



**City of Bellevue  
Development Services Department  
Land Use Staff Report**

---

**Proposal Name:** The Spring District Master Development Plan

**Proposal Address:** 1227 124<sup>th</sup> Avenue NE

**Proposal Description:** Application for a Master Development Plan (MDP) to redevelop 36 acres in the Bel-Red Subarea from industrial use to an urban village which is transit-oriented. This mixed use development will contain office space, neighborhood retail space, housing units, a hotel, parks, and new road/infrastructure facilities. Proposal includes demolition of 6 buildings (approximately 700,000 gross square feet) and construction of 29 buildings. Development is anticipated to occur over 7 phases and approximately 15 years. Approximately 5.4 million gross square feet will be constructed along with approximately 10,000 parking stalls. The building heights range from 11 to 14 stories.

**File Number:** 11-125943-LP

**Applicant:** Wright Runstad & Company

**Decisions Included:** Master Development Plan and SEPA Determination

**Process:** Process II, (LUC 20.35.200)

**Planner:** Carol Hamlin, Senior Planner, 425-452-2731

**Threshold Determination:  
State Environmental Policy Act  
(SEPA):** Determination of Non-Significance with incorporation by reference of the Bel-Red Corridor Project Draft and Final Environmental Impact Statements issued July 19, 2007

Carol V. Helland, Environmental Coordinator  
Development Services Department

**Director's Decision:** **Approval with Conditions**  
Michael A. Brennan, Director  
Development Services Department

By Carol V. Helland, Land Use Director

---

Application Date: 11-07-2011  
Completeness Date: 12-02-2012  
Notice of Decision Date: 05-03-2012  
SEPA Appeal Deadline: 05-17-2012 at 5pm  
MDP Appeal Deadline: 05-17-2012 at 5pm

For information on how to appeal a proposal, visit the Development Services Center at City Hall or call (425) 452-6800. Comments on State Environmental Policy Act (SEPA) Determinations can be made with or without appealing the MDP decision within the noted comment period for a SEPA Determination. Appeals must be received in the City Clerk's Office by 5 pm on the date noted for appeal of the decision.

---

## CONTENTS

I.	Request and Review Process .....	Pg 3
II.	Site Context and Description .....	Pg 12
III.	Consistency with Land Use /Zoning Requirements.....	Pg 15
IV.	Public Notice and Comments .....	Pg 29
V.	Technical Review.....	Pg 41
VI.	State Environmental Policy Act (SEPA).....	Pg 46
VII.	Changes to Proposal Due to City Review.....	Pg 51
VIII.	Decision Criteria.....	Pg 52
IX.	Decision.....	Pg 59
X.	Conditions of Approval.....	Pg 59

