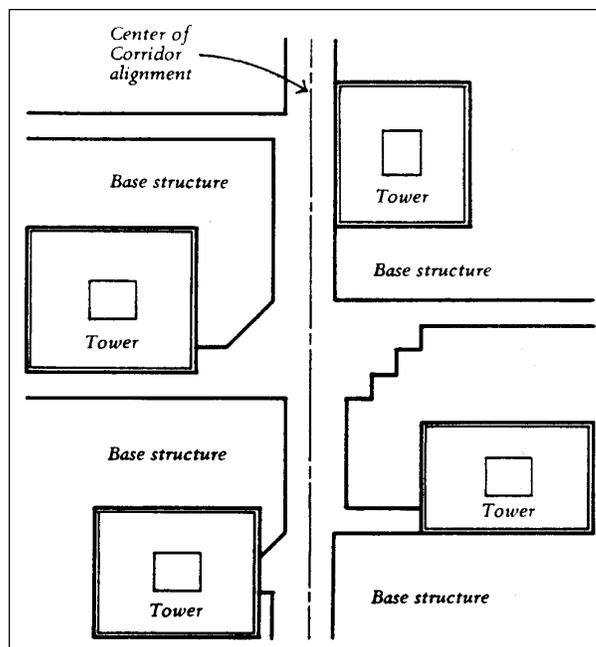


Figure II-14: Massing of Abutting Structures

## 7. ELEMENTS OF CONTINUITY

Intention:

To provide unifying elements along the Corridor.

### **Accomplished by:**

Coordinated system of paving with common accent. (May vary from block to block, if landowners and the City agree-refer to Design Details, Section 3, Paving)

Graphic system (examples: directional and information signs, emphasizing use of international symbols).

Coordinated system of lighting.

Coordinated system of street furnishings.

Landscaping and planting, including seasonal flowers.

Establish a double row of street trees along the north side of the Corridor and a single row on the south side.

### **Principles addressed:**

Flooring  
Furnishings  
Directional information  
Vegetation

## 8. ELEMENTS OF DIVERSITY

### **Intention:**

To encourage a wide variety of designs and activities.

### **Accomplished by:**

Variety of colors, materials, signage, and finishes on building facades.

Variety of planting, including seasonal flowers.

Variety of building configuration, orientation, shape, and proportion.

Variety of devices for weather protection.

Variety of types of seating, including movable chairs, benches, ledges, and steps.

Variety of pedestrian-oriented uses.

Variety of activities and events.

Type of unit paver may vary from block to block if approved by City.

### **Principles addressed:**

Fine-grain use (small design elements)  
Activities and events

## 9.OPEN SPACES

### Intention:

To ensure that open spaces contribute activity and liveliness to the Corridor.

### Accomplished by:

The south half, (15,000 sf.), of the proposed 30,000 square foot open space at 106th has been designed and will be completed by late 1998. The design of the northern half needs to complement the southern half. (Graphic-See illustrative plan of MPOS)

Locating an open space of approximately 10,000 square feet at Bellevue Way NE.

Locating an open space between 10,000 and 15,000 square feet in area at 110th Avenue N.E. The open space should be configured in a linear fashion along the Corridor. This configuration works well with planned mid-block entry to Meydenbauer Place and the existing entry to Meydenbauer Center. Ensure that a portion of the MPOS be located west of the intersection on the "Transit Central block. From this location one is afforded excellent views of the territory and Cascade Range. (Graphic-Diagram showing linear Configuration of MPOS)

Ensuring that the major public open spaces must be designed to provide numerous pedestrian amenities, such as activity areas, event areas, seating, water features, art features, and pedestrian-scaled lighting.

Significant landscaping must be provided. The designs must be coordinated with the design of the Corridor. Pedestrian-oriented frontage is required on two or more sides of major public open spaces.

### Principles addressed:

Focal points  
Activities and events

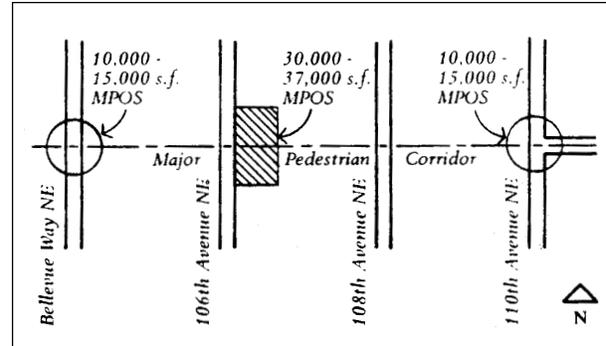


Figure II-15: Open Space Diagram

**10. STREET CROSSINGS**

**Intention:**

To ensure that crossings of major streets provide for pedestrian safety, enhancement of the Corridor, and adequate traffic flow. These crossings will occur at grade.

**Accomplished by:**

Establishing highly visible, very distinctive on-grade crossings with special paving, lighting, planting, and other pedestrian features.

**Principle addressed:**

Safe crossing of traffic

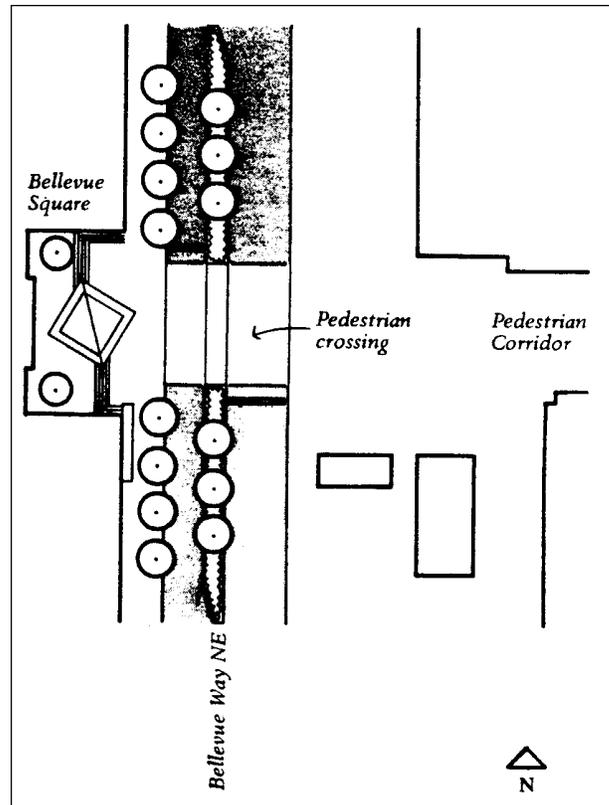


Figure II-16: Street Crossings

### 11. LINEAR SECTORS

**Intention:**

To establish a system of organizing activity areas along the Corridor to produce variety while ensuring good pedestrian flow.

**Accomplished by:**

The minimum width of the Corridor is 60 feet along the entire length. Each blocks maximum width will vary and modulate as each building addresses the Corridor. Primary pedestrian pathways through the Corridor shall have a minimum net pathway of 12.5 feet, except that this width may be lessened to the extent required by topographic constraints. Other pedestrian pathways should be 12 feet net pathway, but no less than six feet if serving only as a secondary connection.

For segments including vehicle access, develop an asymmetrical street section with a wide sidewalk along the northern edge of the Corridor taking advantage of the sun exposure. Widening the northern side will also provide sufficient room to plant a double row of street trees enhancing the continuity of the Corridor.

Center Area of the Corridor: (Vehicles in the Corridor) - Two of the three blocks will have vehicular traffic. Design speeds within these blocks should be established with the pedestrian in mind.

- 1) Within the block, "Street as Plaza", between Bellevue Way and 106th the pavement is to be continuous and of similar material from building edge to building edge, with curbs separating walking from the drive lane.
- 2) The "Garden Hillclimb" block, between 106th and 108th does not have vehicles on it.
- 3) The "Transit Central" block, between 108th and 110th currently accommodates busses and has been designed to reflect a pedestrian quality. The center portion of this block is boulevard in character.

**Principles addressed:**

- Shifts in alignment
- Multiple use of edges
- Mixing of pedestrians and vehicles

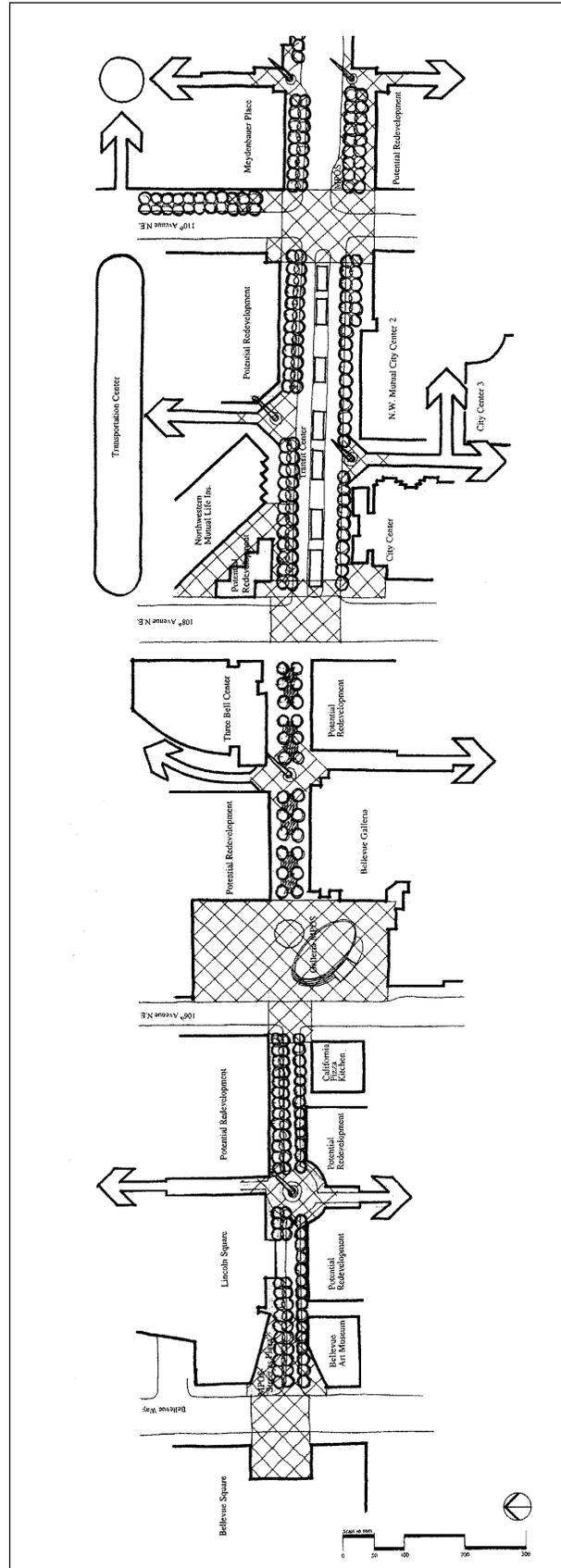


Figure II-17: Design Alternative

## 12. VEGETATION

### **Intention:**

To enhance the appearance of the Corridor through landscaping and plantings.

### **Accomplished by:**

Emphasize continuity and the asymmetrical concept of the street section by installing a double row of street trees on the north edge and a single row along the south edge of the Corridor.

Street trees are to be of similar species within a specific block, however the species may vary from block to block.

Encouraging a variety of plantings and seasonal flowers.

Using tree sizes that will have immediate visual impact.

Landscaping and plantings should not obstruct pedestrian pathways.

Determining appropriate planting details through an ongoing owners' association, in cooperation with the city staff.

### **Principles addressed:**

Vegetation  
Part of a system  
Scale

### 13. ENVIRONMENT

#### (Part A)

##### **Intention:**

To create an appealing pedestrian environment by considering climate and physical setting.

##### **Accomplished by:**

Incorporating devices for weather protection.

Reducing undesirable wind effects.

Using topography to create different levels and viewpoints.

Considering use of water features.

Creating sunny areas at reasonable intervals, such as: at major street crossings, at major open spaces, between buildings, at secondary pathway intersections, and by consideration of building location, massing, and variations of structural form.

Taking advantage of solar penetration by having street be asymmetrical in section with the northern sidewalk being wider and the southern sidewalk narrower.

##### **Principles addressed:**

Weather protection

Views

Focal points

Solar exposure

#### (Part B)

##### **Intention:**

To provide a safe, secure, well-maintained, and effectively operated Pedestrian Corridor.

##### **Accomplished by:**

Creating a Corridor management and maintenance organization.

Developing covenants, conditions, and restrictions for the Corridor.

Considering safety, security, and fire protection in design and construction.

Providing accessibility to security, emergency, and fire protection.

## 14. PEDESTRIAN AMENITIES

### **Intention:**

To ensure that the Corridor emphasizes pedestrian use.

### **Accomplished by:**

Create a theme/story line of common architectural form on which the wayfinding system can be based on giving the Corridor a sense of place.

Providing generous amounts of seating in a variety of forms.

Providing appropriate lighting which shall be both functional and visually distinctive.

Providing drinking fountains, litter receptacles and restrooms.

Providing directories and maps.

Establishing a graphic system using a logo and international symbols.

Encouraging artwork and decorative fountains.

Provide access per ADA or Washington State Barrier Free requirements, whichever is applicable.

Vegetation to complement pedestrian use (e.g. shade, wind protection, seasonal flowers, etc.).

Considering safety, security, and fire protection.

### **Principles addressed:**

Artwork

Directional information

Furnishings

Places to sit

### III. MAJOR PUBLIC OPEN SPACE DESIGN GUIDELINES

#### BACKGROUND

Bellevue was originally planned in the 1950s to provide convenient, efficient automobile access and adequate space for parking and auto-oriented land uses. As Bellevue has grown, demands on available land have increased land values and vehicular congestion. With increased density and urbanization, policies have been adopted to encourage pedestrian and transit trips.

In 1981, the City Council of the City of Bellevue adopted a Land Use Code increasing allowable density in the CBD and requiring the creation of a Pedestrian Corridor with three Major Public Open Spaces linking the Bellevue Square regional shopping center with high-rise office and mixed use development to the east. The Pedestrian Corridor is located in the alignment of NE 6th Street, with Major Public Open Spaces at 110th Avenue NE, 106th Avenue NE and Bellevue Way NE. The Corridor and Major Public Open Spaces are on private land and will be privately designed, developed, and maintained. Transit service has been improved by the creation of an interim transit center near the Pedestrian Corridor. A permanent transit center is being planned.

On May 16, 1983, the City Council of the City of Bellevue amended the Land Use Code to establish a development mechanism for three Major Public Open Spaces in the CBD. These Open Spaces are to serve as focal points and public gathering places within the CBD core and will be located along the Pedestrian Corridor. Among other things, the ordinance required that a Design Plan for each space be approved by the City Council. Property owners abutting two of the spaces hired Don Miles Associates/PPS, Seattle architects and planners, to prepare a set of design guidelines for the spaces which are intended to serve this function. The two locations are:

NE 6th Street and 106th Avenue NE  
NE 6th Street and Bellevue Way NE

The Guidelines include a set of general criteria by which proposed spaces will be reviewed. These criteria address pedestrian movement, adjacent uses and structures, sunlight, traffic crossways, vegetation, activities, and amenities. The detailed design for each open space would be reviewed by the Planning Director through Administrative Design review, as required by the ordinance.

The Planning Department reviewed a draft that was sent to the Planning Commission for review. A number of changes were suggested. Following a Public Hearing held on November 16, 1983, the Planning Commission voted to incorporate the suggested changes. This document was approved by the Planning Commission and was adopted by the City Council, as embodied in Resolution No. 4285, after a Public Hearing held on December 12, 1983.

#### IMAGE

The function of the Major Public Open Space (MPOS) is to serve as a downtown pedestrian focal point and a public gathering place. The MPOS should be designed to host a variety of activities ranging from passive recreation to public events. The MPOS should contain numerous pedestrian amenities to make it comfortable and aesthetically pleasing. The space should have a unified image and be perceived as an extension of the major Pedestrian Corridor.

The space should be physically and visually accessible from the adjacent street right-of-way (ROW), the major Pedestrian Corridor, and abutting development. The MPOS should have a significant amount of vegetation and plantings which reflect the changing seasons.

Because the space beneath the MPOS may be developed for parking and other uses ancillary to abutting development, it is essential that the MPOS is designed integrally with adjacent development. The design of the MPOS will

influence the design of and access to subsurface development, consequently the design development should occur concurrently.

The design of the MPOS should recognize its function and setting. The northern, central, and southern portions of the space and the perimeter of the space should reflect varying conditions of use, degree of sunlight, wind and traffic exposure, gradient, and proximity to major pedestrian flows.

### **GENERAL CONDITIONS**

Allocations of design features on contiguous parcels within the MPOS may be reallocated among the parcels by consent of affected owners and upon approval by the Planning Director. An MPOS parcel is defined as the portion of an MPOS under common ownership.

### 106th Avenue NE MPOS

The southern portion of the MPOS at 106th Avenue N.E. has been designed and is scheduled for completion by the end of 1998. It is the first example of a MPOS as it was outlined in the guidelines. It is bounded on two sides by retail/restaurant. The space includes kiosks, seating, fountain, artwork, and sufficient area for special events and vendors. The central elliptical plaza pays honor to the historical fact that a compass manufacturing company was once located on the site. The northern half of the open space should complement the south half of the space.

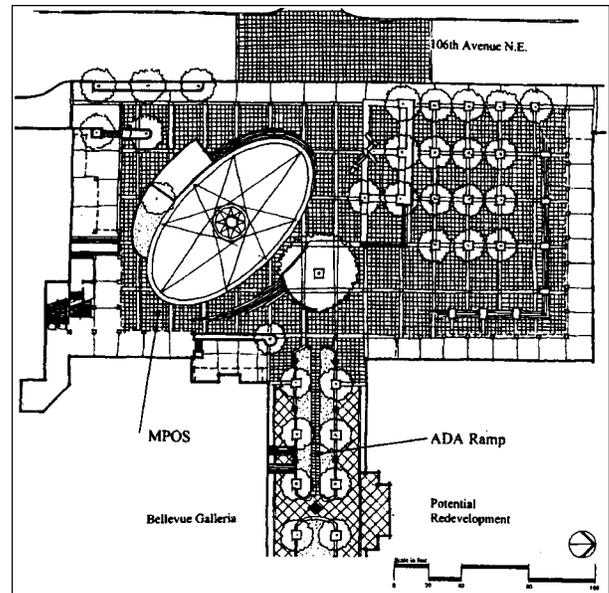


Figure III-1: 106th Avenue NE MPOS Illustrative Plan

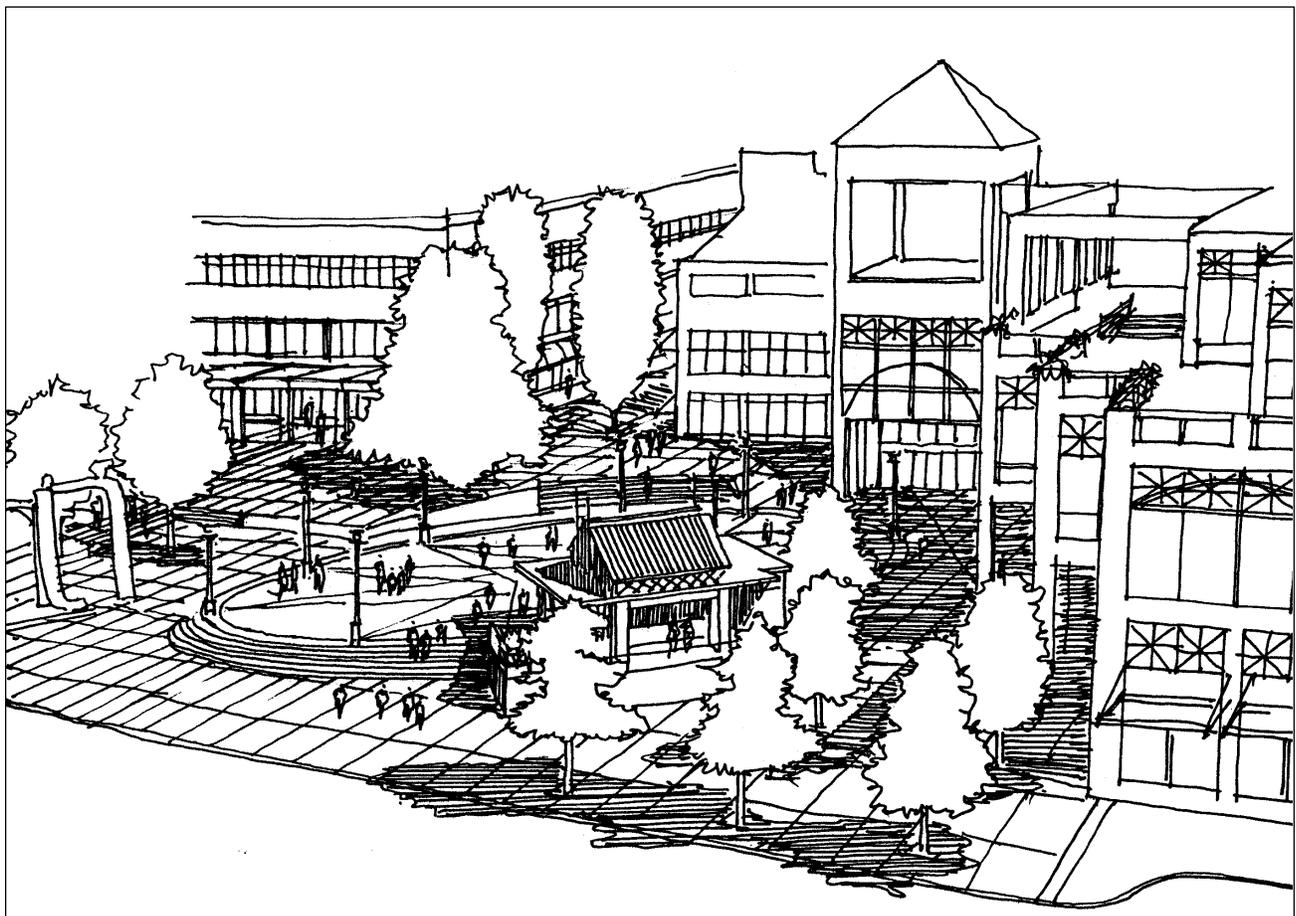


Figure III-2: 106th Avenue NE MPOS Sketch

**Bellevue Way NE MPOS**

An illustrative design concept for the Bellevue Way Major Public Open Space is shown, incorporating the footprints of the new Bellevue Art Museum and the proposed Lincoln Square development. The open space is integrated with the “Street as Plaza” concept for this block of the Corridor. The majority of the space is focused on the Corridor. The Bellevue Way MPOS is the western “Gateway” to the Corridor.

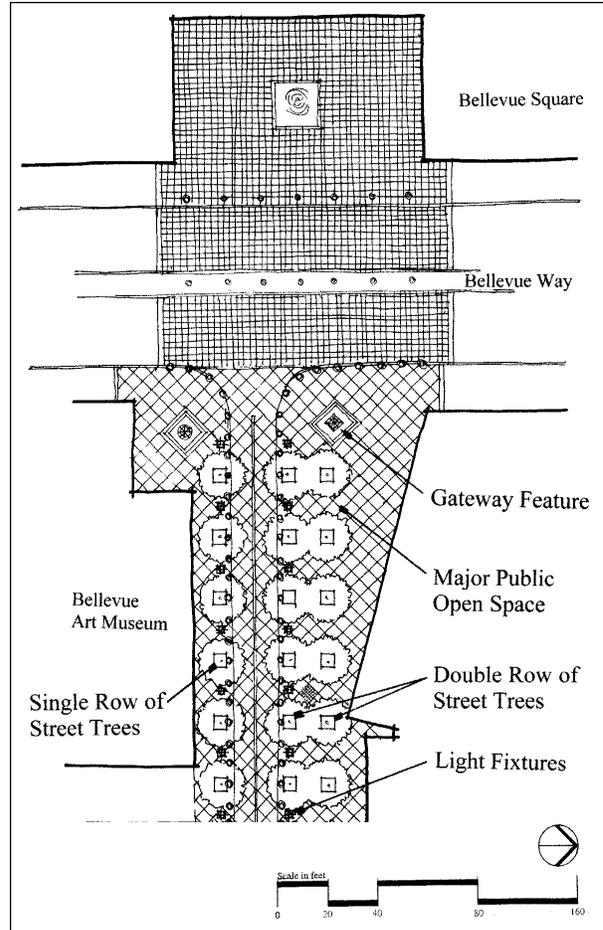


Figure III-3: Bellevue Way MPOS Illustrative Plan

### 110th Avenue N.E. MPOS

The illustrative plan indicates the linear configuration for the MPOS at the intersection of 110th and the Corridor.

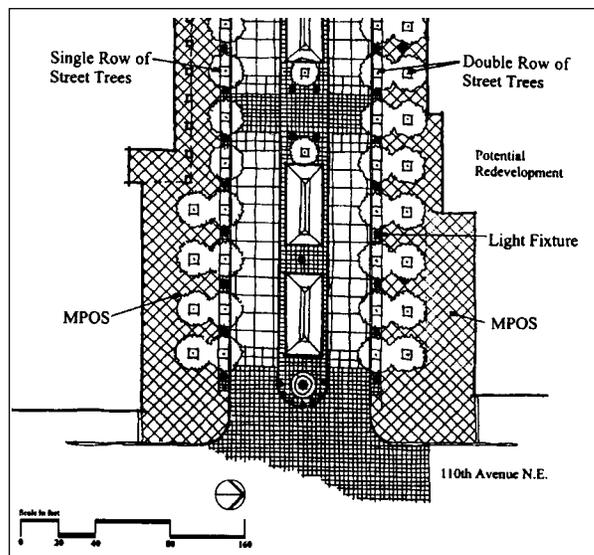


Figure III-4: 110th Avenue NE MPOS Illustrative Plan



Figure III-5: 110th Avenue NE MPOS Sketch

## 1. PRIMARY PATHS OF MOVEMENT

### Intention:

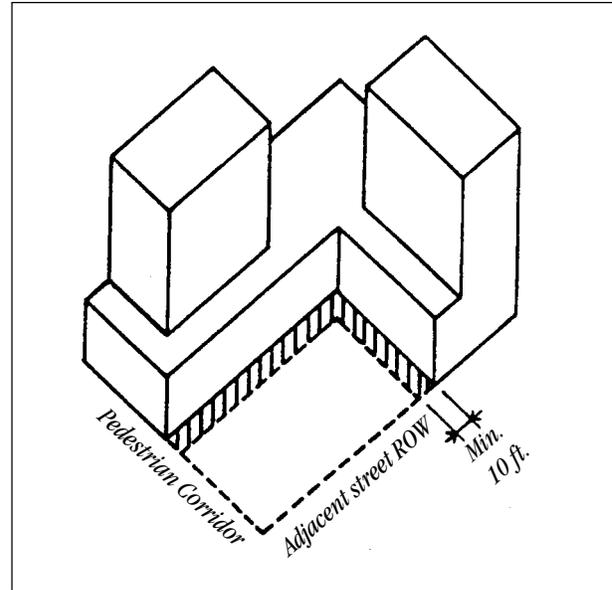
To provide for preferred primary pedestrian movement patterns.

### Accomplished by:

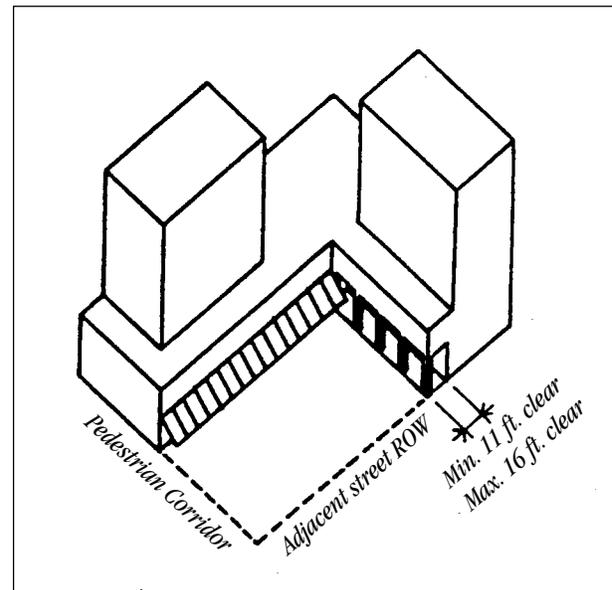
Providing paths of movement connecting the sidewalks on the adjacent street ROW at the corner of the MPOS with the Pedestrian Corridor. These pathways should follow the edge of the MPOS, allowing pedestrians to benefit from storefront views and weather protection of the abutting structures (see 3. Walls of enclosure).

The primary pathway, adjacent to the building walls enclosing the MPOS, should allow for an unobstructed pathway of 10 feet. This pathway may be in a straight path or meander between uses, such as café tables and chairs, displays, kiosks, and planters. A perimeter colonnade, if provided, should allow for 10 feet plus six inches next to the shop fronts and columns, resulting in a minimum clear width of 11 feet (allows two groups of two pedestrians to pass) and maximum clear width of 16 feet (allows groups of three to pass). The sidewalks on the adjacent street ROW are also primary pedestrian paths.\*

The Pedestrian Corridor will intersect the MPOS and will include two paths of pedestrian movement. The pathways in this location may be channeled to a moderate extent by gateways or focal elements, such as sculpture, fountains, archways, or plantings. Pedestrians should be able to maintain the same general direction of travel without major diversion.



*Figure III-6: Primary Paths of Movement*  
The primary pathway, adjacent to the building walls enclosing the MPOS, should allow for an unobstructed pathway of 10 feet.



*Figure III-7: Primary Paths of Movement*  
A perimeter colonnade should allow for a minimum clear width of 11 feet (allows two groups of two pedestrians to pass) and maximum clear width of 16 feet (allows groups of three to pass). Displays, café seating, planters, and similar features within the colonnade should not obstruct more than half the width to allow through pedestrian movement.

\* John J. Fruin, *Pedestrian Planning and Design*, 1971, pp. 19 - 33.

## 2. SECONDARY PATHS

### **Intention:**

To provide pedestrian circulation paths through the MPOS and adjacent development. The orientation and location of these paths are more varied than the primary paths. Pedestrian connections to the MPOS through abutting development, as well as vertical circulation providing access to the towers and parking, are desirable.

### **Accomplished by:**

Providing a variety of secondary pedestrian paths through the MPOS. These paths provide access to the MPOS from primary pedestrian paths. These paths may be defined or undefined. Secondary paths may be direct or may meander through the MPOS.

Frequent MPOS access points should be provided at the perimeter and along the adjacent street ROW sidewalks.

Through-block connections in abutting development, and connections to vertical pedestrian circulation, are encouraged.

### 3. WALLS OF ENCLOSURE

#### **Intention:**

To integrate the building wall, rising vertically immediately adjacent to the MPOS at ground level, with the MPOS to insure active use and the vitality of the space. The walls which enclose the MPOS should be in scale with and related to the pedestrian uses of the space. The uses within these walls should “spill out” into the MPOS (cafe seating, displays, etc.), and the character of open space should extend into the adjacent uses.

#### **Accomplished by:**

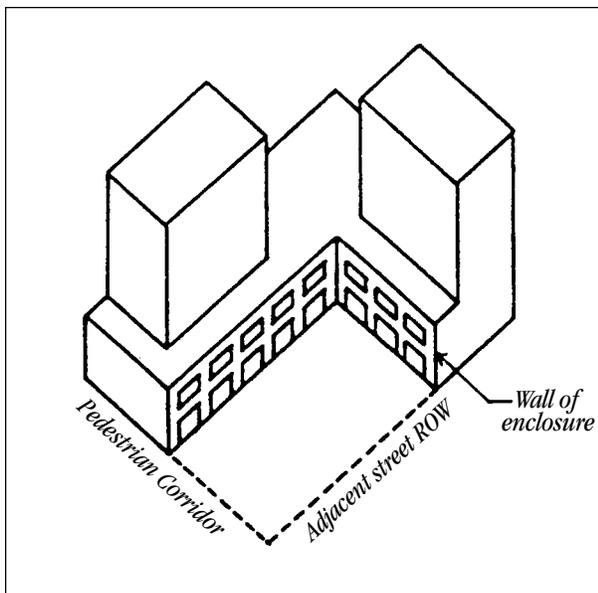
Providing that the building wall, rising vertically immediately adjacent to the MPOS, be generally in the range of 20 to 80 feet in height. (See 4. Massing of abutting structures and provisions for sunlight.) Ensuring that glass windows, providing visual access, are incorporated into the ground level and levels above.

The wall need not be continuous around the perimeter of two sides of the MPOS. The wall may incorporate awnings, canopies, colonnades (arcades), bay windows, and greenhouse-type extensions (structures with roofs and walls predominantly of glass) which are considered part of the MPOS. Greenhouse-type extensions are limited to a total area of 20 percent of the area of an MPOS parcel and may not extend more than 25 feet into the MPOS. Such extensions must allow for frequent pedestrian access from the MPOS, with glass doors that can be easily opened to create an indoor/outdoor character. In the case of a linear configured MPOS, such as at 110th Avenue N.E., no greenhouse-type extension intruding into the space shall be allowed that causes an obstruction to Pedestrian movement. Greenhouse-type extensions may not exceed two stories in height and may not have landings or mezzanines exceeding 50 percent of the area of each floor. The principal purpose of greenhouse extensions is to provide weather-protected space for a variety of small pedestrian-oriented uses.

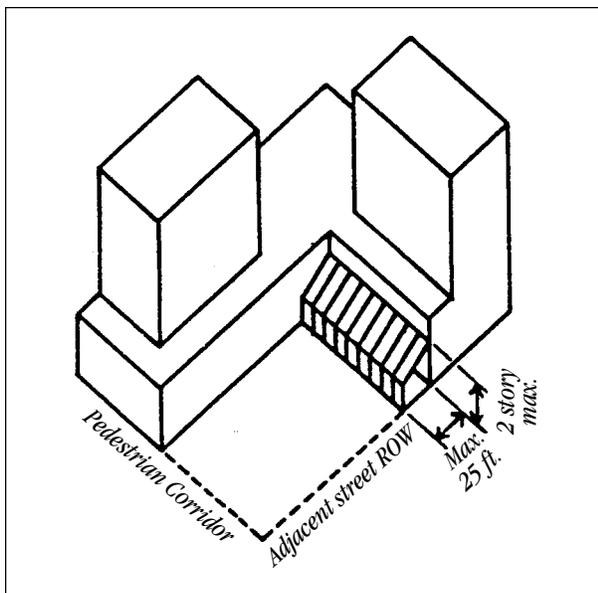
A major portion of the perimeter of the enclosure wall must have clear glass shop/restaurant fronts or display windows. Lobbies, vertical circulation access, artwork, and other pedestrian-oriented uses are also encouraged. Pedestrian access to the above uses should be provided at frequent intervals.

If a colonnade or other partial enclosure is provided, the paving shall be at the same level and of compatible type as the MPOS to insure easy access and continuity with the MPOS. Displays, cafe seating, planters, and similar features may be located within the colonnade or similar enclosure, but should not obstruct more than half the width to allow for through pedestrian movement.

In general, the walls of enclosure should align with the primary pedestrian paths around the perimeter of the MPOS.



**Figure III-8: Walls of Enclosure**  
*A major portion of the perimeter of the enclosure wall must have clear glass shop/restaurant fronts or display windows. Pedestrian access should be provided at frequent intervals. Windows should also be incorporated in levels above ground.*



**Figure III-9: Walls of Enclosure**  
*Greenhouse-type extensions are limited to a total area of 20 percent of the MPOS and may not extend more than 25 feet into the MPOS nor exceed two stories in height. Frequent pedestrian access must be allowed through glass doors, creating an indoor/outdoor character.*

#### 4. MASSING OF ABUTTING STRUCTURES AND PROVISIONS FOR SUNLIGHT

Intention:

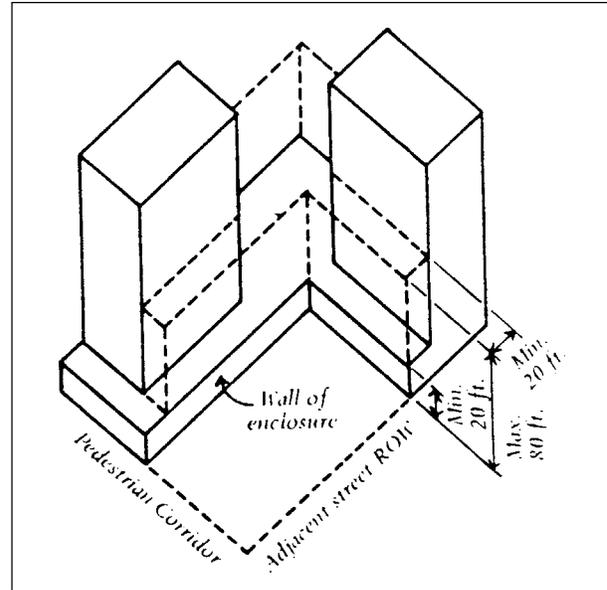
To design structures abutting the MPOS that will scale down the building mass.

#### Accomplished by:

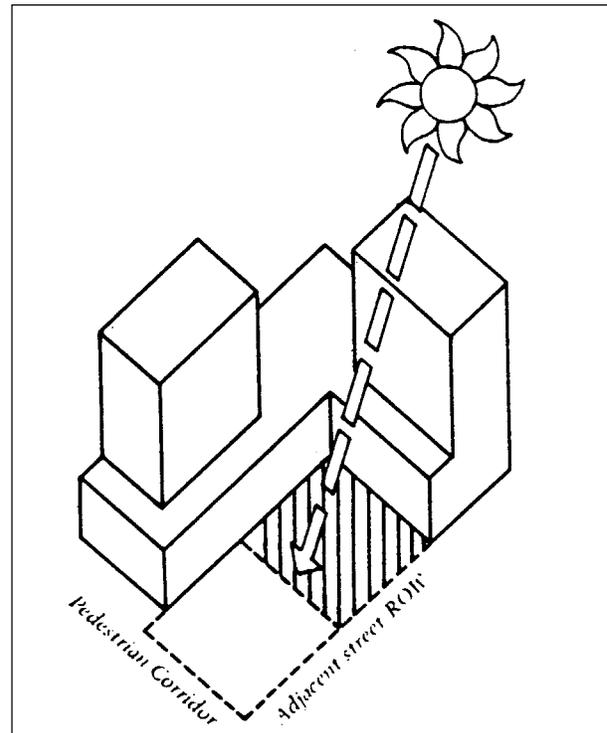
Providing an average upper level stepback of 20 feet or more above the wall of enclosure. This requirement does not apply to structures that are focal points or gateways for the MPOS. The wall of enclosure is to be in the range of 20 to 80 feet in height.

Terraced building massing is encouraged to scale down the abutting development, and to provide sunlight.

Direct sunlight should fall on a contiguous surface area of the MPOS, of not less than 50 percent of its total area (the combined area of contiguous MPOS parcels and adjacent Pedestrian Corridor) at noon, June 21 (using daylight savings time). Spaces north and south of the Corridor are considered contiguous if that portion of the Corridor is in direct sunlight. The street ROW and covered portions of the MPOS are not counted when calculating the percentage of contiguous MPOS in direct sunlight.



*Figure III-10: Massing*  
Average upper level stepbacks of 20 feet are required above the wall of enclosure. The wall of enclosure is to be in the range of 20 to 80 feet in height.



*Figure III-11: Massing*  
Direct sunlight should fall on a contiguous area of not less than 50 percent of the combined area of the MPOS and adjacent Pedestrian Corridor at 12:00 noon, June 21st (using daylight savings time).

## 5. RELATIONSHIP TO PEDESTRIAN CORRIDOR

### **Intention:**

To design the MPOS to appear as an extension, or expansion, of the Pedestrian Corridor, to draw pedestrians into the public spaces. Certain features, or elements, on the MPOS should be designed as part of the same family as those features on the Pedestrian Corridor, to establish continuity.

Although the MPOS should be perceived as an extension of the Pedestrian Corridor, it should also have a distinct identity. The MPOS should be thought of as a single space and an important central focal point in the CBD.

### **Accomplished by:**

Providing lighting, paving, and furnishings. They should be designed as part of the system used for the Pedestrian Corridor, to establish continuity and tie the Corridor to the MPOS. Furnishings may include features such as: drinking fountains, litter receptacles, benches, bollards, kiosks, informational and directional graphics, public telephones, planter tubs, and tree grates and guards (see 9. Pedestrian amenities). Other features may bring a sense of identity and uniqueness to the MPOS (artwork, water features, vegetation, etc.).

## 6. CROSSING OF TRAFFIC

### Intention:

To provide safe crossings of traffic on the adjacent street ROW and across curb cuts and ramps to parking below the MPOS.

### Accomplished by:

Coordinating the design of the MPOS with the design of the pedestrian crossings of the adjacent street. Special paving and lighting, coordinated with the Pedestrian paving and lighting system, should be provided.

A 60- to 85-foot-wide crossing zone and signalization should be provided in the alignment of the Pedestrian Corridor.

Vehicular access to parking beneath the MPOS should not occur along the adjacent street ROW MPOS frontage. Vehicular access to parking beneath the MPOS points should be set back from the northern and southern corners of the MPOS by a minimum of 20 feet, measuring along the property line.

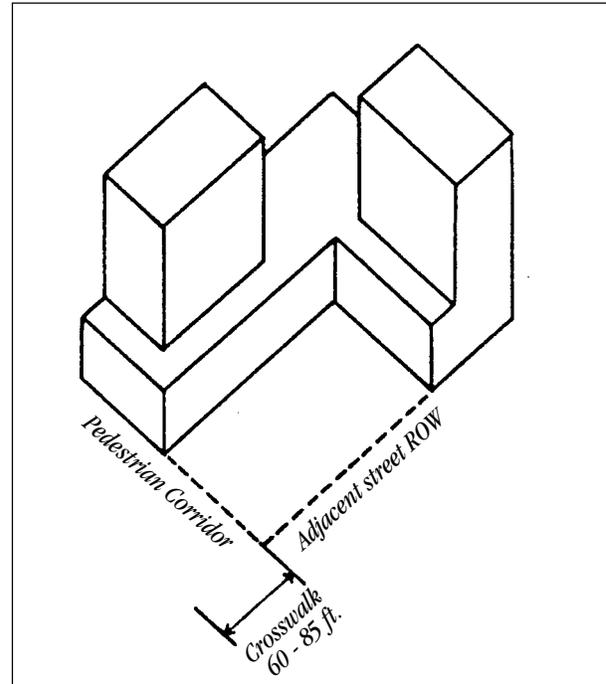


Figure III-12: Crossing of Traffic

The City shall provide a 60- to 85-foot-wide crossing zone and signalization at the adjacent street ROW in the alignment of the Pedestrian Corridor.

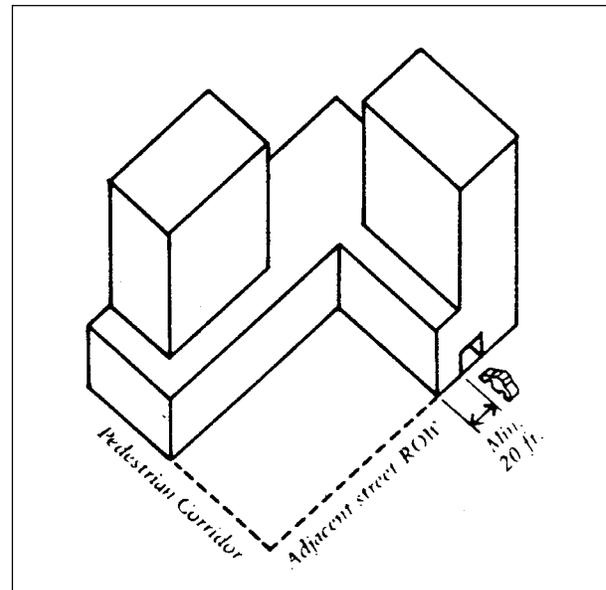


Figure III-13: Crossing of Traffic

Vehicular access to parking beneath the MPOS should not occur along the adjacent street ROW MPOS frontage and should be set back from the corners of the MPOS a minimum of 20 feet.

## 7. ACTIVITY AREAS

### Intention:

To provide a variety of activities for the MPOS. These activities would range from passive recreation (sunning, brown bagging, reading, enjoying plantings, socializing with friends, etc.) to staged events, such as those that are part of “On the Town” program (the free lunchtime entertainment series in downtown parks and plazas).

### Accomplished by:

Providing an area in the northern half of the MPOS at 106th Avenue NE large enough to host medium-sized events (150 to 300 persons standing or seated on features such as lawns, steps, benches, chairs, ledges, etc.).

Small retail and restaurant kiosks and pavilions are encouraged to occupy space in the MPOS, and especially in a shady part of the MPOS, to insure vitality and activity. Vending pushcarts will benefit from a location near the confluence of major pedestrian flows (usually near intersections). Retail/restaurant structures permanently affixed (pavilions, kiosks, etc.) are limited to a total area of 10 percent of the area of the MPOS parcel. Any retail/restaurant structure may not exceed 1,000 square feet in area (excluding awnings and canopies).

Ledges, leaning elements, and planters should be provided at “decision points” on primary pedestrian paths where people stop to talk, prior to going separate ways.

Pedestrian-oriented frontage shall be provided on at least two sides of a MPOS (see 3. Walls of enclosure).

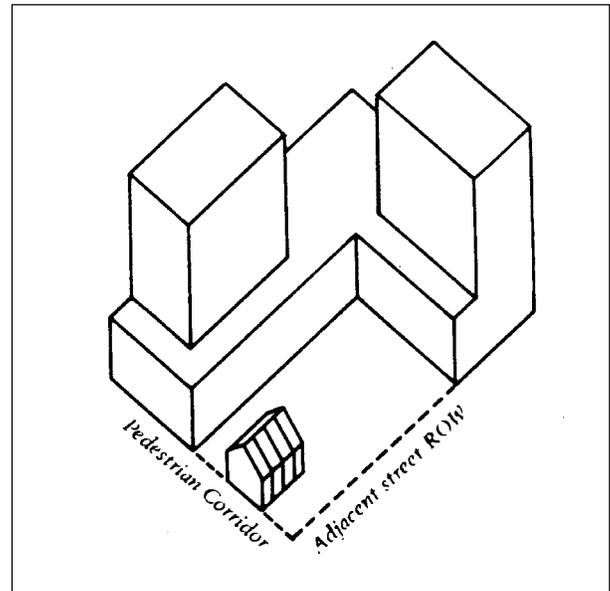


Figure III-14: Activity Areas

Retail/restaurant kiosks and pavilions are limited to a total area of 10 percent of the MPOS and may not individually exceed 1,000 square feet in area (excluding awnings and canopies).

## 8. VEGETATION

### Intention:

To provide greenery and vegetation in the Corridor and MPOS, as shown to be a preference among downtown workers and spouses in the Downtown Employees Survey (May 1981).

Extensive use of vegetation should be provided, including the use of plantings to define spaces and activity areas, highlight the changing seasons, provide color and greenery throughout the year, express various spatial scales (trees/flowers/leaves and petals), contribute to the spatial and visual unity of the MPOS, and provide for important physical and visual connections. The design should accommodate the health and continued maintenance of all plant materials. The MPOS atmosphere should be encouraged to blend with, and extend into, the abutting buildings. Special planting should mark the space as a unique point along the Pedestrian Corridor.

### Accomplished by:

Coordinating the planting design for the adjacent Corridor and the MPOS.

Providing visual and spatial unity of planting as a consideration in creating the perception of the two sides of the Corridor as one entity. The planting design should, however, respond to the unique characters of each side (sun/shadow, slope direction and gradient, element or activity location, etc.). Using plantings to break up large spaces and create a human scale.

Creating colorful planting throughout the year using flowering annuals and perennials along major pedestrian routes and at focal points within the space, and using flowering shrubs and trees and plants with fall color. Providing evergreen planting such as shrubs, annuals, perennials, grass and ground cover. Trees should be predominantly deciduous to allow light through in the winter.

Trees should generally be planted flush with the pavement surface in order to facilitate pedestrian movement. However, because of subsurface structural requirements, there may be areas where trees could be in raised planting pockets. In such cases, seating should be incorporated into the raised elements.

Lawns or ground covers, if provided, should be protected from major pedestrian circulation routes and other heavy usage by some means, such as curbs or low seating walls. Islands formed by containers or planting beds should define activity areas or special places in the MPOS. They should not block important views or access from the street. Locating plantings so as not to interfere with sitting on ledges.

Providing adequate structural support for planting on top of the structure.

Providing adequate planting specifications and detailing for the healthy growth of plant material.

In general, specifying large caliper trees and plant materials hardy to this area.

Providing irrigation and long term, year-round maintenance of all plants.

## 9. PEDESTRIAN AMENITIES

### Intention:

To provide the pedestrian in the MPOS with a sense of place, which are safe, comfortable, and offer the pedestrian an aesthetic experience (artwork, water features, and architectural features). All spaces are to be readily accessible meeting ADA standards.

### Accomplished by:

Providing comfort by including generous amounts of seating in a variety of forms, drinking fountains, litter receptacles, directories and maps, etc. Bicycle racks should be encouraged near adjacent street ROW. Surfaces suitable for leaning should be provided where waiting occurs, and where possible, waiting areas to be sheltered from wind and rain.

Establishing a special sense of place and aesthetic quality; for example, with artwork, decorative fountains, and other water features (fountains can provide “white noise” to block out sounds of traffic and provide privacy for conversations). Encouraging drinking fountains in eating and brown bagging areas.

Providing appropriate lighting (functional and visually distinctive) and coordinating it with the Pedestrian Corridor lighting. Encouraging the use of the Corridor light standard in the MPOS. Encouraging pedestrian-scaled lighting.

Encouraging handicapped access and facilities. Most areas should be barrier-free to the elderly and handicapped. Pedestrian amenities should be accessible to the handicapped (meeting ADA or Washington State Barrier Free requirements, as applicable), such as drinking fountains.

Incorporating ventilation stacks and grates (if required for subsurface parking) into landscaping and furnishings to minimize the adverse environmental impacts.

Providing primary (chairs and benches) or secondary (walls, steps, and planter ledges) seating. The latter count as seating if of a height not less than 12 inches or more than 36 inches and depth not less than 12 inches. At least 10

percent of the seating should be primary. Provide a minimum of one linear foot of seating (chairs counting as 30 inches) for each 30 square feet of MPOS, excluding the adjacent Pedestrian Corridor. Provide choices of seating configuration. Movable tables and chairs allow people to define their own social setting. Linear benches or ledges allow people to establish comfortable distances. Circular benches, by virtue of slightly different orientation of each sitting place, discourage social groupings, but accommodate large numbers of single people. The required seating may be located on the adjacent Major Pedestrian Corridor or in the MPOS.

Providing proper paving material. It should be non-glare, nonslip, and be free of large joints, cracks, toe trips, or uneven surfaces. Changes in pattern or texture can define varying uses.

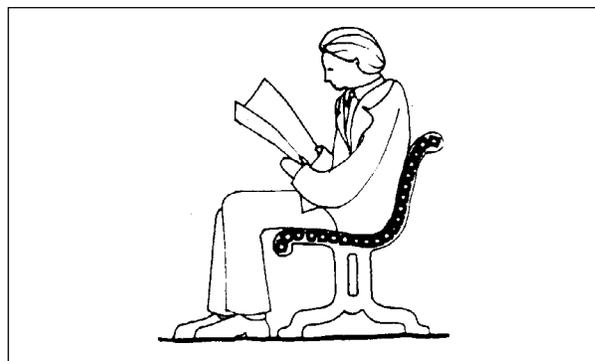


Figure III-15: Pedestrian Amenities

A minimum of one linear foot of seating (chairs counting as 30 inches) for each 30 square feet of MPOS is required. At least 10 percent of the seating should be primary seating (chairs and benches).

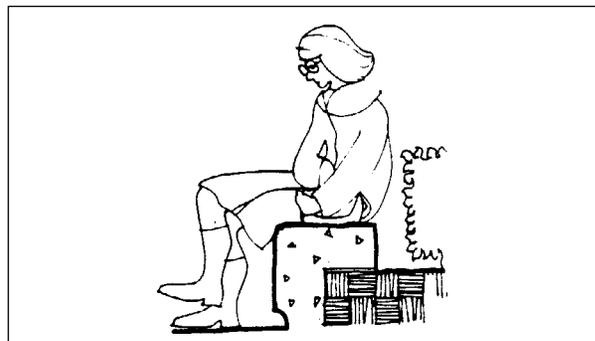


Figure III-16: Pedestrian Amenities

Secondary seating (walls, steps, and planter ledges) must have a height not less than 12 inches or more than 36 inches and depth not less than 12 inches. Plantings should be located so as not to interfere with sitting on ledges.

## 10. ENVIRONMENTAL SETTING

### **Intention:**

To create an appealing pedestrian environment by considering climate and physical setting.

### **Accomplished by:**

Incorporating weather protection where possible around the perimeter of the MPOS in the form of awnings, canopies, or colonnades.

Reducing, where possible, undesirable traffic noise and wind impacts.

Using topography to create different levels and viewpoints.

Using water features and plantings suited to the climate and shady and sunny areas of the MPOS.

## 11. ENVIRONMENTAL MANAGEMENT

### **Intention:**

To provide a safe, secure, well-maintained and effectively operated MPOS, as an extension of the Pedestrian Corridor.

### **Accomplished by:**

Including MPOS management as part of the Pedestrian Corridor management and the use of the same maintenance organization.

Encouraging safety, security, and fire protection in design and construction.

Providing year-round seasonal planting and landscape maintenance which will highlight the MPOS as a downtown public focal point.

Providing special maintenance of water features.

Providing a varied and changing program of events, displays, and environmental art.

Encouraging lighting designed to enhance safety and security (pools of light along buildings and landscaping to avoid dark spaces).

## IV. PEDESTRIAN CORRIDOR AND MAJOR PUBLIC OPEN SPACE DESIGN DETAILS

### BACKGROUND

The City Council, City of Bellevue, adopted general design guidelines for the Pedestrian Corridor in 1981 and for Major Public Open Spaces at Bellevue Way and 106th Avenue NE in 1983. The guidelines for the Pedestrian Corridor describe Elements of Continuity and Elements of Diversity. The Elements of Continuity include streetscape features intended to visually unify the Pedestrian Corridor. These features consist of coordinated systems for paving, lighting, graphics, street furnishings and landscaping.

The purpose of the Design Details is to provide more specific design direction for Elements of Continuity in order to insure a cohesive identity for the Pedestrian Corridor and the Major Public Open Spaces. In February 1984, Don Miles Associates/PPS, architects and planners, were hired by the Corridor property owners' committee to develop the Design Details. The Design Details were developed together with the Pedestrian Corridor Committee, consisting of the property owners and developers, and City and Metro staff.

The Design Details address aspects of lighting, paving, landscaping, seating, and graphics which have been identified as Elements of Continuity and are described as follows: pole top lighting; bollards and bollard lighting; paving, banding, stairs and ramps, tree grates, walls and ledges, benches; fixed planters, movable planters; litter receptacles; drinking fountains; and street name signs and directories. These streetscape elements are specified in detail in this document.

Other aspects have been defined as Elements of Diversity. Examples of features which might vary in character are step and rail lighting, uplighting of trees, fountain lighting, marquee and canopy lighting, storefront and display lighting, pavement mosaics, artwork, paving in enclosed spaces, seating ledges, movable chairs, interior seating,

litter receptacles built into ledges and walls, and business and building identification signage. These features are not described further in this document and a variety of design solutions is encouraged.

The Design Details will assist the Planning Department of the City of Bellevue in their administrative design review of Corridor-related development and will facilitate the advisory review function of the Pedestrian Corridor Committee. The Design Details also aid property owners, developers, and designers in identifying common design features of the Pedestrian Corridor and those features which afford opportunities for differentiated design treatments.

In 1998 the City of Bellevue hired Hewitt Isley to review and revise the design guidelines for the Corridor. This included revisions to specific design details outlined in the guidelines. Changes to this section of the guidelines also included the addition of several new design details relating to wayfinding, mid-block intersections, and the asymmetrical character of street. The revisions to the Design Details were developed together with the Pedestrian Corridor Committee and City Staff.

**OBJECTIVES**

The objectives of the Design Details for the Pedestrian Corridor and Major Public Open Spaces are stated below:

To create a safe, comfortable, lively, inviting and attractive place for pedestrians.

To achieve an identity and an image as a special place and focal point for downtown Bellevue and to provide visual continuity and harmony.

To reflect the qualities of a truly urban environment with intensity, sophistication, diversity, and timelessness.

To reinforce and stimulate high quality adjacent development and to complement retail uses.

To reflect the unique characteristics of Bellevue and its environmental context.

To accommodate uses such as transit, civic events, outdoor commercial use, and passive recreation.

To create a flexible system allowing for a variety of applications and modifications over time, and low maintenance.

## GENERAL CONDITIONS

For the purpose of the Design Details, the Pedestrian Corridor is divided into the following subareas:

1. Pedestrian Corridor segments bordered by Corridor walls (abutting building frontages).
2. Pedestrian Corridor segments bordered by plaza spaces without intersecting secondary paths and/or adjacent street right-of-ways (ROW).
3. Pedestrian Corridor segments bordered by plaza spaces with intersecting secondary paths and/or adjacent street ROW.
4. Pedestrian Corridor segments bordered by the Major Public Open Spaces.

In subareas 1 and 2, the streetscape elements required in this document should be located along the centerline of the Pedestrian Corridor alignment. Paving should be of brick with brick banding, as described in the paving section.

In subareas 3 and 4, the streetscape elements required in this document may be located elsewhere or omitted within the segment. The brick paving, as described in the paving section, may be combined with an accent material consisting of concrete or stone in order to allow for the development of a special identity at pedestrian “crossroad” areas and at a Major Public Open Space. Street crossings and the Transit Center are occasionally addressed by specific references.

The design selection and colors for all streetscape elements defined as Elements of Continuity and described in this document for the Pedestrian Corridor and Major Public Open Spaces (including street crossings and the Transit Center) shall be made jointly between the City Planning and Community Development Director and the Pedestrian Corridor Committee, or their respective representatives, during Administrative

Design Review. Subsequent modifications of the design and color of the above streetscape elements shall be determined jointly by the City Planning and Community Development Director and the Pedestrian Corridor Committee, or their respective representatives.

Unless other wise requested, these standards are to be followed. If a project seeks a variation from the standard, the proponent submits this request to the City with its Design Review application. The City will take this proposal to the Pedestrian Corridor Committee for review and comment, and will respond to any concerns by the Committee. The final approval of any Design Plan is by the Director of Planning and Community Development.

## 1. POLE TOP LIGHTING

### Intention:

To provide unique pedestrian-scaled, (lower level, lower intensity, more frequent, decorative), pole top lighting to create a series of light forms and to add a festive, warm, inviting, and intimate atmosphere.

### Accomplished by:

Using one pole top lighting design, with the variations described herein, throughout the Pedestrian Corridor and Major Public Open Spaces.

Maintaining light levels approximately equal to or greater than surrounding downtown streets (a maximum range of approximately one to two foot-candles). Minimizing bright spots, dark spots and glare created by pole top lighting along the length of the Pedestrian Corridor.

Providing the appearance of visual uniformity in the distribution of light and brilliance of pole top lighting along the length of the Pedestrian Corridor.

Placement of pole top lighting to work in concert with the street trees aligning with the trees, providing a strong rhythm and pattern of light along the Corridor.

Placing pole top lighting on the edges of the Pedestrian Corridor or mounted on buildings with wall-mounting brackets, when the width of the Corridor ROW requires supplemental lighting.

Using a globe-headed, pole top fixture of a design which allows for a number of globe combinations to be mounted.

Using a pole which might accommodate banners and/or flags and a base design motif unique to Bellevue.

A five globe fixture along the Corridor spaced east/west, 50 to 70 feet apart, to boldly mark the

continuity of the Corridor and to create a series of beacons to guide pedestrians. Globes should be 18 inches in diameter, and the overall height should be approximately 16 feet. Use of the five-globe fixture should be limited to the Pedestrian Corridor to provide special visual emphasis. The Transit Center shall incorporate five-globe and single-globe fixtures, as appropriate, on the platform and adjacent sidewalks.

Using a three-globe fixture in the Major Public Open Spaces, at plazas connecting to secondary paths and/or adjacent street ROWs and along secondary paths. Fixtures should be spaced 20 to 40 feet apart and may be in-line or staggered. Globes should be 18 inches in diameter and the overall height should be approximately 16 feet.

Using single-globe fixtures on poles or wall mounted on the Pedestrian Corridor where the width of the Corridor permits, in Major Public Open Spaces, and in plazas connecting with secondary paths and/or adjacent street ROW. Single-globe fixtures should be spaced 10 to 20 feet apart. Globes should be 22 inches in diameter and the overall height should be approximately 15 feet.

Providing high pressure sodium lamps in appropriate wattages.

Using etched or sandblasted, clear globes which allow for the use of refractors to improve light distribution to the walking surfaces.

Painting the above poles and bases in the same color(s) and finish throughout the Pedestrian Corridor and Major Public Open Spaces (including street crossings and the Transit Center) to provide visual continuity.

The “fruit” lighting standard base is to be used along the Corridor to provide continuity.

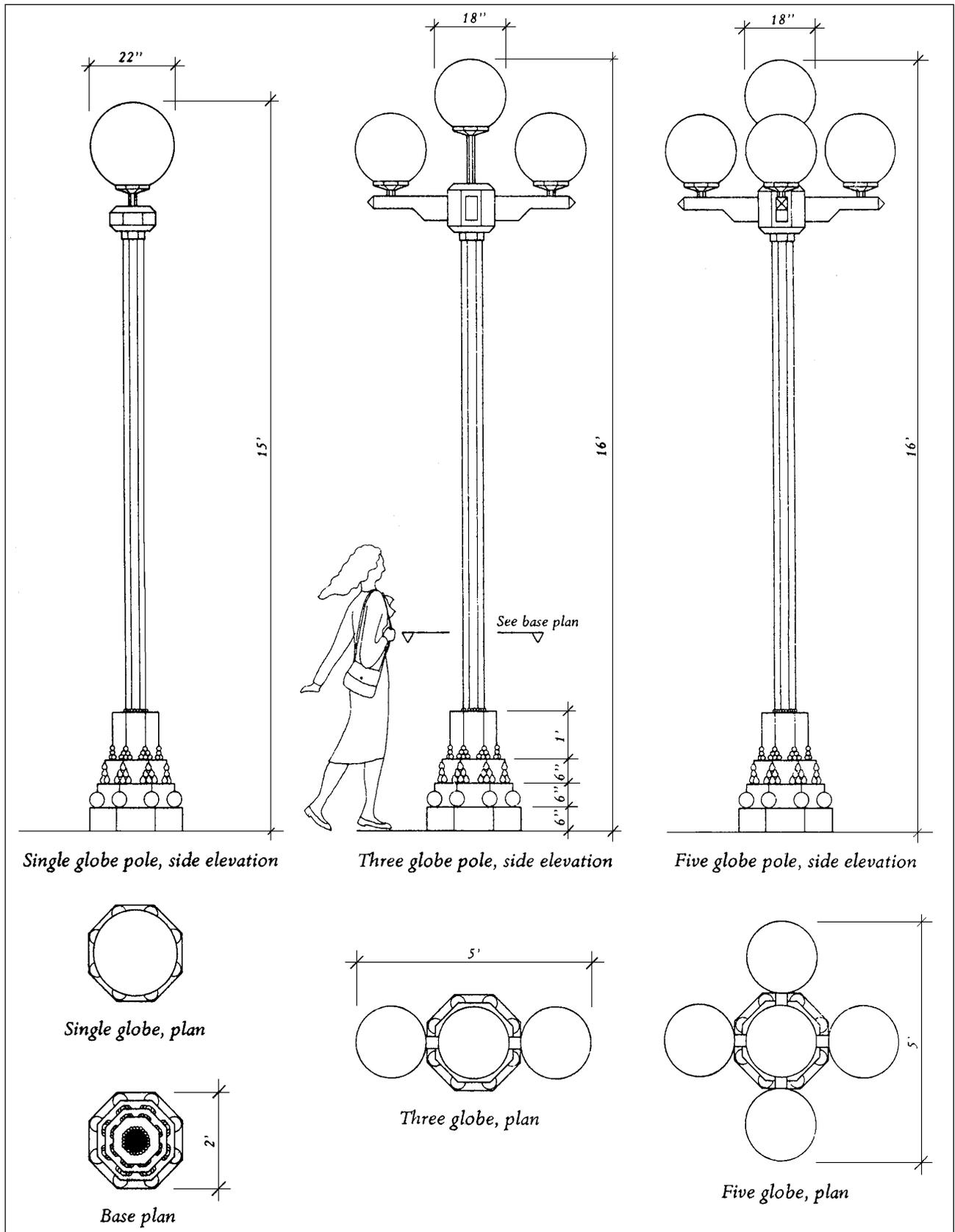


Figure IV-1: Pole Top Lighting

## 2. BOLLARDS AND BOLLARD LIGHTING

### Intention:

To mark areas where pedestrian caution is to be observed or where special features exist with bollards and low profile lighting.

### Accomplished by:

Encouraging the use of bollard lighting near crosswalks, parking ramps, driveways, fountains, and similar features.

Using bollards of sandblasted concrete or stone which are topped with a luminaire with a cast aluminum hood and base. The shape of the bollard should be equal to the unlighted bollard to provide visual continuity when used in combination. The same shape should be used throughout the Pedestrian Corridor and in the Major Public Open Spaces. A different bollard may replace the standard if approved by the City.

Bollards shall be 16 inches in diameter with an overall height of approximately 32 inches. The luminaire shall be 14 inches in diameter and approximately 11 inches in height. The metal hood and base should be painted to match the Corridor light fixture.

Lamps shall be high pressure sodium in appropriate wattages.

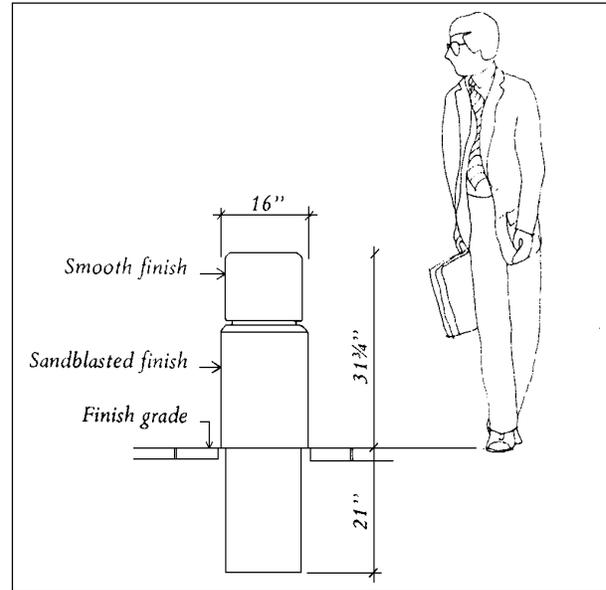


Figure IV-2: Bollard – Side Elevation

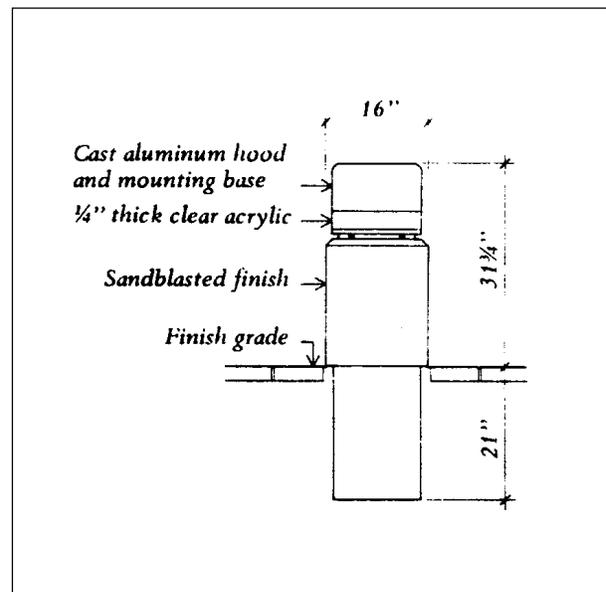


Figure IV-3: Bollard with Light – Side Elevation

### 3. PAVING

**Intention:**

To provide visual continuity along the Pedestrian Corridor and Major Public Open Spaces by using common paving materials, units and patterns. To relate to pedestrian scale and comfort, and create a warm, subtle, subdued paving surface consisting of a high quality paving material.

**Accomplished by:**

Using standard, rectangularly shaped brick pavers in three related colors as a common paving material throughout the Pedestrian Corridor and Major Public Open Spaces.

Brick pavers shall have a rough surface texture and a coarse aggregate throughout to minimize slipping.

Brick paver sizes, colors, and textures shall be manufactured to match throughout the Pedestrian Corridor and Major Public Open Spaces.

Creating a subtle paving treatment by laying a selected color of brick pavers flat in a basket weave pattern, and using flat, soldier courses in the same or contrasting color for banding and to border elements (see Banding). The basket weave pattern facilitates the accommodation of expansion joints and banding and is multidirectional in its visual expression.

Other patterns of brick pavers, such as herringbone, may be used in crosswalks or in Major Public Open Spaces and plazas connecting to secondary paths and/or adjacent street ROWs. Specifying “Medium Ironspot No. 46 (Dark blend)”, “Medium Ironspot No. 77-TK”, and/or “Coppertone-TK”, wirecut texture brick pavers as manufactured by Endicott Clay Products Co., Fairbury, Nebraska, or equal.

Using concrete interlocking pavers or brick pavers in crosswalks at adjacent street ROW.

The prescription for standard brick pavers and patterns may be changed if approved by the City. The substitution must be a unit paver is not to exceed 12"x12" in dimension. The colors of the unit pavers are to be in the same family of color as prescribed for the brick pavers.

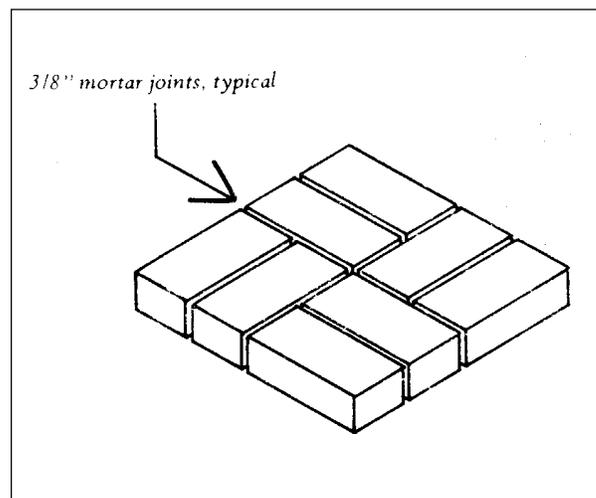


Figure IV-4: Basketweave Brick Paving – Detail

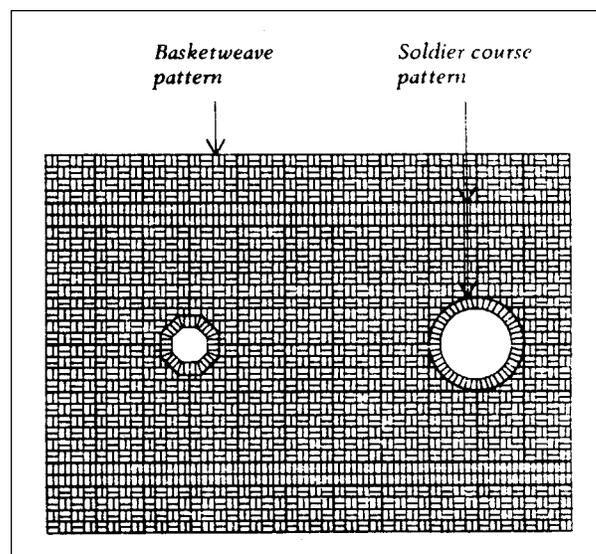


Figure IV-5: Basketweave Brick Paving – Plan

#### 4. BANDING AND BORDERING

**Intention:**

To allow for banding and bordering to create visual accents, patterns and textures to define elements and zones on the Pedestrian Corridor and in the Major Public Open Spaces. To accommodate special paving patterns at Major Public Open Spaces and similar areas.

**Accomplished by:**

Allowing banding or bordering in flat, single or double soldier courses of brick pavers, in the same color as or a contrasting color from the Pedestrian Corridor; and of brick pavers, concrete, or stone in the Major Public Open Spaces, and in plazas connecting to secondary paths and/or adjacent street ROW. Bordered elements may include light standards, bollards, tree grates, manhole covers, fountains, sculpture pedestals, kiosk bases, ledges, walls, ramps, and curbs.

Incorporating concrete curbs as specified by the City to establish visual continuity with adjacent street ROW.

Unit pavers, other than brick pavers, may be substituted if approved by the City. The pavers must not exceed 12"x12" in dimension.

## 5. STAIRS AND RAMPS

### **Intention:**

To provide visual continuity and efficient pedestrian circulation in the design of stairs, ramps, or stramps of the Pedestrian Corridor and Major Public Open Spaces.

To provide access to all areas along the Corridor and within the major Public Open Spaces.

### **Accomplished by:**

Using materials consisting of brick, concrete, or stone.

Ramps requiring handrails should not be located adjacent to building walls fronting on the Pedestrian Corridor.

Encouraging changes in grade to occur in intervals of three to eight steps to allow visual access to the next level, visually leading pedestrians along the Corridor.

Rises limited to three steps are desirable, but not mandatory.

## 6. TREE GRATES

### Intention:

To reinforce visual continuity by defining a tree grate design that contributes to pedestrian comfort and good tree maintenance.

### Accomplished by:

Using cast iron tree grates in a traditional pattern which is available in a rectilinear or square shape throughout the Pedestrian Corridor and Major Public Open spaces. A grate of a similar design with narrower openings may be used on the Transit Center platform to facilitate wheelchair access.

Requiring rectangular or square tree grates to be used in the Major Public Open Spaces, secondary paths and plazas intersecting secondary paths or adjacent street ROWs.

Requiring tree grates to be “Standard Flat” four feet by six feet rectangular or five-foot-square cast iron tree grates, as manufactured by Urban Accessories, Snohomish, Washington, or equal.

Limiting the use of metal tree guards which fasten to tree grates, to areas where frequent leaning will occur, such as on the Transit Center platform.

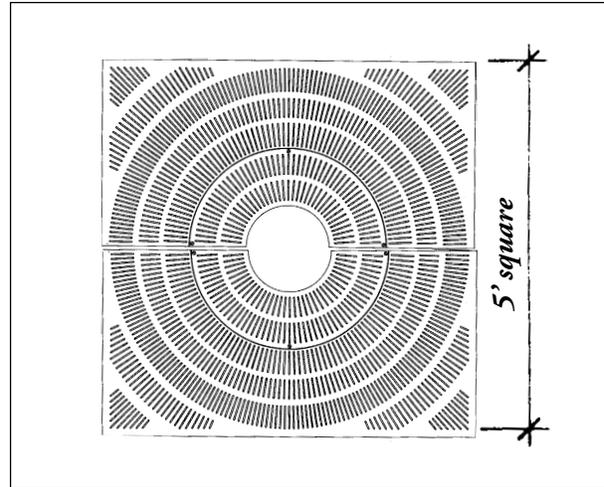


Figure IV-6: Square Tree Grate – Plan

## 7. WALLS AND LEDGES

### **Intention:**

To visually integrate walls and ledges used in landscaping into the paving pattern of the Pedestrian Corridor and Major Public Open Spaces and to provide additional seating space.

### **Accomplished by:**

Providing that walls and ledges used in landscaping should be brick, concrete, or stone to coordinate with the paving of the Pedestrian Corridor and Major Public Open Spaces.

Accommodating seating, where possible, on the top of walls and ledges. Seating should be a relatively smooth and even surface.

Incorporating lighting in walls and ledges to light walkways, where appropriate.

## 8. BENCHES

### Intention:

To ensure that the Pedestrian Corridor and Major Public Open Spaces emphasize pedestrian use by using a bench which provides a sense of place, comfort, and visual interest and harmony.

### Accomplished by:

Selecting a bench design that will perform successfully from both an aesthetic and functional viewpoint and using this bench, in the variations described herein, throughout the Pedestrian Corridor and Major Public Open Spaces.

Providing for a range of bench configurations, as follows:

1. Benches back-to-back facing the adjacent Corridor streetwall.
2. Benches parallel and perpendicular to the Corridor streetwall, encircling a street tree.
3. Benches parallel and/or perpendicular on the Corridor streetwall around a square raised planting bed.

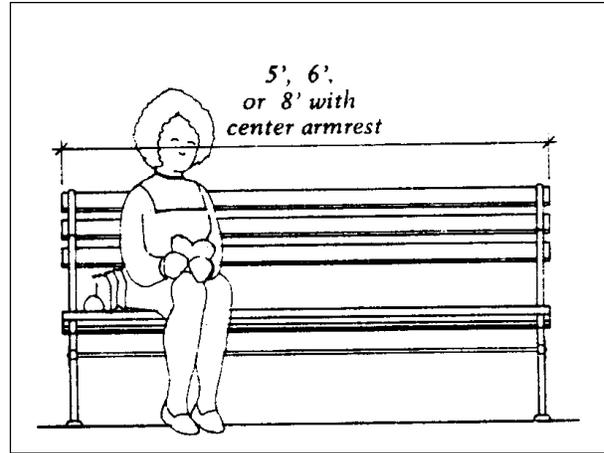
Constructing the benches of cast metal to coordinate with the cast metal used in the light fixture bases and tree grates.

Constructing the bench seats and back rests of wooden slats, with a clear finish.

