



DEVELOPMENT SERVICES DEPARTMENT  
 ENVIRONMENTAL COORDINATOR  
 11511 MAIN ST., P.O. BOX 90012  
 BELLEVUE, WA 98009-9012

## DETERMINATION OF NON-SIGNIFICANCE

**PROPONENT:** Bellevue 116<sup>th</sup> North

**LOCATION OF PROPOSAL:** 316 116<sup>th</sup> Avenue NE

**DESCRIPTION OF PROPOSAL:**

Proposal to develop a 3.15 acre site with an 82,000 square foot retail building and associated two-level parking structure for 309 vehicles.

**FILE NUMBERS: 14-143953-LD and 14-127270-LO**

The Environmental Coordinator of the City of Bellevue has determined that this proposal does not have a probable significant adverse impact upon the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(C). This decision was made after the Bellevue Environmental Coordinator reviewed the completed environmental checklist and information filed with the Land Use Division of the Development Services Department. This information is available to the public on request.

- There is no comment period for this DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's office by 5:00 p.m. on \_\_\_\_\_.
- This DNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on the DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's Office by 5 p.m. on May 21, 2015.
- This DNS is issued under WAC 197-11-340(2) and is subject to a 14-day comment period from the date below. Comments must be submitted by 5 p.m. on \_\_\_\_\_. This DNS is also subject to appeal. A written appeal must be filed in the City Clerk's Office by 5 p.m. on \_\_\_\_\_.

This DNS may be withdrawn at any time if the proposal is modified so that it is likely to have significant adverse environmental impacts; if there is significant new information indicating, or on, a proposals probable significant adverse environmental impacts (unless a non-exempt license has been issued if the proposal is a private project); or if the DNS was procured by misrepresentation or lack of material disclosure.

Carol M. Holland  
 Environmental Coordinator

5/7/2015  
 Date

**OTHERS TO RECEIVE THIS DOCUMENT:**  
 State Department of Ecology,  
 Attorney General  
 Muckleshoot Indian Tribe



**City of Bellevue  
Development Services Department  
Land Use Division Staff Report**

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Proposal Name: Bellevue 116<sup>th</sup> North Parcel  
Proposal Address: 316 116<sup>th</sup> Avenue NE  
Proposal Description: Design Review and Critical Areas Land Use approval to develop a 3.15 acre site with an approximately 82,360 square foot retail building and associated two level parking structure for 309 vehicles.  
File Number: 14-143953-LD and 14-147270-LO  
Applicant: KG Investment Management  
Decisions Included: Design Review and SEPA Threshold Determination (Process II)  
Planner: Sally Nichols  
State Environmental Policy Act Threshold Determination: Determination of Non-Significance

Carol V. Helland  
Carol V. Helland, Environmental Coordinator  
Development Services Department

Director's Recommendation: **Approval with Conditions**  
Michael A. Brennan, Director  
Development Services Department

By: Carol V. Helland  
Carol V. Helland, Land Use Director

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Notice of Application: December 18, 2014  
Notice of Decision: May 7, 2015  
Appeal Deadline for Design Review & SEPA: May 21, 2015  
Expiration Date: May 7, 2017 (if no complete Building Permit application is filed prior to this date - Refer to LUC 20.40.500)

For information on how to appeal a proposal, visit the Development Services Center at City Hall or call (425) 452-6800. Comments on State Environmental Policy Act (SEPA) Determinations can be made with or without appealing the proposal within the noted comment period for a SEPA Determination. Appeal of the Decision must be received in the City Clerk's Office by 5 PM on the date noted for appeal of the decision.

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**City of Bellevue  
Development Services Department  
Land Use Division Staff Report**

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## CONTENTS

I.	Request/Proposal Description.....	Pg. 2
II.	Site Description, Zoning & Land Use Context.....	Pg. 13
III.	Consistency with Land Use Code/Zoning Requirements.....	Pg. 16
IV.	Design Guidelines and Design Standards.....	Pg. 25
V.	Public Notice & Comment.....	Pg. 26
VI.	Technical Review.....	Pg. 27
VII.	State Environmental Policy Act (SEPA).....	Pg. 32
VIII.	Changes as a Result of Design Review.....	Pg. 36
IX.	Decision Criteria.....	Pg. 37
X.	Decision.....	Pg. 40
XI.	Conditions of Approval.....	Pg. 40

### ATTACHED:

- A. Project Drawings
- B. SEPA Checklist
- C. Concurrency Certificate

## I. REQUEST/PROPOSAL DESCRIPTION

### A. Request

The applicant requests Design Review and Critical Areas Land Use Permit (CALUP) approval and a threshold determination under the State Environmental Policy Act (SEPA) to construct a two-story building on a 3.15-acre site in the CB (Community Business) land use district. In addition, the applicant is requesting approval of a Critical Areas Land Use Permit (CALUP) to modify the existing steep slope that runs along the eastern side of the property along the existing BNSF right-of-way.

The project consists of a two-story building that will include approximately 74,360 square feet of retail space for three retail stores with 8,000 square feet of associated mechanical and loading space. Parking will be accommodated in a two-story, open air parking structure with spaces for 152 cars on level 1 and an additional 157 cars on the upper parking deck – level 2 for a total of 309 spaces. The retail sales uses proposed for this building are allowed in the CB land use district per the use charts in Land Use Code (LUC) 20.10.440 – Wholesale and Retail Uses.



### B. Review Process

Design Review is governed by Land Use Code (LUC) 20.30F. The Design Review, CALUP, and SEPA Determination are all Process II decisions. Process II is an administrative process. The Environmental Coordinator issues the SEPA Threshold Determination and the Director of the Development Services Department issues the Design Review and CALUP decision. An appeal of any Process II decision is heard and decided upon by the City of Bellevue Hearing Examiner.

### C. Site Design

The development consists of two parcels, which together will be referred to in this report as the 'site.' The site lies at the northeast corner of the intersection between 116<sup>th</sup> Avenue NE and Phase 1 of the new NE 4<sup>th</sup> Street extension. This new street,

which includes the roadway, bike lanes, landscaping and public sidewalks, has recently been completed. The site itself sits roughly 30 feet below the former BNSF rail corridor to the east at the toe of a critical area steep slope. This corridor is envisioned to be part of a public trail system in the future.

### Site Plan



1. **Retail Building Orientation:** The building will sit at the northeast corner of the 116<sup>th</sup> Avenue NE/NE 4<sup>th</sup> Street intersection. Although building walls will face each street, there will be no public entries off either of these arterials. The doors to the retail spaces will face the parking structure on the eastern side of the building.
2. **Streetscapes:**
  - a. 116<sup>th</sup> Avenue NE: There will be a ten-foot wide public sidewalk along this street frontage. Between the building and the public sidewalk, the project will provide plantings and continuous weather protection via architectural marquees and a free-standing canopy. A generous seven-foot wide planting strip will include a mix of native and adapted low-growing shrubs and groundcover and large street trees. This planting strip will be located between the sidewalk and the curb to protect pedestrians from the vehicular traffic on 116<sup>th</sup> Avenue NE and to provide a pleasant pedestrian experience.

- b. NE 4<sup>th</sup> Street: The pedestrian sidewalk and landscaping (including street trees) adjacent to the curb are in place and were constructed as part of the NE 4<sup>th</sup> Street Extension transportation project. The roadway (and sidewalk) sit higher than the upper deck of the parking structure along the eastern half of the street frontage. There will be a wall on the north side of the sidewalk and due to the drop in grade, there will be no landscaping between the sidewalk and the parking structure wall at the street level.

Along the building edge and adjacent to the public sidewalk, the applicant has provided a generous flight of stairs, terraces and planters to provide interesting outdoor spaces with excellent solar orientation. These stairs will lead to the retail entries on the eastern side of the building and overhead marquees will provide continuous weather protection. While not an accessible space, a landscaped area between the vehicular access driveways will be a visual amenity and will be punctuated with ornamental planting and three large Parrotia specimen trees.



Weather-Protected Walkway from NE 4<sup>th</sup> Street to Retail Entries

Vehicular & Pedestrian Access to Upper Level of Parking Structure & Retail Entry

Landscape Feature with Specimen Trees

Vehicular Access to Lower Level of Parking Structure

**AERIAL VIEW OF LANDSCAPED TERRACE**

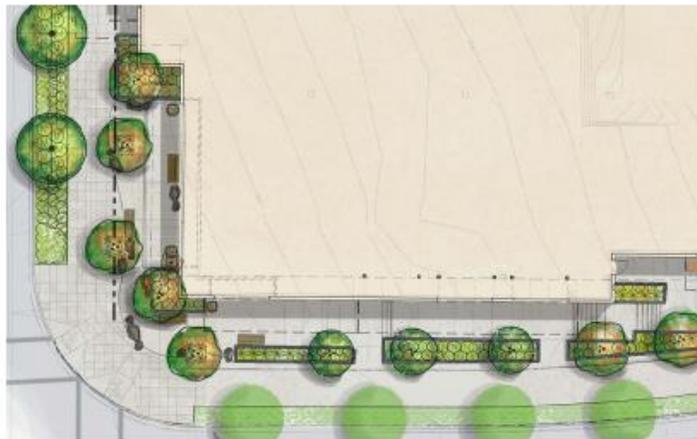


**LANDSCAPED TERRACE PLAN**

3. Southwest (Street) Corner: The southwest corner of the site, at the intersection of NE 4<sup>th</sup> Street and 116<sup>th</sup> Avenue NE, will be highlighted with a pedestrian plaza to invite pedestrians to rest and/or view the large store display window that faces west. The plaza will be enhanced with flowering trees, planted containers, benches and groupings of boulders.



**AERIAL VIEW OF SW CORNER PLAZA**



**SW CORNER PLAZA PLAN**

4. Pedestrian Access:  
NE 4<sup>th</sup> Street Extension:  
The new public sidewalk that runs along the north side of the NE 4<sup>th</sup> Street extension project will create an important pedestrian connection between 116<sup>th</sup> Avenue NE, this project, and activities to the east – including the BNSF corridor, the Best Buy and Home Depot retail center, 120<sup>th</sup> Avenue NE, and the greater Wilburton Community. There will also be a direct connection from this public sidewalk to the retail stores via a sidewalk that will lead directly to the lower level of the building. The retail store on the top floor can then be accessed via an elevator, escalator and/or stairs. Pedestrians may also directly access the upper level retail stores by walking across the upper level of the parking structure.

116<sup>th</sup> Avenue NE:

There will be no direct access for pedestrians into the building from 116<sup>th</sup> Avenue NE. The front doors to the three retail tenants will be on the eastern side of the

building, facing the parking structure. Pedestrians will be able to access these front doors from 116<sup>th</sup> Avenue NE in two ways:

- a) From the north, pedestrians will take a route along the northern side of the building. Where this route of travel traverses the loading dock, a walkway will be delineated in the pavement via a change in materials and color.
- b) From the south, pedestrians will walk along the north side of the NE 4<sup>th</sup> Street extension; either along the public sidewalk or via stairs and resting areas along the southern side of the building referred to as the 'stair climb.' There will also be weather protection in the form of marquees attached to the building.

### Pedestrian Access Points



### 5. Vehicular Access:

Driveway access to parking will be from both NE 4<sup>th</sup> Street, and 116<sup>th</sup> Avenue NE.

- a) Cars will enter the site from 116<sup>th</sup> Avenue NE via a driveway at the northwest corner of the site and continue east to the parking structure. This driveway will be shared with trucks accessing the loading dock located at the northwest corner of the building along the street. Cars exiting on 116<sup>th</sup> Avenue NE will have unrestricted turning movements.

### Vehicular Access off 116<sup>th</sup> Avenue NE



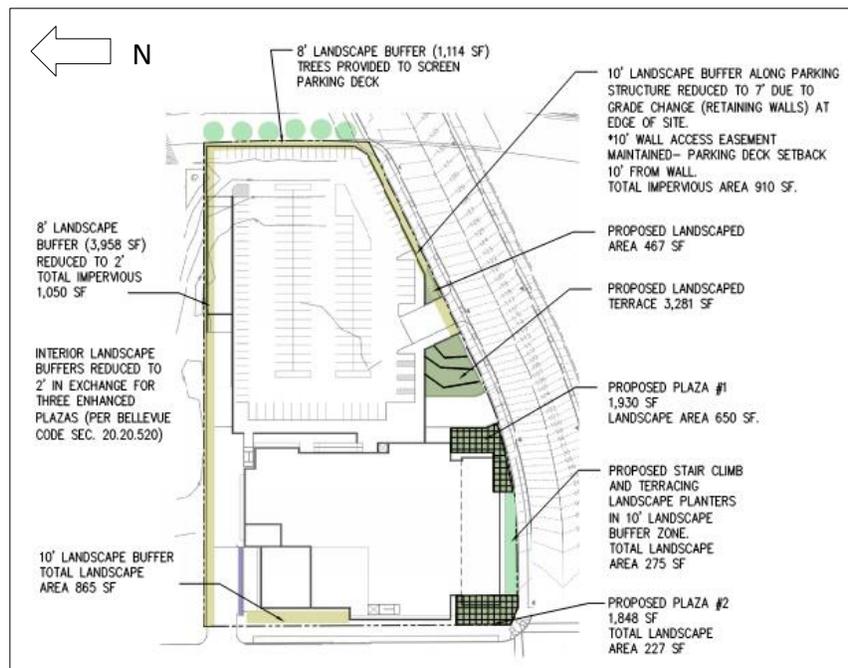
- b) Cars may enter the parking structure off NE 4<sup>th</sup> Street via two driveways; each connecting to a different level of the structure. These driveways will have right in-right out only turning movements. **Refer to Transportation discussion in Sections VI and VII of this report.**

**Vehicular Access Points on NE 4<sup>th</sup> Street**



6. Landscape:

The applicant will provide a variety of landscape treatments throughout the site and will increase the amount of pervious surface on the site. A description of these areas is below. However, many of the required buffers were not provided and the applicant has requested an Alternative Landscape Option (ALO) to reduce the buffers and provide landscaping elsewhere on the site. Refer to Section III.C of this report for how the project met the criteria.



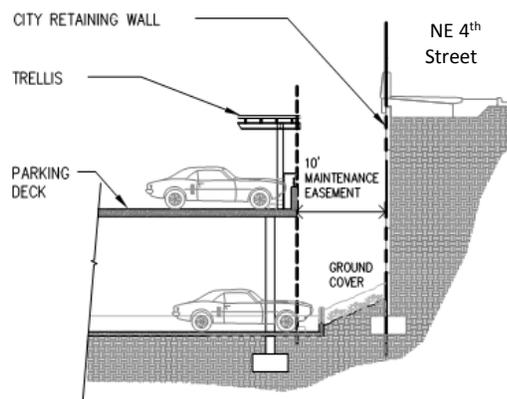
Conceptual Landscape Plan

#### 116<sup>th</sup> Avenue NE:

A seven foot wide planting will be placed between the ten foot wide sidewalk and the back of curb along 116<sup>th</sup> Avenue NE. The planting strip will include street trees as well as shrubs and groundcover plantings. This landscaping will effectively screen the pedestrian from vehicles on this arterial street. Additional landscaping will be placed between the sidewalk and the building wall.

#### NE 4<sup>th</sup> Street:

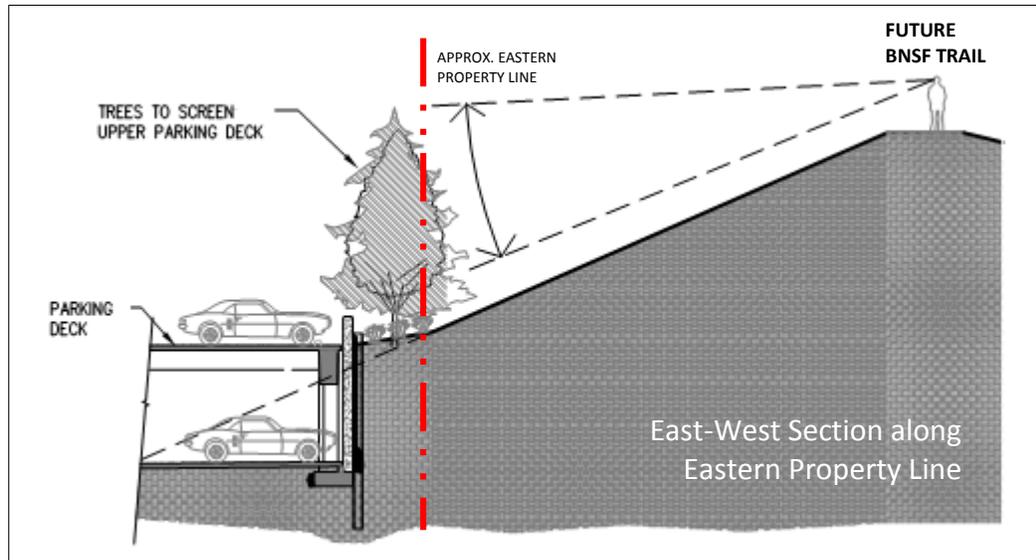
Along NE 4<sup>th</sup> Street extension, there will be a walkway along the building edge to lead pedestrians up from the corner via stairs and small seating areas. This stairway will be separated from the sidewalk at the street by a series of raised, terraced planter walls. They will be planted with a variety of shrubs and trees to create a buffer, accentuate the pedestrian movement to the store entries along the eastern side of the building, and enhance the gathering spaces.



The parking structure on the eastern half of the site will be set back from the retaining wall required for the NE 4<sup>th</sup> Street roadway, planting strip and sidewalk to maintain a ten-foot wide maintenance easement. There will be modest groundcover landscaping at the base of the wall along the edge of the lower level of the parking garage. However, this area will not serve to buffer the parking garage from the public street.

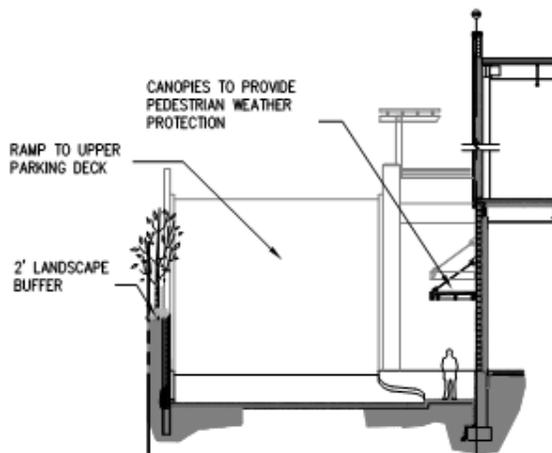
#### Eastern Property Line:

The parking garage will cut into the existing steep slope along the eastern perimeter of the property. By pulling the garage back slightly, the applicant was able to provide a landscaped buffer at the top of the retaining wall that will vary in width from five to a little over seven feet in width. As mitigation for disturbing the critical area slope, this area will be planted with 20-foot tall specimen native conifers, deciduous maple trees and shrubs and groundcover to provide a varied buffer that will begin to screen the upper deck of the parking structure from future BNSF trail users further up the hill to the east, help stabilize the slope, and provide vegetated habitat along with the vegetation on the BNSF property that occupies the rest of the slope.

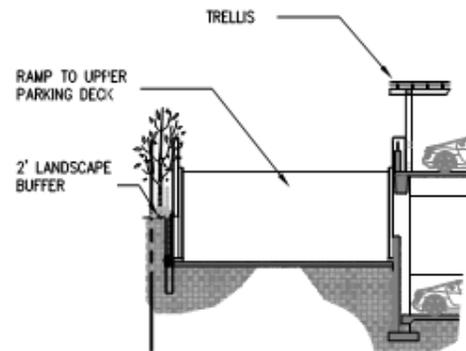


**North Property Line:**

Along the northern property line, the vehicular access road and parking structure have been placed close to the property line. In this location, the proposal will have a two-foot wide planting strip which will be densely planted with large bamboo. These tall shrubs will be approximately 5'-6" tall at time of installation and are fast-growing. Over time, these plants are expected to create a 15 to 20-foot tall screen. Trailing vines and groundcover will be added along the top of the shoring wall to trail down the retaining wall and soften the appearance of the grade change, as viewed from the south when entering the project site from 116<sup>th</sup>. In lieu of plantings, decorative metal panels will be also be placed on the north face of the parking garage to provide screening.



Section thru Drive Aisle at NW Corner of Building



Section thru North Edge of Parking Ramp

**Refer to the discussion regarding the Alternative Landscape Option in Section III.C of this report and Condition of Approval regarding the final landscape and irrigation plans in Section XI of this report.**

**D. Building Design**

**1. Building Design:**

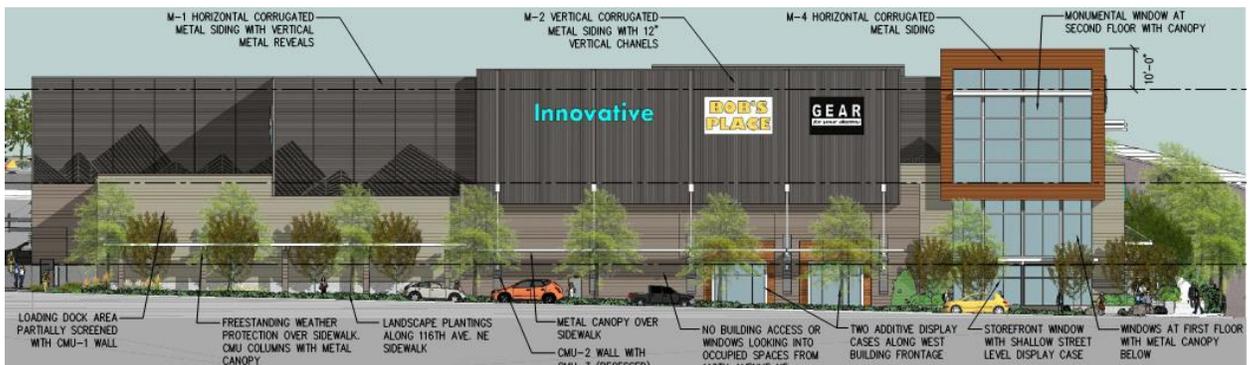
**Retail Building:** The two-story building is designed to have an interesting composition from all vantage points. It will be placed at the street intersection with the parking placed behind. Tenant entries (two on the lower level and one on the upper level) will be on the eastern façade, facing the parking structure.

The retail building will have horizontal and vertical modulation and design elements and materials are consistent on all sides of the retail building. Various corrugated metal patterns and colors wrap the upper story of the building. The lower base of the building is wrapped in various color and textures of concrete block and has large display windows along 116<sup>th</sup> Avenue NE. At the main intersection of 116<sup>th</sup> Avenue NE and NE 4<sup>th</sup> Street, the building will have a large glazed corner feature. A variety of canopy designs along 116<sup>th</sup> Avenue NE and along the southern building wall will add additional interest and weather protection.

**Building Elevations – Street Frontages**

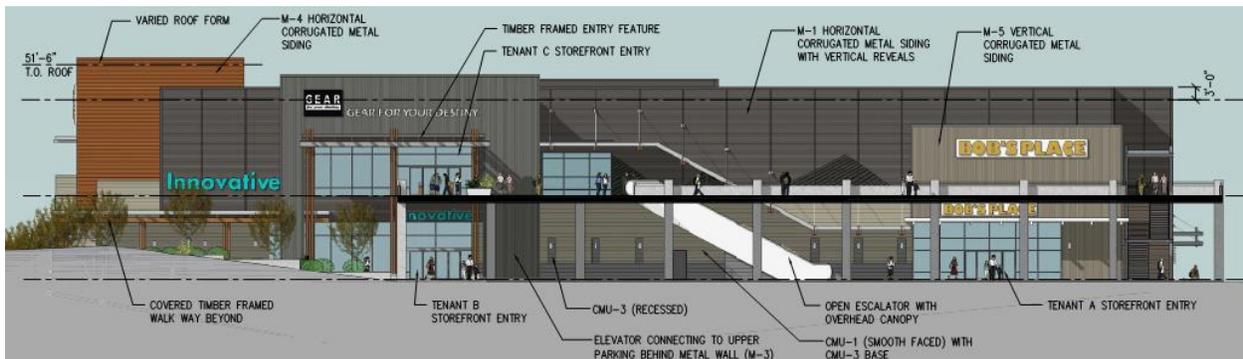


NE 4<sup>th</sup> Street (South)

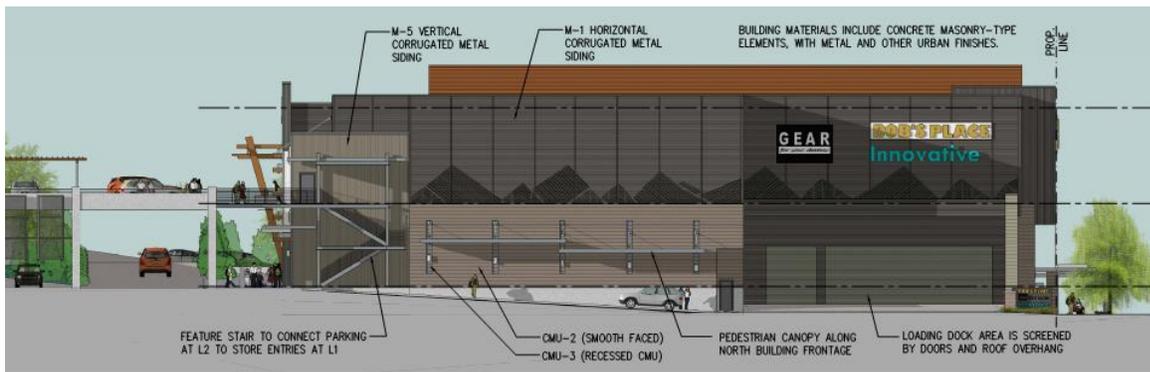


116<sup>th</sup> Avenue NE (West)

**Building Elevations - Interior**



Eastern Façade (facing parking structure)



Northern Façade (loading area)

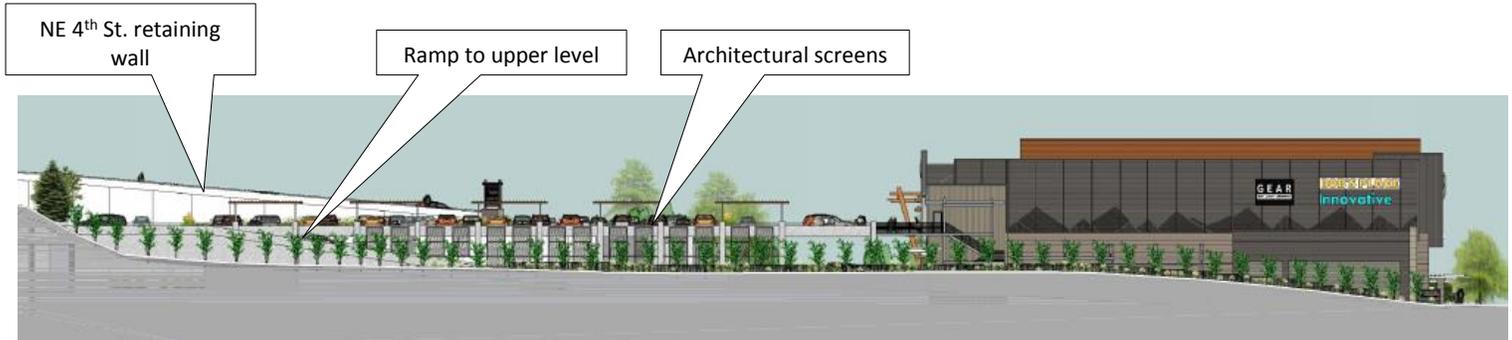


Loading Areas viewed from the north along 116<sup>th</sup> Avenue NE

**Parking Structure:**

The parking structure will be an open structure that is set into the sloped portion of the site along the eastern property line. It will also sit below the elevation of the new NE 4<sup>th</sup> Street roadway and will be adjacent to the large retaining wall required for this street. The structure will be a relatively simple open concrete structure. Along the northern property line, where there could be views into the structure

from the adjacent property, architectural metal screens will be placed. On the upper level of the structure, architectural trellises and planters will help separate parking rows into smaller parking fields and help to soften views of the cars from the future BNSF trail.