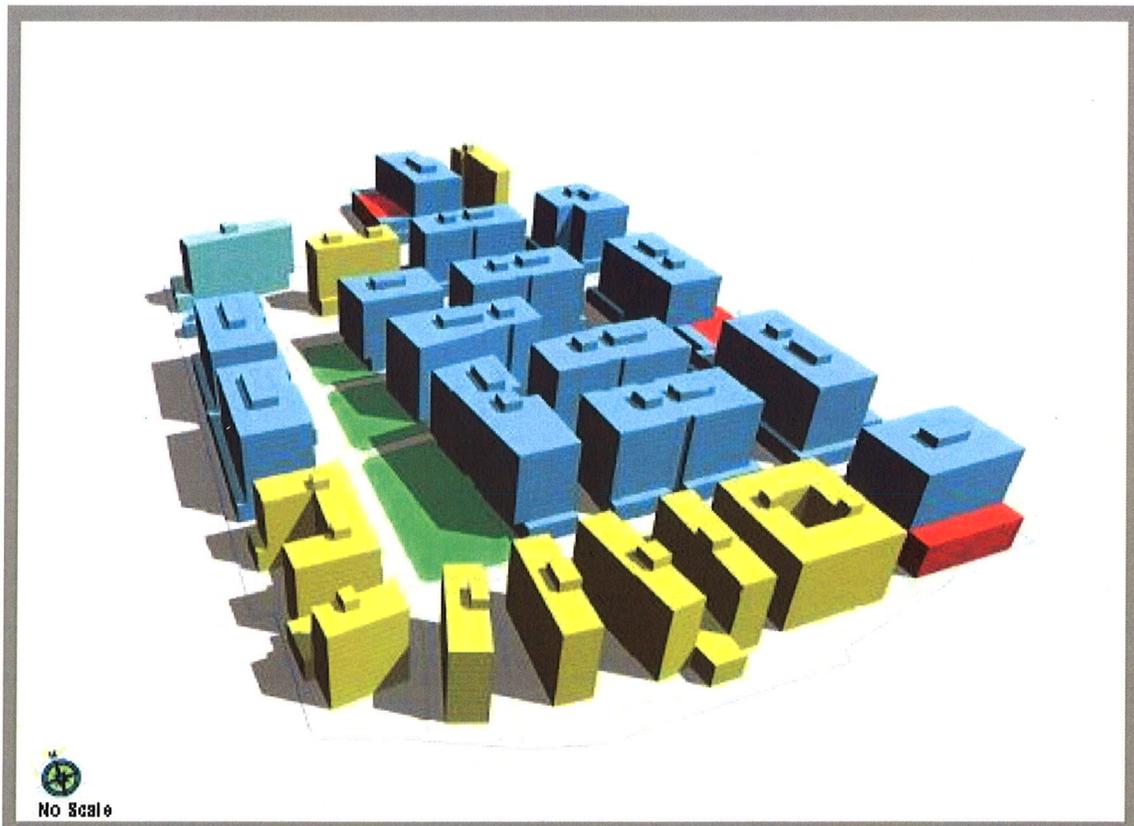
- Attachments:
- A- Project Plans
  - B- Amenity Calculation Sheet
  - C- Environmental Checklist
  - D- Development Agreement signed on July 10, 2009 and recorded September 11, 2009
  - E- Technical Memo by Parametrix, Inc. dated March 28, 2012 regarding Gas Emissions
  - F- Conceptual Phasing Plan
  - G- Construction Cost Estimate for Infrastructure Phase 1
  - H- Modification Maps