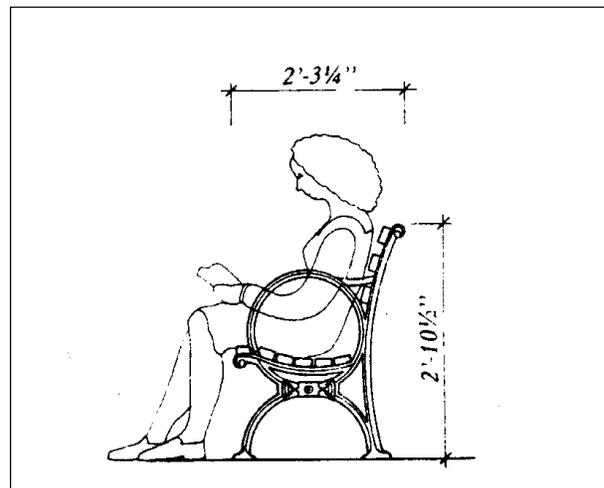
Limiting the lengths of benches to five, six, or eight feet, with a center arm rest on the eight-foot bench.

Leveling benches with spacers below bench supports, or as required.

Specifying bench Model B-16, as manufactured by Bench Manufacturing Company in Boston, Massachusetts, or equal. A different bench may be substituted within a block, if the City and all of the property owners agree to the change.



Front Elevation



Side Elevation

Figure IV-7: Benches

### 9. FIXED PLANTERS

**Intention:**

To provide raised planting beds for seasonal plantings, additional seating, and to create a pleasing pedestrian environment.

**Accomplished by:**

Allowing street trees to be planted in raised, fixed planting beds when parking structures are located beneath the Pedestrian Corridor.

Using raised, fixed beds for seasonal plantings.

Designing the raised, fixed planters to incorporate a sitting ledge if walls of the beds are between one to three feet high. Sitting ledges should be even and relatively smooth. Plantings should not interfere with the use of the sitting ledges.

Discouraging walking on the planting beds, when less than one foot in height, by using a metal rod planter fence, as used in the Bellevue Way crossing.

Allowing for well-drained soil and ease in maintenance.

Using materials in the walls of the fixed planters which blend with the paving materials (brick, concrete, and stone).

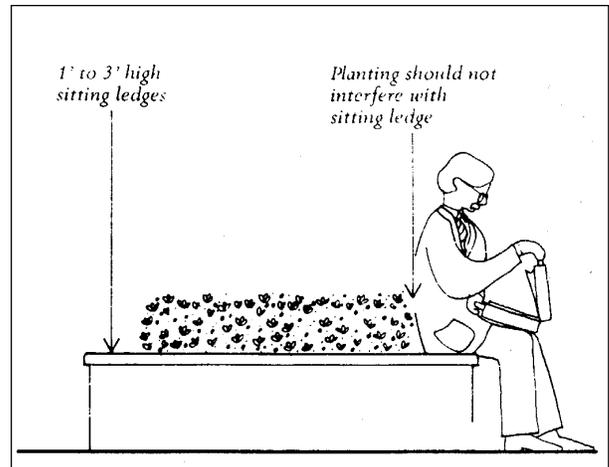


Figure IV-8: Fixed Planters – Side Elevation of Raised Planting Area with Seating

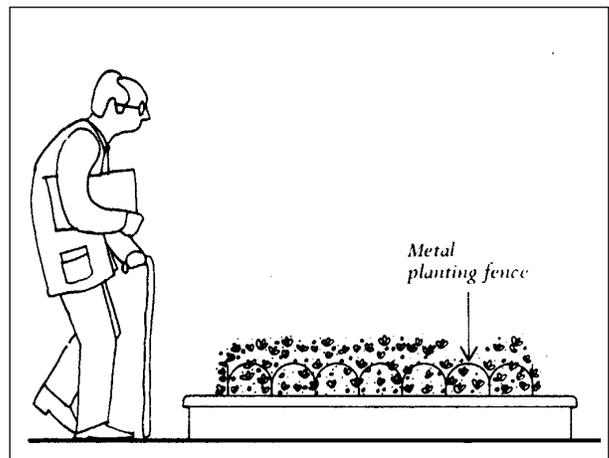


Figure IV-9: Fixed Planters – Side Elevation of Raised Planting Area Less Than One Foot in Height

## 10. MOVEABLE PLANTERS

### Intention:

To provide for seasonal plantings in movable planters within the Pedestrian Corridor to add color and aesthetic quality.

### Accomplished by:

Relating the design of the movable planters to other street furniture and paving elements and being an appropriate style for use on the Pedestrian Corridor.

Positioning the planters anywhere within the pedestrian central paving strip. They should not restrict pedestrian circulation to, and around, benches.

Allowing for plant soil to drain and for ease in changing the soil.

Allowing for plantings to be a minimum of two to three feet in height to be seen and enjoyed and not to create hazards for the visually impaired.

Selecting a narrow range of shapes for use throughout the Pedestrian Corridor.

Using movable planting pots which are round, natural terra cotta.

Inside of pots should be glazed, with one or more drain holes.

Choosing pots which are a minimum outside diameter of two feet at the opening, and a minimum height of approximately two feet. Larger pots may also be used.

Using models No. F-111 and/or No. F-106, as manufactured by Pyro Media, Seattle, Washington, or similar terra cotta pots.

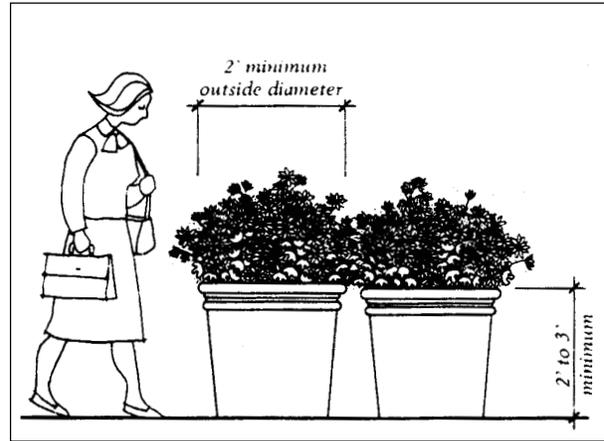


Figure IV-10: Moveable Planters – Side Elevation of Terra Cotta Pots

### 11. LITTER RECEPTACLES

**Intention:**

To provide attractive yet functional waste containers for the public convenience and to encourage brown bag lunching on the Pedestrian Corridor.

**Accomplished by:**

Using one type of design for the litter receptacle throughout the Pedestrian Corridor and Major Public Open Spaces.

Requiring the litter receptacles to be compatible with the other street furniture and paving elements on the Pedestrian Corridor and appropriate in style.

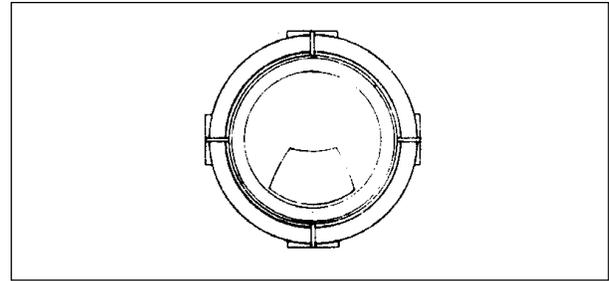
Providing that the litter receptacles are easy to locate and use.

Providing that the receptacles can accommodate being leaned on by pedestrians.

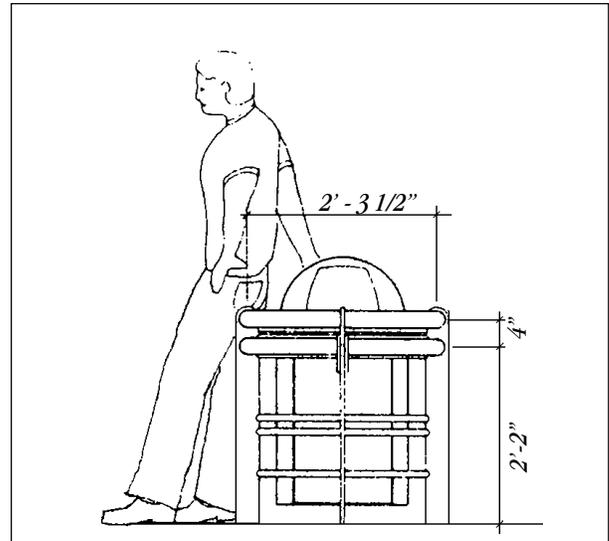
Positioning the litter receptacles near seating areas.

Allowing the receptacles to accommodate the use of standard liners, to be easy to empty, yet which are resistant to tipping, dislocation, fire damage, vandalism, and breakage.

Requiring the litter receptacle to be equal to, or similar to, the type used at the Transit Center.



Plan



Side Elevation

Figure IV-11: Litter Receptacles

## 12. DRINKING FOUNTAINS

### Intention:

To provide a visually distinctive drinking fountain for public convenience and to create visual uniformity throughout the Pedestrian Corridor and Major Public Open Spaces.

### Accomplished by:

Using a cast metal, free-standing drinking fountain of a design coordinated with the design of the pole top lighting fixture and painted to match the color of the lighting fixtures.

Locating free-standing fountains near intersections and seating areas on the Pedestrian Corridor and in the Major Public Open Spaces.

Using an octagonal, single- or double-bowl fountain with a round bronze, brass, or polished chrome liner and bubbler, with or without the image of a face. Bubbler should be mounted at the center of the bowl and should be either continuous flow or push button integral with bubbler, side push button, or foot pedal activated.

Mounting height of bowl shall be three feet, and a step should be provided to facilitate use by children.

Providing for the handicapped and other users with wall-mounted drinking fountains in a variety of designs. Wall-mounted fountains should be located on building and landscaping walls.

Drinking fountains are to meet ADA or Washington State Barrier Free requirements, whichever is applicable.

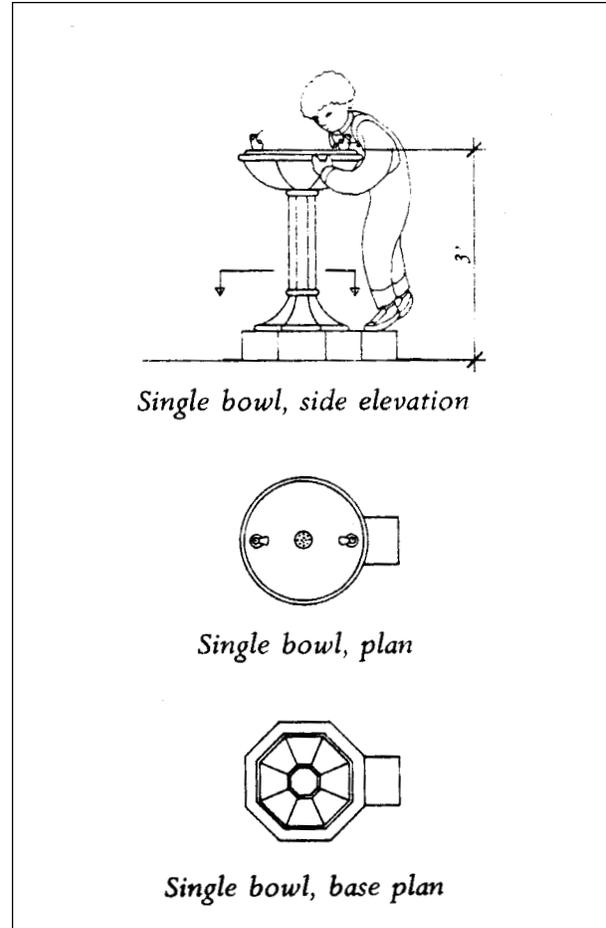


Figure IV-12: Drinking Fountains

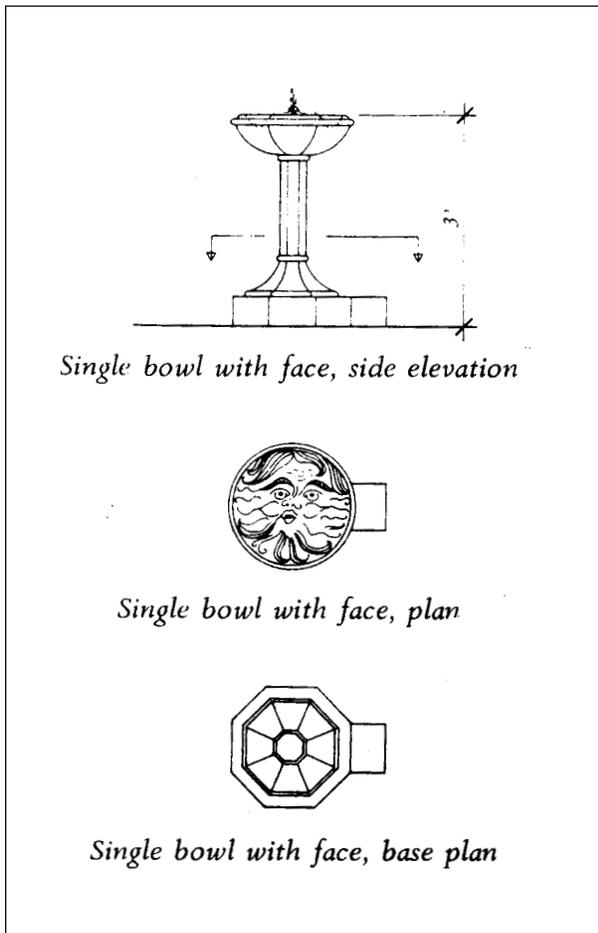


Figure IV-13: Drinking Fountains

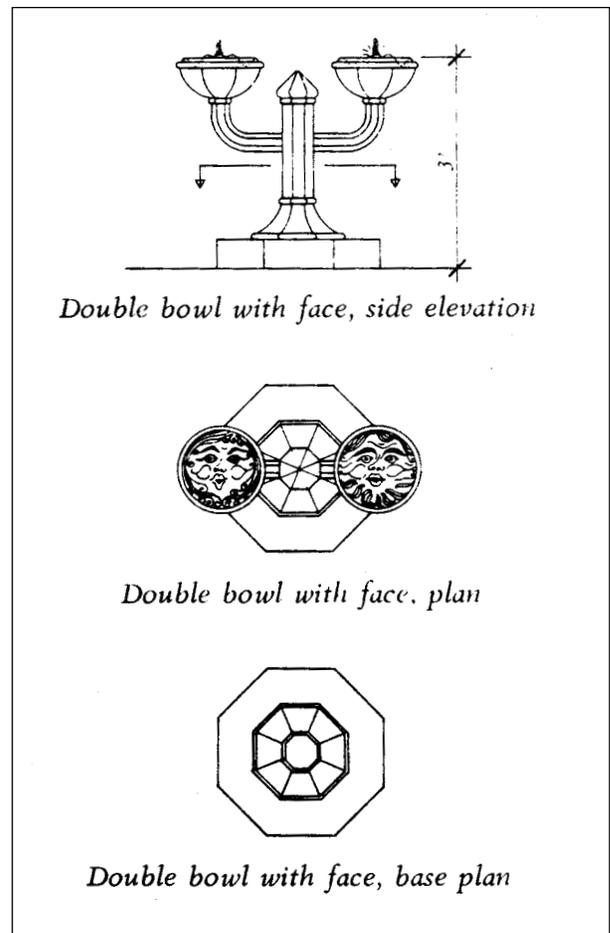


Figure IV-14: Drinking Fountains

### 13. STREET NAME SIGNS

**Intention:**

To clearly identify the streets on the Corridor and to orient the pedestrian by providing a coordinated system of street name signs.

**Accomplished by:**

Using special street name signs on the Pedestrian Corridor along its length, in lieu of standard city street signs, from Bellevue Way to 110th Avenue NE, on both sides of each cross street.

Designing these signs to be uniform in type style, size, materials, and color with a high contrast between the letters and the background.

Relating the size of each sign and boldness of the letterforms to the pedestrian, using a combination of upper and lower case letters for optimum pedestrian readability.

Positioning each street name sign within the range of a pedestrian's vision and no more than 10 feet above the ground.

Mounting each street name sign on intersection lighting poles in such a manner so as to be viewed from both sides and installed with standard hardware.

Providing adequate illumination to allow use of the street name signs during the night.

Constructing the street name signs of a material which allows easy night viewing.

Choosing materials that are durable and easily maintained.

## 14. DIRECTORIES

### **Intention:**

To provide attractive, informative, durable, and accessible general orientation directories for pedestrians on the Pedestrian Corridor and Major Public Open Spaces.

Using flat, two-dimensional graphic information, combining words and symbols, and incorporating historic information and the Pedestrian Corridor logo.

### **Accomplished by:**

Providing directories that include: the location of the Corridor on a map of downtown Bellevue, a map of the Pedestrian Corridor area showing prime locations (Bellevue Square, Major Public Open Spaces, Transit Center, etc.), a “you are here” indicator, and a listing of all office buildings and retail uses within a minimum of 330 feet of the centerline of the Corridor alignment and coded to locations on the Pedestrian Corridor map.

Considering accessibility for the handicapped when designing the directories.

Creating the directories to be viewed as a coordinated system, consisting of uniform materials and design elements.

Locating the directories near the intersections of each cross street on the Corridor: at the east side of Bellevue Way NE, the east side of 106th Avenue NE, the west side of 108th Avenue NE, and the west side of 110th Avenue NE. but not limited to these four locations.

Using a kiosk design for the directories, which is human scaled and provides weather protection. Directories on the Transit Center should be incorporated into the transit shelters.

Designing the directories to be compatible with other street furniture elements on the Corridor, including a modular approach for altering shop listings and other features.

Choosing materials and methods of construction to be compatible with other street furnishings on the Corridor and that are durable and easily maintained.

## 15. ENTRY SYMBOLS

### **Intention:**

To create the east and west threshold or gateway to the Corridor, clearly identifying the entry to a unique and very special place.

### **Accomplished by:**

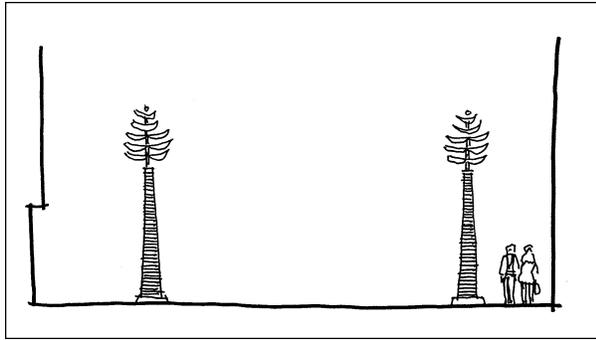
Design and implement an entry symbol to the Corridor near the intersections of Bellevue Way and Corridor, and the west side of 112th Avenue N.E. and Corridor.

Design the gateway/entry symbol based on the theme/story line or stylistic approach of the Corridor. The symbol is to be part of the overall wayfinding system contributing another level of continuity along the Corridor.

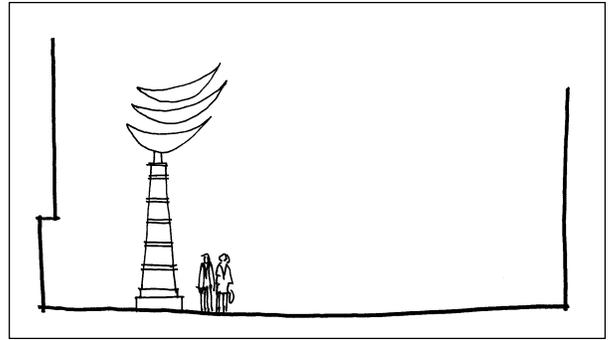
Design alternatives: 1) Structure which spans the Corridor, 2) Columns/pylons on both sides of the roadway framing the Corridor, 3) column/pylon on the north side of the Corridor.

Encourage artist participation in the design of the symbol.

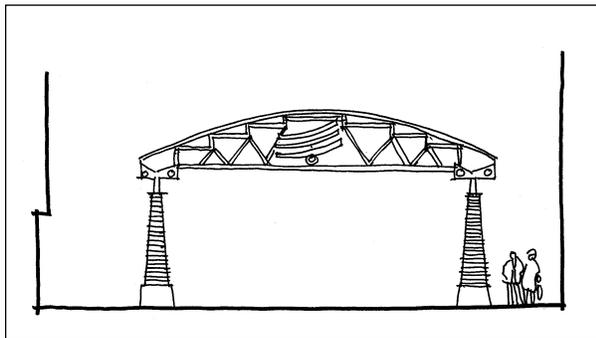
Choose materials that are durable and easily maintained.



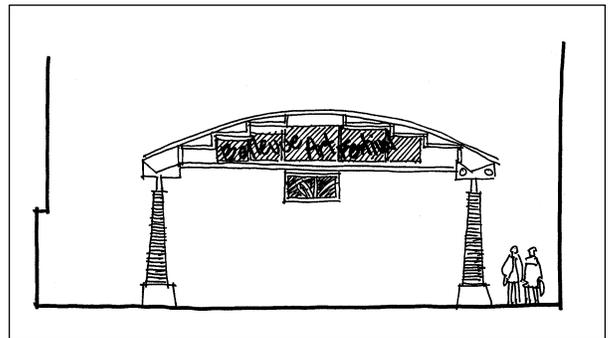
*Symmetrical*



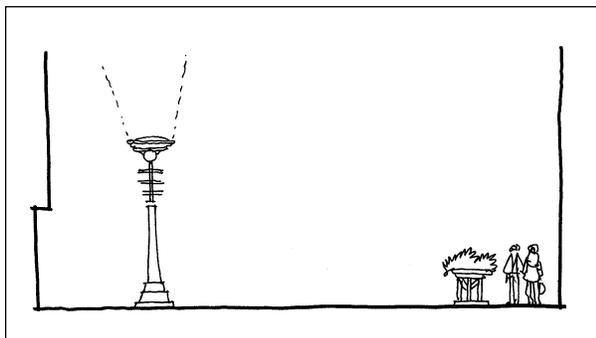
*Large Symbol at One Side*



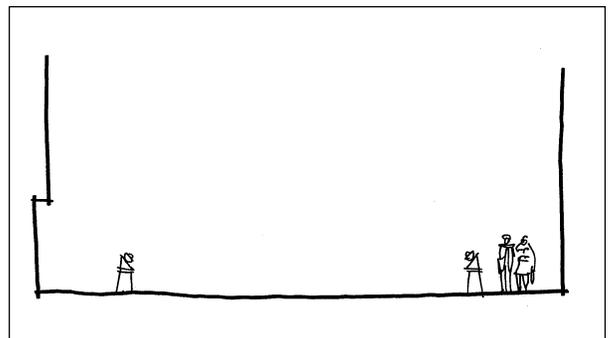
*Span Roadway*



*Span Roadway with Banners*



*Asymmetrical*



*Low Wall*

*Figure IV-15: Entry Symbols*

## 16. MID-BLOCK IDENTIFIERS

### **Intention:**

Provide a landmark element, which is part of the wayfinding system which provides scale, a reference point along the Corridor, a gathering place, and adds to the special pedestrian character of the Corridor. The identifiers are located at the mid-block intersections.

### **Accomplished by:**

Identifiers are to be based on the theme/story line developed for the wayfinding system. Encourage diversity of design, size, and materials. Water may be incorporated or the identifier may be a kinetic sculpture.

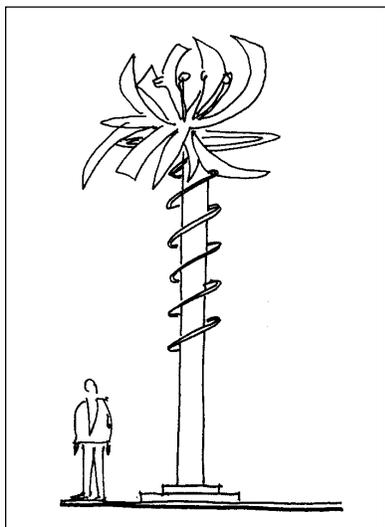
Encourage the participation of artists in the design of the identifiers.

The bases on which the identifiers are placed are to be limited to 20' in diameter. The bases are to complement surrounding pavement pattern and color. The bases may play off the light pole bases.

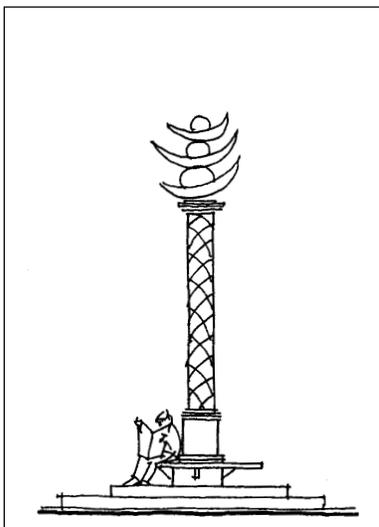
Incorporate seating at the base of the identifiers.

Choose materials that are durable and easily maintained.

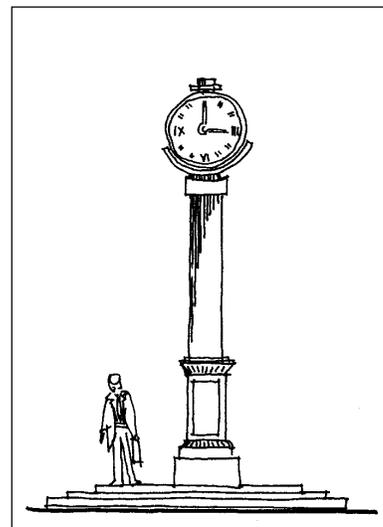
Height of identifiers should be between 20'-30'



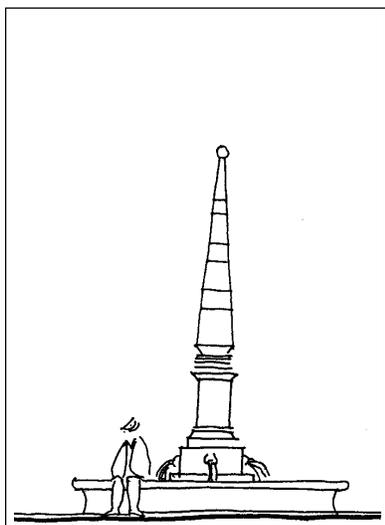
Flower Theme



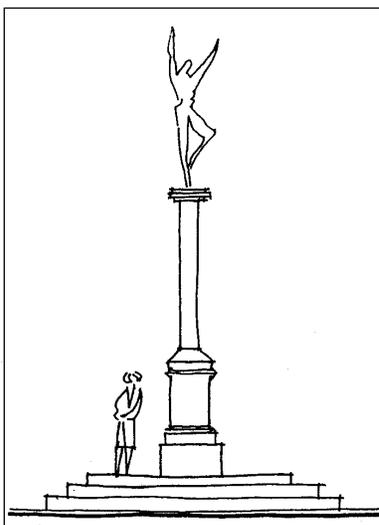
Column with Seating



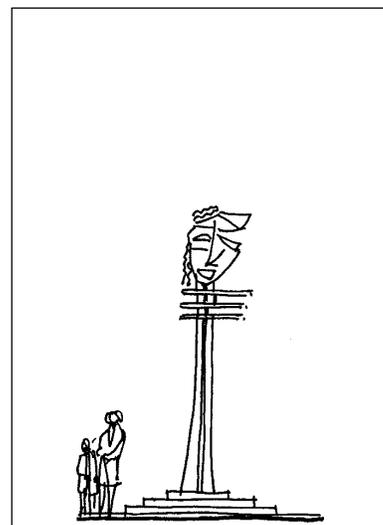
Clock



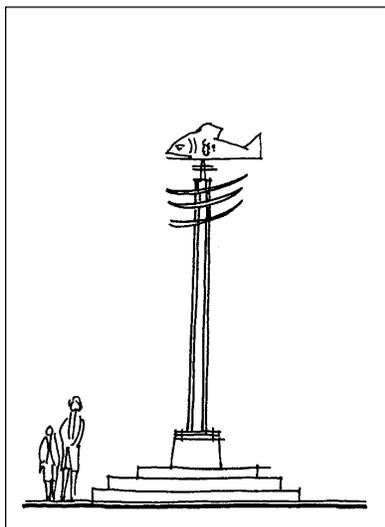
Obelisk with Fountain



Dance Theme



Theatre Theme



Animal Theme

Figure IV-16: Mid-Block Identifiers

## 17. INLAID PAVING MARKERS

### **Intention:**

Provide small icons, which are part of the wayfinding system and relate to the overall theme of the Corridor. They provide additional visual elements that enhance the pedestrian quality of the space.

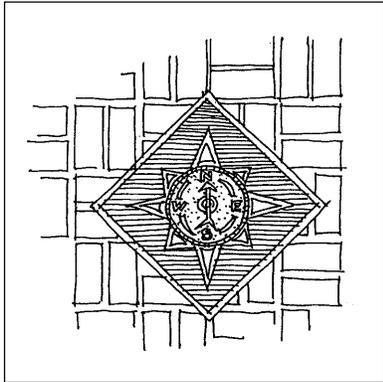
### **Accomplished by:**

Design and implement inlaid pavement markers relating to the theme, placing 5 to 8 through any given block.

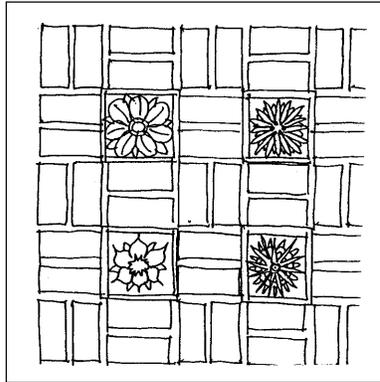
Encourage the participation of artist in the design of the markers.

Markers are to be small and flush with the surrounding pavement. The markers should fit within a 2'x2' format.

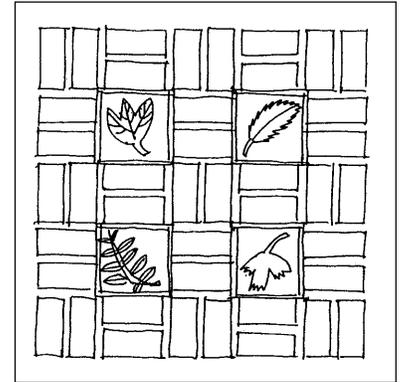
Materials can and should vary, (color, texture, metal, stone, mixed media). Slippery surfaces should be avoided.



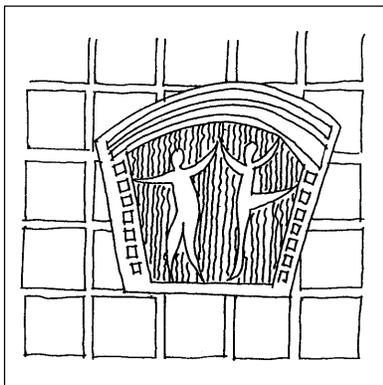
*Historical Theme*



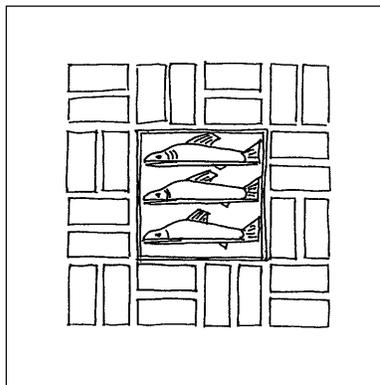
*Flower Theme*



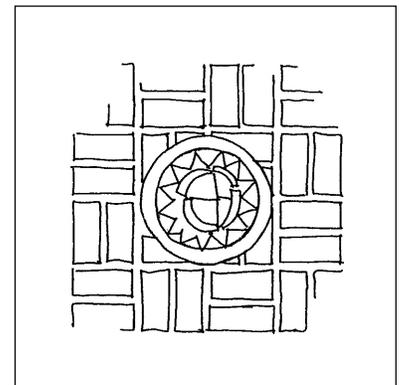
*Trees and Leaves*



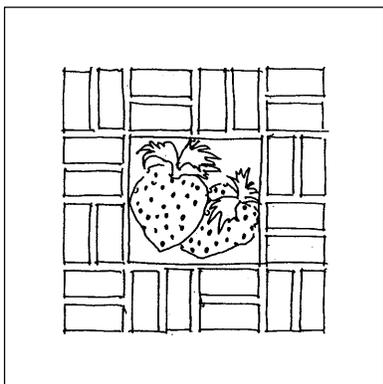
*Dance Theme*



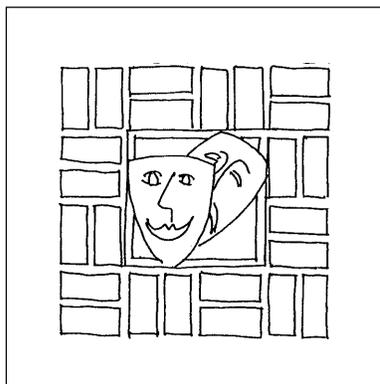
*Animal Theme*



*Sister Cities Theme*



*Fruit*



*Theatre Theme*

*Figure IV-17: Inlaid Pavement Markers*



Downtown Livability

# REFERENCE MATERIALS







# Downtown Livability

## Scoping Comments

11-29-12 Open House

### Enjoying Downtown:

#### Green Space and Parks:

- More open green spaces, necessary for walking/enjoyment (8)
- More mini parks with seating
- Complete the Downtown Park
- Ashwood Park Specific: Add benches to Ashwood Park (2)
- Add exercise equipment and walking track to Ashwood Park
- Ashwood Park needs improvements to be at standards of other city Parks
- Plant more trees to create beautiful boulevards
- Map both open spaces and green space within downtown buildings so residents can find them (2)

#### Miscellaneous:

- Diversify business around high rises
- More small businesses at street level, especially restaurants
- Define downtown Bellevue's identity
- Food/Vendor Carts can destroy look of downtown/compete with restaurants
- More sidewalk cafes/outdoor dining (2)
- Like the idea of food carts
- Too much litter
- Panhandling, homeless, litter (3), more police on foot or bikes to control these issues
- No garbage pickup on sidewalks
- Better street sweeping
- Would like a senior center downtown
- Would like a community center downtown

### Getting Around Downtown:

#### Walkability:

- More walkable pedestrian connections, pedestrian access (11)
- More awnings, skywalks, marquees and weather protection (6)
- Mid block crosswalks to improve pedestrian grid (6):
  - Need mid-block crossing at NE 9th Pl and 106th
  - Need crossing at 105th Ave NE south of Safeway

- Longer crossing times at major intersections (3)
- Shorter traffic signal phases, better signal controls (2)
- Increase wide boulevard sidewalks (2)
- Crossing flags
- Replace bad walk light signals in downtown
- Break up superblocks
- Eliminate dips in sidewalks and driveways
- More street benches to sit on

#### Bus Transit/Light Rail:

- Circulator shuttle between key downtown points (5)
- Covered bus stops and covered walkways between bus stops and shopping
- Keep transit center clean and safe
- Make transit free in downtown zone
- Ensure good access to light rail station
- Integration of transit facilities
- Less emphasis on light rail

#### Parking:

- Less parking coverage on 112th
- Discourage commuter parking – tax parking stalls in office buildings to pay for roads
- Less surface parking (2)
- More parking in downtown
- Encourage parking and walking into downtown
- Civic parking garage with free parking
- Eliminate incentives for underground parking

#### Bikes:

- Make streets more bike friendly, more bike lanes, sharrows, low speeds (8)
- Focus on pedestrian/bike/transit connections (3)
- Have some bike infrastructure on a few essential routes, an E/W and a N/S route (2)
- Underground bike and pedestrian walkway (NE 8th/Bellevue Way/NE 10th & 110th)

#### Miscellaneous:

- Discourage SOV, restrict cars in downtown (3)

### **Designing Downtown:**

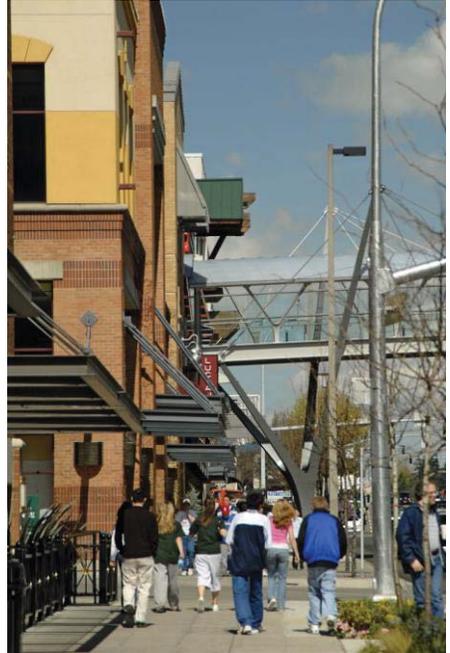
- Height/Bulk/Design/Construction
- More human scale development, informality at street level (3)
- Noise control (2), no construction noise after midnight
- Mix of heights, active building frontage
- Less large, low buildings
- Move away from glass and steel towers
- More housing and density
- More allowable uses in core (residential, mixed use)
- More green building (solar, green energy, electric vehicle charging stations, etc.)
- Upgrade key civic facilities (City Hall, library) with solar retrofit
- Maintain bonus for art incentive
- New construction may block view of existing home owners
- Would Bellevue consider completion bonds for new dev. to avoid vacated spaces?
- Consider incentives for art, bike locker storage

### **Outside Project Scope:**

- Too much city staff wasting tax dollars
- Public dock
- Linkage of land use with Meydenbauer Bay and Downtown Park
- Have a competition to create a City symbol



# Downtown Livability Critique



# Downtown Livability Critique

## AGENDA – Wednesday Dec. 16<sup>th</sup>, 2009

10:00 Introduction

- Purpose of critique
- Ground Rules / Assumptions
- Overview of Downtown projects to be reviewed:  
Why they were chosen

10:20 Critique / Answering key questions (attached)

12:00 Report out / Q&A (Lunch)

12:50 Next steps / What we will do with critique information

1:00 Adjourn and Thank you!

# Introduction

The vision for Downtown Bellevue is a dense, mixed-use urban center that has a high pedestrian orientation and range of complementary land uses. Significant planning that occurred in the early 2000s culminated in the Downtown Implementation Plan (DIP) and the current Downtown Subarea Plan reestablished this vision and provided specific direction to implement it. However, the development we see today remains largely a result of downtown Land Use Code regulations and accompanying Design Guidelines that were initially created in 1980.

With a significant amount of downtown property still available for redevelopment, as well as a number of public infrastructure projects on tap for the coming years, it is important to have an updated framework in place to ensure that future development responds to the needs of the Downtown community and the city as a whole and moves further in the direction of the stated vision.

The focus of the Downtown Livability Critique is to study existing development and collect information about how well current codes and policies reflect the vision for Downtown, what kind of experience Downtown offers, what needs to be changed or preserved to create and maintain a successful, economically and culturally healthy downtown. The critique is an initial step in the Urban Design/Livability Initiative.

## **This effort is being launched to:**

- Respond to trends in development, environment, and technology
- Enhance/maintain critical features of Downtown living such as sun and open space
- Help identify and create more “must experience” environments in Downtown
- Increase the attractiveness of Downtown as a center of national and international commerce
- Improve the public realm, i.e. activated sidewalks, mid-block pedestrian access, recreation opportunities
- Incentivize specific features that help make Downtown more livable and enhance district identity
- Ensure light rail and other mobility initiatives enhance the Downtown experience

# Assignment

City staff is in the process of conducting background research in preparation for launching the Downtown Livability Initiative. With Council's approval, this project will ultimately examine a range of urban design issues affecting Downtown Bellevue, focused on achieving superior urban design outcomes for Downtown's continued development into one of the nation's most livable urban centers.

The Downtown Livability Critique is part of the early scoping for this initiative. We are asking design practitioners familiar with Downtown Bellevue and with other national and international urban centers to engage in an informal, friendly critique of design results ***on the ground***. We will examine representative developments that have been built in Downtown Bellevue over the past decade, drawing from low-rise, mid-rise and high-rise development; office, residential and mixed use projects. At this early stage, our interest is exploring how successfully these buildings have added to the vitality and livability of the city center.

Again, our focus is results on the ground. What works well, what doesn't; what opportunities have been lost? We are not asking why or how these results—good or bad—were achieved, whether City codes and processes, the client, design team, budget, the economy, etc. is responsible. Future work will delve into these questions of causation and how better to achieve the best results.

The intent now is simply to draw conclusions about what we see and experience, with no credit (or blame) assigned. The spirit of this inquiry is one of curiosity and learning. We intend the exercise to be informal and conducted in a very positive, constructive way. Numerous factors influence design outcomes, and no disapproval of any individual's work is intended.

## Process

Participants will break into two to three teams to provide maximum opportunities for all to contribute. Each team will have a facilitator and recorder. Comments can be captured on summary sheets as well as on the site plans. Each team will have about two hours, and during that time is asked to review 3 or 4 developments, providing feedback on the questions below. At the end of this segment, all participants will re-assemble as a whole, and each group will report out with a summary of their findings. City staff will compile the results into a White Paper that will help inform the Downtown Livability Initiative.

## Projects

We have selected a range of representative developments completed since the year 2000. These exemplify what has been built, given market realities and the application of current codes and processes. The examples that follow are drawn from a variety of development types across the Downtown.

# Key Questions

The groups are asked to answer the following questions for each development, with a focus on the physical design features highlighted below that affect the livability and vitality of Downtown Bellevue and its environs.

- What works well and why?**
- What does not work well and why?**
- What opportunities were missed?**

## 1. Building form and design

Items to consider:

- Form, proportion, modulation
- Minimal impact to surrounding neighborhoods
- Visual access – tower spacing
- Rooftop features appropriate to overall height and scale
- Treatment to define base/middle/top
- Quality of materials

## 2. The public realm; building/sidewalk interface and public open space

Items to consider:

- Transparency and access – windows, entries, floor to sidewalk relationship
- Definition/enclosure of the street corridor
- Open space - accessibility, use, sense of welcome to public, amenities, furniture
- Unifying elements within variety – knitting together while maintaining unique character
- Materials , finishes, fixtures
- Pedestrian-level activity opportunities

## 3. Site Design and Pedestrian connectivity

Items to consider:

- Safe, pleasant movement through sites and Downtown
- Concentration versus Openness - dealing with superblocks and mega sites
- Weather protection
- Sun access, wind protection
- Use of landscaping – space definition, contrast with built forms, other
- Pedestrian vs. vehicles

## 4. Memorability and overall character of development

Memorable cities impart an unforgettable experience from having visited there. They have strong, clear identities. A livable city has an urban fabric rich in resources, opportunities, experiences, and quality of life. The overall quality of development and open spaces define a city's character.

- What do you see as being memorable about Bellevue?

What do you see as the overall character of Development downtown and does it contribute to the memorability and livability of Downtown Bellevue?

# Upcoming Urban Design/Livability Initiative

The following is a summary of the more significant topical areas proposed for the 2010 Urban Design/Livability initiative:

## Review of the Amenity Incentive System

The basis for development in Downtown Bellevue is a combination of an amenity incentive system and design standards through which floor area (FAR) and building height are earned by providing particular features with special public benefit. The past few development cycles have operated under this framework written in the early 1980s, so there is a good basis for examination. Issues to be considered in this analysis may include:

1. What are the appropriate amenities to best support the City's goals?
2. What is the value of amenities in relation to the floor area earned?
3. How can potential changes to the incentive system be done in a way that promotes economic development and enhances downtown livability?

## Building Form and Height

As new structures are completed in downtown, it is appropriate to examine how they are adding to the emerging Bellevue skyline. In some areas, the overall result is a somewhat bulky form with only slight variation in building heights. In other areas, the differentiation is much more pronounced with varying heights and building forms, and strong architectural interest. There may be opportunities to re-examine how maximum heights, and to a lesser extent FARs, are treated in downtown while still being consistent with the long-standing "wedding cake" framework of tiered heights from the tallest in the core to progressively shorter buildings at the edges of downtown.

1. Are any areas within downtown appropriate for increased height limits and/or modest FAR increases?
2. How do we ensure light and air to surrounding buildings and open spaces?
3. What incentives and/or guidelines ensure towers with architectural interest?

## Design Guidelines

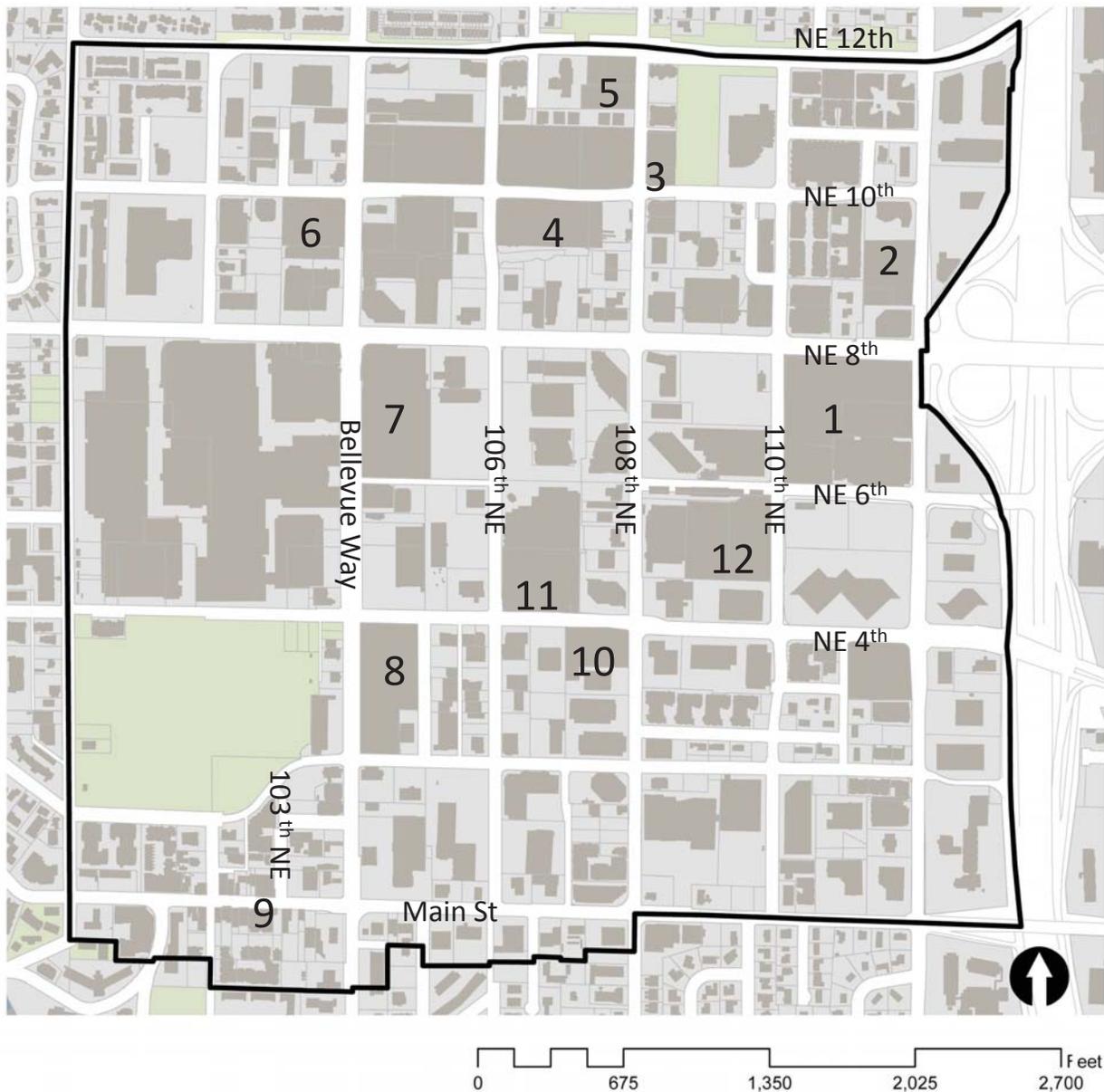
Design Guidelines currently in place address building/sidewalk relationships, Downtown overall, Perimeter Design Districts (on the edges of Downtown), Old Bellevue, the Core, the Pedestrian Corridor, and the Civic Center District. The opportunity exists to revise these to incorporate the recent Great Streets work and possibly reflect some of the district character described in the Downtown Subarea Plan.

1. Are there design guidelines that can enhance the memorability and livability of Downtown?
2. What environmental, design, or technical innovations may be appropriate to be incorporated in Bellevue's design guidelines?

## **Key Outcomes**

In preparation for the next wave of development and to increase neighborhood character and overall sense of place the following products are identified to date. This list will be revised as work progresses.

- Design Guidelines – Building/Sidewalk Relationships: Revise including incorporation of the Great Streets work.
- Amenity Incentive System: Revise and recalibrate to result in better urban form, public-focused amenities, and Downtown Charrette elements.
- FAR and Building Height: Analysis and possible limited increase to FAR limits, building heights, and transferability opportunities.



## Downtown Bellevue Project Locator

- |                      |                        |
|----------------------|------------------------|
| 1. The Bravern       | 7. Lincoln Square      |
| 2. Elements          | 8. Avalon Meydenbauer  |
| 3. Ten20 Tower       | 9. Main Place          |
| 4. Washington Square | 10. Tower 333/ Expedia |
| 5. Belletini         | 11. Bellevue Towers    |
| 6. Avalon Tower      | 12. City Center East   |

# The Bravern

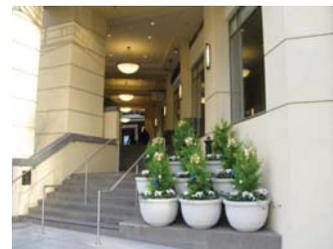
Height Available: 200, 350

Height Achieved:

200, 306, 350

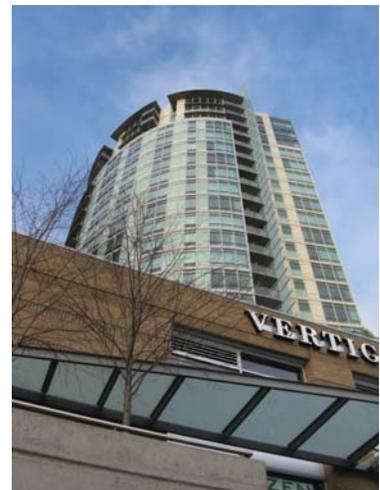
FAR Available: 6

FAR Achieved: 7



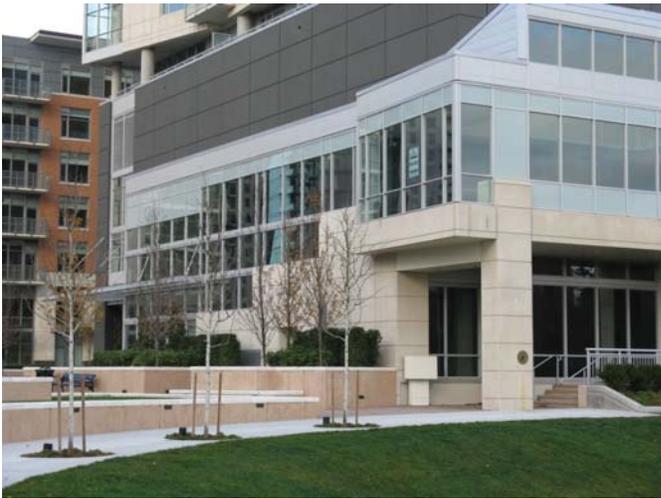
# Elements

Height Available: 220  
Height Achieved: 220  
FAR Available: 5  
FAR Achieved: 5



# Ten20 Tower

Height Available: 230  
Height Achieved: 228  
FAR Available: 5  
FAR Achieved: 5



# Washington Square

Height Available: 235  
Height Achieved: 235  
FAR Available: 5  
FAR Achieved: 5



# Belletini

Height Available: 55/90  
Height Achieved: 55  
FAR Available: 3.5/5  
FAR Achieved: 3.4



# Avalon Tower

Height Available:	245
Height Achieved:	234
FAR Available:	5
FAR Achieved:	4.8



# Lincoln Square

Height Available: 450  
Height Achieved: 450  
FAR Available: 8  
FAR Achieved: 7



# Avalon Meydenbauer

Height Available:	220/275
Height Achieved:	67
FAR Available:	5/6
FAR Achieved:	2.2/5.5



# Main Place

Height Available: 55  
Height Achieved: 55  
FAR Available: 3.5  
FAR Achieved: 3.5



# Tower 333

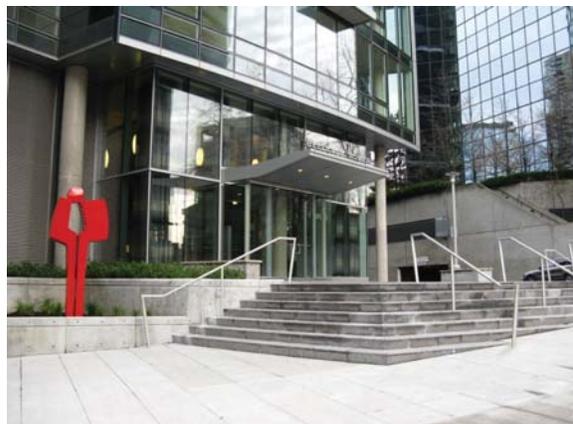
## Expedia

Height Available: 275  
Height Achieved: 257  
FAR Available: 6  
FAR Achieved: 6



# Bellevue Towers

Height Available: 450  
Height Achieved: 450  
FAR Available: NA  
FAR Achieved: 10



# City Center East

Height Available: 450  
Height Achieved: 373  
FAR Available: 8  
FAR Achieved: 4





# Downtown Livability

## Design Professional Critique Notes

12-16-09 Meeting

### Attendees:

John Savo / NBBJ  
Walt Neihoff / LMN  
Dan Foltz / Weber Thompson  
Brian Brand / Bayliss  
Dave Heater / Ankrom Moisan  
Dennis Ryan / UW

### Form:

#### The Bravern

- Based on history
- No “top” to office buildings
- Turns back on convention center
- Challenge to be a gateway – didn’t meet it.
- Good that mass is at the NE 8th 112th corner
- Scale is memorable.
- Effort made to tie buildings together stylistically
- Awkward now – will be better and part of a district in 20 yrs
- No connection to MBC
- Gateway building at 112th and NE 8th provided mass but no permeability at corner.
- Scale is memorable
- Historical reference not appropriate for a new city like Bellevue
- Too literally expresses Rockefeller Center reference/model
- Elements
- Different look than rest of Bellevue
- E2 doesn’t read as residential
- 989 smaller – more compact
- Good roof forms but is there a functional purpose
- E2 is trying too many things
- Why are bases so different? Seems like an accident.
- Eastern building hard to read as residential – when you look at a building you want to discern what it’s use is
- Two left buildings are iconic/sculptural/ risky
- 989 bldg has small (attractive) floor plate. Proportionate
- Roof – interesting at top.

- Counterpoint: Tops seem over the top for the buildings (2nd 2) better to have some thing minimalist (like Tower 333). Now three busy bldgs
- Code used to twist arms to get interesting when we really want iconic
- The three buildings are not complementary
- Top of east building more interesting if roof form had a reason (besides screening mechanical)
- Cast in concrete with curtain wall is edgier – not duplicated elsewhere.

#### Ten20

- Form falls apart on the Ashwood Park frontage
- South face doesn't take into account solar access opportunities
- Doesn't offend
- Good compositional qualities
- Neat twist at the top
- Theater doesn't express itself as a theater. Could have celebrated this more – and expressed itself and “performance” to the park more
- Punched windows adds interest to the building
- Comfortable building
- Stepback at top is good

#### City Center East

- CC uses window treatment instead of modulation – this creates an elegant, simple skyline
- Didn't achieve height or FAR
- Contemporary, sculptural building
- End of 20th century building
- Class A building
- Very attractive
- Falls apart at the base
- Reflects technology tenants
- Feels like the “old city” at the base
- Tried to have a base
- Subtleness of the form
- How did they get by with that transparency of glazing?
- Base didn't add detailing, refinement at the bottom/ped level
- Base would make sense if tower were full height – 450
- Base reflects an older architectural approach and '80 Bellevue.
- Base better than Expedia “no base” approach
- Tower expression using only curtain wall results in good articulation without lots of modulation
- Base has no soul or warmth
- Appropriate for technology tenants

#### Belletini

- Really dislike building
- Overdone
- Good urban scale at corner
- Form and scale generally good
- Good relief to towers
- Nice articulation of facades
- Good level of textures
- Reaching for historical details has no place in COB.
- Detailed cornices should be discouraged.
- Faux details and weird balconies should be discouraged.
- Form and scale are good transition for edge of downtown

#### Lincoln Square

- Street level and base work
- Ped cover is good
- Pretty well modulated

#### Expedia

- Generic tower – modulation needed

### **Public Realm:**

#### The Bravern

- Everything is internalized
- Limited weather protection on the sidewalks
- Not about the sidewalks
- Especially disappointed on 110th & NE 6th frontages..
- Creating the courts was a good idea
- Good mixing of modes in the courts
- Could have more outward facing uses
- No weather protection
- Needs a tie to Meydenbauer Center
- Hard to navigate within project
- Interior may feel like a public space in the future
- City needs to grow up around it
- Needs weather protection and relief along sidewalk
- Visual interest offered by small show windows is bleak
- 110th is bleak
- NE 6th should have been more pedestrian friendly and better pedestrian amenities
- Circulation system is confusing but over time people will figure it out
- Texture at base but no life
- Building turns its back on the sidewalk

## Elements

- 112th is treated as back door of project but is most visible side. Should have green – “City in a Park”
- Good 2-3 level transparency for E2
- Ground floor uses inwardly focused, can take away from street edges
- Some good aspects – elements of surprise
- Atrium and plaza have good potential
- Reference to Opus/DT Seattle project of other use in/near parking areas, serve as buffer maybe to 112th
- Atrium entry is not clear as the front door
- No front door to this project – not intuitive
- 112th/ 405 is front door of city – buildings read as back door. A garden edge would have been more successful and reflect “city in a park”

## Ten20 Tower

- Not clear why small park is there right now.
- Should have had building brought forward.
- Park works well with Black Box
- Not clear how the green space relates to other open spaces
- Plaza on north not likely to be used by public
- Arcade works well – for an arcade
- Critical to coordinate plaza with future master plan of open space/

## Ashwood

- BB hidden
- Circulation to BB could have been a lot better
- Ashwood Park Plaza feels private right now
- Plaza is very controlled
- Need transition to larger park development from plaza
- Good treatment south of plaza
- Will upper level n plaza space be used by other city users?

## City Center East

- Plaza feels like a private space
- Barrier formed between BTC and plaza
- No real activities in the plaza
- Could have been a kiosk building in the plaza
- Drop-off not functional
- Plaza does not but should relate to BTC. Unfortunate that it is almost fully blocked by garage venting
- Lobby does not draw you in

## Belletini

- Landscaping creates a barrier to the retail space
- Good urban scale at the corner
- Plantings up next to building – under canopies is weird. Good to bring green space toward the building but not successful here.

### Lincoln Square

- Cave to hotel opening – not friendly
- Landscaping is great. Good to separate peds from street if parking can't do it.

### General Comments

- 3D models good way to review projects
- Overall good depth of projects but they don't hang together
- General interesting skyline
- Is there an intermediate level for "top of building?"
- Modulation and material change doesn't create visual interest. Rhythm and materials instead of modulation. Modulation can mean difficult to build which means in turn, difficult to maintain, therefore not durable or timeless design/construction

### Open Space:

- Buildings and landscape must all be tied together somehow
- Should not have to rely on a sign to tell people a space is public. Design should do that.
- Need "discovered" courtyards
- Focus on:
  - Nature
  - Light
  - Outdoor performance Space
  - Space for rec activities (bocci, chess, lunch time soccer, tai chi)
  - Get people out of buildings
  - Trail systems – tree walk, art walk, etc
  - Weather protection
- NO SMOKING
- Encourage movement
- Diversity of space types
- Quantity / Quality / Location
- Distribution and proximity

### Floor Plates:

- Max floor plates should be relaxed
- Formula - keep FAR – designers can be flexible for site conditions
- FAR based codes
- Orientation of Towers
- Encourage linearity and E/W orientation
- Use modeling to consider view corridors. Encourage but don't require view protection

### **Base-Middle-Top:**

- Termination more important than “top”. End result of a top may not look natural.
- Base taken care of – distinct base happens because of uses. This relates to use changes – functionality. The top is more about profile.

### **Modulation:**

- Fracture mass to create vertical expression. Bellevue Towers is successful. It is laminated together.
- Important to not mandate modulation or stepping of towers and floor plates.
- Workforce Housing Missing
- Doesn't work in needle towers
- Need bonuses for this

### **Public Open Space:**

- More usable if not super visible
- Let it happen more freely w/in blocks
- Wash Sq townhouses open up open space

### **Sustainability:**

- Add more permeable surfaces downtown
- Rain gardens
- Be progressive
- 2030 challenge
- Mandate for all so there is a level playing field
- Has a “cool” factor and will attract young professionals. This will change the demographic of downtown. They are not drawn to faux historical references

### **Uses:**

- Projects should be designed to accommodate retail in the future if we believe that retail would be successful in that location at some time. In the mean time we should allow interim uses until retail is viable.
- Need strategies to attract mom & pop and less national chain retail/services
- Convenience retail needed
- Superblock
- Elements is a good example of what is happening to superblock development.

Development is being internalized. This takes energy from the sidewalk but does add elements of surprise.

- Breaking down the edges of a superblock is a challenge. It can be difficult to activate them if there is not a call for a lot of retail.

### **Parking:**

- 1521 Building in Seattle (Opus) has above grade parking that is buffered by studio space.
- Parking should be built to also be flexible – other uses in the future.
- Above grd parking should have f/f heights that accommodate conversion to other uses.
- On –street parking is good (Old Bellevue)
- Change parking requirements to reflect sustainability

### **FAR:**

- Biggest tool to drive sustainability – as an incentive or a basic requirement
- Entrepreneurial coordination with private developers (John Su) should be continued

### **Incentives:**

- More interested in seeing sustainable features than interesting building tops. Interesting top not a particularly desirable outcome.
- Develop a fund system for workforce housing and other amenities a developer might not put in a particular project (like a needle tower)
- Super bonus for community uses:
  - Fire station
  - Community Center
- Extra incentive for small retail (can't be assembled for national chains for example)

### **Quality of Materials:**

- Better materials last longer, easier to maintain, will look better longer, and more sustainable

### **Streetscape:**

- Wide, generous sidewalks good
- Wait to cross street is too long
- Peds need to be and feel like the priority

### **Livability / Memorability:**

#### The Bravern

- Whole project is tied together
- Scale of project
- A little ahead of its time
- Retail counterpoint to Bellevue Square
- A Developer driven project

#### Elements

- Building as sculptural element
- Has to do with like/dislike factor
- Cool factor
- Attractive to Microsoft type
- “Alternative” building

#### Ten20

- Not memorable at all
- Good background building
- Plaza and ground flr space add to memorability

### **Competent Themes:**

- Framework has worked well over the years - height, density, use perspective
- Admin DR is positive – more predictable, professional. Consider adding an advisory board
- Need 21st century incentives for projects to accomplish certain public goals (Ten20, Park Expansion). This is an ideal time to do this.
- Use tools to help diversity downtown.
- Overall city landscape plan doesn't tie together based on projects
- Some projects seem like they're looking back (Belletini) rather than forward
- Need help in channeling developers in the “right direction” – contemporary expectations
- Focus on larger, long-term public goals
- Form should express function
- Details matter (Belletini – not good)
- Larger, longer objectives should be considered

- Form should express function (purpose)
- Weather protections should be provided everywhere now because ultimately everywhere will be pedestrian oriented
- Individual parts/pocket parks don't tie together as a discernable whole
- Encourage entrepreneurial

**Report out:**

- Aspirational statements handled in DR – can't mandate with code. i.e. “tall buildings should be slender, not squat”
- Bellevue needing to make quantum leap in sustainable and active living design
- Lots of goods/retail – but the pedestrian experience needs to improve
- Connect the dots for pedestrians with separate hierarchy from car:
  - Street crossings
  - small open spaces
  - Art
  - Shorten signal times to enhance ped experience/movement
- Focus on car is still king
- Scale needs to be broken down
- Convenience retail needs to be added (need to be able to buy a hammer)
- More mom & pop not just national chains
- Incent workforce housing
- Build out mid-block connector system (should be robust before LRT is operational so people can reach the stations via different routes)
- Hits - density
- Process - Admin process positive; predictable, professional negotiation
- Going in right direction
- City Center example of

**Miscellaneous:**

- sustainability, energy efficiency
- buildings sap energy from the street when activity is internalized
- NE 6th lost opportunity
- Need to focus on experiential aspect

**Bellevue design should be forward looking:**

- Brand Bellevue
- Be functional
- Be sustainable

### Design Guidelines:

- Guidelines with expression of stylistic result desired. This will help architects manage clients
- Detail and texture are meaningful and lasting. Applied elements such as a cornice are not
- Need to explain what city wants but also why and push forward
- Don't force generic modulation or stepping
- How do you ensure elegance in flexible design guidelines?
- Go for contemporary expression

### City in a Park:

- Retail and walkability but retail doesn't work everywhere. Focus it and then landscape edges where it doesn't work.
- Incentives are Key
- If it must make business sense and developers get a carrot they will do it.
- Must be a level playing field
- LEED/ Living Building / 2030 Challenge
- Lead into the future – push forward – show courage
- Superblocks –
  - Difficulty presented by superblocks coming to fruition. Front doors not clear when all frontages are a major street
  - Use vurnorf model of City Center East mixing cars and people and not the Lincoln Square model of speed ramp to garage.
  - Big blocks can be an opportunity for mini energy districts. Water handling/ treatment, cisterns, swales, water reclamation and reuse

### 5 over 1:

- Difficult building type but it isn't going away.
- Belletini: scale good but detailing is bad

### Height:

- It should go up
- Proportionate to floor plate

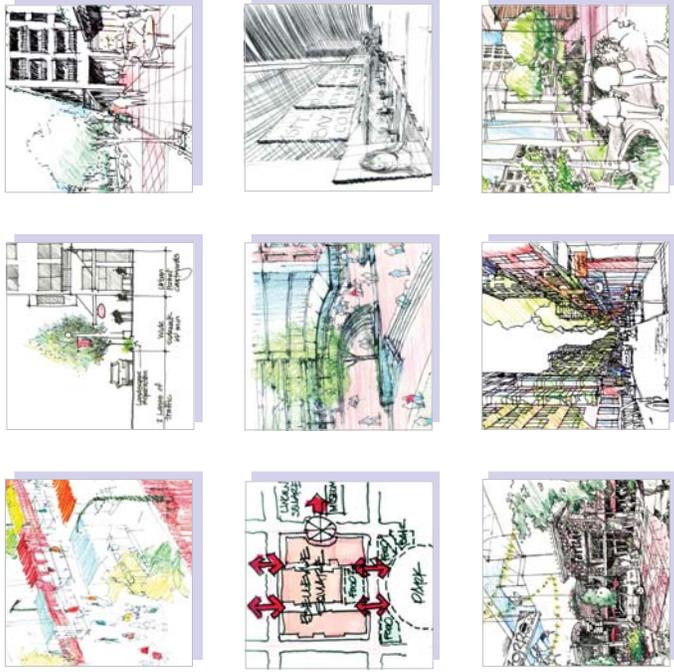
**Floor Plate:**

- Will be a result of sustainable depth for access to natural light and ventilation. Will bring energy use down and create better building form due to taller/more slender buildings.

**Design Process:**

- When a client is very determined and intense it is hard for an architect to drive the result. For Bellevue a more contemporary expression should be driven through design guidelines.
- COB needs to articulate the forward vision and answer “why” so architects can do a better job realizing the vision for developers
- Downtown reads as an extension of Bellevue Square
- National chains with absentee ownership creates a lack of attention to finer grain and reflection of unique place/location.
- Old Bellevue – Main Street very successful.





# Great Ideas for a Great Place

## Downtown Bellevue CHARRETTE SKETCH BOOK

Spring 2005



City of Bellevue



# Acknowledgments

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## Special Thanks

Special thanks to the Bellevue Arts Museum for hosting the pre-charrette orientation, and to Equity Office for providing the Key Center Atrium for the charrette event.

## Video Production

Mei Dayton-Celt of Bellevue TV filmed the pre-charrette presentation and portions of the charrette event. A multi-part series will run in early 2005 on Channel 21. Rick Abe from MediaPortfolio captured footage and interviews from the charrette and has made a short highlights video available at [www.mediaportfolio.com](http://www.mediaportfolio.com).

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# Introduction

## GREAT IDEAS FOR GREAT PLACE

On September 17 & 18, 2004, over 50 local design professionals volunteered for an exciting two-day design charrette—a creative, intense workshop—focusing on the continued evolution of Downtown Bellevue as a great place. The public was invited to observe the charrette and offer comments on works in progress. This sketch book inventories all the ideas generated at the charrette for a more in-depth review. A public process to occur in 2005 will select the best ideas and determine how to put them into action.

The purpose of the charrette was to build on the “Great Place Strategy” in the Downtown Subarea Plan by further defining strategies for creating livable and memorable places. A key challenge posed to the participants was how to use urban design to differentiate Bellevue’s downtown neighborhoods.

### Great Place Strategy

To remain competitive in the next generation, Downtown Bellevue must be viable, livable, memorable and accessible. It must become the symbolic as well as functional heart of the Eastside Region through the continued location of cultural, entertainment and regional uses located in distinct, mixed-use neighborhoods connected by a variety of unique public places and great public infrastructure.

The results of the charrette build on Downtown Bellevue’s past successes and strengthen the role of urban design for future downtown



development. The best ideas may be carried forward in several ways, including helping the city plan and design public projects, updating zoning code incentives for downtown, and providing developers with ideas they might incorporate into the design of their projects.

### Participants

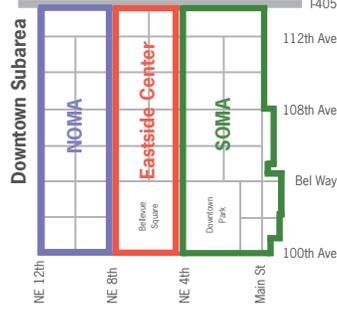
Charrette participants included local design professionals from the architecture, landscape architecture, planning, and urban design fields. The charrette was organized by the City of Bellevue and Bellevue Downtown Association in conjunction with a steering committee of principals from local design firms. There was a sense that the design community would be interested in “giving back to the community” as well as the opportunity to “participate in a unique Eastside challenge.”



### Team Assignments

The charrette focused on the 410-acre Downtown Subarea, extending from NE 12th Street to the north, the Main Street area to the south, 100th Ave NE to the west, and I-405 to the east.

Six teams participated in the charrette. Teams were comprised of a mix of design professionals from different firms. This added greatly to the collaborative, non-competitive atmosphere at the event. Two teams were assigned NOMA (the area north of the mall), two SOMA (area south of the mall), and two Bellevue’s Eastside Center (area between NE 8th and NE 4th Streets). Teams focused primarily on their assigned area, but also paid special attention to relationships with adjacent areas.



## Givens

There were two primary givens that framed the generation of ideas at the charrette. The first was the



transportation framework developed as part of the Downtown Plan. This framework identified future transportation needs, both for access to/from downtown, and travel within downtown. The results from the Downtown Plan include the following:

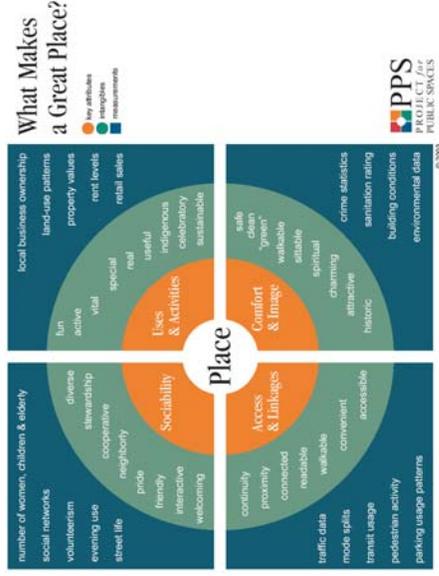
- Retain the existing downtown grid, including the superblock street system.
- Transit service will double (with ridership quadrupling) over the next 20 years. In addition, some type of downtown circulator system will come on line when it becomes viable.
- Develop a one-way street system on 106th Ave (northbound) and 108th Ave (southbound), between Main Street and NE 12th Street.
- Link NE 2nd and NE 10th Streets to 116th Ave (traveling across I-405), and connect these streets to I-405/ SR-520 via the collector distributor system.

The second given was the existing zoning envelope for downtown development. The charrette was not intended to revisit the underlying structure of the downtown “wedding cake,” the maximum allowable floor area ratios

and heights in each zoning district, or the menu of permitted uses (though participants were encouraged to identify some uses as highly desirable or preferred).

## Pre-Charrette Orientation

An orientation and keynote address open to the public took place three days prior to the charrette on September 14. The objectives of the charrette and charrette program were reviewed in detail. Noted urbanist Fred Kent, from the non-profit group Project for Public Spaces, provided his take on how to enhance urban environments like Downtown Bellevue to create a truly great place (see graphic below for key attributes, intangibles, and measurements). He provided examples from around the world, with a focus on the need to balance good design with well-thought programming of public spaces.



A key concept that Mr. Kent used to inspire charrette participants was the “Powers of Ten.” This concept works at multiples levels. It states that truly memorable

**regions** have ten great attractions, truly memorable **cities** have ten great attractions, truly memorable **downtowns** have ten great attractions, and each truly memorable **neighborhood** within a

downtown has ten great attractions. He challenged the design professionals to use this as a measuring stick as they assess the current conditions of each neighborhood in Downtown Bellevue. If there weren't ten great attractions, he encouraged the group to come up with a vision for what they might be.

## DESIGN PRINCIPLES

A set of design principles included in the Downtown Plan were given to the charrette participants to help them become familiar with the details of the overall vision for Downtown Bellevue.

### A Series of Downtown Neighborhoods

Downtown should develop as a series of distinct, mixed-use neighborhoods (or districts) that each capitalize on their locations and unique identities. Downtown was originally laid out in a manner that is defined by its street grid and system of 600-foot superblocks. Rather than one homogenous downtown, the superblocks may be grouped together to form nine districts. Each district has clear boundaries formed by major arterials that extend outside of downtown. Each district is pedestrian-friendly in size, and easily traversed in a ten-minute walk.

By connecting the center row (Bellevue Square to Convention/Civic) into a common district, the symbolic and functional center of the Eastside is created. This is Bellevue's Eastside Center. To the north and south of the Center are three districts each that make up NOMA and SOMA (the "north of mall area" and "south of mall area"). Some of the districts such as Ashwood and Old Bellevue already have clear identities. The identity of others is not as clear and will evolve over time.

### Street Hierarchy

There are a range of street types in Downtown Bellevue and each may be placed in a hierarchy based on their connectivity, cross-section, and current and future auto

**Downtown Neighborhoods**



**Street Hierarchy**



volume. The pedestrian-bias streets of NE 6th and the portion of Main Street in Old Bellevue are unique in Downtown Bellevue. The NE 6th Street Pedestrian Corridor shifts from west to east from a limited auto-access street, to no auto access, to a transit mall. Old Bellevue has a two-lane Main Street with on-street parking, small retail shops, and high levels of pedestrian activity which provides a signature look and feel. At the other end of the spectrum are auto-bias streets. They will provide pleasant pedestrian environments, but are currently intended for high vehicle volumes, and will be required to serve similar and increasing volumes in the future. Bellevue Way, NE 4th Street, NE 8th Street, and 112th Ave NE are examples. The streets in between auto-bias and pedestrian-bias are said to be neutral. They will evolve over time to serve both pedestrians and automobiles in a manner that reinforces the adjacent land uses and travel demands of future development.

**Signature Streets**



### Signature Streets

The functional aspect of Downtown Bellevue's streets can be refined around a set of signature themes. There are three types of signature streets: Bellevue Way, Main Street in Old Bellevue, and the NE 6th Pedestrian Corridor are identified as Shopping Streets. The others are 106th Ave NE as Entertainment Avenue, and 108th Ave NE as downtown's Commerce Avenue. These streets will help tie downtown together with complementary uses and design elements. All these streets will continue to support multiple uses, with their unique identities evolving over time.

## Parking

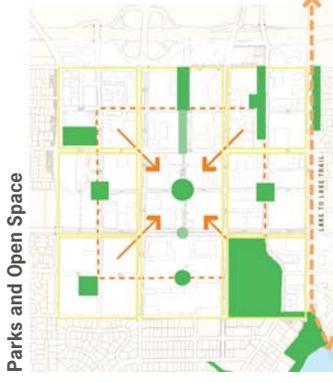
Bellevue is characterized by limited amounts of short-term parking in areas that are not always convenient to key points of access. This principle of parking speaks more to what's not and needs to be, than what is and how to shape it. Three factors will significantly influence parking. First, each district needs to supply short-term parking. Short-term parking facilities will be located in proximity and accessible to retail destinations. Second, on-street parking (possibly off-peak) needs to be provided in appropriate locations. Third, a "park-once" strategy needs to be developed as an overlay for all parking considerations. These three factors will significantly influence the street character of Downtown Bellevue and enhance the ability of people to move between districts conveniently.

## Mid-Block Pedestrian Crossings

The scale of downtown's 600-foot long superblocks provides a challenge in creating a fine-grained pedestrian environment. In select locations, there may be opportunities to improve pedestrian mobility across arterial streets with signalized mid-block pedestrian crossings. The graphic to the right shows the concept for a series of these connections and the impact they could have as a system. The precise location and number of these crossings will be determined by the design of adjacent superblocks, consideration of traffic flow, and the quality of the pedestrian environment. Mid-block crossings would not be appropriate on auto-biased streets, but may be possible on auto-neutral streets and pedestrian-biased streets.

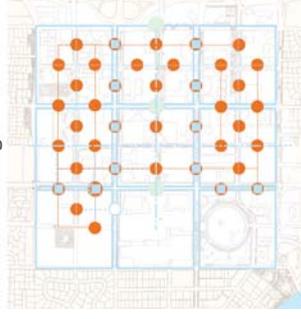
## Parks and Open Space

Open space provided by both the public and the private sectors is a key component of being a livable city. Throughout Bellevue, open space punctuates, accents, and highlights the fabric of a city. Parks, recreation, and open space amenities within downtown will function as a system that reinforces the notion of Bellevue as a "city in a park." Some pieces, like the 20-acre Downtown Park, serve a regional need, while others are oriented to a particular neighborhood or district. The character of downtown's amenities will vary from quiet, contemplative and green, to crowded, high energy and more architectural. The system will be tied together in a planned and deliberate way by creating connections along downtown's sidewalks and mid-block connections.