Elevation along northern property line



View from northeast corner of parking structure

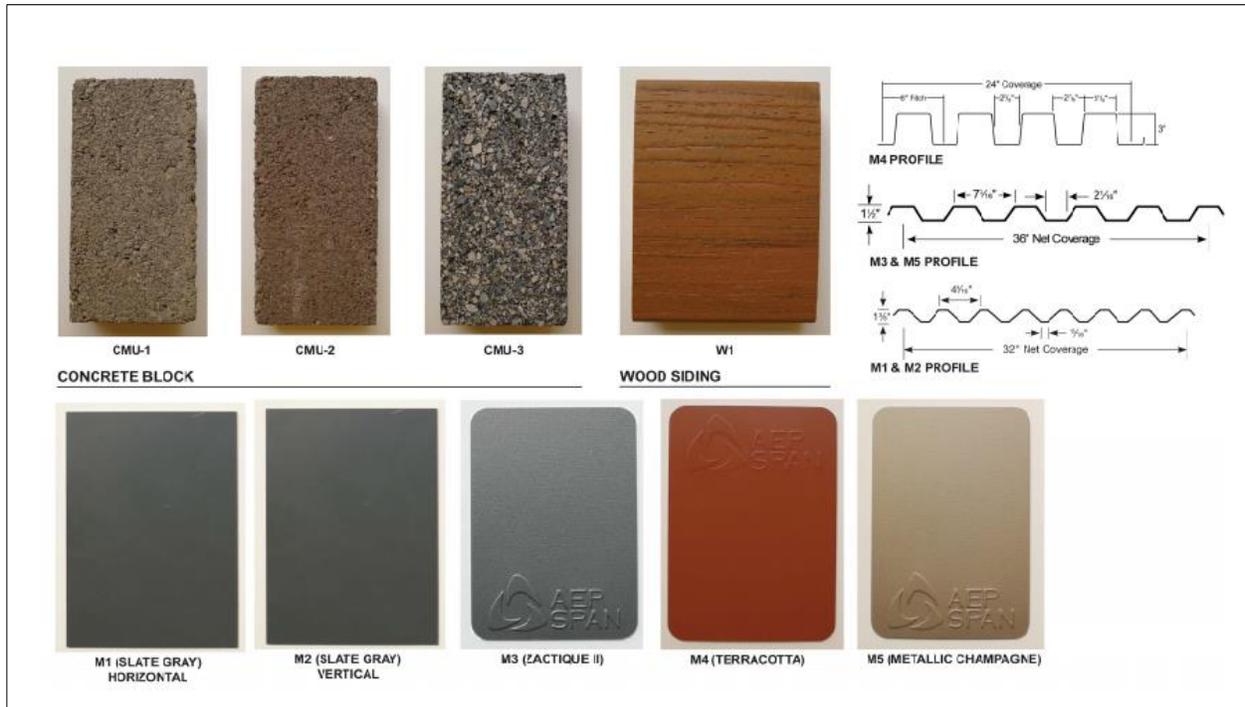
**Roof:**

The building will have a flat roof that will be punctuated with approximately 36 skylights to allow natural light into the upper level retail space. Spaced among these skylights will be mechanical units. The parapet around the edges of the building will, for the most part, hide these units – particularly from the street and surrounding properties. It was determined through design review that requiring screens around these units would defeat the functioning of the skylights and result in a more cluttered rooftop. However, all units must all be painted to match the roof. Review of the actual mechanical units/equipment under the building permit will determine if further action is required to adequately reduce the visual impacts of the mechanical equipment.

**Refer to Condition of Approval regarding rooftop mechanical equipment in Section XI of this report.**

**Color and Materials:** The materials and colors were chosen to be non-reflective and urban in character. Transparent glazing, particularly at the corner and on display windows along 116<sup>th</sup> Avenue NE will enhance inside/outside connectivity. The majority of the building walls will be a combination of horizontal and vertical corrugated metal panels and up to three different kinds of CMU block. The building will also have wood siding accents to provide softer-feeling materials near pedestrian routes.

## Color and Materials



### E. Signage

The applicant has indicated on the architectural drawings the locations and designs for building-mounted signage. Whereas the conceptual designs for this signage have been reviewed and approved under this Design Review, actual design review of the project signage and review for conformance with the Bellevue Sign Code will occur under individual sign permit applications. In addition, wayfinding signage will be necessary to highlight the connections between the proposal and the BNSF trail, the Downtown, transit, and adjacent neighborhoods. **Refer to Condition of Approval regarding signage and individual sign permits in Section XI of this report.**

## II. SITE DESCRIPTION, ZONING, & LAND USE CONTEXT

### A. Background

Comprehensive Plan Policy S-WI-3 expressed the vision of a 'retail village' in the Wilburton/NE8th Street subarea and that rezoning from GC (General Commercial) to CB (Community Business) to support this vision was appropriate. This Policy also stated that transportation improvements would need to take place that could support the anticipated retail village development before any rezone could be considered. The key transportation improvement germane to allowing any rezone to move forward was the NE 4<sup>th</sup> Street extension from 116<sup>th</sup> Avenue NE to 120<sup>th</sup> Avenue NE (CIP PW-R-160). Phase 1 of this project, the segment from 116<sup>th</sup> Avenue NE to the BNSF railway corridor, has recently been completed. A Settlement Agreement between the City and the underlying property owner was secured, giving the City development rights to build Phase 2 of the NE 4<sup>th</sup> Street extension. Included in the Settlement Agreement

was an approval of the conceptual design of this proposal, subject to Design Review for compliance with all City codes. The property was subsequently rezoned to CB via City Council action on July 17, 2014 via Ordinance 6168. A copy of the Settlement Agreement and Ordinance 6168 are available for public viewing in the project file.

**Refer to Condition of Approval regarding the Settlement Agreement in Section XI of this report.**

In addition, other transportation improvements throughout the Wilburton/NE 8<sup>th</sup> Street Subarea will work with the NE 4<sup>th</sup> Street extension to provide increased mobility. This includes the upgrade to 120<sup>th</sup> Avenue NE, which will provide improved north-south access between the Wilburton, Downtown, and Bel-Red subareas.

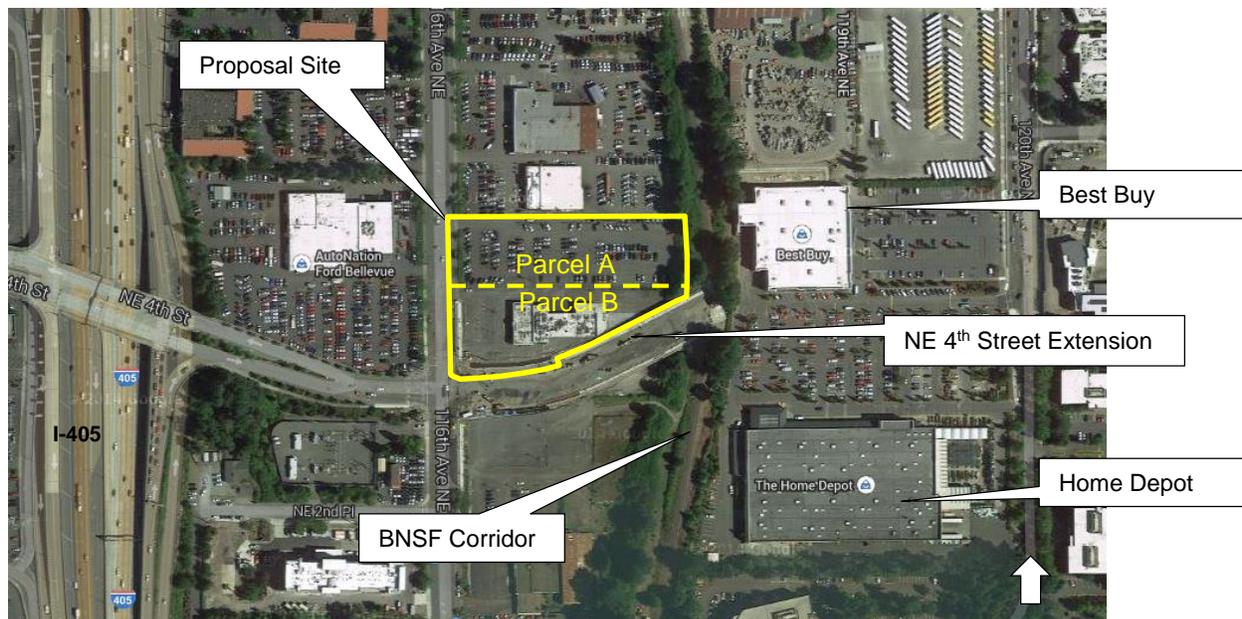
## B. Site Description

### General:

The project site currently is comprised of two parcels (Parcel A – 3325059151 and Parcel B – 3325059012). The two parcels together constitute the 'site'. Phase 1 of the NE 4<sup>th</sup> Street extension runs along the southern edge of Parcel B. It will be a requirement of this Design Review that the two parcels be combined via a Boundary Line Adjustment permit process to create one development parcel. **Refer to Condition of Approval regarding the boundary line adjustment in Section XI of this report.**

The site is located on the corner of 116<sup>th</sup> Avenue NE and Phase 1 of the NE 4<sup>th</sup> Street extension. Both streets are classified as major arterials, thereby giving the building two street frontages. The site is within the Wilburton/NE 8<sup>th</sup> Street Subarea and it is currently occupied by an abandoned auto dealership building and large surface parking lots that supported this dealership and continue to provide parking for the dealership to the north.

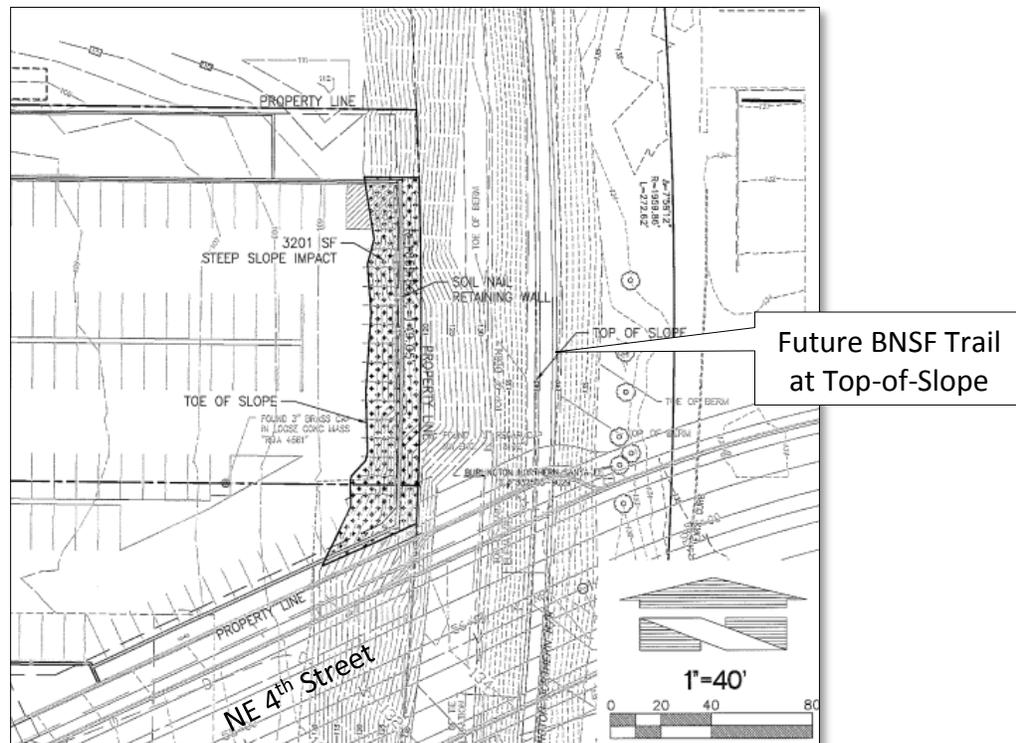
Aerial Photograph/Vicinity Map



Steep Slope Critical Area:

The eastern portion of the site is adjacent to the BNSF railroad corridor and tracks, which is slated to become a public access trail in the future. There is an existing steep slope critical area in this location running along the entire western side of the rail line. The slope is approximately 25 to 35-feet tall, and it is thickly vegetated with a few trees and noxious weeds – predominantly blackberries. The wall of the proposed parking structure, which will act as a retaining wall, will cut into the toe of this slope – resulting in approximately 3,200 square feet of steep slope disturbance. Replanting with tall trees, shrubs and groundcover will occur at the top of the wall as mitigation for the slope disturbance. **Refer to Section III.E of this report for how the proposal has meet the Critical Areas performance standards and Section IX.B and C for how the proposal has met the decision/approval criteria for a CALUP.**

**Area of steep slope disturbance**



**C. Zoning and Context**

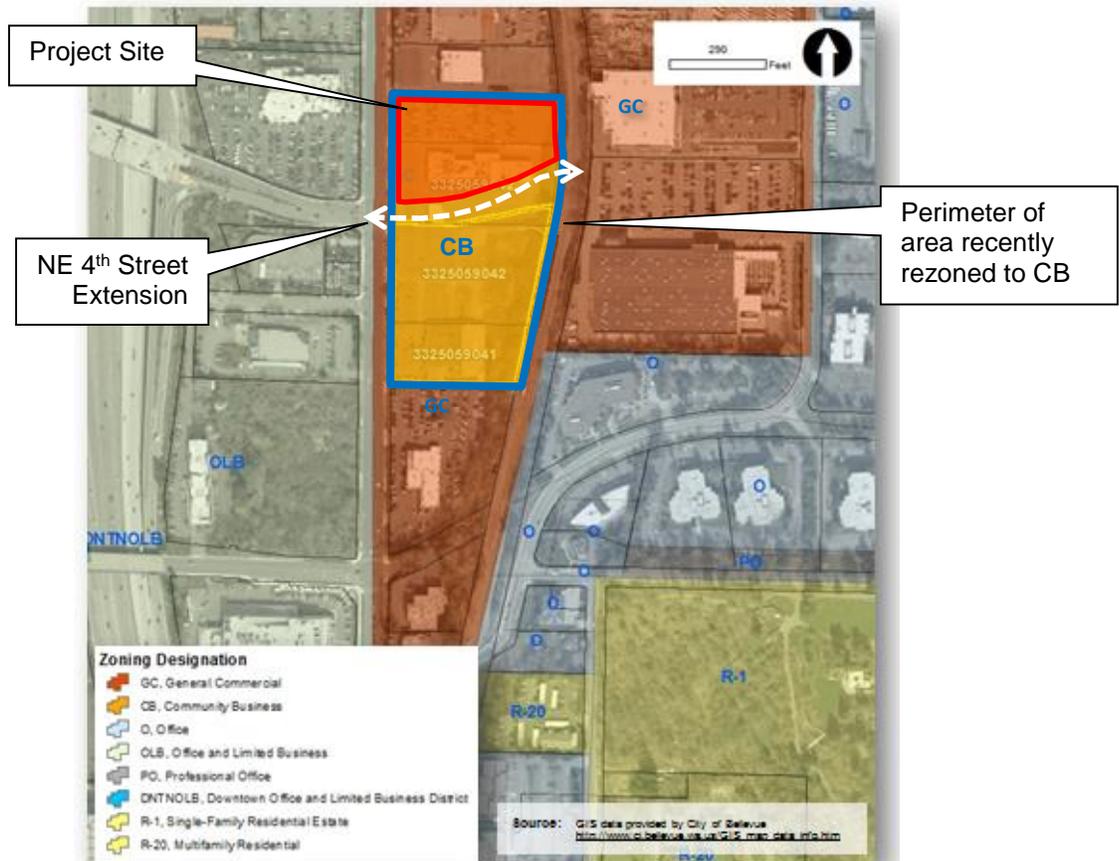
As a result of the July 2014 rezone, the site is in a CB (Community Business) land use district that lies in between the OLB (Office and Limited Business) land use across 116<sup>th</sup> Avenue NE to the west and an O (Office) land use district further to the east. This site is located in the area that is referred to as the “retail village” area in the Wilburton/NE 8<sup>th</sup> Street Subarea section of the Comprehensive Plan.

The surrounding land use districts (zoning) and land uses are a follows:

- North: GC (General Commercial), Existing auto dealership.
- South: CB (Community Business), NE 4<sup>th</sup> Street Extension and two vacant parcels which were previously auto dealerships.
- West: OLB (Office and Limited Business), across 116<sup>th</sup> Avenue NE is the Auto Nation/Ford auto dealership.

- East: GC (General Commercial), BNSF railway corridor and tracks. Just east of the BNSF corridor is Best Buy retail store.

### Zoning Map



### III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS

#### A. General Provisions of the Land Use Code

##### 1. Use

Uses are regulated by LUC 20.10.400 (Use Charts). As a result of City of Bellevue Ordinance 6183, passed by the City Council on October 20, 2014, the subject property has been rezoned from GC to CB. The retail uses are allowed in the CB land use district.

##### 2. Dimensional Requirements

All applicable dimensional requirements of the Land Use Code will be met. Refer to the following chart for specific information, as well as Conditions of Approval and the attached Project Plans - Attachment A.

	Permitted/Required	Proposed/Comments
<b>Site Area/Zone</b>	<p>Site Area: 137,420 SF = 3.15 acres</p> <p>CB Land Use District within the Wilburton Subarea</p>	
<b>Building Height</b>	<p>CB: 75 FT is allowed as a result of a recent Land Use Code Amendment – measured from average finish grade</p> <p>LUC 20.25B.040.A LUC 20.20.010</p>	<p>44'-3" FT – measured from average finish grade elevation of 96'-3"</p> <p>Meets LUC requirement.</p>
<b>Maximum Impervious Surface</b>	<p>Site Area: 137,420 SF Existing pervious: 2,330 SF or 1.6% Proposed: 8,848 SF or 6.4% Net increase of 6,518 SF</p> <p>LUC 20.20.010 LUC 20.20.460.F</p>	<p>Per LUC requirements, a use established prior to August 1, 2006 which exceeds the LUC 20.20.010 impervious surface requirements may do so and be considered conforming. The proposal may not further decrease the pervious surface. Meets LUC requirement.</p>
<b>Building Size</b>	<p>Retail Use: May not exceed 100,000 SF without a Development Agreement per Ordinance 6168.</p> <p>Proposed: Retail: 74,000 SF Mechanical/Loading: 8,000 SF Total: 82,000 SF</p>	<p>Meets LUC requirement.</p>
<b>Setbacks</b>	<p><u>Fronts</u> 116<sup>th</sup> Avenue NE and NE 4<sup>th</sup> Street: 0 FT* from the property line <u>Rear</u>: 0 FT* <u>Side</u>: 0 FT*</p> <p>*Landscape buffers are required. See Landscape section below. LUC 20.20.010</p>	<p>Meets LUC requirement.</p>
<b>Tree Retention</b>	<p>Site Perimeter: 100% of trees 8-inch diameter or greater retained within <b>15 feet</b> of the property line One 8" tree is within the perimeter.*</p> <p>Site Interior: 15% of 40 diameter inches retained = 14 inches* LUC 20.20.900</p>	<p>100 % of the trees on site will be removed (5 trees/48 diameter inches). <b><u>Refer to discussion in Sections III.C regarding the Alternative Tree Retention Option and Section VII.B regarding tree removal and Conditions of Approval regarding the final landscape and irrigation plans and landscape installation and maintenance assurance</u></b></p>

	Permitted/Required	Proposed/Comments
<b>Tree Retention (Cont'd)</b>	* However: All trees are within the Critical Area steep slope and as such are to be retained per LUC 20.25H	<b><u>devices in Section XI of this report.</u></b> As conditioned, meets LUC requirement.
<b>Landscape</b>	Streetscape planting strips: 116 <sup>th</sup> Avenue NE (per Design Guidelines): Min. 7 FT  Interior Property Lines: 8 FT Type III  Compliance with LUC 20.20.520, 20.20.900 and 20.25B.040.C	116 <sup>th</sup> Ave. NE: Min. 7 FT  Interior Property Lines: Rear – eastern property line: Varies between 5-7 FT Sides: Northern property line: 2 FT.  Note: Applicant will be required to provide irrigation in all streetscape planting strips and provide a separate meter for use by the City to maintain the street trees.  <b><u>Refer to discussion in Section III.C below regarding the Alternative Landscaping Option (ALO) and Conditions of Approval regarding the final landscape and irrigation plan, maintenance agreement with the Parks Department/City of Bellevue, maintenance and monitoring plan, and landscape installation and maintenance assurance devices in Section XI of this report.</u></b>  As conditioned, meets LUC requirements.
<b>Mechanical Equipment</b>	Located on the roof & visually screened, within the building or below grade.  LUC 20.20.525	There will be mechanical units located on the roof. Through design review, it was determined that it would be more distracting to place screens around the units than to paint them to match the roof. All mechanical units must be painted to match the roof. As conditioned, meets LUC requirements. <b><u>Refer to Condition of Approval regarding the mechanical equipment in Section XI of this report.</u></b>

	Permitted/Required	Proposed/Comments
<b>Lighting</b>	All exterior lighting will require cut-off shields to protect adjacent neighborhoods.	<b><u>Refer to Condition of Approval regarding lighting in Section XI of this report.</u></b>  As conditioned, meets LUC requirements.

**B. Parking**

The proposal is for one building that currently is shown to have three retail spaces. Within the parking structure, the applicant will provide 12 more spaces than required by the LUC.

All parking required for this proposal must be accommodated within the existing parking structure. In addition, bicycle parking is required in this CB land use district. It will be a condition of this approval that the applicant provide parking for a minimum of 20 bicycles, which will support the new bike lanes on NE 4<sup>th</sup> Street and anticipated future bicycle traffic on the BNSF trail. There will be no electric vehicle charging stations, although the City encourages the applicant to consider providing at least two of these stations/stalls to meet future sustainability goals. It is also suggested that the applicant provide conduit to facilitate adding these stations in the future. **Refer to Condition of Approval regarding bicycle parking in Section XI of this report.**

**Parking**

Use	Net Square Feet	Parking Requirement	Stalls Required
Retail Use	74, 360 NSF	4/1,000 NSF	297
<b>TOTAL REQUIRED</b>			297
<b>TOTAL PARKING PROVIDED</b>			309 (excess of 12 stalls)

**Parking Stalls:**

- Standard Stalls: 231 stalls
- Compact Stalls (may not exceed 50%): 66 stalls
- Handicapped Stalls: 12 stalls

**C. Alternative Landscaping Option (ALO)**

Due to the shape and size of the proposal site, the presence of a steep slope critical area along the eastern property line, and the recent construction of the NE 4<sup>th</sup> Street Extension, the applicant is requesting approval of an Alternative Landscape Option to reduce the eight-foot wide landscape buffers along the interior property lines to two feet along the northern property line, and five feet to seven feet four inches along the

eastern property line. The request also includes reduction of the required ten-foot wide landscaping along NE 4<sup>th</sup> Street to the width of the landscape planters, which are approximately three to five feet wide. The design of these areas is described in Section I.C of this report. How the applicant has addressed the criteria of the ALO (LUC 20.20.520.J) is discussed below.

**1. The proposed landscaping represents an equal or better result than that which could be achieved by strictly following the requirements of LUC 20.20.520;**

***Finding:*** The perimeter plantings along the north and east perimeter will use large tree and shrub plantings to provide a dense and layered green barrier between the building structures (primarily the parking structure), and the adjacent properties. The provision of large trees at time of planting and bamboo will provide a faster coverage of vertical areas than traditional perimeter plantings could provide.

The applicant has chosen to reduce the required width for the interior property line plantings and place wider planting areas and landscape emphasis along the street frontages. This treatment will include large street trees, weather protections in the form of marquees and canopies, smaller accent flowering trees, and shrub plantings; all of which are more compatible with the community business design standards and guidelines and the vision of this area as a 'retail village.'

A large portion of the plants will be native or adapted to the Northwest where appropriate and will exemplify the Pacific Northwest character. All new landscaping will be irrigated with a water-conserving automated irrigation system to provide optimal growing conditions.

**2. The proposed landscaping complies with the stated purpose of this section;**

***Finding:*** The proposed landscaping will meet the intent of the landscape development section of the LUC. Through the use of plantings, the proposal will create inviting pedestrian spaces at the corner plaza at the intersection of NE 4<sup>th</sup> Street and 116<sup>th</sup> Avenue NE, at the northeast corner of the retail building, and along the pedestrian realm along both street frontages. High quality, contemporary landscape elements will provide lasting visual interest and create pedestrian friendly environments. The additional weather protection, benches, tables and smaller plaza areas will enhance the appearance of the streets and intersection.

A dense planting native and adapted tree and shrub planting along the eastern side of the development and a vigorous planting of bamboo along the northern property line will also reflect the Pacific Northwest character.

**3. If a modification is requested, the proposed landscaping fulfills the following:**

- **Better accommodates or improves the existing physical conditions of the subject property; or**
- **Incorporates elements to protect or improve water quality.**

***Finding:*** The existing site contains challenging steep slopes and retaining walls

associated with the NE 4<sup>th</sup> Street extension. These conditions made meeting the buffering requirements difficult to achieve. The transformation of the site, which currently is almost entirely a paved surface parking lot, into a landscaped development is an improvement over the existing physical conditions. The increase in pervious surface on the site via planted areas will improve storm water infiltration and recharge. The proposed planting of native conifers and native understory along the eastern perimeter in the critical steep slope area will improve the hillside stability and positively affect the slope ecosystem.

Appropriate use of native and drought tolerant plants will reduce the irrigation water needs in select areas and overall, irrigation will be provided by a water-conserving automated irrigation system, and the plantings provided will exemplify the Pacific Northwest vegetated character.

**Refer to Conditions of Approval regarding the final landscape and irrigation plans, maintenance agreement with the Parks Department/City of Bellevue, and installation and maintenance assurance devices in Section XI of this report.**

**D. Community Business Design Standards (LUC 20.25I.050)**

**1. Special Corner Feature: At the intersection of two arterial streets, the site development shall incorporate a special corner feature.**

***Finding:*** The applicant has provided a corner feature at the southwest corner of the building which includes plantings, decorative rocks, weather protection and display windows.