## I. REQUEST and REVIEW PROCESS

### A. Request

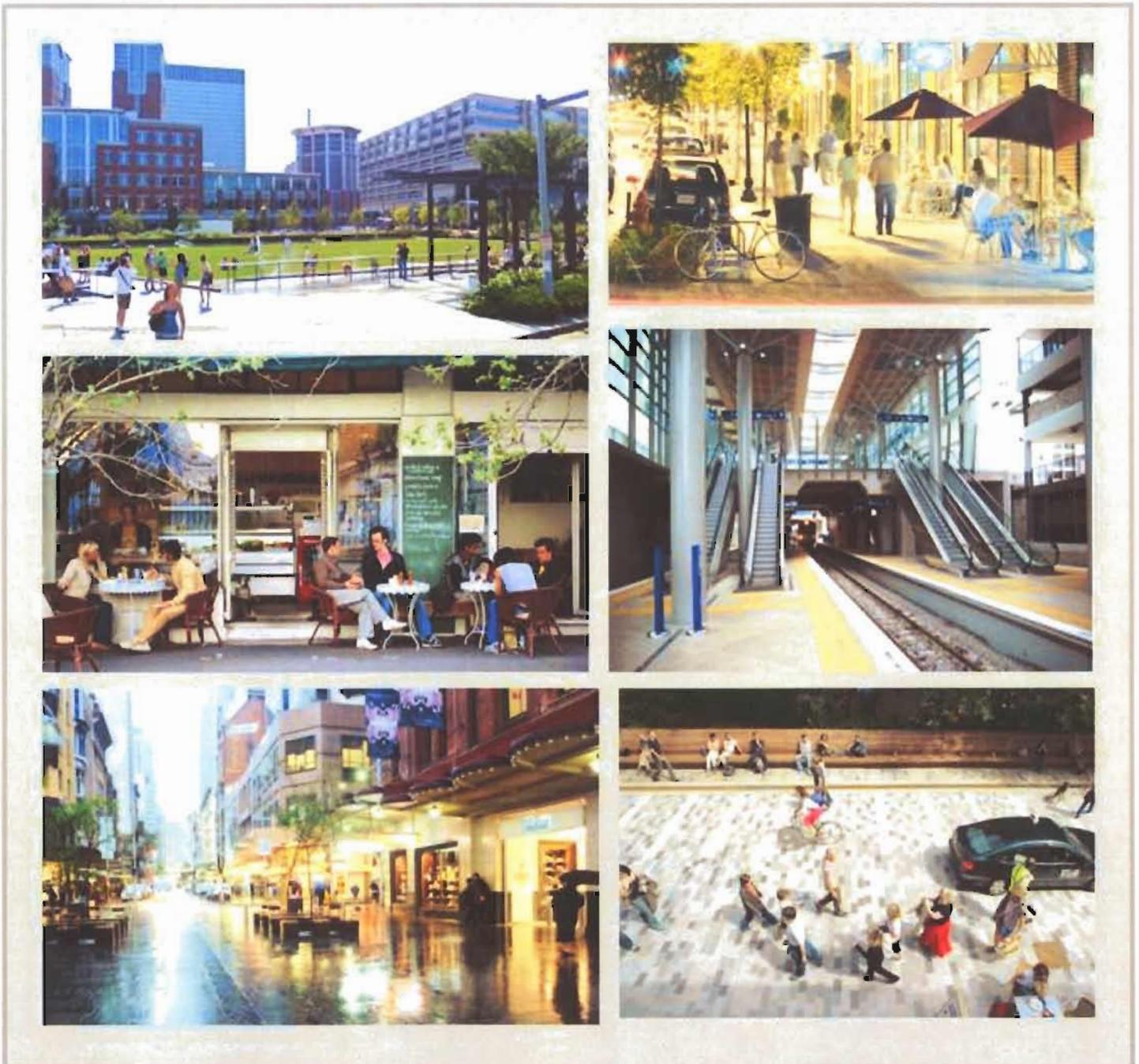
Wright Runstad & Company requests approval of a Master Development Plan (MDP) to plan for the phased redevelopment of 36 acres in the Bel-Red Subarea from industrial use to an urban village which is transit-oriented at ultimate build-out. This mixed use development will contain office space, neighborhood retail space, housing units, a hotel, parks, and new road/infrastructure facilities. The proposal includes demolition of 6 buildings (approximately 700,000 gross square feet) and construction of 29 buildings. Development is anticipated to occur over 7 phases and approximately 15 years until the year 2028. Approximately 5.4 million gross square feet will be constructed along with approximately 10,000 parking stalls. This includes 4.1 million gross square feet of commercial, 1.3 million gross square feet of residential, and approximately 2.2 acres of parks and open space. At full build-out, it is expected to house approximately 3,000 residents, 200 retail/hotel workers, and 18,560 office workers. The building heights range from 11 to 14 stories. The applicant's MDP represents the maximum anticipated development potential. This application does not include Design Review approval for any individual buildings. Design Review for individual buildings will occur under separate applications, and will include separate project level threshold determinations under the State Environmental Policy Act.

### The Spring District Master Development Plan



The Spring District Master Plan is part of a larger vision within the Bellevue-Redmond (Bel-Red) Corridor of Bellevue to create a new eastside community that complements the downtown and provides a new sustainable vision of urban living and working.

### The Spring District Graphic Images of the Vision of the Urban Village



### **Master Development Plan Goals**

The applicant has identified 6 goals for The Spring District Master Development Plan:

1. Develop a New Urban Community:  
Develop a new urban community, composed of high quality regional business, commercial, retail, residential, and open spaces. A key goal is to ensure that the Master Development Plan achieves compact and sustainable urban growth that is consistent with Bellevue's vision for nodes in the Bel-Red corridor.
2. Establish Neighborhood Identity:  
Establish a new neighborhood identity that is inclusive and responsive to its surrounding context. The neighborhood will encompass approximately 1200 residential units.
3. Create a Catalyst Project:  
Create a world class project that will become a catalyst for new development within the Bel-Red Corridor, helping to achieve the City's goal of creating a vibrant new district with a diversified economic base.
4. Create a Complementary Project to Downtown Bellevue:  
Create a project that does not compete with, but complements downtown Bellevue by providing an attractive alternative consistent with the planning goals of the Bel-Red Subarea Plan.
5. Create a New Business Environment:  
Create a new business environment that is flexible for future market demands and achieves an economically sustainable future. The project includes 4.1 million square feet of commercial space.
6. Provide Vibrant Parks and Open Spaces:  
Provide vibrant new parks, plazas, and open spaces that are environmentally responsive and encourage social interaction. A network of open spaces will flow through the site, creating a pedestrian friendly environment that could connect to the existing trail and bike network in Bellevue. Vehicular parking will be located in subsurface garages planned under most buildings. Additional visitor surface parking is planned along the development perimeter, in arrival courtyards, auto-courts, and on-street parking.