Parks and Open Space

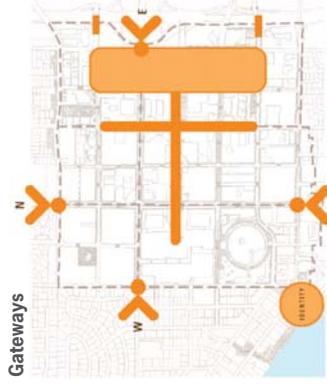
## Mid-Block Crossings



Mid-Block Crossings

## Gateways

The graphic to the left shows gateway and identity opportunities for Downtown Bellevue. Major gateway locations are on Bellevue Way from the north and south, and on NE 8th Street from the east and west. Major identity opportunities are shown at Meydenbauer Bay, the Pedestrian Corridor, the ridge along 108th Ave NE (the highest point in downtown, with some of the tallest buildings), the properties directly visible from I-405, and new bridges to be constructed across I-405 at NE 10th Street and NE 2nd Street.



Gateways

## Memorability

Viability, livability, and memorability are all important in the evolution of Downtown Bellevue. Memorability can take many forms. It can be recognizable in the form of

some local icons, such as the Space Needle, or Pike Place Market. It can be more generally recognized as an identity, such as the neighborhoods of the Back Bay or Beacon Hill in Boston. It can be established as a multi-use district, such as Fisherman's Wharf in San Francisco. For Downtown Bellevue, it is important that memorability be authentic—a genuine expression of Bellevue's place and adjacency to those geographic and natural features most strongly identified with the Pacific Northwest. This may ultimately take either a symbolic or physical form. It may be a combination of a lot of little things, as well as some significant grand gestures.

## SPECIFIC TASKS



The charrette began with a walking tour of each team's assigned area. Each team was asked to respond to the following tasks over the remainder of the two days. The deliverables for each team itemized in this sketch book include a narrative of their approach followed by a host of supporting sketches and plans.

### 1. Neighborhood Image and Identity

Develop a design intent to characterize an image and identity for each neighborhood. Describe and illustrate how this intent will be expressed. For example, with preferred ground-level uses, landscaping, sidewalk treatments, menu of urban amenities, etc.

## 2. Special Places

Identify a general location, design concept and function for ten special places within each neighborhood (e.g.



Ashwood, Old Bellevue).

These may include both public spaces and quasi-public spaces on the interior and exterior of buildings and within the right-of-way. Explore how these places will contribute to downtown livability and memorability, and to the specific user profiles who might experience these places.

## 3. Pedestrian Linkages

Explore how linkages for the pedestrians can be enhanced, both along and within superblocks, and how these tie together the special places identified above.

## 4. Signature Streets

Explore how the portion of each signature street is addressed within a given study area, and how this relates to the balance of street along the corridor.

## 5. Iconic Elements

Explore how the use of iconic elements within downtown will contribute to livability and memorability for the city.



# Team One: The 10th Street Posse

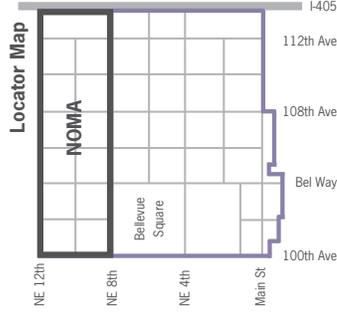
## PARTICIPANTS

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- Mike Upston, Bellevue Staff Resource
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- Don Watts, Baylis Architects



## ASSIGNMENT

Team One (along with Team Two) was given the assignment of NOMA, or the “north of mall area.” NOMA is the northern third of Downtown Bellevue, delineated by NE 12th Street to the north, NE 8th Street to the south, 100th Ave NE to the west, and I-405 to the east.



## APPROACH

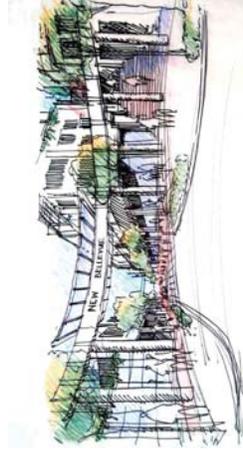
### Neighborhood Identity

Our approach to the NOMA district has been to conceptualize it as three very distinct neighborhoods strung as beads along the common thread of NE 10th Street. Although not currently apparent, the planned connection of NE 10th Street to I-405 and the Overlake Hospital area will change the character of this street in significant ways, and drive change in these three neighborhoods. We have tried to identify what is working well in these neighborhoods, to understand the future urban design and development opportunities within them, and make specific recommendations related to these redevelopment opportunities. In each case, we have tried to reinforce unique neighborhood character. We had the following perceptions of the character of the three neighborhoods. The names are the ones we gave them during the charrette.

- **Ashwood Park:** A medium density, predominantly residential neighborhood in the northeast corner of downtown.
- **Hidden Gardens:** Also known as City Center North, it is currently characterized largely by older low-density commercial uses. It is poised to undergo significant large-scale redevelopment in the near future.

- **New Bellevue:** Also known as Northwest Village, this older low-rise commercial area has an opportunity to grow into a unique, fine-grained, and street-oriented retail district.

As reflected in our name, “The 10th Street Posse,” we have focused on NE 10th Street as a Signature Street (in addition to those identified in the Downtown Plan). The pending connection to I-405 and eastward is anticipated to result in a large increase in traffic volume. At the same time, there will be a major change to certain blocks adjacent the street as the result of planned development currently on the drawing boards. Careful attention to developing a quality streetscape along NE 10th Street that will be comfortable for both pedestrians and motorists is needed. Developing this streetscape character will be a central challenge in the NOMA area, particularly between 108th Ave NE and Bellevue Way (due to adjacent development). West of Bellevue Way, our team’s vision for NE 10th Street is that of a very different street. We believe the intersection of NE 10th Street and Bellevue Way should create a “gateway” of sorts. This vision imagines a quieter street, designed to



Great Ideas for a Great Place

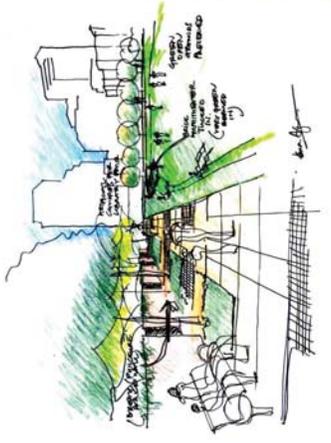
support new boutique retail along its borders rather than functioning only to move volumes of cars through the neighborhood.

### Iconic Elements

There are a few items within NOMA that could be considered iconic, from a neighborhood identity perspective. Not all of them are buildings: the George Tsutakawa fountain and the sequoia tree on NE 8th Street at 106th Ave, the public sculpture in front of the library, and McCormick Park are notable examples. These elements provide image and identity to the area, as well as creating a sense of history. They should be preserved. There are a few iconic buildings, the Regional Library and Bellevue Place, that help create a sense of place and community. Others are planned, such as a new performing arts center and a black box theatre. These will serve to draw people into downtown during evenings and weekends, thus supporting other businesses. They will also help create the sense that Downtown Bellevue is about more than only jobs and retail. One urban iconic element that has not yet developed in NOMA: the neighborhood watering hole (i.e. good bars). Such after-hours gathering places are essential to creating a sense of community, and should be encouraged in future development, where appropriate.

### “Ashwood Park” Neighborhood

Of the three NOMA neighborhoods, “Ashwood Park” in the northeast corner is the furthest along the road towards achieving an enduring form. Substantial amounts



of attractive mid-rise residential buildings, often with ground-floor commercial uses, have been completed in recent years. This dense residential development gives the neighborhood its essential character. It is supplemented by public uses such as the Regional Library and Ashwood Park. Other existing and planned cultural amenities, including the Doll Museum (existing), a small black box theatre planned next to Ashwood Park on 106th Ave NE, and future community center located in the park will bolster the growing sense of neighborhood identity. A small neighborhood restaurant district is emerging along NE 10th Street near the library. Major change within this neighborhood in the future will be powerfully influenced by the planned connection of NE 10th Street through to I-405. Major elements that can positively affect future change in this neighborhood include:

- **Mid-block connectors:** Existing mid-block pedestrian connectors should be clearly identified and linked to

create an attractive and inviting pathway system within the interior of the individual large superblocks. A separate street signage system should be established and implemented marking these paths.

- **Improvements to Ashwood Park:** Ashwood Park has the opportunity to be improved to better serve the neighborhood, including such features as a community center at the north end and a small amphitheater built into the grade at the southeast corner. A proposed private black box theatre is being planned at the southwest corner and should be supported and integrated into the public space in a compatible way. Each of these public serving uses on this block; the library, the park, the theatre, the community center, the doll museum, should work as a harmonious whole. This list should include the on-going use of the park as a playing field for youngsters.

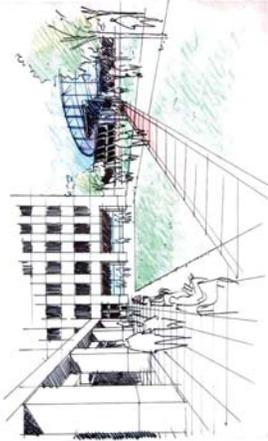


- **On-street parking:** On-street parking should be encouraged wherever possible in this district. In particular, this includes parking on the one-way couplet streets (106th/108th Ave) and where it already exists.
- **Neighborhood commercial:** Commercial uses other than sit-down restaurants are absent in this neighborhood. Opportunities to introduce other uses as more families move in, such as day care,

convenience stores, pubs, and similar uses will create a more liveable residential neighborhood.

### “Hidden Gardens” Neighborhood

This NOMA neighborhood has the greatest possibility of the three for major redevelopment as an intensive high-rise mixed-use district. Several existing developments such as Bellevue Place, and Palazzo I & II, have begun to develop this character and will remain in place principally as they are now. However, over a span of the next decade or so, the majority of parcels within the four superblocks are poised for redevelopment. The Wasatch Superblock in particular, bounded by NE 8th Street, 106th Ave NE, NE 10th Street, and 108th Ave NE is currently under one ownership and prepared for change. In general, land assemblage is an obstacle to cohesive major redevelopment with the Bellevue superblock environment. However, in this neighborhood the Wasatch Superblock and portions of others are amenable to this approach.



Another exciting opportunity in this neighborhood is the potential for the creation of a performing arts center adjacent to the Bellevue Place and the Hyatt Regency Wintergarden, on the south side of NE 10th Street. Our group’s name for this neighborhood, “Hidden Gardens,” comes from this existing feature and the potential for extensive and coordinated development of semi-public open spaces within private property throughout these large blocks as they redevelop. This open space network can become a priceless public amenity including mid-block connectors, retail plazas with seating, restaurants and outdoor café seating, hidden seating nooks, building forecourts and other “gems” to be discovered. Critical elements included within this district are as follows:

- **Performing arts center:** The proposed Performing Arts Center Eastside located on NE 10th Street is an exciting opportunity. Along with other existing and proposed cultural elements nearby, such as the library, the Wintergarden, the Doll Museum, the proposed Ashwood Park community center, and a proposed black box theatre, the vision of an emerging cultural district along NE 10th Street becomes apparent.
- **Associated courtyards and passages:** Development of a series of courtyards and passages with the development of the performing arts center. These should be integrated into Bellevue Place and the Wintergarden as well, helping fulfill the promise of public places. Variety should be emphasized, and they

can be designed to support commercial or public venues as forecourts, entryways, or connections.

- **Semi-public seating and café space:** These uses should be encouraged on the sunny north side of NE 8th Street and NE 10th Street between 106th Ave NE and 108th Ave NE. One local model for such a lively street-oriented development is Ascadia Bistro on NE 10th Street at 110th Ave NE.
- **Wider sidewalks with landscaping:** Sidewalk dimensions need to be appropriately scaled at the larger mixed-use developments, along with landscape strategies to buffer pedestrians from vehicular traffic. Sidewalks 14 feet wide are suggested as a minimum dimension, while sidewalks as broad as 18 feet or even more may be appropriate in certain places. Existing street configurations will require that some of this sidewalk area be yielded from private property. Density bonuses can be an appropriate compensation. Objections regarding private ownership of land should not be permitted to derail the development of a pedestrian-oriented streetscape.

- **Vibrant street level development:** Streetlevel character for major development such as the Wasatch Superblock needs to be carefully planned and reviewed with each street front having its own character and personality related to the particular street it abuts.

- **Retain existing character:** Existing character in the neighborhood is provided by subtle elements such as the George Tsutakawa fountain and the sequoia tree located at the corner of NE 8th Street and 106th Ave

NE. Observation and public inquiry may reveal others. These elements should be protected and preserved, to the degree that is feasible and appropriate.

- **Dual character of NE 10th Street:** The character of NE 10th Street as a major vehicular arterial, as well as a friendly pedestrian environment needs to be developed. The use of a “green street” approach whereby landscaping and street furniture are designed to create buffers between fast moving traffic and pedestrians should be encouraged. A higher quality of streetscape development is desirable here, including lighting, pavements, etc. Also, small “pocket urban courtyards” at intersections or mid-block can create variety and enhance the “green street” concept. These can be spaces developed to support commercial uses, not necessarily publicly-maintained space.



- **Pedestrian crossings at NE 10th Street and NE 12th Street:** Connections across NE 10th Street and NE 12th Street must be considered that will allow the “Hidden Gardens” neighborhood to be relatively

porous to pedestrian traffic between downtown and the single family neighborhoods to the north, including the mid-block connectors. Connection through the Wasatch Superblock may want to move diagonally from northeast to southwest to direct pedestrians to connect to the entertainment and shopping streets.

- **One-way couplet:** Reconfiguration of 106th Ave NE and NE 108th Ave NE as a one-way couplet allows for the reduction of street width and addition of on-street parking.
- **Major grocery/drugstore development:** A site located on one of the two blocks between NE 10th Street and NE 12th Street may be more appropriate for the redevelopment of the QFC and Bartell Drugs as a mixed use building as discussed in the “New Bellevue” neighborhood. This location creates a more central location with better vehicular and pedestrian access. It also brings neighborhood retail services within walking distance of the Ashwood Park residential neighborhood.

### “New Bellevue” Neighborhood

We have chosen the name “New Bellevue” for this neighborhood at the west end of NE 10th Street as a deliberate reference to Old Bellevue and the existing character of Main Street. Our vision is to assimilate that character of fine-grained boutique/specialty retail, and distribute it throughout the district as the older ubiquitous retail plazas redevelop. New Bellevue is an area where the 600-foot superblock can begin to give way to smaller blocks through coordinated action during redevelopment, allowing for development of narrower

pedestrian friendly streets which define the smaller blocks. This has already begun to spontaneously occur as parking access lanes develop into mid-block crossings.

This neighborhood thus retains and strengthens its character as a vibrant and thriving retail district which acts as a transition zone between the high-rise downtown and the adjacent residential neighborhoods. It develops a unique charm with antique stores, art galleries, bookstores/café,



restaurants, and day spas that intermix with neighborhood-serving retail such as grocery stores and drugstores. Development of a new two to three acre urban park and plaza at the northwest corner of NE 10th Street and 102nd Ave NE helps catalyze this development, attracting a weekend farmer's market, and other programming opportunities, particularly during the summer and fall months.

The neighborhood can include mid-rise residential uses above retail areas providing a variety of living units ranging from affordable rentals to luxury condominiums.

Key components in the future development of this neighborhood are as follows:

- **Development of narrow "retail alleys" at the mid-blocks:** These alleys would have the flavor of a street in an older European neighborhood allowing for cars, pedestrians and loading, constructed with special

pavers, speed-control devices, and bollards separating pedestrians and vehicles. These alleys are envisioned as small scale, intimate streets, with very low vehicle speeds. The entire street would be a continuous level, from building face to building face, with no curbs. Use of unit pavers to create street character is highly recommended. Outdoor kiosks, dining, and vendors, as well as closing off several alleys for weekend public markets, would be encouraged.



- **A new urban park/plaza:** A new two to three acre urban park and plaza at the northwest corner of NE 10th Street and 102nd Ave NE with programmable hardscape would serve as a neighborhood "square" providing opportunities for a farmers market and festivals, a tot lot/playground and passive park.

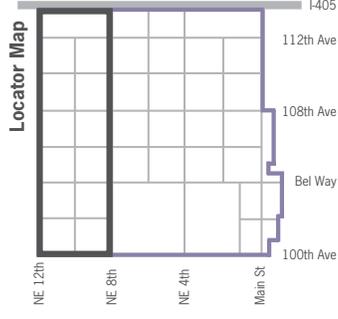
- **Development of a city-owned parking garage:**

This would be a city-sponsored, subsidized public/private parking structure including retail storefronts at ground level. Availability of this structure would reinforce a "park once" strategy.

- **Changing character of NE 10th Street:** NE 10th Street should narrow west of Bellevue Way allowing on-street parking during non-peak traffic times and reinforcing its pedestrian character. Planted medians, islands, elimination of turn lanes and street furniture should delineate arrival at a different and distinct neighborhood.

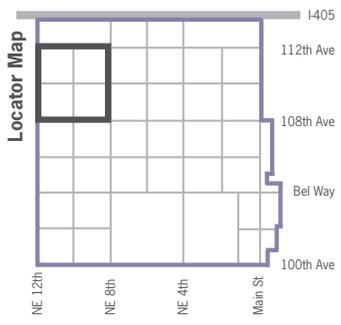
- **Eventual redevelopment of QFC:** QFC is an amenity to this neighborhood but eventually will and should redevelop as a mixed-use block with housing and underground parking, retail at street level and office/residential on upper floors. It may make sense for the city to promote redevelopment of it at a site to the east, perhaps between NE 10th Street and NE 12th Street, as identified in the "Hidden Gardens" narrative.

# NOMA Neighborhoods



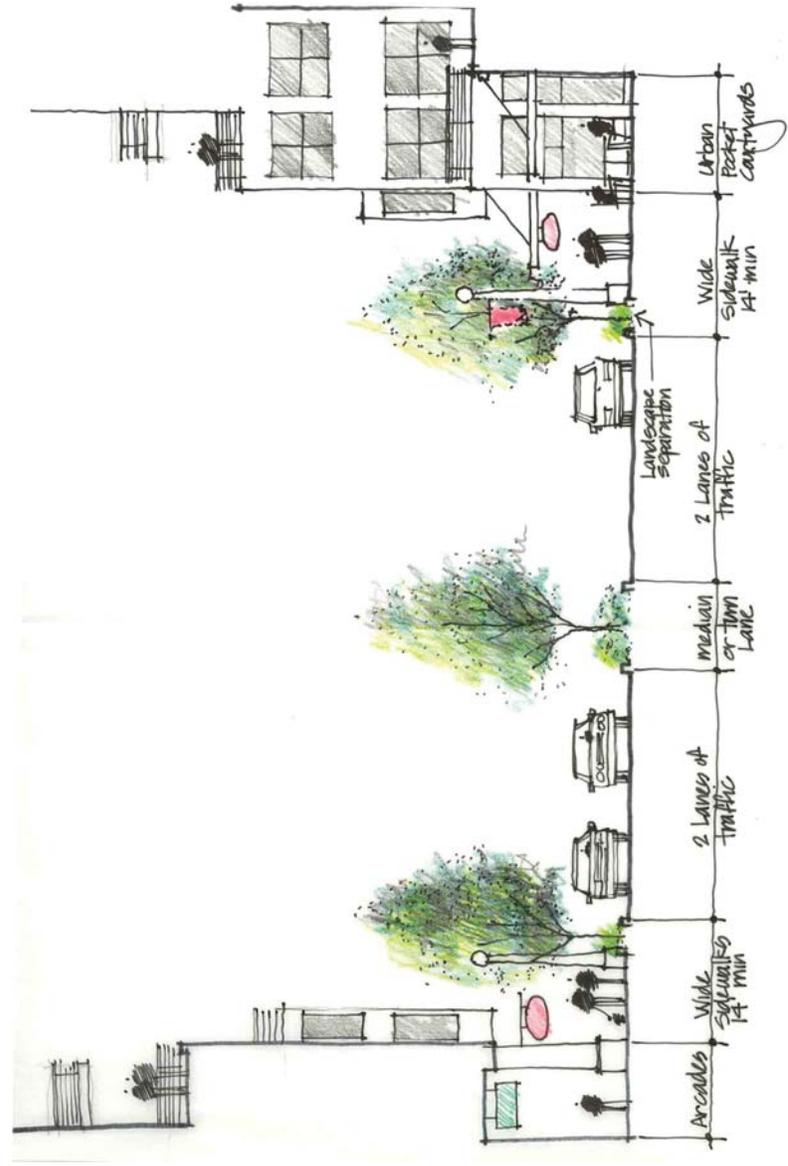
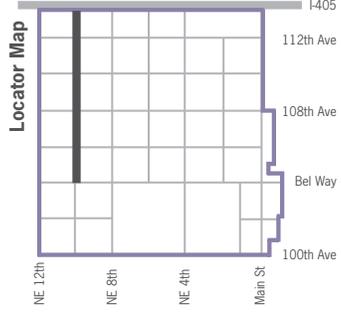
NOMA includes three very distinct neighborhoods strung as beads along the common thread of NE 10th Street.

# "Ashwood Park" Neighborhood



This neighborhood is well on its way to achieving an enduring form and includes substantial new and attractive mixed-use residential, much of which includes ground-floor commercial.

# NE 10th Green Street

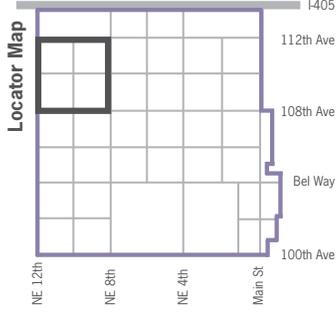
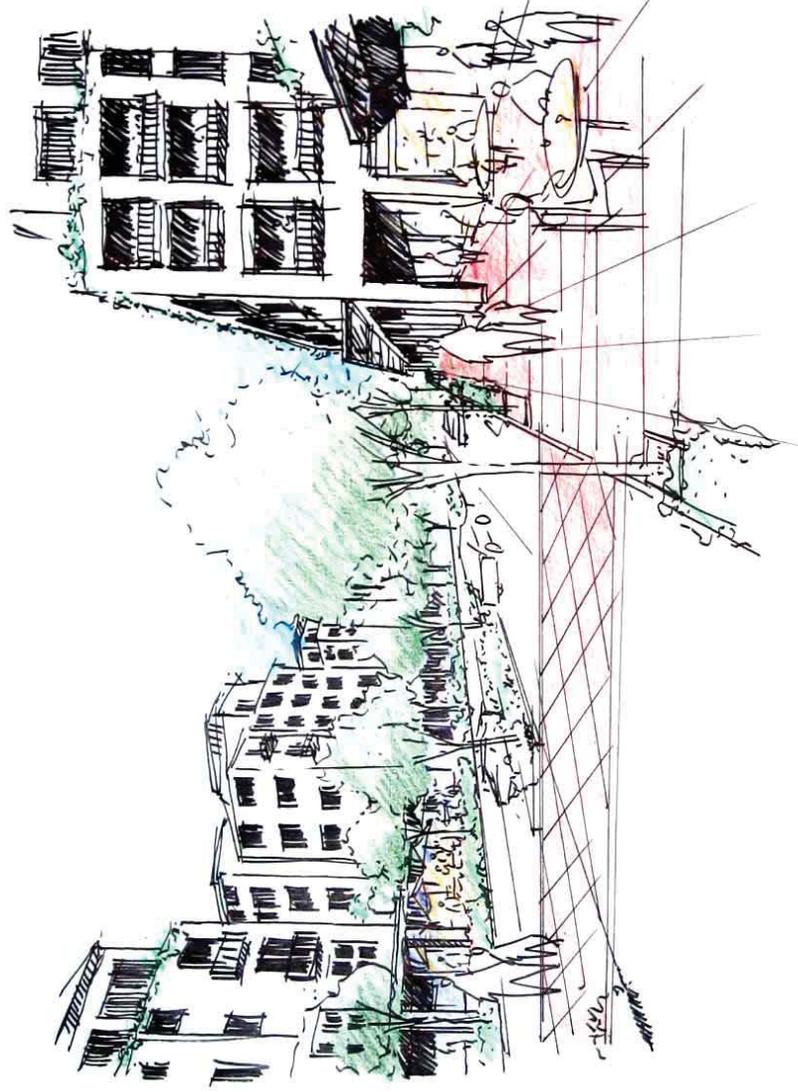


NE 10th Street has a dual character; one as a major vehicular artery and the second as a friendly pedestrian environment.

The sidewalks should be extra wide, 14 feet minimum, maybe larger in some areas to tie in with pocket urban courtyards.



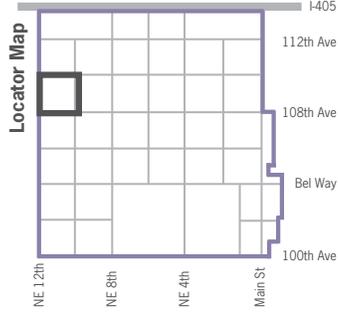
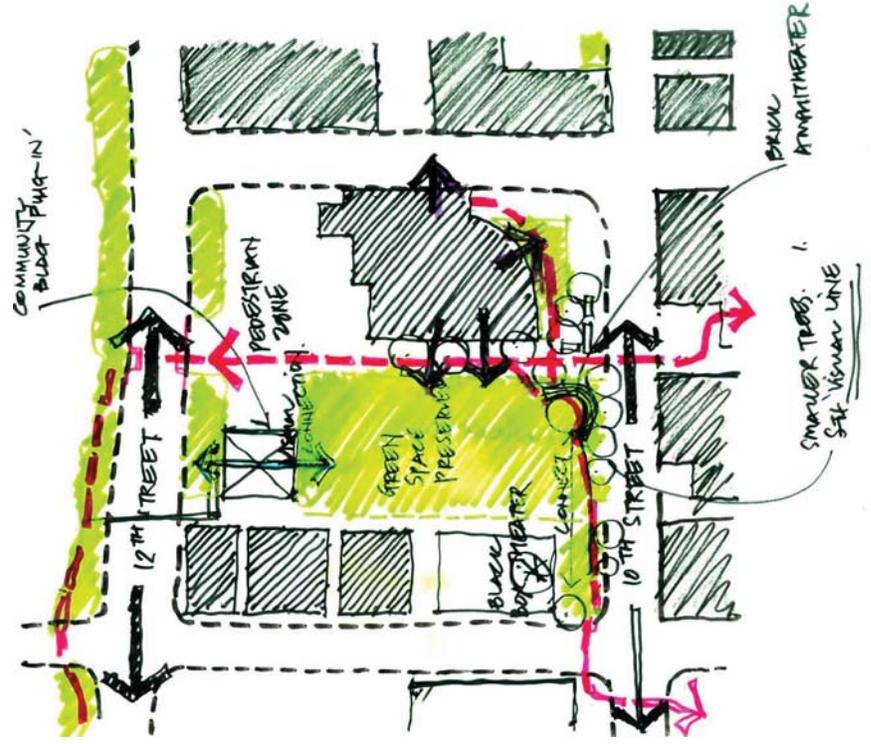
# “Ashwood Park” Neighborhood



Ashwood is the furthest of NOMA's three neighborhoods towards an enduring urban form.

Major opportunities are: mid-block crossings and through-block connections, improvements to Ashwood Park, on-street parking, and additional neighborhood commercial.

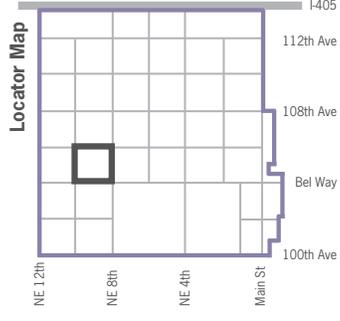
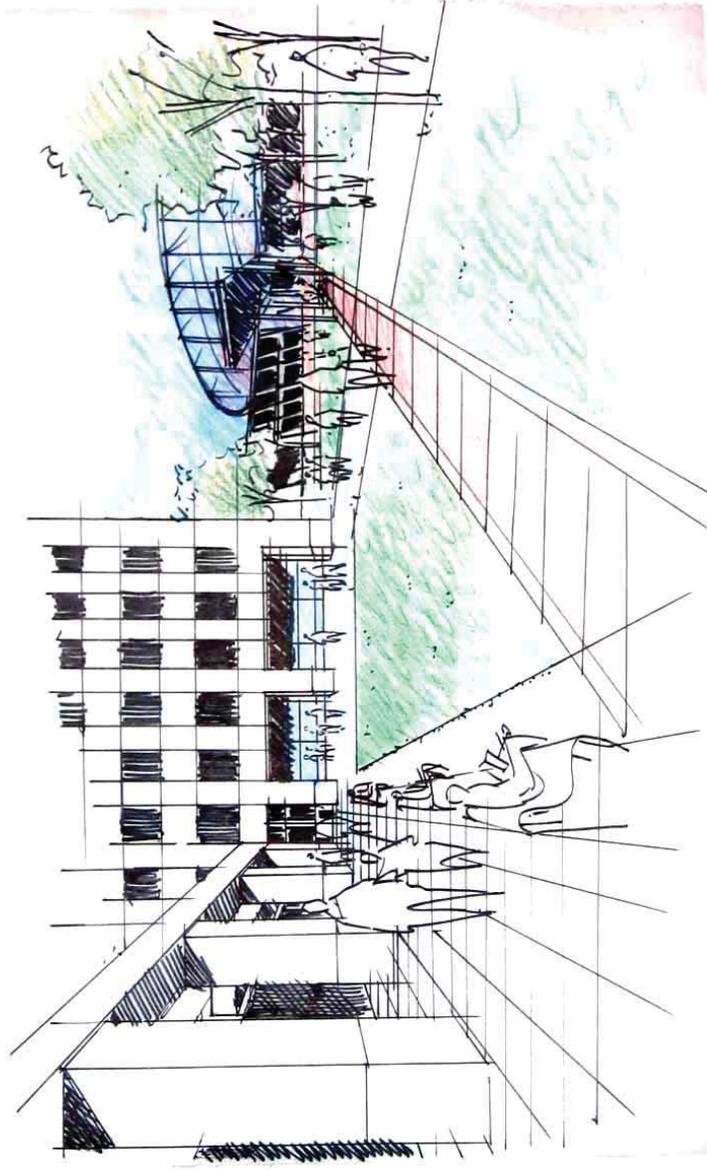
# Ashwood Park Connections



An attractive and inviting pathway system within the interior of the individual superblocks could enhance the "Ashwood Park" Neighborhood. The King County Library/Ashwood Park superblock is shown as an example.

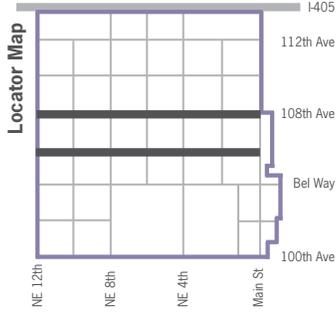
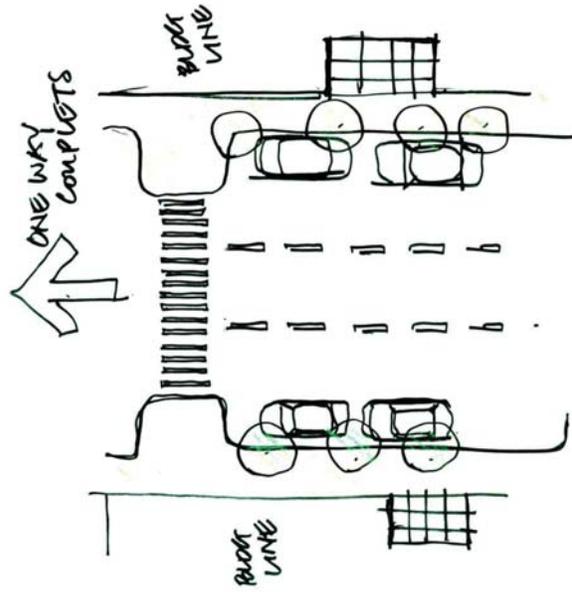


## “Hidden Garden” Leading to New Performing Arts Center

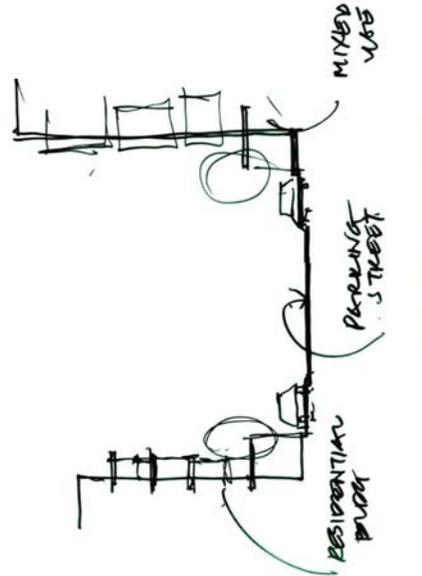


The “Hidden Gardens” Neighborhood could have a series of courtyards and passages integrated with large uses such as the planned Performing Arts Center Eastside.

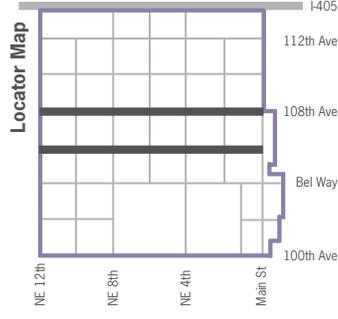
# One-Way Couplet Treatments



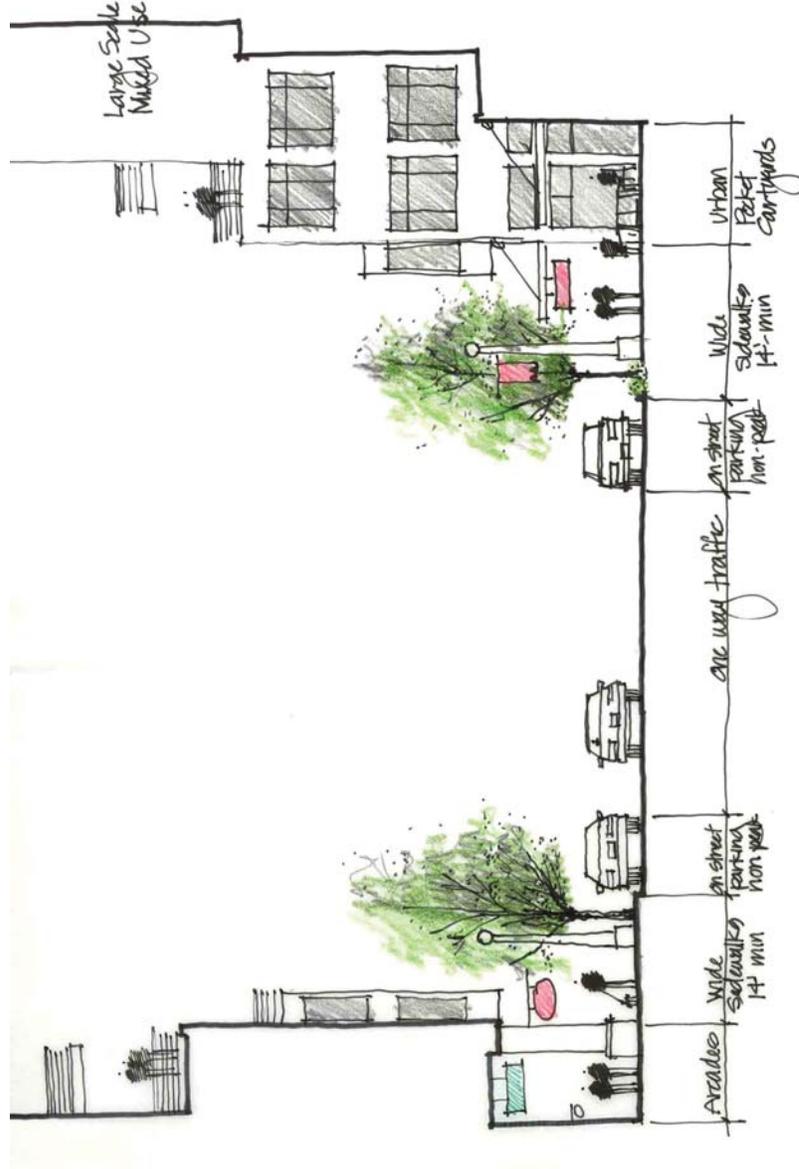
Reconfiguration of 106th Ave and 108th Ave as a one-way couplet allows for the reduction of street width and addition of on-street parking and pedestrian amenities.



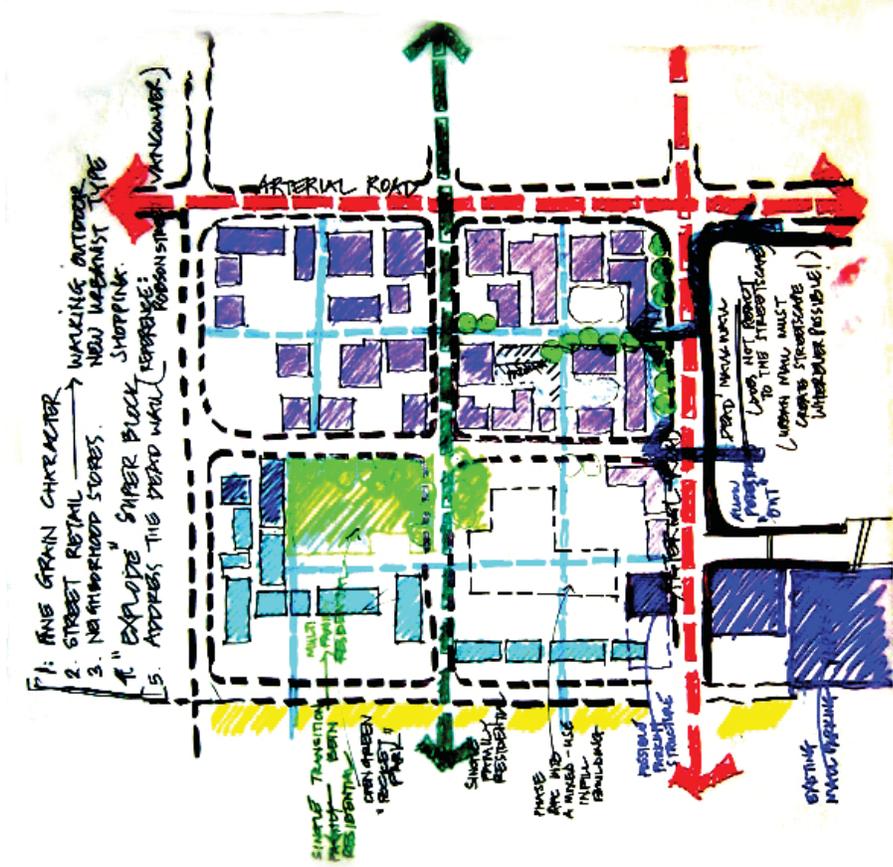
# One-Way Couplet Treatments



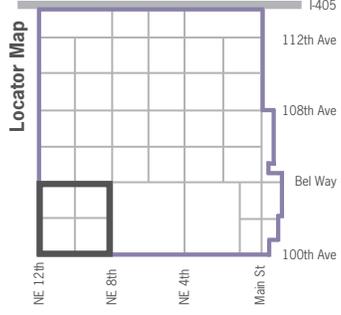
The potential character of 106th Ave and 108th Ave, designated as a one-way couplet in the Downtown Plan, could include more pedestrian amenities in place of travel lane(s). This sketch shows 106th Ave.



# "New Bellevue" Neighborhood



1. THIS GRAIN CHARACTER
2. STREET RETAIL
3. NEIGHBORHOOD STORES
4. "EXPLORE" SUPER BLOCK
5. ADDRESS THE DEAD WALL (PROBATION)

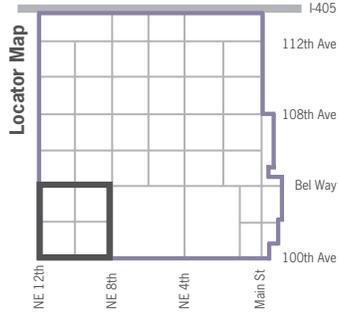


The "New Bellevue" Neighborhood is currently an older, low-rise commercial area that has the opportunity to grow into a unique, street-oriented retail district.

The name "New Bellevue" mirrors the "Old Bellevue" Neighborhood to the south.

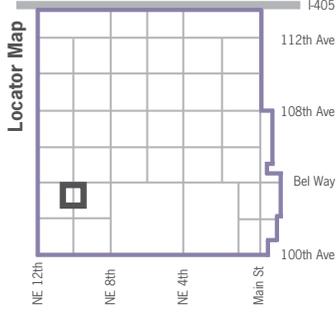
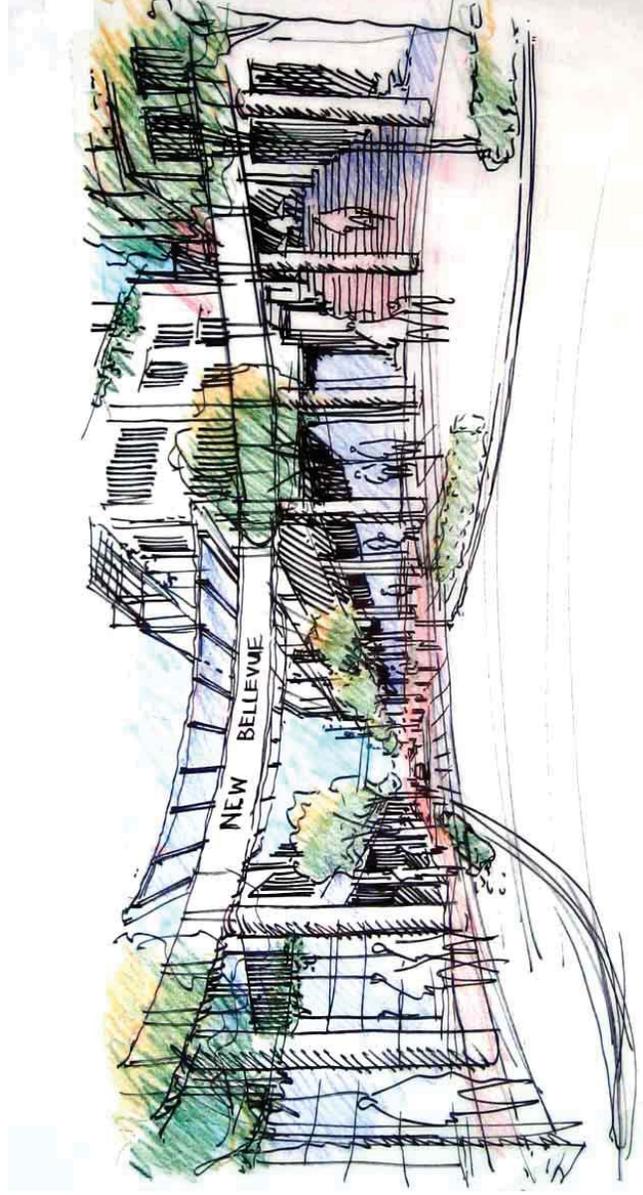
QFC is a big part of this neighborhood and should remain a key focal point as part of a larger mixed-use development in the future.

## “New Bellevue” Street Character



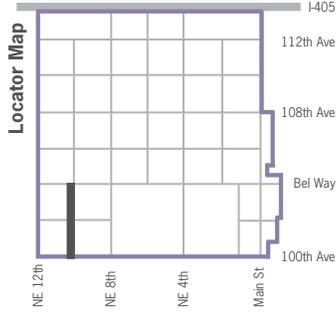
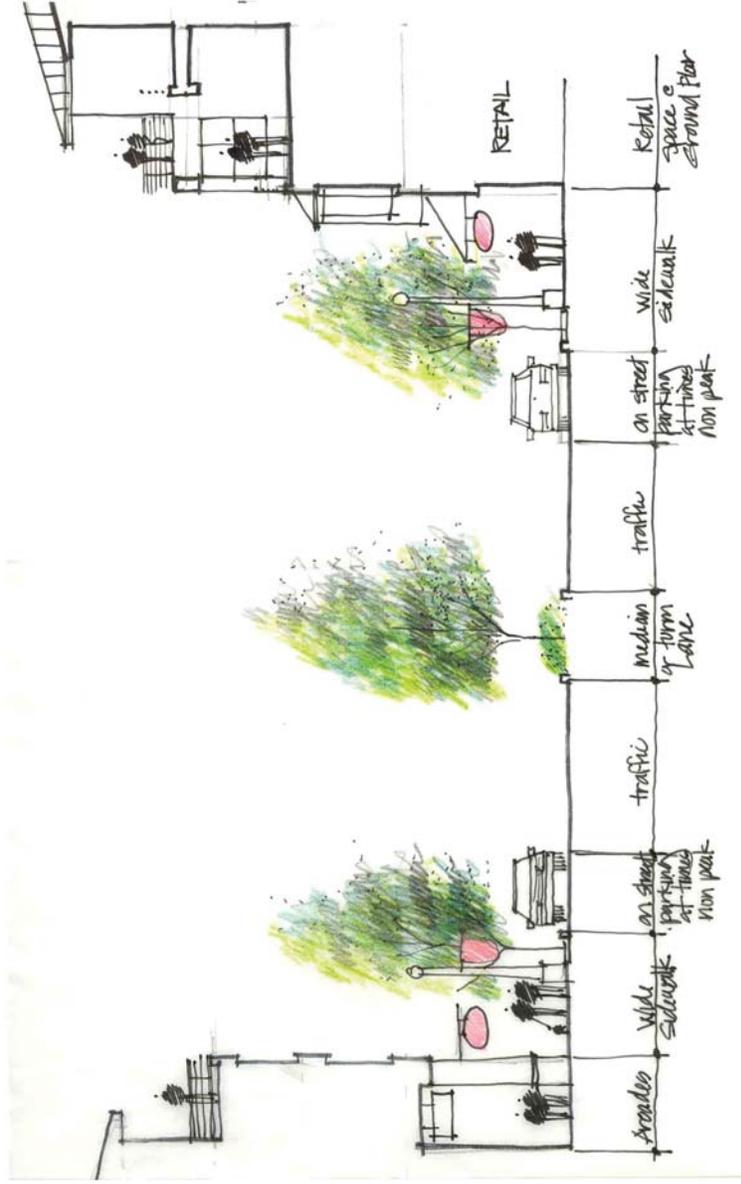
“New Bellevue” will be characterized by fine-grained retail streets and a network of “alleys with addresses” through the superblocks.

## Gateway to “New Bellevue”



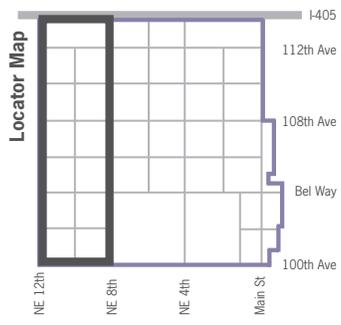
This sketch shows an urban gateway to the “New Bellevue” Neighborhood in the vicinity of NE 10th Street and Bellevue Way. It signals a transition along NE 10th from a busier street east of Bellevue Way to a quieter street in this neighborhood.

# NE 10th Street in "New Bellevue"



A quieter street character will be evident on NE 10th Street west of Bellevue Way with wide sidewalks, off-peak on-street parking, and landscaped medians.

# Pocket Urban Courtyards



Pocket urban courtyards could occur throughout NOMA. Located at mid-block locations or intersections, they would be part of the downtown open space network and a key public amenity. Uses could include retail plazas with seating, outdoor cafés, building forecourts, and other “gems” to be discovered.

# Team Two: The NOMAdS

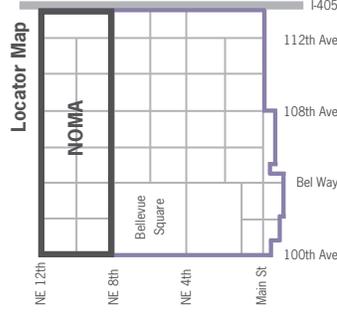
## PARTICIPANTS

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- Linda Abe, EDAAW
- Gaurav Chopra, NBBJ
- Jason Henry, The Berger Partnership
- David Huang, NBBJ
- Jessie Skidmore, CollinsWoerman
- Jason Steiner, Mithun
- Deanna Swatek, ZGF
- Mark Tilbe, Murase Associates
- Patti Wilma, Bellevue Staff Resource



## ASSIGNMENT

Team Two (along with Team One) was given the assignment of NOMA, or the “north of mall area.” NOMA is the northern third of Downtown Bellevue, delineated by NE 12th Street to the north, NE 8th Street to the south, 100th Ave NE to the west, and I-405 to the east.



## APPROACH

### One Thing Leads to Another

Our team believes that NOMA is potentially threatened in its transition to a vibrant, memorable, coherent, mixed-use neighborhood. Our concern is based on several factors:

- Increased traffic volume from the NE 10th Street connection to I-405 is a major concern. As stated by traffic engineers during the 2002 Downtown Implementation Plan study, “traffic volumes are anticipated to resemble current NE 8th Street conditions by 2020.” Several of the more memorable aspects of the NE 10th Street streetscape such as small-scale restaurants and the library plaza are difficult to imagine succeeding under such conditions.



- Future uncoordinated point tower development may result in a chaotic, incomprehensible street and land use pattern. We believe coordinated, thoughtful point tower developments have the ability to form districts and define gateways and street edges much like the "Commerce Avenue" ambiance of 108th Ave NE between NE 4th Street and NE 8th Street.

- Several "superblocks" consist of low-density, residual uses which lower the potential value and coherence of NOMA, such as the Wasatch Superblock, the block to the immediate north of the Wasatch Superblock, as well as the block diagonally northwest of the Wasatch Superblock, abutting Bellevue Way and NE 12th Street. We believe this area requires an extra degree of intervention to achieve a focus and sense of place.

To address these concerns, our team focused on leveraging the unique aspects of NOMA, especially where current uses and activities have "organically" developed and prospered over time. We considered the most likely three-dimensional future development based on existing and planned development. Most importantly we



proposed creating three districts within NOMA, each comprised of a realistic four superblock development zone. We focused on NE 10th Street as NOMA's Main Street, and considered limiting it to one-way traffic eastbound to mitigate future traffic volumes. We also view NE 10th Street as an urban space having a

clear beginning and end with a perceivable rhythm of "showcased" open green spaces within each of the three neighborhoods helping give each a focus and an authentic "reason for being."

Within each of these open green spaces would reside "urban magnets" fronting on NE 10th Street designed to stimulate a whole series of activities in addition to what one thinks is currently available in Downtown Bellevue. These open greens would also give an identity to what we define as three neighborhoods: the Library Quarter, the Tower Row Cultural District between 108th Ave NE and Bellevue Way, and the Village Green where Northwest Village is currently.

### Library Quarter District

The Library Quarter's memorability and focus comes from the King County Library's iconic architecture surrounded by the open green of Ashwood Park. The open green is well defined by the adjacent low-rise residential street wall. Our plan would extend the character of Ashwood Park south across NE 10th Street to help organize residential development as well as link to the existing east-west through-block connection between 110th Ave NE to 108th Ave NE. We think interior library functions such as book sales and food service should be "externalized" to enliven the adjacent exterior space. Our team believes ground-related residential units such as the stoops at NE 11th Street, as well as corner-related neighborhood cafés such as Andiamo, are key signatures of the Library Quarter and should be extended into a "restaurant row" along 110th Ave between NE 8th Street and NE 10th Street. Such an



amenity would be, and is, utilized by Library Quarter residents and lunchtime office workers. This organic link could develop into a viable pedestrian link to the downtown core across the formidable traffic volumes of NE 8th Street if people grew to really use it.

At the district's eastern boundary, two residential point towers, one with a gallery-retail program, the other with a small neighborhood grocery program, would create a portal framing the NE 10th Street and 112th Ave NE intersection with an "edge park" adjacent to I-405 and the NE 10th Street overpass. Because of the future connection to the Overlake Hospital Campus, this district may continue being relevant to the "empty nester" demographic.

At the west boundary along 108th Ave and NE 10th Street, another residential point tower, programmed with a black box theatre at its base, would help transition to the Tower Row Cultural District in massing and program. It is obvious to our team that the library as a destination "urban magnet" has put this district on the map, given it

a focus, and generated numerous activities and "layers" of relevance. It is a successful urban strategy we strongly believe is applicable to the other districts.

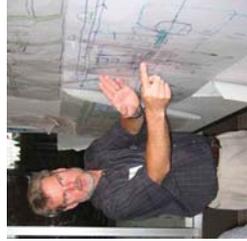
### Tower Row Cultural District

The Tower Row Cultural District's memorability comes from the iconic new Technology Learning Museum at the north terminus of 106th Ave NE. We see this "urban magnet" in conjunction with the performing arts center proposed for the southwest corner of NE 10th Street and 106th Ave NE. These cultural amenities will provide a significant culmination to the proposed "Entertainment Avenue" running north-south along 106th Ave NE. Unlike the Library Quarter, most of the residential developments proposed for this district have been point towers where zoning allows. The value of this building type is of course the greater availability of open space. Consequently, we propose shifting much of this area into the green space surrounding the Technology Learning Museum. The surrounding point towers would be sited adjacent to the green space, defining it as well as defining a comprehensible "tower row" pattern along NE 10th Street.



Downtown Bellevue Design Charrette

Another memorable aspect of the Tower Row Cultural District is the dip in elevation from 108th Ave NE to 106th Ave NE. Waterfalls and terraced landscaping opportunities within the green space surrounding the

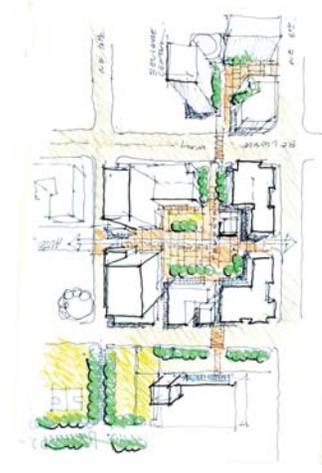


Technology Learning Museum should be developed. The green space in this district should be programmed to accommodate requirements of high-density residential uses such as urban gardens, playgrounds, and recreational courts. Residential demographics could include some children, especially in the lower, more ground-related units, along NE 12th Street. We believe the primary residential demographic to be younger singles or double-income childless couples based on proximity to the "Entertainment Avenue" of 106th Ave NE.

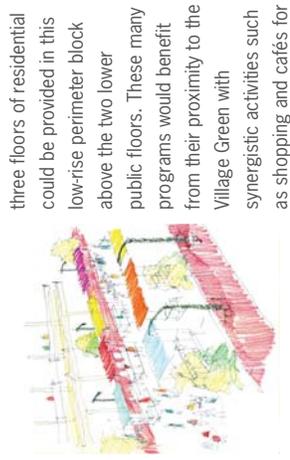
### Village Green District

The Village Green's memorability is derived more from the "town square" open green pattern than the "iconic object in the park" identity of the other two districts. We see this as leveraging the current village character of smaller scale, neighborhood service retail that serves not only this district but also the adjacent single family neighborhoods to the north and west. We also view the Village Green as providing an emphatic western vehicular terminus to NE 10th Street at 102nd Ave NE. In a similar way to ending 106th Ave NE at the Technology Learning

Museum, we believe not all existing roadways in NOMA need extend to what is thought of as "the edge of downtown." Through-traffic to adjacent single family neighborhoods may be reduced, as well as creating truly urban vistas, especially where roads like 106th Ave NE don't extend past NE 12th Street.



In order to "charge" the Village Green with a significant variety of uses for such a large space, we propose a continuous "Main Street" retail edge along its perimeter. Angle parking with a textured paving and curbless edge detail would signal a shared pedestrian/auto realm ensuring viability for neighborhood service retail. We envision the sidewalk zone be wide enough to support a variety of social interactions. At the southern edge of the Village Green a large-scale grocery store would service this district in addition to the surrounding neighborhoods, much as it functions today. To better define the space of the Village Green, we see additional uses above the retail such as neighborhood services, meeting rooms, continuing education facilities, and daycare. An additional



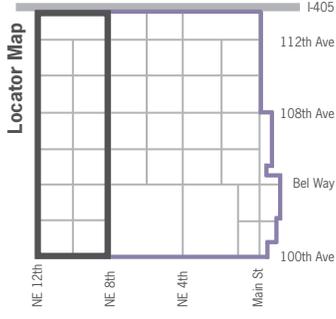
three floors of residential could be provided in this low-rise perimeter block above the two lower public floors. These many programs would benefit from their proximity to the Village Green with synergistic activities such as shopping and cafés for

parents, playgrounds and sport courts for kids all being available. Of all the NOMA districts, this one could have a real opportunity for more affordable residential units, especially for young adults that have grown up in the adjacent single family neighborhoods as they avail themselves of more job related and educational opportunities.

### Conclusion

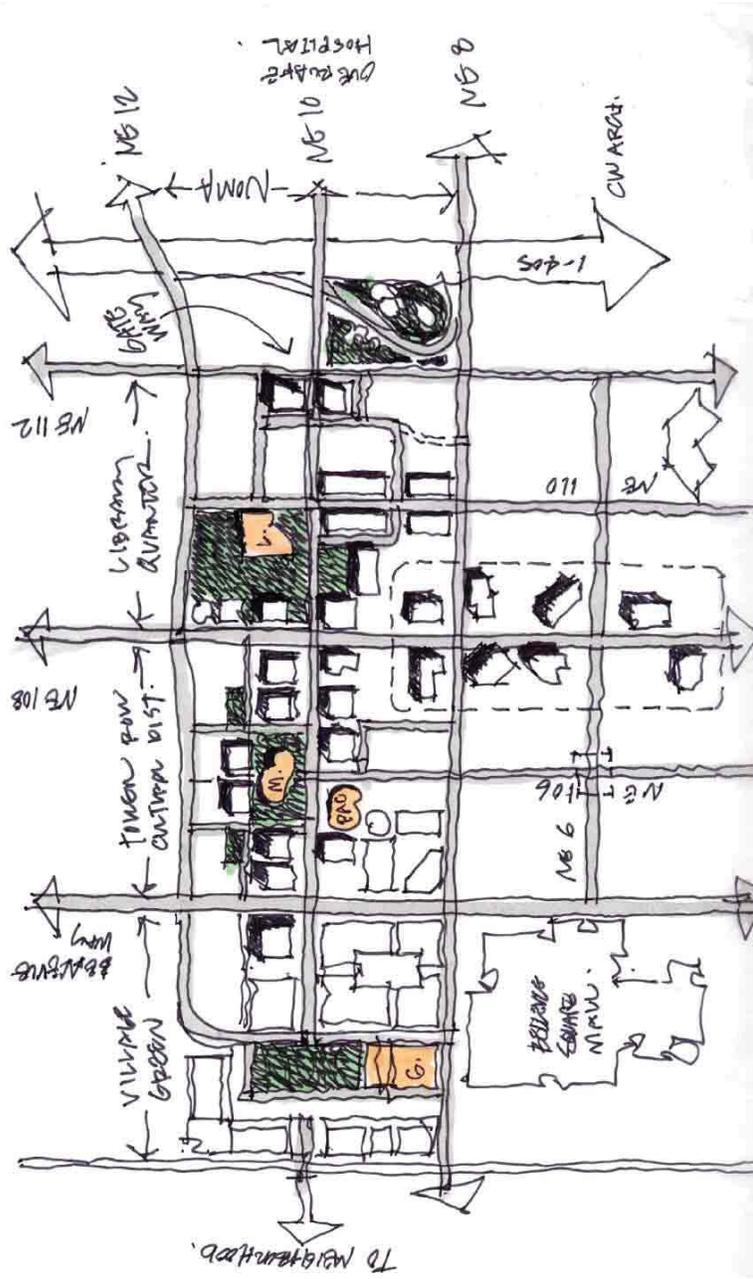
Our team has suggested somewhat radical solutions to what we see as a “threatened district.” We believe larger scale interventions such as terminating NE 10th Street and 106th Ave NE with “civic vistas” is critical to stopping through-traffic in some cases in order to create a “pause” in the fabric in which a new echelon of “urban magnets,” and their surrounding green space may best be perceived, programmed and enjoyed. Even if street closures are a bit over the top, we think many of these suggested principles will still apply.

# NOMA Neighborhoods and Green Spaces

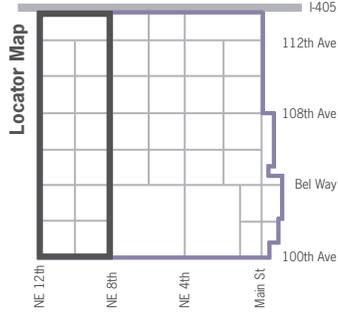
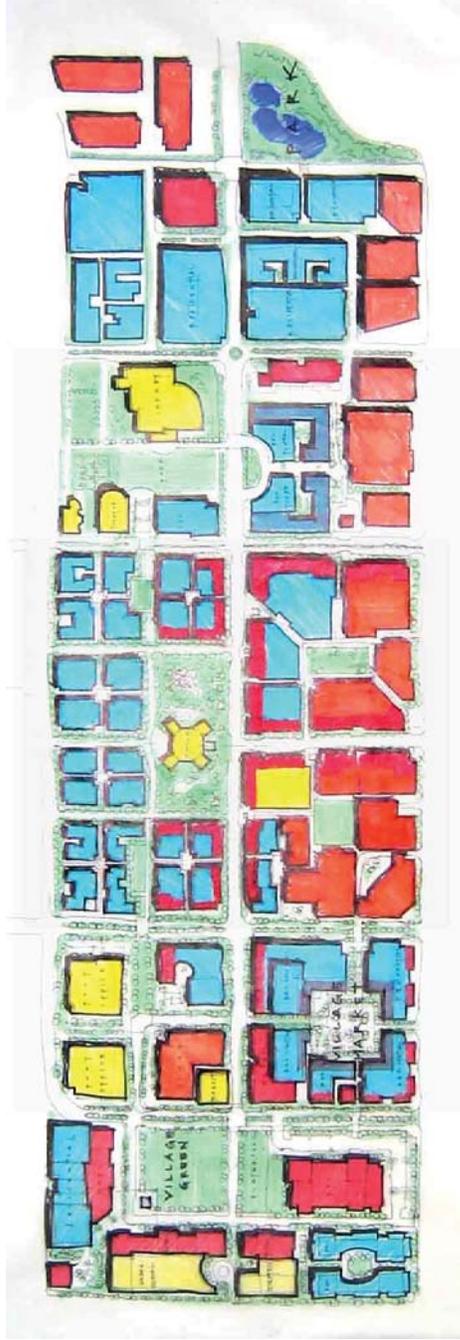


Alternative neighborhood names include Village Green, Tower Row, and the Library Quarter.

This plan shows an "edge park" south of NE 10th along I-405, an expanded Ashwood Park, and new green spaces that terminate and crown NE 10th at 102nd Ave and 106th Ave at NE 10th.

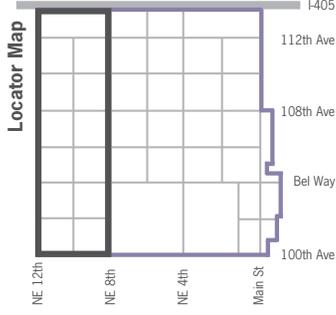
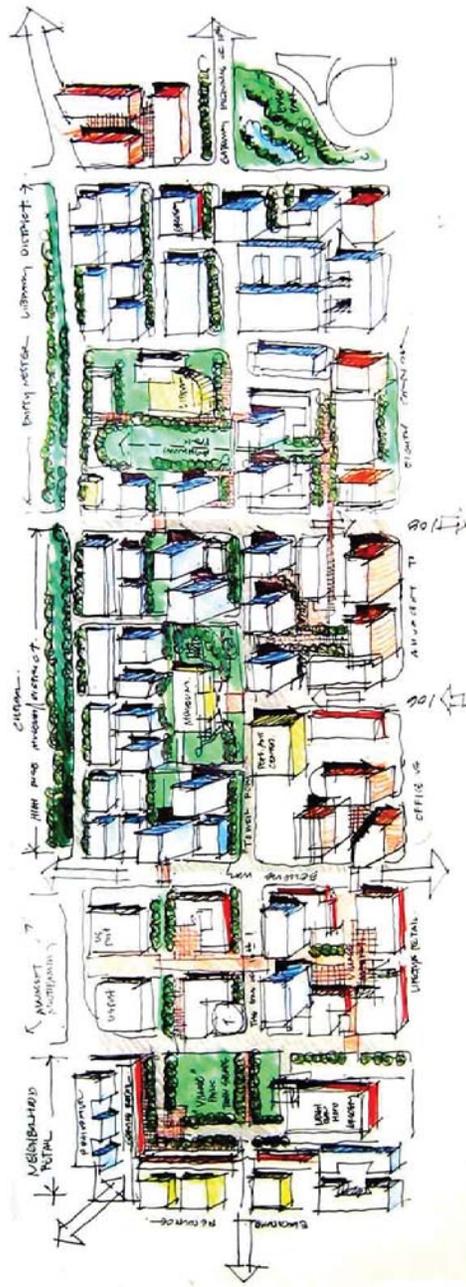


# NOMA Neighborhoods



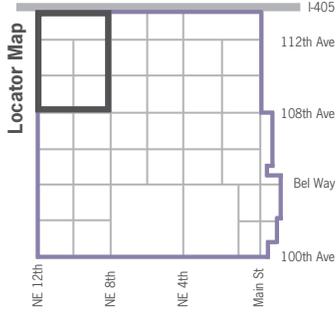
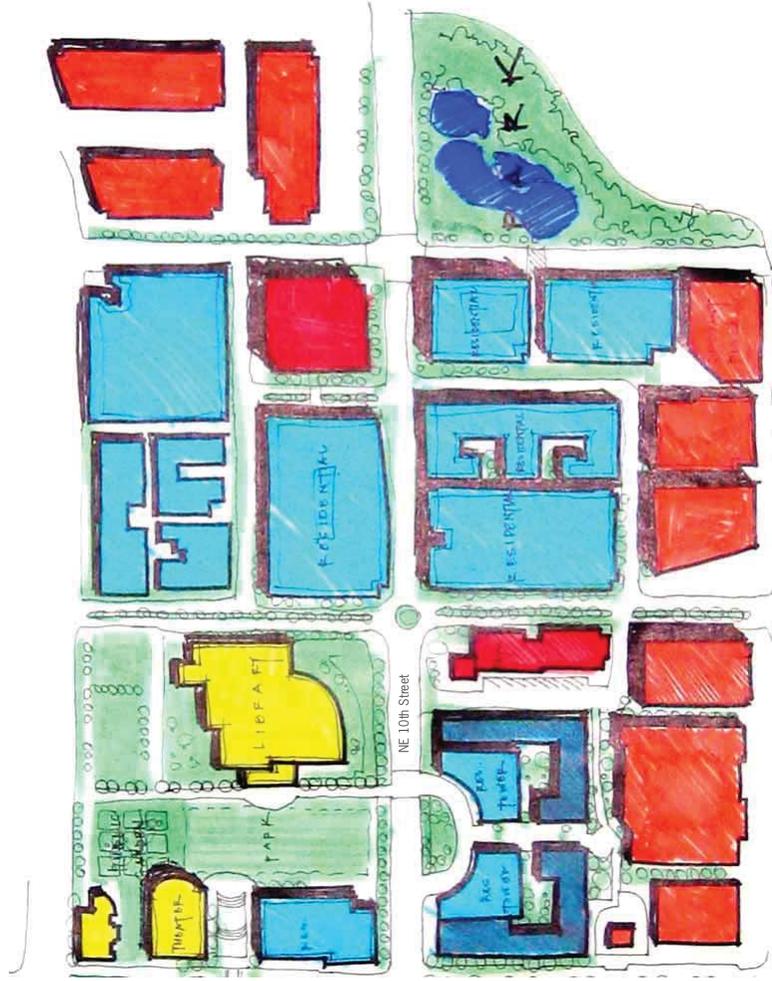
NOMA will develop into a series of dense urban neighborhoods, each with a large green space and community-oriented uses.

# NOMA Neighborhoods: One Thing Leads to Another



NE 10th Street may be viewed as an urban space having a clear beginning and end with a perceivable rhythm of "showcased" open green spaces.

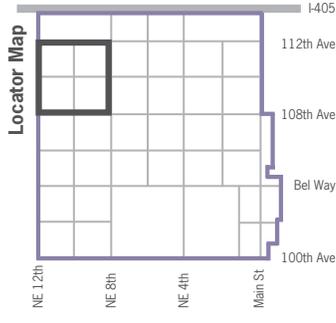
# “Library Quarter” Neighborhood



The Bellevue Regional Library and Ashwood Park will continue to be the focal points for this neighborhood. Some of the library functions (book sales, food service) should be “externalized” to enliven adjacent exterior space. The character of Ashwood Park should be extended south across NE 10th Street. A continuation of the restaurant row along 110th Ave would also add greatly to this neighborhood.

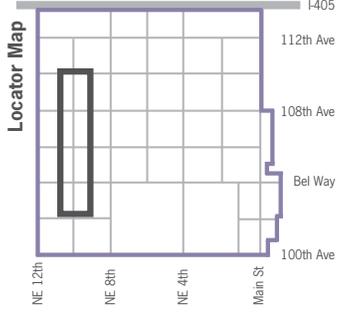
There may be an opportunity for an “edge park” as the eastern portal of NE 10th, when the connection is made across I-405 to the Overlake Hospital Campus.

# Pedestrian Environment



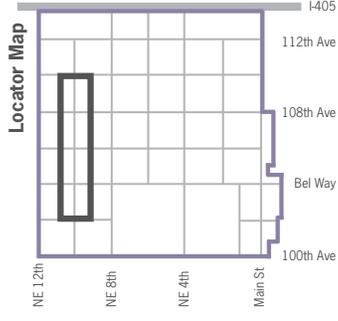
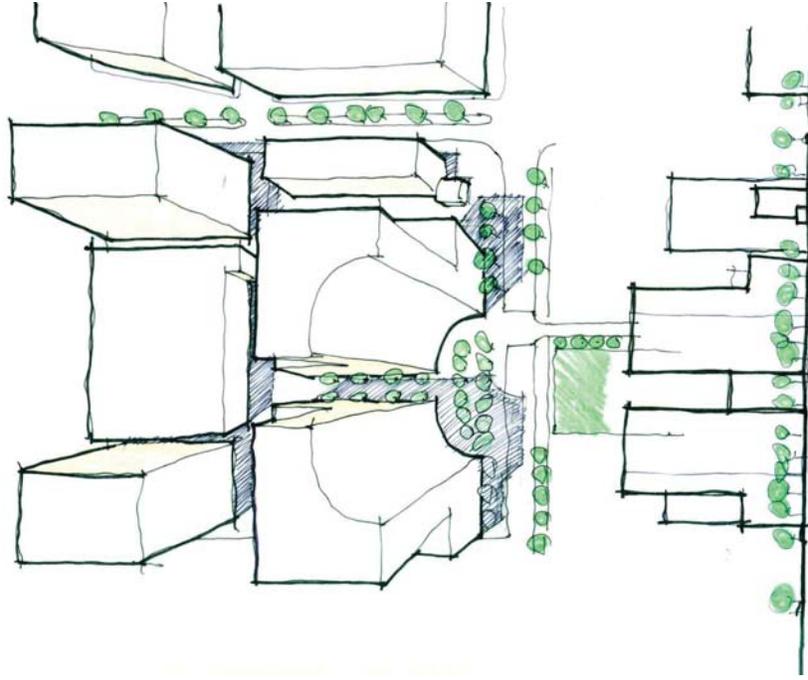
Certain streets may be enhanced with increased landscaping, on-street parking, and additional sidewalk-oriented uses.

# NE 10th Street Building Form



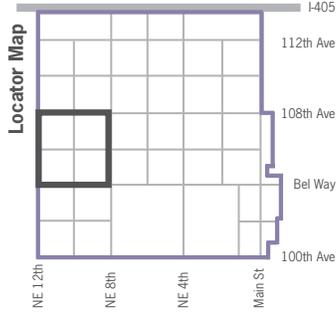
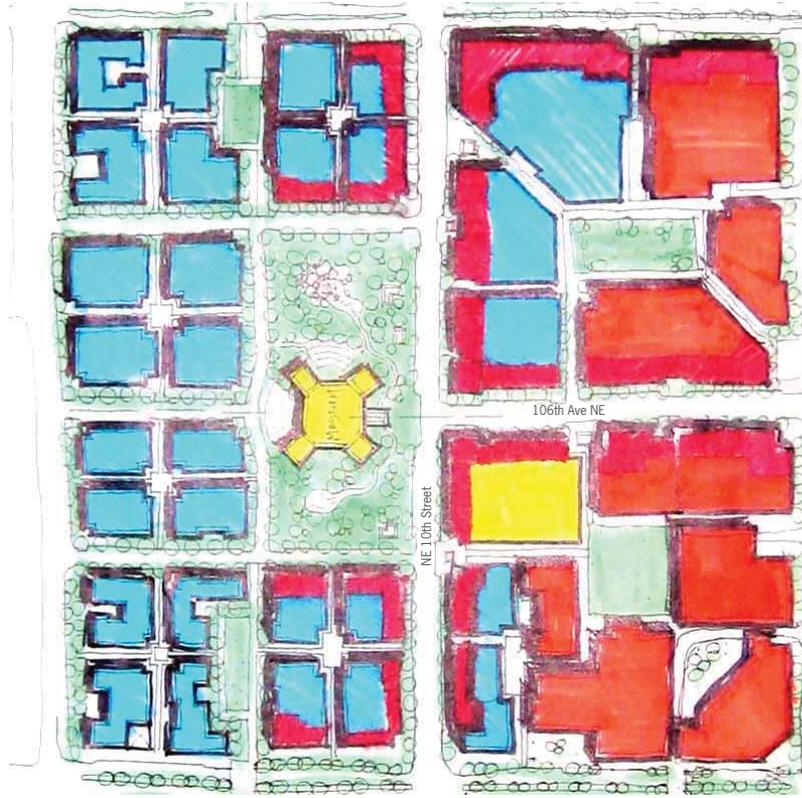
There are opportunities along NE 10th for tall, slender towers spaced to enhance solar access to public open spaces.

# Point Tower Form



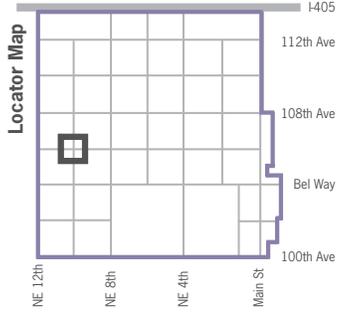
The point tower form provides a great opportunity for framing public open spaces and developing a unique urban fabric along NE 10th Street.

# Tower Row Cultural District



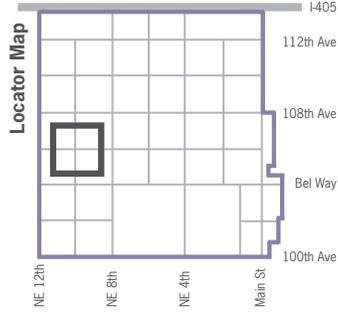
There is the potential for this district to be a hub of cultural activity on the Eastside. The district includes Bellevue Place, the planned Performing Arts Center Eastside, the Wasatch Superblock, and a new Technology Learning Museum at the northern terminus of 106th Ave.

## View of Technology Learning Museum

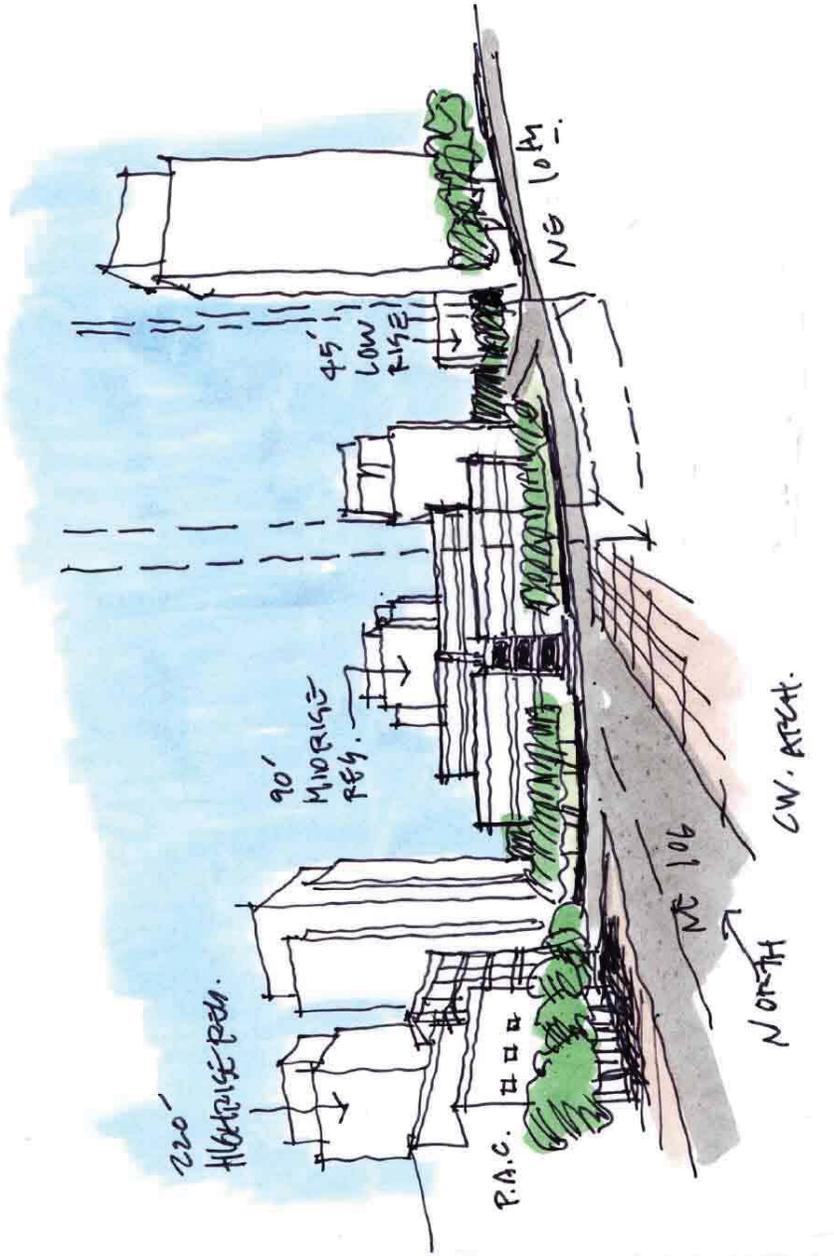


The view looking north on 106th Ave will be crowned by the Technology Learning Museum and accompanying open space.