**2. Building Design Standards**

- a) **The design of the buildings shall incorporate elements such as special architectural details, distinctive color schemes, special art and other features, which are sensitive to and enhance the surrounding area and serve to distinguish the complex from other retail complexes in the City.**
- b) **The sides of a building which face toward a public street shall include public entrances to the building and windows to provide visual access to the activity within the building.**
- c) **The sides of a building which face toward an adjoining property, but not toward a public street, shall include elements such as windows, doors, color, texture, landscaping or wall treatment to provide visual interest and prevent the development of a long continuous blank wall.**
- d) **Building design shall provide for architecturally integrated signage. Signs shall be in proportion to the development and oriented to the main direction access and to pedestrian movement.**

***Finding:*** Per the Settlement Agreement between the applicant and the City of Bellevue to construct the NE 4<sup>th</sup> Street extension, the design of the building did not provide any pedestrian entrances off the public streets. Instead, the applicant will provide landscaping, display windows, areas of refuge and seating, and weather protection via the stair climb on the eastern façade of the building to help guide the pedestrian to the front entries and to provide visual interest. All four sides of the building have been designed to be interesting from all adjacent properties and signage has been reviewed conceptually to ensure that the signage and the proportions of the signs are integrated with the overall building design. **Refer to Condition of Approval regarding signage in Section XI of this report.**

### 3. Internal Walkways

- a) Walkways, six feet in width minimum, shall be provided from the public sidewalk or right-of-way to the building. At a minimum, walkways shall be located to connect focus points of pedestrian activity such as transit stops and street crossings to the major building entry points.
- b) Walkways shall be provided to connect with walkway locations on adjoining properties. On sides of the building which provide public access into the building, the walkway should be wide enough to allow for sidewalk seating area or window gazing as well as pedestrian travel. Weather protection of the building walkway should be provided at a minimum at the entrance area and, if appropriate, along the entire building walkway.
- c) Internal walkway surfaces shall be designed to be visually attractive and distinguishable from driving surfaces.

***Finding:*** Walkways are provided to both levels of the retail building from the NE 4<sup>th</sup> Street sidewalk and at a minimum, will be separated from vehicular driveways by a curb. As conditioned, both will be a minimum of six-feet wide to allow for safe connectivity with the active public street. **Refer to Condition of Approval regarding the internal walkways in Section XI of this report.**

### 4. Other Development Standards

- a) Rooftop equipment shall be visually screened and the design and color of rooftop mechanical equipment should be integrated with the building architectural style.
- b) The outdoor display of building materials and similar bulky products shall be screened from the public street.
- c) Bicycle racks shall be provided on-site. Facilities for a minimum of 10 bicycles shall be provided for developments have 100 or fewer parking stalls. For each 100 additional stalls, facilities for five additional bicycles shall be provided.

***Finding:*** The applicant will be required to provide parking for 20 bicycles throughout the site. Any rooftop mechanical equipment that is not shown in the Design Review application drawings must be included on all construction permit drawings for review by Land Use and shall be screened per these standards. **Refer to Condition of Approval regarding bicycle parking and rooftop mechanical equipment in Section XI of this report.**

## E. Critical Areas

### 1. Performance Standards for Critical Areas

- a. Consistency with Land Use code Steep Slope Critical Areas Performance Standards – LUC 20.25H.125:

- 1) Structures and improvements shall minimize alterations to the natural contour of the slope, and foundations shall be tiered where possible to conform to existing topography;

***Finding:*** Per an initial Geotechnical Report, prepared by Earth Solutions NW, LLC, dated September 8, 2009 and updated November 7 and December 4, 2014, there are currently no obvious signs of instability on the slope, the overall stability of the slope area can be characterized as good, and the proposed structure and associated site improvements will enhance the existing steep slope critical area by providing further bank stabilization through the use of structural retaining walls and landscaping.

These slopes were created by the grading associated with the railroad line and they are not naturally created contours. The Geotechnical Report can be found in the project file for public viewing.

In addition, the construction of the NE 4<sup>th</sup> Street extension has effectively removed the slope in the street right-of-way, thereby destroying any continuity the proposal slope might have to properties to the south. Existing grades in the location of the NE 4<sup>th</sup> Street extension have been entirely filled and stabilized with new retaining wall. The proposed development has been designed to meet the finish grades and retaining wall of the NE 4<sup>th</sup> Street extension.

**2) Structures and improvements shall be located to preserve the most critical portion of the site and its natural landforms and vegetation.**

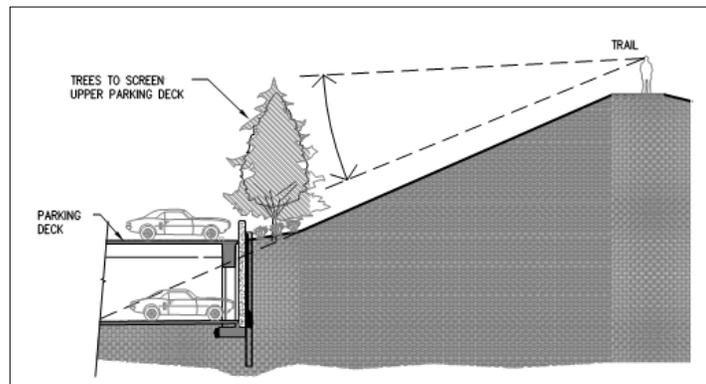
**Finding:** The existing slope is heavily overgrown with blackberries and other noxious weeds. The slope was created by grading to accommodate the railroad construction and an existing sewer trunk line. This included cuts at the toe of the slope and fill placed over the crest. Therefore, little of the natural landform currently exists. What little vegetation exists is degraded and saving it would be less advantageous to the health of the slope than replanting it with a dense planting of native and/or semi-native trees, shrubs and groundcover.

**3) The proposed development shall not result in greater risk or a need for increased buffers on neighboring properties;**

**Finding:** A slope stability analysis presented on page 4 of the Geotechnical Report found the existing slope to be stable and development would present a low potential for landslide activity. The geotechnical engineer expects that the proposed development will not decrease the stability of the site, nor will it adversely affect the stability of slopes on neighboring properties. The slope to the south along the NE 4<sup>th</sup> Street extension has already been significantly modified and is retained with engineered walls.

**4) The use of retaining walls that allow the maintenance of existing natural slope area is preferred over graded artificial slopes where graded slopes would result in increased disturbance as compared to use of retaining wall;**

**Finding:** Structural retaining walls in combination with grading are proposed to retain the eastern slope. Per the December 4, 2014



addendum to the Geotechnical Report, it is the Geotechnical Engineers' opinion that "because the slopes exhibit good stability and the proposed

development will require only minor alteration to a small portion of the steep slope area”, the proposal will not decrease stability of the site or surrounding properties. The portions of the un-retained steep slope beyond the property line will remain in place.

**5) Development shall be designed to minimize impervious surfaces within the critical area and critical area buffer;**

**Finding:** For the proposed development, the required size of the parking structure made it impractical to significantly reduce the impervious area proposed within the steep slope. However, the replacement vegetation on the reconfigured slope will be of higher value than the noxious plant material that currently exists and no improvements are proposed within the top-of-slope buffer, which is offsite.

**6) Where change in grade outside the building footprint is necessary, the site retention system should be stepped and re-grading should be designed to minimize topographic modification. On slopes in excess of 40 percent, grading for yard area may be disallowed where inconsistent with this criteria.**

**Finding:** Disturbance outside the building footprint will be very limited where soil nail retaining walls are used. Once the walls are in place, any additional disturbance will be limited to landscaping activities associated with the planting of trees between the wall and property line as mitigation for steep slope disturbance and provision of a required vegetated buffer. The remaining steep slope will meet the grades of the BNSF corridor along the eastern property line of the site and the NE 4<sup>th</sup> Street extension to the south.

**7) Building foundation walls shall be utilized as retaining walls rather than rockeries or retaining structures built separately and away from the building wherever feasible. Freestanding retaining devices are only permitted when they cannot be designed as structural elements of the building foundation;**

**Finding:** All major retaining walls will be structurally integrated with the entire parking structure building.

**8) On slopes in excess of 40 percent, use of pole-type construction which conforms to the existing topography is required where feasible. If pole-type construction is not technically feasible, the structure must be tiered to conform to the existing topography and to minimize topographic modification;**

**Finding:** Use of pole-type construction is not feasible for the proposed building because a majority of the building will be constructed below grade at the toe of the slope.

**9) On slopes in excess of 40 percent, piled deck support structures are required where technically feasible for parking or garages over fill-based construction types; and**

**Finding:** Piled deck support structures are not feasible for the proposed building because a majority of the parking structure will be constructed below the steep slope.

**10) Areas of new permanent disturbance and all areas of temporary disturbance shall be mitigated and/or restored pursuant to a mitigation and restoration plan meeting the requirements of LUC 20.25H.210.**

**Finding:** Effectively the entire site will be temporarily and permanently disturbed by the proposed development. The entire site will be restored as part of the development. Conceptual landscape mitigation plans have been provided and reviewed as part of this approval. Final landscape and irrigation plans, final maintenance and monitoring plans, erosion and sediment control plans, and drainage plans will be prepared and reviewed under the Clearing and Grading Permit.

**b. Critical Areas Report – Additional Provisions – LUC 20.25H.145:**

**Finding:** The applicant has submitted information via the Geotechnical Report to provide assurance that the proposed development will not increase the threat to slope stability. Through the placement of engineered retaining walls and increased landscape development, the proposal will actually increase the overall slope stability over conditions that currently exist.

#### **IV. DESIGN GUIDELINES**

**A. Community Retail Design Guidelines (LUC 20.25I.040)**

This proposal is required to meet the Community Retail Design Guidelines, which are included in a Settlement Agreement between the property owner and the City. Conformance with these Guidelines is discussed below:

**1. Building Design Guidelines – LUC 20.25I.040.A**

**a. All buildings within a multi-building complex should achieve a unit of design.**

**b. Individual buildings should incorporate similar design elements.**

**Finding:** Primary architectural emphasis will be given to the retail building. The open parking structure will have a more simple design to reduce its visual impact and presence on the site. However, it will still have architectural details that relate back to the retail building, including metal screens and architectural trellis features.

**2. Site Design Guidelines – LUC 20.25I.040.B**

**a. Any relocated landscape area should be visible from the public street.**

**b. Parking areas should be designed to minimize conflicts between pedestrian and vehicular movements.**

**c. The landscape design for the site should include plantings which emphasize the major points of pedestrian and vehicular access to the site.**

**d. Site features such as walls and light fixtures should be designed to be consistent with the scale and architectural design of the primary structure. Site features should be designed and located to contribute to the pedestrian environment of the site.**

**e. Loading areas should not be located between the building and the street unless there is no alternative location possible. Loading areas, if located between the building and the street, should be oriented away from the street and screened to minimize views of the loading areas**

- from the street and sidewalk.**
- f. In multiple-building complexes, buildings should be located to facilitate safe and comfortable pedestrian movements between buildings.**
  - g. Opportunities should be found for safe, convenient, and pleasant pedestrian connections to existing transit facilities.**

***Finding:*** The primary focus of new landscaping for the proposal will be along both public streets and, in conjunction with street furnishing and building details, this landscaping will help to guide access into the site. Dedicated, wide walkways will guide the pedestrian from NE 4<sup>th</sup> Street to the retail entries and are designed to minimize conflicts with vehicular traffic. The pedestrian walkway from 116<sup>th</sup> Avenue NE along the northern side of the building will pass by the loading dock. However, it will be delineated with a change in material, texture, and/or color to provide strong visual clues for the pedestrian and drivers alike. Site features, including seating, overhead protection, trellises and landscaping have been designed with the pedestrian in mind. A major pedestrian gathering spaces has been created at the corner of NE 4<sup>th</sup> Street and 116<sup>th</sup> Avenue NE, and smaller pedestrian spaces will be located along the stair climb and between NE 4<sup>th</sup> Street and the entries to the retail spaces on the lower level of the retail building. Connections to existing transit, the future light rail station one block to the north, and the future BNSF trail will be enhanced with weather protection and landscaping that will create a pleasant pedestrian experience.

The loading area will be located along 116<sup>th</sup> Avenue NE in the northwest corner of the site. However, the loading dock itself will be oriented away from the street and screened with a concrete block wall and landscaping. It will be a condition of this approval that the loading dock doors be closed at all times when the dock is not in use. **Refer to Conditions of Approval regarding internal walkways and the solid waste, recycling, and loading dock in Section XI of this report.**

## **V. PUBLIC NOTICE AND COMMENT**

Application Date: November 7, 2014  
Application Completeness Date: December 4, 2014  
Notice of Application published: December 18, 2014  
Public Notice Sign installed: December 18, 2014  
Minimum Comment Period ended: January 5, 2015

A public meeting for this Design Review was held on January 27, 2015. There were four attendees. There were no substantial comments by the attendees. All were there to see what was being proposed.

Although the minimum required public comment period ended on January 5, comments were accepted up to the date of this decision. One written comment was received asking to be a Party of Record for this Design Review application, but no comments or questions regarding the proposal were provided.

## VI. TECHNICAL REVIEW

### A. Clearing & Grading

The Clear and Grade Reviewer reviewed the plans and materials submitted for this project and determined that clearing and grading portion of this Design Review application can be approved. All City of Bellevue Clearing and Grading and Erosion Control requirements will be addressed under the Clearing and Grading Permit application, which must comply with City of Bellevue Clearing and Grading Code (BCC 23.76).

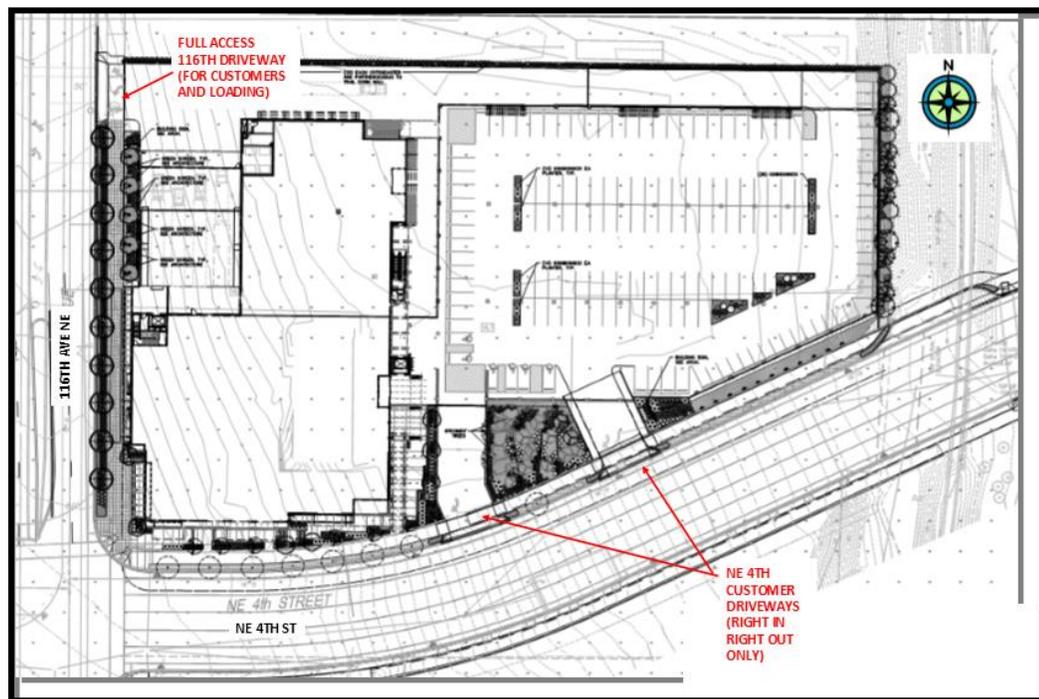
### B. Utilities

The Utilities Department technical staff confirms that there is sufficient capacity in existing utilities to provide service to this site. **Refer to Conditions of Approval regarding utilities in Section XI of this report.**

### C. Transportation

#### Site Access

All site access locations and restrictions were determined during settlement negotiations between the City and the property owner of the project site (referenced in this report as the Settlement Agreement). See access configuration diagram below.



Approved access configuration (identifiers added).

#### **NE 4<sup>th</sup> Street Access:**

Access from NE 4<sup>th</sup> Street shall be provided by two 30 foot wide commercial driveways. Turning restrictions will be applied at these driveways per Section 7 and

Exhibit C of the Settlement Agreement. Section 7a from the agreement reads in part, as follows:

*...There shall be no left in from or left out onto NE 4<sup>th</sup> Street from the North Parcel.*

The driveway shall be constructed per DEV-7A (or acceptable modification).

**116<sup>th</sup> Avenue NE Access:**

Access from 116<sup>th</sup> Avenue NE shall be provided by one 36 foot wide commercial driveway with separate left and right turn exit lanes. This driveway will also provide access for all loading and unloading activities.

The driveway shall be constructed per DEV-7A (or acceptable modification). See drawing below for the approved access configuration.

**Refer to Condition of Approval regarding vehicular access restrictions in Section XI of this report.**

**Street Frontage Improvements**

In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, the construction of street frontage improvements is required as a condition of development approval. The design of the improvements must conform to the requirements of the Americans with Disabilities Act, the Transportation Development Code (BCC 14.60), and the provisions of the Transportation Department Design Manual.

The majority of street frontage improvements along NE 4<sup>th</sup> Street have been installed by the City's NE 4<sup>th</sup> Street Extension Project (CIP PWR-160). The City's work is expected to be completed by May 2016. The applicant will be responsible for c-curb and associated pavement markings to enforce the driveway restrictions on NE 4<sup>th</sup> Street. In addition, streetlight relocation and mitigation for damages to NE 4<sup>th</sup> Street (sidewalks, pavement, etc.) caused by construction activity related to this project will be the responsibility of the applicant.

The applicant will be responsible for full frontage improvements on 116<sup>th</sup> Avenue NE as listed below.

1. The curb, gutter, and sidewalk on 116<sup>th</sup> Avenue NE shall be completely removed and reconstructed with a sidewalk width of at least seventeen feet, not including the curb. Planter strips between the curb and the sidewalk are included in the seventeen-foot width and they shall have a minimum width of seven feet. Minimum sidewalk width is ten feet. Bicycle lane channelization may be needed as well. At any location where the sidewalk extends over a basement or parking garage, a construction method that will prevent differential settling must be used. Such method must be acceptable to the Transportation Department.
2. The Americans with Disabilities Act (ADA) requires that sidewalk cross slopes not exceed two percent. The sidewalk cross slope may be less than two percent only if the sidewalk has a longitudinal slope sufficient to provide adequate drainage. Bellevue's standard for curb height is six inches, except where curb ramps are needed. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm

compliance. New curb and sidewalk shall be constructed in compliance with these requirements. Curb and sidewalk elevations will not be revised to fit the building, and city inspectors may require spot surveys during construction in order to confirm the required elevations.

ADA also requires provision of a consistent travel path for visually handicapped pedestrians. Potential tripping hazards are not allowed in the main pathway. Buildings shall be designed so that doors do not swing out into the pedestrian path. Installation of colored or textured bands to guide pedestrians in the direction of travel is advisable, subject to the requirements for non-standard sidewalk features. ADA-compliant curb ramps shall be installed where needed, consistent with relevant transportation standards and codes. Building elevations shall be consistent with the required curb and sidewalk elevations. Spot elevations must be included in the building plans in a manner that proves that building elevations are designed to correspond to the sidewalk elevations shown in the engineering plans, especially at entrances and other key points.

3. The design and appearance of the sidewalk and landscaping on 116<sup>th</sup> Avenue NE shall comply with relevant city codes and standards. The sidewalk shall be constructed of standard concrete with a broom finish and a two-foot by two-foot score pattern, with four-foot by six-foot tree wells, unless the Transportation Department agrees to accept any non-standard pattern, color, or other features. Alternative paving samples must be submitted for review. If approved, any non-standard patterns, colors, or other features may be installed only if an agreement is recorded against the property to hold the landowners responsible for maintenance and replacement of all such non-standard sidewalk features.
4. Any non-standard features or vegetation shall not create a sight obstruction within any required sight triangle, shall not create a tripping or slipping hazard in the sidewalk, and shall not create a raised fixed object in the street's clear zone. The materials and installation methods must meet typical construction requirements.

Continuous planting strips and other landscaping within the sidewalk on 116<sup>th</sup> Avenue NE shall be irrigated with a private metered water source. Electrical connections for lighting in tree wells or planter strips may be allowed, if installed in compliance with the electrical code and subjected to an electrical inspection. Irrigation devices and electrical components shall not create a tripping hazard in the sidewalk. **Refer to Condition of Approval regarding final landscape and irrigation plans in Section XI of this report.**

5. No new building structure or garage shall be constructed under a street right of way or existing public sidewalk/utility easement. In some conditions (to be finalized during engineering and building plan review), new structure may be allowed under a new sidewalk/utility easement. No soil nailing is allowed under a street right of way or sidewalk/utility easement without an indemnification agreement that protects the city.
6. No new utility vaults that serve only one development will be allowed within a public sidewalk. Vaults serving a broader public purpose may be located within a public sidewalk.
7. Any awning, marquee, balcony, etc. over a sidewalk or utility easement must be

at least 16 feet above the sidewalk, or be removable (with an agreement regarding removal and replacement); and must have at least 3 feet horizontal clearance from any streetlight or traffic signal pole.

8. No new overhead utility lines will be allowed within or across any right of way or sidewalk easement, and existing overhead lines must be relocated underground.
9. A combined street tree and street light plan is required for review and approval prior to completion of engineering and landscape plans for their installation. The goal is to provide the optimum number of street trees while not compromising the light and safety provided by streetlights. Street trees and streetlights must be shown on the same plan sheet with the proper separation (generally 25 feet apart) and the proper spacing from driveways (ten feet from Point A in standard drawing DEV-7A).
10. No fixed objects, including fire hydrants, trees, and streetlight poles, are allowed within ten feet of a driveway edge, defined as Point A in standard drawing Dev-6. Fixed objects are defined as anything with breakaway characteristics stronger than a 4-inch by 4-inch wooden post.

**Refer to Condition of Approval regarding street frontage improvements, civil engineering plans for transportation, and building and site plans for transportation in Section XI of this report.**

#### **Easements and Right of Way**

The property owner is providing sidewalk, utility, construction, and retaining wall easements per the Settlement Agreement along NE 4<sup>th</sup> Street. The property owner is also providing right of way per the provisions of the Settlement Agreement.

In addition, the applicant will be required to provide pedestrian easements along 116<sup>th</sup> Avenue NE where the sidewalk width extends into the project site. **Refer to Condition of Approval regarding pedestrian easements in Section XI of this report.**

Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible. **Refer to Condition of Approval regarding easements for signal control and street light boxes and vaults in Section XI of this report.**

#### **Holiday Construction & Traffic Restrictions**

From November 15<sup>th</sup> to January 5<sup>th</sup>, construction activities such as hauling and lane closures will be allowed only between the hours of 10:00 p.m. and 6:00 a.m. due to holiday traffic. The dates and times of these restrictions are subject to change. The applicant shall contact the Transportation Department Right-of-Way Section to confirm the specifics of this restriction prior to applying for a Right-of-Way Use Permit, which is issued directly by the Transportation Department. **Refer to Condition of Approval regarding holiday construction and traffic restrictions in Section XI of this report.**

### **Use of the Right-of-Way during Construction**

Applicants often request use of the right of way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right-of-Way Use Permit for such activities must be acquired prior to issuance of any construction permit including demolition permit. Sidewalks may not be closed except as specifically allowed by a Right-of-Way Use Permit.

**Refer to Condition of Approval regarding the Right-of-Way Use Permit in Section XI of this report.**

### **Pavement Restoration**

The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration

Program, every street in the City of Bellevue has been examined and placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are, "No Street Cuts Permitted," "Overlay Required," and "Standard Trench Restoration." Each category has different trench restoration requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that often comes with small strip patching.

Near this project, 116<sup>th</sup> Avenue NE has been classified as a "No Street Cuts Permitted," street. The applicant must obtain permission to cut into this street from the city's Right of Way Manager prior to any street cut. Minimum pavement restoration requirements will consist of a full grind and overlay extending 50 feet from the center of the trench in both directions for the full width of the street.

The applicant will be responsible for any damage on NE 4<sup>th</sup> Street resulting from construction activities related to this proposal. Restoration requirements will be determined by the Right of Manager. **Refer to Condition of Approval regarding pavement restoration in Section XI of this report.**

### **Transportation Management Program**

In order to reduce single occupant vehicle trips and provide enhanced options to employees and infrastructure users, the City has adopted code provisions for a transportation management program. The owner of this development shall, prior to any initial occupancy of the building structure, sign and record an agreement approved by the City of Bellevue to establish a transportation management program to the extent required by BCC14.60.070. This project will be required to post and distribute ridesharing and transit information from King County Metro or other approved sources in a visible, central location in the building lobby or other public area near the major entrance to the building on a continual basis. **Refer to**

**Conditions of Approval regarding the transportation management program and implementation of the transportation management program in Section XI of this report.**

**D. Fire**

The Fire Reviewer reviewed the plans and materials submitted for this project and determined that the fire-related portion of this Design Review application can be approved. **Refer to Conditions of Approval regarding fire in Section XI of this report.**

**VII. STATE ENVIRONMENTAL POLICY ACT (SEPA)**

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the proposal (see Environmental Checklist in the project file at City Hall Records Office). Therefore, issuance of a Determination of Non-Significance (DNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements with the incorporation by reference of the 2013-2024 Transportation Facilities Plan Final Environmental Impact Statement (TFP EIS), adopted August, 2013 (available in the Records Office at City Hall). This document analyzes the transportation and air quality impacts of the City's Traffic Task Force recommendations to meet the Comprehensive Plan, Transportation Element, and Mobility Management goals.

This section of the staff report is an addendum to the adopted EIS referenced above and an Environmental Checklist has been reviewed and the annotated checklist is available for public viewing in the project file at City Hall. Adverse impacts which are less than significant are usually subject to City Code or Standards which are intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code Section 22.02.140 provides substantive authority to mitigate impacts disclosed through the environmental review process. A discussion of the impacts is noted below together with specific conditions of approval. These impacts will be mitigated through exercise of Code authority as well as through project-specific conditions of approval, contained in Section XI of this report.

**A. NOISE**

**Construction Noise:** The Bellevue Noise Control Ordinance BCC 9.18 limits noise levels at the property line to 55 dBA (A-weighted sound level), except from 7:00 a.m. to 6:00 p.m. on weekdays and 9:00 a.m. to 6:00 p.m. on Saturdays that are not legal holidays. Expanded hours may be approved by the Land Use Director per BCC 9.18.020.B and approval via an LY permit. Restricting the construction hours will reduce noise impacts to neighboring properties. Expanded construction hours during evening or early morning hours should be avoided to minimize noise impacts to the adjacent hotel. In addition, the contractor must use the best available noise abatement technology consistent with feasibility during construction. **Refer to Condition of Approval regarding noise and construction hours and use of best available noise abatement technology in Section XI of this report.**

**B. PLANTS**

Disturbance of the critical areas slope to build the parking structure retaining wall will result in approximately 3,300 square feet of disturbance, including the removal of existing vegetation. However, the majority of the vegetated area is covered with blackberries – a noxious understory plant. In addition, four significant trees (trees with a diameter of 8 inches or more) that include one Red Alder, two Bigleaf Maples, and one Bitter Cherry will be removed. As mitigation for the removal of these trees,

the applicant has proposed to plant 16 trees with a minimum diameter of 2 ½ to 3 inches at the time of planting. The trees will be a mix of 12 coniferous and 4 deciduous trees and will be planted between the top of the new retaining wall and the eastern property line. According to the Arborist Report, prepared by Tree Solutions, dated February 19, 2015, the new trees proposed would be longer lived than the existing trees, will be more suitable for the conditions, and will provide greater ecological function due to the mix of conifers and deciduous trees. In addition, the proposed trees will help to further stabilize the steep slope that continues up to the proposed BNSF trail to the east from the eastern property line. Lastly, the applicant will also plant Northwest-adapted understory to replace the existing noxious weeds (blackberries) and create improved vegetated habitat that will also complement the natural character of the Pacific Northwest. Refer to the Arborist Report, Geotechnical Addendum regarding Tree Removal within Geotechnical Critical Areas, prepared by Earth Solutions NW, dated March 6, 2015, and narrative prepared by the Weisman Design Group, dated March 10, 2015 in the project file.