**Connection to Sound Transit**

A unique feature of this Master Development Plan is its connection to the mass transit link, Sound Transit’s future light rail line (East Link). The East Link will provide connection from The Spring District to downtown Bellevue, Seattle and Redmond. Sound Transit has proposed a station within The Spring District project called “120<sup>th</sup> Avenue NE.” Construction of this future light rail line is targeted for Phase 2 of this project (estimated 2015-2023). The details of construction funding and property transfer are in separate agreement with Sound Transit and the applicant and are not a part of this Master Development Plan Approval.

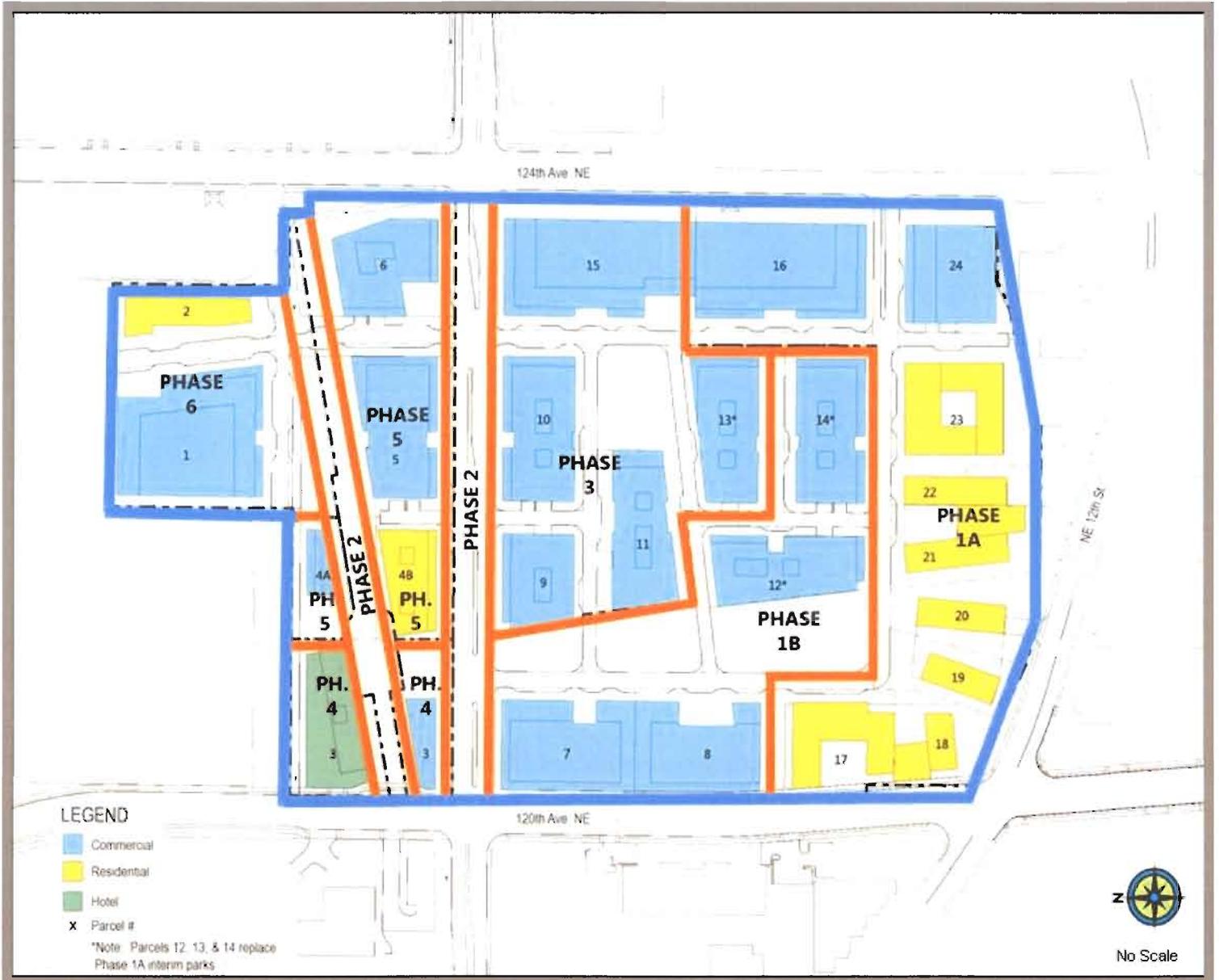
**Aerial Photograph showing proposed Sound Transit Station**