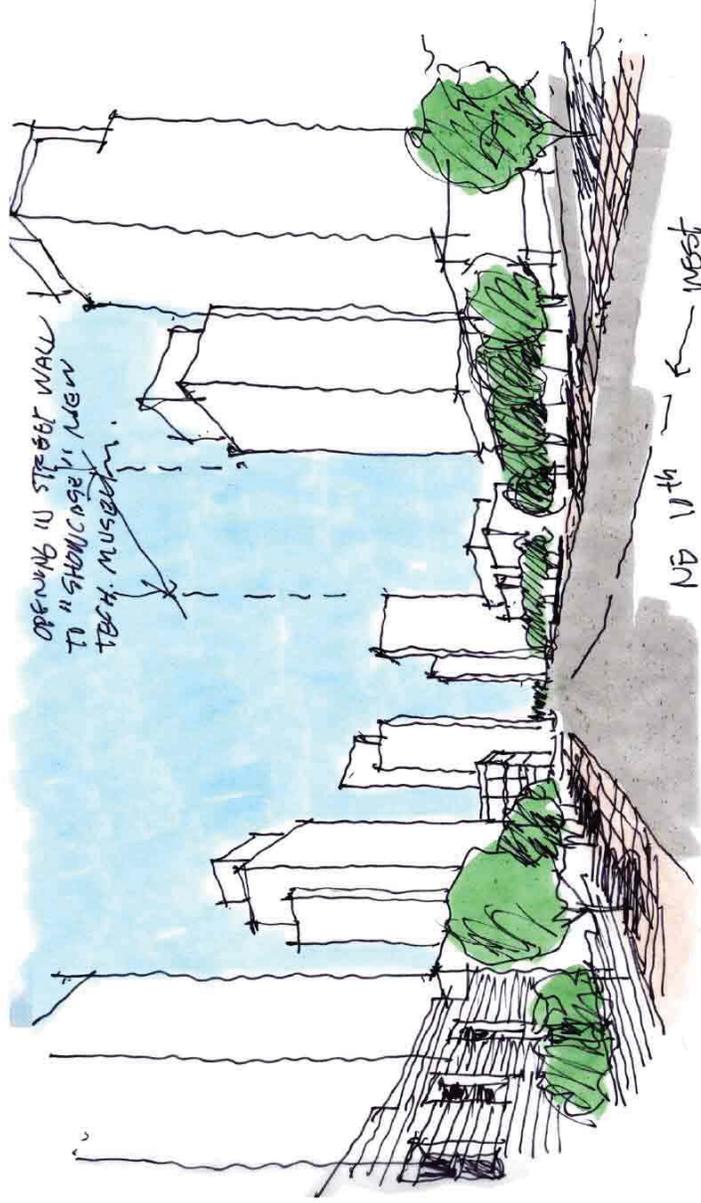
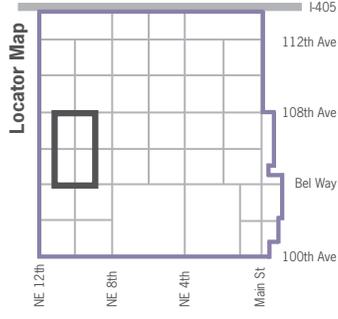
# Tower Row Cultural District



The Tower Row Cultural District's memorability comes from the iconic new Technology Learning Museum at the north terminus of 106th Ave.

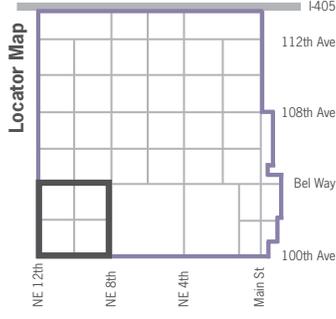
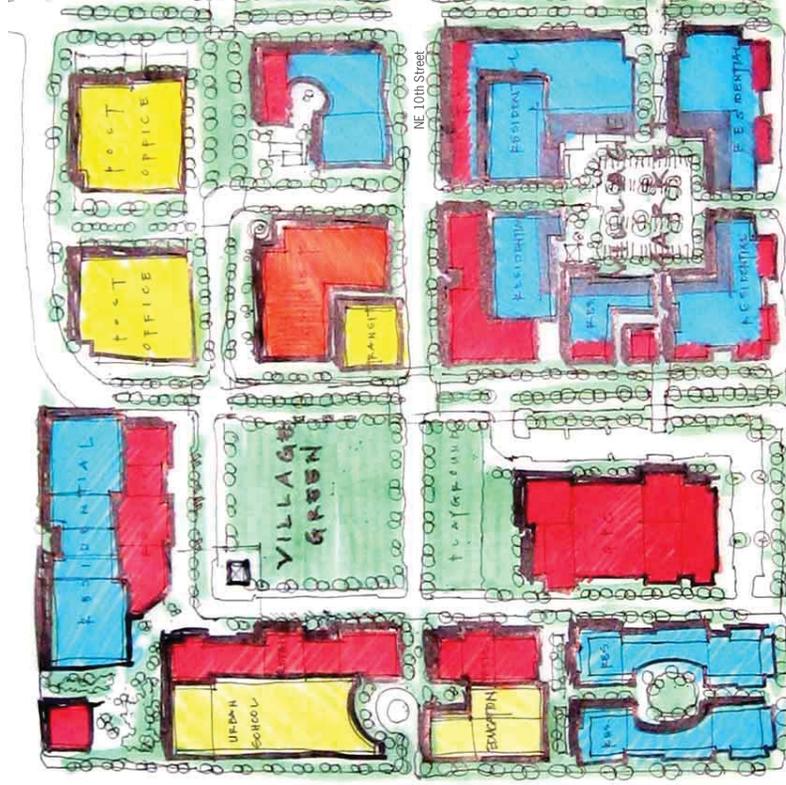


# Tower Row Cultural District



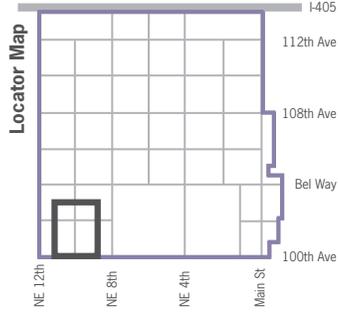
This view looking west on NE 10th shows the point tower form with an "opening" to showcase the new Technology Learning Museum.

# Village Green District



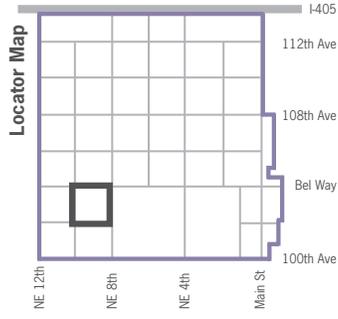
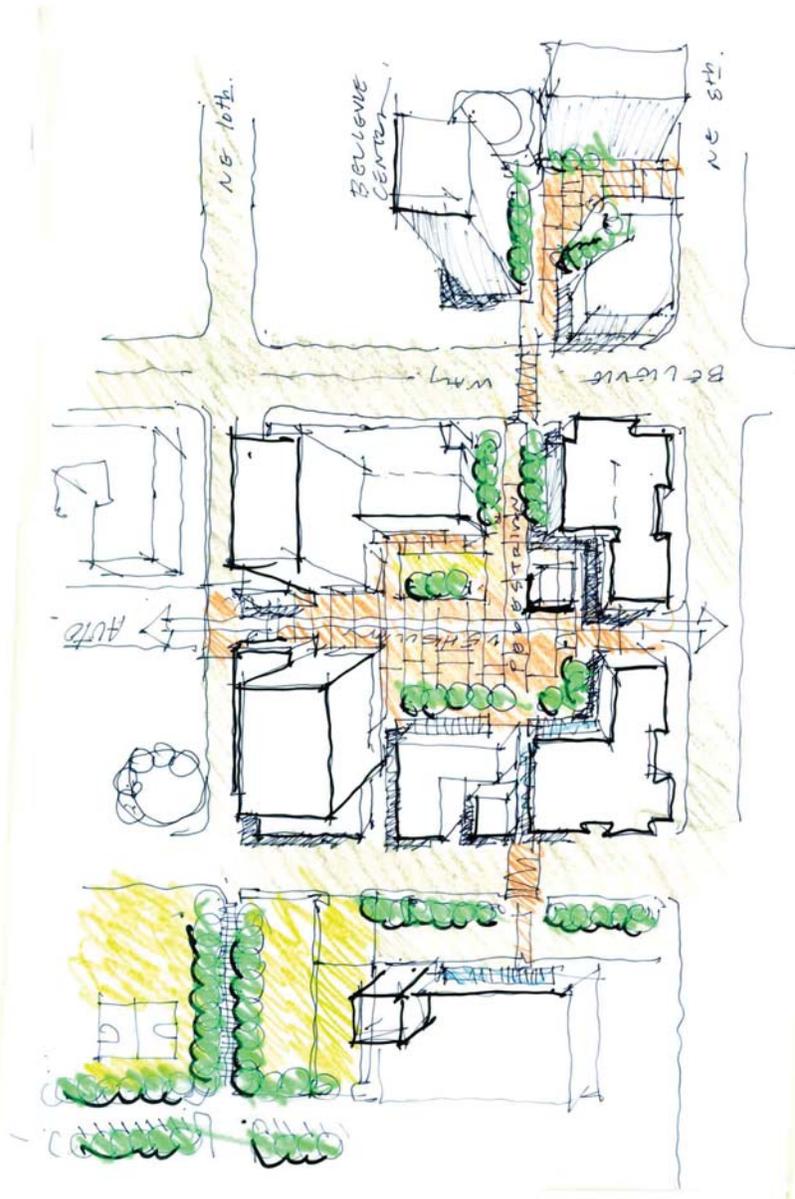
The Village Green District will continue to be a neighborhood-service area for West Bellevue and the Points Communities, but will also include more residential and a new "Village Green" Park as well as other community uses such as an urban school, playground, and transit facilities.

# Village Green Park



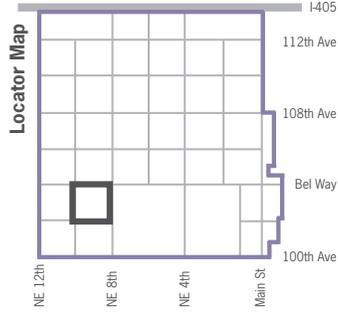
The Village Green Park will have a "town square" feeling. It will leverage the current village character of the area and be a truly memorable downtown amenity.

## Mixed-Use Development with Central Plaza



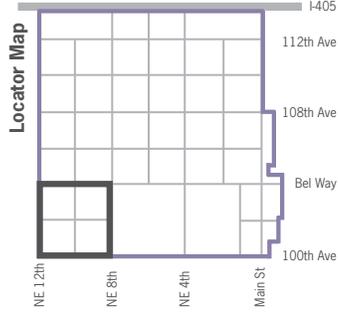
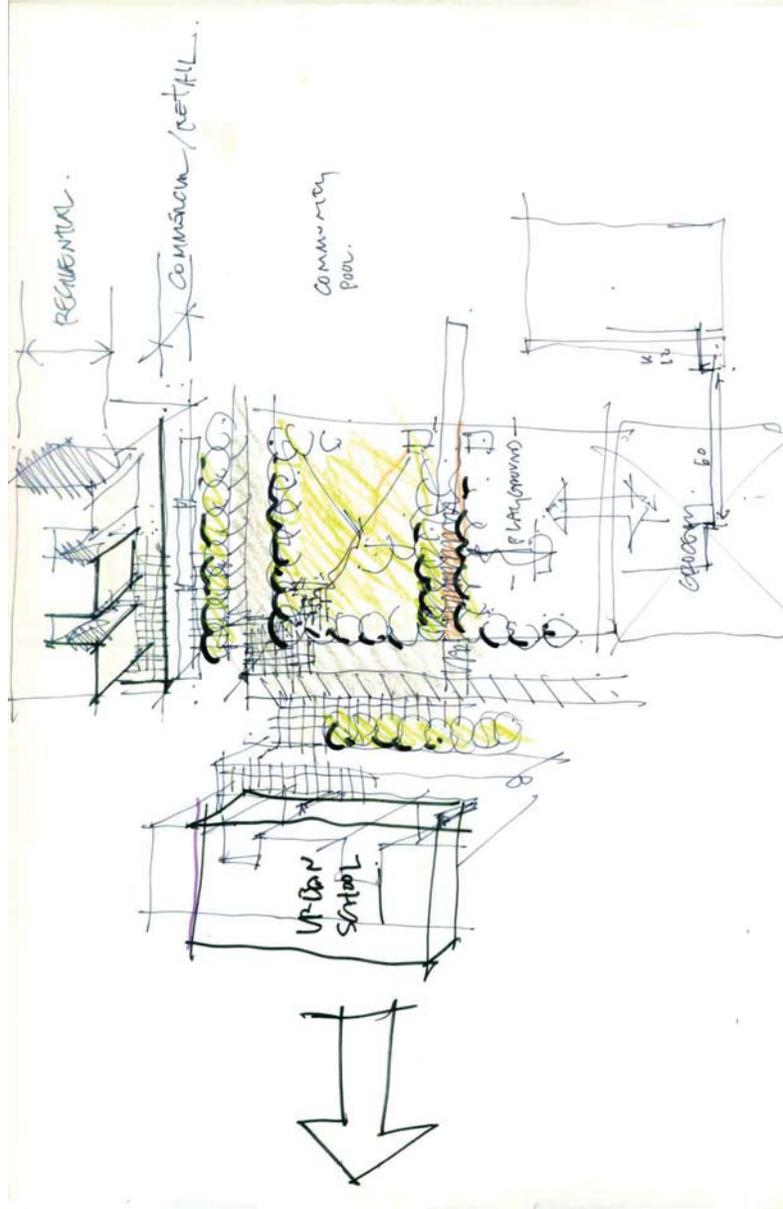
Superblocks in Downtown Bellevue may be broken down into smaller development pieces similar in size to other downtown city blocks. This example shows a mixed-use development with an organizing central plaza.

# Public Market, "Alley with Address"



The internal connections through superblocks may be developed as "alleys with addresses," connecting arterials with internal uses such as public markets as shown here.

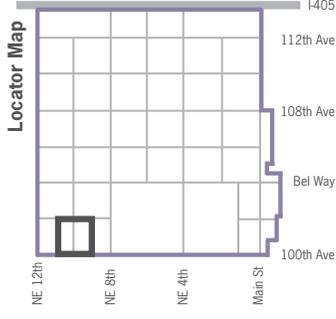
## Mix of Uses Around the Village Green Neighborhood



There may be opportunities to incorporate more community uses within the Downtown Subarea. This example shows an urban school, park, playground, and community pool.

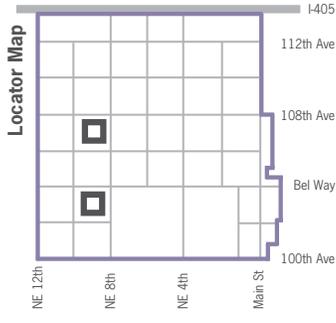
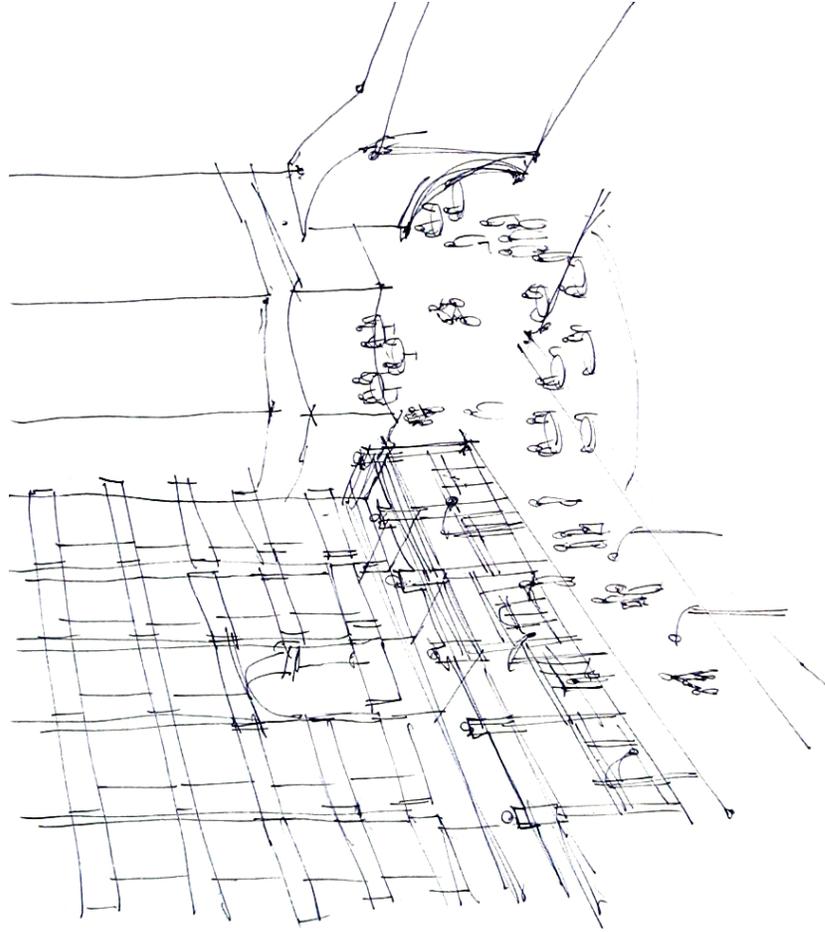
QFC could be reconfigured to relate to the playground to the north, creating a synergy between family and shopping activities in this neighborhood.

## View of Village Park



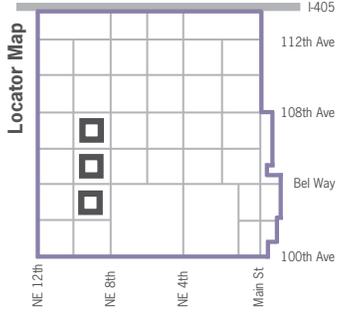
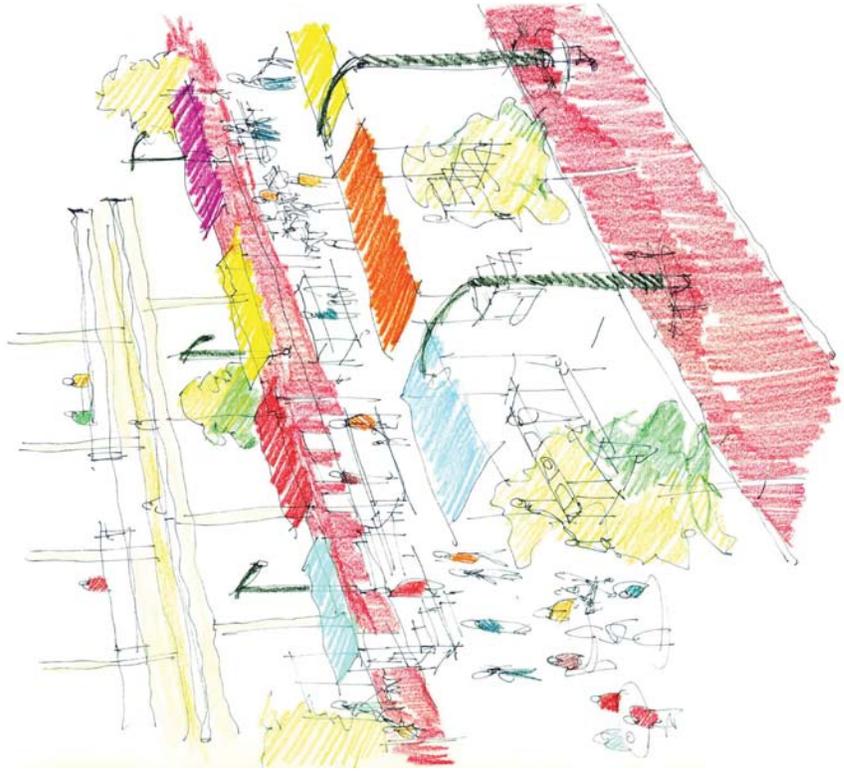
The character of NE 10th leading to the Village Park could include an array of Green Streets enhancements.

# Large Urban Plaza



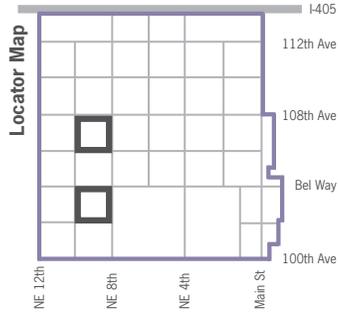
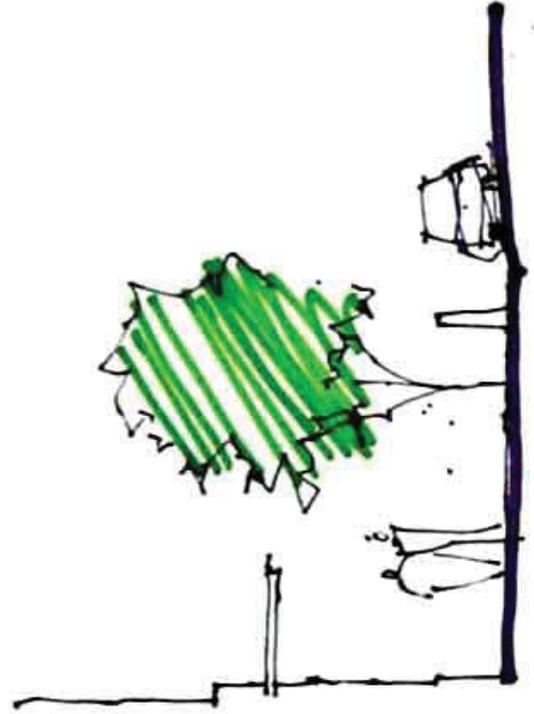
The redevelopment of certain superblocks in NOMA provides a set of unique opportunities for large urban plazas that could act as defining public gathering places.

# Concentrated Pedestrian Activity



With more density and diversity of uses, a vibrant pedestrian realm will be formed both on the exterior and interior of superblocks.

## A Curbless "Alley with Address"



There may be opportunities to explore innovative design features such as a "curbless street." Bollards would be used in place of a curb to have a continuous paving surface where cars and people can coexist.

# Team Three: Tapestry

## PARTICIPANTS

- Vince Vergei de Dios, NBBJ (Team Leader)
- Thomas Bath, Callison Architecture
- Mark Brennan, Callison Architecture
- Arian Collins, CollinsWoerman
- Patrick Foran, Bellevue Staff Resource
- Frank Lo, MulvannyG2
- Amy Portz, NBBJ
- Dave Reddish, Sciater Partners
- Alan Sciater, Sciater Partners
- Blythe Von Reckers, NBBJ



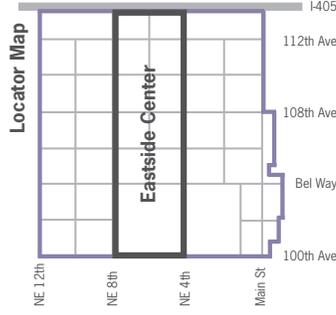
Before



After

## ASSIGNMENT

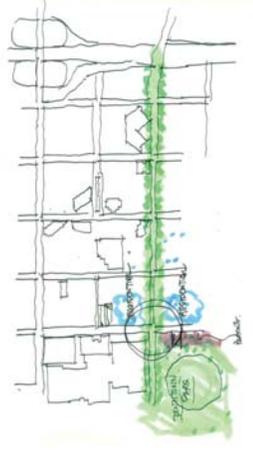
Team Three (along with Team Four) was given the assignment of the "Eastside Center." The Eastside Center is the middle third of Downtown Bellevue, delineated by NE 8th Street to the north, NE 4th Street to the south, 100th Ave NE to the west, and I-405 to the east.



## APPROACH

### Reach Out and Connect

Current development in Downtown Bellevue's center district functions independently with activities focused inward (see "before" photo to the far left). Our vision distinguishes the center of the Eastside Region as a tapestry that reaches out and connects (see "after" photo to the far left). This scheme creates: a new public center located "off-center," a Park Street Boulevard, three distinct districts, a downtown circulator and parking, and a "ridge to bay" trail. East-west connections between Bellevue Square and the Convention/Civic Center district are enhanced, reaching out to the surrounding neighborhoods, creating diverse activities and improved quality of life.



## The Name Tapestry

The name Tapestry symbolizes what Downtown Bellevue is and can become to be more distinguished:

- Diverse and unique
- Made of different threads
- A whole knitted together
- Woven by many hands
- Enriched by age



## Observations

- Downtown development is inwardly focused and functions independently.
- The downtown's large scale of development challenges pedestrians.
- The uniform grid lacks the special places that irregularity offers.
- Incremental plan implementation causes incompleteness for years.
- Avoid over design and control; allow serendipity and variety.

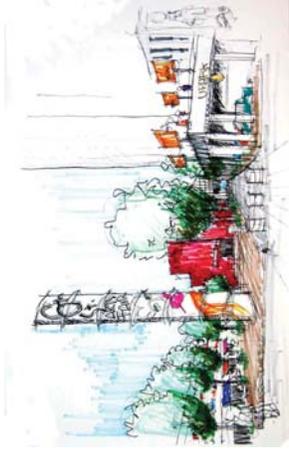


## Five Conceptual Ideas

1. **Center Off-Center:** The downtown's major public open space could be located off the street grid (off-center) at several possible, nearby alternate locations, and be achieved at once.
2. **Park Street Boulevard:** NE 4th Street could become a major landscaped boulevard; a linear park extending from and connected to Downtown Park.
3. **Ridge to Bay Trail:** A meandering path from the ridge along 108th Ave NE to Downtown Park and Meydenbauer Bay offers an urban retreat for varied recreational activity and a pedestrian short-cut.



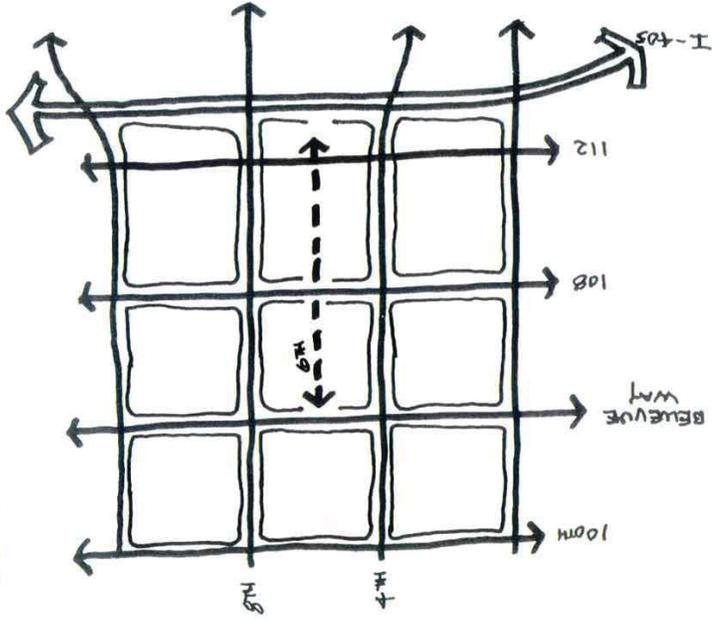
4. **Branded Street Districts:** Imagine NE 6th Street as a wireless high-tech communication corridor with media displays at the transit center plus links with the retail district (Bellevue Way), the arts and entertainment district (106th Ave NE), and the financial district (108th Ave NE) all ablaze with unique communication screens and signage.



5. **Downtown Circulator and Parking:** A downtown circulator could connect a spiral of public and private places around the downtown plus a new major public parking garage at the "Center Off-Center."

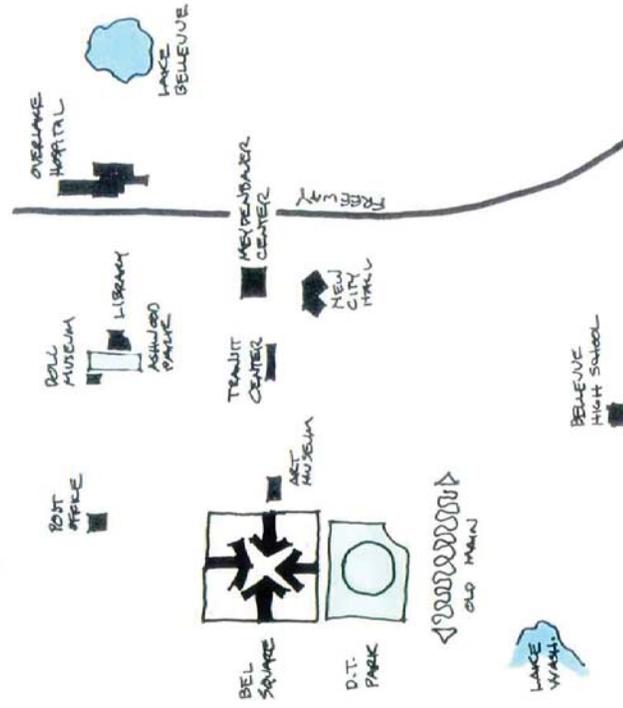
## The Challenge

HOW CAN DOWNTOWN BELLEVUE NEIGHBORHOODS BE BEST DISTINGUISHED?



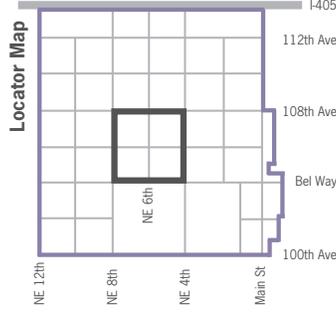
## The Opportunity

STRENGTHEN THE RECOGNITION & IMAGE OF MAJOR LANDMARKS - PUBLIC & PRIVATE

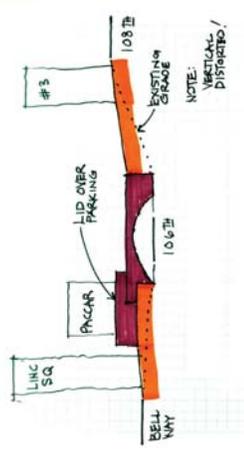
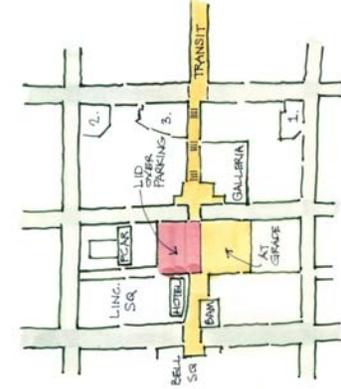




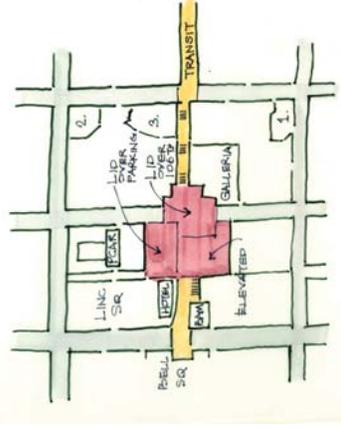
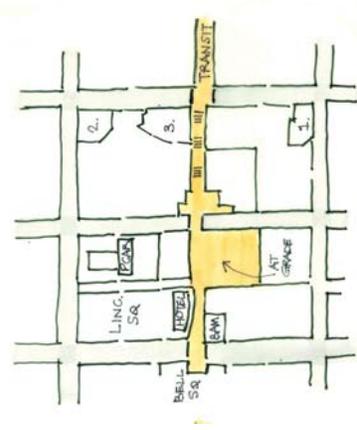
# Options for “Center Off-Center”



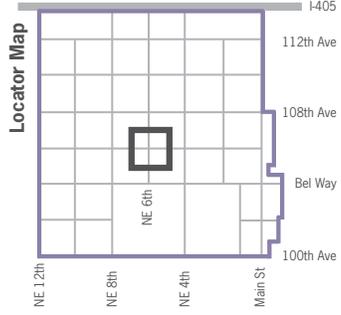
What about creating a little bit of irregularity?  
 Downtown's major public open space at NE 6th Street and 106th Ave could be located off the street grid, or “off-center.” Alternative designs are shown here for at-grade and elevated options.



Elevated Terraces

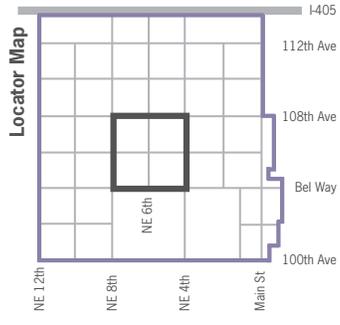
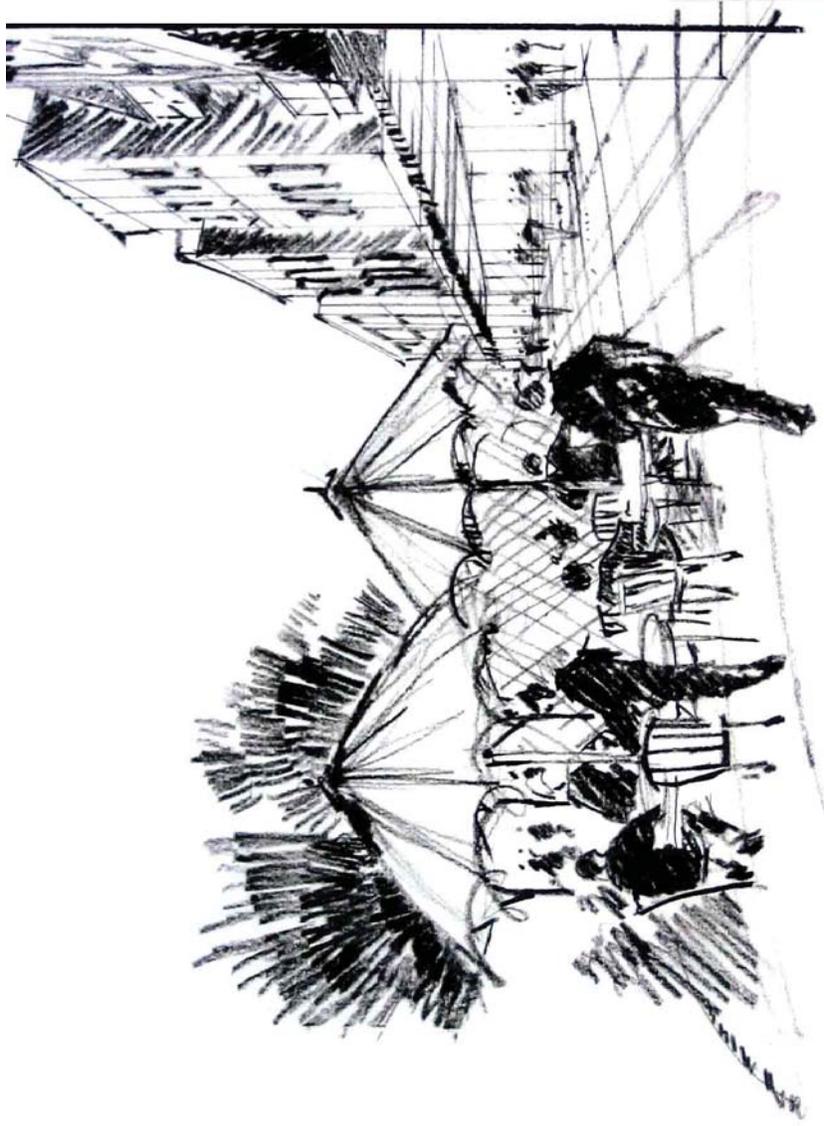


## A Hub of Activities at the “Center Off-Center”



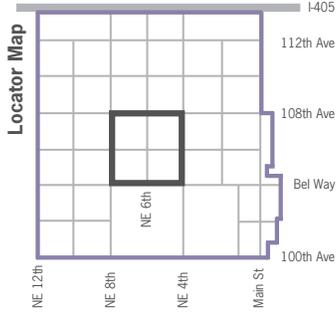
The “center off-center” would be home for a variety of activities—basically the place where people come together. Shown here are restaurants, outdoor cafés, a skating rink, and water features.

## Sidewalk-Oriented Uses



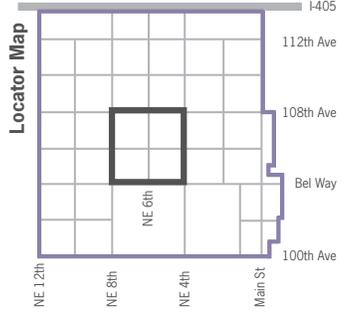
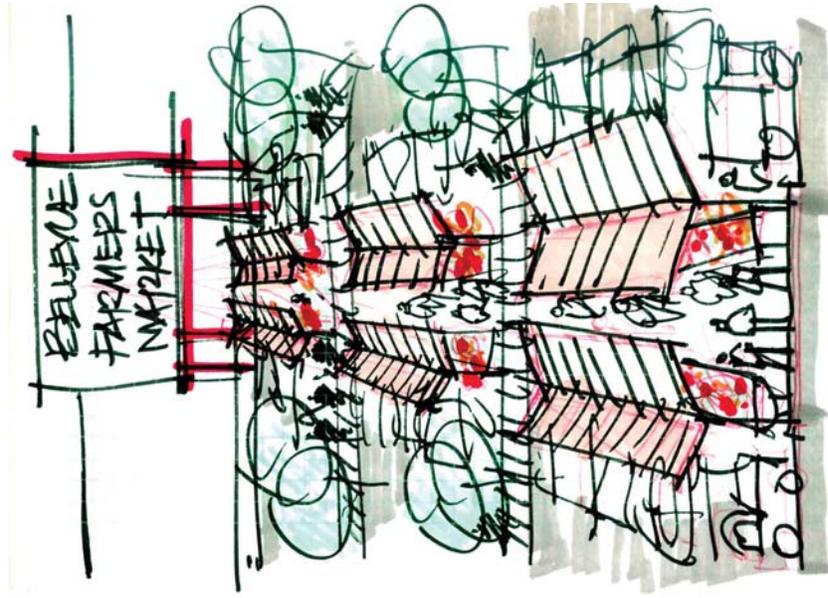
The key to the richness and vitality of the "center off-center" will be uses that spill out onto the sidewalks.

## Artist Space



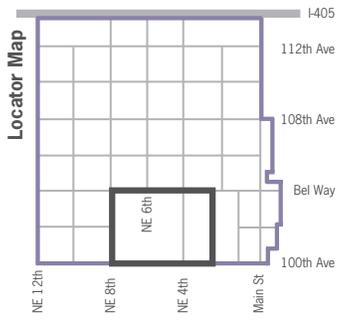
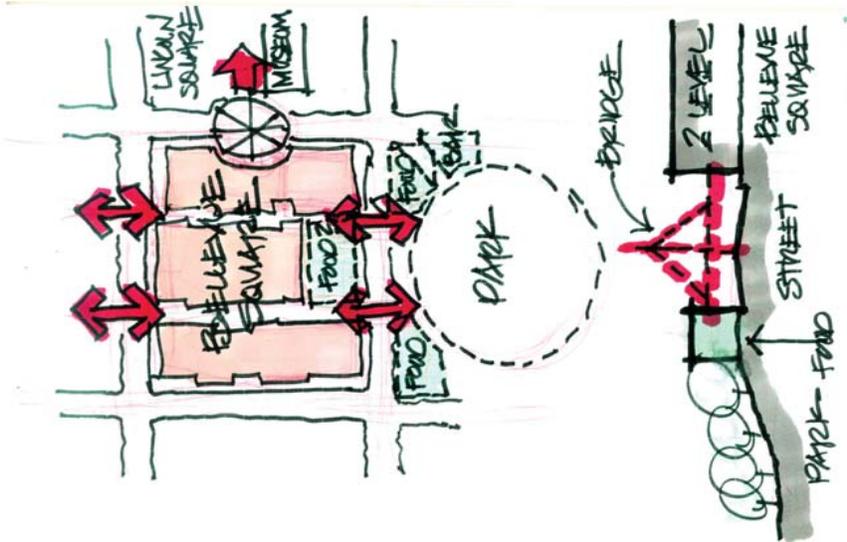
One idea to help enliven downtown as well as increase housing diversity is to locate artist live/work spaces near the "center off-center."

# Downtown Farmers Market



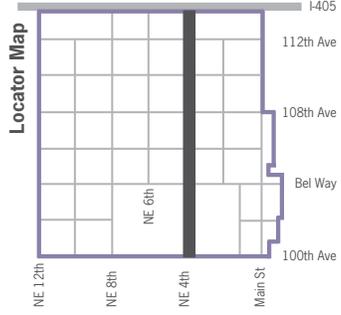
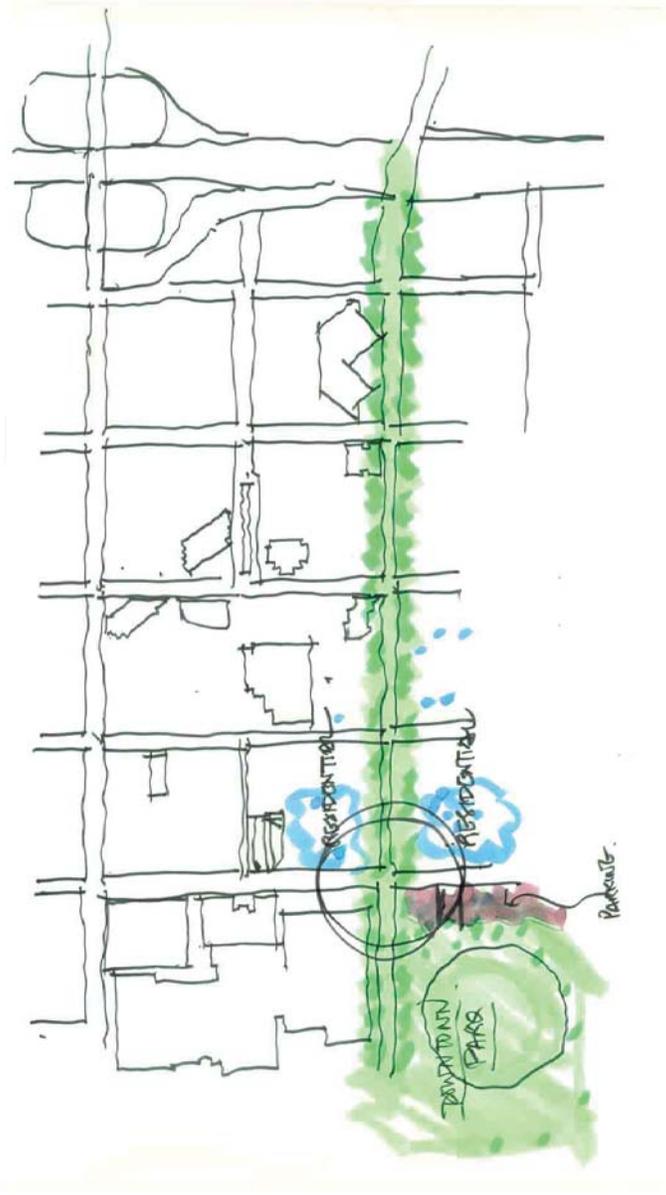
The Bellevue Farmers Market (currently held north of downtown) may someday want to locate in the core of downtown. Flexible space could be developed at the "center off-center" to host what could be a signature event.

# Connection from Bellevue Square to Downtown Park



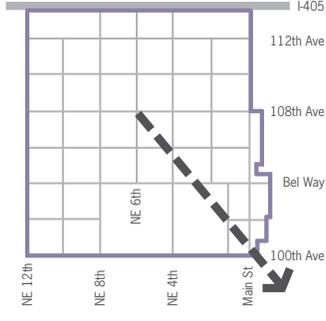
There currently isn't much connectivity between Downtown Park and Bellevue Square. There may be the opportunity to significantly enhance this interface by spanning NE 4th Street with a pedestrian bridge and providing park-oriented retail at the north end of the park and south end of Bellevue Square.

# Park Street Boulevard



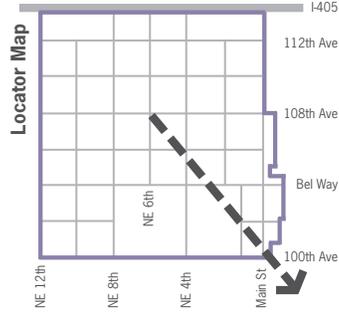
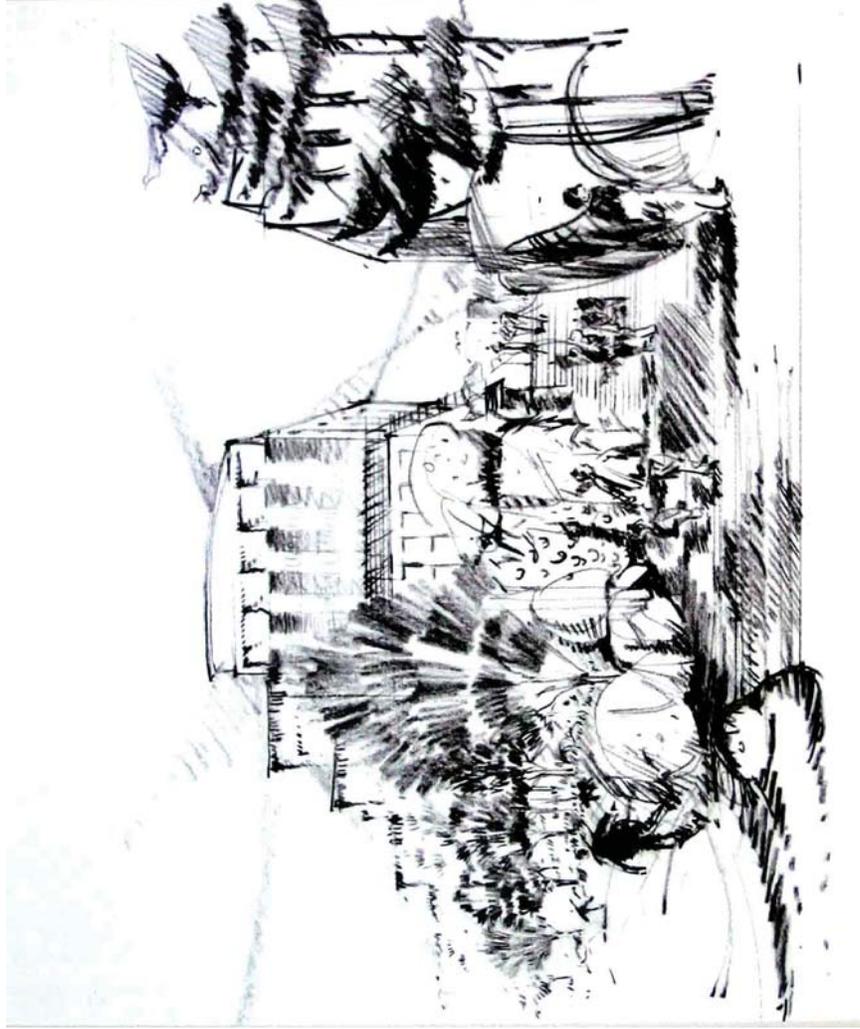
Park Street Boulevard (NE 4th Street) is shown here as a linear park feature that begins at Downtown Park and "reaches out and connects with the downtown." It would not affect auto volumes on NE 4th, but would mark this as a signature design element for the city.

# Ridge to Bay Trail



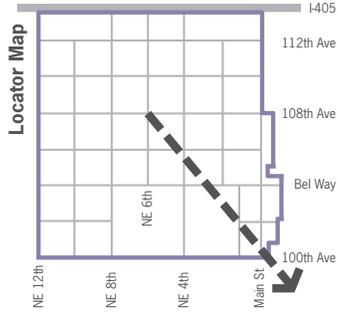
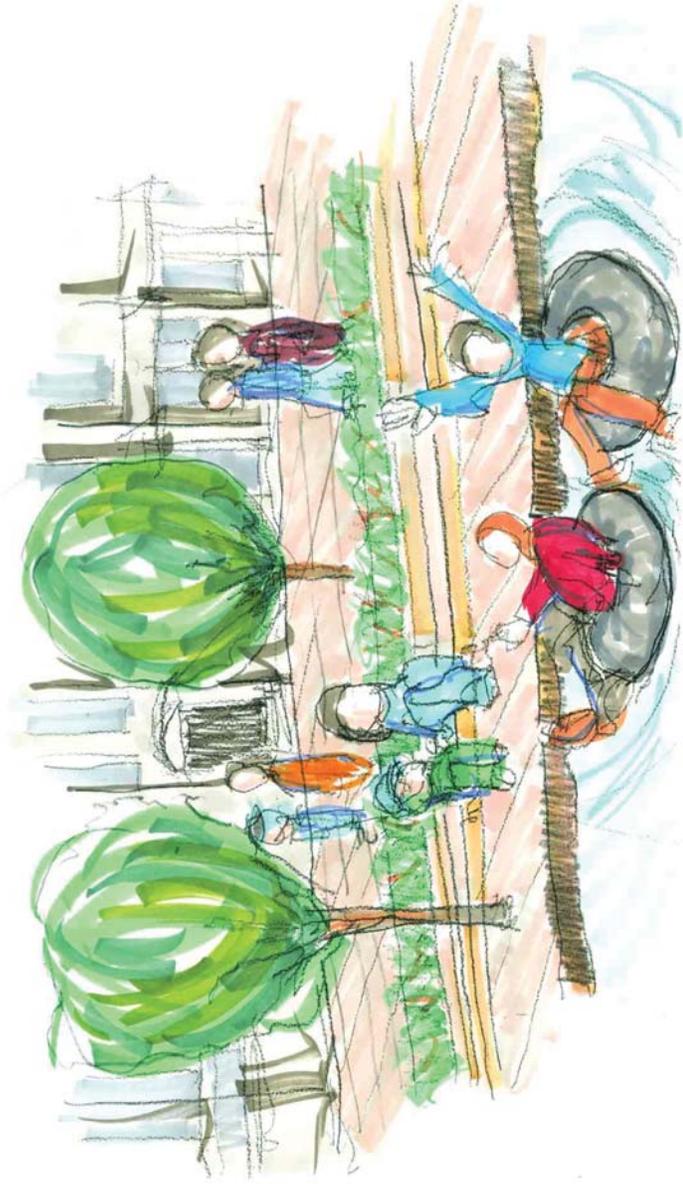
A Ridge to Bay Trail could extend from the core of downtown (near the high point at 108th Ave) to Downtown Park and on to Meydenbauer Bay.

## Ridge to Bay Trail



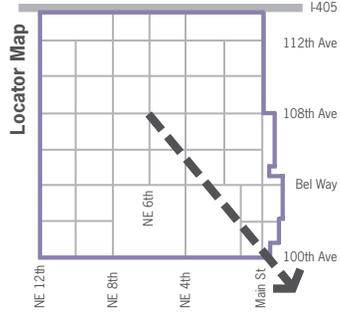
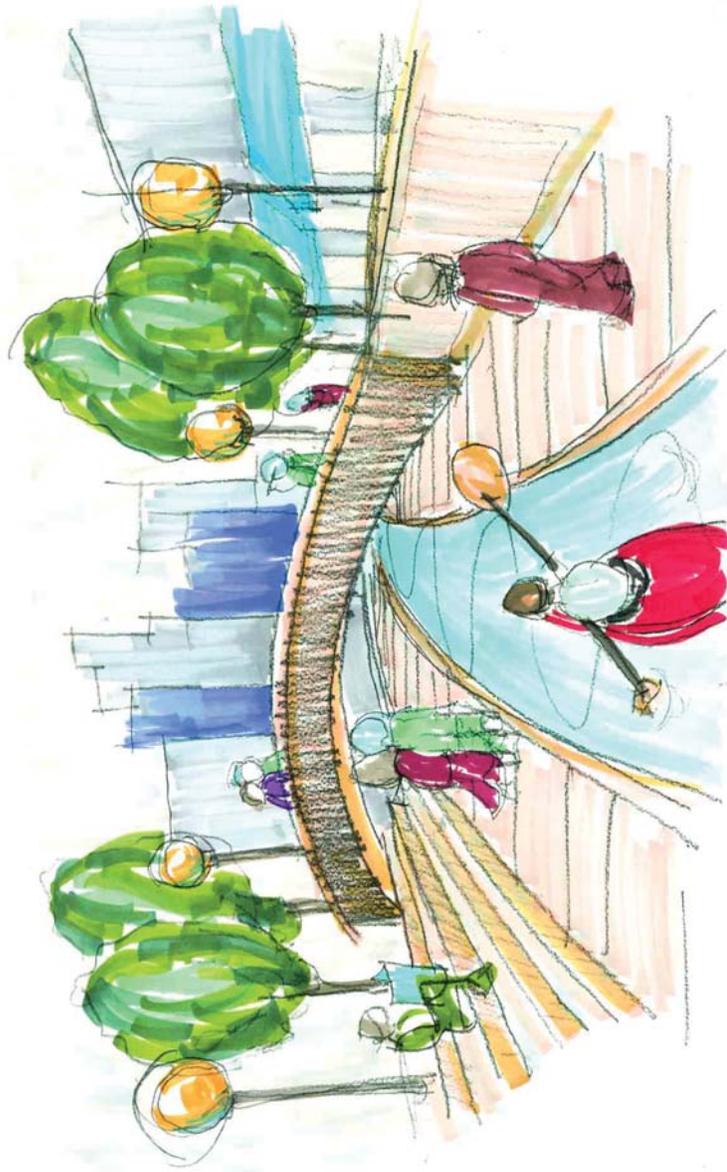
The Ridge to Bay Trail would infuse serendipity into the street grid by occurring along street edges as well as through superblocks. The change in elevation along the route would provide the opportunity for interesting design treatments.

## Downtown Waterways



The vision may want to include a series of waterways that connect the core of downtown with Downtown Park and Meydenbauer Bay.

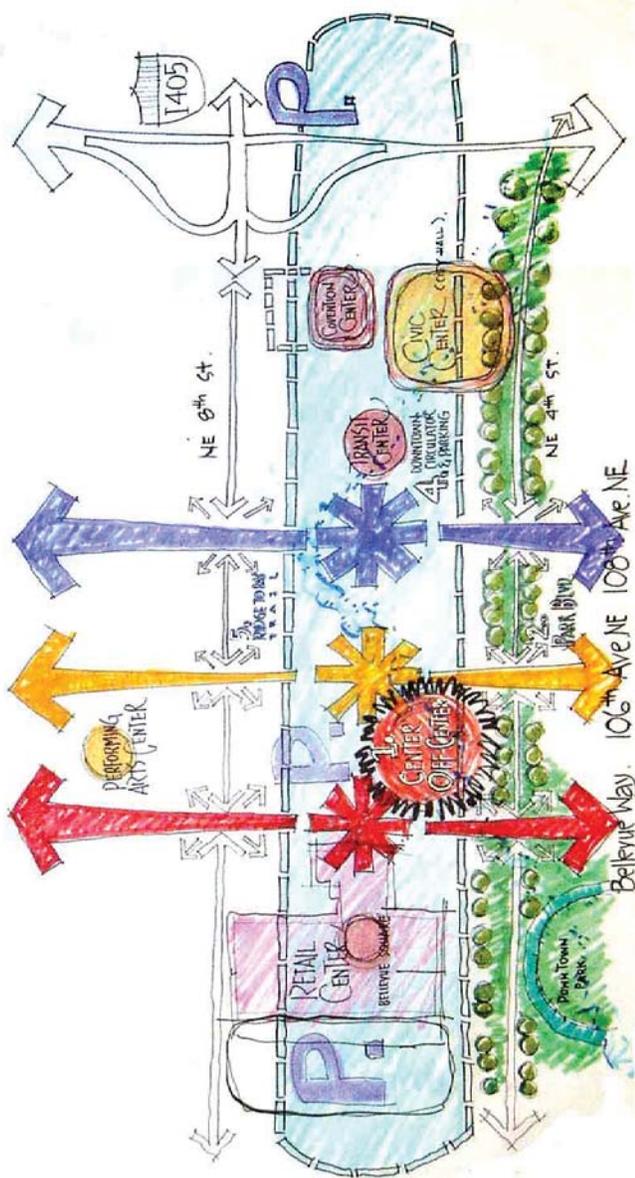
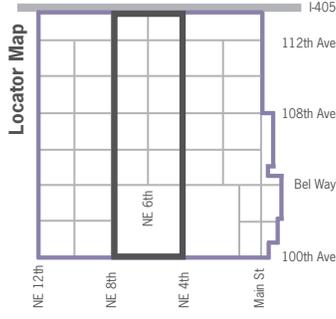
## Recreation Opportunities



Truly unique and memorable recreation opportunities may be possible through a series of downtown waterways.

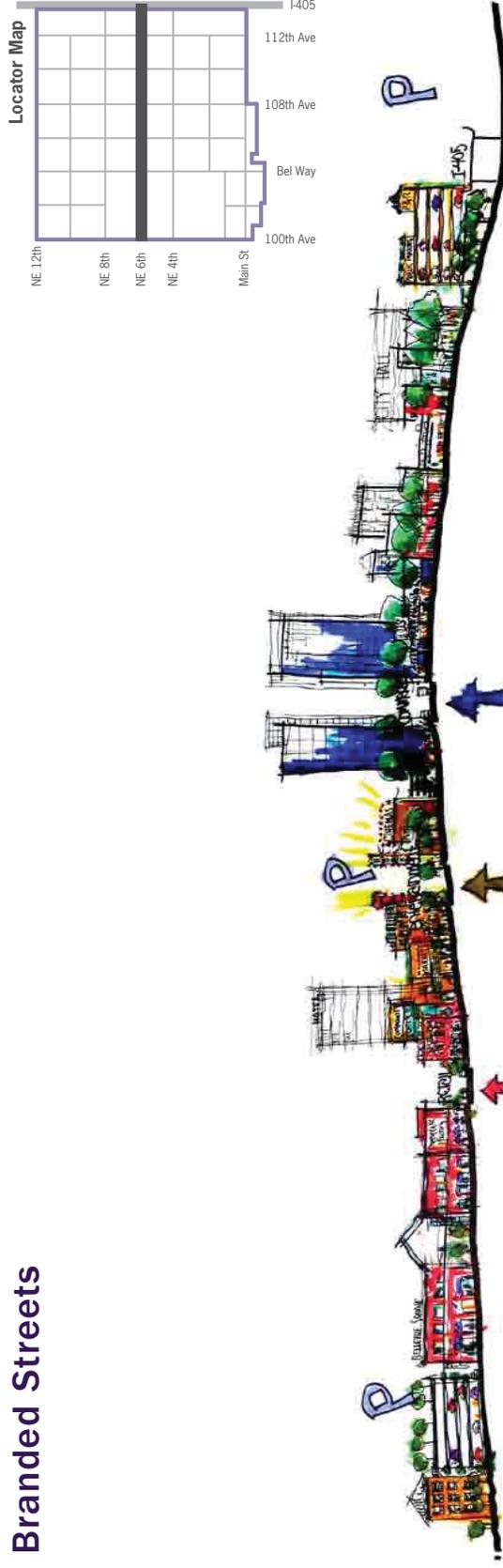
# Branded Streets

RETAIL  
 ARTS & ENTERTAINMENT  
 COMMERCIAL



The three major north-south "branded streets" (Bellevue Way, 106th Ave, and 108th Ave) that run through the Eastside Center district will have major focal points where they intersect with the NE 6th Street Pedestrian Corridor. Each focal point should provide a distinguishable design feature that anchors the branded street and links NE 6th from east to west.

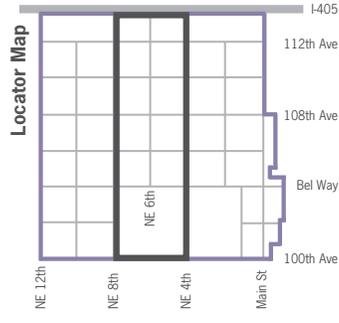
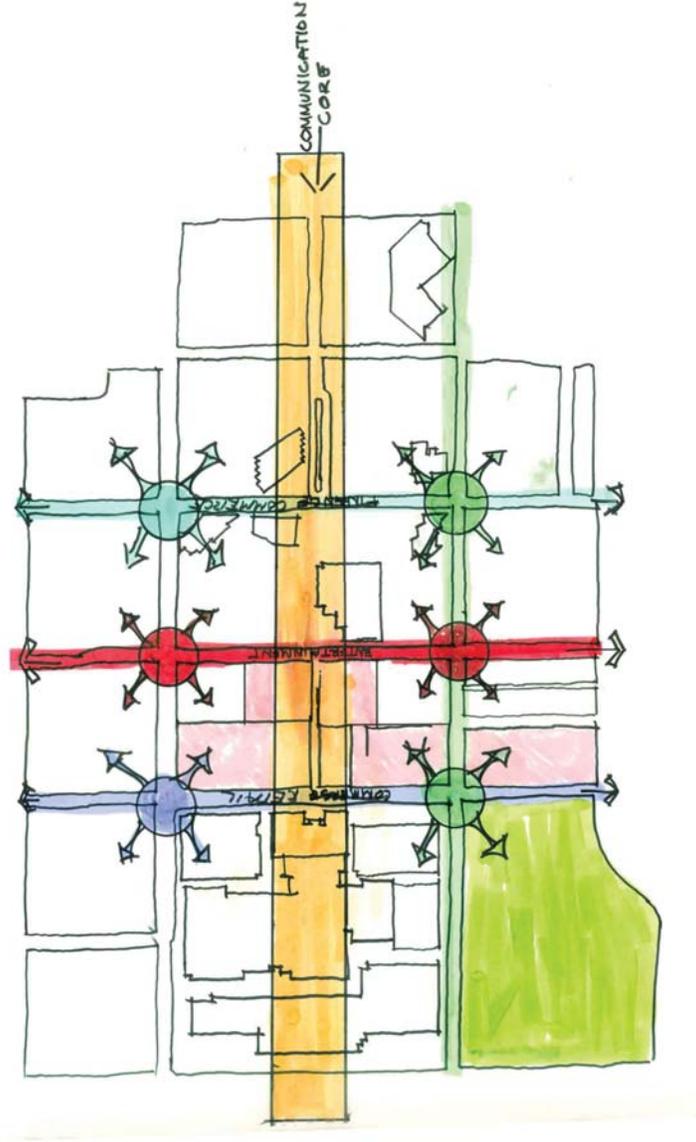
# Branded Streets



This sketch of NE 6th Street shows the elevation change from east to west and the general building forms adjacent to each branded street. The three "P's" show major parking locations.

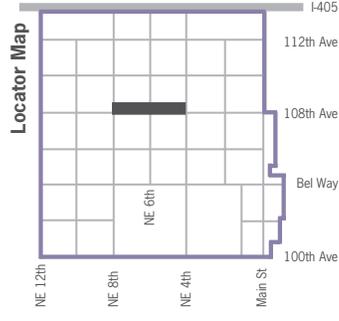
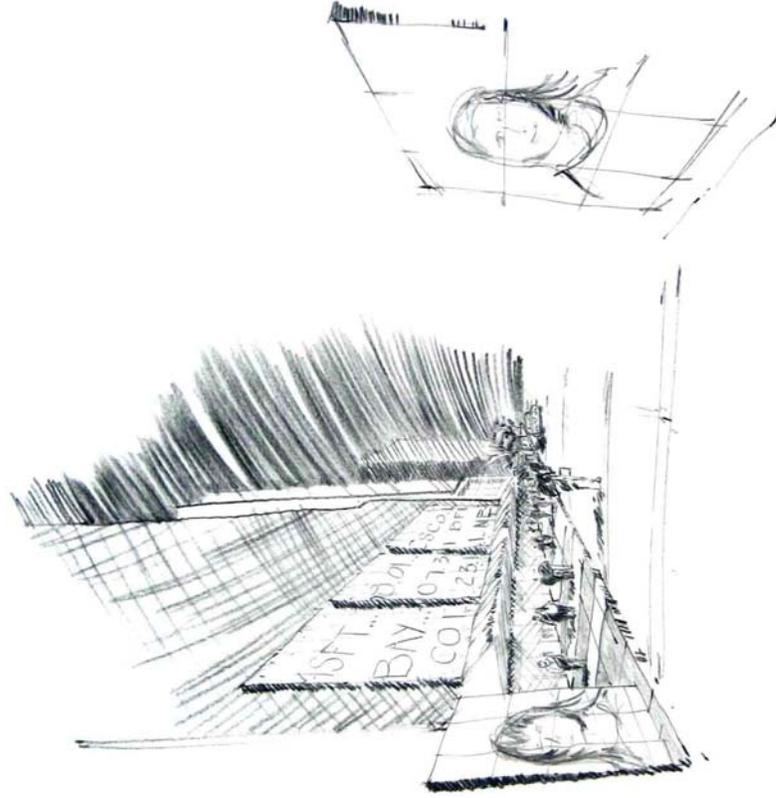
Bellevue Way "Grand Shopping"  
 106th Avenue "Entertainment Ave"  
 108th Avenue "Commerce Ave"

## Branded Streets and Communication Core



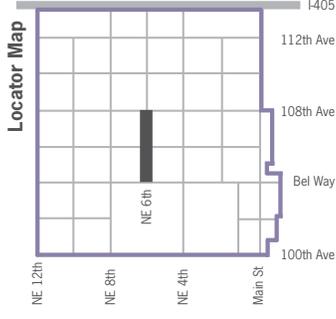
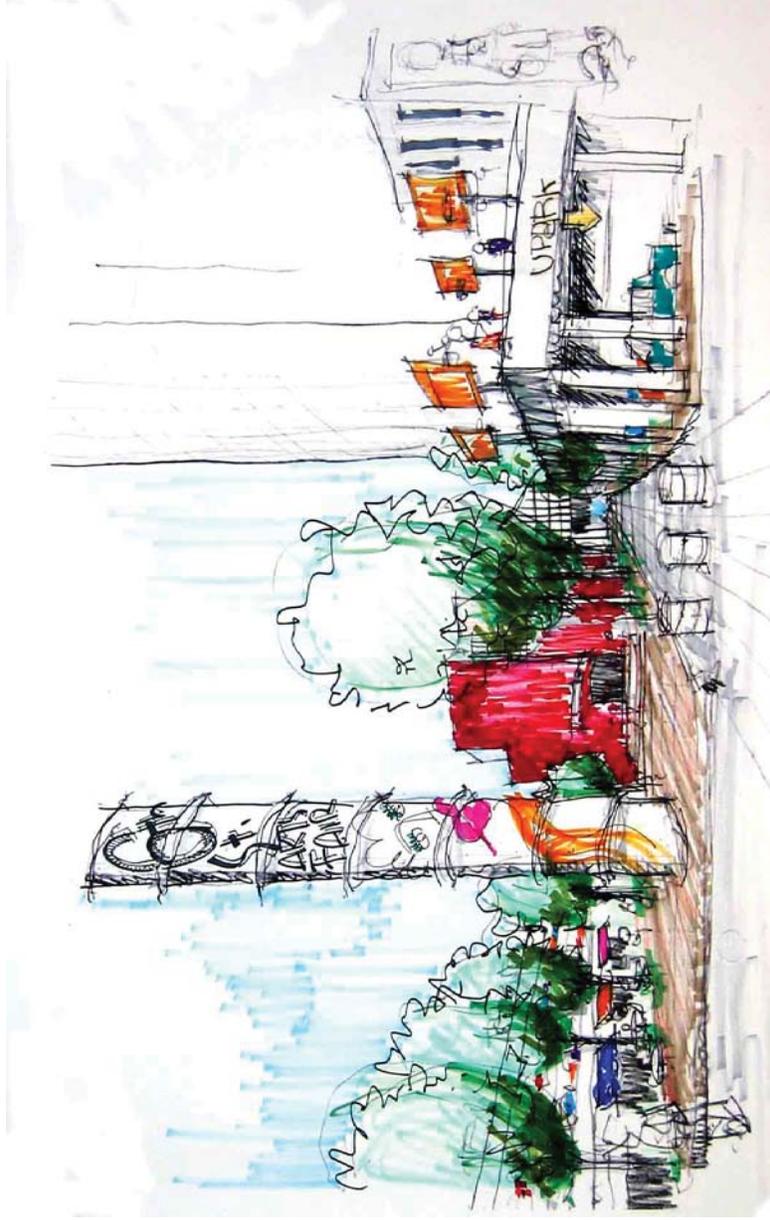
The north-south branded streets all cross the NE 6th Street Pedestrian Corridor, which is itself a branded street. There may be the opportunity to enliven NE 6th Street from east to west by developing a communication core that has wireless internet access, reader boards, and other high-tech features that tie the corridor together.

# Commerce Avenue on 108th



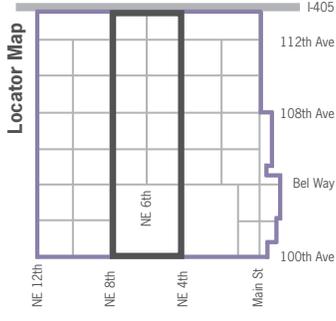
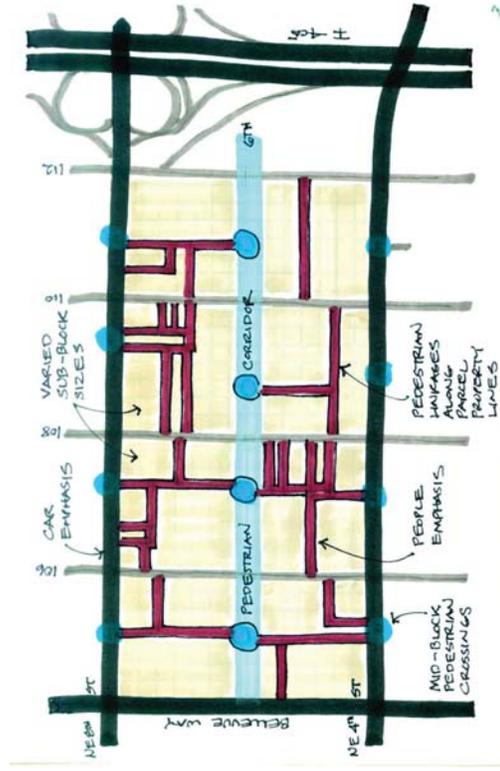
Commerce Avenue could have continually changing large wireless video screens for things such as news headlines, stock quotes, or images from around the world. It would help signify 108th Avenue as Downtown Bellevue's, as well as the greater Eastside's, primary business address.

## 6th Street Arts Fair

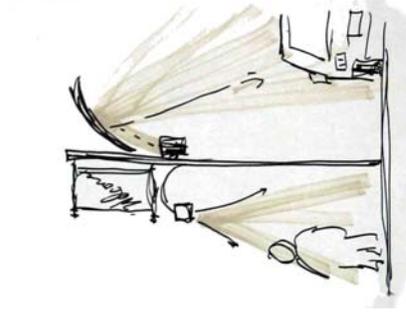


The area around the major public open space could be home to unique design features such as a symbol for the "6th Street Arts Fair."

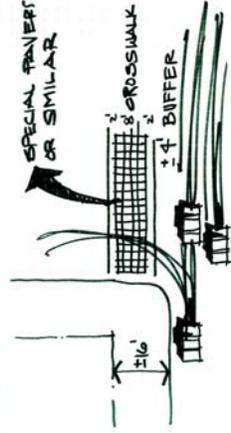
# Pedestrian Realm



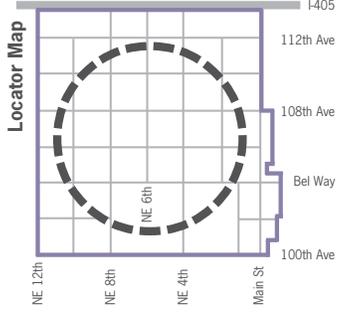
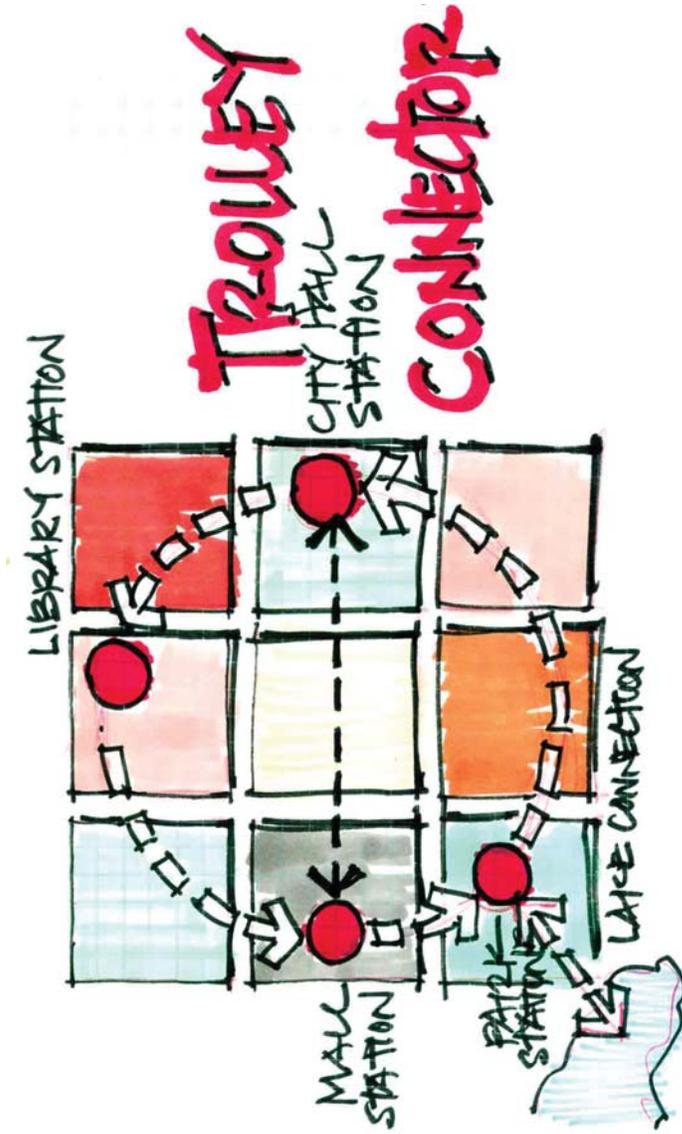
There are a number of potential improvements that could be made to the pedestrian realm in Downtown Bellevue. These include more pedestrian-scaled lighting, crosswalk treatments, and better recognition of midblock and through-block connections.



## PEDESTRIAN CROSSWALKS



## Downtown Circulator Concept



The circulator concept shown here ties together the nine downtown districts. Residents, workers, and shoppers would benefit from an alternative to driving from place to place. The key is to have something that is very frequent, has an easy to remember route, and connects key attractions.

# Team Four: Hearts & Carrots

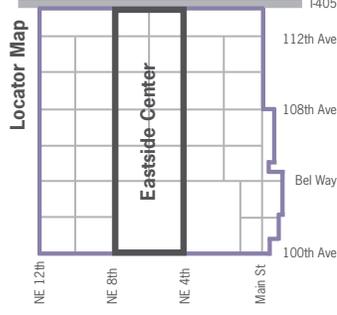
## PARTICIPANTS

- Don Miles, ZGF (Team Leader)
- Becky Ellis, MuivannyG2
- Greg Laird, OTAK
- Scott Melbourne, Mithun
- Chris Peterson, ZGF
- Jamie Roberts, CH2M Hill
- Jon Taylor, Callison Architecture
- Uyen Tran, ZGF
- Carla Weinheimer, Bellevue Staff Resource
- Chad Weiser, OTAK



## ASSIGNMENT

Team Four (along with Team Three) was given the assignment of the "Eastside Center." The Eastside Center is the middle third of Downtown Bellevue, delineated by NE 8th Street to the north, NE 4th Street to the south, 100th Ave NE to the west, and I-405 to the east.



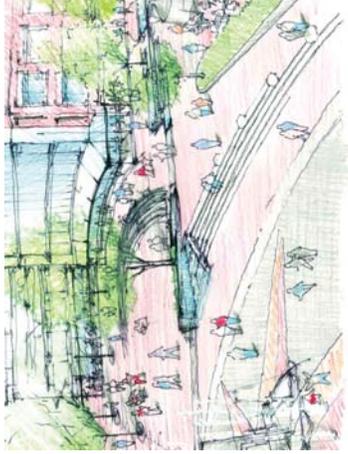
## APPROACH

### Hearts & Carrots

Hearts (special public spaces) are an important part of the Central District. Most of the regional "hearts" are located within this district including the regional mall (Bellevue Square), the regional transit center, a proposed "6th Street Place," and the Civic Plaza (currently under construction). There are also a number of smaller scale "hearts" sprinkled throughout this district.



Along with a collection of great public spaces (hearts), this district offers some important opportunities for enhanced critical infrastructure design in order to achieve appropriate "place" and "streetscape" design. The successful development of this infrastructure is critical to the success of the public experience of the district as well as the economic benefit of the developments adjoining the public infrastructure. Carrots (public investment, incentives and public/private partnerships) will be needed to achieve an appropriate public infrastructure for a high quality urban experience.



The public experience of the district is defined by the quality of its streets and places. An appropriate pedestrian experience needs to be designed into the streetscapes and places in the district. Vehicular traffic was the basis of the original network design and must be included in the vision for the future. However, the



vision needs to expand to include better accommodations for the pedestrian to allow the city's residents, workers and visitors to engage the community at street level. Streets need to be viewed as "places to be." They need to accommodate and balance all modes of travel. Our team has proposed section designs for the primary streets in this district that will

enhance the pedestrian experience and will help define the appropriate development response for a particular street.