### **C. TRANSPORTATION**

#### **Long Term Impacts and Mitigation**

The long-term impacts of development projected to occur in the City by 2024 have been addressed in the City's 2013-2024 Transportation Facilities Plan EIS. The impacts of growth which are projected to occur within the City by 2024 are evaluated on the roadway network assuming that all the transportation improvement projects proposed in the City's current Transportation Facilities Plan are in place. The Transportation Facilities Plan EIS divides the City into several Mobility Management Areas (MMAs) for analysis purposes. The Bellevue 116<sup>th</sup> North Parcel Project lies within MMA #4 (Wilburton Area), which has a 2024 total growth projection of 186,916 square feet of new retail space (as well as 428,342 square feet of new office space). This development proposes 74,000 square feet of new retail space (or gross leasable square feet). Therefore, the volume of proposed development is within the assumptions of the Transportation Facilities Plan EIS.

Traffic impact fees are used by the City to fund street improvement projects to alleviate traffic congestion caused by the cumulative impacts of development throughout the City. Payment of the transportation impact fee, as required by BCC 22.16, contributes to the financing of transportation improvement projects in the current adopted Transportation Facilities Plan, and is considered to be adequate mitigation of long-term traffic impacts. Fee payment is required at the time of building permit issuance. This project is eligible for a traffic impact fee credit based on the pre-existing use of the site. Credit will not be given for right of way dedication as the property owner has received compensation per the Settlement Agreement. **Refer to Condition of Approval regarding transportation impact fee in Section XI of this report.**

#### **Mid-Range Impacts and Mitigation**

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more new p.m. peak hour trips undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained.

This development will generate approximately 148 new p.m. peak hour trips based on 75,000 square feet of new gross leasable area (an additional 1000 square feet for lobbies, stairwells, etc. outside of the leasable area was used to obtain a more conservative analysis). City staff distributed and then assigned project-generated trips to the street network using the City's EMME-2 travel forecasting model with the current Capital Investment Program network. By adding the expected project-generated trips to the traffic volumes in the model, the area average levels of service were determined. To create a baseline condition for comparison, the levels of service were also determined using traffic volumes without the project-generated trips.

Concurrency results showed that for all MMAs neither the maximum area-average levels of service (LOS) nor the allowed congestion allowances would be exceeded as a result of traffic generated from this proposal. In addition, total intersections exceeding allowed LOS thresholds across all MMAs did not increase. Therefore, this proposal has passed the concurrency test and is in compliance with all relevant sections of the Traffic Standards Code. A concurrency determination will be issued on the date of issuance of the land use decision via a Certificate of Concurrency (see Attachment C).

The rules of concurrency reservation are outlined in the Traffic Standards Code Director's Rules. The concurrency determination was reserved to this project on January 26, 2015, when the building permit application (15 103611 BB) was deemed complete. The Certificate of Concurrency will remain in effect for the life of the building permit application, pursuant to BCC 23.05.090.H. At issuance of the building permit, the Certificate of Concurrency will be extended and remain in effect for one additional year (with the possibility of up to two one-year extensions) as provided for in BCC 23.05.100.

### **Short Term Operational Impacts and Mitigation**

City staff analyzed the short term operational impacts of this proposal in order to recommend mitigation if necessary. Major transportation items analyzed were access design and coordination with the construction schedule of the city's NE 4<sup>th</sup> Street extension project (CIP-PW-R-160) located adjacent to the site's southern property line.

To minimize traffic impacts, this project has been designed with three access driveways for customers: two driveways on NE 4<sup>th</sup> Street and one driveway on 116<sup>th</sup> Avenue NE.

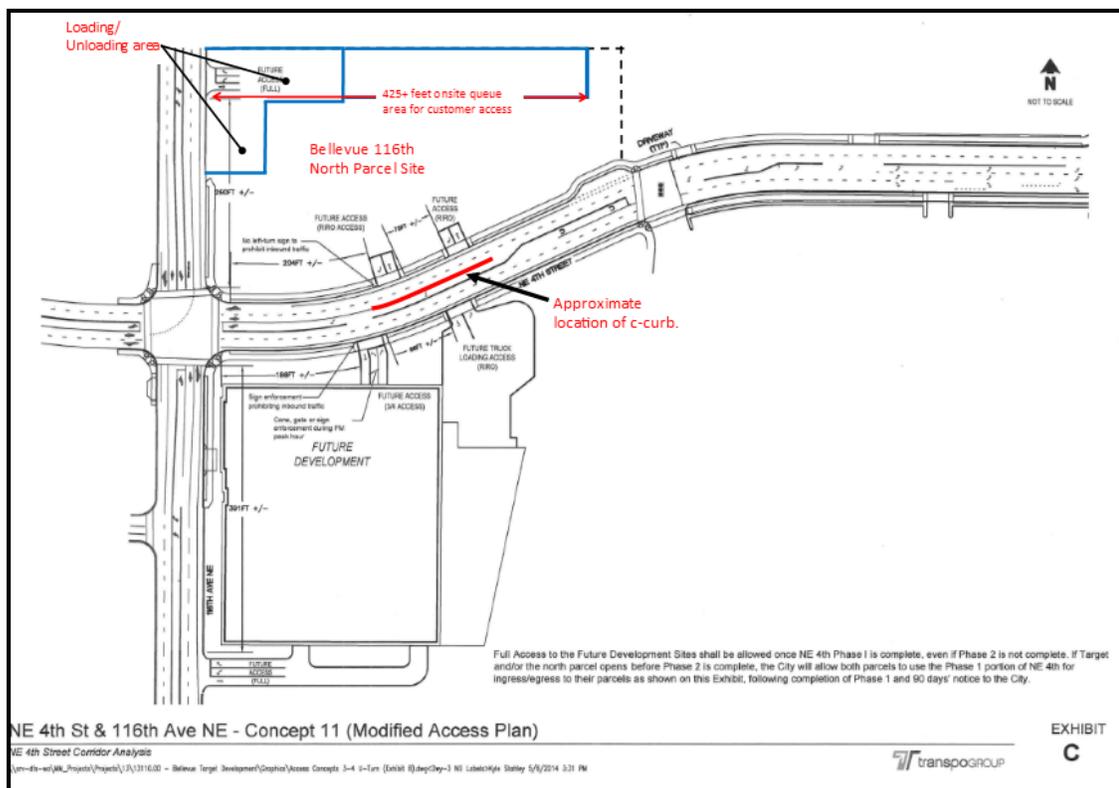
The driveways on NE 4<sup>th</sup> Street will provide access for customers only and will be restricted to right-in / right-out only. The applicant will be responsible for installing curb and associated pavement markings to enforce these restrictions. Driveway dimensions (30 foot widths, appropriate landing grades) have been designed to maximize vehicular and pedestrian sight distance and safe ingress / egress operations at the driveway locations. Driveway throat length and onsite parking configuration have been designed to maximize on site queuing areas. These improvements will significantly mitigate for traffic impacts to through traffic (westbound) on NE 4<sup>th</sup> Street from this project.

The driveway on 116<sup>th</sup> Avenue NE will provide access with no turning restrictions for customers and delivery trucks. This driveway will contain separate exit lanes for left and right turns and a 425 foot long queuing lane (contained entirely onsite). The

unloading / loading area has been designed so that delivery trucks can maneuver totally onsite as well. Driveway dimensions (36 foot width, appropriate landing grades) have been designed to maximize vehicular and pedestrian sight distance and safe ingress / egress operations at the driveway locations. These improvements will significantly mitigate for traffic impacts to through traffic (north and southbound) on 116<sup>th</sup> Avenue NE from this project. **Refer to Condition of Approval regarding provisions for loading in Section XI of this report.**

All access locations are in compliance with Exhibit C of the Settlement Agreement. These locations (and restrictions on NE 4<sup>th</sup> Street) were chosen by City staff to mitigate for potential traffic impacts to NE 4<sup>th</sup> Street from adjacent development to the most feasible extent. Refer to drawings on the following page for a summary site plan of mitigation.

The building and associated landscaping features at the southwest corner of the intersection of NE 4<sup>th</sup> Street and 116<sup>th</sup> Avenue NE have been designed so that intersection sight distance (both for vehicles and pedestrians) is maximized and in compliance with AASHTO standards. The applicant will be required to fund any needed changes to the cycle timing at the intersection to mitigate for traffic impacts from this development.



Summary site plan for mitigation & Access Locations per Exhibit C of the Settlement Agreement (identifiers added)

Detailed diagrams showing modeled trip distribution as well as other pertinent results of the short-term traffic analysis are published in the Bellevue 116<sup>th</sup> North Retail Transportation Impact Study, March 2015, by TranspoGroup. The study is included in the Transportation Department file for this development.

## VIII. CHANGES TO PROPOSAL DUE TO CITY REVIEW

### Site and Building Design

- Continuous weather protection was provided along 116<sup>th</sup> Avenue NE.
- A stairway and more pedestrian friendly path along the southern façade was provided to provide a weather-protected pathway to the building entries on the eastern side of the building. Seating, landscaped planters, and resting places were included.
- The corner open space feature and building corner were enhanced to create a stronger focal point and provide a pedestrian friendly environment at the intersection.
- Trellises were placed on the parking structure to partially screen cars from the future BNSF trail.
- The parking structure was pulled back further along the eastern property line to allow for planting of trees to provide screening from the future BNSF trail.

## IX. DECISION CRITERIA

Below is a discussion of how the proposal has met the decision criteria for the Design Review request.

### A. Design Review: The Director may approve, or approve with modifications, an application for Design Review under LUC 20.30F.145.A–E if:

#### 1. The proposal is consistent with the Comprehensive Plan.

This project is consistent with the Comprehensive Plan and it is in alignment with the City's commitment to maintain, strengthen, protect and enhance its existing properties and neighborhoods.

**Policy UD-1:** *Encourage high quality, attractive, architecturally appealing designs for major buildings in order to create distinctive visual reference points in the community.*

*and*

**Policy UD-2:** *Support designs for the building environment that are visually stimulating and thoughtful and which convey excellence in architecture and workmanship, and durability in building materials.*

The attractive design and use of urban, high-quality materials will result in a building that will be a visual reference point at this important intersection.

**Policy UD-3:** *Encourage a variety of site and building designs which are compatible and consistent with surrounding development and that implement the policies of this plan.*

By introducing new materials and updated colors, the proposal project will provide a fresh, urban appearance to the buildings.

**Policy UD-11:** *Encourage architectural elements that provide for both rain cover and access to sunlight in pedestrian areas.*

Weather protection will be provided along all sides of the building and pedestrian spaces will be oriented to the south and west.

**Policy S-WI-2:** *Support the provision of commercial services in Wilburton that complement Downtown such as large retail and auto sales; mixed use*

*opportunities; and services that provide convenient shopping for the adjacent neighborhoods. Implement this through zoning and development regulations.*  
The proposed development will provide shopping opportunities near the Downtown and Wilburton neighborhoods and will help realize the vision of a “retail village” along 116<sup>th</sup> Avenue NE.

**Policy S-WI-48:** *Promote pedestrian amenities which include the provision of sidewalks, street trees, landscape strips, and bus passenger shelters. Building facades should have awnings, windows, offsets, and material texture or colors that add interest for the passing motorist and pedestrian. Signage should be integrated with building design.*

**Policy S-WI-3:** *Support the long term development of a “retail village” in the commercial area on the west side of 120th Avenue to provide a transition from more intense commercial areas to the west and the residential area to the east. Designate the area west of 120th Avenue NE between NE 8th and about NE 4th Streets, and on the east side of 116th Avenue from about NE 4th to SE 1st Streets General Commercial/Community Business (GC/CB). Without access improvements, the area remains appropriate for General Commercial uses. As NE 4th Street (East Bellevue Transportation Plan project #582) is extended to increase access to the area, Community Business uses are appropriate.*  
The site was rezoned in 2014 from GC to CB and the rights for the entire NE 4<sup>th</sup> extension have been secured. This proposal will be able provide community businesses anticipated for the “retail village.”

**Policy S-WI-49:** *Allow flexibility for commercial buildings to be sited near frontage property lines.*

The southern and western facades of the building were sited near the frontage property lines of NE 4<sup>th</sup> Street and 116<sup>th</sup> Avenues respectively and parking was placed behind the building.

**Policy S-WI-51:** *Encourage buildings developed in the Auto Row area to enhance the area’s urban design character and pedestrian environment. Buildings should be sited near the street front, with limited intervening surface parking, and include street oriented pedestrian entrances. Where buildings are visible from streets and pedestrian corridors, they should be designed with visual interest and landscaping.*

*and*

**Policy S-WI-48:** *Promote pedestrian amenities which include the provision of sidewalks, street trees, landscape strips, and bus passenger shelters. Building facades should have awnings, windows, offsets, and material texture or colors that add interest for the passing motorist and pedestrian. Signage should be integrated with building design.*

*and*

**Policy S-WI-52:** *Provide aesthetic and landscaping enhancements on 116<sup>th</sup> Avenue consistent with its designation as a Boulevard.*

The buildings were placed along the two intersection street frontages. Although there will be no direct entrances to the retail establishments, generous pedestrian walkways that include weather protection, street trees, generous sidewalk widths plantings, and display windows will help guide the pedestrian to the entries along the building’s eastern façade.

**Finding:** The proposed development is consistent with the Policies listed above. By applying the requirements of the Land Use Code and Community Retail Design Guidelines, the proposal will provide a development that will provide uses and amenities consistent with the vision of a “retail village.” **Refer to Section I.C and D of this report for discussion of the proposal’s site and building design and Section IV.A for discussion of the proposal’s conformance with the Community Retail Design Guidelines.**

2. **The proposal complies with the applicable requirements of this Code.**

**Finding:** The proposal complies with all applicable requirements of the Land Use Code. **Refer to Section III of this report for specific information on Land Use Code consistency.**

3. **The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent.**

**Finding:** As conditioned, the proposal complies with all applicable design guidelines and standards for the Community Retail Design District. **Refer to Section III.D and IV.A of this report for how this proposal has met these standards and guidelines.**

4. **The proposal is compatible with, and responds to, the existing or intended character, appearance, and quality of development and physical characteristics of the subject property and immediate vicinity.**

**Finding:** The proposed site development, streetscape improvements, and building design are compatible with surrounding neighborhood and the Comprehensive Plan vision for this location in the Wilburton/NE 8<sup>th</sup> Street subarea as a “retail village.”

5. **The proposal will be served by adequate public facilities including streets, fire protection, and utilities.**

**Finding:** All required public services and facilities are currently available to the site.

#### **B. Critical Areas Report Decision Criteria 20.25H.255 A**

1. **The modifications and performance standards included in the proposal lead to levels of protection of critical area functions and values at least as protective as application of the regulations and standards of this code;**

**Finding:** Per the information submitted for the Critical Areas Land Use Permit, the functions and values of the steep slope will be protected and enhanced with added stability via an engineered retaining wall for the parking structure and new native plantings consisting of tall native conifers, deciduous maples and native shrubs and groundcovers on a slope currently covered primarily with noxious plants.

2. **Adequate resources to ensure completion of any required mitigation and monitoring efforts;**

**Finding:** Adequate resources are in place to ensure completion of required mitigation. **Refer to Conditions of Approval regarding final landscape, maintenance and monitoring plan, and installation and maintenance assurance devices in Section XI of this report.**

3. **The modifications and performance standards included in the proposal are not detrimental to the functions and values of critical area and critical area buffers off-site;**

**Finding:** The proposed development is not detrimental to the functions and values of this man-made steep slope. Construction of the proposed parking structure will further stabilize the slope via an engineered retaining wall. Plantings on the slope will not only help to stabilize the slope but add a habitat using native and Northwest-adapted trees, shrubs and groundcovers to supplant noxious vegetation currently on the slope. **Refer to Condition of Approval regarding the maintenance and monitoring plan and maintenance assurance device in Section XI of this report.**

4. **The resulting development is compatible with other uses and development in the same land use district.**

**Finding:** The proposed development is compatible with the NE 4<sup>th</sup> Street extension to the south and other retail and service uses within the same land use district. It is also compatible with all adjacent sites – even those in different land use districts.

**C. Critical Areas: The Director may approve, or approve with modifications, an application for a Critical Areas Land Use Permit if (LUC 20.30P.140):**

1. **The proposal obtains all other permits required by the Land Use Code.**

**Finding:** As conditioned, the proposal will be required to obtain all necessary permits, including a Clearing and Grading Permit, to construct the proposal.

2. **The proposal utilizes to the maximum extent possible the best available construction, design and development techniques which result in the least impact on the critical area and critical area buffer.**

**Finding:** A thorough analysis of the slope and proposed construction techniques has been submitted and reviewed. As proposed, the applicant will use, through application of the City's development codes, the best possible construction, design and development techniques to build within the steep slope and match up with the new grades created by the NE 4<sup>th</sup> Street extension along the southern property line. Additional review of the construction plans will occur under both the Clearing and Grading Permit and the Building Permit applications.

3. **The proposal incorporates the performance standards of Part 20.25H to the maximum extent applicable.**

**Finding:** The proposal has addressed the performance standards of LUC 20.25H. **Refer to discussion in Section III.E of this report for how the proposal has met the steep slope performance standards.**

4. **The proposal will be served by adequate public facilities including street, fire protection, and utilities.**

**Finding:** The proposal site is currently served by adequate public facilities, including streets, fire protection and utilities.

5. **The proposal includes a mitigation or restoration plan consistent with the requirements of LUC 20.25H.210.**

**Finding:** A conceptual landscape plan for the area between the parking structure retaining wall and the eastern property line, along with supporting

geotechnical information, was included in this application. A Final Landscape and Irrigation Plan and a Final Mitigation Plan, which will include a maintenance and monitoring schedule, will be required as part of the Clearing and Grading Permit submittal. **Refer to discussion of tree removal in Section VII.B and Condition of Approval regarding the final landscape and irrigation plans, maintenance and monitoring plans, and installation and maintenance assurance devices in Section XI of this report.**

6. **The proposal complies with other applicable requirements of this code.**  
**Finding:** As described in this staff report, the proposal complies with all applicable requirements of the Critical Areas section of the Land Use Code – LUC 20.25H.

## X. DECISION

After conducting the various administrative reviews associated with the proposal, including applicable Land Use consistency, City Code & Standard compliance reviews and SEPA review, the Director does hereby **APPROVE WITH CONDITIONS** the subject proposal, which includes Design Review and SEPA threshold determination.

## XI. CONDITIONS OF APPROVAL:

### **Compliance with City Codes and Documents**

The applicant shall comply with all applicable Bellevue City Codes, Standards, and Ordinances, including, but not limited to the following:

<b>Applicable Codes, Standards and Ordinances</b>	<b>Contact Person</b>
Clearing & Grading Code – BCC 23.76	Savina Uzunow, 425-452-7860
Construction Codes – BCC Title 23	Bldg. Desk, 425-452-4121
Fire Code – BCC 23.11	Adrian Jones, 425-452-6032
Land Use Code – BCC Title 20	Sally Nichols, 425-452-2727
Environmental Procedures Code – BCC Title 22.02	Sally Nichols, 425-452-2727
Noise Control – BCC 9.18	Sally Nichols, 425-452-2727
Right of Way Use Code – BCC 14.30	Tim Stever, 425-452-4294
Sign Code – BCC Title 22	Sally Nichols, 425-452-2727
Transportation Code – BCC 14.60	Ray Godinez, 425-452-7915
Utility Code – BCC Title 24	Art Chi, 425-452-4119

The following conditions are imposed on the applicant under the authority referenced:

### A. **GENERAL CONDITIONS: The following conditions apply to all phases of development.**

#### 1. **Noise & Construction Hours**

The proposal will be subject to normal construction hours of 7 a.m. to 6 p.m., Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturdays, except for Federal holidays and as further defined by the Bellevue City Code. Proximity

to existing residential (hotel) uses will be given special consideration. Upon written request to DSD, work hours may be extended to 10:00 p.m. if the criteria for extension of work hours as stated in BCC 9.18 can be met and the appropriate mitigation employed.

AUTHORITY: BCC 9.18.020.C & 9.18.040  
REVIEWER: Sally Nichols, Land Use

**2. Settlement Agreement**

The applicant shall implement all terms agreed upon for the site in settlement of the Condemnation Action filed under King County Cause No. 12-2-19166-2 SEA as part of the approved project (also referred to as the Settlement Agreement in this report).

AUTHORITY: Settlement Agreement, City of Bellevue 116<sup>th</sup> Avenue LLC,  
King County Cause No. 12-2-19166-2 SEA (Resolution  
8740 and Clerk's Receiving No. 53162)  
REVIEWER: Sally Nichols, Land Use

**3. Use of Noise Abatement Technology**

The use of best available noise abatement technology consistent with feasibility is required during construction to mitigate construction noise impacts to surrounding uses.

AUTHORITY: BCC 9.18.020F  
REVIEWER: Sally Nichols, Land Use

**4. Provisions for Loading**

The property owner shall provide an off-street loading space which can access a public street. On-street loading and unloading will not be permitted.

AUTHORITY: LUC 20.20.590.K.4  
REVIEWER: Sally Nichols, Land Use  
Ray Godinez, Transportation

**5. Holiday Construction & Traffic Restrictions**

Construction activities such as hauling and lane closures between November 15<sup>th</sup> and January 5<sup>th</sup> will be allowed only between the hours of 10:00 pm and 6:00 am due to holiday traffic. The Transportation Department will be monitoring traffic and may modify this restriction accordingly.

AUTHORITY: BCC 14.30.060  
REVIEWER: Tim Stever, Right-of-Way/Transportation

**6. Vehicular Access Restrictions**

All access to this site from NE 4<sup>th</sup> Street will comply with the agreed terms per the Settlement Agreement. Access will be restricted to right-in / right-out only at both driveways.

AUTHORITY: BCC 14.60.150  
REVIEWER: Ray Godinez, Transportation

**7. Utilities**

Utility Department approval of this Design Review application is based on the conceptual design only. Changes to the site layout may be required to accommodate the utilities after utility engineering is approved.

The water, sewer, and storm drainage systems shall be designed per current City of Bellevue Utility Codes and Utility Engineering Standards. All design review, plan approval, and field inspection shall be performed under the Utility Developer Extension Agreements.

A water budget for all irrigation shall be submitted.

AUTHORITY: BCC 24.02, 24.04, 24.06  
REVIEWER: Art Chi, Utilities

**8. Fire**

- a) Demolition and construction shall conform to the requirements of International Fire Code Chapter 33.
- b) Provide automatic fire sprinklers throughout the building and parking garage designed per NFPA 13. (IFC 903)
- c) Provide a Fire Department connection at an approved location and within 100 feet of a fire hydrant. (IFC 503 & 912)
- d) Provide an Outside sprinkler control valve for the sprinkler system. (NFPA 13)
- e) Provide a fire alarm system that activates the occupant notification system. (IFC 907.2.7)
- f) Provide street access (doors and stairs) to both floors from 116th Ave NE. (IFC 503)

AUTHORITY: IFC 503.3 Bellevue Amended  
REVIEWER: Adrian Jones, Fire

**9. Solid Waste, Recycling, and Loading Area**

All pick-up functions will be handled on-site and all rights of way and public easements shall not be occupied by trash receptacles, dumpsters, recycling bins or other such items.

Receptacles may not be left out in the loading area, but must be returned to the building immediately after pick-up.

The loading dock doors shall remain closed at all times when loading functions are not being performed.

AUTHORITY: LUC 20.20.590 and 20.25I.040  
REVIEWER: Sally Nichols, Land Use

**10. Boundary Line Adjustment**

Prior to the issuance of **any** construction permits, the applicant shall submit and receive City approval of a Boundary Line Adjustment (LW Permit) to combine the two existing parcels to create one project site.

AUTHORITY: LUC 20.45B.260  
REVIEWER: Sally Nichols, Land Use

**C. PRIOR TO CLEARING & GRADING PERMIT: These conditions must be complied with on plans submitted with the Clearing & Grading or Demolition permit application:**

**11. Right-of-Way Use Permit**

Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

- a) Designated truck hauling routes.
- b) Truck loading/unloading activities.
- c) Location of construction fences.
- d) Hours of construction and hauling.
- e) Requirements for leasing of right of way or pedestrian easements.
- f) Provisions for street sweeping, excavation and construction.
- g) Location of construction signing and pedestrian detour routes.
- h) All other construction activities as they affect the public street system.
- i) All other construction activities as they affect the city's CIP PWR-160 project.

In addition, the applicant shall submit for review and approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevents access. General materials storage and contractor convenience are not reasons for preventing access.

The applicant shall secure sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.

AUTHORITY: BCC 11.70 & 14.30  
REVIEWER: Tim Stever, Right-of-Way/Transportation

**12. Civil Engineering Plans - Transportation**

Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all street frontage improvements and driveway accesses must be in conformance with the requirements of the Americans with Disabilities Act, the Transportation Development Code, the provisions of the Transportation Department Design Manual, and specific requirements stated elsewhere in this document. All relevant standard drawings from the Transportation Department Design Manual shall be copied exactly into the final engineering plans. Requirements for the engineering plans include, but are not limited to:

- a) Traffic signs and markings.
- b) Curb, gutter, sidewalk, and driveway approach design. The engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as needed.
- c) ADA ramps, crosswalk revisions, and crosswalk equipment such as pushbuttons.
- d) Installation or relocation of streetlights and related equipment.
- e) Sight distance. (Show the required sight triangles and include any sight obstructions, including those off-site.)
- f) Relocation of fixed objects in the sidewalk or near the driveway approach.
- g) Trench restoration within any right of way or access easement.

Specific requirements are detailed below:

- h) Sidewalk and planter strip widths will vary on 116<sup>th</sup> Avenue NE but their combined width cannot be less than 16 feet. Minimum sidewalk width is eight feet.
- i) Irrigation system for the required street trees and landscaping.
- j) Channelization for five foot wide bike lanes along east side of 116<sup>th</sup> Avenue NE.
- k) Relocation of street lighting (as needed due to new driveway locations) along 116<sup>th</sup> Avenue NE and NE 4<sup>th</sup> Street.
- l) Undergrounding of all existing overhead power lines. All new power to the site must be routed underground as well.
- m) ADA ramps at the intersection of NE 4<sup>th</sup> Street and 116<sup>th</sup> Avenue NE (beyond the project limits of CIP PWR-160).
- n) Driveway approaches per DEV-7A (or acceptable modification) with appropriate channelization and signing enforcing driveway restrictions.
- o) Signal and equipment modifications at the intersection of NE 4<sup>th</sup> Street and 116<sup>th</sup> Avenue NE. Street light and traffic signal modifications, if required, must be completed prior to occupancy.
- p) Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.