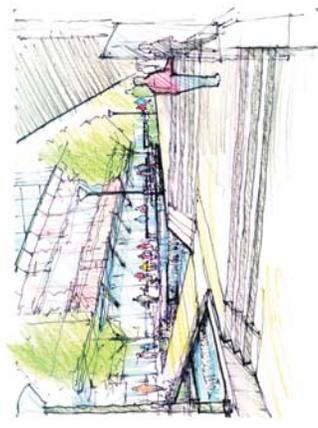
The Central District is currently divided into sections that are defined by Bellevue Way, 106th Ave NE, 108th Ave NE, and 110th Ave NE respectively. Traversing these zones is NE 6th Street which is the only pedestrian dominated street in the downtown. One block (between 106th Ave NE and 108th Ave NE) is pedestrian only. Our team sees the development of NE 6th Street as crucial for the development of the downtown core. This corridor



runs east-west and currently runs from the transit center to an entrance to Bellevue Square. The team feels strongly that the character of this street changes dramatically from block to block, from the vehicular/people intensity of the transit center section, to the pedestrian only/regional gathering place of the Compass Plaza/Cascade Stair section, and then ending with the European style small-scale (two lane) street leading up to the primary Bellevue Square entrance on Bellevue Way.

The team has proposed some design concepts for the Compass Plaza/Cascade Stair section including hardscape, landscape, and water features. This section (called 6th Street Place) is meant to be experienced as a

singular destination for downtown residents and employees. It is also meant to be a regional destination as events are planned for the large public gathering space. At the intersection of the NE 6th Pedestrian Corridor and Skyline Drive (108th Ave NE) we see the addition of a fountain or sculpture as an attraction for pedestrians on both sides of the ridge. The heights and setbacks of the buildings surrounding Compass Plaza should be set to promote views into the plaza, drawing people to 6th Street Place day and night. These building heights and setbacks should also promote the pedestrian exploration of the nearby business and retail areas.



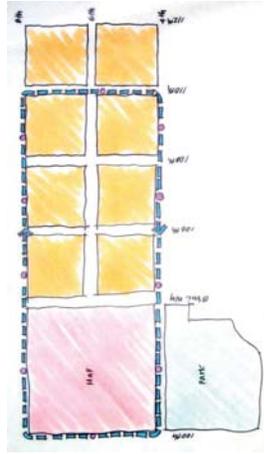
The team has also proposed some specific design ideas for the section of NE 6th Street between 106th Ave NE and Bellevue Way. The uniqueness of the smaller scale (two-lane) street section should be celebrated with

encouragement of smaller scale retail, restaurants, and cafés that create a destination street place of special scale and quality. At the end of the smaller scale street the pedestrian transitions to the excitement of the larger scale retail environment on Bellevue Way and the primary entrance to Bellevue Square. We see



pedestrians encountering a larger and more inviting entrance into the mall with significant transparency that will visually extend NE 6th Street into the central public space in the mall.

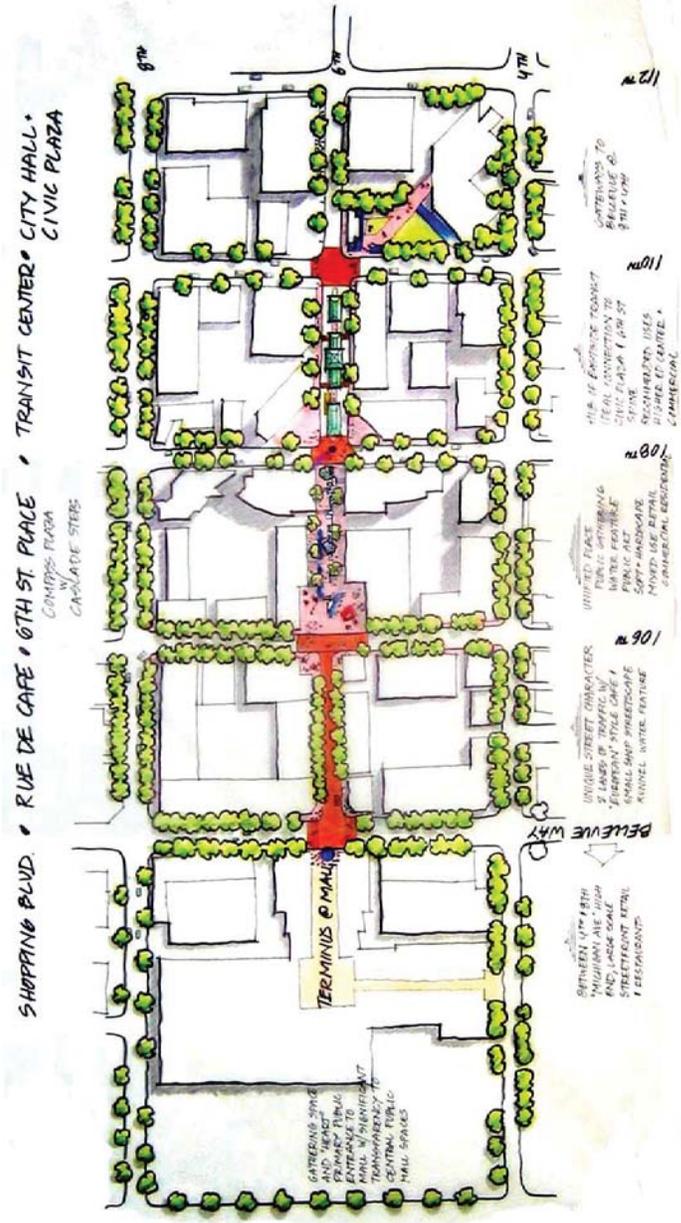
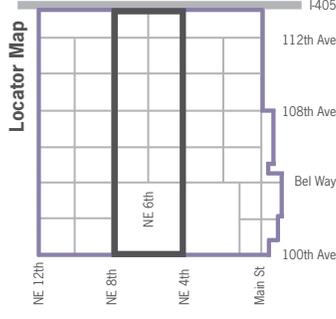
We see the Central District being able to better serve the pedestrians (and the "park-once" goal) through the provision of a transit loop. This transit loop will allow more pedestrians to access all zones throughout the day as well as connect to existing features that lie outside the city core. The route for this transit loop should travel in a clockwise direction and run along NE 8th Street to 110th Ave NE to NE 4th Street to 100th Ave NE. The



loop will be no further than one block from the pedestrian corridor. Transit stops are proposed to deposit pedestrians at locations that will allow for further explorations of the district as well as the Downtown Park. Mid-block stops will serve and connect the retail, entertainment, commercial, and civic zones as well as allow the residents and shoppers surrounding the core to navigate all corners of the Central District. The transit technology might be a rubber tire electric trolley or a modern rail trolley similar to Portland's.

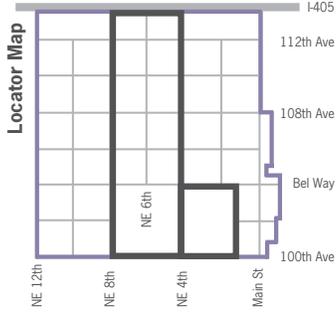
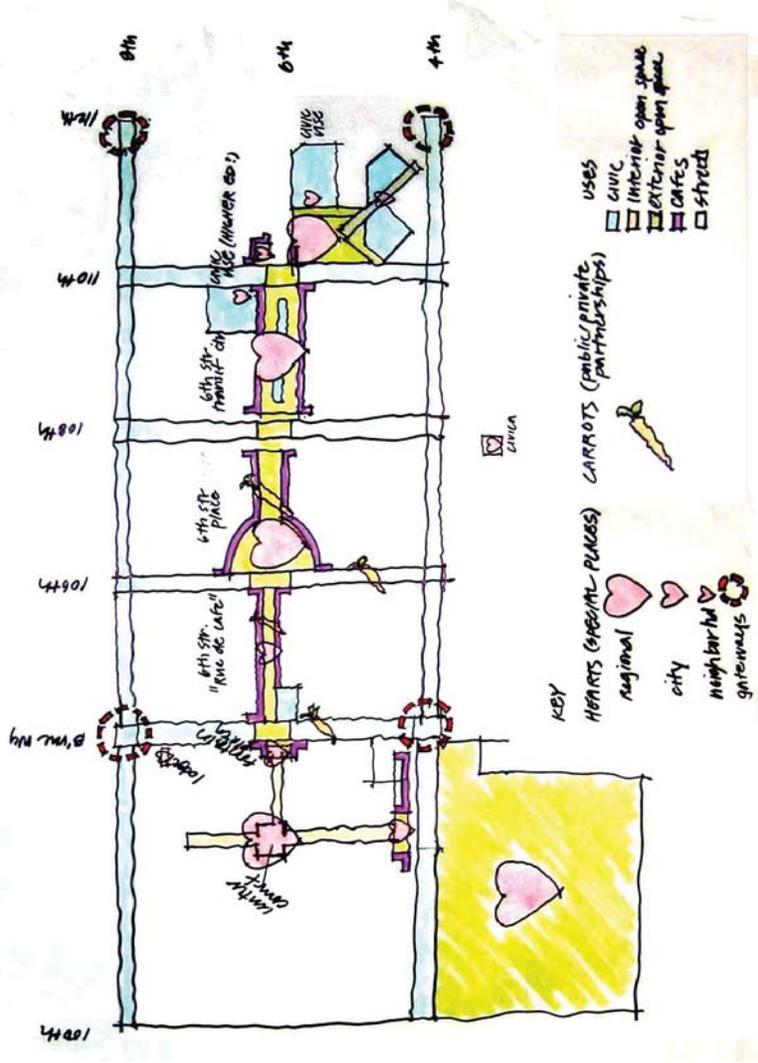
As development continues in Downtown Bellevue, the success of the Central District will be defined by the success of its "hearts" (public places of gathering) and its high quality development of public infrastructure requiring the "carrots" of public investment, incentives and partnerships with private developers.

# NE 6th Street as Organizing Feature



The Eastside Center District is currently divided into sections that are defined by Bellevue Way, 106th Ave NE, 108th Ave NE, and 110th Ave NE respectively. The NE 6th Street Pedestrian Corridor acts as a key organizing feature for this area. The vision depicted in this graphic is for unique design features and activities to occur along the length of the street, each with a very special and memorable character.

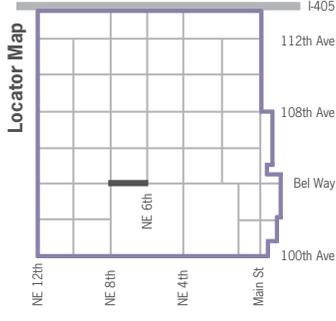
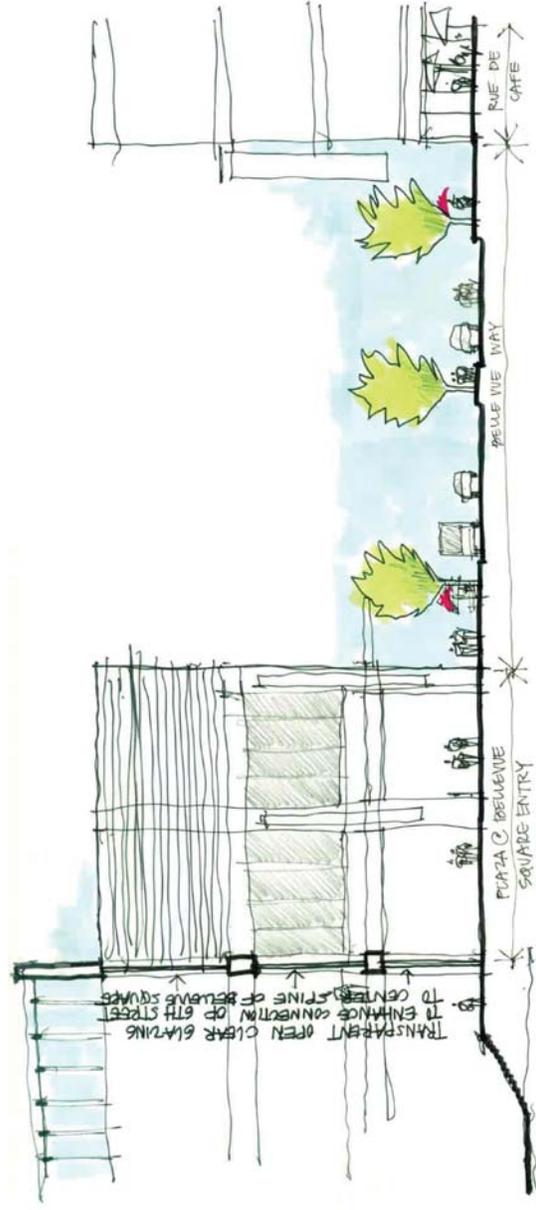
# Hearts & Carrots Framework



There are a number of hearts (special public places) sprinkled through the Eastside Center District. There are both regional "hearts" such as Bellevue Square, Downtown Park, and the "6th Street Place" major public open space, to smaller scale "hearts" like the Rue de Cafe.

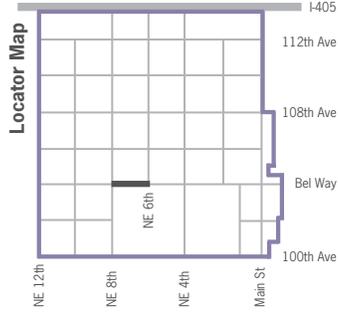
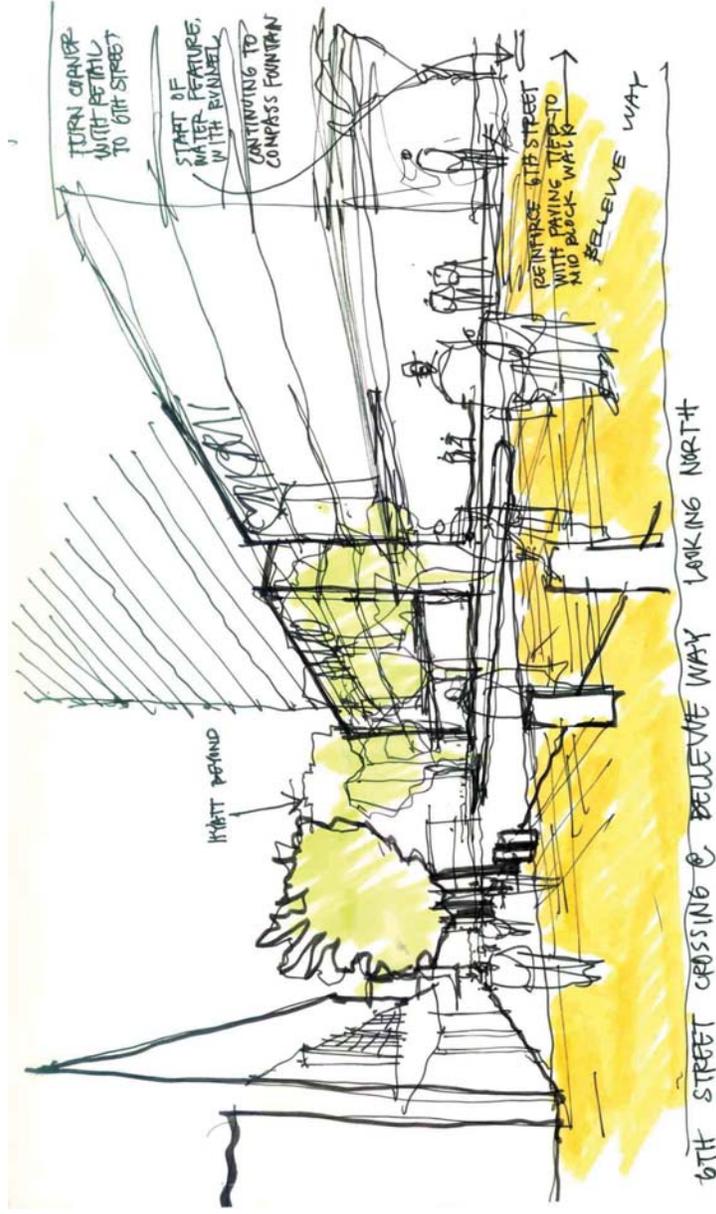
The carrots signify opportunities for public investment, incentives, and public/private partnerships.

# Bellevue Way (Looking North)



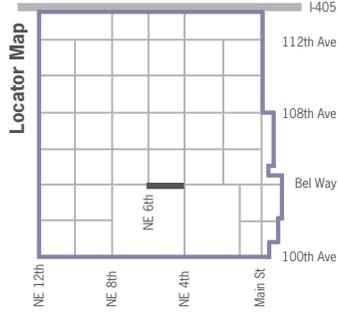
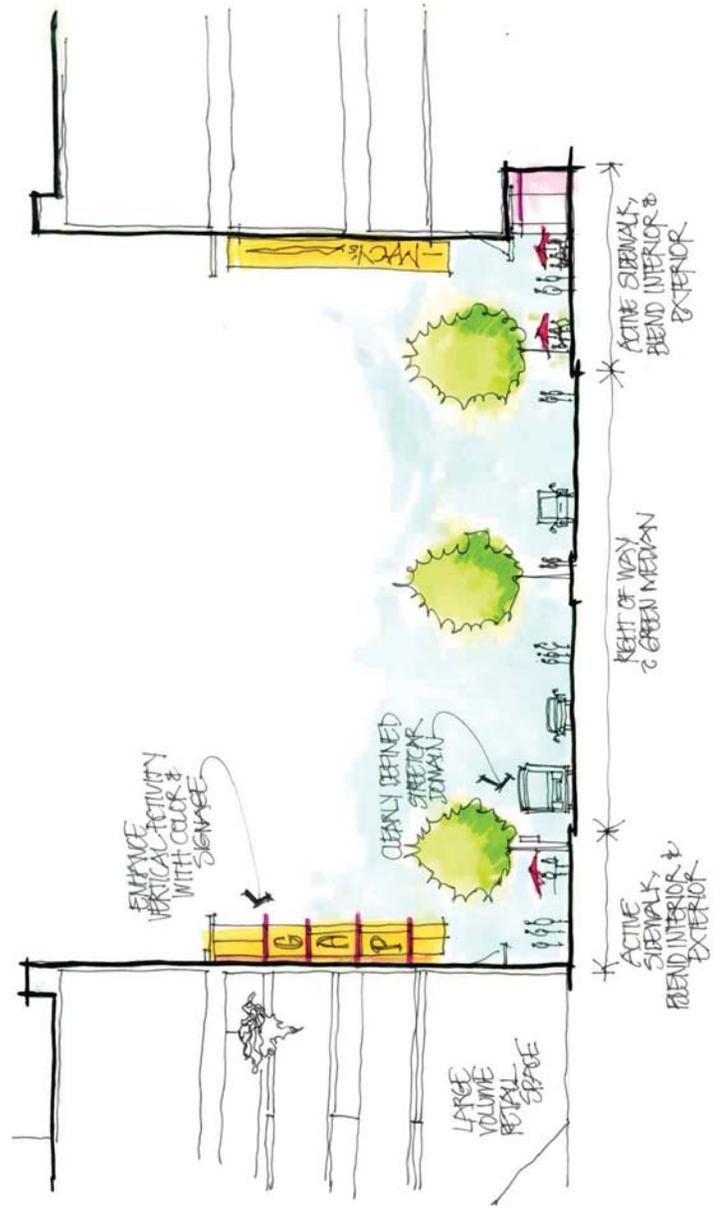
Bellevue Way will continue to evolve as a grand shopping street, similar to North Michigan Avenue in Chicago, that anchors the west end of the NE 6th Street Pedestrian Corridor.

## Bellevue Way (Looking North)



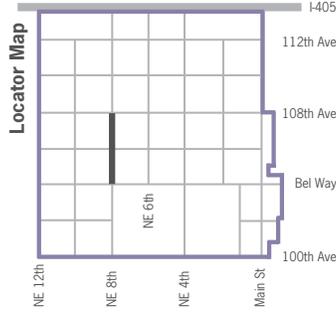
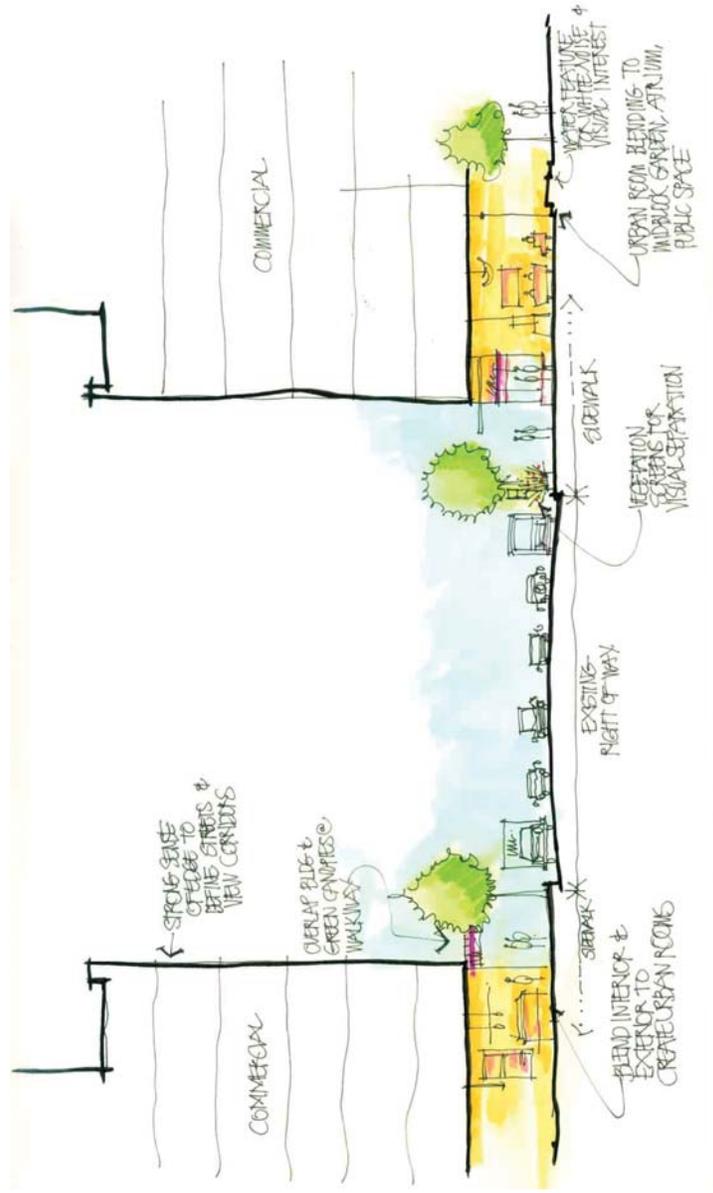
Pedestrian activity along Bellevue Way has grown considerably over the past few years. With more development oriented to the sidewalks and investments made to celebrate the pedestrian, this trend will continue.

# Bellevue Way (Looking South)



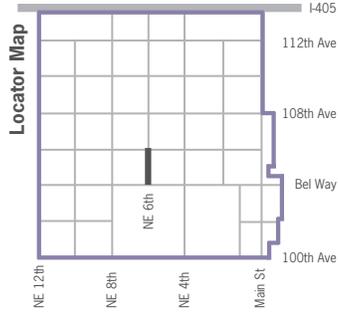
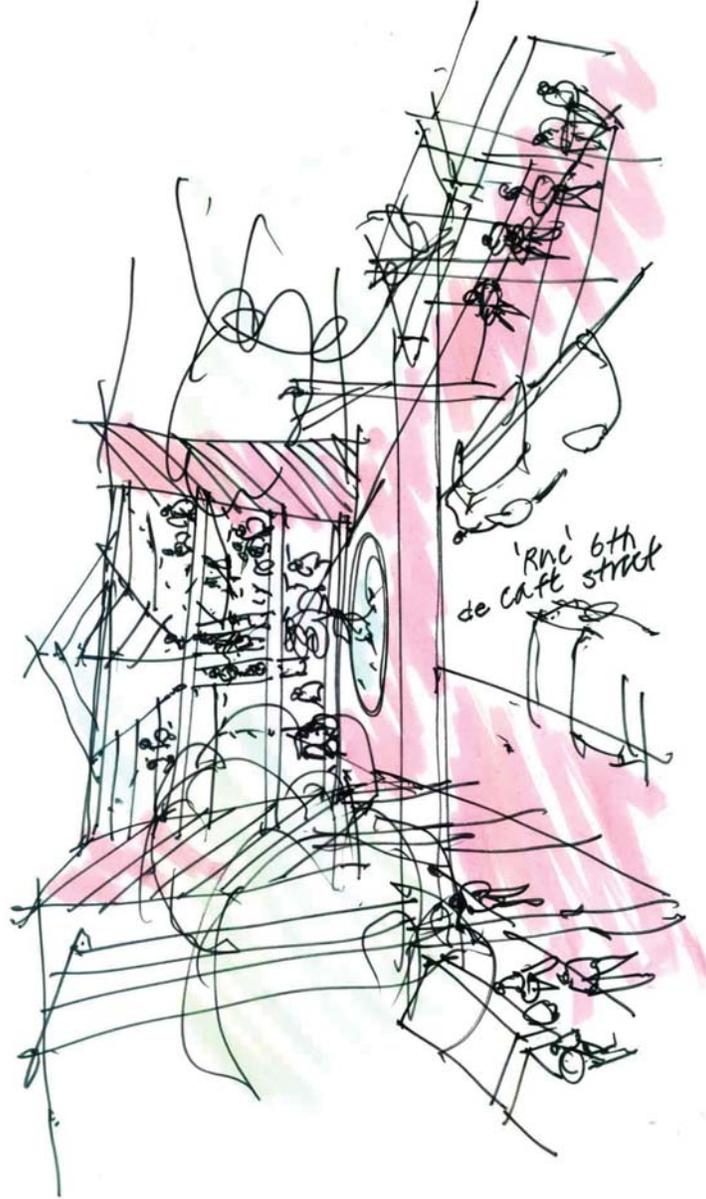
The character of Bellevue Way as a grand shopping street will continue south of NE 6th as redevelopment occurs in the future.

# NE 8th Street (Looking East)



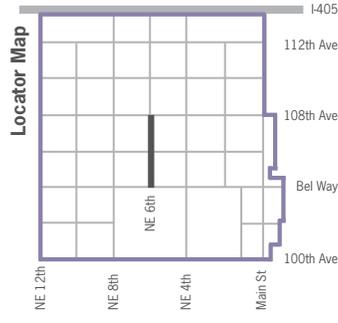
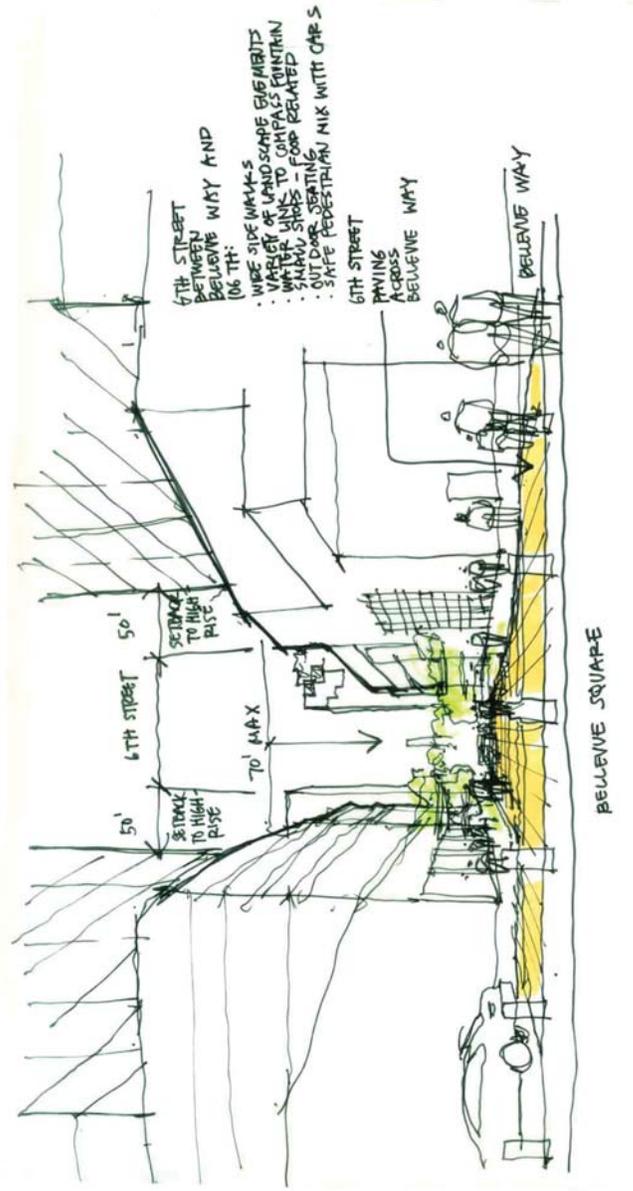
NE 8th will continue to be an auto-bias street with high auto volumes and large retail and office uses, but there may be opportunities for vegetative buffers and other urban amenities that make the pedestrian experience more pleasant.

## Open Up Bellevue Square to NE 6th Street



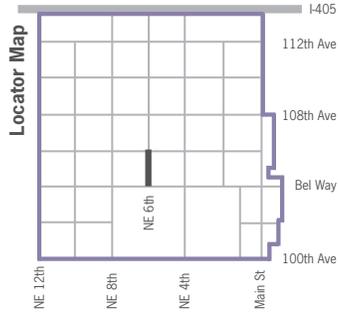
There is a big opportunity to have an enhanced connection between Bellevue Square and NE 6th Street. A redesign of the fountain plaza and entrance to Bellevue Square, as well as public investment in the pedestrian crossing at Bellevue Way, would help draw activity from one area to the other.

# NE 6th Street (Looking East from Bellevue Square)



The view from Bellevue Square down NE 6th Street will change significantly as projects under construction are finished and future redevelopment of other sites occurs.

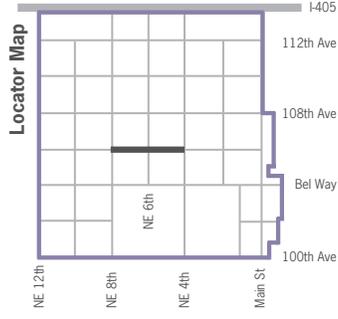
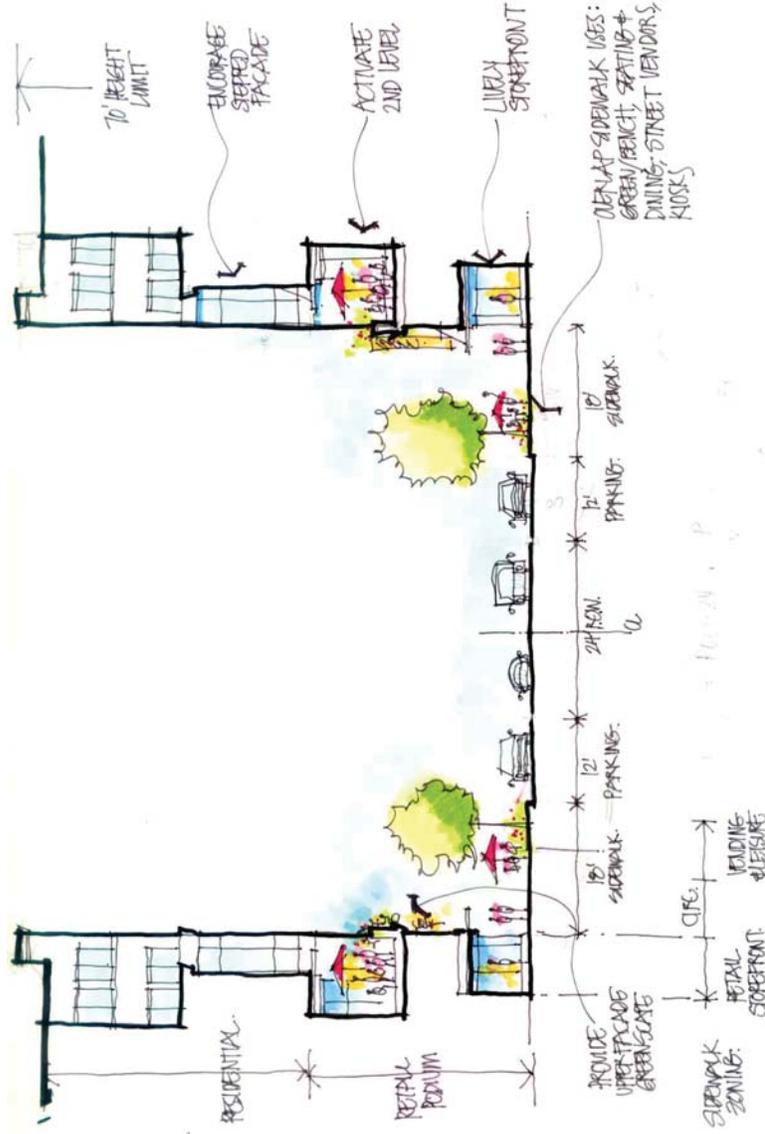
# Rue de Café (Bellevue Way to 106th Ave)



The Rue de Café (NE 6th between Bellevue Way and 106th Ave) will be a narrow street with outdoor dining and green pockets lining the sidewalks. Traffic volumes and speeds would be low, with provisions made for valet parking and drop off areas.

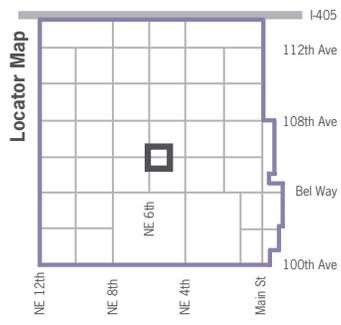
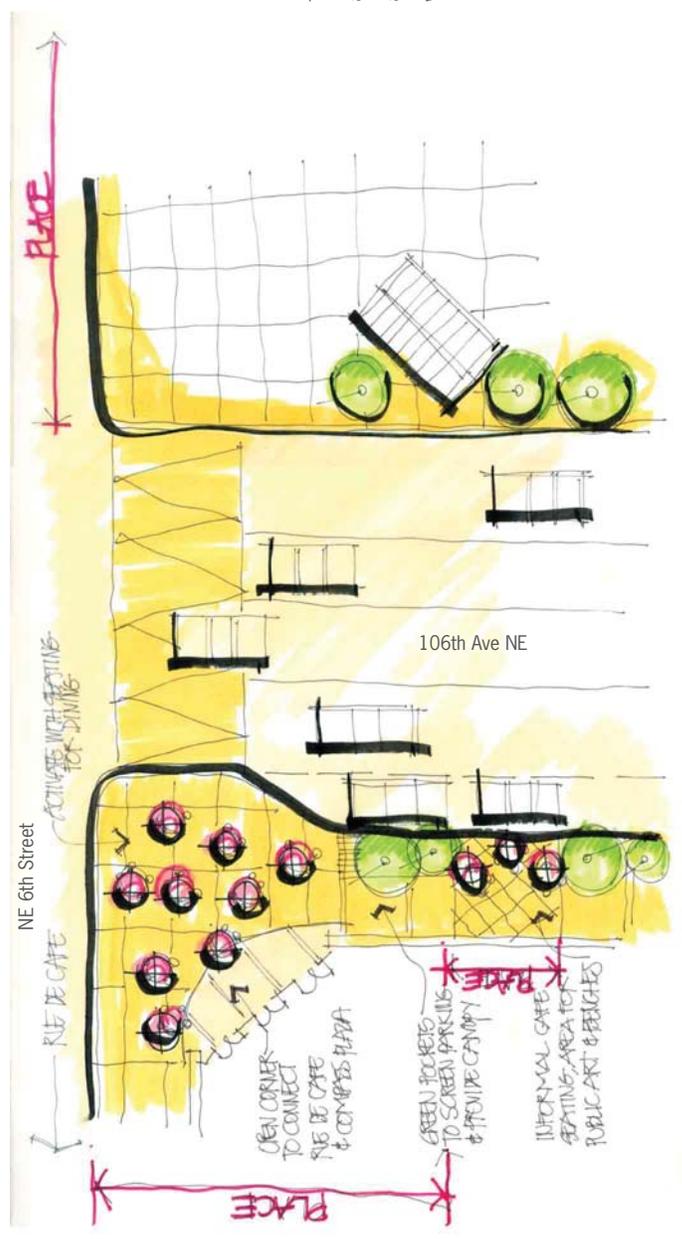


# 106th Ave Section



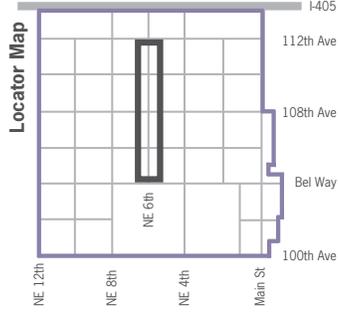
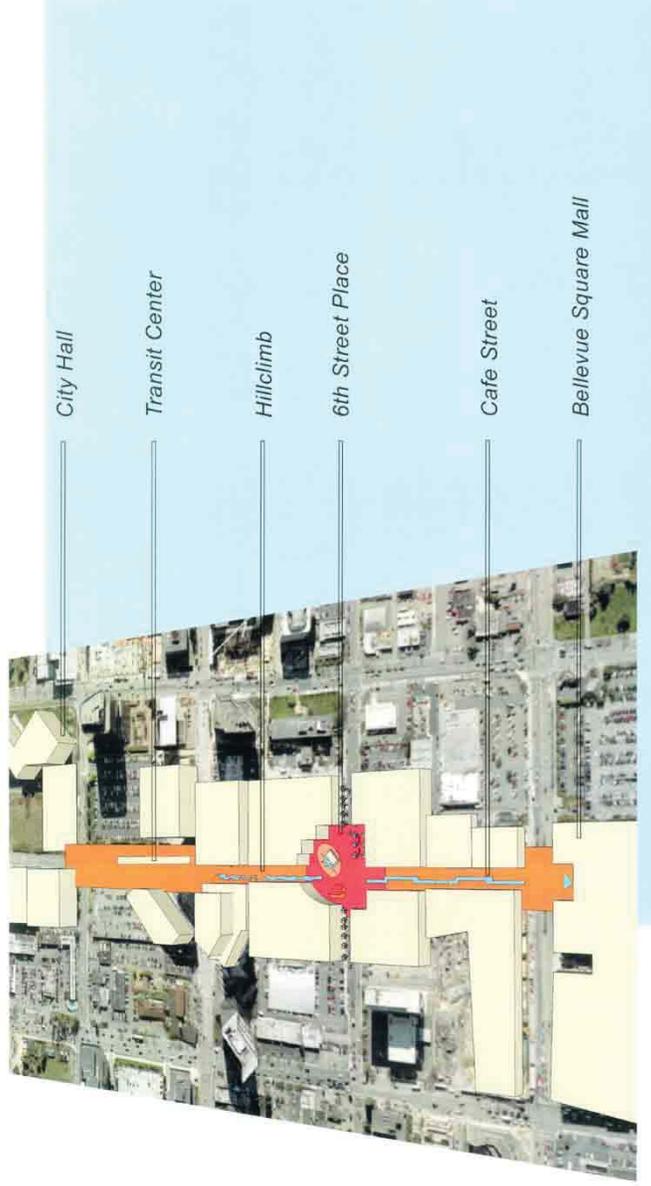
The vision for 106th Ave could include portions with residential stacked above active ground-floor retailing and cultural uses. The use of an activated second level would enhance the vitality of this street as Bellevue's "Entertainment Avenue".

# 106th Ave and NE 6th Intersection



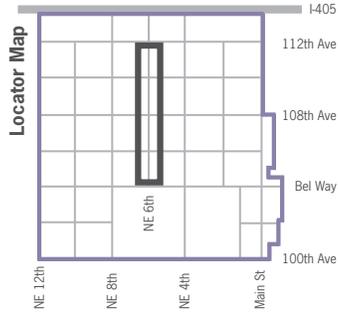
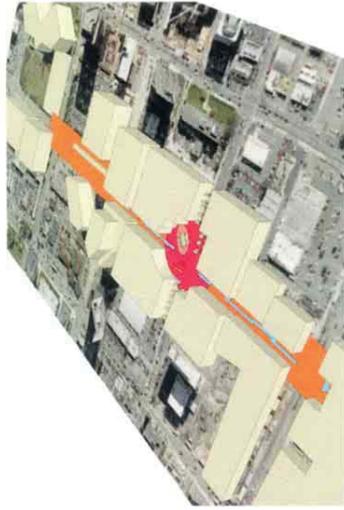
The west side of the intersection of 106th Ave and NE 6th Street provides an opportunity for an open corner to connect the Rue de Cafe with Compass Plaza to the east.

## 6th Street Connections

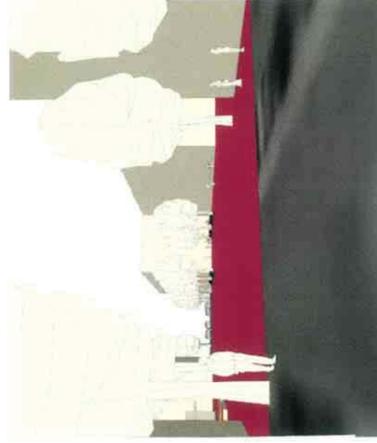
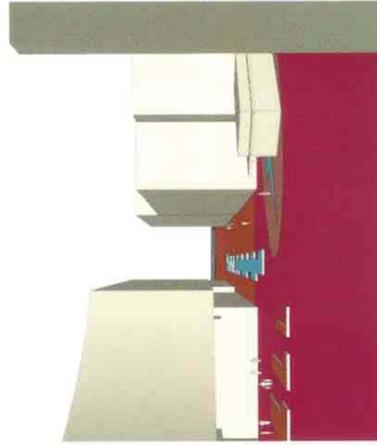


NE 6th Street is the key organizing feature for the Eastside Center District

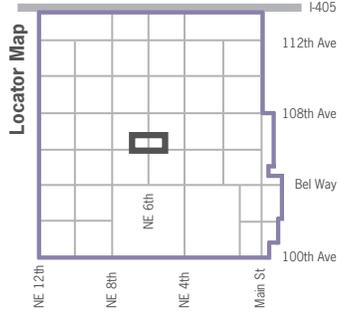
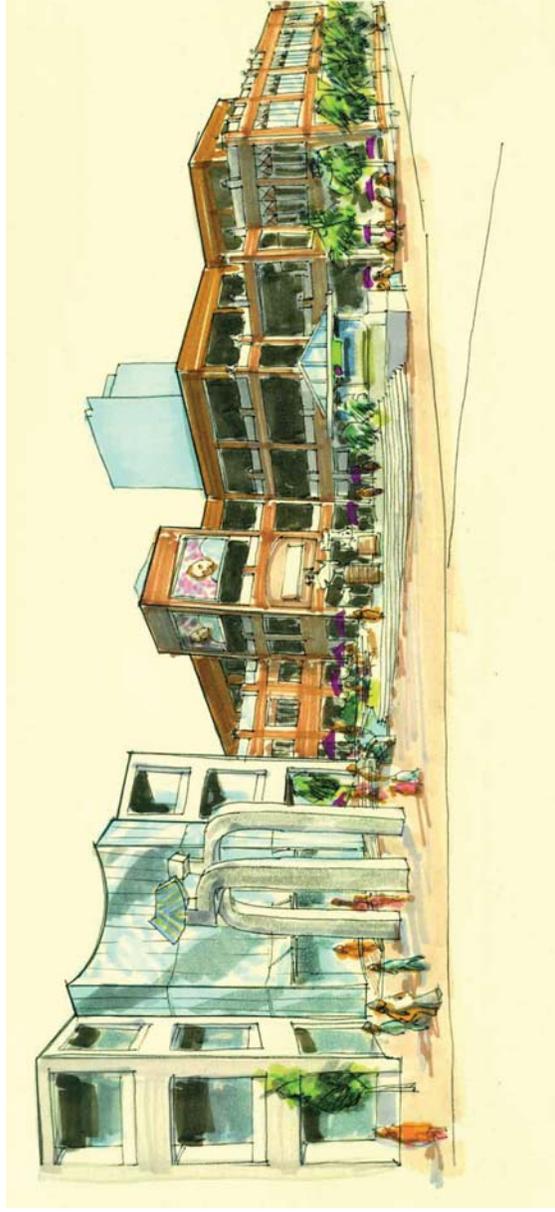
## 6th Street Connections



The significance of the major public open space at NE 6th and 106th Ave is evident by its size and central location in downtown.

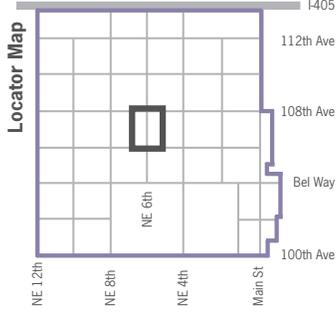
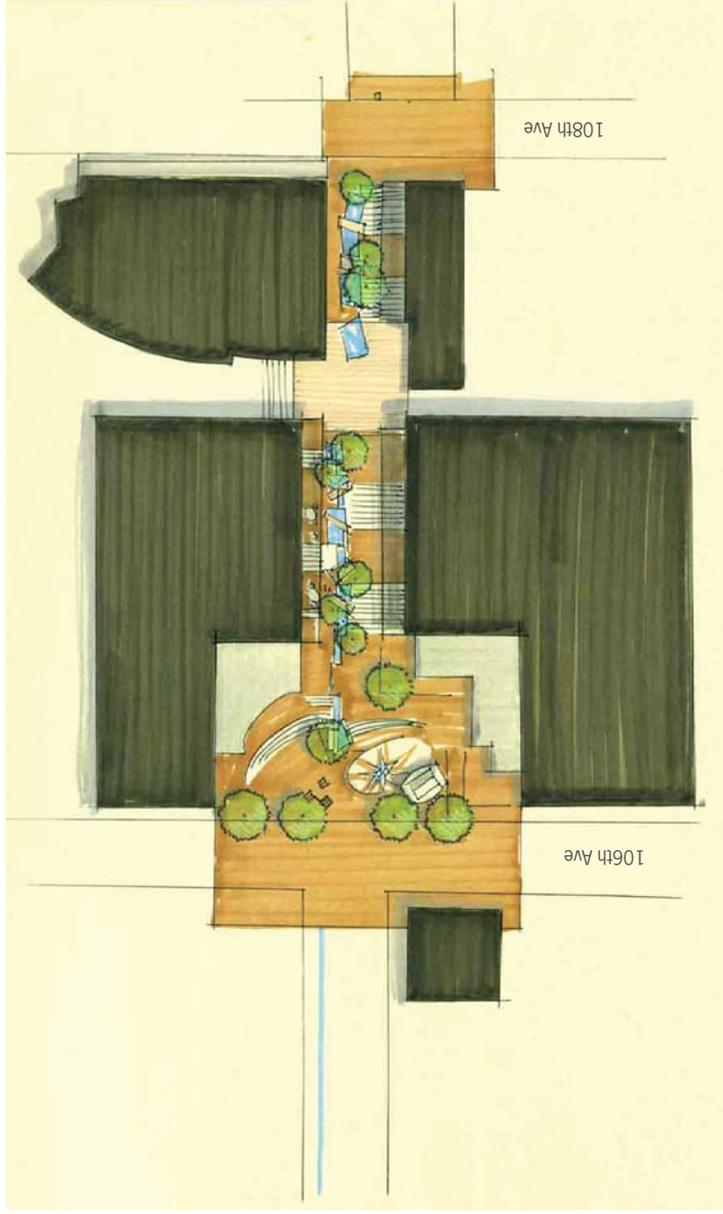


# Compass Plaza Expanded



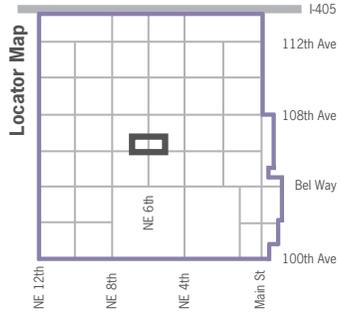
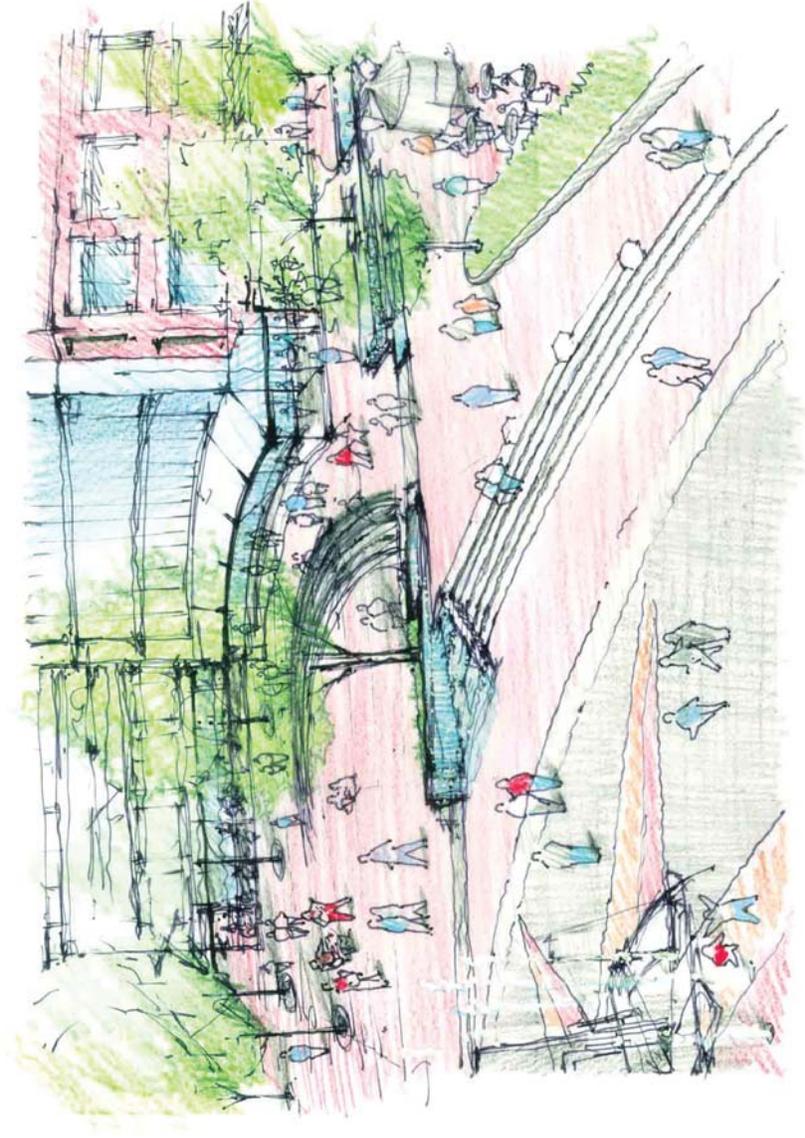
An expanded Compass Plaza would have great exposure to 106th Ave and function as a true town square.

## 6th Street Hillclimb (106th to 108th)



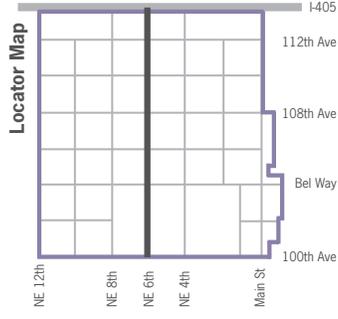
The 6th Street Hillclimb begins at an expanded Compass Plaza and extends up to 108th Ave and the transit center.

## Compass Plaza Expanded



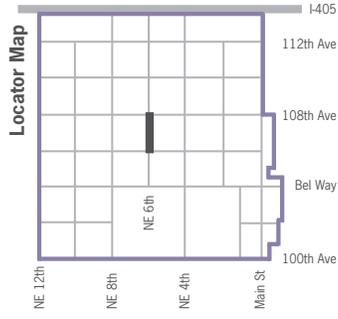
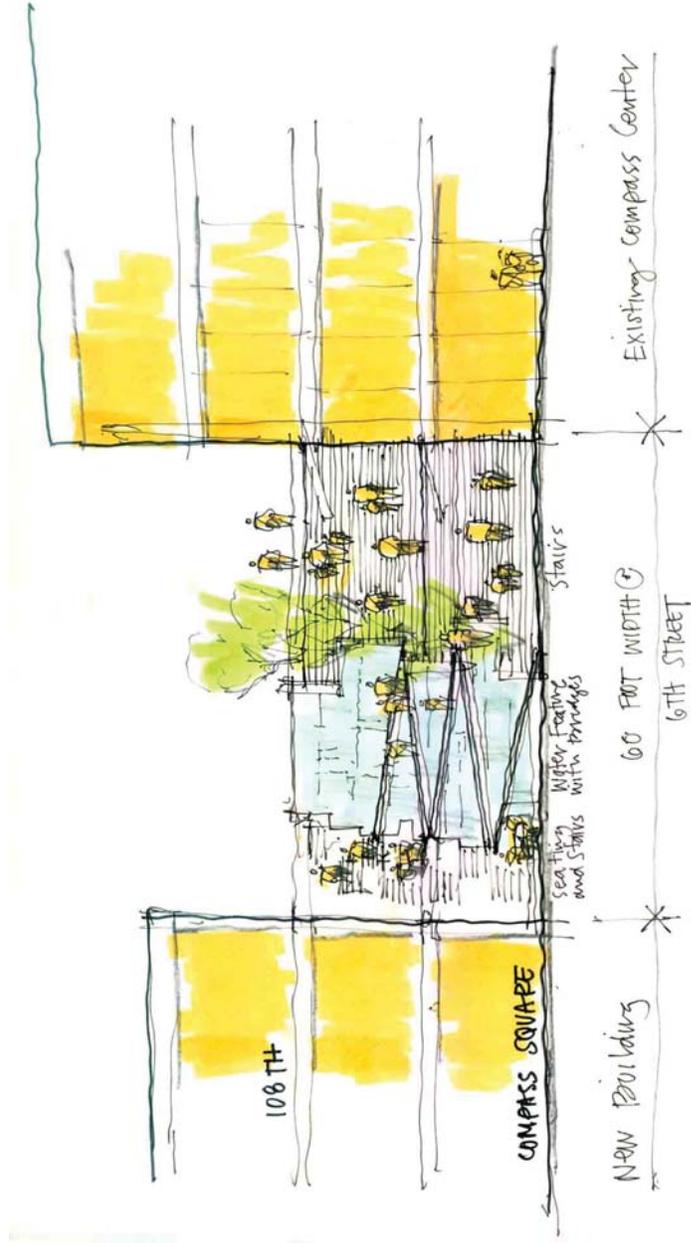
An expanded Compass Plaza would be a regional attraction—ringed with shops, restaurants, and cafés. It would continue to be home to concerts and other major public events.

# 6th Street Section



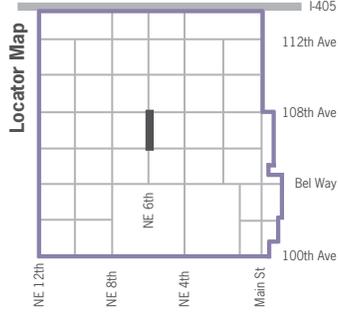
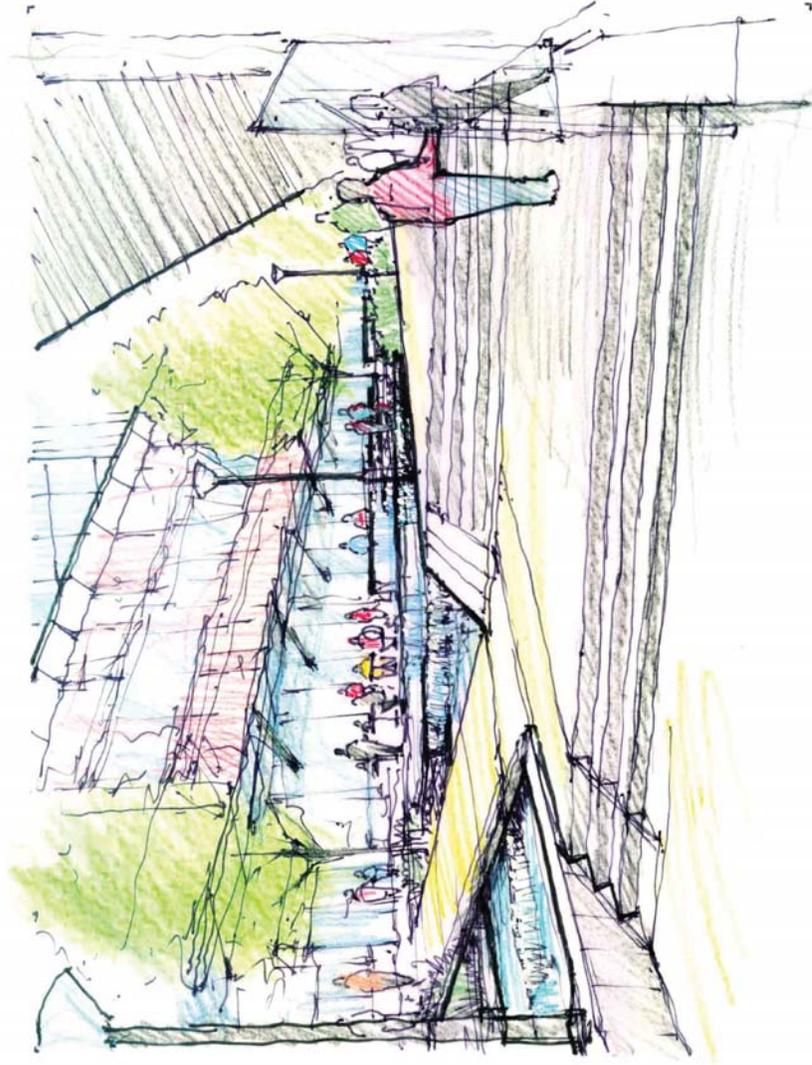
This section of NE 6th Street shows how the character of the Pedestrian Corridor changes from Bellevue Square to the transit center.

# 6th Street Hillclimb (Looking East From Compass Plaza)



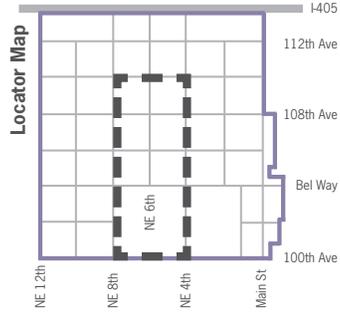
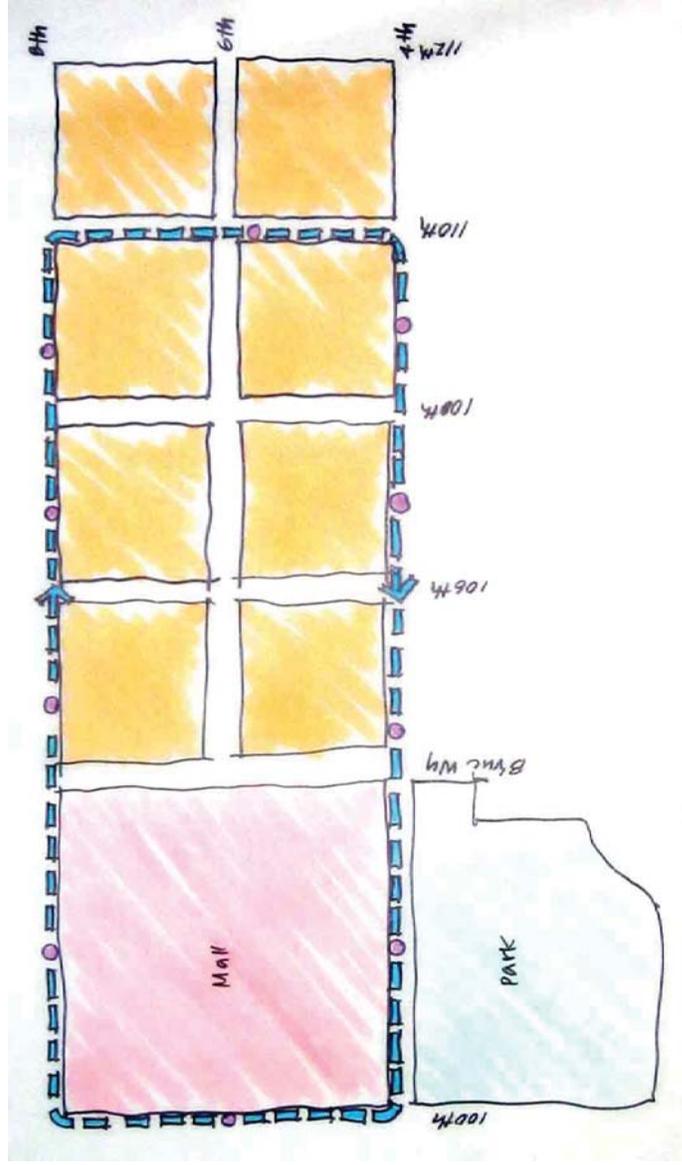
The total grade change between 106th Ave and 108th Ave along NE 6th is surprising. The hillclimb would include a water feature with bridges to create a very dramatic view from Compass Plaza.

## 6th Street Hillclimb (Looking East)



The 6th Street Hillclimb would include a water feature that cascades down from 108th Ave to Compass Plaza. Diagonal bridges would provide an alternate route to the stairs.

# Downtown Circulator Concept



There is an opportunity for a downtown circulator to help define the Center District while providing connectivity for surrounding neighborhoods. A modern street car could operate in a clockwise direction with traffic and have frequent stops each block.

# Team Five: Corps of Discovery

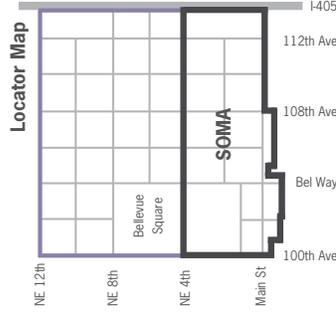
## PARTICIPANTS

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 Jim Hinkle, ZGF  
 Cristian Perez, ZGF  
 Jane Preuss, GeoEngineers  
 Steve Schlenker, CollinsWoerman  
 Kris Snider, Hewitt Architects  
 Marilee Stander, EDAW  
 Ken Thiem, Bellevue Staff Resource



## ASSIGNMENT

Team Five (along with Team Six) was given the assignment of SOMA, or the “south of mall area.” SOMA is the southern third of Downtown Bellevue, delineated by NE 4th Street to the north, the Main Street area to the south, 100th Ave NE to the west, and I-405 to the east.



## APPROACH

### Downtown Neighborhoods

By breaking down the mega-scale blocks, distinct and unique neighborhoods have been articulated. Bellevue is a place with the multiplicity of environments:

#### Walk it

#### We live it

#### WE LOVE IT

We used small scale and disintegration of the superblock to create themed neighborhoods. What creates the personalities:

- Residential backbone
- Activities, streets, spaces, views
- Diversity of scale texture, color pallets, water + activities



## Hierarchy of Linkages

The city will need to implement a finer grain to its urban design guidelines. It must create a hierarchy of linkages stitched together with mid-block connectors and a variety of spaces.

### Themed Signature Streets:

- Bellevue Way (shopping core)
- 106th Ave NE (cultural and entertainment)
- 108th Ave NE (business and finance)
- Main Street Boulevard (the southern gateway)



### Secondary Finger Streets:

- Parallel parking
- Parking one side
- Sidewalks
- Designated bike trails



### Alleys and Trails:

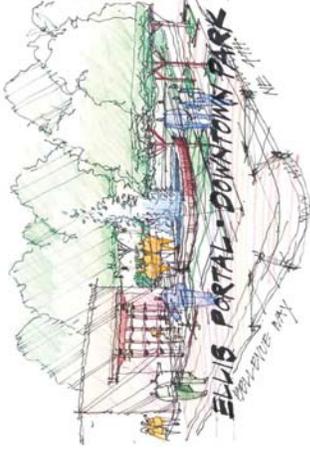
- Discovery trails throughout the community

## Stitched Together by Green Spaces

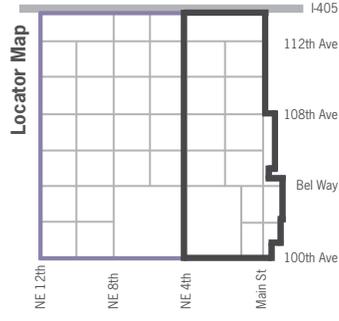
We are articulating six neighborhoods. Like a quilt, our neighborhoods are stitched together by green "staples" that fasten the pieces together.

Residential is the underlying thread and vibrant key to creating livable neighborhoods that will generate vitality throughout the day and night. Residents will be able to relax and take advantage of the outdoors through a hierarchy of spaces:

- Downtown Park
- Intimate pocket spaces
- Larger urban spaces such as community garden, and foundation green
- Lake to Lake Trail



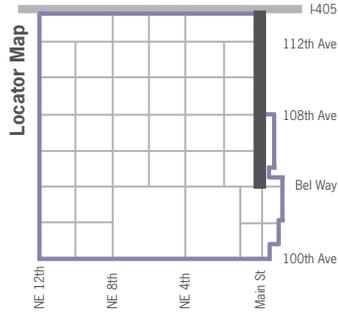
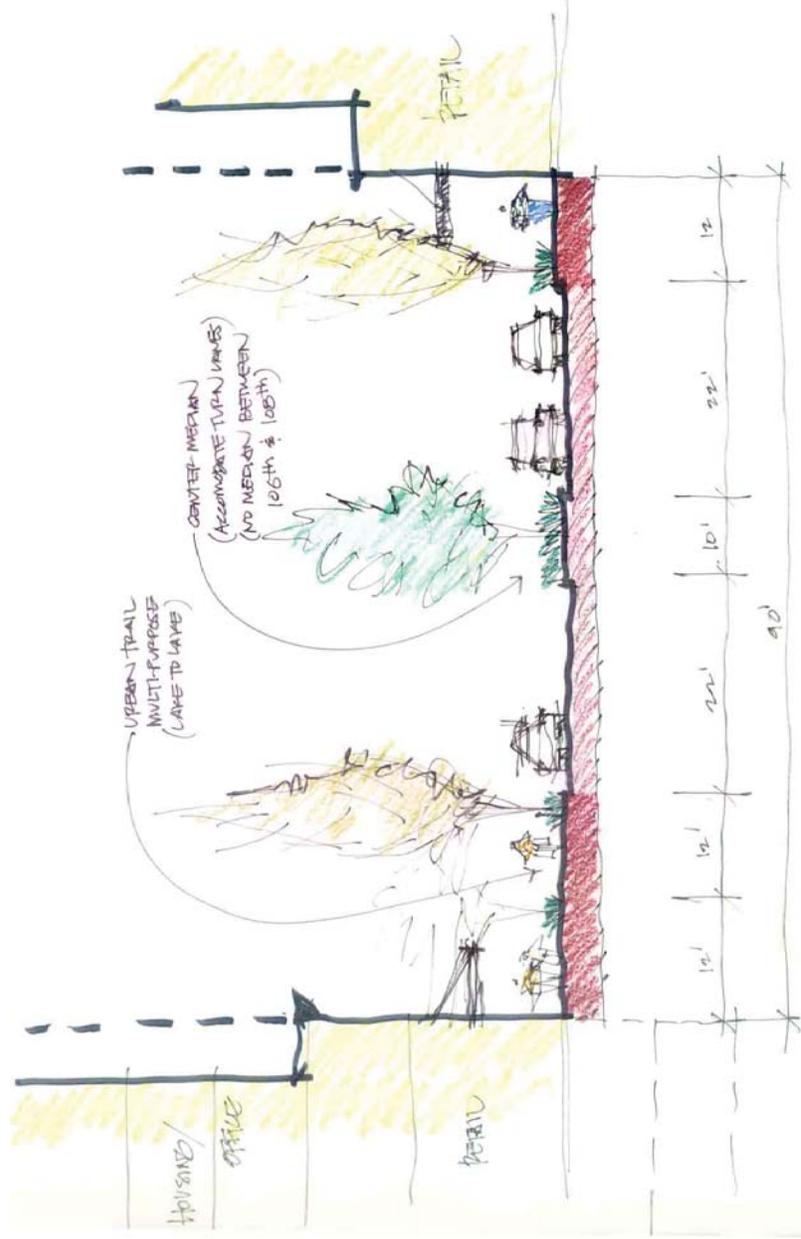
# SOMA: A Series of Neighborhoods



The plan for SOMA envisions an eclectic series of diverse neighborhoods and experiences which fuel a variety of urban lifestyles. The neighborhoods include Old Bellevue, Brewtown, the ARTery/Art District, Spanish Steps, Town Square, Ridgeline, and Hospitality Row.

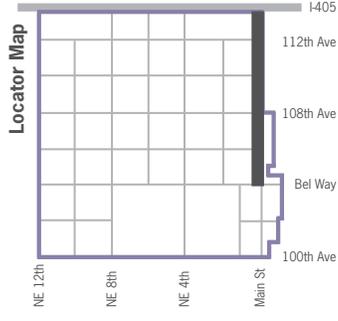
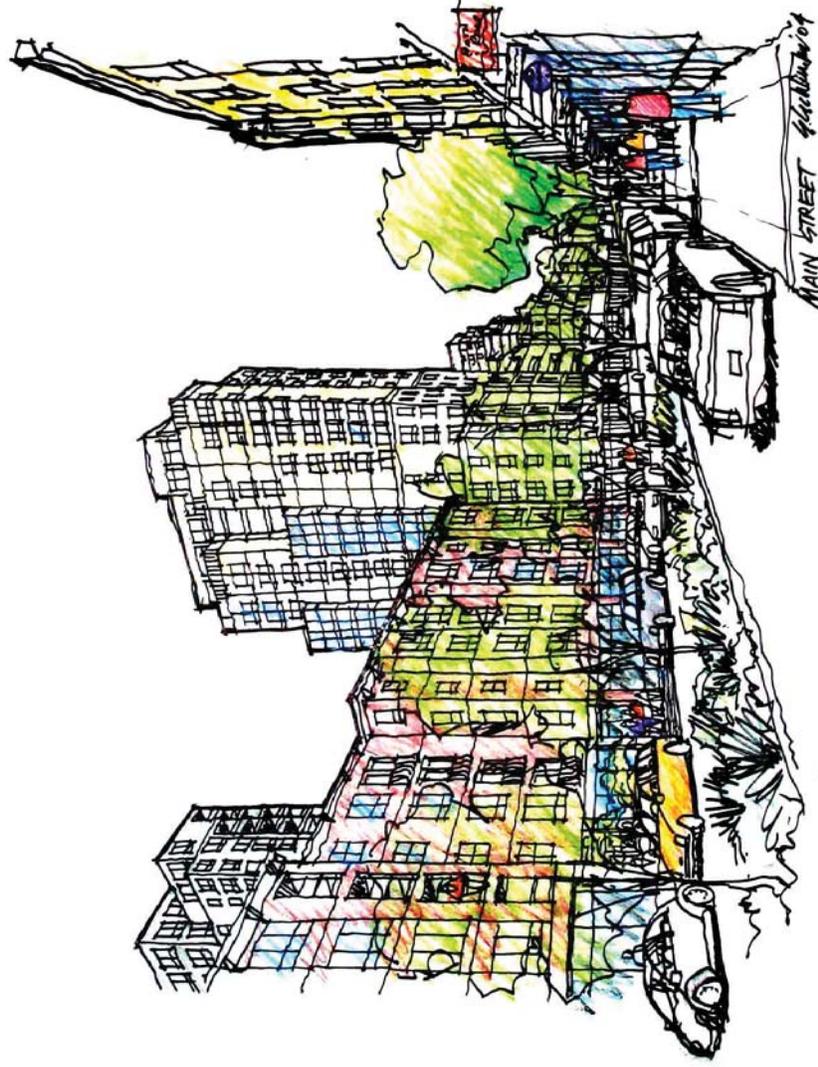


# Main Street



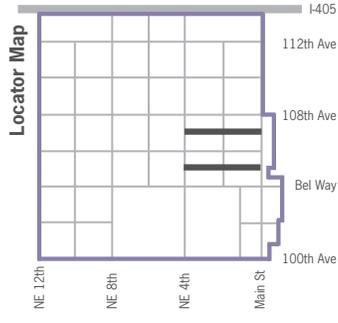
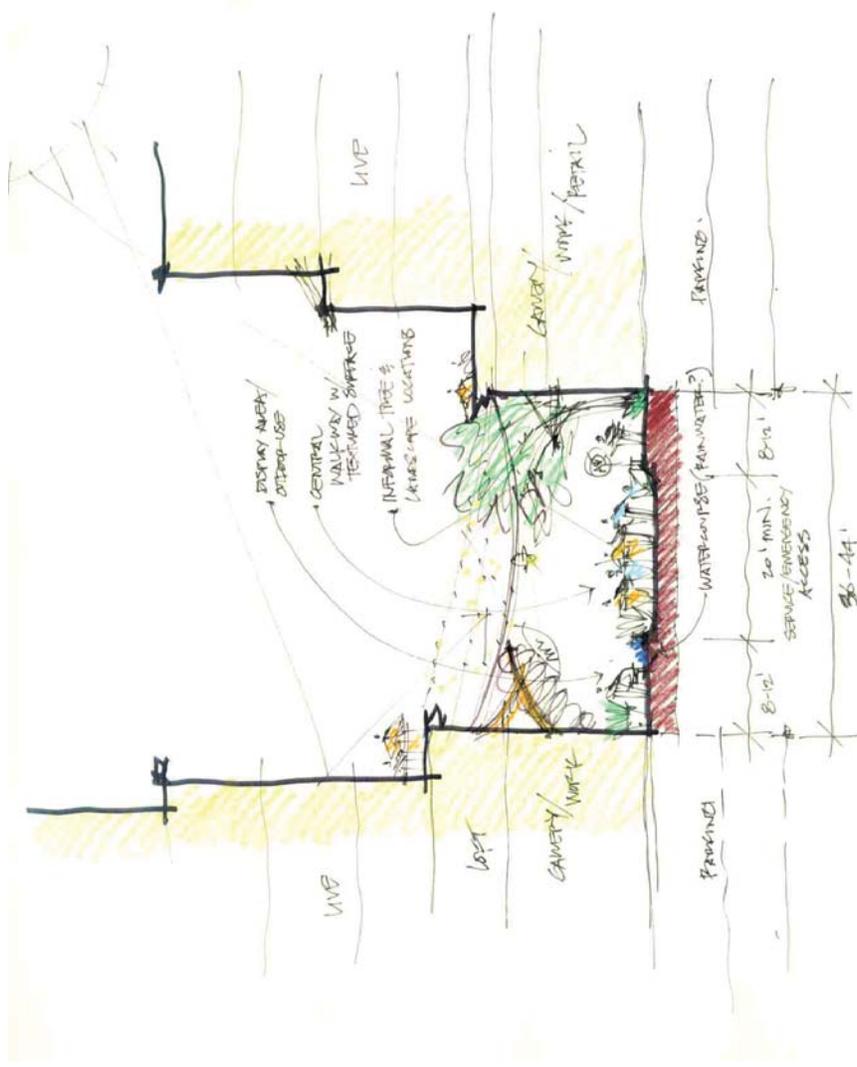
There are opportunities on Main Street, between Bellevue Way and 112th Ave, for a greener, more activated character. Main Street could incorporate the Lake to Lake Trail, landscaped medians, and more sidewalk-oriented uses.

# Main Street



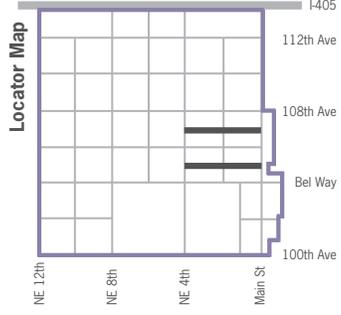
Main Street will evolve into a vibrant corridor. A diverse housing supply will be a key component to spur the uses and activities to make this portion of Downtown Bellevue great.

# Art Mews

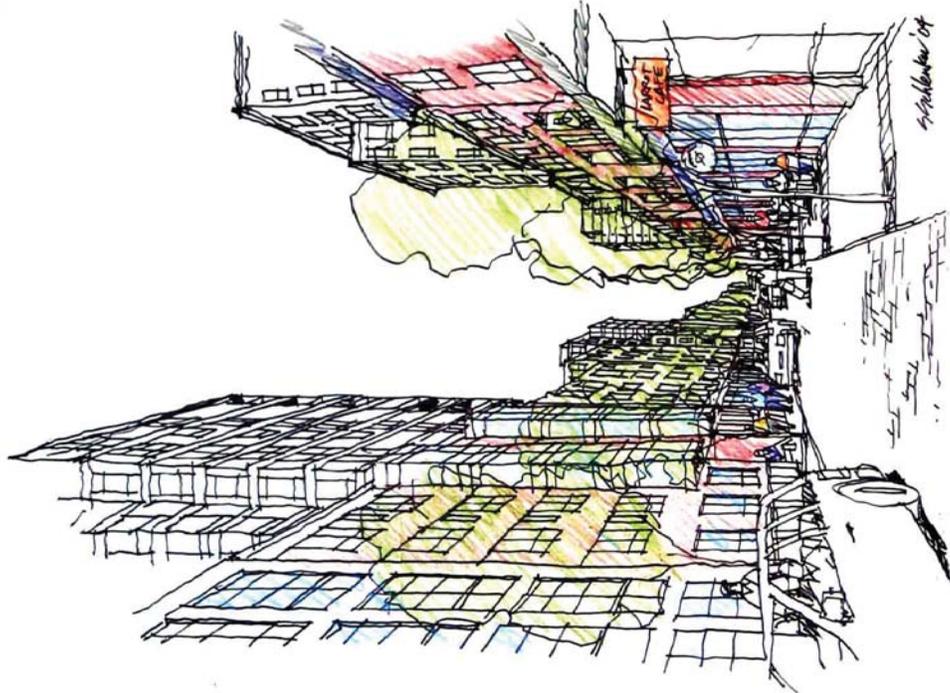


Art Mews could occur on some of the odd-numbered avenues in the SOMA district. The streets would be able to be closed for special events.