Miscellaneous:

- q) Maximum driveway approach widths on NE 4<sup>th</sup> Street is 30 feet. Maximum driveway approach width on 116<sup>th</sup> Avenue NE is 36 feet. All driveway approaches shall be construction per Transportation Design Manual drawing DEV-7A.
- r) Landings on sloping approaches are not to exceed a 7% slope for a distance of 30 feet approaching the back edge of sidewalks. Driveway grades must be designed to prevent vehicles from bottoming out due to abrupt changes in grade.
- s) Vehicle and pedestrian sight distance must be provided per BCC 14.60.240 and 14.60.241. Sight distance triangles must be shown at all driveway locations and must consider all fixed objects and mature

landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance.

AUTHORITY: Transportation Department Design Manual Sections 21 & 22, drawings TE-1 & TE-3  
REVIEWER: Ray Godinez, Transportation

**13. Sidewalk and Planting Strip on 116<sup>th</sup> Avenue NE**

The sidewalk along 116<sup>th</sup> Avenue NE shall have a minimum width of **ten feet** and the continuous planting strip located between the curb and sidewalk shall have a minimum width of **seven feet**. The dimensions of all site work, including the sidewalk and planting areas, shall be included in and shown on the drawings in the Clearing and Grading permit submittal.

AUTHORITY: LUC 20.25I  
REVIEWER: Sally Nichols, Land Use

**14. Internal Walkways**

Internal walkways connecting the retail entries from NE 4<sup>th</sup> Street and from 116<sup>th</sup> Avenue NE shall be minimum of **six feet** wide and shall be differentiated from vehicular driving surfaces.

AUTHORITY: LUC 20.25I.050  
REVIEWER: Sally Nichols, Land Use

**15. Bicycle Parking**

The applicant shall provide bike racks for a minimum of 20 bicycles. The exact location of these racks shall be provided on the Clearing and Grading Permit drawings and reviewed under this Permit.

AUTHORITY: LUC 20.25I.050.D.3  
REVIEWER: Sally Nichols, Land Use

**16. Maintenance and Monitoring Plan**

The applicant shall hire a qualified professional to design and implement a Maintenance and Monitoring Plan for the planting area along the entire eastern property line – from the property line to the proposed retaining/building wall. **The Maintenance and Monitoring Plan must be reviewed and approved prior to TCO.** The applicant shall submit documentation each year, including photos from designated photo points, for five years to the Land Use Division under this Critical Areas Land Use Permit #14-147270-LO to demonstrate compliance with the conditions of this report. The monitoring may be discontinued after three years if, in the opinion of the Department, the long-term success is assured. The following schedule and performance standards apply and are evaluated in the report each year:

- 1) Year 1 (from date of plant installation)
  - 100% survival of all planted species in the enhancement area at the end of the first year.
  - 0% coverage of invasive plants in the planting area

- 2) Year 2 (from date of plant installation)
  - At least 90% survival of all installed material & 100% survival of all trees
  - Less than 5% coverage of planting area by invasive species
- 3) Years 3, 4, and 5 (from date of plant installation)
  - At least 80% survival of all installed material and 100 % survival rate for all trees
  - Exotic/invasive species will be maintained at levels below 5% total cover in all planted areas.

In addition, a Land Use inspection will be required to inspect initial installation and maintenance of the irrigation and landscape.

The necessary documentation outlined in the Maintenance and Monitoring Plan shall be sent either by mail or electronically each year to the following:

City of Bellevue  
Development Services Department/Land Use Division  
c/o Sally Nichols, Land Use Planner  
P.O. Box 90012  
Bellevue, WA 98009-9012  
*or*  
spnichols@bellevuewa.gov

AUTHORITY: LUC 20.25H.220  
REVIEWER: Sally Nichols, Land Use

#### 17. Final Landscape and Irrigation Plans

Final Landscape and Irrigation plans shall be submitted and approved under the Clearing and Grading permit application to ensure compliance with all Land Use Code requirements. The following items shall be reflected on the plans:

a) Right-of-Way:

Planting in the rights-of-way shall be done according to the Parks Department Best Management Practices and Design Standards in place at the time of construction.

A Parks Department representative shall be on-site to inspect all street trees planting soil, and plant material in the right-of-way planting strip prior to planting and at the time of planting to observe the installation. Contact Parks Contacts listed below at least 24 hours before planting to schedule the inspection.

Street trees on 116<sup>th</sup> Avenue NE shall be Platanus x acerifolia 'Bloodgood'. All street trees shall be installed with a caliper of 2-inches at time of planting.

Electrical connections for lighting in tree wells or planter strips may be allowed, if installed in compliance with the electrical code and subjected to an electrical inspection. As-built drawings that show the exact location of all electrical connections in tree wells shall be submitted to the Parks Department prior to TCO.

Parks Department Contacts:

- Tom Kuykendall, tkuykendall@bellevuewa.gov or (425) 452-7925; or
- Melissa Kerson, mkerson@bellevuewa.gov or (425) 45204100

b) Plant Material:

All plant material shall be reviewed for an appropriate mix of native and locally-adapted plant species. Final landscape and irrigation plans shall be reviewed under the Clearing and Grading permit.

c) Irrigation:

1. No drip irrigation will be allowed within any City right-of-way.
2. There shall be a minimum 4-inch diameter sleeves under all new sidewalks and driveways.
3. Schedule 40 irrigation pipe is required in the right-of-way.
4. The irrigation system for all landscaping within the City rights-of-way will be on a separate meter and controller which can be accessed 24 hours a day by the City of Bellevue staff. Coordinate the exact location and design with Parks Department staff prior to irrigation installation.

AUTHORITY: LUC 20.20.520 and BCC 24.02.205

REVIEWER: Sally Nichols, Land Use

**D. PRIOR TO ISSUANCE OF BUILDING PERMIT: Unless specified otherwise below, these conditions must be complied with on plans submitted with the Building Permit Application:**

**18. Transportation Impact Fee**

Payment of the traffic impact fee will be required at the time of building permit issuance. Impact fees are subject to change and the fee schedule in effect at the time of building. This project is eligible for an impact fee credit for the preexisting use of the project site.

AUTHORITY: BCC 22.16

REVIEWER: Ray Godinez, Transportation

**19. Building and Site Plans – Transportation**

The building grade and elevations shall be consistent with the curb and sidewalk grade shown in the approved civil engineering plans. During construction, city inspectors may require additional survey work at any time in order to confirm proper elevations. Building plans, landscaping plans, and architectural site plans must accommodate on-site traffic markings and signs and driveway design as specified in the engineering plans. Building plans, landscaping plans, and architectural site plans must comply with vehicle and pedestrian sight distance requirements, as shown on the engineering plans.

AUTHORITY: BCC 14.60.060, 110, 120, 150, 180, 181, 190, 240, 241

REVIEWER: Ray Godinez, Transportation

**20. Easements for Signal Control and Street Light Boxes and Vaults**

The applicant shall provide easements to the City for location of signal and street light facilities such as above-grade boxes and below-grade vaults between the building and sidewalk within the landscape area.

AUTHORITY: BCC 14.60.100  
REVIEWER: Ray Godinez, Transportation

**21. Pedestrian Easements**

The applicant shall provide sidewalk and utility easements to the City such that sidewalks outside of the City right of way along the property frontage are located within a pedestrian easement area.

AUTHORITY: BCC 14.60.100  
Reviewer: Ray Godinez, Transportation

**22. Transportation Management Program**

The owner of the property being developed shall sign and record at the King County Office of Records and Elections an agreement to establish a Transportation Management Program to the extent required by Section 14.60.070

AUTHORITY: BCC 14.60.070  
REVIEWER: Ray Godinez, Transportation

**23. Rooftop Mechanical Equipment**

All rooftop mechanical equipment shall be designed to be as visually intrusive as possible and shall be painted to match the color of the roof.

The installed mechanical units shall be reviewed during building permit review and again before TCO, when a decision shall be made whether addition screening will be required.

AUTHORITY: LUC 20.20.525 and 20.25I.050.D.1  
REVIEWER: Sally Nichols, Land Use

**24. Lighting**

To protect adjacent properties and vehicular traffic in the right-of-way, all exterior lighting fixtures shall utilize cutoff shield or other appropriate measures to conceal the light source. There shall be no light spillover glare beyond the site boundaries. The lighting in the parking garage shall utilize appropriate shielding to prevent light spillover.

Applicant shall submit manufacturers' cut-sheets/information for all exterior lighting fixtures to demonstrate that cutoff shields or other appropriate measures are being used to conceal the light source from adjacent properties and rights-of-way.

AUTHORITY: LUC 20.20.522  
REVIEWER: Sally Nichols, Land Use

**E. PRIOR TO ISSUANCE OF ANY CERTIFICATE OF OCCUPANCY: The following conditions are required by City Code and supported by City Policy and shall be complied with prior to issuance of the Temporary Certificate of Occupancy (TCO):**

**25. Street Frontage Improvements**

All street frontage improvements and other required transportation elements, including street light and traffic signal revisions, must be constructed by the applicant and accepted by the City Inspector. All existing street light and traffic signal apparatus affected by this development, including traffic controllers, pedestrian signal poles, traffic signal poles, and power sources, must be relocated as necessary. Existing overhead lines must be relocated underground. All required improvements must be constructed as per the approved plans or as per direction of the Transportation Department inspector. Bonding or other types of assurance devices will not be accepted in lieu of construction.

AUTHORITY: BCC 14.60; Comprehensive Plan Policy UT-39;  
Transportation Department Design Manual.  
REVIEWER: Ray Godinez, Transportation

**26. Pavement Restoration**

Pavement restoration associated with street frontage improvements or to repair damaged street surfaces shall be provided as follows:

- a) 116<sup>th</sup> Avenue NE: This street is classified as a “No Street Cuts Permitted,” street. The applicant must obtain permission to cut into this street from the city’s Right of Way Manager prior to any street cut. Minimum pavement restoration requirements will consist of a full grind and overlay extending 50 feet from the center of the trench in both directions for the full width of the street.
- b) NE 4<sup>th</sup> Street: The applicant will be responsible for any damage on NE 4<sup>th</sup> Street resulting from construction activities related to this proposal. Restoration requirements will be determined by the Right of Manager during right of permitting for this project.

AUTHORITY: BCC 14.60. 250  
REVIEWER: Tim Stever, Right-of-Way/Transportation

**27. Implement the Transportation Management Program**

The Transportation Management Program required by Bellevue City Code Section 14.60.070 per approval condition C.6 must be recorded and implemented prior to issuance of the initial certificate of occupancy.

AUTHORITY: BCC 14.60.070, 14.60.080  
REVIEWER: Ray Godinez, Transportation

**28. Landscape Installation Assurance Device**

All site landscaping shall be 100% complete per the plan approved by the City. Alternatively, the applicant shall submit the following: 1) a red-marked plan identifying which landscape areas are incomplete; 2) an estimate for the total

cost to complete these areas; and 3) a notarized Assignment of Savings dedicated to the City for 150% of the estimated cost to complete these areas per the approved Landscape Plan. The assurance device will be released upon complete installation and inspection approval by Land Use and Clearing and Grading.

AUTHORITY: LUC 20.40.490  
REVIEWER: Sally Nichols, Land Use

**29. Landscape Maintenance Assurance Device**

After final inspection of the landscape installation by Land Use and the Clearing and Grading inspector, a landscape maintenance assurance device must be filed with the Development Services Department for a one-year period in the form of an assignment of savings or letter of credit for 20% of the cost of all labor and materials for all required landscaping including the right-of-way.

AUTHORITY: LUC 20.40.490  
REVIEWER: Sally Nichols, Land Use

**30. Maintenance Agreement with Parks Department/City of Bellevue**

After one-year, the landscape in public rights-of-way shall be inspected by Land Use and the Parks Department. Prior to the release of the Landscape Maintenance Assurance Device, the applicant and the City of Bellevue shall enter into an agreement to determine future maintenance responsibilities for the streetscape and streetscape plantings.

AUTHORITY: LUC 20.25A.060  
REVIEWER: Sally Nichols, Land Use

**E. PRIOR TO ISSUANCE OF SIGN PERMIT(S)**

**31. Signage and Individual Sign Permits**

a) This Design Review approval addresses the conceptual design of signage and does not include approval of any specific sign(s). Design review of each sign and compliance with the sign Code will occur through review of each sign permit application. The sign permit submittal package plans, elevations, and/or sketches shall include but are not limited to:

- Location
- Illumination
- Color and Materials
- Design (no cabinet or freestanding signs are permitted)

b) In addition to building signage, the applicant must provide wayfinding signage to highlight connections to the BNSF trail, transit, the Downtown, and adjacent neighborhoods. The design and review of these signs and their location(s) will occur under the sign permit application.

AUTHORITY: LUC 20.25A.110, LUC 20.30F, BCC 22B.10.025.B.4  
REVIEWER: Sally Nichols, Land Use

**Attachments**

- A. Project Plans
- B. SEPA Checklist
- C. Concurrency Certificate



**BELLEVUE NORTH**  
316 116TH AVE NE  
BELLEVUE, WA

ADMINISTRATIVE DESIGN REVIEW SUBMITTAL  
BELLEVUE PROJECT #14-143953-LD

**FULLER SEARS**  
ARCHITECTS



**BELLEVUE NORTH - BELLEVUE, WA**  
ADR SUBMITTAL

1411 Fourth Ave., Suite 1306  
Bellevue, WA 98005  
Tel: 206.882.9170

REVISED IN RESPONSE TO THE  
1-27-2015 CITY COMMENT LETTER

FEBRUARY 20, 2015



**VICINITY MAP**

**PROJECT ADDRESS:**  
 316 116TH AVE NE  
 BELLEVUE, WA 98004  
 (INTERSECTION OF 116TH AVE NE  
 AND NE 4TH STREET)

ASSESSOR'S #: 332505-9151 & 332505-9012

**PROJECT DESCRIPTION:**

THE PROJECT CONSISTS OF A 82,000 SF, TWO STORY RETAIL BUILDING. THE SITE WILL PROVIDE AT GRADE PARKING FOR 152 CARS AND ONE LEVEL OF STRUCTURED PARKING FOR 157 CARS, FOR A TOTAL OF 309 CARS.

**NOTE:**  
 THE DEVELOPMENT PLAN PRESENTED HERE IS CONSISTENT WITH THE ONE PRESENTED AND WORKED THROUGH TO CONCEPT APPROVAL WITH THE CITY DURING THE SETTLEMENT AGREEMENT PROCESS.

**PROJECT TEAM:**

**Owner :** KG INVESTMENT, 11225 SE 6th St #215, Bellevue, WA 98004, phone: 425 450 1550, contact: Bob Beebe, email: bbeebob@kginvestment.com

**Structural:** DCI Engineers, 818 Stewart St, #1000, Seattle, WA 98101, phone: 206 332 1900, contact: John Tessem, email: jtessem@dc-engineers.com

**Architect:** FullerSears Architects, 1411 Fourth Avenue, Suite 1306, Seattle, WA 98101, phone: 206 682 6170, contact: Neil Heston, email: vestrup@fullersears.com

**Landscape:** Weisman Design Group, 2329 E Madison St, Seattle, WA 98112, phone: 206 322 1732, contact: Andy Nasmussen, email: andy@wdgnc.com

**General**

**Contractor:** Bayley Construction, 8005 SE 28th St, Mercer Island, WA 98040, phone: 206 821 8884, contact: Jerry Crowell, email: jerry@bayley.net

**Civil:** Barghausen Engineering, 10100 1st Ave S, Kent, WA 98032, phone: 425 251 8223, contact: Jason Hubbell, email: jhubbell@barghausen.com

**SHEET INDEX**

- 1 PROJECT INFORMATION
- 2 SITE PLAN
- 3 SITE AND BUILDING SECTIONS
- 4 LANDSCAPE RENDERING
- 5 LANDSCAPE PLAN
- 6 PLANT SCHEDULE
- 7 LANDSCAPE SECTIONS
- 8 LANDSCAPE/PERVIOUS CALCULATIONS
- 9 LEVEL 1 PLAN
- 10 LEVEL 2 PLAN
- 11 ROOF PLAN
- 12 SITE PHOTOS
- 13 NORTH & EAST ELEVATIONS
- 14 SOUTH & WEST ELEVATIONS
- 15 BUILDING SECTIONS
- 16 SW CORNER PLAZA & LANDSCAPED TERRACE VIEWS
- 17 SITE AERIALS & STREET VIEWS
- 18 VIEWS OF NORTH EDGE OF PROJECT
- 19 AERIAL VIEWS, PARKING SCREENING VIEWS
- 20 BUILDING MATERIALS
- 21 SITE SURVEY
- 22 PRELIMINARY SITE PLAN (CIVIL)
- 23 PRELIMINARY CLEARING AND GRADING
- TRUCK TURNING DIAGRAMS (3 SHEETS)

**PROJECT INFORMATION**

**FULLER SEARS ARCHITECTS** 1411 Fourth Ave., Suite 1306, Seattle, WA 98101, Tel. 206.682.6170

**KG INVESTMENT MANAGEMENT** BELLEVUE NORTH - BELLEVUE, WA ADR SUBMITTAL

FEBRUARY 20, 2015 1

SITE AREA: 137,420 SF

BUILDING AREA:  
FIRST FLOOR RETAIL 34,000 SF  
MECH RM & LOADING 8,000 SF  
SECOND FLOOR RETAIL 40,000 SF  
TOTAL 82,000 SF

ZONING INFORMATION:  
ZONE: CB

MAX HEIGHT: 75'  
PARKING REQUIRED: MIN (RETAIL) 4/1000, (286)  
MAX 4.5/1000, (333)

SETBACKS: NONE

LANDSCAPE BUFFERS:

REQUIRED: 8' (INTERIOR PROP. BOUNDARIES),  
10' (STREET FRONTAGE)  
PROVIDED: 8' (INTERIOR PROP. LINE AT EAST),  
10' (ALONG 116TH & 4TH AVE),  
7' (INTERIOR PROP. LINE AT NORTH)

PERVIOUS/IMPERVIOUS: MIN. REQ'D - MUST MEET EXIST. PER  
SEC. 20.20.460F (EXIST. - 135,090 SF, 98.3% IMPERVIOUS)

PARKING PROVIDED:

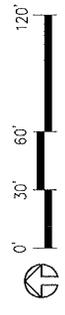
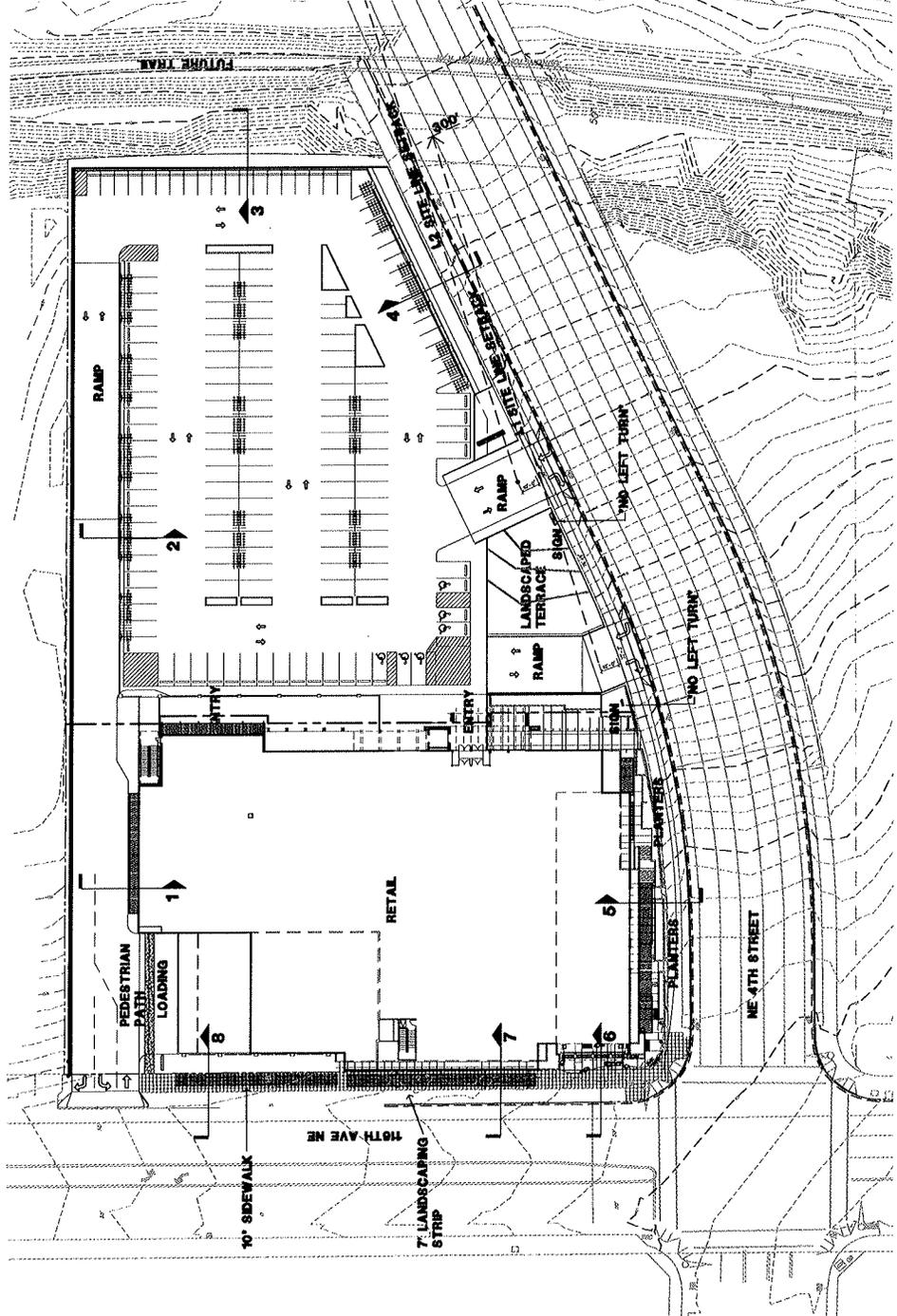
L-1: 152, L-2: 157  
237 STANDARD STALLS (8'-10" X 18')  
66 COMPACT STALLS (7'-5" X 13')  
12 HANDICAPPED STALLS  
309 TOTAL (41/1000)

CALCULATED AREAS OF THE REQUIRED LANDSCAPE BUFFERS:

8' NORTH PROPERTY BOUNDARY 3,958 SF  
8' EAST PROPERTY BOUNDARY 1,298 SF  
TOTAL 5,256 SF

CALCULATED AREAS FOR THE ALTERNATE TO THE REQUIRED  
LANDSCAPING PROVIDED (PER SEC. 20.20.520D):

LANDSCAPE TERRACE 2,704 SF  
SE ENTRY PLAZA 1,930 SF  
SW CORNER PLAZA 1,648 SF  
TOTAL 6,482 SF



**SITE PLAN**

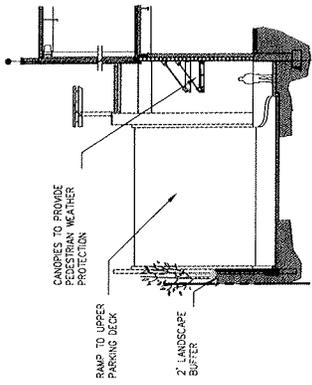
**FEBRUARY 20, 2015**

**BELLEVUE NORTH - BELLEVUE, WA**  
ADR SUBMITTAL

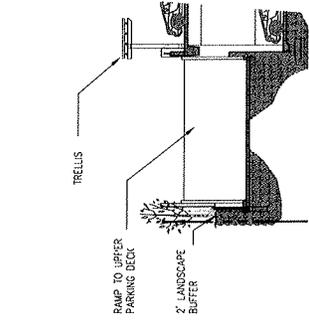


1411 Fourth Ave., Suite 1306  
Seattle, WA 98101  
Tel: 206.862.8170

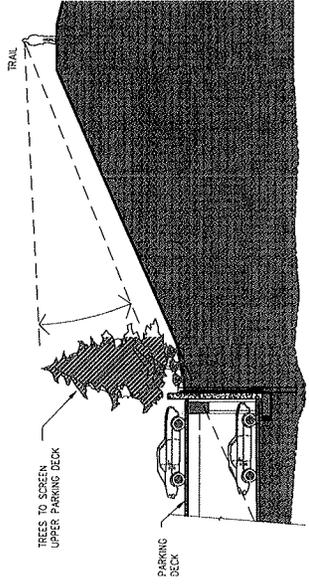
**FULLER SEARS**  
ARCHITECTS



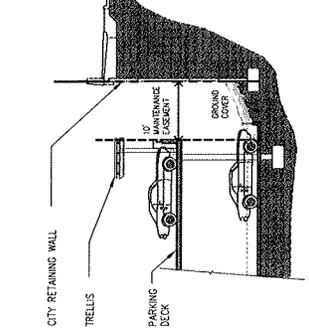
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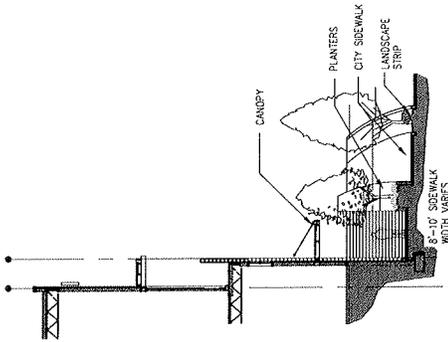
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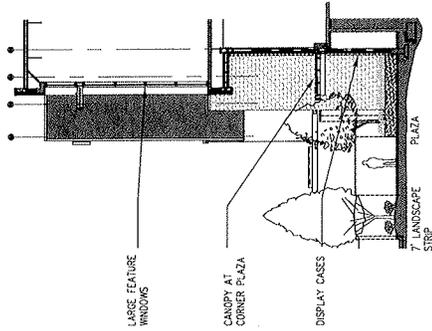
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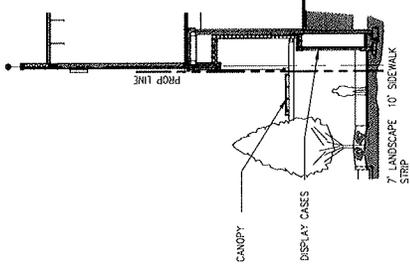
**4 - SECTION THRU PARKING AT NE 4th WALL**



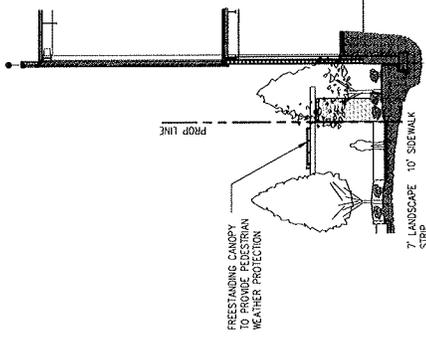
**5 - SECTION THRU PLANTERS ALONG NE 4TH STREET**



**6 - SECTION AT SW CORNER PLAZA**



**7 - SECTION THRU 116TH AVE SIDEWALK**



**8 - SECTION THRU FREESTANDING CANOPY ALONG 116TH AVE NE**



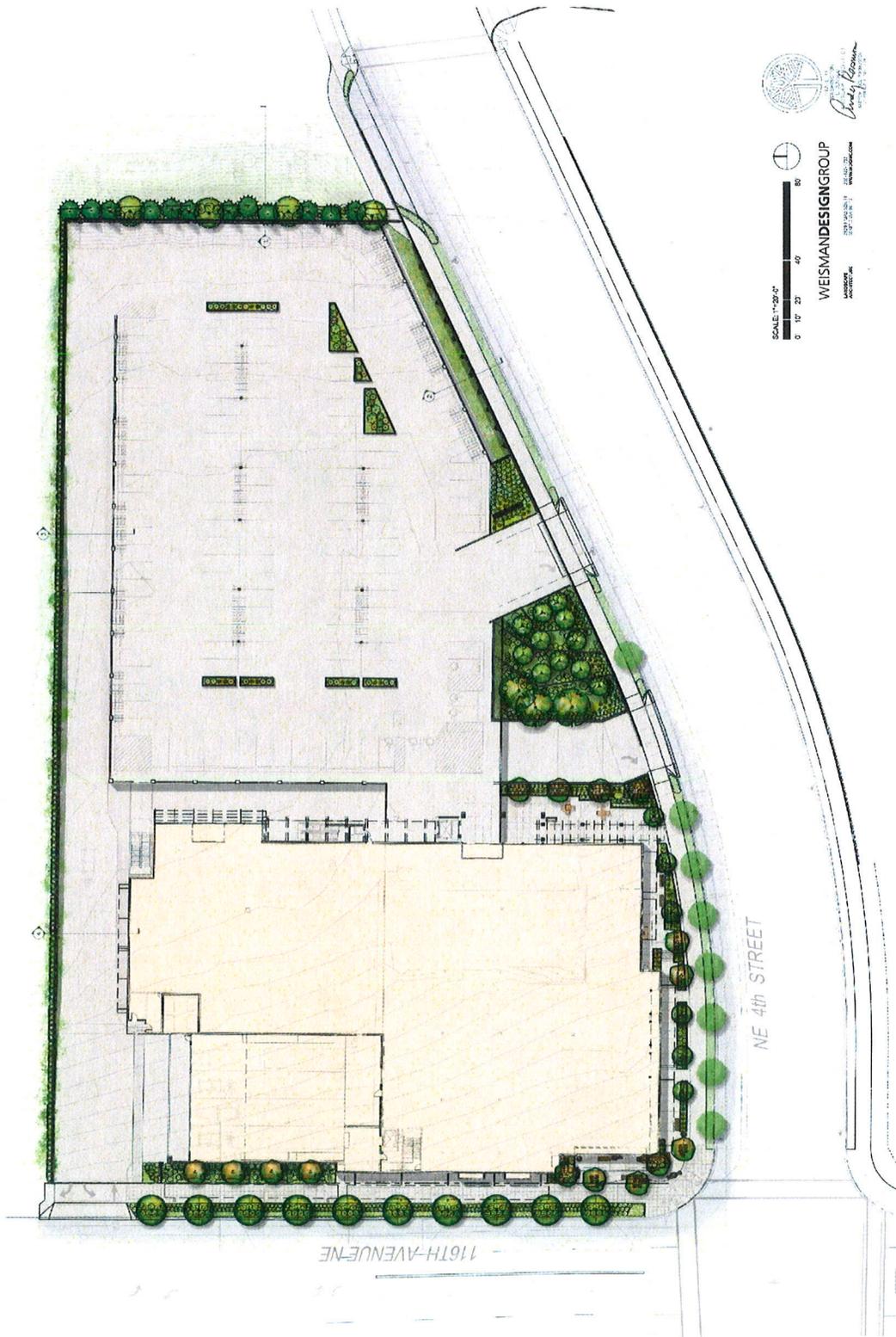
**SITE AND BUILDING SECTIONS**

FEBRUARY 20, 2016

**BELLEVUE NORTH - BELLEVUE, WA**  
ADR SUBMITTAL

**FULLER SEARS ARCHITECTS**  
1111 Fourth Ave., Suite 1306  
Seattle, WA 98101  
Tel. 206.682.6170

**KG**  
KIMLEY-HORN  
AND ASSOCIATES  
MANAGEMENT

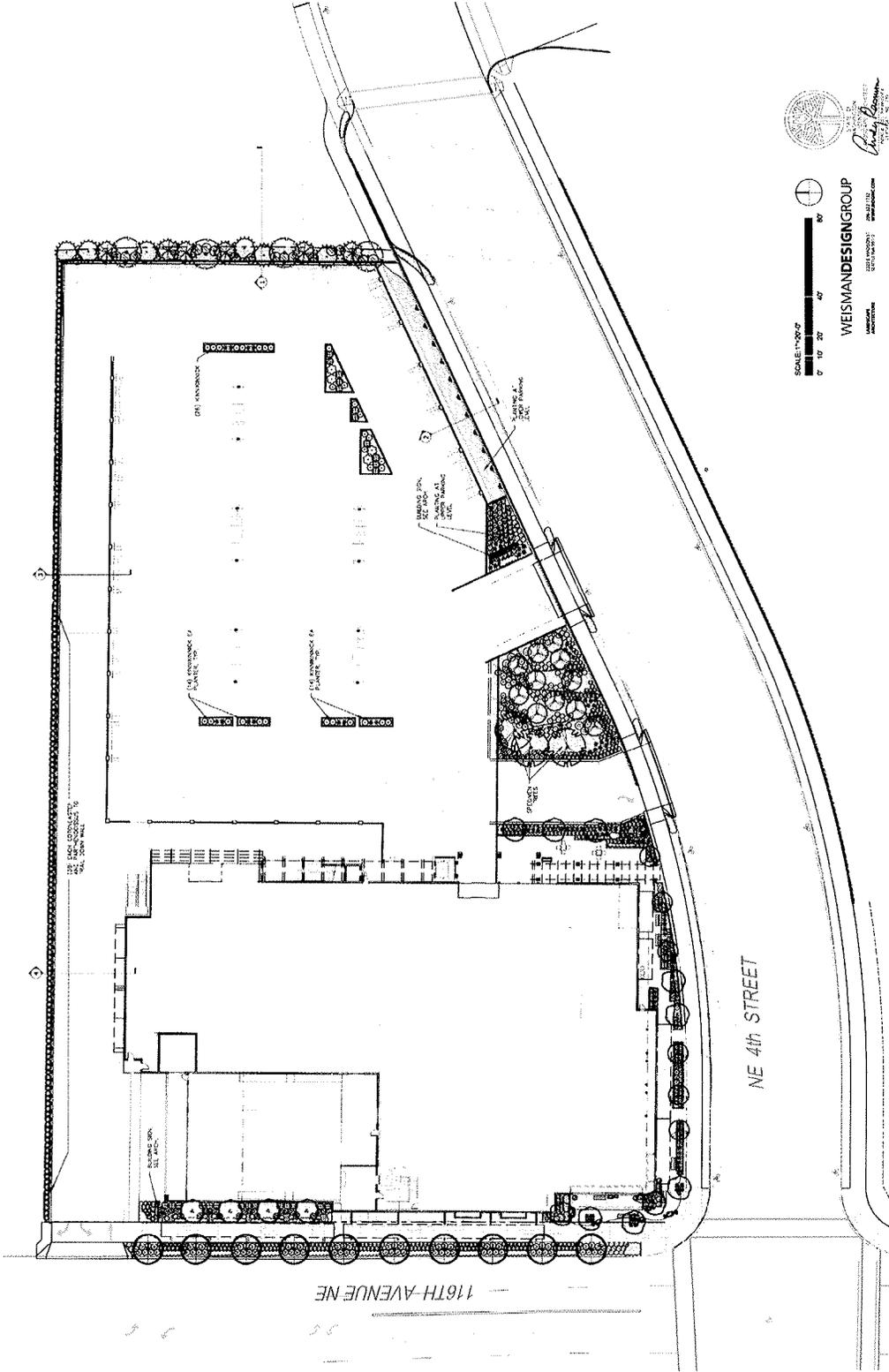


SCALE: 1"=20'-0"

0 10' 20' 40' 80'

WEISMANDESIGNGROUP  
 LANDSCAPE ARCHITECTS  
 10217 4th SW, #1  
 SEATTLE, WA 98148  
 WWW.WEISMANDSGROUP.COM

*Paula Roman*  
 Principal  
 2015.02.20

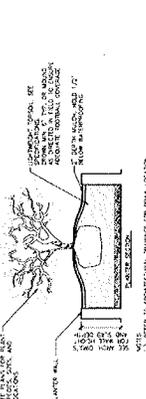
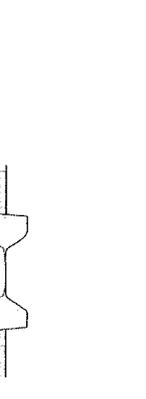
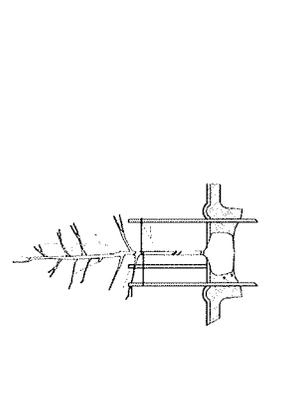
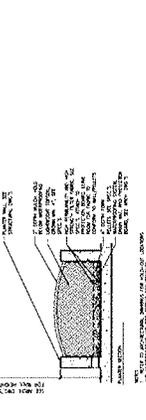
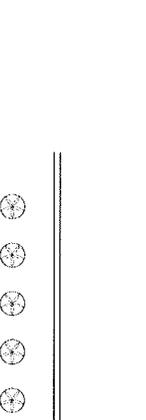
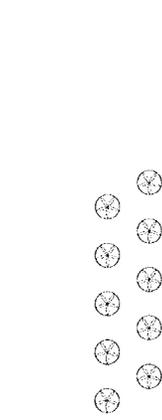
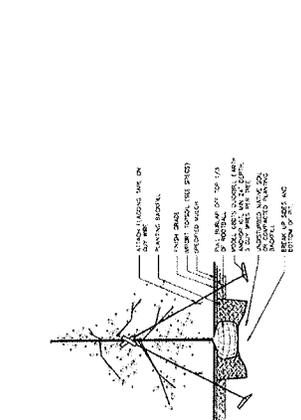
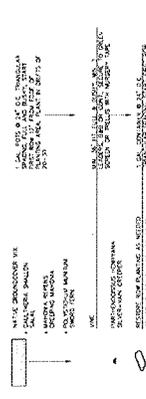
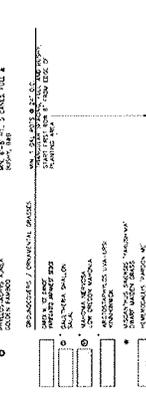
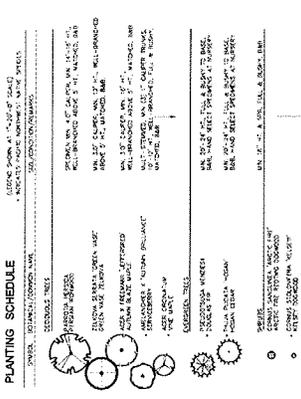


SCALE: 1"=20'-0"

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WEISMANDESIGNGROUP

1001 10th Avenue, Bellevue, WA 98004  
 TEL: 206.835.1100  
 WWW.WEISMANDESIGNGROUP.COM



**PLANTING SCHEDULE**  
 (SEE PLAN FOR LIST OF PLANTS)  
 (SEE PLAN FOR LIST OF PLANTS)  
 (SEE PLAN FOR LIST OF PLANTS)

**GENERAL NOTES:**  
 1. ALL PLANTS TO BE PLANTED IN ACCORDANCE WITH THE PLANTING SCHEDULE.  
 2. ALL PLANTS TO BE PLANTED IN ACCORDANCE WITH THE PLANTING SCHEDULE.  
 3. ALL PLANTS TO BE PLANTED IN ACCORDANCE WITH THE PLANTING SCHEDULE.

**PLANTING OVER STRUCTURE**  
 (SEE PLAN FOR LIST OF PLANTS)  
 (SEE PLAN FOR LIST OF PLANTS)  
 (SEE PLAN FOR LIST OF PLANTS)

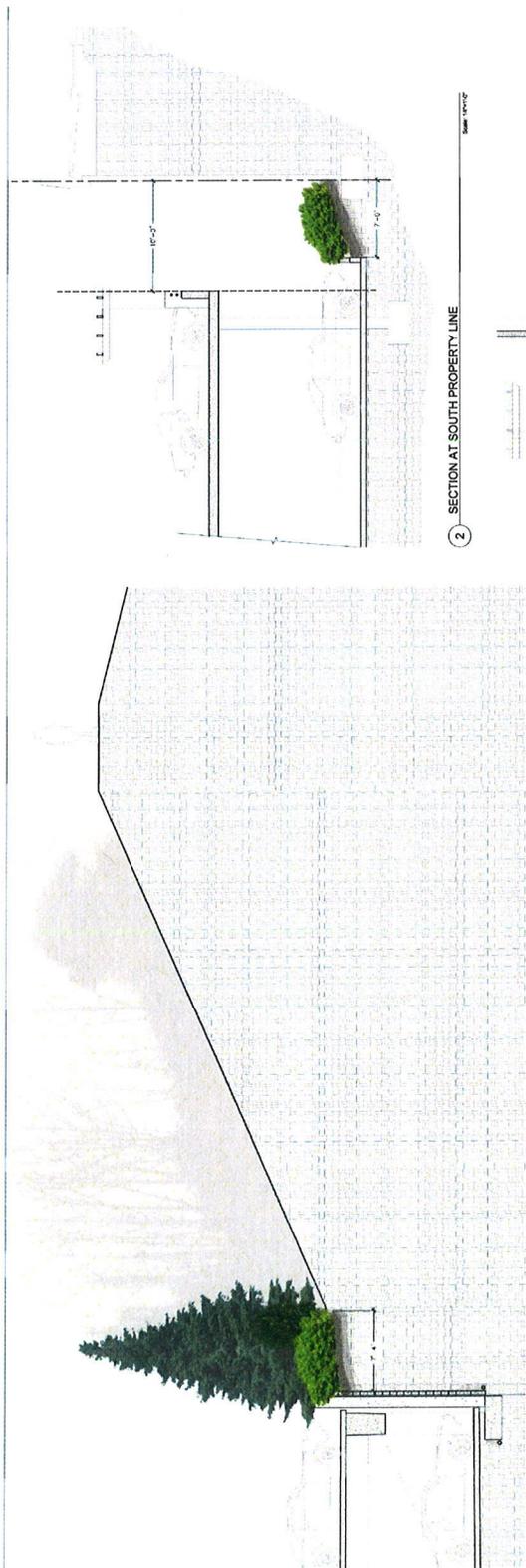
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**PLANTING SCHEDULE**  
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**GENERAL NOTES:**  
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**PLANTING OVER STRUCTURE**  
 (SEE PLAN FOR LIST OF PLANTS)  
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**GENERAL NOTES:**  
 1. ALL PLANTS TO BE PLANTED IN ACCORDANCE WITH THE PLANTING SCHEDULE.  
 2. ALL PLANTS TO BE PLANTED IN ACCORDANCE WITH THE PLANTING SCHEDULE.  
 3. ALL PLANTS TO BE PLANTED IN ACCORDANCE WITH THE PLANTING SCHEDULE.



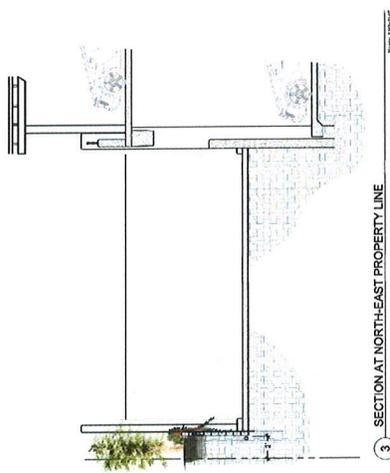
1 SECTION AT EAST PROPERTY LINE

Scale: 1/4" = 1'-0"



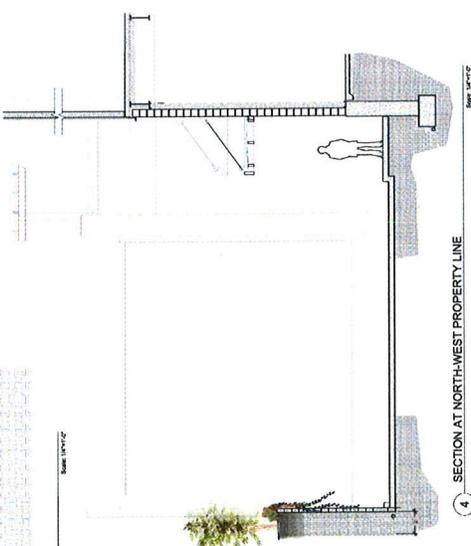
2 SECTION AT SOUTH PROPERTY LINE

Scale: 1/4" = 1'-0"



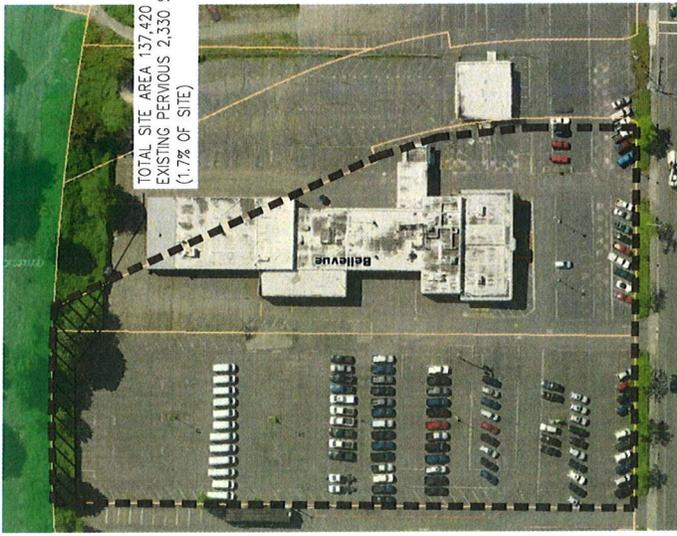
3 SECTION AT NORTH-EAST PROPERTY LINE

Scale: 1/4" = 1'-0"

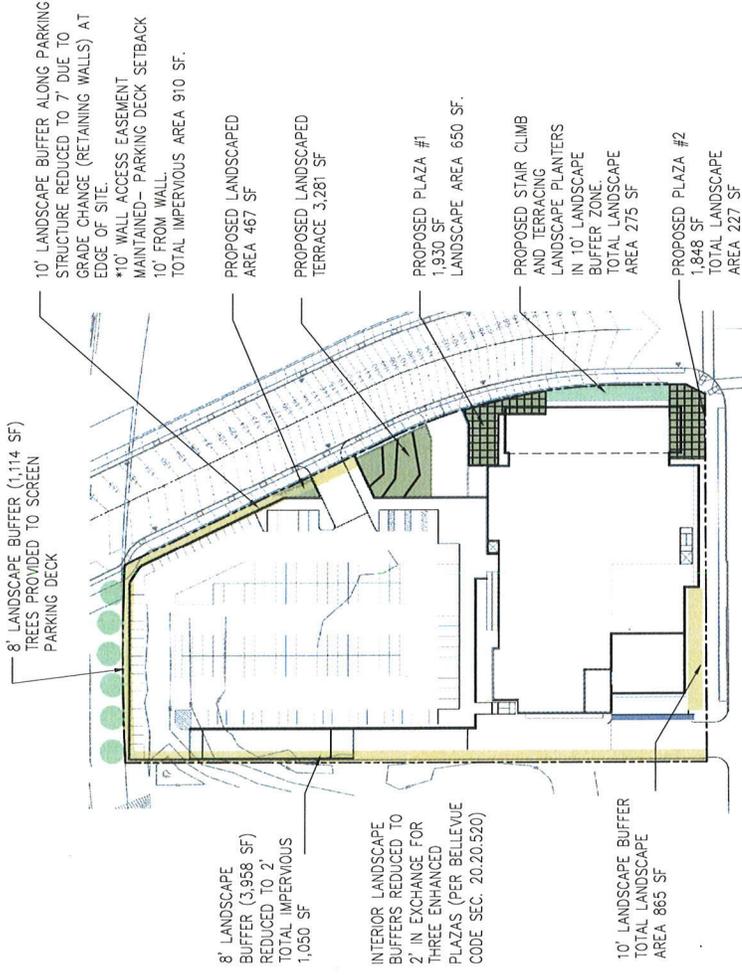


4 SECTION AT NORTH-WEST PROPERTY LINE

Scale: 1/4" = 1'-0"



**EXISTING IMPERVIOUS SURFACE**

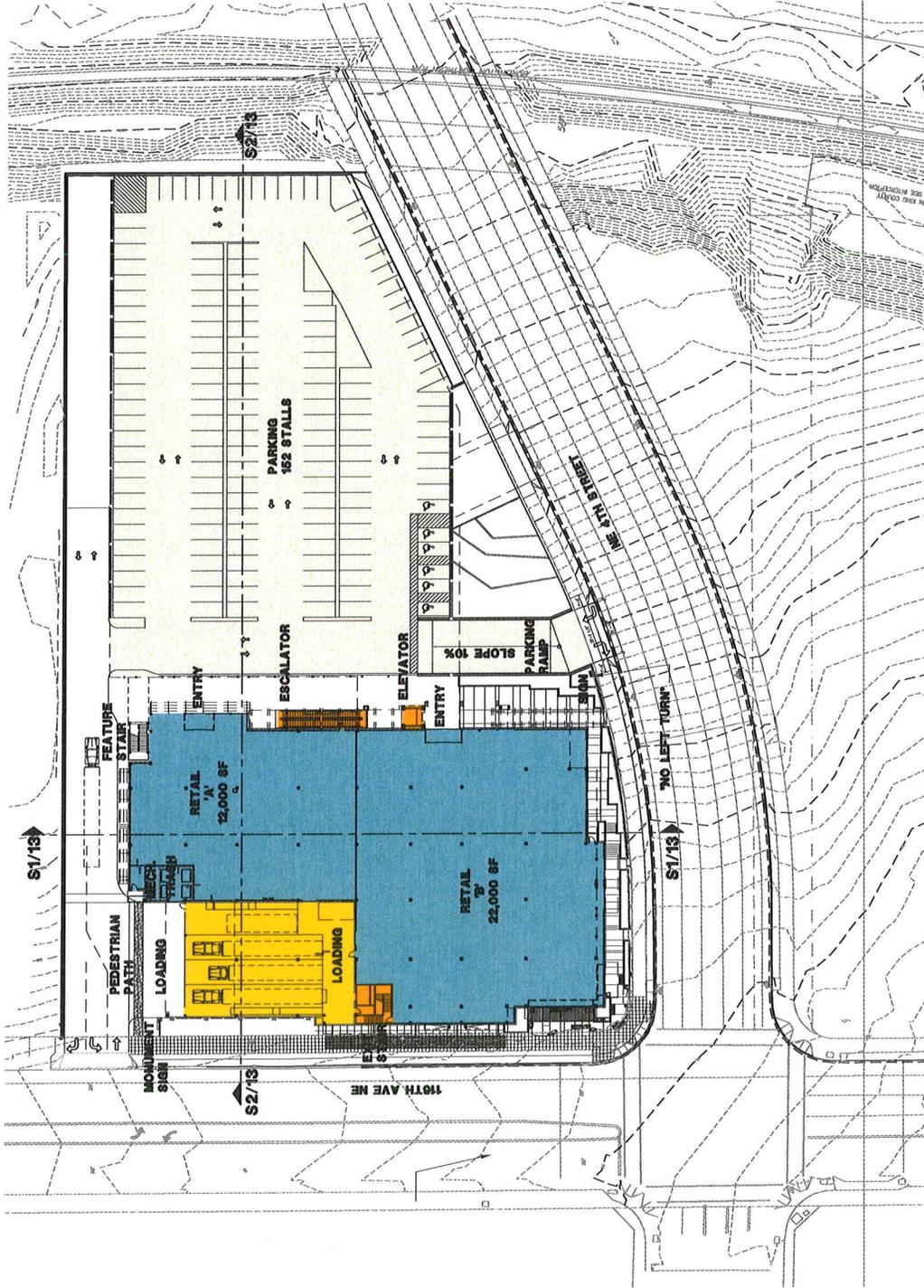


**PROPOSED IMPERVIOUS SURFACE**

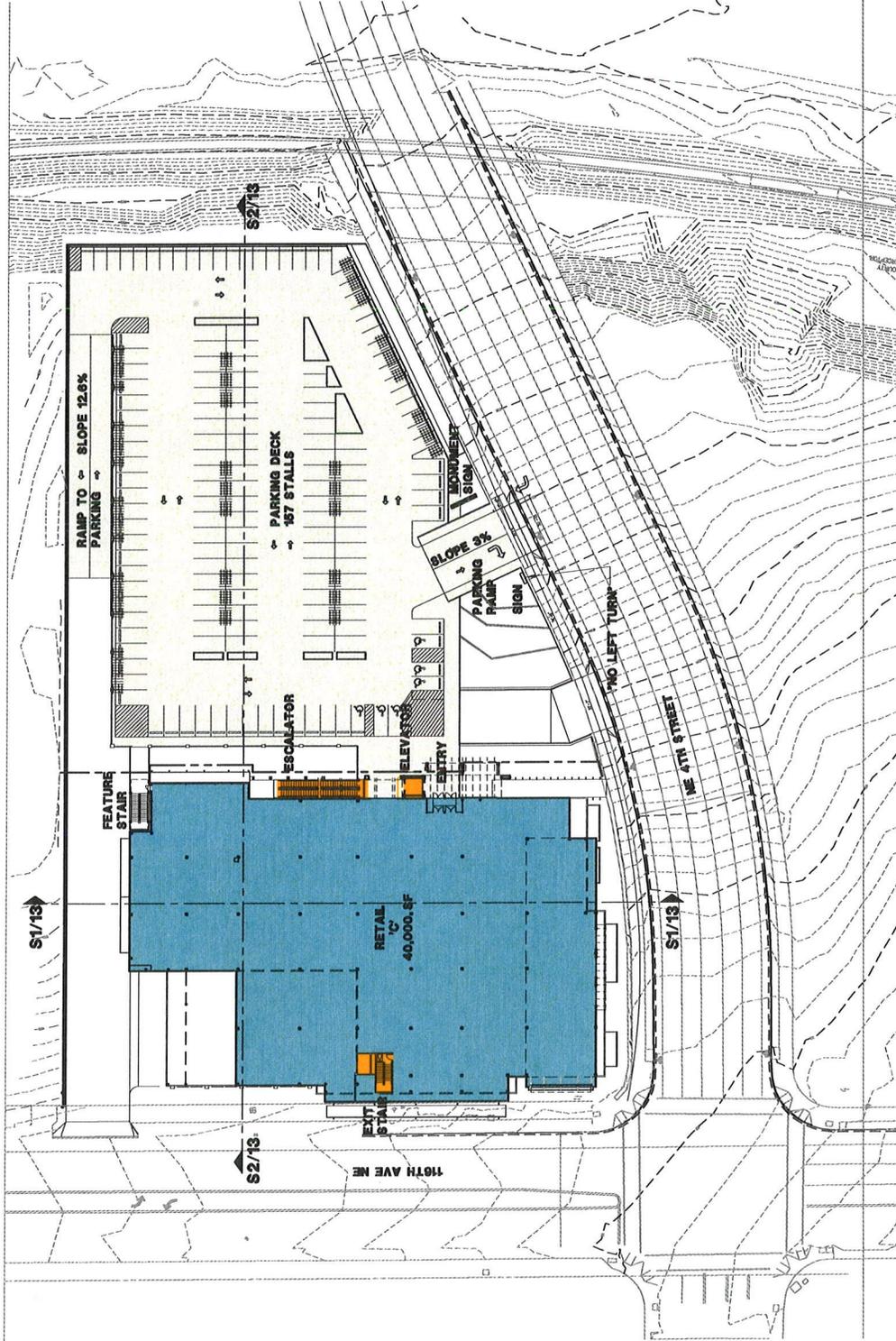
NEW IMPERVIOUS AREA:	
NORTH PROPERTY AREA	1,050 SF
EAST PROPERTY AREA	1,114 SF
SOUTH PROPERTY AREA	650+910+467+3,281+184+91 = 4,933 SF
WEST PROPERTY AREA	96+1,311+865 = 1,092 SF
<b>TOTAL</b>	<b>8,848 SF</b>
	(6.4 %)