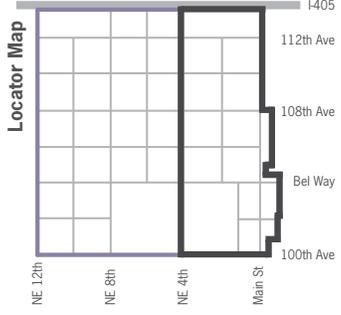
# Art Mews



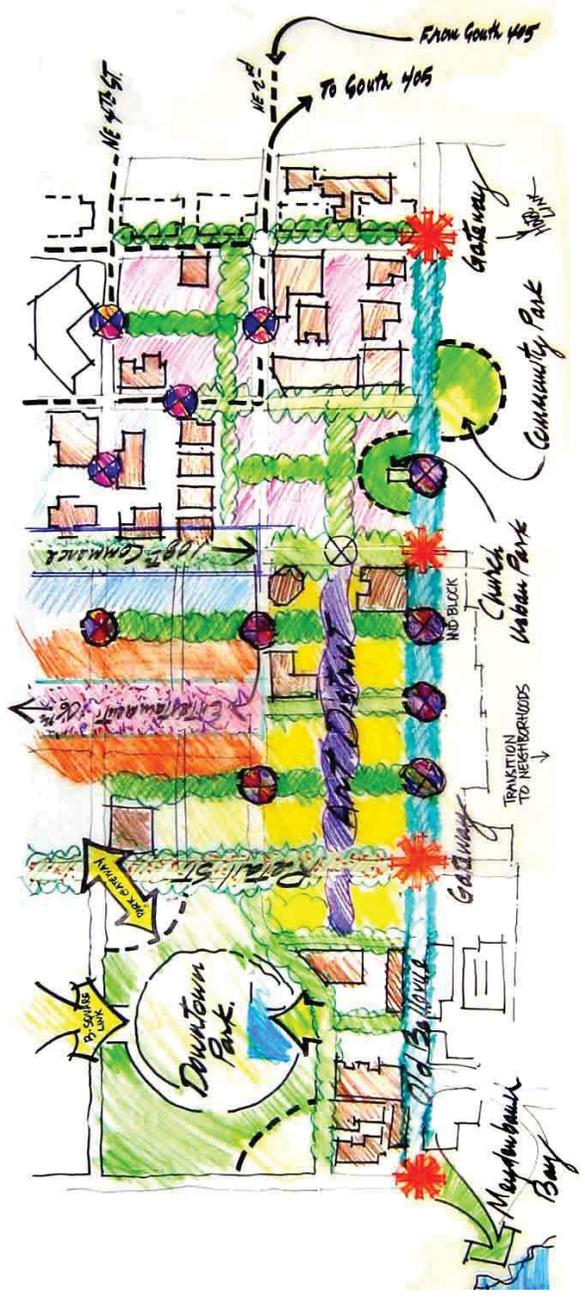
The scale of the Art Mews would include space for two travel lanes (for very low speeds) and sidewalks that are not overly wide. They would have a heightened sense of interest and enclosure, very different from the character of the major arterials.



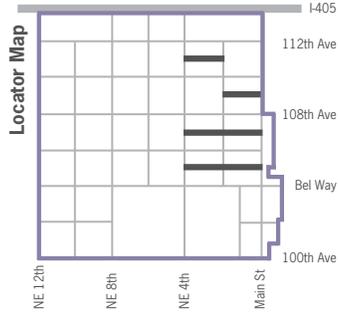
# SOMA Connections



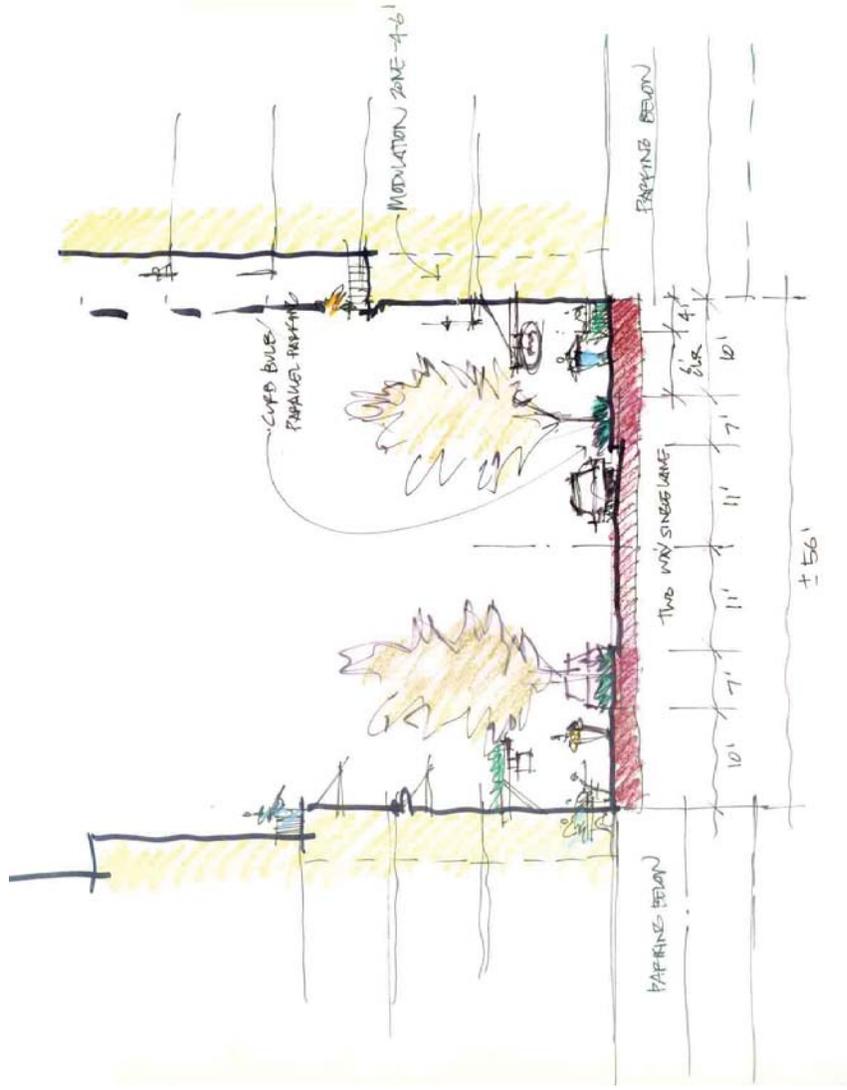
A series of connections through the SOMA district will help tie the disparate blocks of development into a series of true downtown neighborhoods. An example shown in the graphic is a continuous east-west pedestrian connection at the approximate NE 1st alignment from Old Bellevue to 110th Ave.



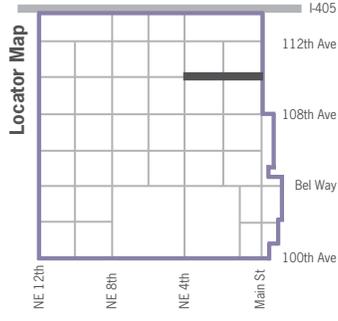
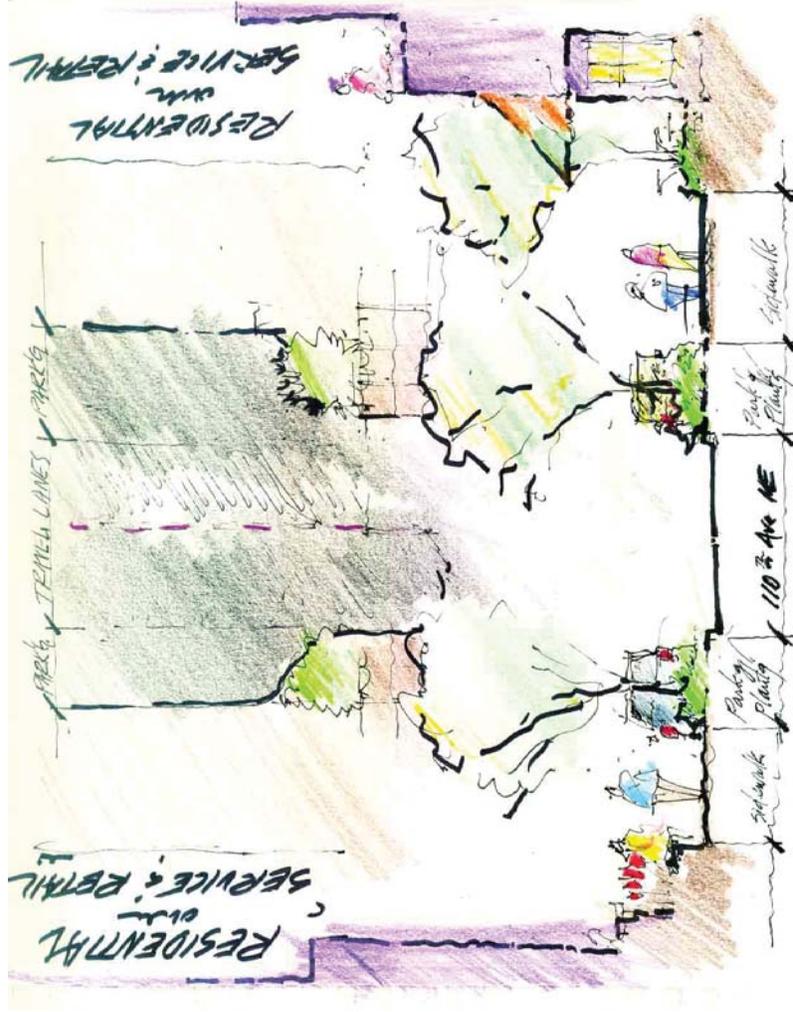
# Green Streets



Green Street treatments could occur on portions of some of the odd-numbered, north-south avenues.

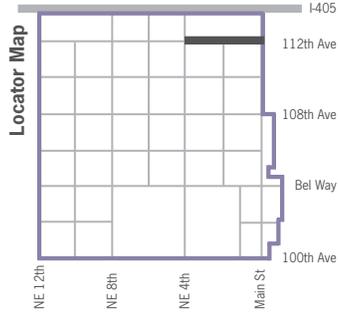


# 110th Ave NE



The portion of 110th Ave NE within SOMA is an example of a quieter, largely urban residential street.

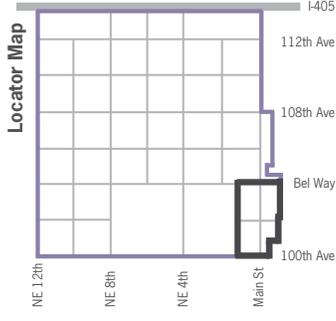
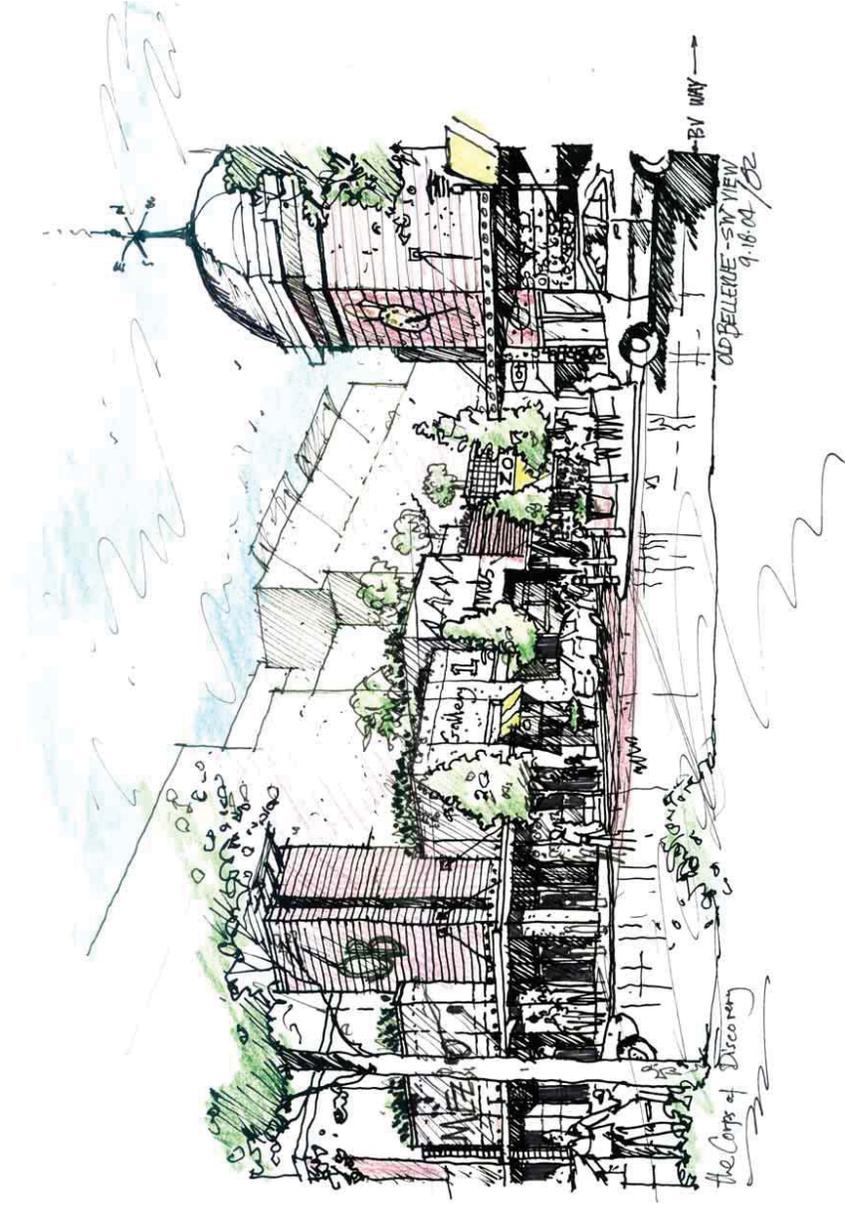
# 112th Ave NE



112th Ave NE within SOMA will continue to be a busy traffic street but could also provide a gracious pedestrian experience.

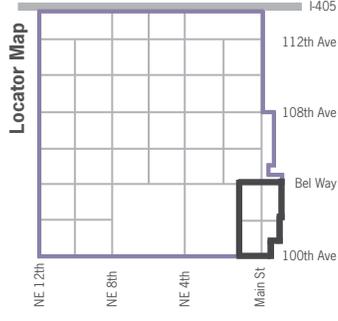


# Old Bellevue



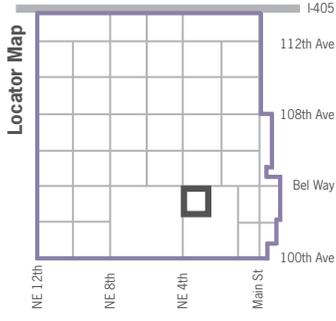
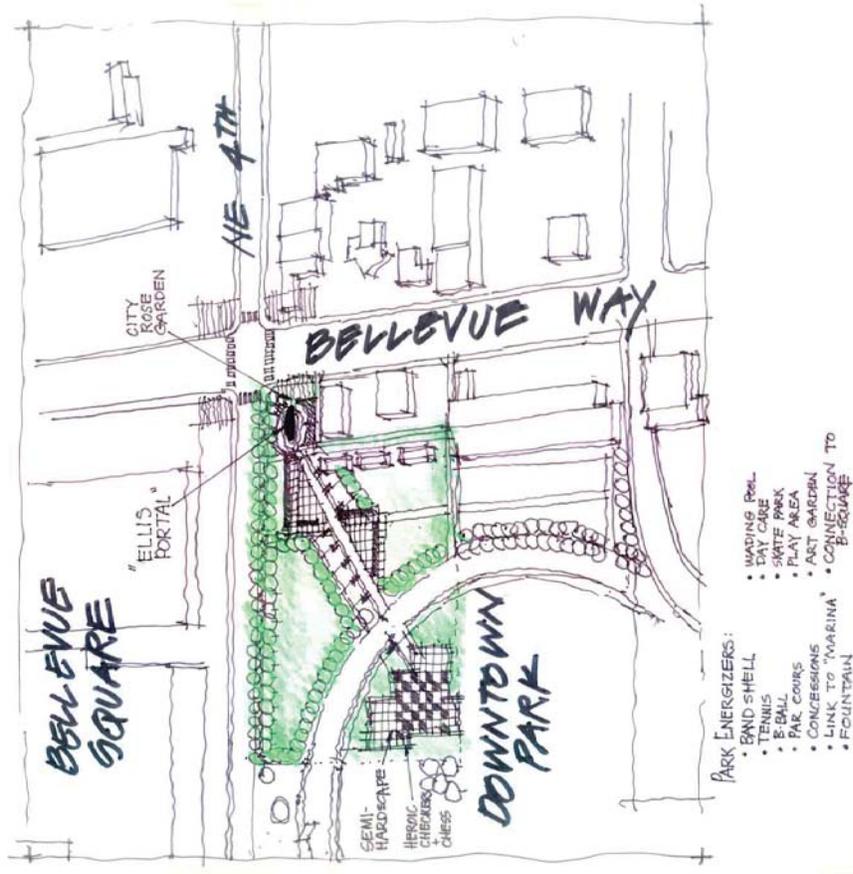
Old Bellevue is by far the most recognizable neighborhood in Downtown Bellevue. It will continue to evolve with interesting uses and a real sense of vitality.

# Old Bellevue



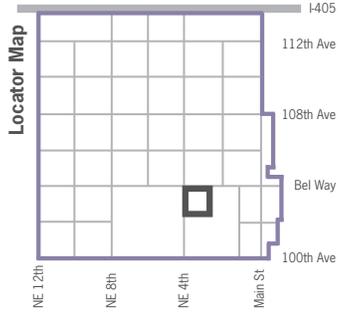
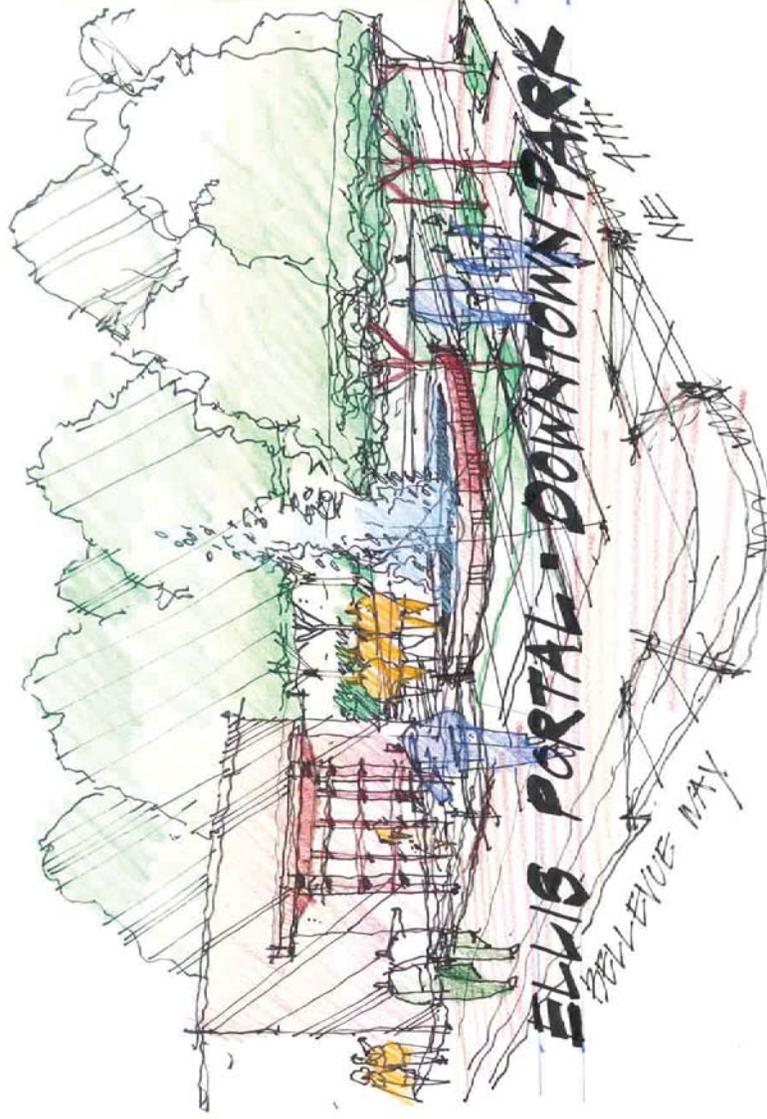
Old Bellevue is the place to take out-of-town visitors because of the pedestrian-scaled character and mix of uses.

# Gateway to Downtown Park



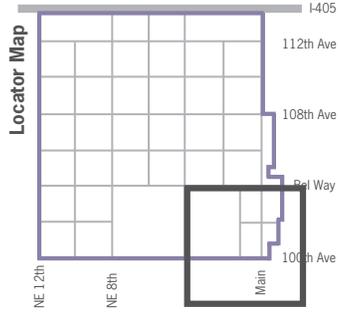
There is an opportunity to open up Downtown Park to Bellevue Way. The park would also benefit from more "energizers" such as active recreation opportunities, a band shell, wading pool, skate park, and park-oriented concessions.

## Downtown Park Presence on Bellevue Way



The connection from the downtown core to Downtown Park would occur at the Ellis Portal at Bellevue Way and NE 4th Street.

## Connection to Meydenbauer Bay



An inviting waterfront at Meydenbauer Bay will be a tremendous asset for Downtown Bellevue.

# Team Six: Las Ramblas

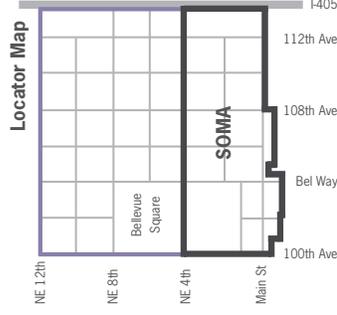
## PARTICIPANTS

- Bill LaPatra, Mithun (Team Leader)
- Michael Cannon, EDAW
- Jason Dardis, MulvannyG2
- Debra Guenther, Mithun
- Glenn Kost, Bellevue Staff Resource
- Guy Michaelson, The Berger Partnership
- Christine Mullen, MulvannyG2
- Bob Murase, Murase Associates
- Joshua Scott, Mosaic



## ASSIGNMENT

Team Six (along with Team Five) was given the assignment of SOMA, or the “south of mall area.” SOMA is the southern third of Downtown Bellevue, delineated by NE 4th Street to the north, the Main Street area to the south, 100th Ave NE to the west, and I-405 to the east.



## APPROACH

### Five Principles

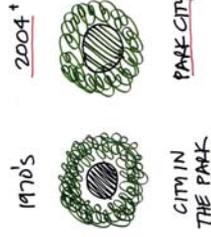
Our team participants included three architects, five landscape architects, and a member of the city’s Department of Parks & Community Services as a resource.

Our approach focused on five primary strategic principles that, if implemented, will dramatically change the existing suburban fabric which currently exists in two thirds of the SOMA district, to a much more urban and modern series of three great place neighborhoods. Our five urban design principles are:

- Green street urbanism
- Charles Street park blocks
- Neighborhood distinction
- Park-once strategy
- Water connections

### Green Street Urbanism

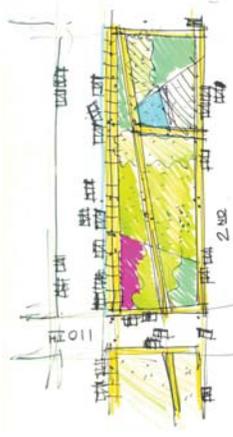
This principle develops a series of street sections that create an overriding ecologically-based set of guidelines



responding to climate, environment, private landowners, pedestrians, and the car. These streets range from high capacity boulevards to a new one-way grid system allowing equal car capacity, pedestrian-friendly sidewalks, and on-street parking.

### Charles Street Park Blocks

This principle is our boldest strategy that converts NE 2nd Street to a north-biased series of linear park blocks connecting 112th Ave NE to Downtown Park. Both the purchase of some land by the city as well as private development dedications are requirements for the linkage of this Park Block strategy. NE 2nd Street will become Charles Street (its original name). It is located directly in the center of SOMA, and will become the strongest identifier for the area linking all three of the neighborhoods.



### Neighborhood Distinction

We have developed three distinct identifiers for SOMA. "Old Bellevue" is already considered one of the most cherished and successful existing neighborhoods. We only intend to amplify its existing qualities and better

connect it to Downtown Park and Meydenbauer Bay.

"Funky Town" (formerly City Center South) capitalizes on its smaller street grid and adjacency to Old Bellevue. We propose a true mixed-use vocabulary of smaller scaled projects with retail edges, on-street parking, a mix of office and residential uses, and a strong use of mid-block



connectors that break down the scale of the neighborhood. "New Bellevue" (formerly East Main) will command a new anchor use. Ideas range from a college campus, an art or music school, or a large public or private institution. This new use will add a needed identity and activity to this opportunistic neighborhood.

### Park-Once Strategy

To promote an overriding goal of a pedestrian-friendly neighborhood, we are proposing a minimum of three city-owned central garages.

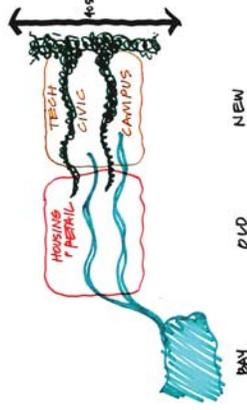


One occurring at 108th Ave NE, between Charles Street (NE 2nd Street) and Main Street. Another would occur at Main Street and Bellevue Way. A third smaller garage would be located at the east edge of Downtown Park. We would include incentives for private landowners to build less private parking and contribute some money to a central garage.

### Water Connections

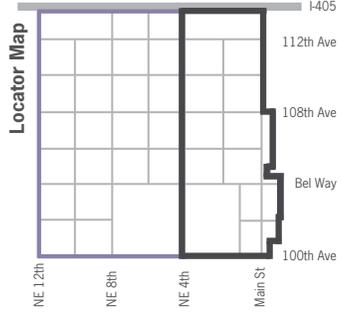
We have proposed both a wonderful aquatic destination park at Meydenbauer Bay, as well as smaller water features that are linked together throughout SOMA.

Developer incentives will help promote mid-block water elements and a new spray park is proposed by the central parking garage in the "New Bellevue" neighborhood. In conjunction with the city's purchase of 800 feet of waterfront at Meydenbauer Bay we have proposed a Whaling Village Museum and plaza as well as an aquatic park. This reconnects the waterfront to downtown through a series of prominent viewpoints and pedestrian connections.



Our five strategic principles can happen incrementally. The key will be the overall implementation of this framework and some catalyst projects that will begin the new urbanism of Downtown Bellevue and a creation of great neighborhoods with great places.

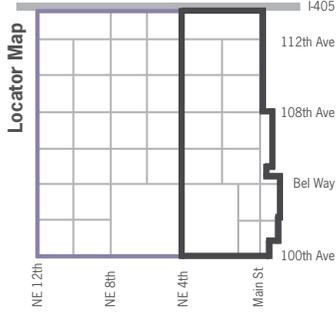
# SOMA Overview



The name Las Ramblas derives from inspiration from the famous avenue in Barcelona, Spain. Barcelona has huge blocks like Downtown Bellevue along with a very rich pedestrian experience.

The vision for SOMA includes five principles: green street urbanism, Charles Street park blocks, neighborhood distinction, park-once strategy, and water connections.

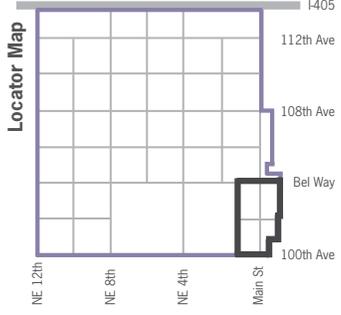
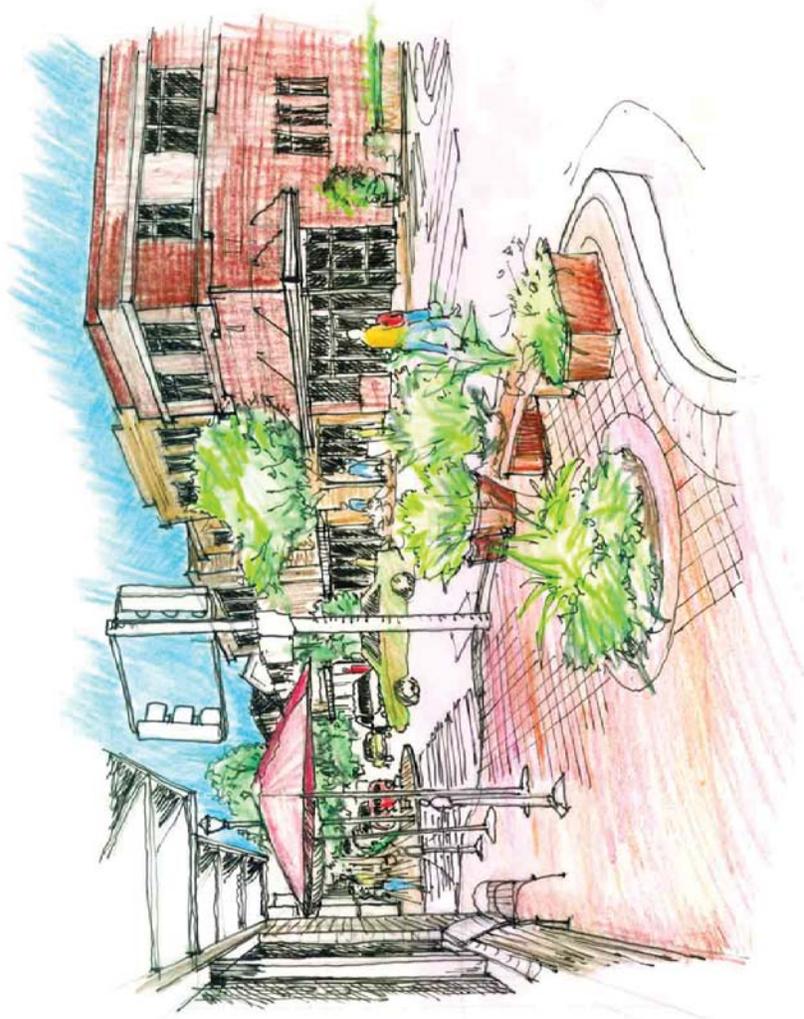
# Neighborhood Identity



SOMA will include three very distinct neighborhoods: Old Bellevue, Funky Town, and New Bellevue.

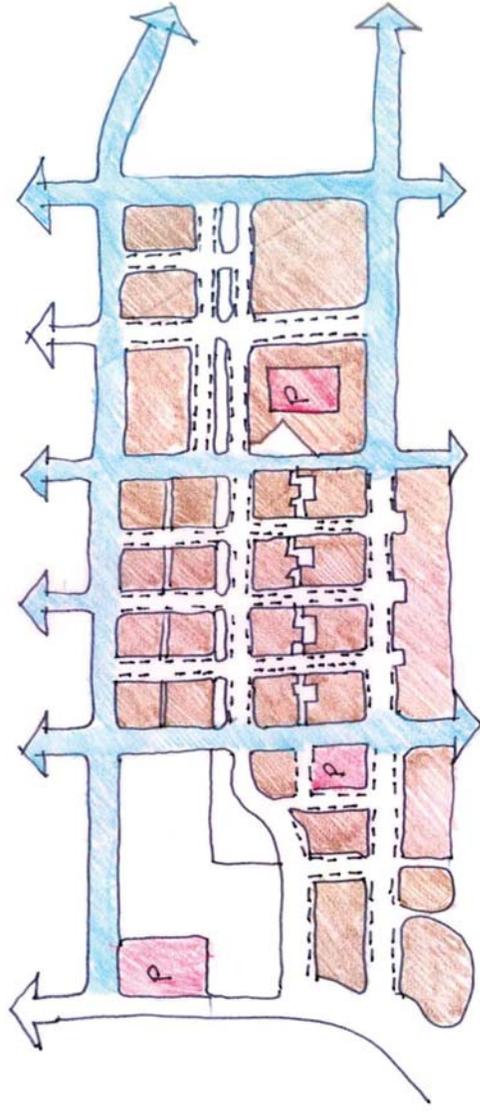


# Old Bellevue

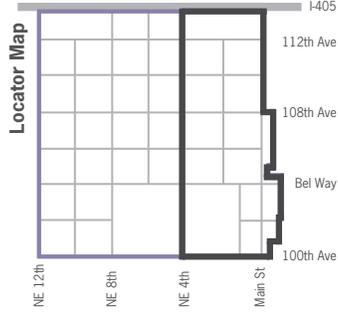


Old Bellevue is already considered a very successful and cherished neighborhood. There will be opportunities to both amplify its existing qualities and better connect it to Downtown Park and Meydenbauer Bay.

# Vehicular Strategy



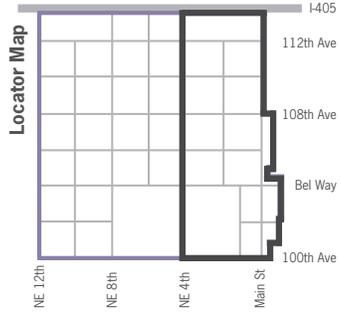
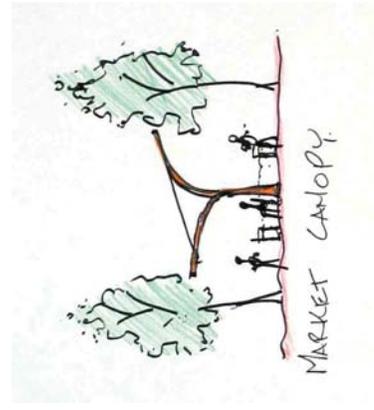
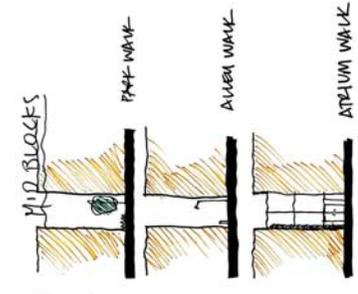
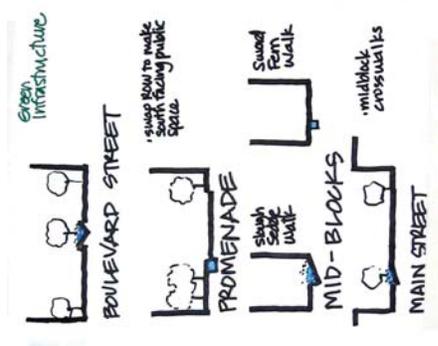
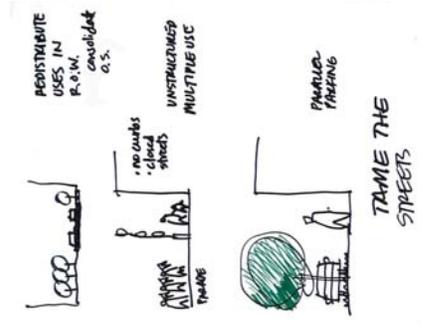
- Auto Bias Street
- Park Once Strategy (city owned)
- On-Street Parking



The vehicular strategy for SOMA promotes an overriding goal of fostering pedestrian friendly neighborhoods. NE 4th, Bellevue Way, 112th Ave, and a portion of Main Street will be auto-biased, but still friendly to the pedestrian.

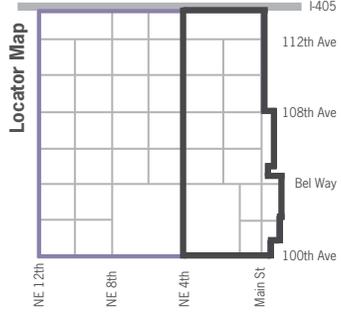
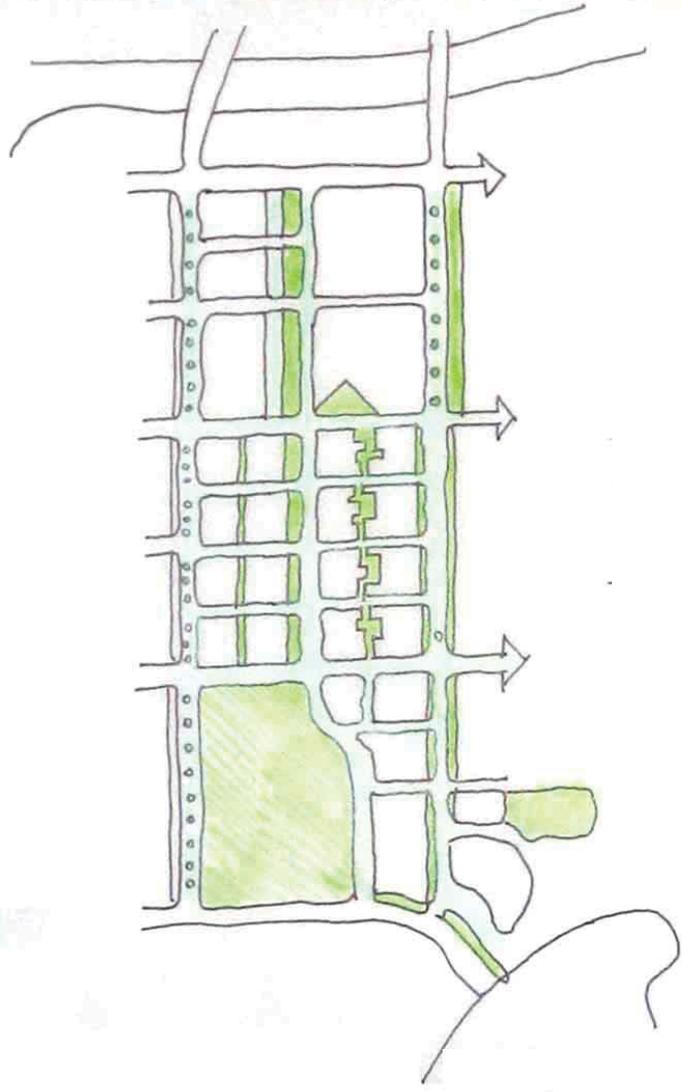
Three strategically-located public parking garages and additional on-street parking will help to further the "park-once" concept.

# Building Streets and Pathways for the Pedestrian



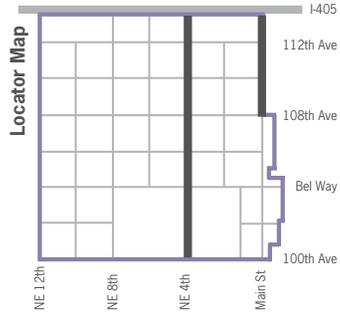
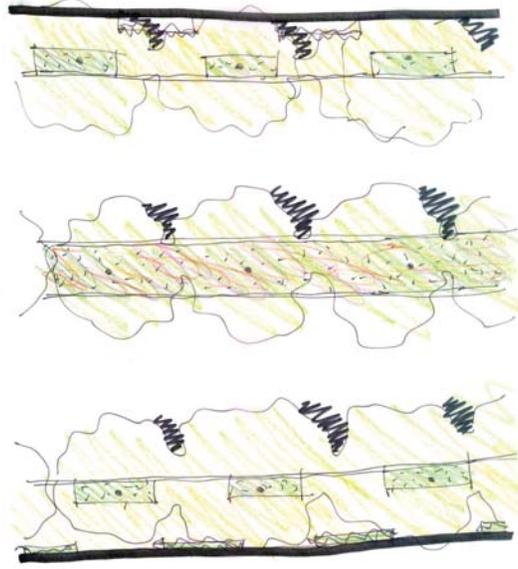
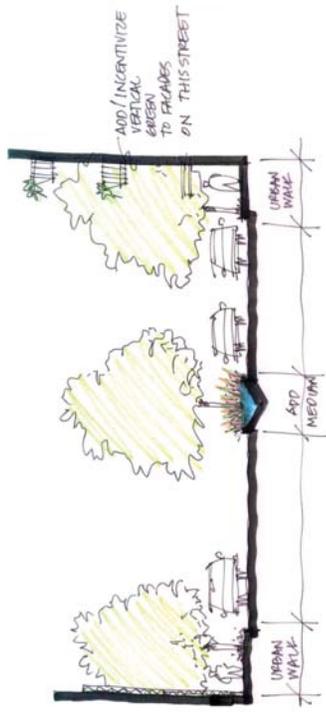
There are opportunities throughout SOMA to create streets and pathways that encourage pedestrian activity.

# Green Street Urbanism



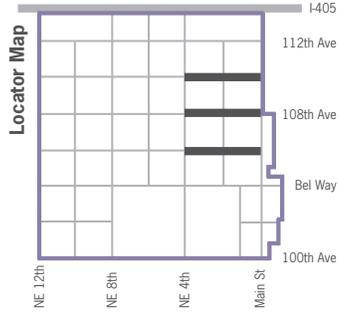
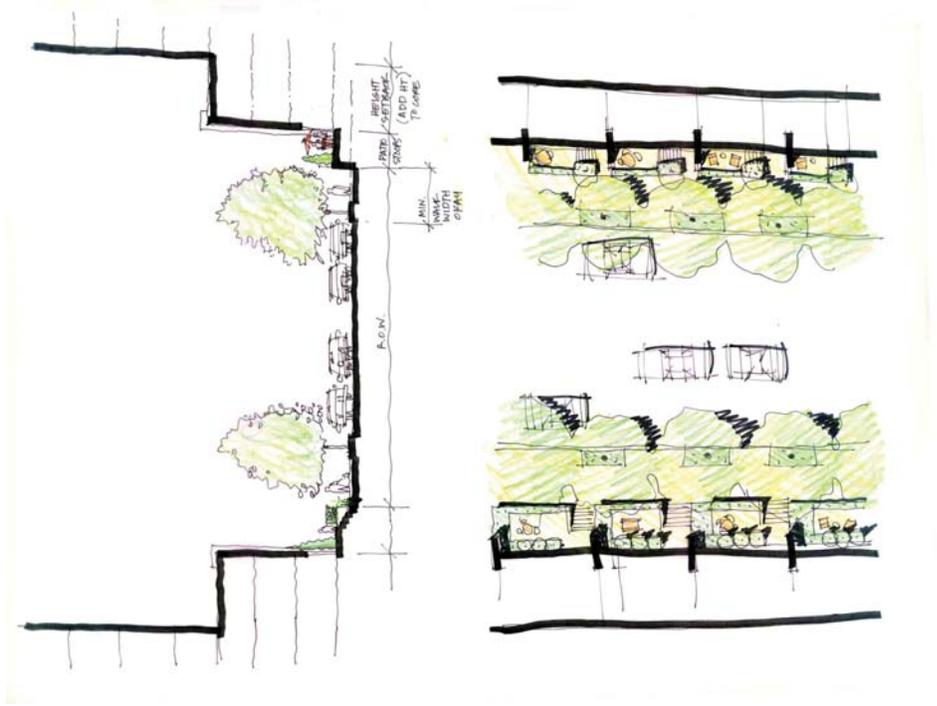
Green street urbanism would use landscaping, sidewalk treatments, pedestrian activities, and on-street parking to bring a very definable unifying character to SOMA.

# Boulevard Treatment



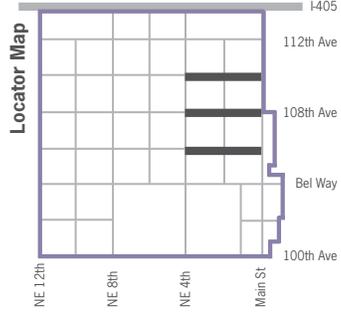
A boulevard treatment applied to NE 4th and Main Street (I-405 to 108th Ave) could enhance these auto-bias streets.

# Active Residential Edge on North-South Streets



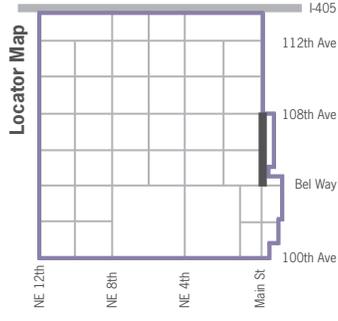
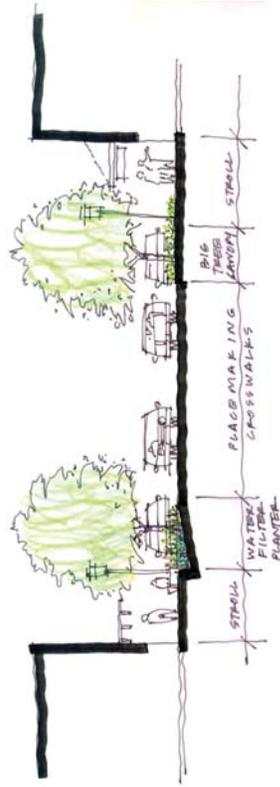
Some of the north-south streets provide the opportunity to enrich the street edge where residential uses abut.

## Active North-South Streets

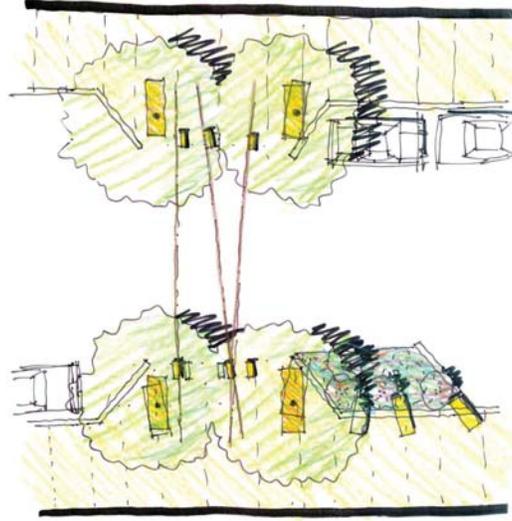


The north-south streets in SOMA could add to the neighborhood character that will develop over the next 20 years. Housing that is set up from the street with integrated landscaping will make this a wonderful route for pedestrians, cyclists, and automobiles.

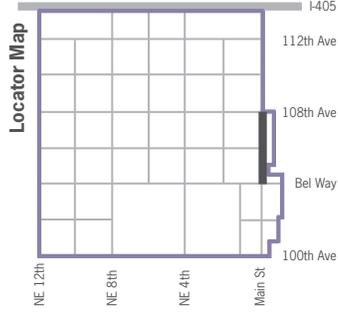
# Main Street



The portion of Main Street between Bellevue Way and 108th Ave could incorporate some interesting place-making features as redevelopment occurs. This example shows a water filtering planter, dramatic tree canopy, and possible treatments for a mid-block crossing.

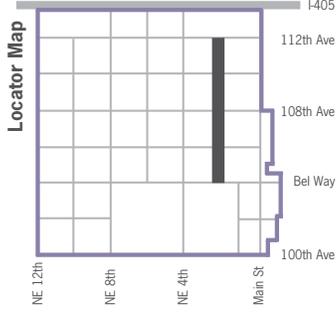
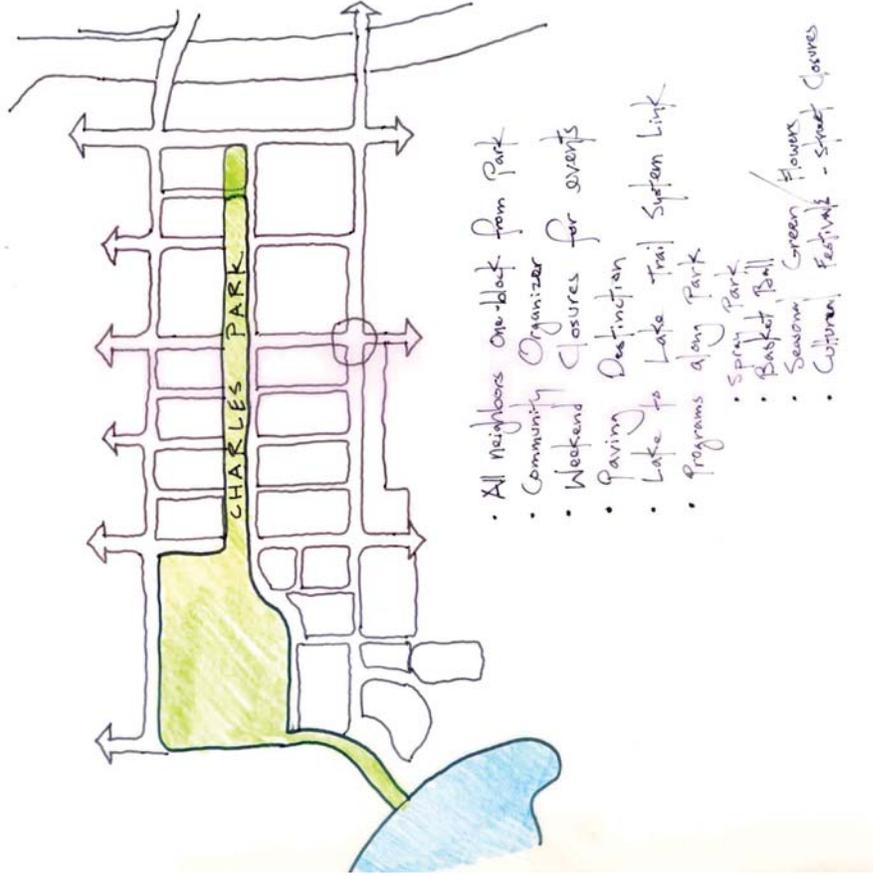


# Main Street



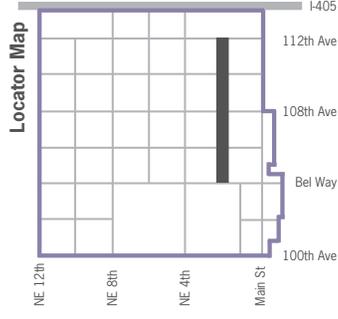
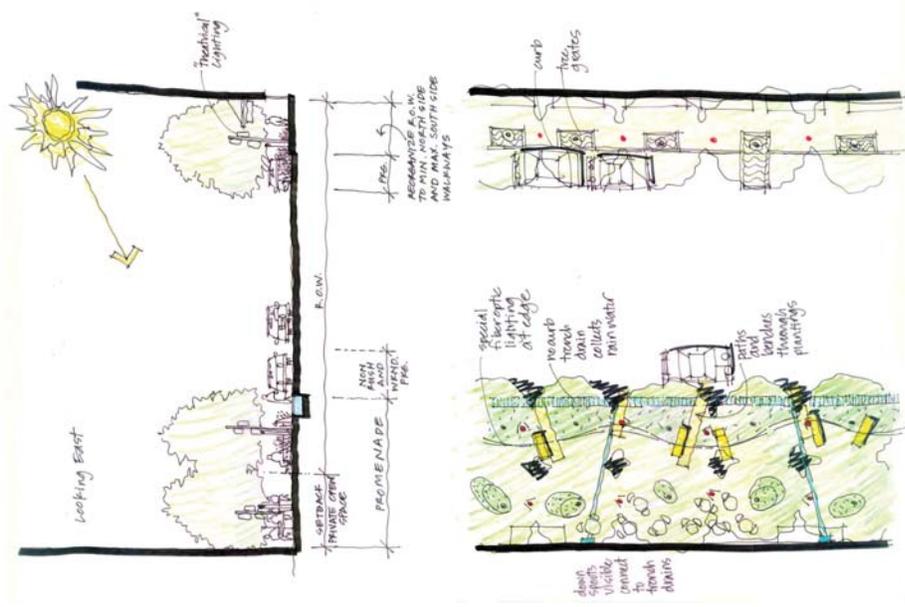
An idea that has been considered before but rejected, is the notion of extending the character of Old Bellevue east of Bellevue Way along Main Street. This idea may warrant further consideration based on the possibilities to create a more unified Main Street corridor and capitalize on what is working in the SOMA district.

# Charles Street Park (on NE 2nd Street)



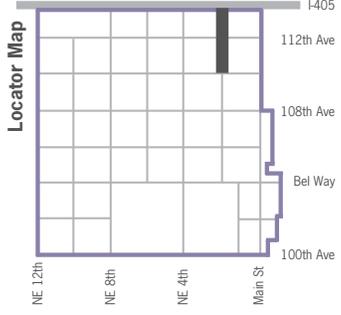
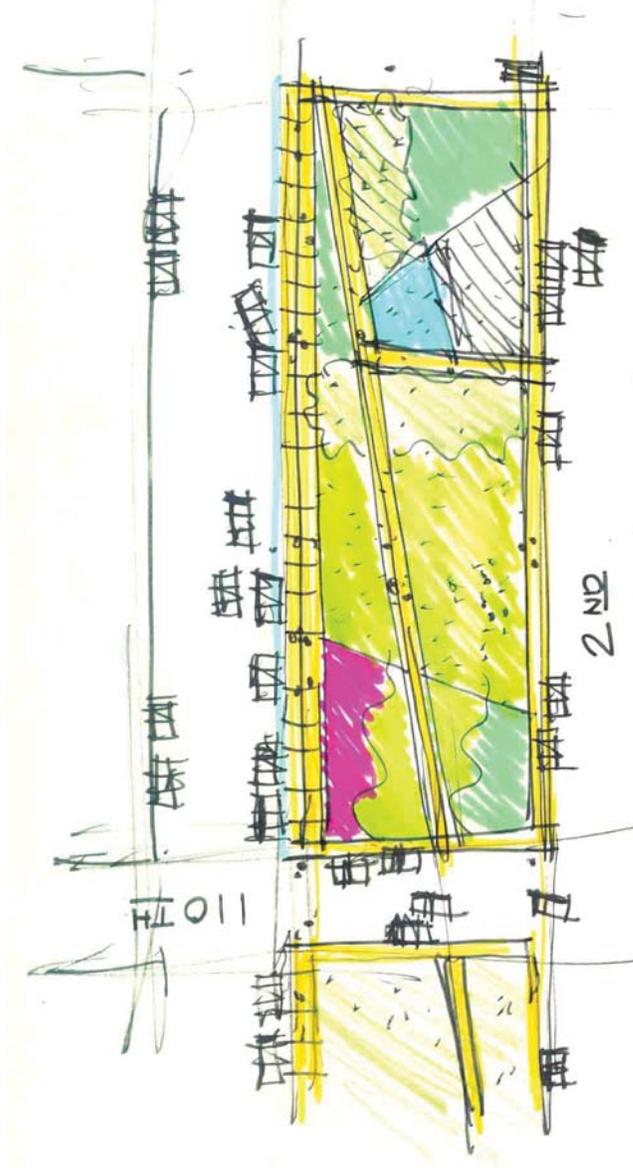
The linear Charles Street Park (along NE 2nd) would refer back to the historic name for the street while linking SOMA from east to west.

# Charles Street Park



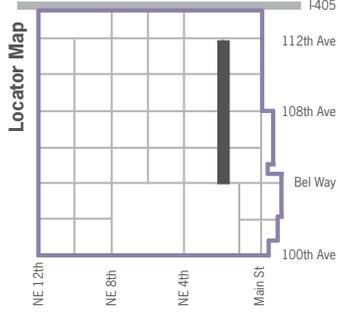
The Charles Street Park would have a very organic design and occur primarily north of NE 2nd to take advantage of exposure to sunlight.

## Charles Street Linear Park (Near 110th Ave)



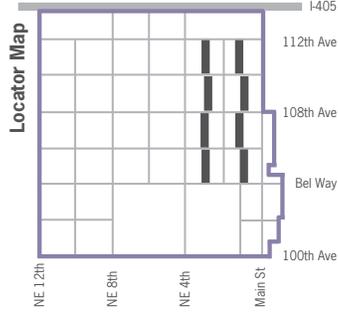
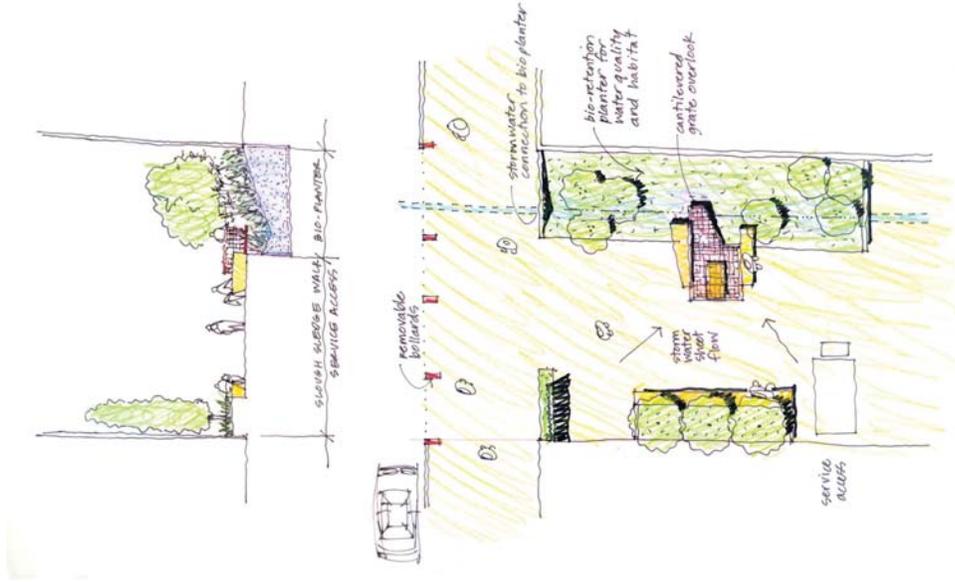
The Charles Street Park would extend to 100 feet in depth between NE 2nd Street and NE 2nd Place. This would provide a neighborhood park function for the New Bellevue neighborhood and tie in with the larger linear park.

# Charles Street Linear Park



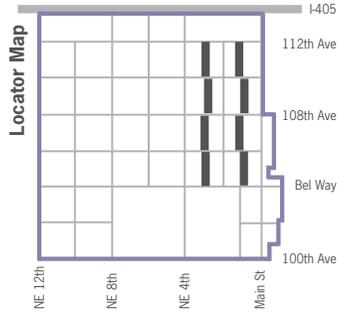
The linear park would require both city acquisitions as well as dedications made as part of private development proposals. The result would be a defining downtown design feature.

# Mid-Block Park



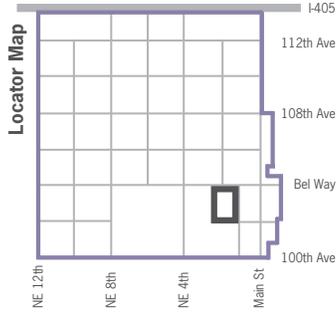
Mid-block parks could systemically occur in the NE 1st and NE 3rd Street alignments within SOMA. These parks could include innovative design concepts such as small bio-planters or rain collectors.

# Mid-Block Park



The mid-block park blocks would provide a meandering pedestrian-only path, linked together by mid-block crossings of arterials.

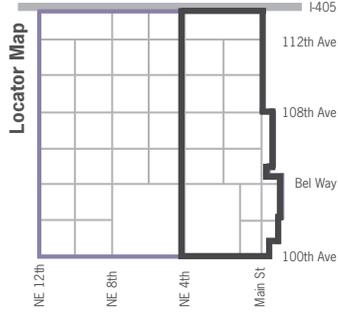
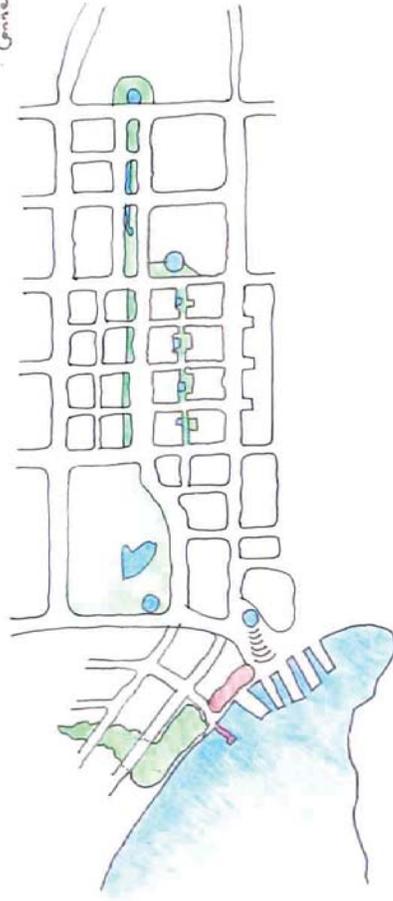
# Completion of Downtown Park



There may be the opportunity to complete Downtown Park in a manner that facilitates programmed uses such as a Farmers Market.

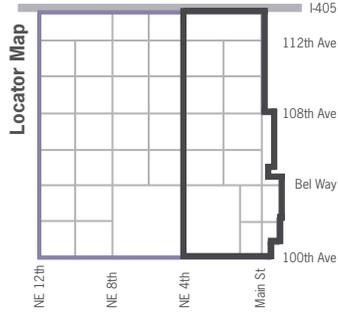
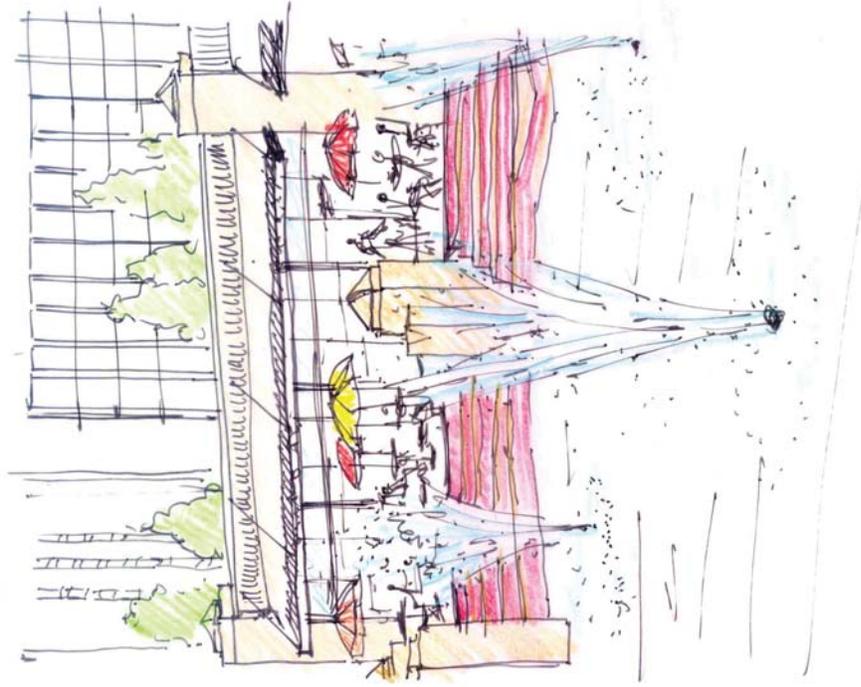
# Water Connections

- Destination Urban Waterfront
- Connection to Parks
- Meydenbauer Bay Park
  - Soft-scape/Green
  - Aquatic
  - Connection to Common
- Whaling Village
  - Handicaps
  - Vendors
  - Events



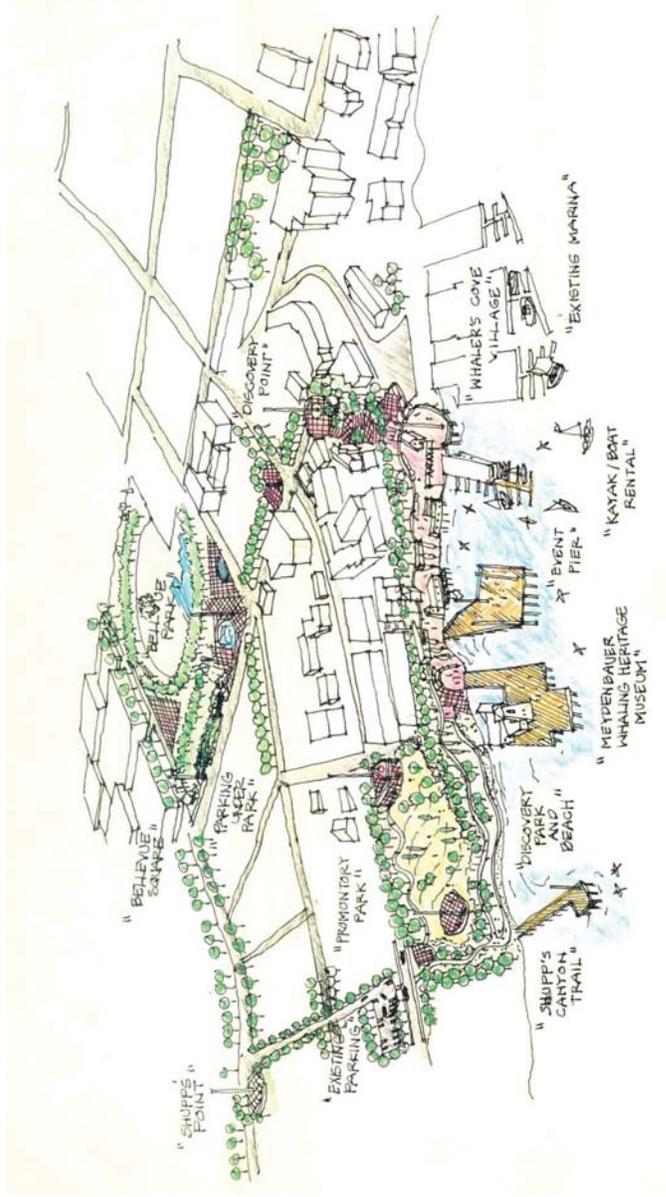
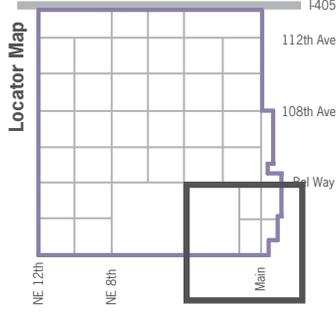
The major water feature at Downtown Park and proximity to Meydenbauer Bay provide a clear basis for the use of water as a unifying design feature for SOMA.

## Series of Connected Water Features



The use of both literal and symbolic water features could be a recurring design element throughout SOMA. This image shows a "spray park" located in the New Bellevue neighborhood.

# Connection to Meydenbauer Bay



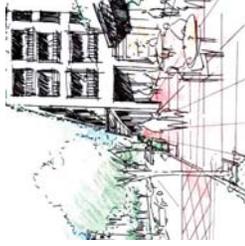
The connection of downtown to Meydenbauer Bay will be the crowning jewel for SOMA. It will be the terminus of the Lake to Lake Trail with a whole host of active recreation uses. It will also provide an opportunity to refer back to the historic whaling-related uses along this part of Lake Washington.



# Closing Thoughts

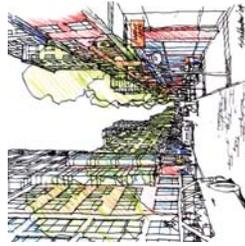
## Inspiring Ideas

The design charrette sparked many inspiring ideas for making our great downtown even greater. The results of the charrette will build on Downtown Bellevue's past successes and strengthen the role of urban design for future downtown development. The charrette ideas will help create livable and memorable places for current and future generations to enjoy.



Some common themes that emerged include:

- Create neighborhoods, streets, and individual features that increase downtown livability.
- Use "branding" to create a powerful identity that people remember.
- Use "green street" treatments to soften the public right-of-way and sidewalk environment.
- Coordinate publicly and privately developed open space to provide a cohesive downtown system.



- Add new signature park elements that function as true neighborhood gathering places.
- Bring streets and through-block connections to life with interesting uses and a pleasant walking environment.
- Include a range of residential uses as a key to continued urban vitality.
- Envision new and exciting ideas for Downtown Bellevue that push the limits of what's possible.



## Selection of the Best Ideas

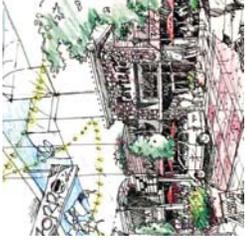
A public process to occur in 2005 will select the best ideas and determine how to put them into action. The best ideas may be carried forward in several ways, including:

- Help the City of Bellevue plan and design public projects.
- Update the Bellevue Land Use Code incentives to encourage more place-making features.
- Provide developers with ideas they might incorporate into the design of their projects.



## Thanks to Participants

The City of Bellevue and Bellevue Downtown Association are very grateful to the design professionals who volunteered for the charrette. It was rewarding to see this level of talent in one place for a common purpose.



There was a tremendous diversity and richness of ideas created over the course of the two days. As many of the participants commented during the event, it was a special opportunity to give back to the community, and work on a unique Eastside challenge. Downtown Bellevue will be a better place for their efforts.

## Comments

Comments or questions may be directed to Emil King, AICP, at the City of Bellevue (eking@ci.bellevue.wa.us).





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# | DOCUMENT STRUCTURE |

## Chapter 1: Introduction



Purpose, background, vision, and principles for design direction and build-out of street corridors for downtown Bellevue.

## Chapter 2: Design Concepts



Planning and design concepts with an overview of the context, opportunities, constraints, framework and design approaches for downtown.

## Chapter 3: Signature Streets



Corridor plans for the initial signature streets, Bellevue Way – the Shopping Street, 106th – the Entertainment Street, and 108th – the Commerce Street. This chapter illustrates how sidewalks should be developed and cross references the toolkit, where description on specific streetscape elements are provided.

## Chapter 4: Crosstown Connections



Corridor plans for the east/west Great Streets, NE 8th Street and NE 4th Street. This chapter illustrates how sidewalks should be developed and cross references the toolkit, where details on specific streetscape elements are provided.

## Chapter 5: Art Walk Elements

- shopping/display circuit
- green/landscaping circuit
- water circuit
- commerce circuit
- culture/entertainment circuit

Downtown's Art Plan includes a set of five themes or circuits linked to contextual conditions; water, landscape, shopping, entertainment, and civic activities as well as the Art Walk which combines all circuits. This chapter provides explanation and ideas on how art can be incorporated throughout downtown.

## Chapter 6: Streetscape Design Toolkit



Detailed explanation of the elements that comprise a Great Street including geometry, materials, landscape standards, lighting, and site furnishings. Descriptions offer standard (basic improvements) and opportunities for options.

# | DOCUMENT NAVIGATION |



## HOW TO USE THIS DOCUMENT

- Review the overarching design concepts such as signature streets, crosstown connections, the art plan and the streetscape design toolkit in Chapter 2.
- Locate the street that interests you in Chapters 3 and/or 4. Familiarize yourself with the Corridor Plan and the different frontage treatments along that corridor.
- The Art Plan in Chapter 5 provides direction for coordinating art along a set of circuits or themes that run through Downtown.
- You will then use the toolkit in Chapter 6 for recommendations regarding geometry, materials, landscape elements, and furnishings.

### USE BY DECISION MAKERS

Development review staff will use this document to communicate to designers and property owners design direction for Downtown Great Streets and as a review manual for proposed development in order to achieve a great pedestrian environment.



### USE BY DESIGNERS

The document sets guidelines for the design and use of Downtown sidewalks and to some extent street improvements.



### USE BY STAKEHOLDERS

This document is a guide for community members, organizations, property owners, or developers making streetscape improvements or seeking to understand what improvements are required within the pedestrian environment.



# | ACKNOWLEDGEMENTS |

## **CITY OF BELLEVUE TEAM**

The departments of Transportation, Parks and Community Services, and Planning and Community Development have collaborated in advancing the Great Streets initiative.

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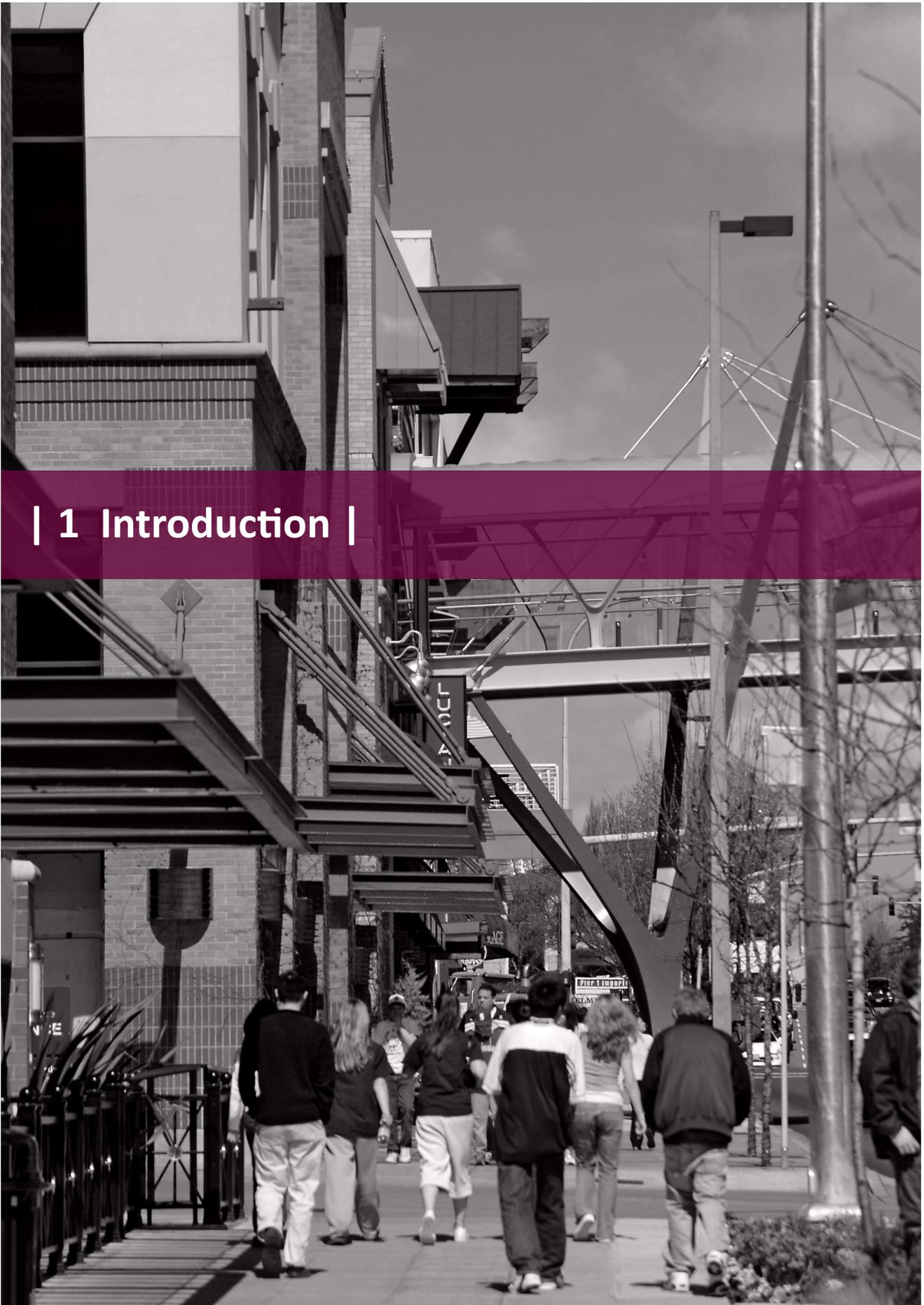
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Tom Drugan  
Laura Haddad

# | 1 Introduction |





# | INTRODUCTION: CHAPTER CONTENTS |

## **Introduction**

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Principles.....	1.6

# INTRODUCTION: PURPOSE



## PURPOSE OF THIS DOCUMENT

This document provides implementation direction for the build-out of street corridors in the Downtown. The designs in this document advance the 'Great Place' urban design vision and concepts identified in the Downtown Subarea Plan. In practical terms, this document:

*Brings together the design guidance developed for various downtown streets through different initiatives;*

*Identifies a set of preferred designs and materials for use on street frontages in downtown;*

*Identifies a framework for incorporation of public art into street corridors in downtown;*

*Facilitates the coordination of frontages built out by different projects so that the incremental improvements undertaken by various separate projects—public and private—contribute to the realization of a larger vision for particular streets and areas of downtown.*

The Downtown Bellevue Streetscapes Design Guidelines project was conceived to create urban design solutions that respond to this need and is one part of a concerted effort by the City of Bellevue to re-imagine its Downtown. This project initially addresses five key streets, focusing on the needs of people (as opposed to cars) and creating a new paradigm for Downtown's street network. It incorporates ideas established in other planning documents such as the Land Use Code, the Design Guidelines for Building/Sidewalk Relationships, A Neighborhood in the Urban Village: Main Street and the Pedestrian Corridor and Major Public Open Space Design Guidelines. The project will also identify a palate of materials and designs that are acceptable along street frontages in downtown.

The initial framework identifies corridor specific solutions for Bellevue Way, 106th and 108th Avenues and NE 4th and 8th Streets. These corridor designs demonstrate optimal sidewalk width, hardscape material and planting elements, site furnishings options and the overall function and aesthetic character. A design toolkit establishes streetscape design options for implementing the vision for the downtown and for achieving the 'Great Places' concept including the 'signature streets' and district identification. An Art Plan provides art vision analysis and plan is a key component in the urban design vision.

## Urban Design Vision for Downtown

*Downtown Districts, in which distinct subareas within downtown are recognized, including Old Bellevue in the SW corner, East Main in the SE, Ashwood in the NE corner, as well as others.*

*Street Hierarchy, in which the function of each major street is recognized as 'auto' bias, 'neutral' or 'pedestrian' bias;*

*Signature Streets, in which an identity is articulated for Bellevue Way as a grand 'shopping' street, 106th Avenue as an 'entertainment' street and 108th Avenue as a 'commerce' street*



# INTRODUCTION: BACKGROUND

## BACKGROUND

Downtown Bellevue is the primary urban center for the Eastside. It functions not only as a locus of employment and commerce, but increasingly also as a place for residential living and for entertainment activities. According to current estimates, by 2030, there will be 2.5 million square feet of new commercial space and 5,000 (19,000 total) new residents. The magnitude and nature of this growth has created several urban design imperatives. The most important of these is the need for Downtown to become a more livable and appealing place, so that it can function effectively in its new role as an urban residential neighborhood, while retaining its traditional attractiveness for commercial and retail uses.