**LANDSCAPE CALCULATIONS**



**LEVEL 1 PLAN**



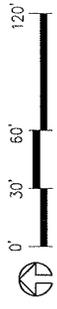
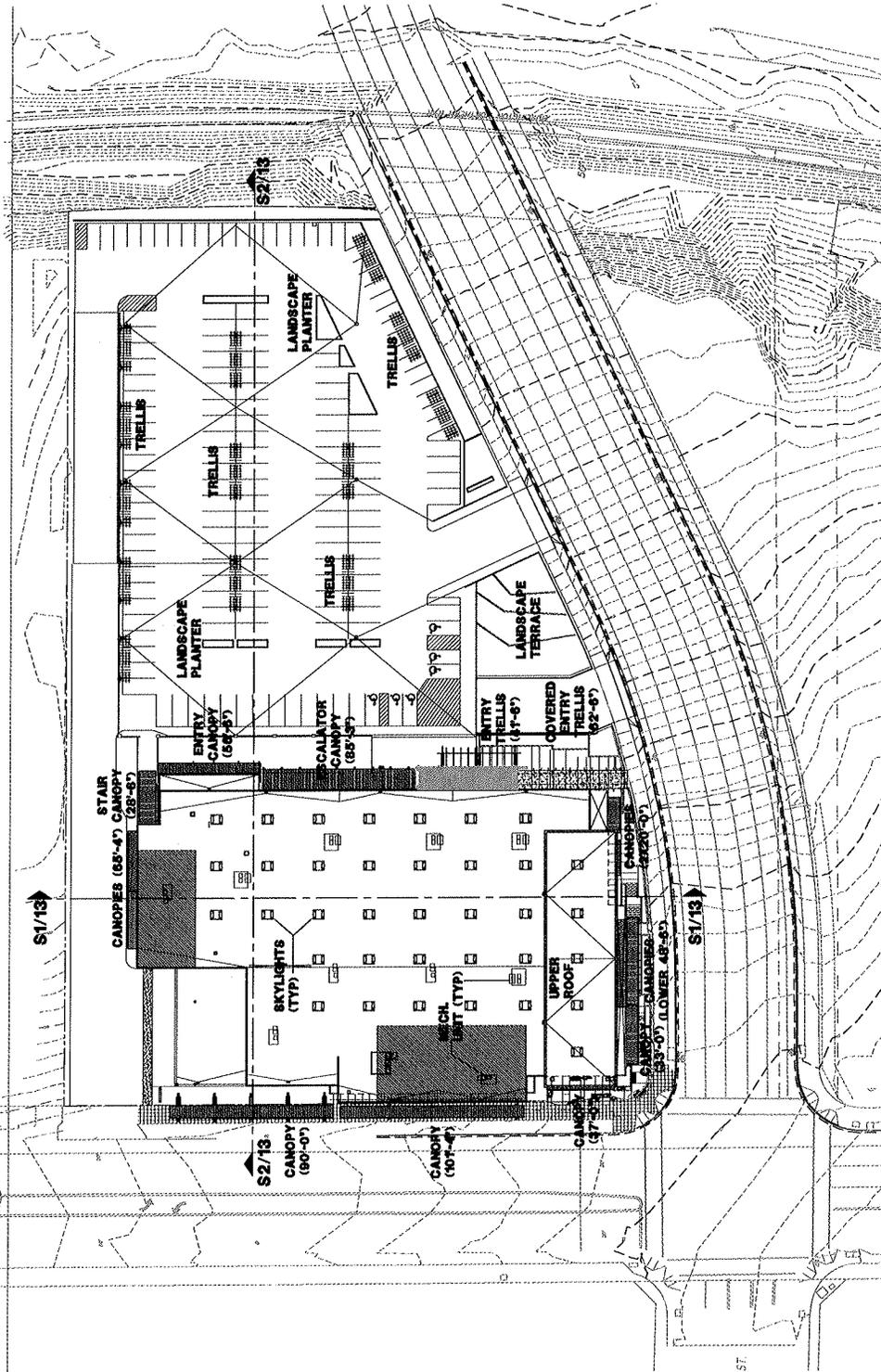
**LEVEL 2 PLAN**

FEBRUARY 20, 2015

**BELLEVUE NORTH - BELLEVUE, WA**  
ADR SUBMITTAL

**FULLER SEARS** ARCHITECTS  
1411 Fourth Ave., Suite 1305  
Seattle, WA 98101  
Tel. 206.682.6170





**ROOF PLAN**

FEBRUARY 20, 2016

**FULLER SEARS** 1411 Fourth Ave., Suite 1306  
 Seattle, WA 98101  
 Tel. 206.882.6170

**KG** BELLEVUE NORTH - BELLEVUE, WA  
 ARCHITECTS MANAGER  
 ADR SUBMITTAL



**1 - SITE PHOTO**



**2 - SITE PHOTO**



**3 - SITE PHOTO**



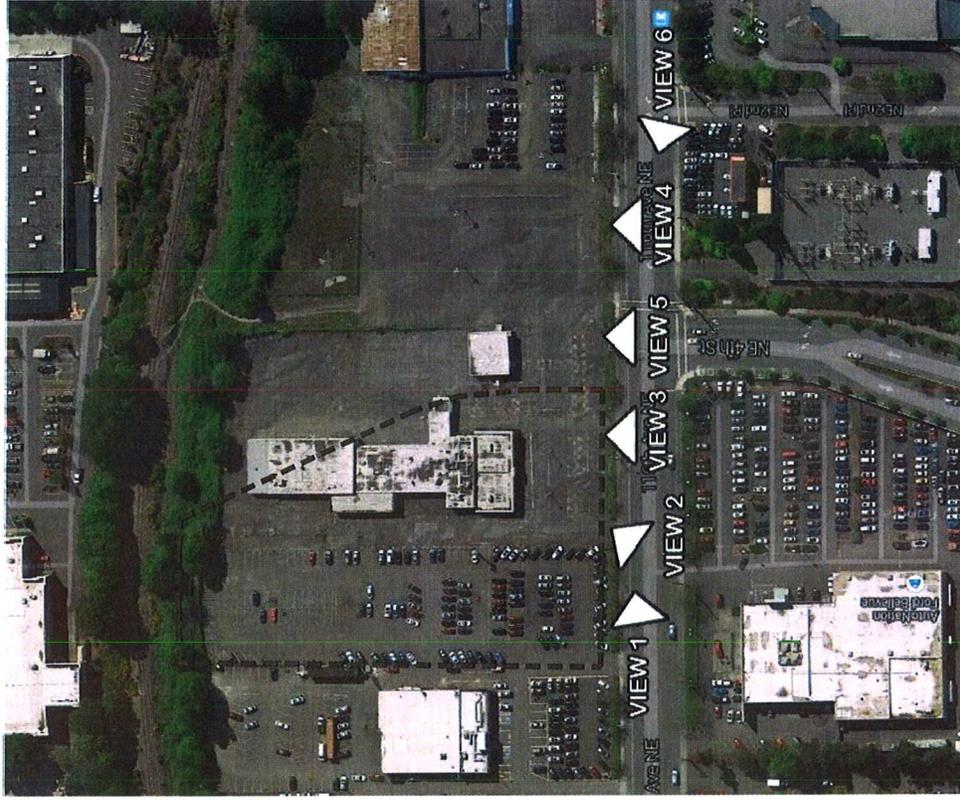
**4 - SITE PHOTO**



**5 - SITE PHOTO**



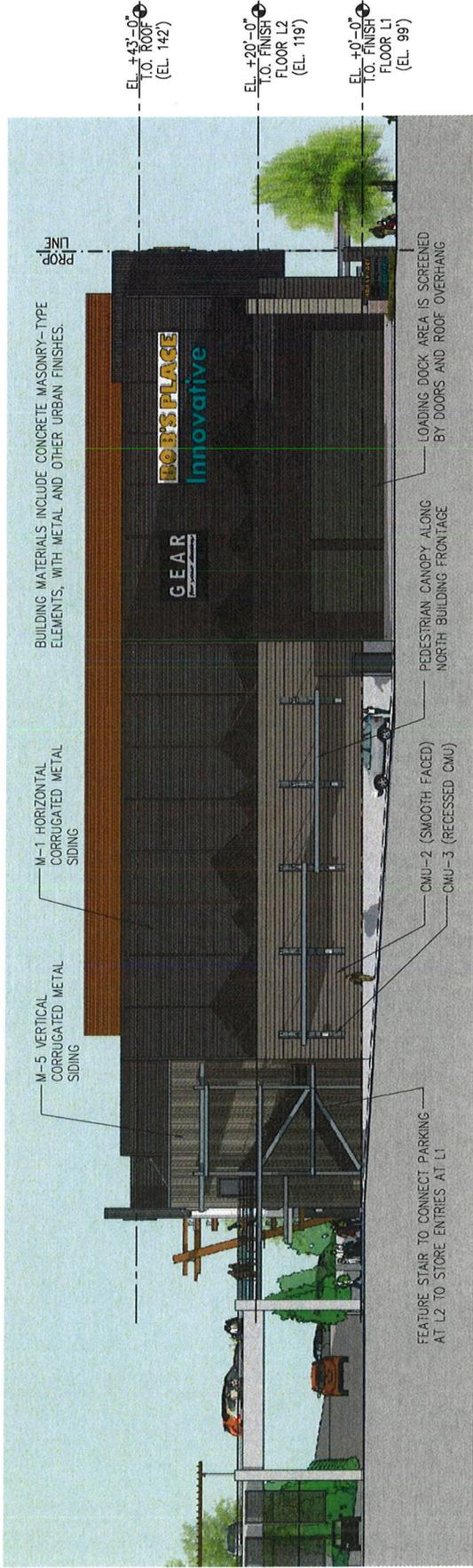
**6 - SITE PHOTO**



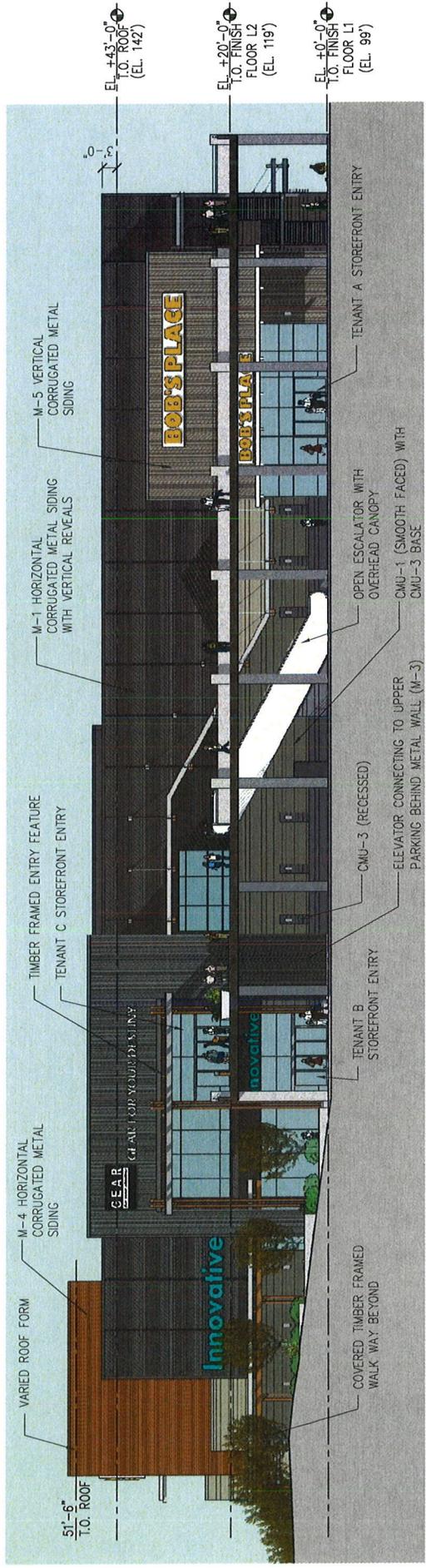
**AERIAL PHOTO**

**SITE PHOTOS**

**FEBRUARY 20, 2015**



**NORTH ELEVATION**



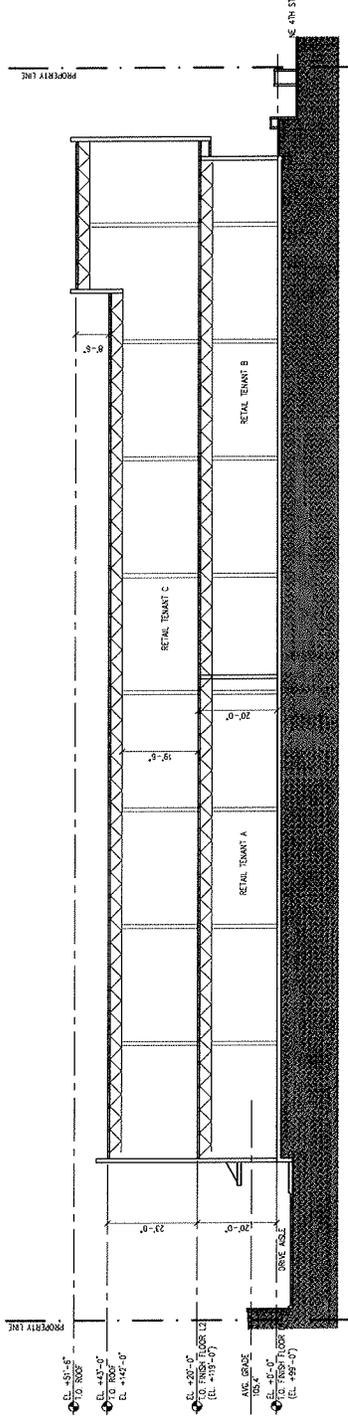
**EAST ELEVATION**



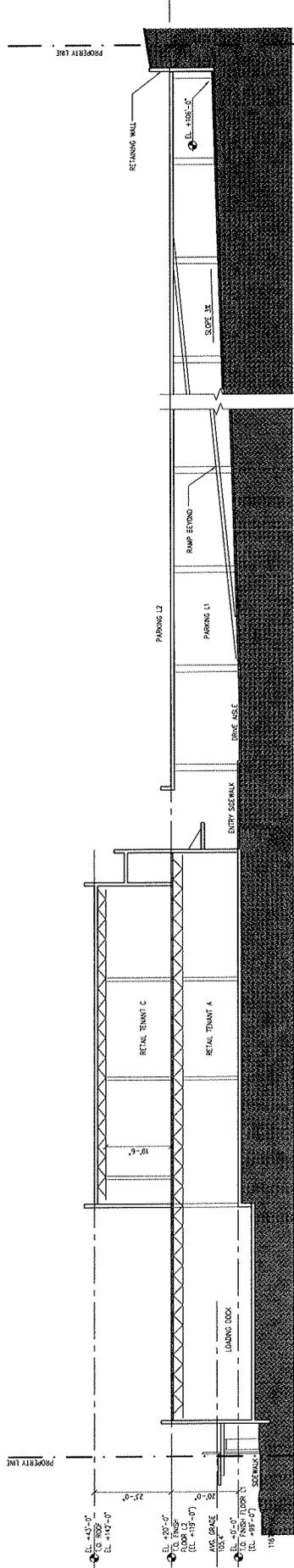
**SOUTH ELEVATION**



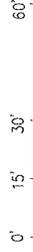
**WEST ELEVATION**



**S1 - NORTH-SOUTH BUILDING SECTION**



**S2 - EAST-WEST BUILDING SECTION**



**BUILDING SECTIONS**

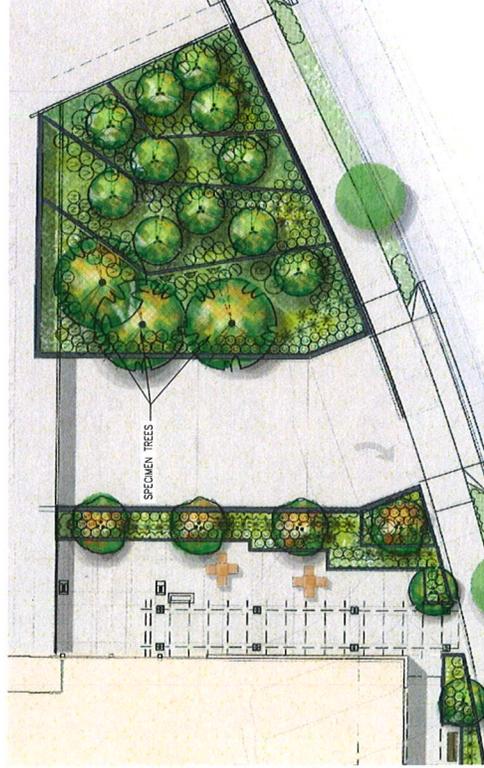
**15**  
**FEBRUARY 20, 2015**



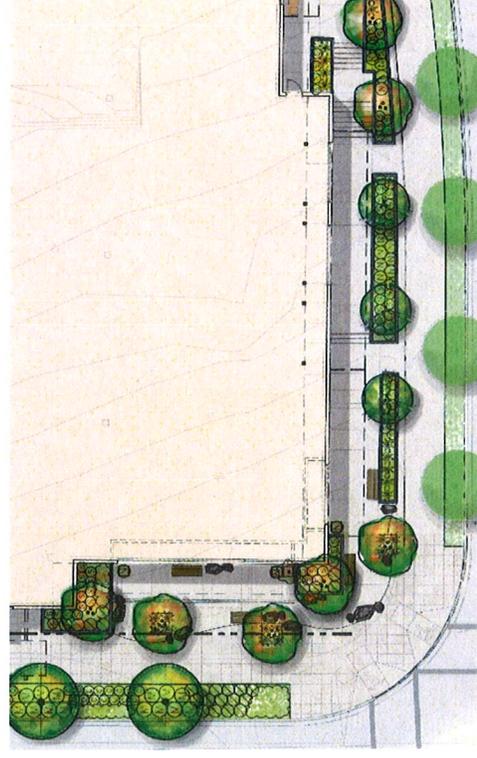
**AERIAL VIEW OF LANDSCAPED TERRACE**



**AERIAL VIEW OF SW CORNER PLAZA**



**LANDSCAPED TERRACE PLAN**



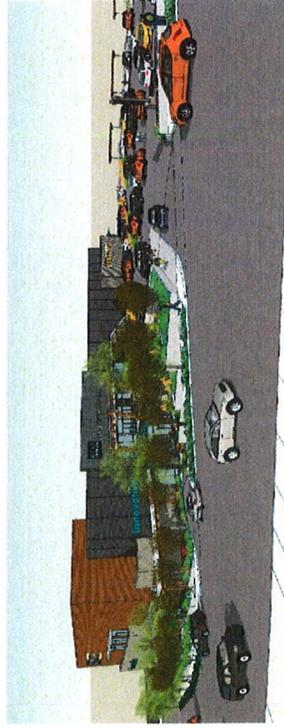
**SW CORNER PLAZA PLAN**



**AERIAL VIEW OF NE 4TH ENTRY TRELLIS**



**STREET VIEW OF SW CORNER PLAZA**



**STREET VIEW FROM NE 4TH ST.**



**AERIAL VIEW FROM NE 4TH ST.**



**NE AERIAL VIEW LOOKING SW**



**NW STREET VIEW LOOKING SOUTH**



**VIEW OF NORTH ELEVATION FROM NEIGHBOR**

**VIEWS OF NORTH EDGE OF PROJECT**



**NW CORNER STREET VIEW**



**NW CORNER AERIAL**



**VIEW OF PARKING SCREENING**



**VIEW OF PARKING SCREENING**



**NW SITE ENTRY VIEW**



**NW SITE ENTRY VIEW**



CMU-1

CONCRETE BLOCK



CMU-2

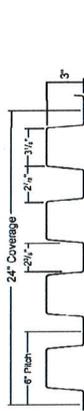


CMU-3

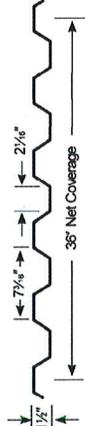


W1

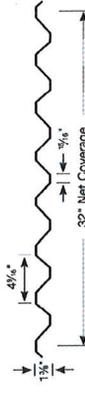
WOOD SIDING



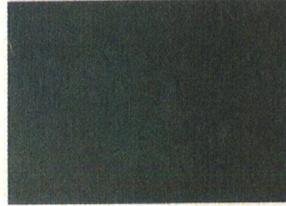
M4 PROFILE



M3 & M5 PROFILE



M1 & M2 PROFILE



M5 (METALLIC CHAMPAGNE)



M4 (TERRACOTTA)



M3 (ZACTIQUE II)



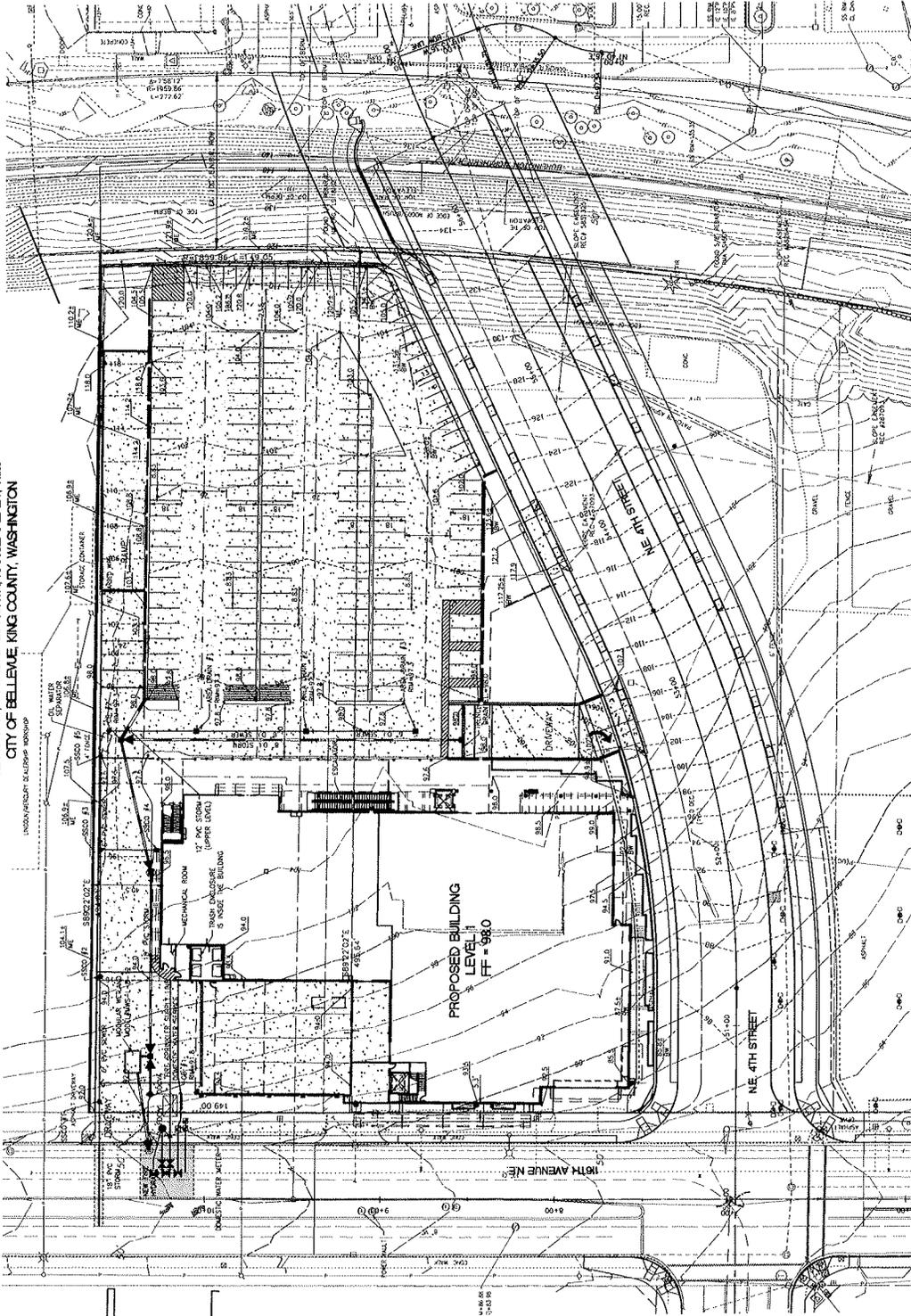
M2 (SLATE GRAY) VERTICAL



M1 (SLATE GRAY) HORIZONTAL



PRELIMINARY SITE PLAN  
FOR  
**BELLEVUE NORTH ON 116TH**  
SECTION 38, TOWNSHIP 25 NORTH, RANGE 5 EAST, 11M  
CITY OF BELLEVUE KING COUNTY, WASHINGTON



**SITE AREA QUANTITIES:**

TOTAL SITE AREA	137,420 SQ.FT. (3.15 ACRES)
EXISTING IMP. AREA	114,886 SQ.FT. (2.63 ACRES)
EXISTING PAVEMENT AREA (ROADS)	23,990 SQ.FT. (0.55 ACRES)
EXISTING PAVEMENT AREA (PARKING)	35,137 SQ.FT. (0.80 ACRES)
PROPOSED IMP. AREA (TOTAL)	22,534 SQ.FT. (0.52 ACRES)
PROPOSED PAVEMENT AREA (ROADS)	17,791 SQ.FT. (0.40 ACRES)
PROPOSED PAVEMENT AREA (PARKING)	4,743 SQ.FT. (0.11 ACRES)



Know what's below.  
Call before you dig.

12364  
12364  
12364

KG Investment Management  
Extension of NE 4th



18215 2ND AVENUE SOUTH  
KENN, WA 98032  
(425)291-8722  
(425)291-8722 FAX

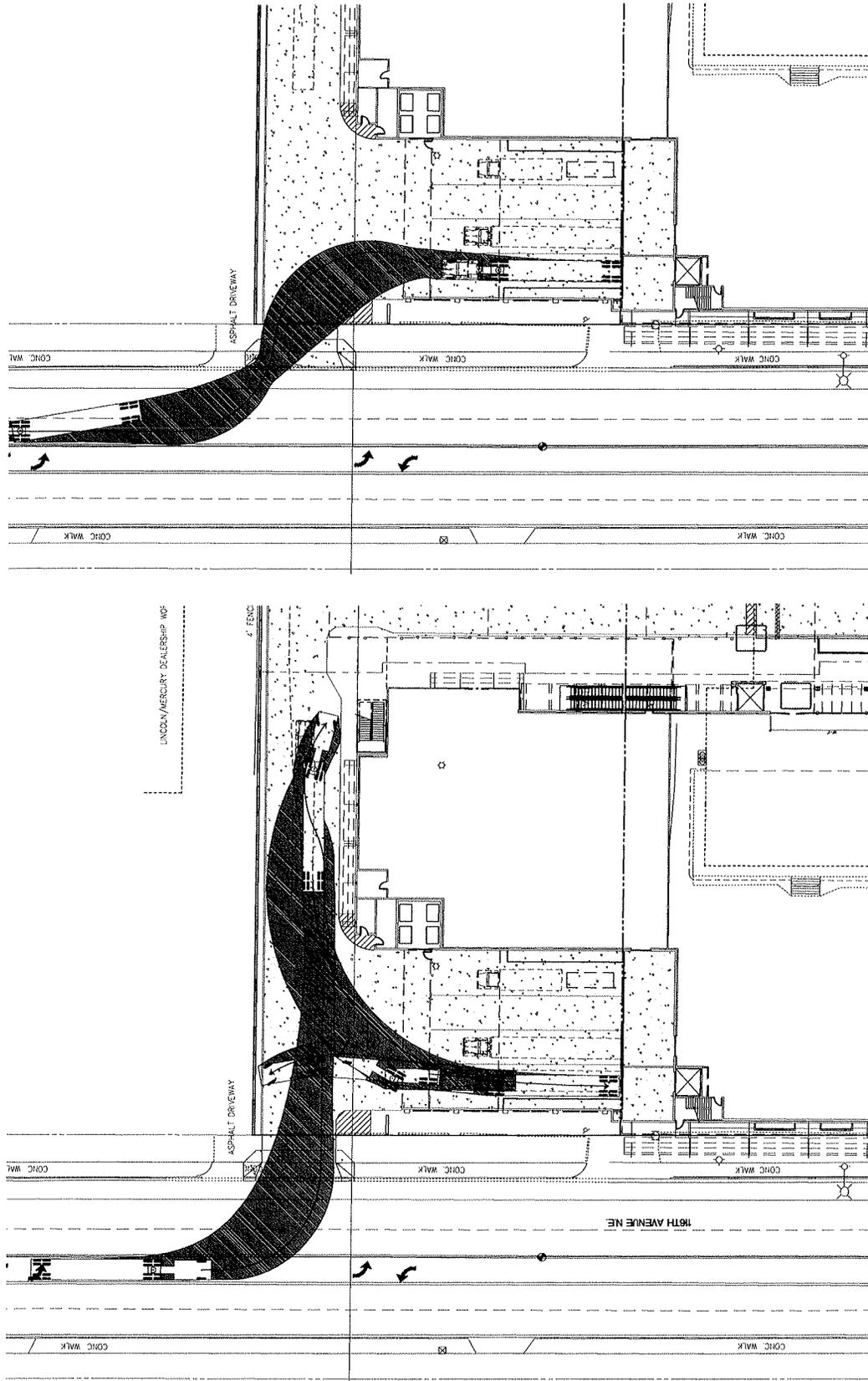
DESIGNED BY: [Signature]  
CHECKED BY: [Signature]  
DATE: 10/20/14



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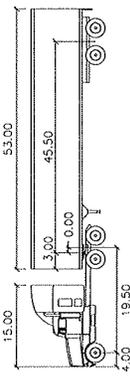
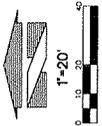


TRUCK TURNING PLAN - ALTERNATE BUILDING LAYOUT



EXITING

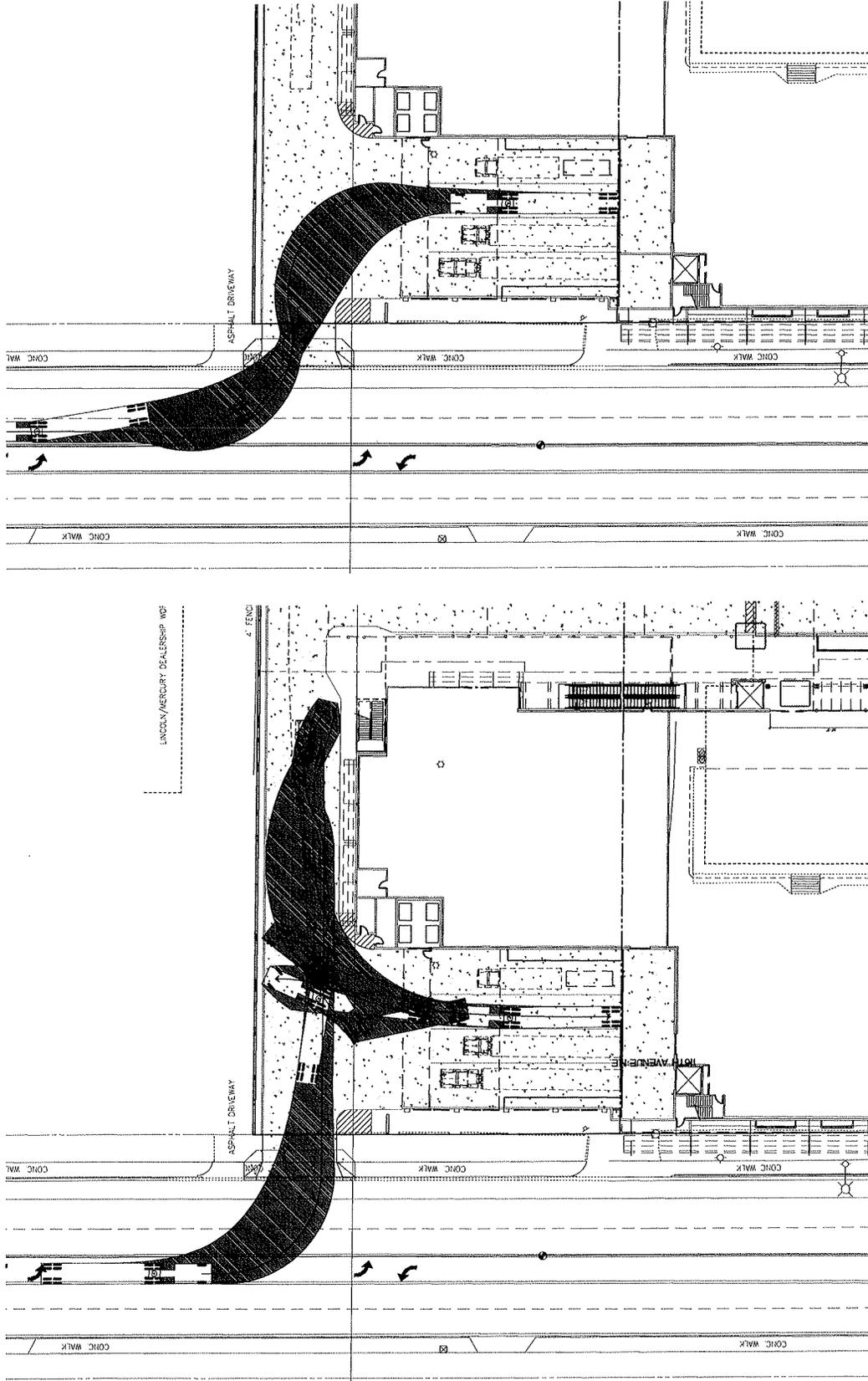
ENTERING



WB-67	
Tractor Width	: 8.00
Tractor Length	: 15.00
Tractor Track	: 6.50
Trailer Length	: 45.50
Trailer Width	: 8.50
Trailer Track	: 6.50
Lock to Lock Time	: 6.0
Steering Angle	: 28.4
Articulating Angle	: 75.0

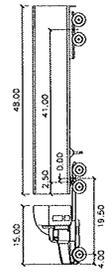


TRUCK TURNING PLAN - ALTERNATE BUILDING LAYOUT



EXITING

ENTERING



WB-62  
 Tractor Width : 8.80  
 Tractor Length : 8.80  
 Trailer Width : 8.80  
 Trailer Length : 41.00  
 Total Length : 50.40  
 Wheelbase : 19.50  
 Turn Radius : 62.00  
 Articulating Angle : 30.00

TRUCK TURNING PLAN  
 TRADER JOES

KG Investment Management  
 Extension of NE 4th

For

Scale: 1" = 20'  
 Date: 3/20/14  
 Project: 14-001  
 Client: KG Investment Management

18215 72ND AVENUE SOUTH  
 KENT, WA 98032  
 (425)251-8222  
 (425)251-8782 FAX



12364

Sheet

1 of 1

No.	Date	By	Appr.	Revision

Sally Nichols  
April 27, 2015

**ENVIRONMENTAL CHECKLIST**

10/9/2009

Thank you in advance for your cooperation and adherence to these procedures. If you need assistance in completing the checklist or have any questions regarding the environmental review process, please visit or call Development Services (425-452-6800) between 8 a.m. and 4 p.m., Monday through Friday (Wednesday, 10 to 4). Assistance for the hearing impaired: Dial 711 (Telecommunications Relay Service).

**INTRODUCTION**

**Purpose of the Checklist:**

The State Environmental Policy Act (SEPA), Chapter 43.21c RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An environmental impact statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the City of Bellevue identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the City decide whether an EIS is required.

**Instructions for Applicants:**

This environmental checklist asks you to describe some basic information about your proposal. Answer the questions briefly, with the most precise information known, or give the best description you can. You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer or if a question does not apply to your proposal, write "do not know" or "does not apply." Giving complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the Planner in the Permit Center can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. Include reference to any reports on studies that you are aware of which are relevant to the answers you provide. The City may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impacts.

**Use of a Checklist for Nonproject Proposals:** *A nonproject proposal includes plans, policies, and programs where actions are different or broader than a single site-specific proposal.*

For nonproject proposals, complete the Environmental Checklist even though you may answer "does not apply" to most questions. In addition, complete the Supplemental Sheet for Nonproject Actions available from Permit Processing.

For nonproject actions, the references in the checklist to the words *project*, *applicant*, and *property* or *site* should be read as *proposal*, *proposer*, and *affected geographic area*, respectively.

**Attach an 8 ½" x 11 vicinity map which accurately locates the proposed site.**

Received

NOV 07 2014

Permit Processing

**BACKGROUND INFORMATION**

Property Owner: Bellevue 116th Associates, LLC ✓

Proponent:

Contact Person: Tom Jordan ✓

(If different from the owner. All questions and correspondence will be directed to the individual listed.)

Address: Fuller Sears Architects  
1411 4th Ave #1306 ✓

Phone: (206) 682-6170 ✓

Proposal Title: Bellevue North ✓

Proposal Location: 316 116th Ave NE Bellevue WA 98114 ✓

(Street address and nearest cross street or intersection) Provide a legal description if available.

116th Ave NE and NE 4th Ave

Please attach an 8 1/2" x 11" vicinity map that accurately locates the proposal site.

Give an accurate, brief description of the proposal's scope and nature:

1. General description: A two story 82,086 SF retail building with one level 51,440 SF parking at grade and a one level 55,606 SF structured parking deck. *parking for 309 vehicles*

2. Acreage of site: 3.08 acres *137,420 sq ft approx 3.1 acres*

3. Number of dwelling units/buildings to be demolished: 0 ✓ *abandoned car dealership & parking lot - 1 bldg.*

4. Number of dwelling units/buildings to be constructed: 0 ✓

5. Square footage of buildings to be demolished: 20,906 SF ✓

6. Square footage of buildings to be constructed: 82,086 SF ✓

7. Quantity of earth movement (in cubic yards): 28,000 SF +

8. Proposed land use: CB ✓ *Retail*

9. Design features, including building height, number of stories and proposed exterior materials: ✓

The two story building will be 57' above average grade. Exterior materials are CMU, wood timbers, concrete and Corrugated metal

10. Other ✓

Estimated date of completion of the proposal or timing of phasing: ✓

Construction to start in late Spring 2015 - approximately 44 week construction time frame.

*Summer*

Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

NO ✓

*NS*

List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

Soils report, traffic study ✓

*Geotechnical Report by Earth Solutions NW dated 9/8/09 and revised/updated 11/7/14 and 12/4/14*

Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. List dates applied for and file numbers, if known.

None ✓

*see below*

List any government approvals or permits that will be needed for your proposal, if known. If permits have been applied for, list application date and file numbers, if known.

Bellevue building permits, Administrative Design Review, concurrency Certificate, Developer Extension Agreement

*14-143953-LD*

*15-103561-UE*

Please provide one or more of the following exhibits, if applicable to your proposal.

(Please check appropriate box(es) for exhibits submitted with your proposal):

Land Use Reclassification (rezone) Map of existing and proposed zoning

*14-128488-LQ  
current land use dist. (CB -  
result of 2014 rezoning from  
GC (Ord. 6168))*

Preliminary Plat or Planned Unit Development  
Preliminary plat map

Clearing & Grading Permit  
Plan of existing and proposed grading  
Development plans

*15-103562-GD app date 1/23/15*

Building Permit (or Design Review)  
Site plan  
Clearing & grading plan

*15-103611-BB app date: 1/26/15*

Shoreline Management Permit  
Site plan

A. ENVIRONMENTAL ELEMENTS

1. Earth

*CA steep slope along east prop. line*

a. General description of the site:  Flat  Rolling  Hilly  Steep slopes  Mountains  Other

b. What is the steepest slope on the site (approximate percent slope)?

Majority of the site is 3% (Parking at grade), 10% Parking ramps, 40-50%-Hill slope along east prop. line. ✓

c. What general types of soil are found on the site (for example, clay, sand, gravel, peat, and muck)? If you know the classification of agricultural soils, specify them and note any prime farmland. ✓

Medium to very dense silty sand, silt and clay over sand deposits. Further down, the soil consisted of dense silty sand w/gravel. USDA soil mapping shows Alderwood gravelly sandy loam on the western side with the remainder being urban land. ✓

d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. ✓

No

*slope stable*

*See Geotech Report by Earth Solutions NW - 9/8/2009 / updated 12/2014*

- e. Describe the purpose, type, and approximate quantities of any filling or grading proposed. Indicate source of fill.

The site will have 3,000 CY of on site cut and fill and 25,000 CY of export.

- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

No

- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

The existing site (since 1963) has 96.7 % impervious area. The proposed site will be approximately 95% impervious.

- h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any:

Erosion control measures and BMP's as required by the City of Bellevue regulations will be incorporated into the project's erosion control and development plans. Neighboring properties will be protected. Management of storm water during construction will be addressed.

## 2. AIR

- a. What types of emissions to the air would result from the proposal (i.e. dust, automobile odors, and industrial wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known.

There will be typical dust and odors generated by the construction equipment and process.

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.

No

- c. Proposed measures to reduce or control emissions or other impacts to the air, if any:

The project will follow the standard guidelines to help minimize construction emissions.

## 3. WATER

- a. Surface

- (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

NO

- (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If Yes, please describe and attach available plans.

NO

✓  
Erosion control techniques  
per Bear and Grade  
inspection and BCC  
23.76

✓  
BCC 23.76 - C&S  
code - erosion and  
sedimentation  
control

Construction Dust  
Suppression measures  
per BCC 23.76

- (3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material. ✓

N/A

- (4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known. ✓

NO

- (5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan. ✓

NO

- (6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

No waste water will be discharged into the ground.

b. Ground

- (1) Will ground water be withdrawn, or will water be discharged to ground water? Give general description. ✓

NO

- (2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals...; agricultural; etc.) Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve. ✓

N/A - No waste material will be discharged into the ground.

c. Water Runoff (Including storm water)

- (1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe. ✓

Storm water runoff from the parking deck and hard surfaces (sidewalks) will be diverted to the on site storm water system.

- (2) Could waste materials enter ground or surface waters? If so, generally describe. ✓

NO

d. Proposed measures to reduce or control surface, ground, and runoff water impacts, if any:

The site's perimeter landscaping will help contain any further storm water runoff in addition to the site's storm water system.

*Per utilities Code 24.06 - storm and surface water*

**4. Plants**

a. Check or circle types of vegetation found on the site:

- deciduous tree: alder, maple, aspen, other
- evergreen tree: fir, cedar, pine, other
- shrubs
- grass
- pasture
- crop or grain
- wet soil plants: cattail, buttercup, bulrush, skunk cabbage, other
- water plants: water lily, eelgrass, milfoil, other
- other types of vegetation

*noxious weeds - primarily blackberries on steep slope*

*4 trees removed plus noxious shrubs/understory  
Refer to Arborist Report by Tree Solutions dated 2/19/15 and Earth Solutions addendum dated 3/16/15*

b. What kind and amount of vegetation will be removed or altered?

The eastern edge of the site has an existing 20' x 150' strip of native landscaping. Approximately 10' x 150' area will be removed. The remaining 10' depth will be landscaped with evergreen and deciduous trees.

*Trees and native p.c. planted @ top of retain wall*

c. List threatened or endangered species known to be on or near the site. ✓

None

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any: ✓

Portions of the existing landscape will be retained and new landscape areas added throughout the site and along the streets. The proposed landscape consists predominantly of native and adapted plants typical for this area. It includes deciduous and evergreen trees, shrubs, and groundcover.

**5. ANIMALS**

a. Check or circle any birds and animals which have been observed on or near the site or are known to be on or near the site: ✓

- Birds: hawk, heron, eagle, songbirds, other:
- Mammals: deer, bear, elk, beaver, other:
- Fish: bass, salmon, trout, herring, shellfish, other:

*SN*

b. List any threatened or endangered species known to be on or near the site. ✓

None

c. Is the site part of a migration route? If so, explain.

No

d. Proposed measures to preserve or enhance wildlife, if any:

None

*Pacific Flyway - includes entire Puget sound region  
improved veg. @ toe of slope and more veg. throughout the site*

**6. Energy and Natural Resources**

a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy need? Describe whether it will be used for heating, manufacturing, etc. ✓

Natural gas will be used for heating and electrical will be used for cooling and lighting.

b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. ✓

No

c. What kinds of energy conservation features are included in the plans of the proposal? List other proposed measures to reduce or control energy impacts, if any: ✓

Low flow plumbing fixtures, skylights and large storefront windows for natural lighting.

**7. Environmental Health**

a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe. ✓

No

*Regulated by BCC 23.76 & DOE chapter in WAC*

(1) Describe special emergency services that might be required. ✓

No special services required

(2) Proposed measures to reduce or control environmental health hazards, if any. ✓

None

*SN*

b. Noise

- (1) What types of noise exist in the area which may affect your project (for example, traffic, equipment, operation, other)?

Freeway noise - I 405 is located approximately 500' from the site.

construction noise

- (2) What types and levels of noise would be created by or associated with the project on a short-term or long-term basis (for example, traffic, construction, operation, other)? Indicate what hours noise would come from the site.

Noise associated with construction will have a short-term impact during regular working hours, 7am- 7pm Monday-Saturday. Full development of the project will have no long term noise impacts.

- (3) Proposed measures to reduce or control noise impacts, if any:

The project will follow the Bellevue noise ordinance as it relates to construction.

Noise and construction hours regulated per BCC 9.1B Condition of Design Review to use noise suppression tech. thru out construction

8. Land and Shoreline Use

- a. What is the current use of the site and adjacent properties?

Former automobile sales - currently vacant

- b. Has the site been used for agriculture? If so, describe.

No

- c. Describe any structures on the site.

An existing auto sales and repair facility. 50% has been demolished prior to this submittal.

perrows - 1.6%

- d. Will any structures be demolished? If so, what?

Yes. The remaining half of the existing structure will be demolished.

- e. What is the current zoning classification of the site?

GC

GC per Ord. 6168 (rezone in 2014)

- f. What is the current comprehensive plan designation of the site?

General Business/Community Business

- g. If applicable, what is the current shoreline master program designation of the site?

N/A

- h. Has any part of the site been classified as an "environmentally sensitive" area? If so, specify.

No

Yes - steep slope critical area along east. prop. line

- i. Approximately how many people would reside or work in the completed project?

Approximately 30 - 50 employees will work at the retail stores.

no res.

- j. Approximately how many people would the completed project displace?

None

k. Proposed measures to avoid or reduce displacement impacts, if any: ✓

None

NA ✓

i. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

The project team has met with the city of Bellevue planners prior to design to work out any design issues. The project is also required to go through Administrative design review.

14-143953-LD

### 9. Housing

a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing. ✓

None

b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing. ✓

None

c. Proposed measures to reduce or control housing impacts, if any: ✓

None

### 10. Aesthetics

a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? ✓

The highest portion of the building will be approximately 57' above average grade.

44'-3"  
as measured  
from ave fin. grd.  
96'-3"

b. What views in the immediate vicinity would be altered or obstructed? ✓

None ✓

c. Proposed measures to reduce or control aesthetic impacts, if any: ✓

The project team has met with the city of Bellevue planners prior to design to work out any design issues. The project is also required to go through Administrative design review.

14-143953-LD

LUC 20.251 -  
CB design dist.  
guidelines & stds

11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? ✓  
There will be light fixtures located along the exterior facade of the building used mainly during the evening hours.
- b. Could light or glare from the finished project be a safety hazard or interfere with views?  
No
- c. What existing off-site sources of light or glare may affect your proposal? ✓  
None
- d. Proposed measures to reduce or control light or glare impacts, if any: ✓  
Light fixture located along the exterior facade will be down illuminating lights.

*Light and glare  
LUC 20.20.522  
cut off shields req'd*

12. Recreation

- a. What designated and informal recreational opportunities are in the immediate vicinity? ✓  
None
- b. Would the proposed project displace any existing recreational uses? If so, describe. ✓  
No
- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: ✓  
None

*Will be req'd to make  
connections w/ future  
BNSF trail to east -  
trail along rail  
corridor*

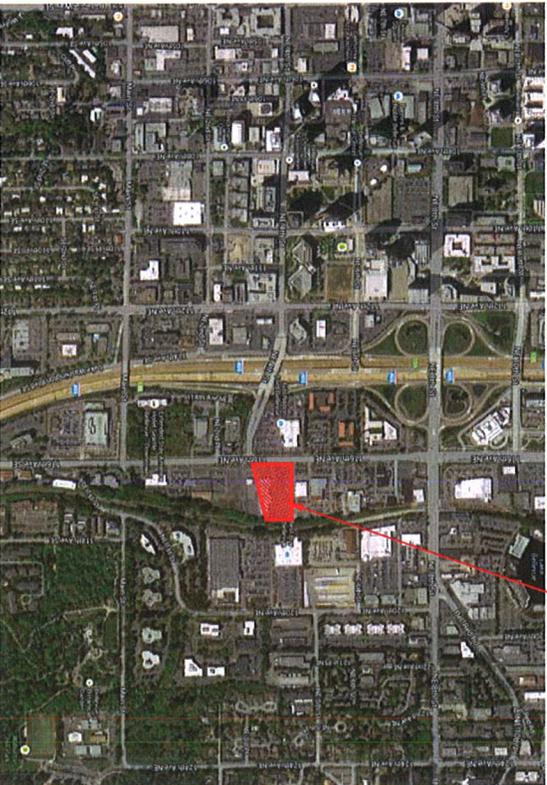
13. Historic and Cultural Preservation

- a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? If so, generally describe. ✓  
No
- b. Generally describe any landmarks or evidence of historic, archeological, scientific, or cultural importance known to be on or next to the site. ✓  
None
- c. Proposed measures to reduce or control impacts, if any: ✓  
None

14. Transportation

- a. Identify public streets and highways serving the site, and describe proposed access to the existing street system. Show on site plans, if any. ✓  
*Phase I*  
I-405 to the west, 116th Ave NE (western edge), new NE 4th street, NE 8th street to the north.
- b. Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop? ✓  
The Bellevue transit center is approximately 1/2 mile from the site. Currently, no buses stop at the site.  
*Light rail station will be located 1 block to north.*
- c. How many parking spaces would be completed project have? How many would the project eliminate? ✓  
309 new parking stalls.

*NS*



**SITE**

**VICINITY MAP**



**PROJECT DESCRIPTION:**

THE PROJECT CONSISTS OF A 82,000 SF, TWO STORY RETAIL BUILDING. THE SITE WILL PROVIDE AT GRADE PARKING FOR 152 CARS AND ONE LEVEL OF STRUCTURED PARKING FOR 157 CARS, FOR A TOTAL OF 309 CARS.

**PROJECT ADDRESS:**

316 116TH AVE NE  
BELLEVUE, WA 98004  
(INTERSECTION OF 116TH AVE NE  
AND NE 4TH STREET)

ASSESSOR'S #: 332505-9151 & 332505-9012

NE 4th Ext.  
bully funded -  
Phase 1 completed

d. Will the proposal require any new roads or streets, or improvements to existing roads or streets, not including driveways? If so, generally describe (indicate whether public or private).

No new streets required - Coordinate project street improvements with the city's new NE 4th street construction.

e. Will the project use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

No

f. How many vehicular trips per day would be generated by the completed project? If known, indicate when peak volumes would occur.

City of Bellevue will calculate number of vehicular trips.

148 new pm peak hr. trips  
based on 75,000 SF retail

g. Proposed measures to reduce or control transportation impacts, if any:

None

Anticipated in Trans. Facilities Plan EIS  
concurrency test run.  
see staff report.  
1/26/15

Bell 116th North Retail Trans Impact Study March 2015 by Tranapo

15. Public Services

a. Would the project result in an increased need for the public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe.

The project has no specific needs that would require additional services.

b. Proposed measures to reduce or control direct impacts on public services, if any:

None

16. Utilities

a. Circle utilities currently available at the site: electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other.

Electric, Natural gas, water, refuse service, telephone and sanitary sewer

b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

Puget Sound Energy (electricity & gas), Bellevue Utilities (water/drainage/sewer), Allied Waste (solid waste)

Republic services

Signature

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature.....*Sam Gask*.....

Date Submitted.....11/07/2014.....

NS

# CERTIFICATE OF CONCURRENCY

## BELLEVUE NORTH PARCEL PROJECT

This certificate documents the Transportation Department Director's decision that the development project at 316 116<sup>th</sup> Ave NE (Design Review File No. 14 143953 LD) complies with the requirements of the Traffic Standards Code (BCC 14.10). This decision reserves 148 p.m. peak hour trips to this project, subject to Process II appeal of either the concurrency determination or the SEPA determination. Building permits have been filed for the project and deemed complete. This concurrency reservation will remain in effect for the life of the building permit application (BCC 23.05.090.H). Upon issuance of the building permit, concurrency is reserved for one year; the applicant may request up to two one-year extensions (BCC 23.05.100.E).

  
\_\_\_\_\_  
Director, Transportation Department

May 5, 2015  
\_\_\_\_\_  
Date

Certificate No. 92