“City in a Park” is the motto and identity of Bellevue and through much of the city this label feels like a good fit; however, with the notable exception of the Downtown Park and a few particular street frontages, this quality is largely absent in the Downtown area. Streets too often tend toward grey and bland, in some cases provide poor accommodation for pedestrians and generally offer little of interest to reward moving about on foot.

The Land Use Code specifies the base sidewalk and landscaping standards for Downtown streets. The code specifies the width of the sidewalk and landscape area, the street tree requirements, and provides general direction for landscape enhancements along certain major corridors. What has been largely absent is a vision for the look and feel of individual streets in downtown and specific direction for how to achieve a desired result.

The design vision for the streets in this Downtown Bellevue Streetscape Plan document is guided by the urban design vision and policies in the City’s Comprehensive Plan that identify the Downtown as a viable, livable and memorable urban center.

Bellevue’s downtown has a larger street grid form than is typical of traditional urban centers. The 600 foot-long “superblock” pattern offers approximately half the street density of Seattle (300’ blocks) and even less than half the density of Portland (250’ blocks). Downtown Bellevue effectively has fewer streets available to serve needs for vehicular circulation and for pedestrian movement. As such, every downtown street in Bellevue is important for both vehicles and pedestrians. None can be effectively assumed to function primarily as a conduit for traffic and allowed to provide poor accommodation for pedestrians. Conversely, few options are viable for designating and designing streets to serve only low volumes of vehicular traffic. The great majority of downtown streets must provide reasonable accommodation for vehicle movement and for pedestrian circulation. The street hierarchy identified in the Downtown

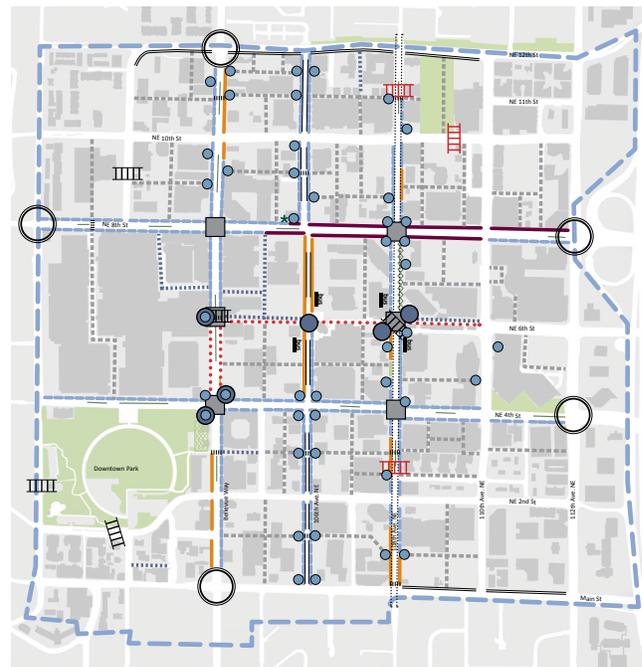
Subarea Plan is used in this document as a guide for the form of frontage improvements and pedestrian accommodation appropriate to each downtown street corridor and block.

In 2007, the City undertook the Great Streets project to develop updated design guidance for downtown streets. This document includes the development streetscape plans initially for five corridors, sidewalk segment plans and a toolkit of elements for use along downtown street frontages. This document also includes an Art Elements Plan that outlines a framework for use of public art along downtown streets and adjacent publicly accessible spaces.

### Streets Included

*This document is a design resource for the street corridors in downtown with an emphasis on the five identified corridors of Bellevue Way, NE 4th, NE 8th, 106th and 108th. This document does not address the design for the NE 6th Street Pedestrian Corridor between Bellevue Way and 110th Avenue NE; design direction for this linkage is provided in a separate document.*

*Design direction for other streets in the Downtown will be incorporated into this document as it is developed.*



# | INTRODUCTION: VISION & GOALS |

## VISION

The Downtown Bellevue Streetscape Design Guidelines provide implementation direction for street corridor development in downtown. It will carry forward the 'Great Place' urban design vision and concepts identified in the Downtown Subarea Plan, including the signature streets and incorporate ideas established in other planning documents such as the Design Guidelines for Building / Sidewalk Relationships, A Neighborhood in the Urban Village, Main Street and the Pedestrian Corridor, and Major Public Open Space Design.

## GOALS

To make the downtown street corridors more interesting and friendly environments for pedestrians.

To make downtown street corridors greener with increased urban tree canopy and planting.

To allow for more differentiation in the treatment and appearance of street frontages along different corridors and in different areas of downtown so that streets gain distinctive character appropriate to their function and location in downtown.



# INTRODUCTION: PRINCIPLES

## PROJECT PRINCIPLES

These principles will guide Great Streets project process and the development of the street corridor designs. The City of Bellevue establishes the following set of planning principles as definition to the Great Streets project. These principles provide the project framework and inform the decision making process for the streetscape design concepts for the downtown. The vision provided in this document implements the ideas set forth in these Planning Principles and enhances the 'Great Places' planning concepts by identifying an experiential pedestrian environment and thus finding new balance for the predominantly auto-biased existing street network.



### Complement the Urban Design Vision

- Support the Great Place Strategy that Downtown Bellevue must be viable, livable, memorable, and accessible to remain the symbolic and functional heart of the Eastside.
- Augment the urban design vision specified in the Downtown Subarea Plan, including the signature street corridors, the street hierarchy and the distinct districts.
- Complement the specific design direction in existing street plans and guidelines such as the Pedestrian Corridor, NE 2nd, Main Street, and Old Bellevue.

### Cultivate the 'City in A Park'

- Reflect the tradition of Bellevue as a "city in a park", through exceptional quality of landscape design.

### Increase Access and Safety

- Accommodate pedestrians with disabilities with accessible facilities that meet or exceed ADA requirements.

### Design for Urban Quality and Beauty

- Enhance the aesthetic quality of downtown street corridors for all users.
- Complement the streetwall formed by adjacent buildings and the building frontage design guidance provided in the City's Building/Sidewalk Relationships Design Guidelines document.

### Create a Vibrant Public Realm

- Substantially improve the quality of the pedestrian environment and experience along streets in downtown.
- Improve and encourage pedestrian circulation with additional mid-block pedestrian connections through superblocks and mid-block crosswalks.
- Complement the series of Major Public Open Spaces envisioned on the Pedestrian Corridor and the Minor Publicly-accessible Open Spaces required in the Old Bellevue and Downtown Core Design Districts.

### Promote Multi-Modal Mobility

- Accommodate parallel parking in areas where it currently exists and maximize opportunities in the future
- Have no adverse impact on overall downtown vehicular circulation.
- Accommodate existing and future transit service needs, including bus routes, downtown circulator route, light rail, and passenger access to bus stops and stations. Include provision of passenger amenities such as shelter and benches.



| 2 Urban Design Concepts |





## | 2 URBAN DESIGN CONCEPTS: CHAPTER CONTENTS |

### Analysis

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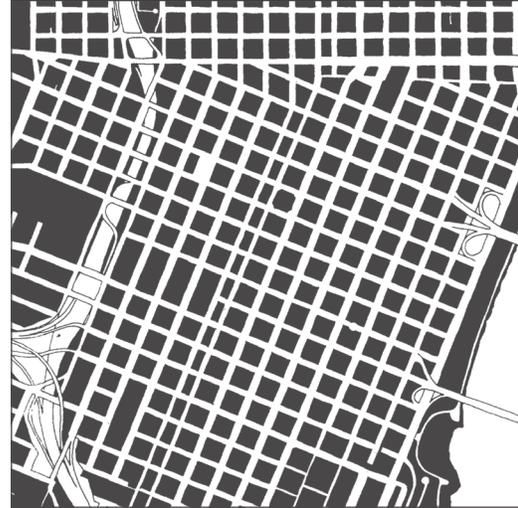
## | ANALYSIS: CONTEXT |

 A comparison of similarly sized cities highlights the importance of streetscape and public space in Bellevue. The difference is most apparent when comparing the figure ground of Bellevue with Portland, Oregon. Portland's 200' square blocks offers 20 times more public space than Bellevue's 400' by 600' superblocks.

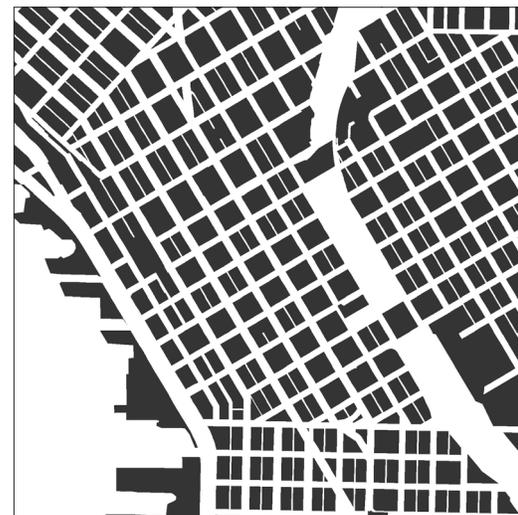
It has been observed—in the DIP and elsewhere—that Bellevue's downtown has a larger street grid form than is typical of traditional urban centers. The 600 foot-long "superblock" pattern offers approximately half the street density of Seattle (300' blocks) and even less than half the density of Portland (250' blocks). Downtown Bellevue effectively has fewer streets available to serve needs for vehicular circulation and for pedestrian movement. As such, every downtown street in Bellevue is important for both vehicles and pedestrians. None can be effectively assumed to function primarily as a conduit for traffic and allowed to provide poor accommodation for pedestrians. Conversely, few options are viable for designating and designing streets to serve only low volumes of vehicular traffic. The great majority of downtown streets must provide reasonable accommodation for vehicle movement and for pedestrian circulation. The street hierarchy identified in the DIP is used in this document as a guide for the form of frontage improvements and pedestrian accommodation appropriate to each downtown street corridor and block.



Percentage of public city land, including right-of-ways



Downtown Portland, Oregon block structure



Downtown Seattle, Washington block structure



Downtown Bellevue, Washington block structure

# ANALYSIS: STREETSCAPE CONSTRAINTS

These constraints were observed through various site visits and walking tours of the downtown as well as feedback solicited at multiple workshops and meetings with the community and development groups.



2 URBAN DESIGN CONCEPTS

CONSTRAINTS OBSERVED	DESCRIPTION
1. Heavy traffic	Sidewalk experience is intimidating and uncomfortable.
2. Crosswalk wait time	Lengthy crosswalk wait time pedestrian frustration invites jay-walking occurrences
3. Long block structure	Crossing in non-crossing zones is intimidating and dangerous for pedestrians.
4. 60'- long crossing distance	Crosswalk length can be difficult for older pedestrians and children.
5. Lack of street buffer	Increases potential for accidents and the lack of human scale creates uncomfortable experience.
6. Parking lot exits cross sidewalks	The risk of pedestrian and automobile conflicts increases where design does not maintain pedestrian priority.
7. No bike lanes	Lack of designated lane space makes bicyclists vulnerable to traffic, and decreases multi-modal travel.

# ANALYSIS: STREETSCAPE CONSTRAINTS



8



9



10



11



12



14



13

CONSTRAINTS OBSERVED	DESCRIPTION
8. Above-ground utility boxes	Above-ground utility boxes impede pedestrian circulation, and add visual clutter to the streetscape.
9. Poorly located vaults	Siting utility vaults in pedestrian travel zones should be avoided.
10. Insufficient sidewalk clearance	Pedestrian bottlenecks are created, which inhibits mobility for all users.
11. Poorly oriented gathering areas	Seating areas fail to activate the street edge and take eyes off the street
12. Poorly located benches	poorly located benches inefficient use of space and resources results in less use of benches
13. Non-intuitive wayfinding	Lack of wayfinding cues leads to confused navigation for pedestrians.
14. Off-street entrances	non-intuitive wayfinding, limited hours of accessibility, and pedestrians taken off of the streets

# ANALYSIS: OPPORTUNITIES

These opportunities were observed through various site visits and walking tours of the downtown as well as feedback solicited at multiple workshops and meetings with the community and development groups.



1



2



3



4



5

CONSTRAINTS OBSERVED	DESCRIPTION
1. Public Transportation	Allows for cars to be left at home, leading to potential reductions in traffic.
2. Mid-block crossing	Discourages illegal pedestrian crossings, bringing the human scale back to the street. Varied patterns and textures also make the streetscape more attractive, allowing for the expression of distinct identities throughout the city.
3. Sidewalk buffer	Creates safety buffer between pedestrians and vehicles, which softens the scale and geometry of the street.
4. Planted sidewalk buffer	Planted buffers offer a more attractive street edge for pedestrians, while also providing ecological benefits. Planted medians also discourage pedestrian crossing through the buffer.
5. Rain Shelters	Shelters pedestrians and encourages outdoor circulation in inclement weather.

# | ANALYSIS: OPPORTUNITIES |



CONSTRAINTS OBSERVED	DESCRIPTION
6. Outdoor retail seating	Allows people to sit and enjoy street life, thus activating the street.
7. Sidewalk gathering areas	Creates spaces for community and civic celebration and enhances wayfinding outside in urban environment
8. 'Green' gathering places	Creates green rooms outside, encouraging 'life' downtown.
9. Street furniture	Artful, well placed furniture enhances the quality of the street and encourages community experience.
10. Public Art	Monumental art installations throughout downtown create places for meeting, and an enhanced sense of place.
11. Landmark trees	Large, historic trees bring grandeur and a sense of environmental context to the street, as well as ecological benefits.

# PLANNING & DESIGN CONCEPTS: PLANNING FRAMEWORK



## DISTRICTS

A key piece to the Great Place Strategy is a series of distinct mixed-use districts that capitalize on their locations and unique identities. Though the exact identities and boundaries of these districts is evolving each district is pedestrian friendly with a Signature Street or Crosstown Connection as part of it.

**Northwest Village** - A neighborhood shopping area. The development of “alleys with addresses” will add the village feel and provide an intimate contrast to Bellevue Way and NE 8th Street which border the district.

**City Center North** - Home to large mixed use development and bisected by 106th - the Entertainment Street.

**Ashwood** - Dense urban housing and the King County Library define this district.

**Eastside Center** - This is the heart of the Downtown with high rise mixed use development and an 18 hour day spirit.

**Old Bellevue** - This area is home to small shops and Downtown’s oldest buildings displaying the roots of Downtown. It’s “Main Street USA” identity is strong and memorable.

**City Center South** - A true mixed-use district that is positioned to serve traditional neighborhoods south of Downtown.

**East Main** - An emerging district influenced by connections to the proposed East Link Light Rail.



## SIGNATURE STREETS & CROSTOWN CONNECTIONS

These Signature Streets and Crosstown Connections form the main network of through-streets in the Downtown and tie it together through complementary uses listed below and design elements as identified in the Downtown Subarea Plan;

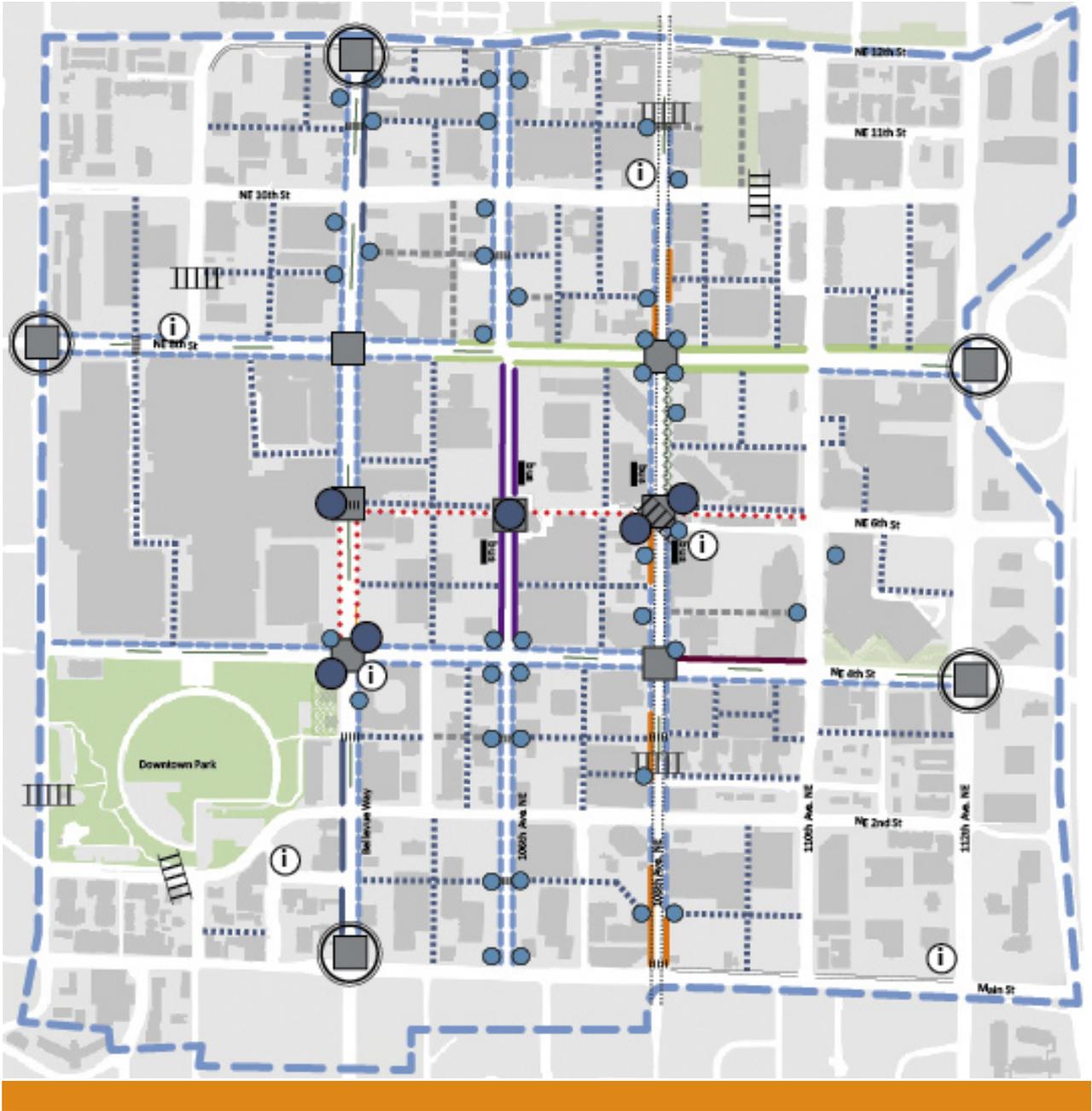
**Bellevue Way NE** - The Shopping Street

**106th Avenue NE** - The Entertainment Street

**108th Avenue NE** - The Commerce Street

**NE 4th Street and NE 8th Street** - City as a Park Streets

# PLANNING & DESIGN CONCEPTS: STREETSCAPE DIAGRAM



## LEGEND

- |  |                             |  |                                   |  |                         |
|--|-----------------------------|--|-----------------------------------|--|-------------------------|
|  | downtown boundary           |  | event frontage                    |  | mid-block crossing      |
|  | req. 20' perimeter buffer   |  | tree preservation frontage        |  | pocket plaza            |
|  | art walk frontage           |  | planted median                    |  | art walk plaza          |
|  | urban neighborhood frontage |  | potential bike route / lane       |  | celebrated intersection |
|  | standard frontage           |  | future expansion of Downtown Park |  | existing bus stop       |
|  | retail frontage             |  | signalized pedestrian crossing    |  | gateway                 |
|  | terrace frontage            |  | through-block connection          |  |                         |
|  | garden walk frontage        |  |                                   |  |                         |

## | PLANNING & DESIGN CONCEPTS: TERM DEFINITIONS |

**Celebrated Intersections** - Similar to the idea of town squares, these intersections allocate more open-space and incorporate specialized materials and paving, while also making a connection to the pedestrian scale through site furnishings and landscape elements. (See Chapter 6 - Toolkit, 6.1.2.)

**Downtown boundary** - Shows the limits of Bellevue's downtown area.

**Frontage** - These segments illustrate the overall widths of proposed sidewalks for downtown Bellevue, and provide general spatial arrangements of landscape elements and site furnishings. Different types of frontages occur on different streets. (See chapters 2 and 3 for more detailed information.)

**Gateways** - Gateways are enhanced celebrated intersections, which mark the entrances of downtown, creating a greater sense of arrival into Downtown Bellevue.

**Landmark trees** - Significant and often large trees within downtown Bellevue that serve as a cultural marker and a link to the historic ecology of Downtown Bellevue.

**Mid-block crossing**- This is a pedestrian crossing that occurs within a block, to facilitate pedestrian crossings. (See Chapter 6 - Toolkit, 6.1.6)

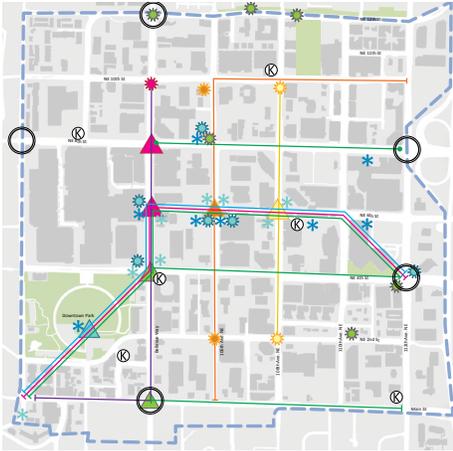
**Planted median**- This is a road median that incorporates landscape elements, such as low growing plants and approved trees. (See Chapter 6 Toolkit- 6.1.9.)

**Pocket Plazas and Art Walk Plazas**- These are small open spaces within the downtown area that provide opportunities for gathering, seating, performance and other plaza-type activities. Art Walk plazas incorporate public art elements into their design. (See Chapter 6- Toolkit, 6.1.3 and 6.1.4.)

**Signalized pedestrian crossing**- These crossings occur at intersections, and include a timed pedestrian crossing signal.

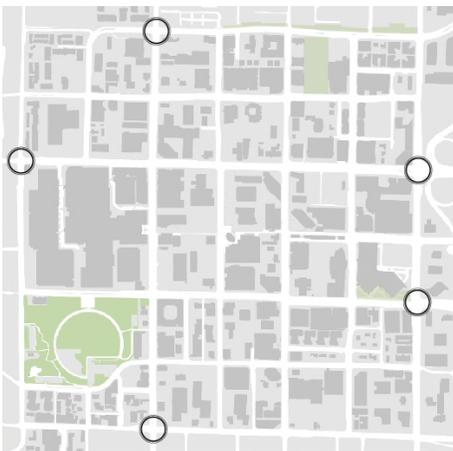
**Through-block connections**- These are alleys or pathways which facilitate circulation through Bellevue's large block structure.

# PLANNING & DESIGN CONCEPTS: DESIGN APPROACHES



## ART PLAN

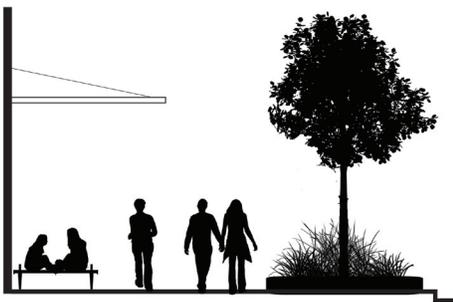
The Great Streets Art Plan includes a methodology for integrating art into the Downtown’s streets and public open spaces. This plan builds upon the street themes of shopping, entertainment and commerce, and introduces the additional elements of water, and landscape, as well as a new Art Walk that may use any or all themes.



## GATEWAYS

Five gateways indicate key entry points into Downtown. Based on the specific district and art themes in the vicinity these locations should be involved in telling the story of Bellevue at these entries through landscape, water, paving, lighting, and possibly sculpture.

- Bellevue Way at NE 12th - Neighborhood and shopping
- Bellevue Way at Main St - Shopping and Meydenbauer Bay access
- 100th at NE 8th - Neighborhood and shopping
- NE 8th at 112th - Commerce
- NE 4th at 112th - Civic activities



## FRONTAGES

Frontage segments are specific to areas of design to accomplish either a different type of use or a different type of activity. District and site-specific needs should always be taken into consideration.

- |                             |                            |
|-----------------------------|----------------------------|
| Standard Frontages          | Tree Preservation Frontage |
| Retail frontage             | Terrace Frontage           |
| Urban Neighborhood Frontage | Garden Walk Frontage       |
| Art Walk Frontage           | Planted Median Frontage    |
| Event Frontage              |                            |



## TOOLKIT

The Great Streets Toolkit provides information on detailed design, materials, furnishing and landscape elements. Criteria are provided for both standard elements and options that can be customized to enhance a particular streetscape, use, or development.

Developer options are subject to the same performance and safety criteria as the city standards. Developers and/or property owners will be responsible for the maintenance of optional treatments.



| 3 Signature Streets |





# | 3 SIGNATURE STREETS: CHAPTER CONTENTS |

## **Bellevue Way NE: Shopping Street**

Signature Street Overview .....	3.1.1
Signature Street Life .....	3.1.2
Corridor Plan.....	3.1.3
Frontage Overview .....	3.1.4
Frontages .....	3.1.5

## **106th Ave. NE: Entertainment Street**

Signature Street Overview .....	3.3.1
Signature Street Life .....	3.2.2
Corridor Plan.....	3.2.3
Frontage Overview .....	3.2.4
Frontages .....	3.2.5

## **108th Ave. NE: Commerce Street**

Signature Street Overview .....	3.3.1
Signature Street Life .....	3.3.2
Corridor Plan.....	3.3.3
Frontage Overview .....	3.3.4
Frontages .....	3.3.5



| Bellevue Way NE: *Shopping Street* |



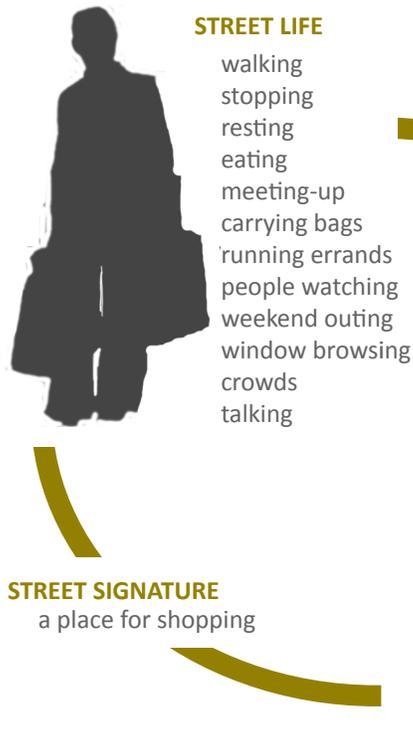


## A PLACE FOR SHOPPING...

Bellevue Way is a significant retail shopping street. Regional visitors are attracted to the corridor's year-round interest and events, while local residents interact on the street on a daily basis. The street serves as a major gateway into the downtown from the north and south, linking several districts together along the corridor. Variability along the street enhances the seasonal attraction and allows the districts to demonstrate their unique identities. Extending the Lincoln Square model, central planted medians and diverse plant palettes will help build continuity along the corridor. New gateways should celebrate the seasonal quality and shopping signature, with new celebrated intersections identifying the heart of the shopping corridor. Pocket plazas should be encouraged as the scale of the corridor transitions towards urban residential districts to the north and south. Site materials, such as paving, and site furnishings should further express the identity of each district. There is also a strong art presence on Bellevue Way, as the Art Walk route runs south from NE 6th Street, connecting people to Downtown Park.



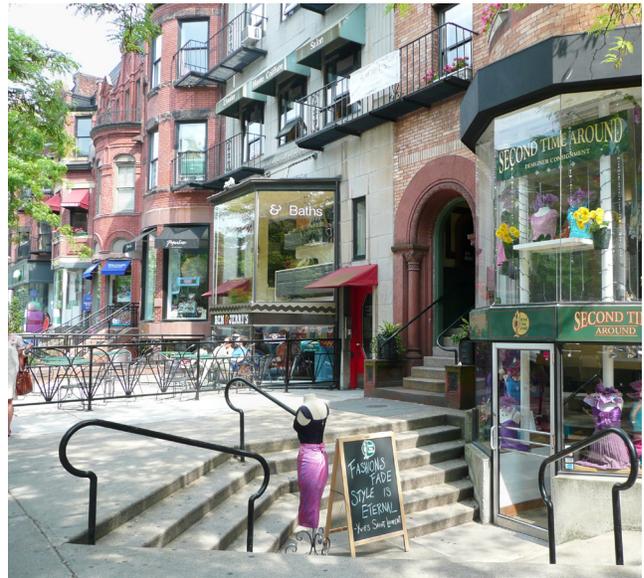
**BELLEVUE WAY NE: SIGNATURE STREET LIFE |**



**ELEMENTS OF A SHOPPING STREET**



Street furniture locations should be selected carefully, to provide resting places outside shops, cafes and other busy street fronts. The arrangement of seating should provide opportunities for small gathering and conversation, as well as opportunities for sitting alone.  
Image courtesy of [www.pbic.org](http://www.pbic.org)/Annie Lux



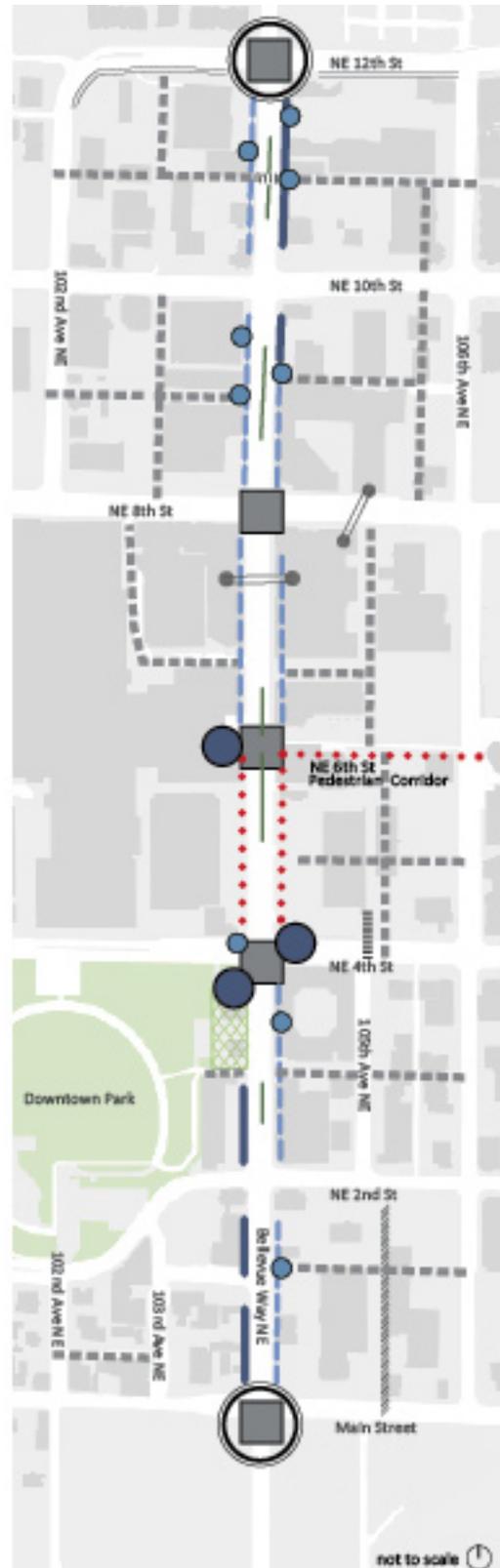
Successful retail streets encourage slower walking speeds, to promote window-browsing and shopping. Shelter, engaging facades and window displays, wide store entrances and sidewalk planting areas are common elements that encourage shoppers to linger.  
Image of Newbury Street, Boston, MA.

# | BELLEVUE WAY NE: CORRIDOR PLAN |

This plan provides an overview of proposed changes which will enable Bellevue Way NE to better express its unique signature of a retail shopping street. The designs for the frontages were conceived to accomplish specific objectives, creating standards of street design that are sensitive to the different physical and programmatic contexts. Additional street elements, such as planted medians, pedestrian crossings, pocket and art walk plazas and celebrated intersections are also located on this plan. These items are discussed in more detail in Chapter Six.

## LEGEND

-  req. 20' perimeter buffer
-  art walk frontage
-  urban neighborhood frontage
-  standard frontage
-  planted median
-  future expansion of Downtown Park
-  signalized pedestrian crossing
-  through-block connection
-  existing pedestrian bridge
-  mid-block crossing
-  pocket plaza
-  art walk plaza
-  celebrated intersection
-  gateway

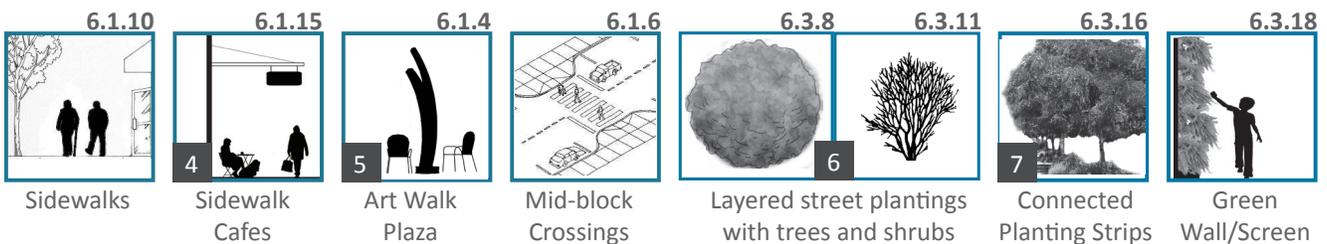
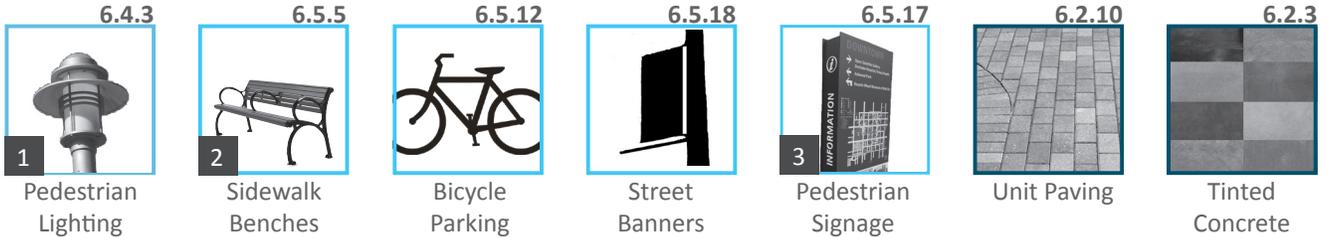


3 SIGNATURE STREETS

Bellevue Way NE  
Corridor Plan

# BELLEVUE WAY NE: FRONTAGE OVERVIEW

Along Bellevue Way NE, standard frontages, art walk frontages and urban neighborhood frontages are the foundation of Bellevue's retail shopping street. An active and successful block face will include all frontage types. The frontages are supplemented with a variety of toolkit items, including themed street furniture, engaging art spaces, outdoor displays and sidewalk cafes. Each of these elements support the needs and activities of pedestrians along Bellevue Way, and can help transform the corridor into a signature shopping street.



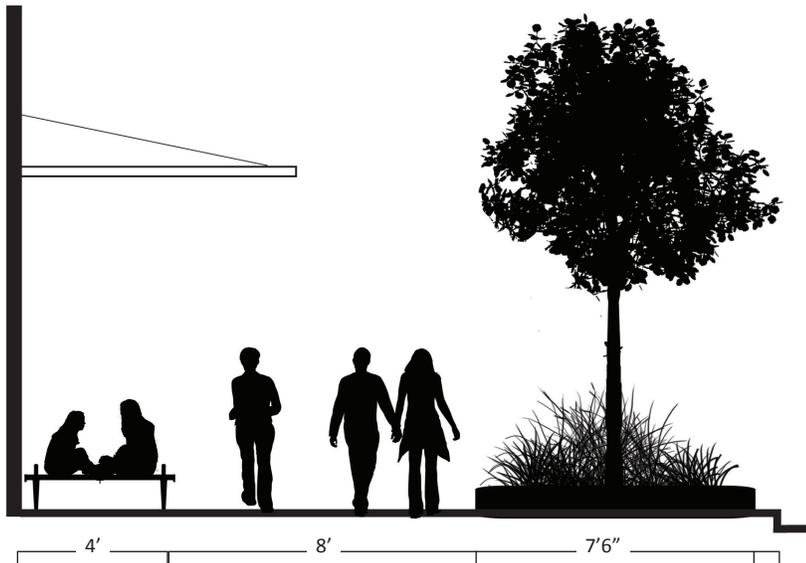
# BELLEVUE WAY NE: FRONTAGES



## STANDARD FRONTAGE - 16'

This frontage provides an element of continuity along a street within a typical 16' sidewalk which includes a 4' planting strip. Generally three modules occur between the corner of a block and the mid-block; two provide seating, the third may have bicycle parking, art, bus shelter, etc. They can occur in any sequence.

- 8' clear walk zone
- 2' seating zone
- 5'-6" planter zone
- 6" curb

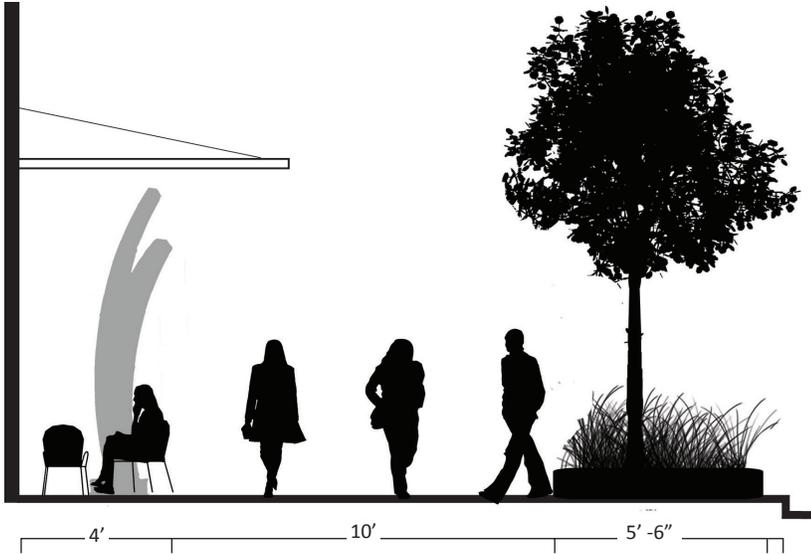


## URBAN NEIGHBORHOOD FRONTAGE - 20'

This streetscape includes up to 20' wide sidewalks which include a planting strip up to 6' wide, provisions for sidewalk cafés and urban street furniture. This is designed to accommodate higher pedestrian use with elements such as large areas for outdoor seating and other amenities.

- 4' building/seating zone
- 8' clear walk zone
- 7'-6" planter zone
- 6" curb

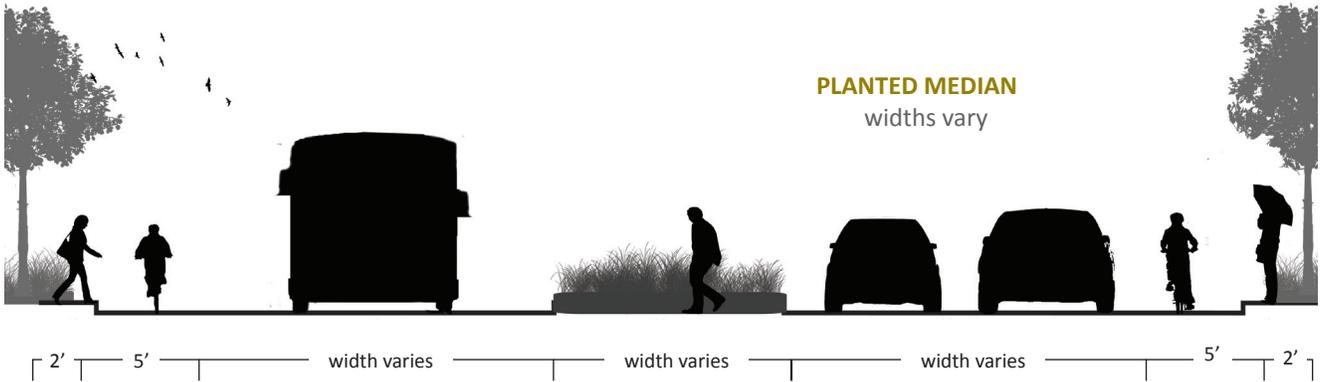
# | BELLEVUE WAY NE: FRONTAGES |



### ART WALK FRONTAGE - 20'

The Art Walk is an overlay to the Pedestrian Corridor, Bellevue Way, and traverses the Downtown Park to Meydenbauer Bay. Refer to Chapter 6 for specifics about elements recommended and opportunities along the Art Walk.

- 4' building/seating zone
- 10' clear walk zone
- 5'-6" planter zone
- 6" curb



### PLANTED MEDIAN

widths vary



| 106th Avenue NE: *Entertainment Street* |





## A PLACE FOR ENTERTAINMENT...

This north-south corridor is identified as the entertainment street. It will connect people from the residential areas to key community nodes, such as the new Tateuchi Performing Arts Center, Compass Plaza, outdoor dining, and outdoor events and activities. The focus of this corridor should encourage 18 hour liveliness and optimize social activities for the community by incorporating variable building façades, allowing wider sidewalk areas for café seating and retail spill out. Artist designed features, such as lighting integral to the sidewalk and building facades, will provide a unique element of continuity along this signature corridor. In addition, artistically designed detailing of paving patterns and unique furnishings will provide an element of discovery and attraction to the corridor.



| 106TH AVENUE NE: SIGNATURE STREET LIFE |



**STREET LIFE**

- watching & listening
- strolling
- waiting in line
- grabbing a drink
- having dinner
- meeting friends
- movie night
- attending a family festival
- going to a show
- going on a date
- visiting the market

**STREET SIGNATURE**

a place for entertainment



**STREETScape**

- places to stand
- large space to gather
- evening lighting
- shelter from rain
- places to talk & listen

**ELEMENTS OF AN ENTERTAINMENT STREET**



Entertainment streets are often busy in the evening, as people gather for dinner, to see a show, or to meet for drinks. Pedestrian lighting can enhance the pedestrian experience, and help transform the street into a destination. Photo of Vancouver, BC.



Wide streets and sidewalks allow for more flexible uses of space, such as Sunday farmer's market or sidewalk festivals. Regular markets can bring valuable foot traffic to adjacent stores, and festivals can bring tourists from throughout the region to a city's downtown.

# | 106TH AVENUE NE: CORRIDOR PLAN |

This plan provides an overview of proposed changes which will enable 106th Avenue NE to better express its unique signature as an entertainment street. The frontages were designed to accomplish specific objectives, creating standards of street design that are sensitive to the different physical and programmatic contexts. Additional street elements, such as planted medians, pedestrian crossings, pocket plazas and celebrated intersections are also located on this plan. These items are discussed in more detail in Chapter Six.

## LEGEND

-  standard frontage
-  event frontage
-  req. 20' perimeter buffer
-  through-block connection
-  potential signaled pedestrian crossing
-  potential off-peak parking
-  existing bus shelter / stop
-  new conifer tree planting
-  landmark tree
-  significant tree
-  pocket plaza
-  art walk plaza
-  celebrated intersection

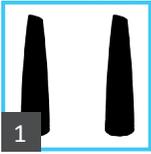
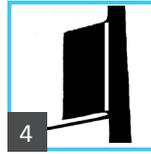


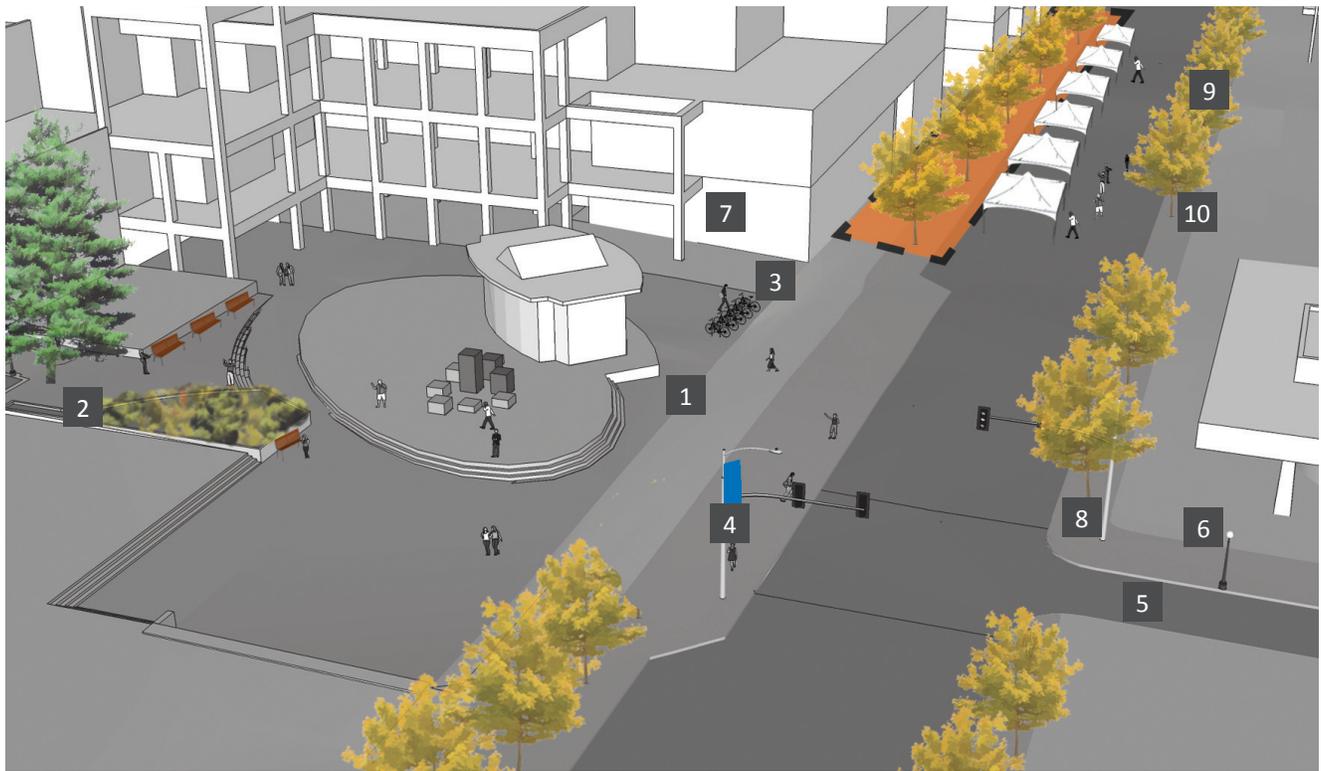
3 SIGNATURE STREETS

106th Avenue NE  
Corridor Plan

# | 106TH AVENUE NE: FRONTAGE OVERVIEW |

On 106th Avenue NE, standard frontages and event frontages integrate to form a festive, flexible streetscape. Event frontages allow space for special events, such as markets and fairs, while the standard and retail frontages provide sidewalk amenities. The frontages are supported by a diverse array of toolkit items, from pedestrian and building facade lighting to clear wayfinding signage and street banners. These elements contribute to a festive pedestrian environment, and help 106th Avenue NE become a vibrant, 18-hour entertainment street.

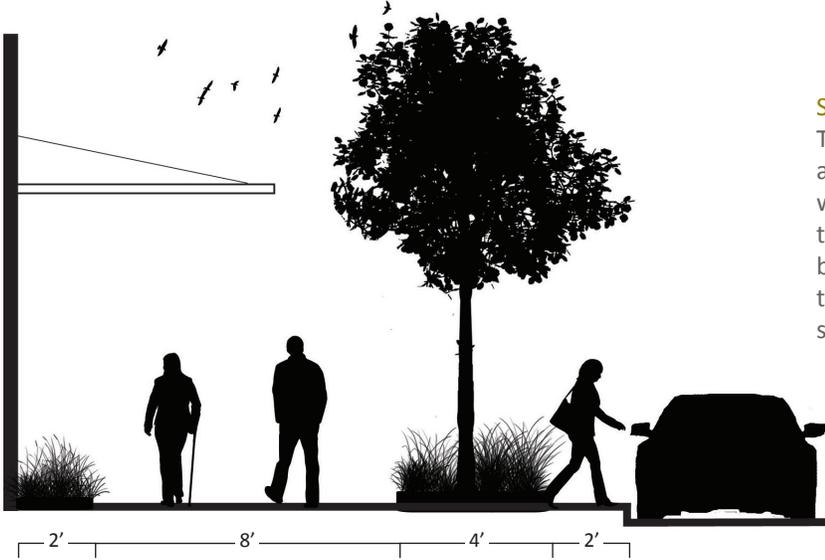
- |   |  |   |   |  |  |  |
|---|--|---|---|--|--|--|
| 6.5.10<br> | 6.5.7<br> | 6.5.12<br> | 6.5.18<br> | 6.5.17<br> | 6.4.3<br> | 6.4.4<br> |
| 1<br>Bollards   | 2<br>Seat Wall   | 3<br>Bicycle Parking  | 4<br>Street Banners   | 5<br>Pedestrian Signage  | 6<br>Pedestrian Lighting   | 7<br>Building Facade Lighting  |



<b>Event Frontage</b>	<b>Standard Frontage (not shown)</b>
-----------------------	--

- |   |   |   |   |   |   |   |   |
|---|---|---|---|---|---|---|---|
| 6.1.10<br> | 6.1.15<br> | 6.1.16<br> | 6.1.14<br> | 6.1.13<br> | 6.3.8<br> | 6.3.11<br> | 6.3.16<br> |
| 8<br>Sidewalks.   | 9<br>Sidewalk Cafes.  | 10<br>Building Canopies   | 11<br>Planter Strip Walkway.  | 12<br>Courtesy Strip  | 13<br>Layered planting areas with trees and shrubs  | 14<br>Connected Planting Strips.  | 15<br>Connected Planting Strips.  |

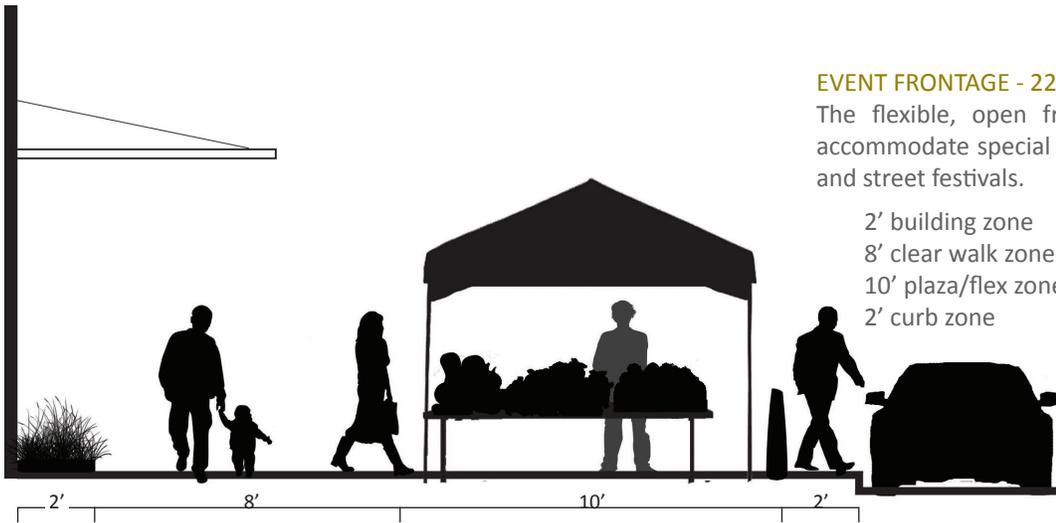
# | 106TH AVENUE NE: FRONTAGES |



### STANDARD FRONTAGE - 16'

This frontage provides an element of continuity along a street within a typical 16' sidewalk which includes a 4' planting strip. Generally three modules occur between the corner of a block and the mid-block; two provide seating, the third may have bicycle parking, art, bus shelter, etc. They can occur in any sequence.

- 2' building zone
- 8' clear walk zone
- 4' planter zone
- 2' curb zone



### EVENT FRONTAGE - 22'

The flexible, open frontage is designed to accommodate special events, such as markets and street festivals.

- 2' building zone
- 8' clear walk zone
- 10' plaza/flex zone
- 2' curb zone



| 108th Avenue NE: *Commerce Street* |



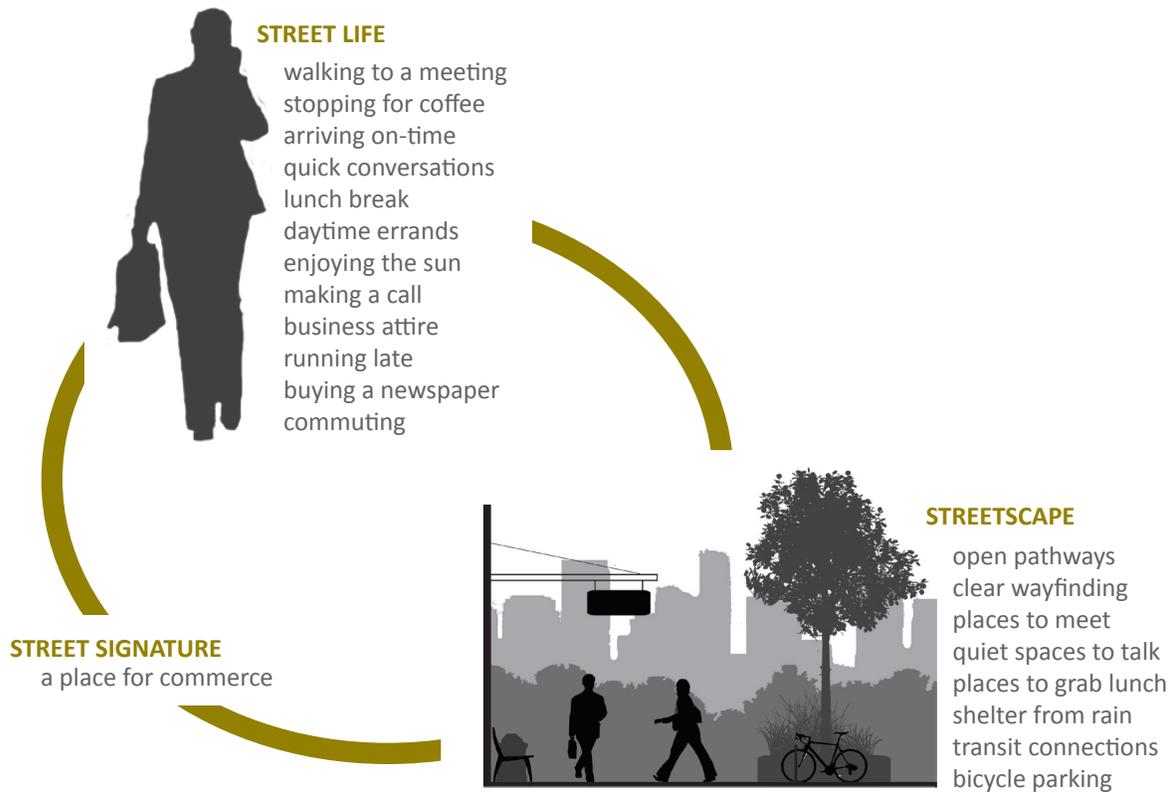
# | 108TH AVENUE NE: SIGNATURE STREET INTRODUCTION |



## A PLACE FOR COMMERCE...

This north-south corridor rests along the ridge line in downtown Bellevue. Capitalizing on this unique feature is important in place making. Expanding upon the 'City in a Park' and the city's dedication to urban forestry, the key design concept along this corridor celebrates the ridge line by introducing corner treatments at all intersections (where possible) at a scale that supports new signature conifer tree plantings. The commerce street distinction should be emphasized in the downtown core by optimizing paving treatments to accommodate multi-modal commuting. The concept of shared space with pedestrians, bicycles, buses and cars, the introduction of mid-block crossings, small pocket plazas, and outdoor coffee shop seating, will be a welcome greeting to workers.





**ELEMENTS OF A COMMERCE STREET**



Commerce streets are full of workday action, and need to accommodate the rush hour commuters, coffee breaks, lunch hours, meetings and quick errands. Successful streets offer a variety of dining options, sidewalk stands and vendors, clear sidewalks and wayfinding signage.



While commerce streets tend to be faster paced than shopping or entertainment streets, quiet spaces are also important to the streetscape. These spaces make it possible for people to grab a bit to eat, have a quick conversation, or make a phone call.

# | 108TH AVENUE NE: CORRIDOR PLAN |

This plan provides an overview of proposed changes which will enable 108th Avenue NE to better express its unique signature of a commerce street. The designs for the frontages were conceived to accomplish specific objectives, creating standards of street design that are sensitive to the different physical and programmatic contexts. Additional street elements, such as new plantings, proposed bike lanes, pocket plazas and celebrated intersections are also located on this plan. These items are discussed in more detail in Chapter Six.

## LEGEND

-  req. 20' perimeter buffer
-  standard segment
-  planted median
-  tree preservation segment
-  retail frontage
-  art walk frontage
-  potential through-block connection
-  potential signaled pedestrian crossing
-  potential bike route / lane
-  existing bus shelter / stop
-  significant tree
-  new conifer tree planting
-  pocket plaza
-  celebrated intersection
-  artwalk plaza



3 SIGNATURE STREETS

108th Avenue NE  
Corridor Plan

# | 108TH AVENUE NE: FRONTAGE OVERVIEW |

The commerce signature along 108th Avenue NE is supported by standard frontages, retail frontages and tree preservation frontages, which together form an integrated, business-oriented streetscape. These frontages are supplemented by a planted median frontages, which provides traffic calming along the street, and allows for easier pedestrian street crossings. Toolkit items, such as bike parking, on-street vending, kiosks and wayfinding further articulate the unique signature at downtown Bellevue's business center.

- 

6.1.18  
1  
Bike Lane
- 

6.5.16  
2  
Gateway Marker
- 

6.5.12  
3  
Bicycle Parking
- 

6.5.8  
4  
Trash/Recycling Receptacles
- 

6.5.17  
5  
Pedestrian Signage
- 

6.5.19  
6  
On-street Vending
- 

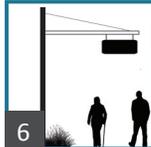
6.5.14  
7  
Bus Shelters

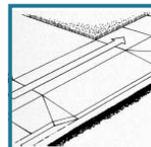


- Standard Frontage
- Retail Frontage
- Tree Preservation Frontage  
(not shown)
- Median Frontage  
(not shown)

- 

6.1.10  
5  
Sidewalks
- 

6.1.3  
6  
Pocket Plaza
- 

6.1.16  
7  
Building Canopies
- 

6.1.8  
8  
Driveway Crossings
- 

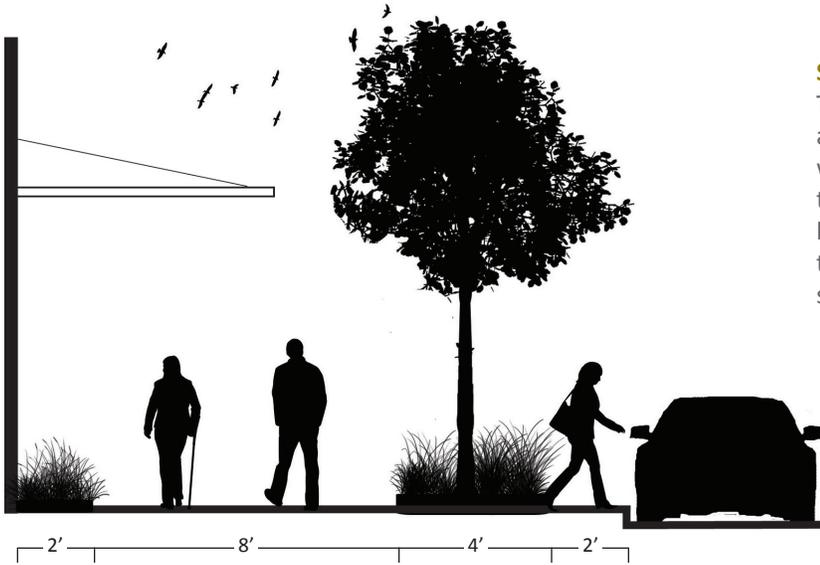
6.1.9  
9  
Medians
- 

6.3.8  
10  
Layered planting areas with trees and shrubs
- 

6.3.11  
11  
Connected Planting Strips
- 

6.3.16  
12  
Connected Planting Strips

# | 108TH AVENUE NE: FRONTAGES |



### STANDARD FRONTAGE - 16'

This frontage provides an element of continuity along a street within a typical 16' sidewalk which includes a 4' planting strip. Generally three modules occur between the corner of a block and the mid-block; two provide seating, the third may have bicycle parking, art, bus shelter, etc. They can occur in any sequence.

- 2' building zone
- 8' clear walk zone
- 4' planter zone
- 2' curb zone

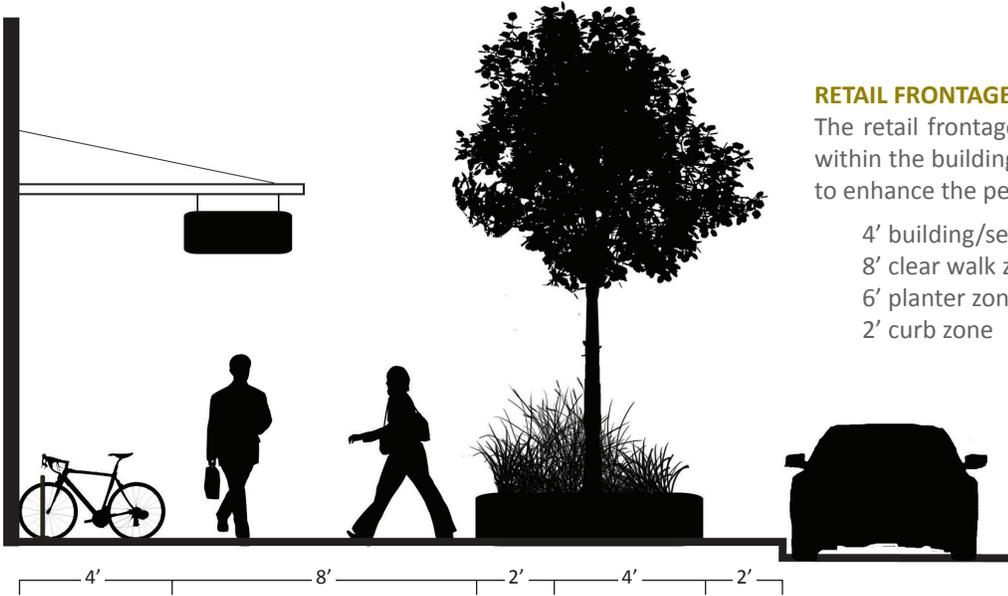


### TREE PRESERVATION FRONTAGE - 18'

The tree preservation frontage allocates extra space within the planter zone, in order to protect existing trees during new development.

- 2' building zone
- 8' clear walk zone
- 6' planter zone
- 2' curb zone

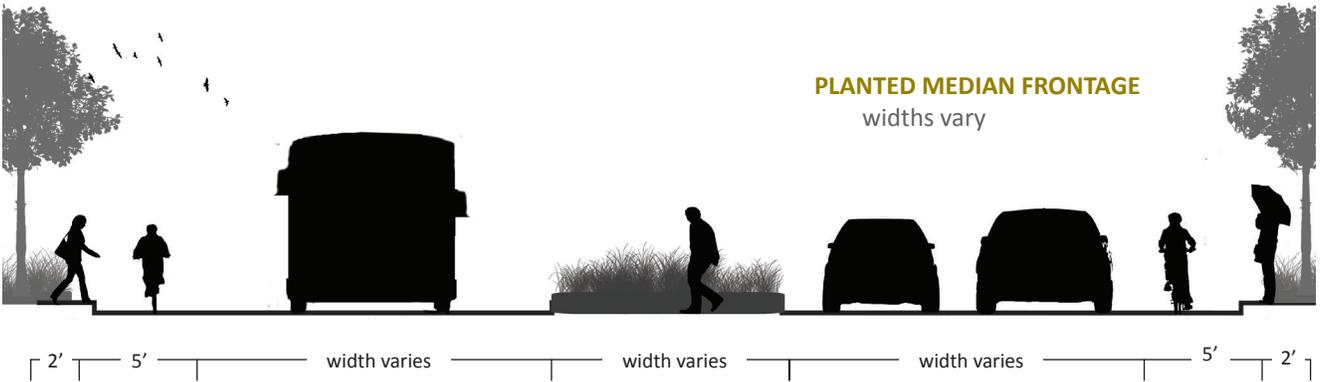
# | 108TH AVENUE NE: FRONTAGES |



### RETAIL FRONTAGE - 20'

The retail frontage allocates a bit more room within the building and planter zones, in order to enhance the pedestrian experience.

- 4' building/seating zone
- 8' clear walk zone
- 6' planter zone
- 2' curb zone



### PLANTED MEDIAN FRONTAGE

widths vary



# | 4 Crosstown Connections |





# | 4 CROSTOWN CONNECTIONS: CHAPTER CONTENTS |

## **CROSTOWN CONNECTIONS**

Overview .....	4.1.1
Street Life .....	4.1.2
Before & after simulation .....	4.1.3

## **NE 4TH & NE 8TH STREETS**

NE 4th Corridor Plan .....	4.2.1
NE 4th Frontages .....	4.2.2
NE 8th Corridor Plan .....	4.2.1
NE 8th Frontages .....	4.2.2

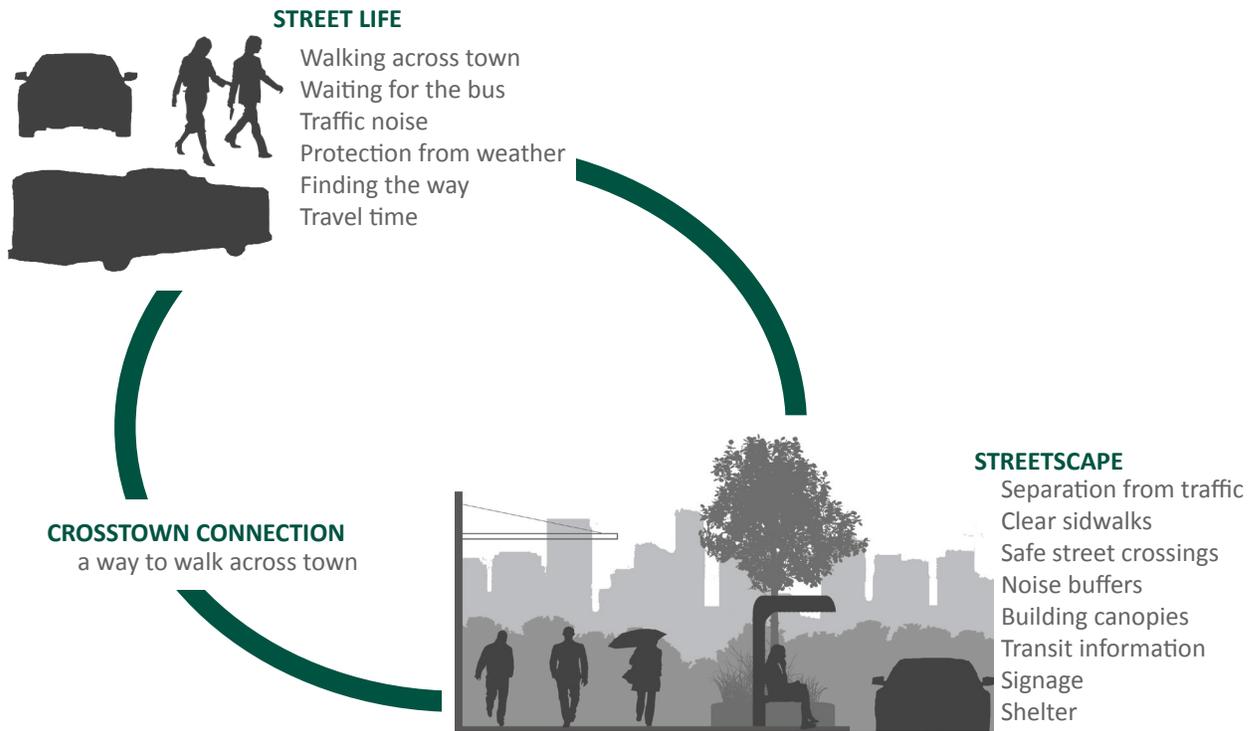


## A WAY TO WALK ACROSS TOWN...

Running parallel to the district grain, this east-west connector supports significant vehicular capacity resulting in a scale less conducive for pedestrian comfort. Key design elements need to accommodate visibility and user interaction from moving vehicles while enhancing pedestrian comfort and enjoyment to encourage walking along these corridors. The introduction of a central planted median and terrace segment, where possible, will allow for a softening of scale while providing an element of continuity along the corridor and linear gateway treatments. This combined with new distinct street lighting will provide a quick glance welcome from the vehicular viewpoint. An enhanced planting buffer combined with properly located benches will provide a more enjoyable walking experience within the standard frontage.



| CROSTOWN CONNECTIONS: STREETLIFE |



ELEMENTS OF A CROSTOWN CONNECTION

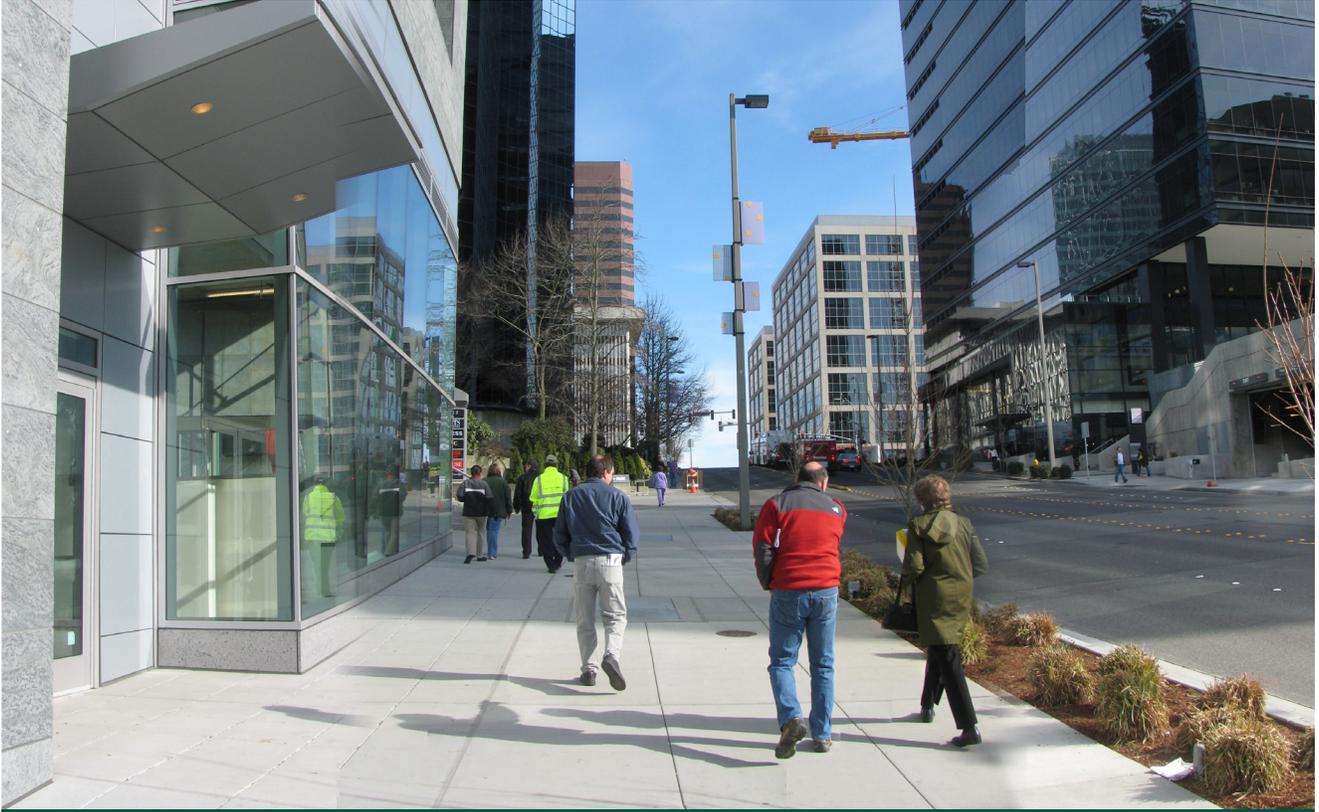


Shelters at bus and transit stops provide an important amenity for pedestrians waiting to use public transportation. Shelters increase the safety and comfort of commuters, and can also add visual interest to the streetscape. Courtesy of Greg Griffin, [www.pedbikeimages.org](http://www.pedbikeimages.org).



Planted buffers between pedestrians and cars not only improve the aesthetics of the street, but also its safety. Plants buffer walkers from traffic noise, and the flow of traffic, by designating a clear pedestrian corridor. Courtesy of Dan Burden, [www.pedbikeimages.org](http://www.pedbikeimages.org).

# | CROSTOWN CONNECTIONS: SIMULATIONS |



NE 4th Street & 106th Avenue NE - BEFORE



NE 4th Street & 106th Avenue NE - AFTER

| NE 4th & NE 8th Streets |



# NE 4TH STREET: CORRIDOR PLAN

NE 4th Street is an auto-bias street; one that calls for a pedestrian friendly environment, but is intended to accommodate high volumes of traffic. The corridor design is predicated on buffering pedestrians from the traffic with landscape elements. The frontage designs call for curbside plantings, terraced planting where possible, planted medians and preserving some of the most mature and beautiful trees in the downtown area.

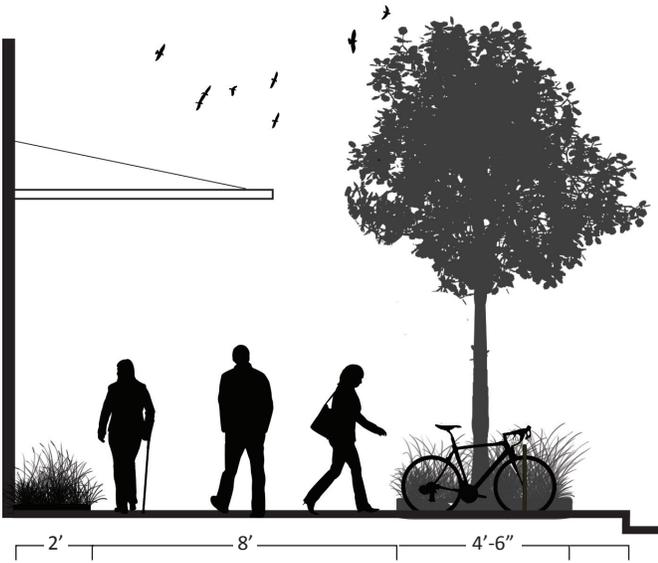
## LEGEND

-  standard frontage
-  garden walk frontage
-  planted median
-  req. 20' perimeter buffer
-  existing through-block connection
-  potential through-block connection
-  potential signaled pedestrian crossing
-  mid-block crossing opportunity (potential pedestrian bridge accommodation)
-  new conifer tree planting
-  significant tree
-  landmark tree
-  pocket plaza
-  art walk plaza
-  celebrated intersection
-  gateway



NE 8th Street Corridor Plan

| NE 4TH STREET: FRONTAGES |



**STANDARD FRONTAGE - 16'**

This frontage provides an element of continuity along a street within a typical 16' sidewalk which includes a 4' planting strip. Generally three modules occur between the corner of a block and the mid-block; two provide seating, the third may have bicycle parking, art, bush shelter, etc. They can occur in any sequence.

- 2' building zone
- 8' clear walk zone
- 4'-6" planter zone
- 1'-6" curb zone

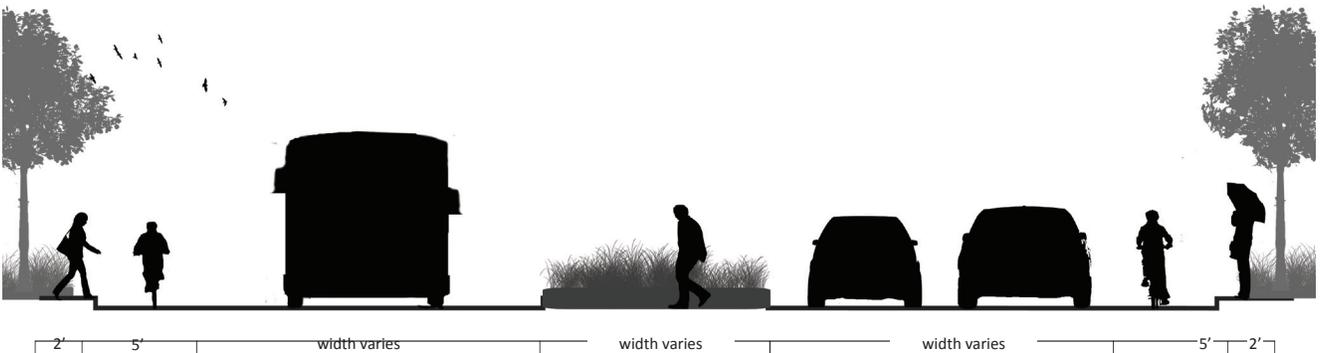


**TERRACE FRONTAGE - 20'**

This frontage can be used when there are significant grade changes between along the sidewalk.

- building zone varies
- 8' clear walk zone
- 5'-6" planter zone
- 6" curb

**PLANTED MEDIAN**  
widths vary

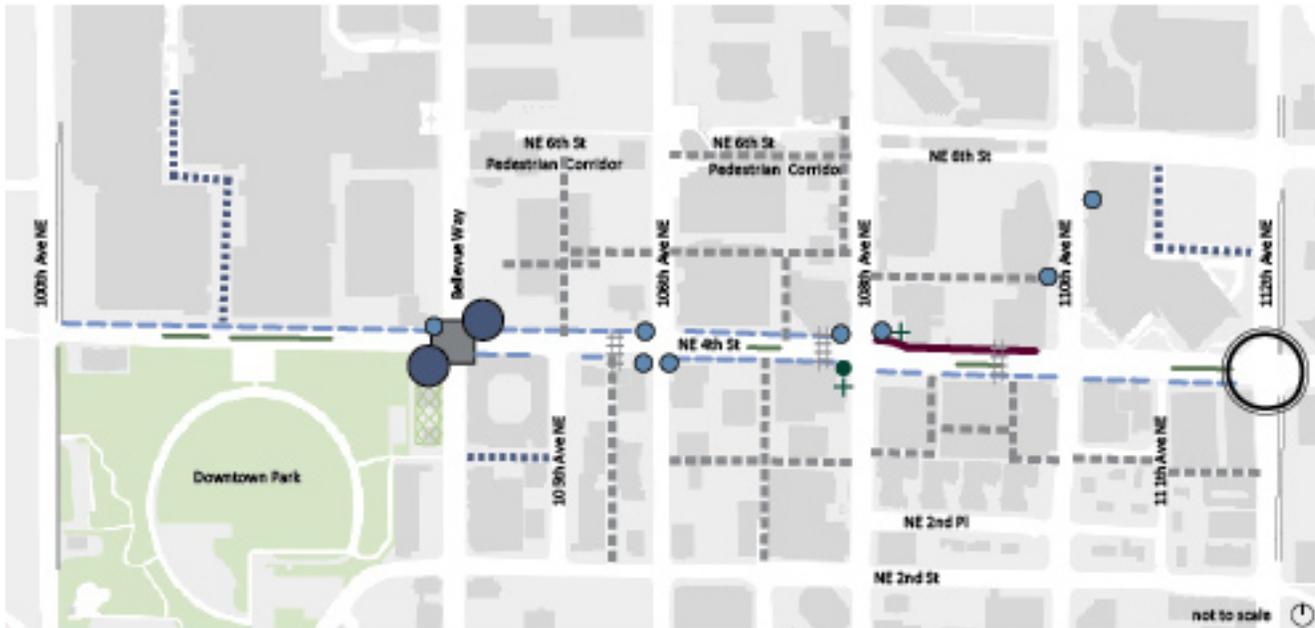


# NE 8TH STREET: CORRIDOR PLAN

NE 8th Street is an auto-bias street providing the key entryway to the Downtown. It will continue to carry high traffic volumes, but must also provide a pedestrian-friendly environment as it is a main frontage for The Bravern and The Bellevue Collection. The corridor design is predicated on buffering the pedestrians from traffic with landscape elements, taking advantage of existing garden-like streetscapes, and expanding them where possible. The frontage design identified call for curbside plantings, preserving some of the most mature trees in teh Downtown, installing new conifers to tell the story of the city in a park, and planted medians.

**LEGEND**

- standard frontage
- terrace frontage
- planted median
- req. 20' perimeter buffer
- future expansion of Downtown Park
- existing through-block connection
- potential through-block connection
- mid-block crossing opportunity (potential pedestrian bridge accommodation)
- new conifer tree planting
- significant tree
- pocket plaza
- art walk plaza
- celebrated intersection
- gateway



NE 4th Street Corridor Plan

NE 8TH STREET: FRONTAGES



STANDARD 16' FRONTAGE

This frontage provides an element of continuity along a street within a typical 16' sidewalk which includes a 4' planting strip. Generally three modules occur between the corner of a block and the mid-block; two provide seating, the third may have bicycle parking, art, bus shelter, etc. They can occur in any sequence.

- 8' clear walk zone
- 2' seating zone
- 5.5' planter zone
- 6" curb



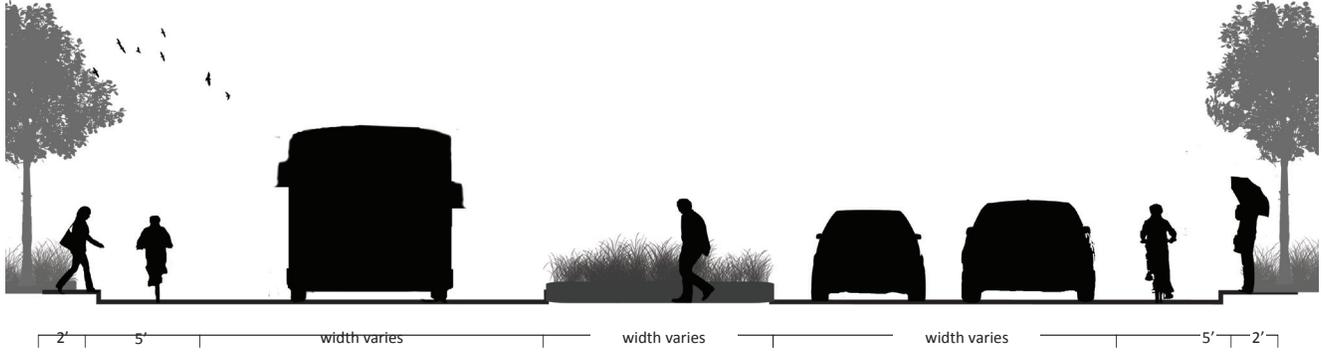
GARDEN WALK 19' FRONTAGE

The garden walkway integrates planted areas on either side of the clear walk zone, to bring a more buffered and gardenesque feeling to the streetscape.

- planter width varies
- 8' clear walk zone
- planter width varies

PLANTED MEDIAN 16'

widths vary





| 5 ART PLAN |





# | 5 ART ELEMENTS: CHAPTER CONTENTS |

## **Art Plan Overview**

System of Conductors .....	5.1.1
Art Plan Diagram .....	5.1.2
Catalysts of Flow .....	5.1.3

## **Art Walk**

Route Overview .....	5.2.1
----------------------	-------

## **Circuits**

Water .....	5.3.1
Landscape .....	5.3.3
Shopping .....	5.3.5
Entertainment .....	5.3.7
Commerce .....	5.3.9

# | ART PLAN: SYSTEM OF CONDUCTORS |

## FLOW

The Great Streets Art Element uses art to express the flow of activity and life in the downtown. Organized into thematic strands, or circuits, along several routes, art functions as a “System of Conductors,” catalyzing a flow of perception and experience as one moves along the circuit.

This “System of Conductors” locates art in specific downtown locations and links specific themes to those locations. The spine of this System is a network of “Circuits,” or strands of art. Just as a current can change character as it intersects or parallels another current, art themes alter, evolve or transform as they come into contact with other flows. These contacts are punctuated by special art events referred to as “Nodes” and “Interfaces.”

## CIRCUITS

The “Circuits” consist of linear bands or integrated art elements running primarily through the signature street corridors and crosstown connections, in which an artist takes a functional piece of the streetscape, such as sidewalk paving or pedestrian lighting, and treats it artistically. These treatments are generally smaller in terms of their visual impact as art, although the messages they communicate can be quite profound.

A different theme, characterizes each Circuit: water, landscape, shopping, entertainment, and commerce that are linked to contextual conditions. The Circuits intersect, run parallel, intertwine, and build upon each other. One unique Circuit, the Art Walk, crosses all the other typological Circuits and passes through many of the Interface project areas.

## NODES

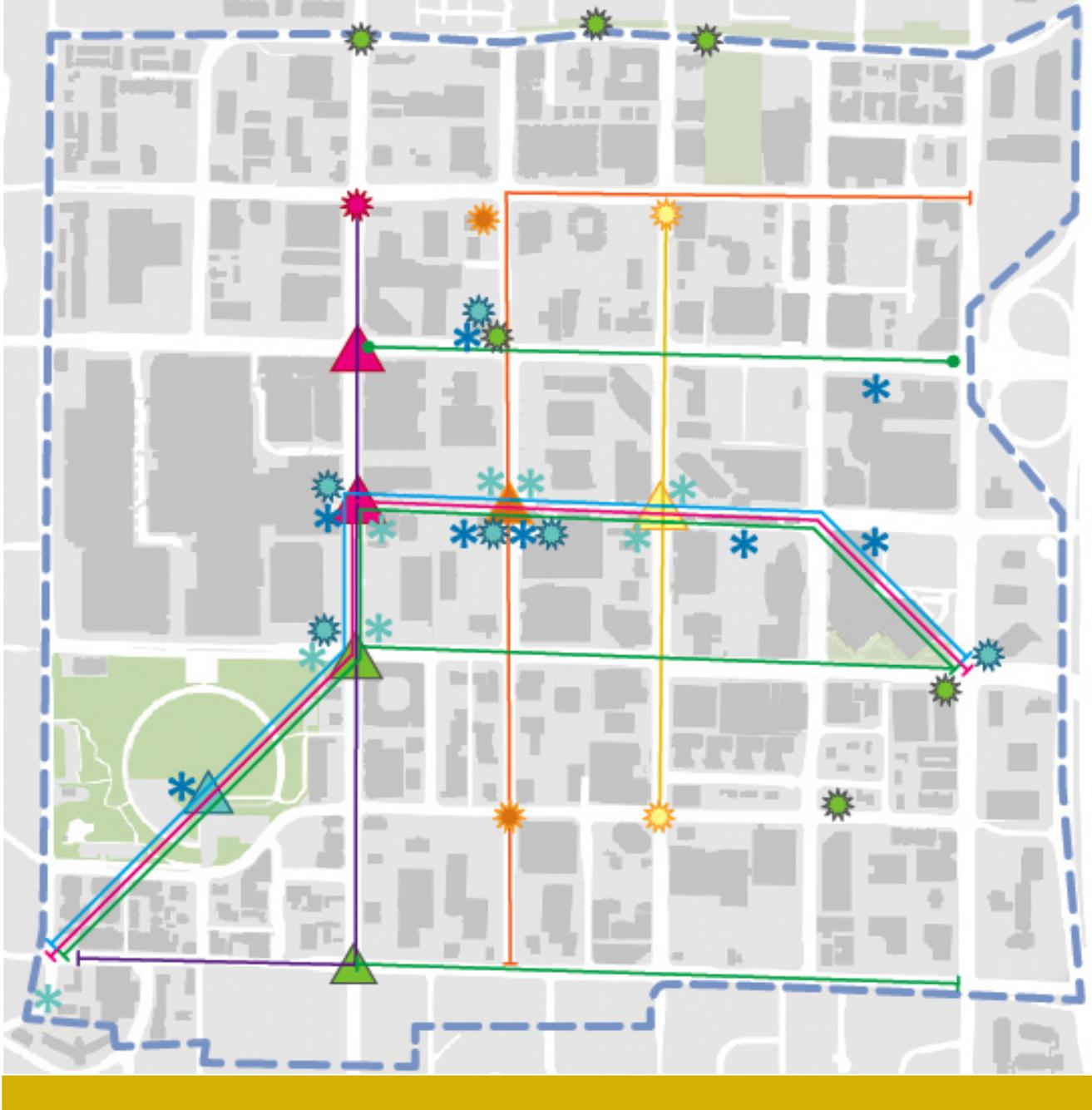
“Nodes” are stand-alone or integrated art projects. Each Node explores a single theme (water, landscape, shopping, entertainment, or commerce) within the System of Conductors. Key existing art projects already serve as Nodes to be built upon.

## INTERFACES

“Interfaces” are major artworks at significant street intersections. They occur at gathering spaces where various Circuits intersect, and are conceived as places of transfer between the different art themes. At each Interface, one theme should be dominant. The Interface artworks should be at a scale large enough to be perceived by both vehicular passengers and pedestrians. The Art Walk runs through many of the Interfaces.

Walking through downtown Bellevue a sense of dynamic energy is apparent. To tap into this energy, art projects resulting from the Great Streets Art Elements Plan should incorporate various media that embody a quality of “flow,” creating environments responsive to and expressing the flow of people, nature, technology, information, and transportation that characterizes downtown.

# | ART PLAN: OVERVIEW |



## ARTWALK LEGEND

	downtown outline		commerce interface		commerce node
	artwalk circuit		water interface		water node
	shopping/display circuit		landscape interface		landscape node
	green/landscaping circuit		shopping interface		shopping node
	water circuit		entertainment interface		entertainment node
	commerce circuit		existing water		potential water
	culture/entertainment circuit				

## | ART PLAN: CATALYSTS OF FLOW |

Whereas the “System of Conductors” locates the art in specific downtown locations and links specific themes to those locations, the “Catalysts of Flow” are the media that activate the art.

The Catalysts of Flow include:

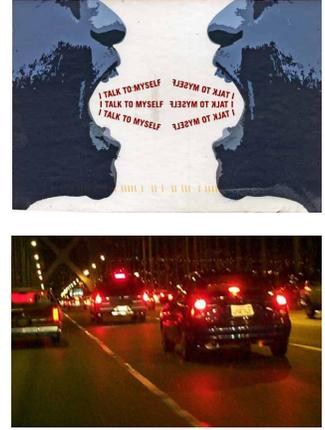
<b>Water</b>	Manifestations of water in its different states, different surface appearances, and different rates.
<b>Seasonal Change</b>	Compositions of plants with changing colors, density, texture, and size.
<b>Light</b>	Artificial, solar-powered, and natural light emphasizing brightness, color, reflectivity, and interactivity.
<b>Sound</b>	Audio soundscapes of artificial and natural sound amplified, distorted, re-contextualized, and made interactive.
<b>Pedestrian Motion</b>	Kinetic expressions of walking that tap into the pedestrian cycles of work and leisure, street interactions, and social behavior.
<b>Vehicular Motion</b>	Kinetic expressions of vehicle movements that respond to speed, perception when moving, and stop and go rhythms.
<b>Performance</b>	Temporary, staged, and non-staged performances and celebrations of events that are people-activated, culturally diverse, and unite the community.
<b>Speech</b>	Gatherings and assemblies of the community activated by dialogue and public speaking.
<b>Time</b>	Temporal activations and fluctuations expressed by momentary changes as well as diurnal, seasonal, and annual cycles.
<b>Virtual Space &amp; Technology</b>	Cultural shifts of technology revealed through public interactions, networks, and cellular phones.

The Conductors promote and channel the Catalysts of Flow which in turn connect the real place to aesthetic revelation, allowing the artworks to continually exist “in the present moment” through the flow of their constantly transforming physical manifestations.

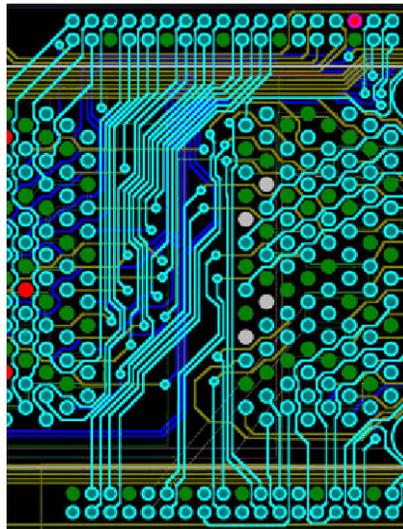
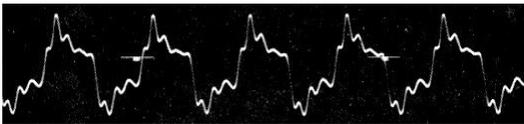
### CITY IN A PARK

Bellevue’s identity as the “City in a Park” is reinforced in the art plan through a series of Nodes in the Landscape Circuit around the perimeter of downtown. These act as gateways into and out of the city. This landscape-based art emphasizes the use of landscape materials and processes in interesting and unexpected ways, while also tying into “green” landscape practices.

# | ART PLAN: CATALYSTS OF FLOW |



S ART PLAN



Images of Catalysts of Flow themes

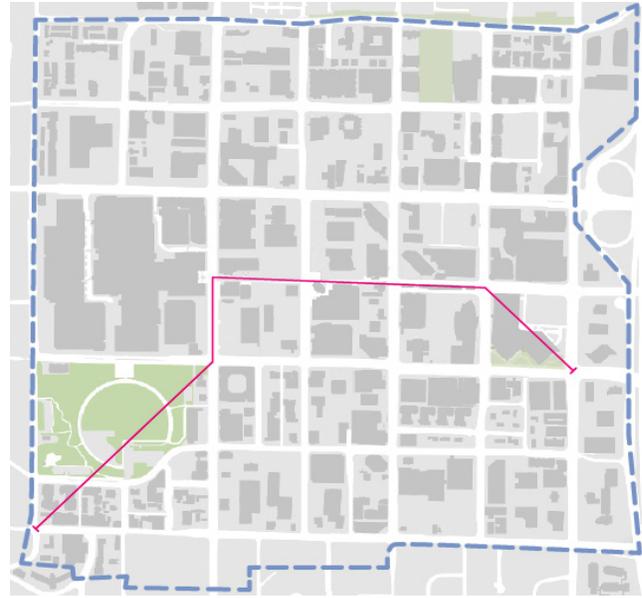
# | ART WALK : ROUTE OVERVIEW |

## ROUTE

The Art Walk begins at City Hall, runs down NE 6th Street to the Bellevue Arts Museum, turns south down Bellevue Way, leads into Downtown Park and on to Meydenbauer Park and Bay

## CONCEPTS

While the Art Walk crosses all circuits, it is especially linked to the Water Circuit. The art along this circuit activates the passage from downtown to the waterfront through the use of water, reinforcing Bellevue's relationship to Lake Washington. The Art Walk becomes a directional, wayfinding device, flowing downhill to the lake.



S ART PLAN



Intersection of NE 6th Street and 110th Avenue NE, looking southeast to City Hall.



Existing pedestrian corridor on NE 6th Street, between 106th and 108th Avenues NE.



Bellevue Art Museum entry on Bellevue Way.



Bellevue Way at NE 6th Street, in front of Bellevue Square.



NE 6th Street, between Bellevue Way and 106th Avenue NE, with Bellevue Art Museum on right.



Existing special paving at pedestrian intersection at the corner of Bellevue Way and NE 6th Street.



# | ART WALK: ROUTE OVERVIEW |



Art in the City of Bellevue

## | CIRCUITS: WATER |

### CONCEPT

Art in the Water will make water more visible through its integration into multiple systems, including rainwater, irrigation water, drinking water, water fountains, and water paths—when possible combining several of these systems. The art will collect, release, and channel water. In places where “real” water is not possible, water can be simulated through paving, lighting, electronic and other media arts.

### CIRCUITS

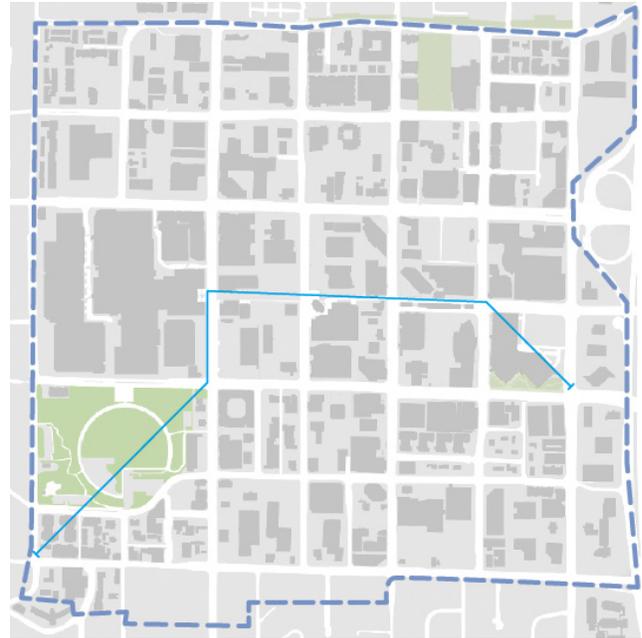
The Water Circuit parallels the Art Walk into the Downtown Park and on to Meydenbauer Bay. Water Circuit art is focused primarily on using rainwater and irrigation water both functionally and artistically, and might include: sculptural awnings, roof gutters, and drainpipes integrated into building facades that channel rainwater; runnels in sidewalks, planter strips, and medians that activate rainwater; irrigation expressed artistically; and artist-designed drinking fountains that could also supplement irrigation.

### NODES

Water Nodes are envisioned as fountains that combine sculpture, water, light (and other Catalysts of Flow) to express aspects of their context. The Water Node marking the Downtown Park entry at Bellevue Way and NE 4th Street connects the city’s commercial energy with its natural energy through the element of water.

### INTERFACES

A Water Interface occurs on the south side of Downtown Park. It combines Water and Landscape themes, with an emphasis on Water, for instance a sculptural cistern that collects water and channels it to a rain garden.



Captured rainwater held in cistern and redirected to plants. © 2003 Buster Simpson, Beckoning Cistern



Interactive irrigation water. © 2004 Nobuho Nagasawa, Seeding Time

## | CIRCUITS: WATER |

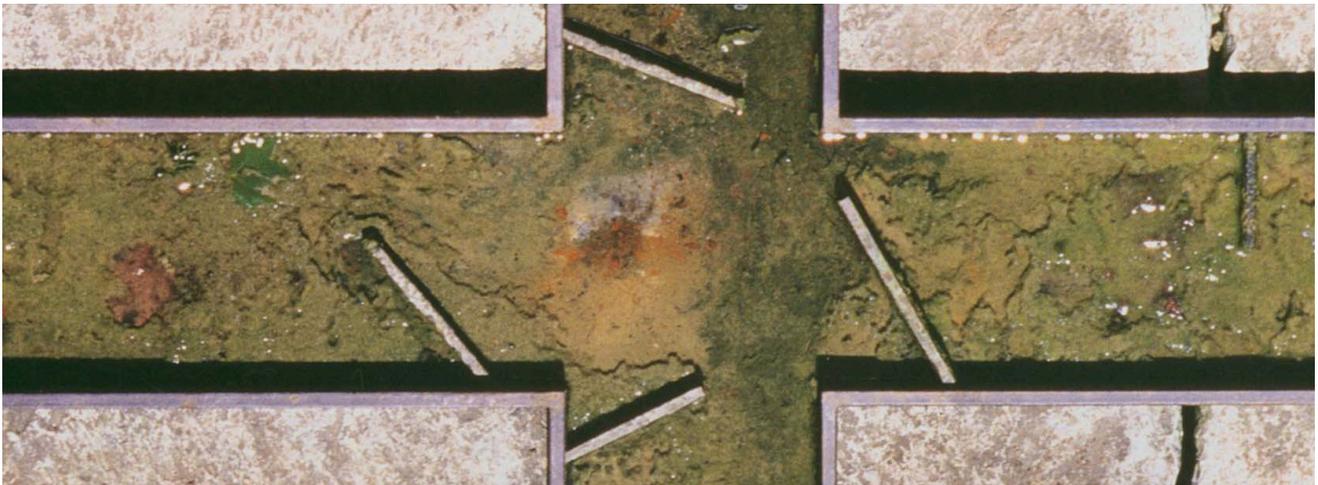


Rainwater channeled through sculptural paving features. © 2003 Laura Haddad, Undercurrents



Sculptural drainpipe transports water. © 2007 Haddad|Drugan, Lineage

5 ART PLAN



Rainwater runnels that activate water. © 1995 Kathryn Gustafson, Court of Appeals



Water expressed through paving pattern. Cobblestone paving at Rossio Square, Lisbon



Water expressed through light at Seattle City Hall. © 2005 Nabuho Nagasawa, Water Weaving Light Cycle

## | CIRCUITS: LANDSCAPE |

### CONCEPT

Art in the Landscape uses plants, soil, and grade change to emphasize the “city in a park” theme. The art might include painterly use of plant colors, textures, fragrances, and seasonal effects; and sculptural pots, trellises, and green walls. Art with a Landscape theme is a component found in each of the gateways ringing the perimeter of downtown.

### CIRCUITS

Landscape Circuits occur along various downtown street corridors, primarily in planter strips and medians, with areas of emphasis on parts of Bellevue Way, NE 6th Street, NE 8th Street, NE 4th Street, and Main Street which connects to the Lake to Lake Trail.

### NODES

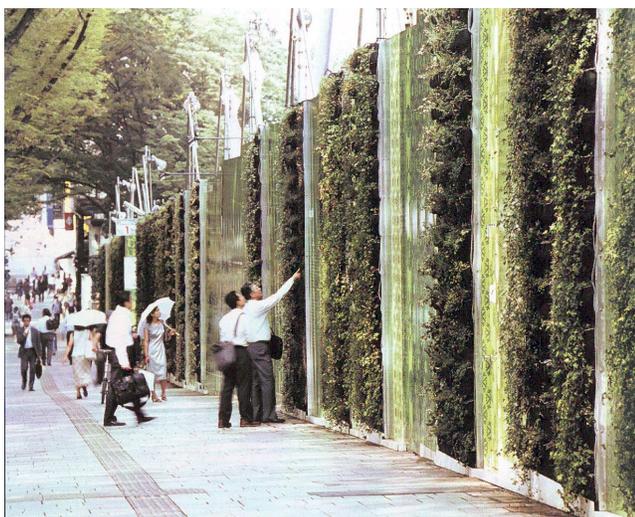
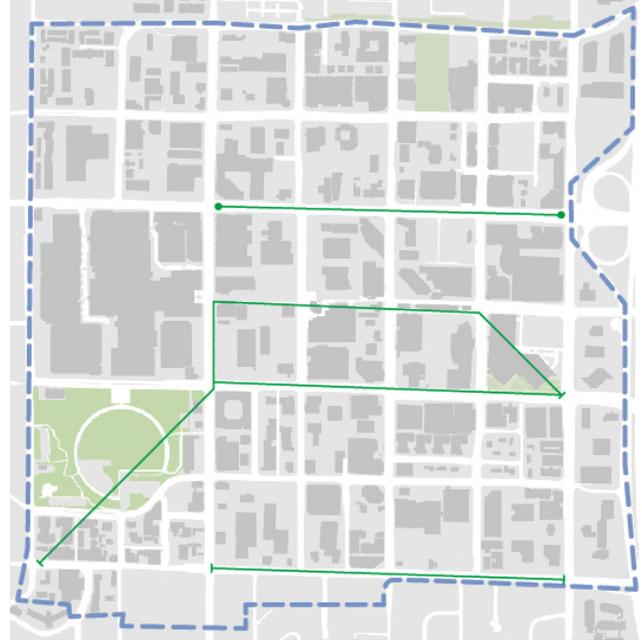
Landscape Nodes – let the gateways be stand-alone elements – see “concepts” above

### INTERFACES

Landscape Interfaces are place-making artworks at four intersections. While art for these intersections will likely include special paving, a landscape theme should be expressed, either through the paving pattern or additional planted/sculptural/earth elements that might express other relevant themes such as Water or Shopping.

Special emphasis should be given to the Landscape Interface at Bellevue Way and NE 4th Street, as it is along the Art Walk and the primary link between Downtown Park and the urban streetscape of downtown.

The Landscape Interface at the intersection of Bellevue Way and Main Street is a gateway to both the Shopping District and Old Bellevue, and provides a unique opportunity for art to come along with shopping, water, and landscape motifs.



Green wall. © 2003 Klein Dytham, Green Green Screen



Lighting dramatizes paintings.

| **CIRCUITS: LANDSCAPE** |



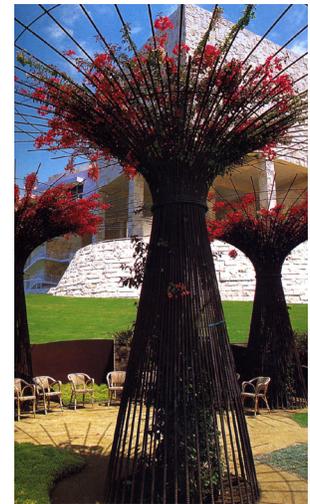
Plantings in cultural patterns. © 2005 Ann Chamberlain, Golden Gate Park Basket Pattern Earthwork



Plantings with bands of color. © 1994 Jenny Holzer, Black Garden



Utilitarian surprises and micro ecosystems. © 1999 Buster Simpson, Vertical Landscape Downspouts



Sculpture as vine trellis. © 1997 Robert Irwin, Central Garden (Getty Center)



Earthwork as monumental gateway. © 1980 Andrew Leicester, Cloverleaf



Green gateway over I-405. Burlington Northern Railroad Bridge, over I-405 in Bellevue

## | CIRCUITS: SHOPPING |

### CONCEPT

Art in the Shopping theme is focused on Bellevue Way, reinforcing its designation as a “shopping street”.

Since Bellevue Way also includes the Bellevue Arts Museum, art within the Shopping Typology may include aspects of display and exhibition, as well as the shopping experience of seeing and being seen. The art might incorporate reflective materials that draw on shop mirrors, and “art windows” in storefronts and sidewalks with both permanent and temporary displays. Another way to manifest the Shopping Circuit is through art masquerading as signage.

### CIRCUITS

Shopping Circuits extend along Bellevue Way from NE 10th Street to Main Street, passing by Bellevue Square; and along Main Street, through the heart of Old Bellevue. Art along the Shopping Circuit could be integrated into sidewalks, building facades and canopies as well as seating and lighting.

### NODES

A Shopping Node is located at the intersection of Bellevue Way and NE 10th Street. Art here should act as a gateway into the shopping district. It could incorporate many of the ideas described above, but at a larger scale that can be perceived from a vehicle.

### INTERFACES

A Shopping Interface occurs at the intersection of Bellevue Way and NE 6th Street. As a nexus of multiple conditions and Circuits, this Interface threads the main pedestrian spine of the Art Walk through the Bellevue Art Museum entry, Bellevue Square entry, and Westin Hotel entry. The art Interface could include paving that acts as a foreground for shopping, and emphasizes relationships of seeing and being seen.

A second Shopping Interface is located along Main Street in the heart of Old Bellevue. Art here could mark linkages between current and historic retail operations, as well as the elements of landscape and water that characterize its nearby context.



## | CIRCUITS: SHOPPING |



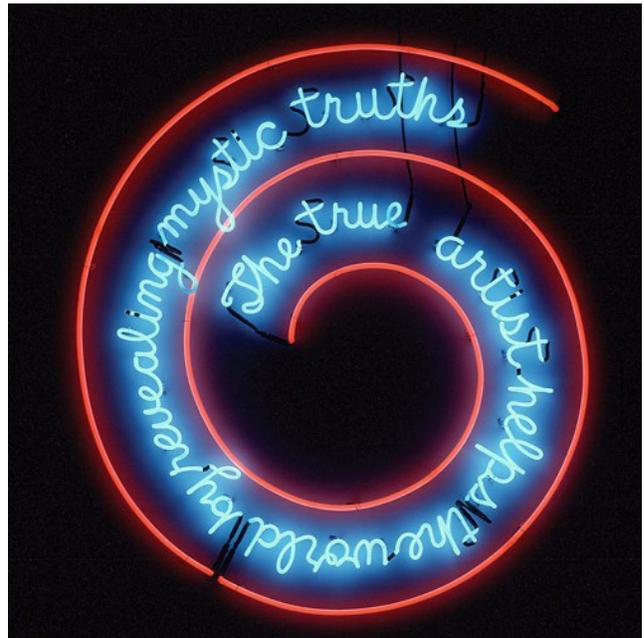
Signature sculpture with reflectivity and interaction speaks to the phenomenon of “see and be seen”. © 2004 Anish Kapoor, Cloud Gate



Shop windows house art. © 2006 Olafur Eliasson, Eye See You



Artist-designed signage speaks to themes of display and consumerism. Shop windows as art Shop windows as art. © 2006 Rebecca Hackemann, Peek (left). © 1967 Jenny Holzer, The True Artist Helps the World by Revealing Mystic Truths (Window or Wall Sign) (right)



## | CIRCUITS: ENTERTAINMENT |

### CONCEPTS

Art in the Entertainment theme is focused on 106th Avenue NE, reinforcing its designation as the “entertainment street” and occurs on adjacent legs of NE 6th Street and NE 10th Street as well.

Throughout the entertainment district the streetscape is conceived as outdoor theater, with pedestrians acting as both the performers and audience, and the art activated by human interaction. Some art here could be temporary.

### CIRCUITS

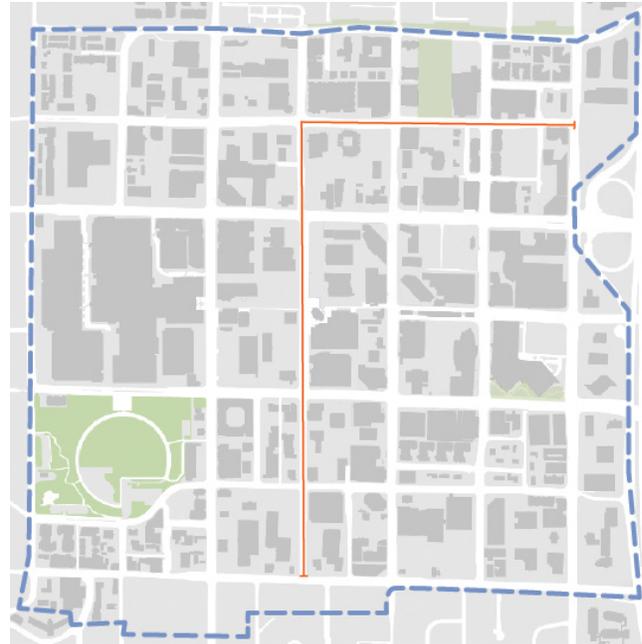
Art along the Entertainment Circuit includes enhancements to standard streetscape elements like sidewalks, pedestrian lighting, and planting. Paving is treated like a stage, with integrated interactive elements of electronic media and sculpture, and theatrical lighting that turns landscape into a stage set.

### NODES

Entertainment Nodes are located in front of the Tateuchi Performing Arts Center and at the northeast corner of the Bellevue Arts Museum. These might use various entertainment media such as electronic music players, wireless access to artworks, sound and light shows, and projections; or include kiosks that unfold to become temporary theater spaces. Entertainment Nodes might also include a continuously changing series of temporary artworks integrated into sidewalks, plazas, and building facades.

### INTERFACES

The Entertainment Interface at the intersection of 106th Avenue NE and NE 6th Street has the potential to be the landmark, signature place that identifies Bellevue. It



5 ART PLAN

is in the center of downtown, and at the base of NE 6th Street’s pedestrian staircase. It could build upon Compass Plaza and the existing water and kinetic art pieces already located there. Multiple Circuits, including the Art Walk, Entertainment, Water, and Landscape Circuits, intersect at this Interface, setting up a scenario for the joint performance of culture and nature through art that is activated by natural forces such as rain and plants, cultural forces such as people and performance, and artificial media such as lighting and electronic-based artwork.



Theatrically-lit trees. © 2007 Dan Corson, Luminous Conjunction

# | CIRCUITS: ENTERTAINMENT |



Temporary art enlivens the street. © 1986 Jenny Holzer, Truisms (left); Paving and lighting as outdoor theater. © 1998 Martha Schwartz, Flying Saucer Grove (right).



Artist-designed kiosks unfold into temporary stages. © 1993 Jorg Joppien, Grid Iron (left); © 2006 Brut Deluxe Arquitectos, Kiosco (right).



Interactive paving, both electronically and physically activated. © 2004 Chris Salter, Erik Adigard, Mathew Biederman, Gregory Cowley, chronopolis (left); © 1982 Jack Mackie, Dance Steps on Broadway (right).

# | CIRCUITS: COMMERCE |

## CONCEPTS

Art in the Commerce Typology is focused on 108th Avenue NE, between NE 8th Street and NE 2nd Street. This reinforces the designation of 108th Avenue NE, the central locus for Bellevue’s flourishing business community, as the “commerce street”.

Art tying into the Commerce theme could include international languages, currency, the technology and devices that characterize business. A fascinating aspect of the district, which art may draw on, is the phenomenon of waves of people moving through the streetscape during commuting times at the beginning and end of the workday, as well as at lunchtime. The adjacent transit center is a pulse point for this daily flow and ebb as may be light rail if the downtown alignment is located along 108th. Throughout the commerce district art might use materials that acknowledge business’ vision, creativity and innovation.

## CIRCUITS

The Commerce Circuit should include art integrated into sidewalks and building facades. Art in the sidewalks might reflect the pulsing streams of pedestrians and the coinciding streams of information and transportation that flow through the commerce district.

## NODES

Commerce Nodes, such as that located at the intersection of 108th Avenue NE and NE 2nd Street, might include sculpture that explores business devices and themes.

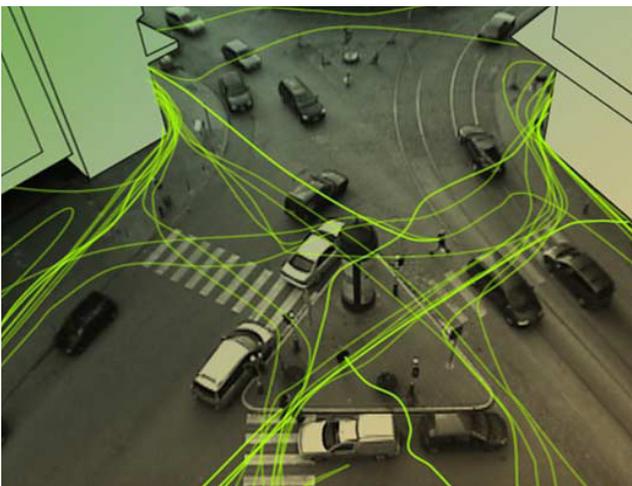
## INTERFACES

The Art Interface at the intersection of 108th Avenue NE and NE 6th Street, focusing on the west side of 108th, is at the high point of downtown Bellevue. This Interface transitions



5 ART PLAN

between Circuits of Commerce, Water, and Landscape, and is an important point along the Art Walk. Art here can act as a conceptual “headwaters” that describes the flow of water and people west down NE 6th, toward the waterfront. The art could consist of either real or virtual water. This location might also include an artist-designed “outdoor conference/ garden room” that ties together themes of commerce and landscape, again reinforcing the notion of Bellevue as the “City in a Park.”

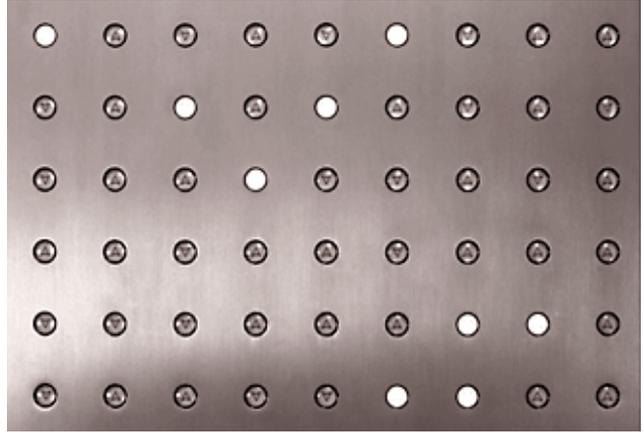


Art that reflects streams of information and workers. © 2005 Thomas Laureyssens, Pedestrian Levitation

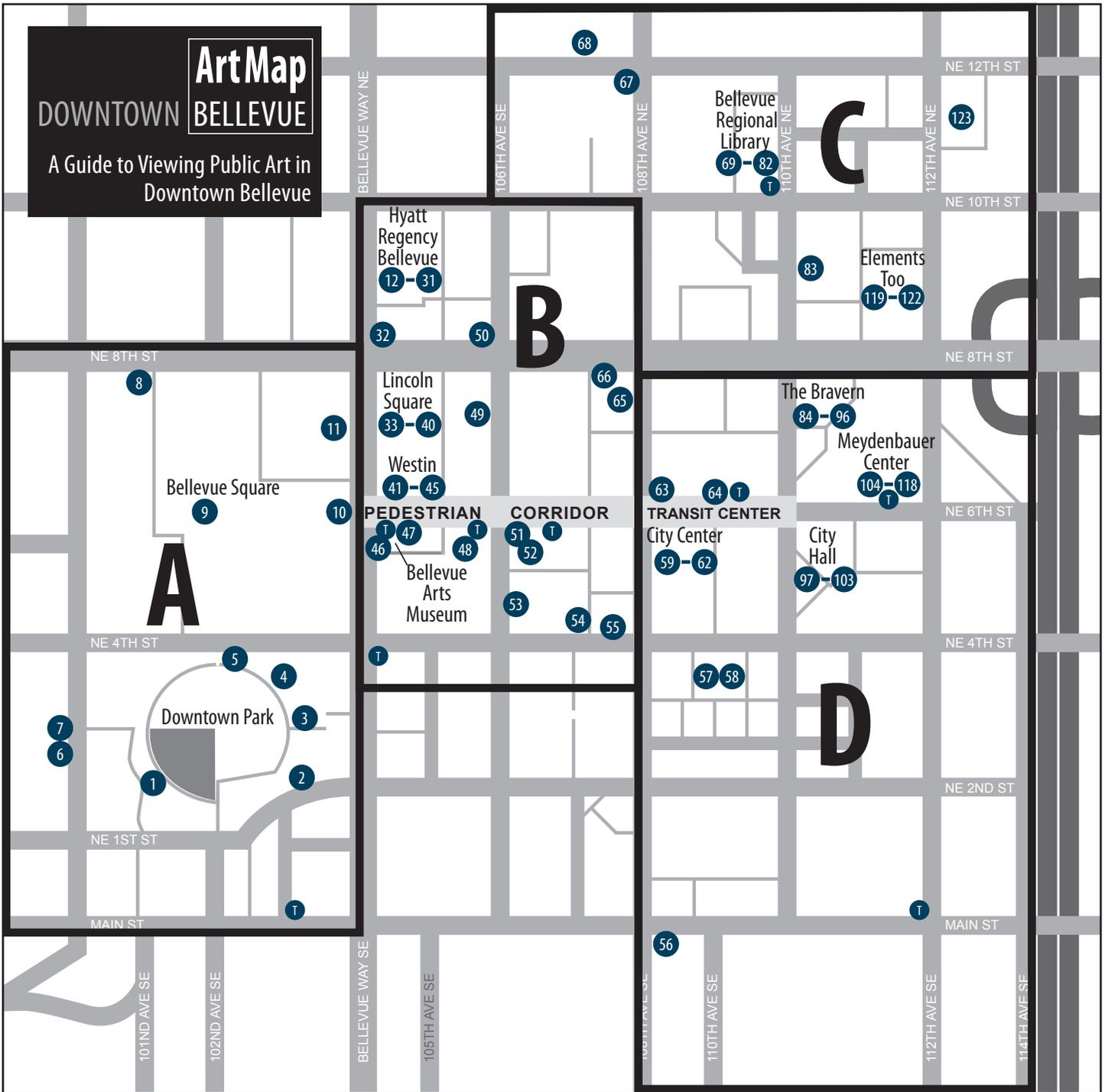


Art as outdoor conference room. © 1995 Richard Turner, Untitled (Metrorail Aviation Boulevard Station, Los Angeles)

# | CIRCUITS: COMMERCE |



Art about the international languages, currencies, technologies, and devices of business. Clockwise from top left: © 2001 Brad Miller, 50,000 Pennies; © 2001 Brad Miller, Elevator Life; © 2004 Jenny Holzer, Departures Board; © 1997 Janet Zweig, Your Voices; © 2004 Jaume Plensa, Crown Fountain; © 2004 Ann Hamilton, Floor of Babble.



**Key**

- **Artwork**
- **Topos**
  - Maria Cristalli, Marc Brown
  - Steel, paint
  - Artwork and wayfinding kiosks
  - Nine locations
  - Owned by City of Bellevue
- **Pedestrian Cut Through**
- **Road**

**About the Bellevue Arts Commission & Arts Program**

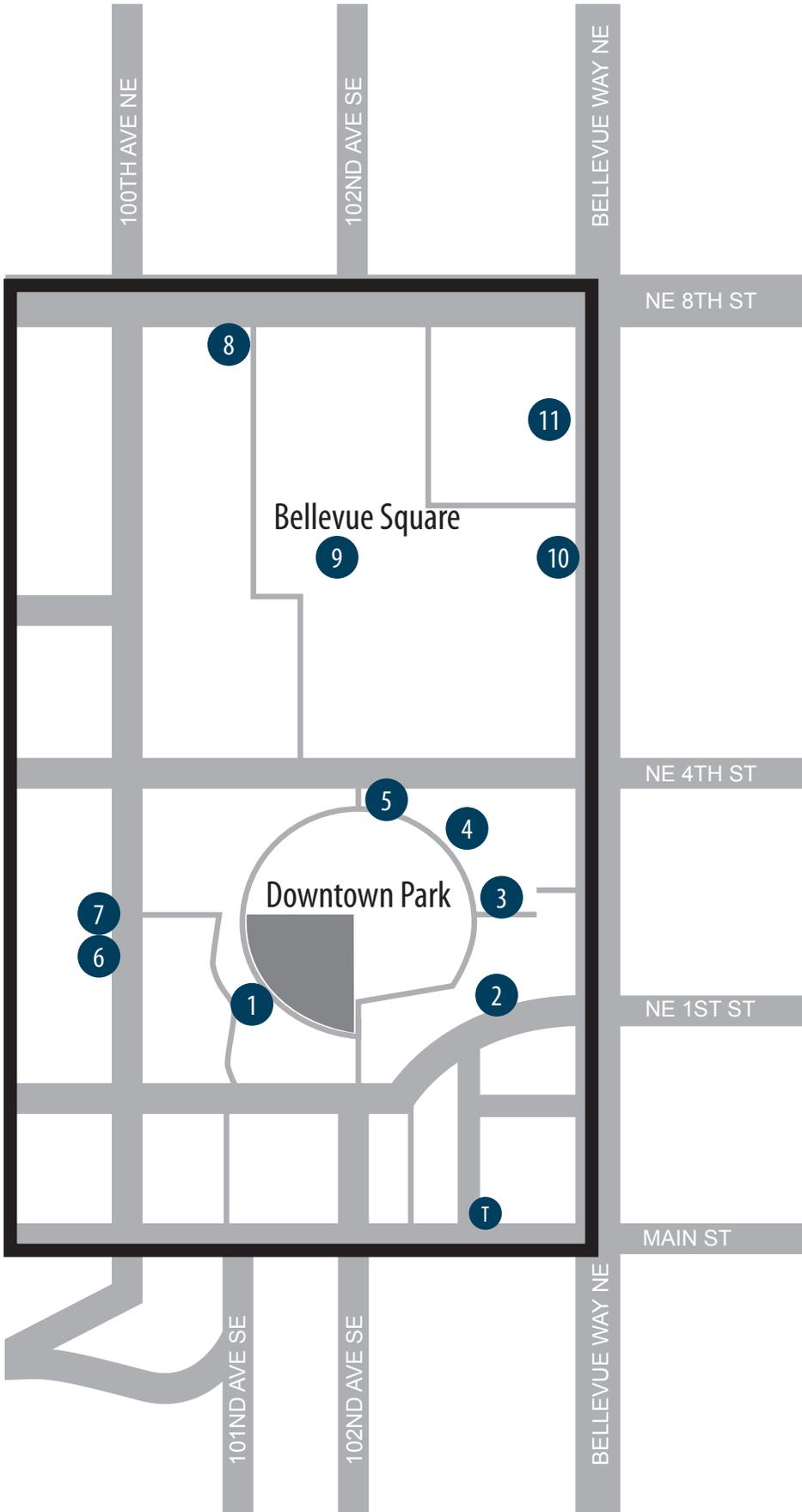
The Bellevue Arts Commission advises City Council on how the City should invest in the arts as well as on arts issues in the community. It guides implementing the Cultural Compass, Bellevue's plan for developing arts and culture in our community. The Arts Commission also advises the City's Arts Program. Seven Bellevue residents appointed by City Council volunteer as Arts Commissioners. Bellevue's Arts Program administers city programs funding artists and arts organizations serving Bellevue, public art, cultural policy development and implementation, and other cultural development activities.

City of Bellevue Arts Program  
 450 110th Ave NE  
 Bellevue, WA 98004  
 425-452-4105 or email [mpbyrne@bellevuewa.gov](mailto:mpbyrne@bellevuewa.gov)  
[www.bellevuewa.gov/arts\\_program.htm](http://www.bellevuewa.gov/arts_program.htm)  
[www.facebook.com/BellevueArtsProgram](https://www.facebook.com/BellevueArtsProgram)

*Note: If you know of, or find artwork that is not on this map, please let us know!*



## ARTWORK

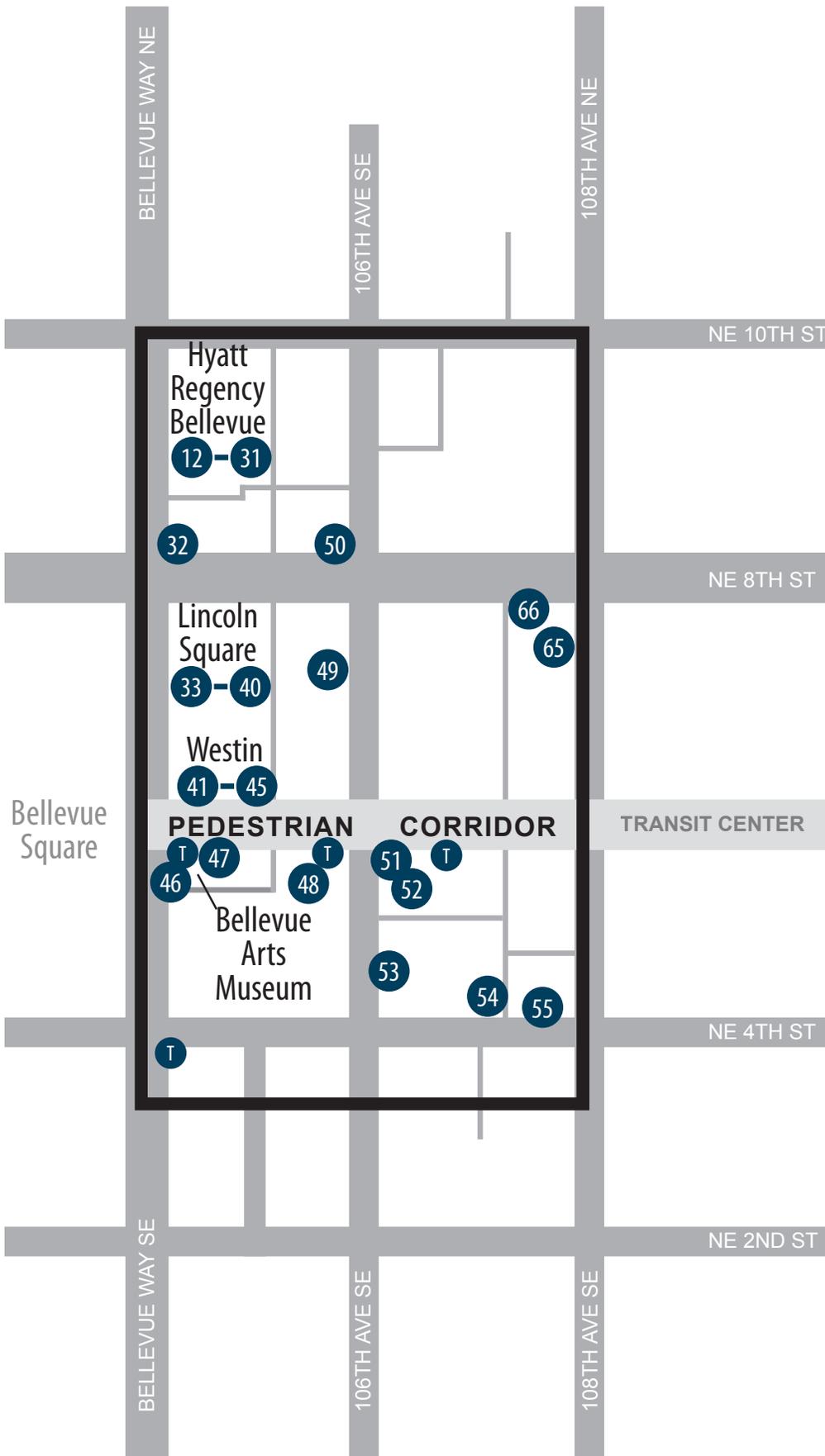


### Downtown Park 1-5

- 1 **Mountain Song**  
Gunnar Anderson  
Stainless steel, CorTen steel  
Southwest area of Downtown Park  
Owned by City of Bellevue
- 2 **Water, Precious Water**  
Verena Schwippert  
Granite  
Southeast area of Downtown Park  
Owned by City of Bellevue
- 3 **Giga Now**  
Verena Schwippert  
Granite  
Northeast area of Downtown Park  
Owned by City of Bellevue
- 4 **Arc with Four Forms**  
George Baker  
Stainless steel, CorTen steel  
Northeast area of Downtown Park  
Owned by City of Bellevue
- 5 **Three Graces**  
Michael Dennis  
Granite  
North area of Downtown Park  
Owned by City of Bellevue
- 6 **TV Stonehenge**  
Teens (Ground Zero Teen Center)  
Ruth Tomilinson, Deborah Lawrence (Teaching Artists)  
Concrete, ceramic tile  
257-100th Ave. NE  
Privately owned
- 7 **Dream Pole**  
Teens (Ground Zero Teen Center)  
Romson Bustillo (Teaching Artist)  
Wood, recycled material  
257-100th Ave. NE  
Privately owned

### Bellevue Square 8-11

- 8 **Forest Deity**  
Dudley Carter  
Red cedar  
NE 8th St. and 100th Ave. NE  
Privately owned
- 9 **Ambrosia**  
Gerard Tsutakawa  
Bronze fountain  
Macy's Home Store, lower level  
Privately owned
- 10 **Bellevue Portholes**  
Steve Jensen  
Reclaimed wood  
Bellevue Way NE at NE 6th St.  
Privately owned
- 11 **Full Circle**  
Anna Hanson  
Atlantic Cedar  
The Lodge, 523 Bellevue Way NE  
Privately owned



## ARTWORK

### Hyatt Regency Bellevue 12-31

- 12 **Goodnight Kiss**  
Geoff Garza  
Oil on panel  
Hyatt Regency Bellevue, Lobby  
Privately owned
- 13 **Unknown title**  
Tracy Taylor Grubbs  
Oil on canvas  
Hyatt Regency Bellevue, Lobby  
Privately owned
- 14 **Poplar, Bamboo, Tree Peony, Japanese Maple, Large Rose, Ginko, Eucalyptus, Fig**  
Stephen Yates  
Acrylic on Canvas  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 15 **Katagami – The Japanese Art of Making Paper**  
Unknown Artist  
Mulberry paper  
Hyatt Regency Bellevue  
Olympic Tower Lobby (4 works), 2nd floor (5 works)  
Privately owned
- 16 **Wiffen 6**  
Allan Cox  
Acrylic on canvas  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 17 **Silver Creek Sunset B**  
Valerie Stuart  
Plaster, oil glazes on canvas  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 18 **Vine Round**  
Artist Unknown  
Wood collage  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 19 **Silver Creek Sunset A**  
Valerie Stuart  
Plaster, oil glazes on canvas  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 20 **Collection of Vine Roots**  
Chista  
Wood  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 21 **Primal Squares**  
Amber Brookman  
Paint on wood  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 22 **Caramel Glaze**  
John Douglas  
Oil on canvas  
Hyatt Regency, Olympic Tower Lobby  
Privately owned

- 23 **Coleman Dock Totems**  
Steve Jensen  
Reclaimed wood  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 24 **Wildwood A**  
Allan Cox  
Oil, wax on linen  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 25 **Wildwood B**  
Allan Cox  
Oil, wax on linen  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 26 **Wings of Apollonius**  
Michael Schultheis  
Oil on canvas  
Hyatt Regency Bellevue, Olympic Tower Lobby  
Privately owned
- 27 **Airwave**  
Dennis Carney  
Mixed media on canvas  
Hyatt Regency Bellevue, Olympic Tower, 2nd fl.  
Privately owned
- 28 **Solar Winds, Warm Waters, Autumn**  
Rebecca Koury  
Acrylic on canvas  
Hyatt Regency Bellevue, Olympic Tower, 2nd fl.  
Privately owned
- 29 **Factory Beam**  
Steve Jensen  
Reclaimed wood  
Hyatt Regency Bellevue, Olympic Tower, 2nd fl.  
Privately owned
- 30 **Gathering Forces**  
Renee Dinauer  
Ash wood ribbons  
Hyatt Regency Bellevue, Olympic Tower, 2nd fl.  
Privately owned
- 31 **Amtul, Arnica, Axon, Cadmia, Colothar, Range** (6 works)  
Michael Kessler  
Acrylic on canvas  
Hyatt Regency Bellevue, Olympic Tower, 2nd fl.  
Privately owned
- 32 **Endless Celebration**  
Gesso Cocteau  
Bronze  
Bellevue Way NE and NE 8th St.  
Privately owned

## Lincoln Square 33-40

- 33 **FIORI IN THE FOUNTAIN**  
Dale Chihuly  
Blown glass  
Lincoln Square, Atrium  
Privately owned
- 34 **Drawing Wall** (18 works)  
Dale Chihuly  
Acrylic on paper  
Lincoln Square, Atrium, 1st floor  
Privately owned

- 35 **Makena**  
Stephen Yates  
Acrylic on canvas  
Lincoln Square, Atrium by Microsoft, 1st floor  
Privately owned
- 36 **Warm Springs**  
Stephen Yates  
Acrylic on canvas  
Lincoln Square, Atrium by Microsoft, 1st floor  
Privately owned
- 37 **Kona**  
Stephen Yates  
Acrylic on canvas  
Lincoln Square, Atrium by Microsoft, 1st floor  
Privately owned
- 38 **Sun, Moon, Stars**  
Gerard Tsukawa  
Bronze  
Lincoln Square, Atrium by Microsoft, 1st floor  
Privately owned
- 39 **Aqua Verde**  
Pat Fridell  
Oil on canvas  
Lincoln Square, Atrium by Microsoft, 1st floor  
Privately owned
- 40 **END OF DAY CHANDELIER**  
Dale Chihuly  
Blown glass  
Lincoln Square, Atrium, 2nd floor  
Privately owned

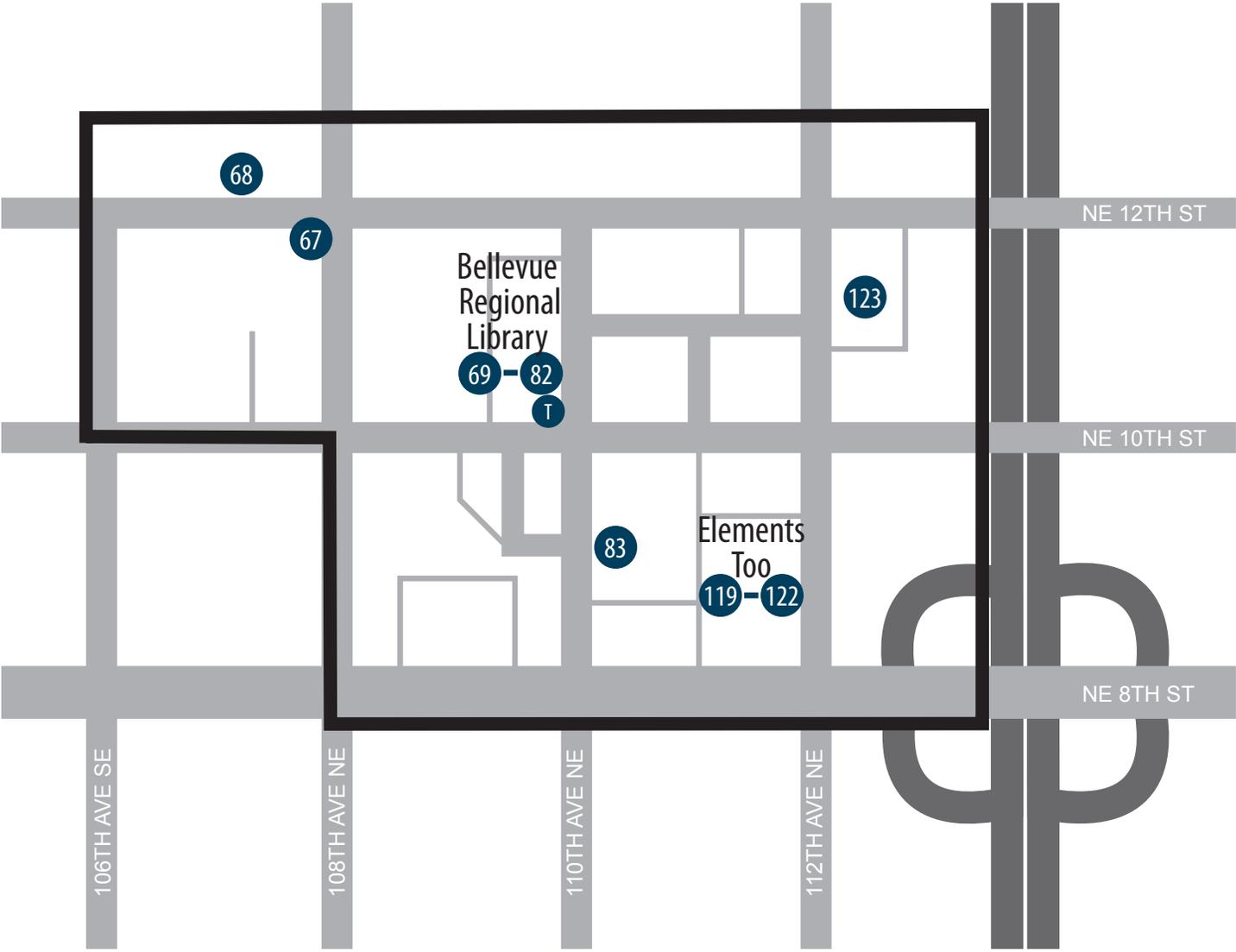
## Westin 41-45

- 41 **Quiet Morning**  
Jared Rue  
Oil on canvas  
Westin, 10476 NE 6th St., Lobby  
Privately owned
- 42 **Dreams of Bosphorous**  
Betsy Eby  
Encaustic  
Westin, 10476 NE 6th St., Lobby  
Privately owned
- 43 **Wall o Balls**  
Larry Halvorsen  
Ceramic  
Westin, 10476 NE 6th St., Lobby  
Privately owned
- 44 **Moonlit Path**  
William Robinson  
Basalt  
Westin, 10476 NE 6th St., Lobby  
Privately owned
- 45 **Earth**  
Ann Gardner  
Glass Mosaic  
Westin, 10476 NE 6th St., Lobby  
Privately owned

## Bellevue Arts Museum 46-47

- 46 **Miach**  
Julie Speidel  
Sandstone, bronze  
Bellevue Way NE near NE 6th Street  
Owned by Bellevue Arts Museum
- 47 **Chair Stack**  
Peter Pierobon  
Bronze  
Bellevue Arts Museum, indoors, 1st floor free entry  
Owned by Bellevue Arts Museum
- 48 **Untitled (fountain)**  
John Geise  
Stone, bronze  
California Pizza Kitchen, 10525 NE 6th St.  
Privately owned
- 49 **Winter Hawk**  
John Richen  
Stainless steel  
Paccar Building, 777 – 106th Ave. NE  
Privately owned
- 50 **Fountain**  
George Tsutakawa  
Bronze  
10550 NE 8th St.  
Privately owned
- 51 **Bellgate**  
Lee Kelley  
Stainless steel, enamel  
106th Ave. NE at NE 6th St.  
Owned by City/Bellevue Allied Arts Council
- 52 **The City within the City**  
Nancy Hammer, Hewitt Isley  
Stone, water  
106th Ave. NE at NE 6th St.  
Owned by City of Bellevue
- 53 **Mam-kiya**  
Julie Spiedel  
Painted steel  
106th Ave. NE near NE 4th St.  
Privately owned
- 54 **Russalka**  
Julie Spiedel  
Painted steel  
NE 4th St. between 106th and 108th Ave. NE  
Privately owned
- 55 **Porta Colona**  
Walter Dusenbery  
Stone  
One Bellevue Center, NE 4th St, near steps  
Privately owned
- 65 **Vestibulo Tuscano**  
Walter Dusenbery  
Marble  
Symetra Building, 777 – 108th Ave NE, Lobby  
Privately owned
- 66 **Bellevue's Early Years**  
Heidi Soehren  
Stone etchings, 7 panels  
Symetra Building, 777 – 108th Ave. NE, exterior  
Privately owned

# MAP C



## ARTWORK

- 67 **Rock Totem**  
James Fitzgerald  
Bronze  
Bellettini, 108th Ave. NE and NE 12th St.  
Owned by City of Bellevue
- 68 **Garden of Alternatives**  
Brian Goldbloom  
Granite  
McCormick Park, NE 12th St. near 108th Ave. NE  
Owned by City of Bellevue
- 69 **Howell Memorial Garden**  
Artist Unknown  
Bellevue Regional Library, north entry  
Owned by King County Library System
- 70 **Check It Out**  
Paul Marioni, Ann Troutner  
Cast glass  
Bellevue Regional Library, windows (north entry)  
Owned by King County Library System
- 71 **Historical Portraits**  
Garth Edwards  
Porcelain enamel panels  
Bellevue Regional Library, Gallery  
Owned by King County Library System
- 72 **Reynard the Fox**  
Richard S. Beyer  
Aluminum  
Bellevue Regional Library, south entry  
Owned by King County Library System
- 73 **One Generation to Another AKA Father and Child**  
Ray Jensen  
Bronze  
Bellevue Regional Library, Bookstore  
Owned by King County Library System
- 74 **Untitled**  
Anne Storrs  
Ceramic tile  
Bellevue Regional Library, Restrooms  
Owned by King County Library System
- 75 **Amber Pod**  
Ann Robinson  
Kiln cast crystal  
Bellevue Regional Library, 1st floor  
Owned by King County Library System

### Bellevue Regional Library 69-82

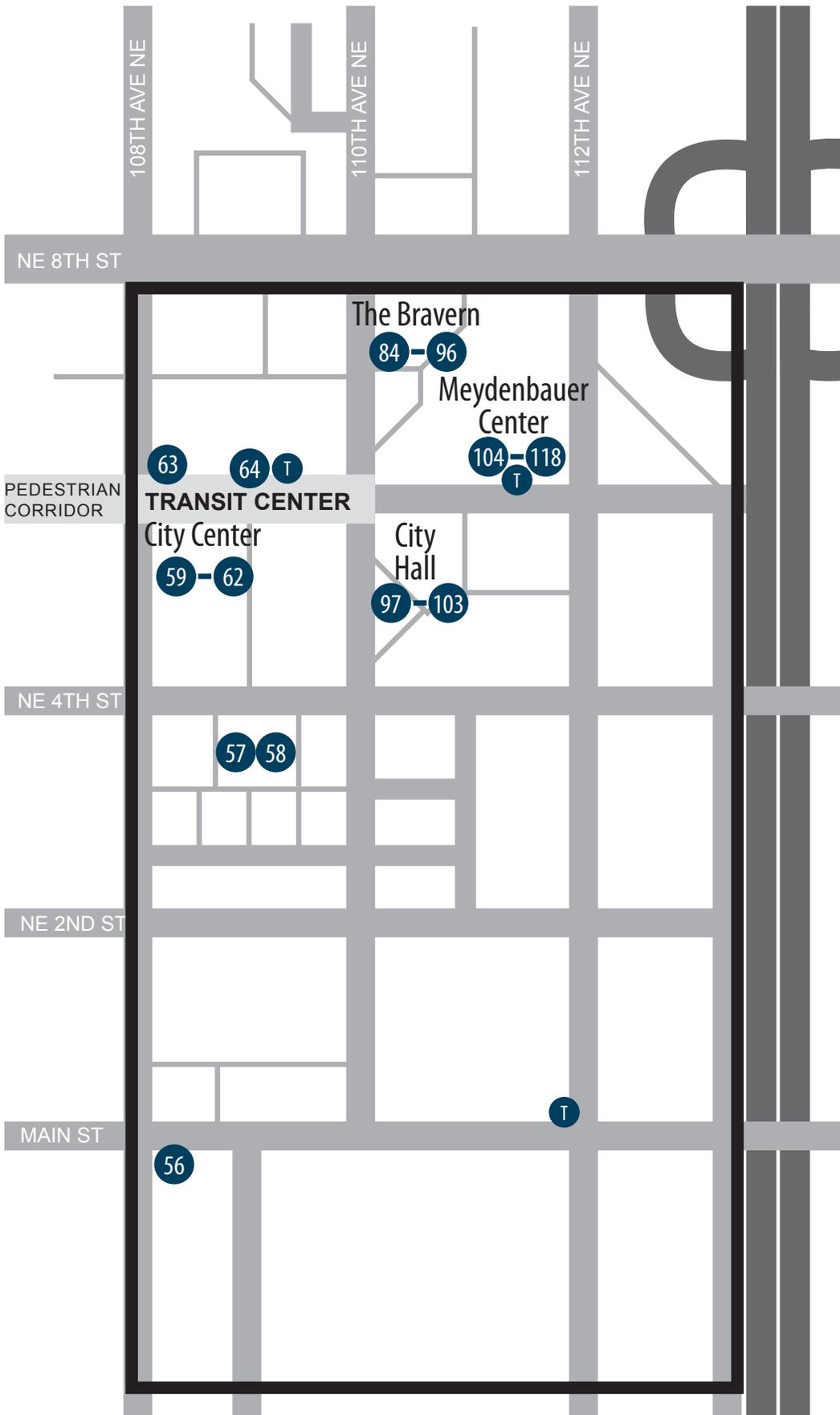
- 76 ***Turtle Island***  
 Mark Stevenson, Susan Vierth  
 Bronze  
 Bellevue Region Library, 1st floor  
 Owned by City of Bellevue
- 77 ***Golden Boy***  
 Leo Osborne  
 Bronze  
 Bellevue Regional Library, stairway  
 Owned by King County Library System
- 78 ***Mother Nature***  
 Reginald Brooks  
 Marble  
 Bellevue Regional Library, 2nd floor  
 Owned by City of Bellevue
- 79 ***Untitled***  
 Ellen Ziegler  
 Clay, several small works  
 Bellevue Regional Library, 2nd fl. windows  
 Privately-Owned
- 80 ***Wisdom with Book of Knowledge***  
 Sepp Mayrhuber  
 Paint, lime, marble dust, wax  
 Bellevue Regional Library, 3rd floor  
 Owned by King County Library System
- 81 ***Double Inquiry***  
 Larry Kirkland  
 Bronze, dichroic glass, engraved granite paving  
 Ashwood Plaza, NE 10th St. & 110th Ave. NE  
 Owned by City of Bellevue
- 82 ***Gandhi***  
 Anil R. Sutar  
 Bronze  
 Gift from the Government of India  
 Ashwood Plaza, NE 10th St. & 110th Ave. NE  
 Owned by City of Bellevue
- 83 ***Frank the Greeting Man***  
 Richard S. Beyer  
 Aluminum  
 Tewl Salon, 938 110th Ave. NE  
 Privately owned

- 122 ***Fire***  
 Stephen Yates  
 Acrylic on canvas  
 Elements Too, 958 – 111th Ave. NE, Atrium  
 Privately owned
- 123 ***The Gathering***  
 Julie Speidel  
 Metal  
 CH2MHILL Headquarters  
 Privately owned

## Elements Too 119-122

- 119 ***3 TIERED YELLOW CHANDELIER***  
 Dale Chihuly  
 Blown Glass  
 Elements Too, 958 – 111th Ave. NE, Atrium  
 Privately owned
- 120 ***Untitled***  
 Steve Jensen  
 Plywood  
 Elements Too, 958 – 111th Ave. NE,  
 Atrium, lower level  
 Privately owned
- 121 ***Kayak Dreams***  
 Sheryl Westergreen  
 Oil on Canvas  
 Elements Too, 958 – 111th Ave. NE,  
 Atrium, lower level  
 Privately owned

## ARTWORK



- 56 **Watercarrier**  
Alan Houser  
Bronze  
Baylis Architects, 10811 Main St.  
Privately owned
- 57 **Chumon**  
Yoshikawa  
Stone  
Summit Building, NE 4th and 108th plaza  
Privately owned
- 58 **Stainless Steel #1**  
Bruce West  
Stainless steel  
Summit Building, NE 4th and 108th plaza  
Privately owned
- City Center 59-62
- 59 **Magnetic Moment**  
Virginia Weisel  
Metal  
City Center, Lobby  
Privately owned
- 60 **Valdes II**  
Steve Jensen  
Painting  
City Center, Lobby  
Privately owned
- 61 **Mirrored Plane**  
Lloyd Blakely  
Oil on canvas  
City Center, Lobby  
Privately owned
- 62 **Salmon Fountains**  
John Geise  
2 Water features, now planted  
City Center, north side and south side  
Privately owned
- 63 **Windswept**  
Barbara Grygutis  
Light, granite, aluminum  
Bellevue Transit Center at 108th Ave NE  
Owned by Sound Transit
- 64 **The High Road**  
Stuart Keeler, Michael Machnic  
Bronze, steel, pavement treatment  
Bellevue Transit Center, mid block  
Owned by Sound Transit
- The Bravern 84-96
- 84 **Quantum Man**  
Julian Voss-Andraea  
Stainless steel  
The Bravern, Plaza on NE 8th St., 1st floor  
Privately owned
- 85 **Omnimodus**  
Tyler Aiello  
Stainless steel  
The Bravern, Plaza on NE 8th St., 1st floor  
Privately owned
- 86 **In Transition 2**

Bob Kimball  
Stainless steel, slumped glass  
The Bravern, escalators near Building 2  
Privately owned

87 **Salmon**  
Bob Kimball  
Stainless steel, blown glass  
The Bravern, escalators near Building 2  
Privately owned

88 **Tipsoo 3**  
Fred Holcomb  
Oil on canvas  
The Bravern, Building 2, Lobby  
Privately owned

89 **Light Maker-Omega Series**  
Brian Berman  
Onyx, granite  
The Bravern, Building 2, 2nd fl.  
Privately owned

90 **Deai Series**  
Etsuko Ichikawa  
Glass pyrographs  
The Bravern, Building 2, 2nd fl.  
Privately owned

91 **Color Therapy**  
David Lawson  
Blown glass  
The Bravern, Building 2, 2nd fl.  
Privately owned

92 **Family Tree Gathering**  
Stephen Kishel  
Stainless steel  
The Bravern, West Plaza on 110th Ave. NE  
Privately owned

93 **Preening Duck**  
Georgia Gerber  
Bronze  
The Bravern, West Plaza on 110th Ave. NE  
Privately owned

94 **Steerage**  
Ann Morris  
Bronze  
The Bravern, Building 1, Lobby  
Privately owned

95 **Adrift**  
Ann Morris  
Bronze  
The Bravern, Building 1, Lobby  
Privately owned

96 **Old Bracken Boat**  
Ann Morris  
Bronze  
The Bravern, Building 1, Lobby  
Privately owned

## City Hall 97-103

97 **The Root**  
Dan Corson

Bronze, nickel coating  
City Hall Plaza  
Owned by City of Bellevue

98 **Guan Yin**  
Gift from Sister City, Haulien, Taiwan  
Marble  
City Hall Plaza  
Owned by City of Bellevue

99 **Fu Dog**  
Gift from Sister City, Haulien, Taiwan  
Marble  
City Hall Plaza  
Owned by City of Bellevue

100 **Longboat.Reed.Rookery**  
Dan Corson  
Steel, metal clad & fiber optic cable, copper, aluminum & telecommunications wiring, copper & ABS tubing, zipties  
City Hall, exterior, east of 1st floor entrance  
Owned by City of Bellevue

101 **Current**  
Linda Beaumont  
Terrazzo floor, glass cane, steel, stone, beads, abalone  
City Hall, 1st fl.  
Owned by City of Bellevue

102 **Compass**  
Alan Storey  
Stainless steel, mirror  
City Hall, south exterior  
Owned by City of Bellevue

103 **Northwall**  
Lois Graham  
Oil on canvas  
City Hall, 2nd floor back Lobby  
Owned by City of Bellevue

## Meydenbauer Center 104-118

104 **Untitled (Lobby Treatment)**  
Ed Carpenter  
Plate and dichroic glass, aluminum, stainless steel, computerized lighting  
Meydenbauer Center, main entry, 2nd fl.  
Owned by Bellevue Convention Center Authority/City of Bellevue

105 **Apsara**  
Julie Speidel  
Bronze  
Meydenbauer Center, 2nd fl. lobby, exterior  
Owned by City of Bellevue

106 **Twine**  
Margaret Ford  
Clay, wood  
Meydenbauer Center, main entry, 2nd fl.  
Owned by City of Bellevue

107 **Presenting A Most Astonishing Feat**  
Judy Onofrio  
Mixed media  
Meydenbauer Center, Theater lobby  
Owned by City of Bellevue

108 **Pentagons: Concave, Convex**

Robert Maki  
Graphite on paper  
Meydenbauer Center, 2nd fl. lobby  
Owned by 4 Culture

109 **Mercurial Miss**  
Patti Warashina  
Ceramic  
Meydenbauer Center, south stair landing  
Owned by 4 Culture

110 **Wedge**  
Margaret Tomkins  
Oil on canvas  
Meydenbauer Center, south stair landing  
Owned by Tacoma Arts Museum

111 **Stream Cleft**  
Kathleen Gemberling Adkinson  
Oil on canvas  
Meydenbauer Center, 2nd fl. balcony  
Owned by Tacoma Arts Museum

112 **Legacy Archway**  
Robert Harrison  
Ceramic, metal, concrete, found objects  
Meydenbauer Center, between 1st and 2nd fl. balcony  
Owned by City of Bellevue

113 **Untitled**  
William Ivey  
Oil on canvas  
Meydenbauer Center  
North stairway between 1st and 2nd fl.  
Owned by Tacoma Art Museum

114 **Light as a Feather**  
Delos Webber  
Basalt, bamboo, rattan  
Meydenbauer Center  
South stairway between 3rd and 4th fl.  
Owned by City of Bellevue

115 **Improved Geometry No. 2**  
Patrick Humble  
Acrylic on canvas  
Meydenbauer Center, 4th fl.  
Owned by Tacoma Arts Museum

116 **Immanent Blue**  
William Ingham  
Oil on canvas  
Meydenbauer Center, 4th fl.  
Owned by Tacoma Art Museum

117 **Upprowth**  
Louis Bunce  
Oil on linen  
Meydenbauer Center, 4th fl.  
Owned by Tacoma Art Museum

118 **Celebration**  
Richard Gilkey  
Oil on canvas  
Meydenbauer Center, 4th fl. north hallway  
Owned by Tacoma Art Museum