**Phasing**

Development of the site is planned to occur in multiple phases, with an integrated sequence of infrastructure and building development. The western and southern-most blocks are anticipated to be developed in the first phase, including the central park and residential buildings. Subsequent phases will include development of the central and eastern-most blocks, concluding at the northern edge of the site.

### Phasing Plan Graphic Representation



The applicant's proposed 2012 Master Development Plan will be complete in 7 distinct phases, summarized below.

- Phase 1a includes demolition of 3 buildings, construction of 10 residential buildings along the south end of the property, two office buildings with ground-floor retail, and an interim park (about 3 acres) to meet park space requirements per the Development Agreement.<sup>1</sup> Impervious surface will be reduced by about 2 acres. Associated parking will be provided. (2013-2015)
- Phase 1b includes demolition of 1 building, construction of 4 new office buildings with ground-floor retail, relocating a park property (about 1 acre). About 1000 square feet of impervious surface will be added. Associated parking will be provided. (2015-2018)

Note: Phase 1 is divided into two phases (Phase 1a & 1b) to better match forecasted phased delivery of commercial and residential buildings and to meet the Development Agreement as it relates to the parks requirement. The city and applicant anticipate each phase might be further divided in the future in response to market demand (i.e. separate buildings may have their own phases, 1a, 1b, 1c, 1d, etc.)

- Phase 2 includes demolition of 2 buildings, 15<sup>th</sup>/16<sup>th</sup> in their future alignment, and construction of the Sound Transit Light Rail Transit (LRT), station. (2015-2023)
- Phase 3 includes construction 5 new office buildings. The applicant will provide about ½ acre of park. Impervious surface will be reduced by about ½ acre. Associated parking will be provided. (2020-2022)
- Phase 4 adds a landmark hotel where the applicant proposes to construct an additional entry to the LRT station. (Note: This would be under separate approval with Sound Transit and a separate Design Review approval with the City of Bellevue.) One new office building will be constructed. Impervious surface will be reduced by about 1000 square feet. Associated parking will be provided. (2022-2024)
- Phase 5 adds development north of NE 15<sup>TH</sup> Street, including 1 residential building and 3 office/retail buildings. Impervious surface will be reduced by about ½ acre. Associated parking will be provided. (2024-2026)
- Phase 6 adds the final buildings: 1 office building and 1 residential complex. Impervious surface will be reduced by about ¾ acres. Associated parking will be provided. (2026-2028)

Refer to Conditions of Approval regarding Phasing Plan in Section X.A.

---

<sup>1</sup> The Development Agreement (DA) is an agreement between the City of Bellevue and Wright-Runstad & Company (WR-SRI 120<sup>th</sup> LLC). The DA was approved and signed on July 10, 2009 and recorded on September 11, 2009. This agreement requires specific items for the development of The Spring District. See Section III.B for more information and how the proposal meets the requirements of the Development Agreement.

Table 1 below (also as Attachment G) provides details about each phase, based upon maximum development potential. During the phasing of this project and through submitted Design Review applications for individual buildings, the applicant may chose to build at less than maximum development potential.

**Table 1**  
**The Spring District**  
**Conceptual Phasing Plan**

Item	Phase 1a	Phase 1b	Phase 2	Phase 3	Phase 4	Phase 5	Phase 6	Totals
<b>Demolition</b>	3 Buildings 358,310 GSF	1 Buildings 18,575 GSF	2 Buildings 323,263 GSF	- Buildings - GSF	- Buildings - GSF	- Buildings - GSF	- Buildings - GSF	6 Buildings 700,148 GSF
<b>New Residential Max Height 125' - 150' Max FAR 4.0</b>	10 Buildings 883,000 GSF	- Buildings - GSF	- Buildings - GSF	- Buildings - GSF	- Buildings - GSF	1 Buildings 170,000 GSF	1 Buildings 169,000 GSF	12 Buildings 1,222,000 GSF
<b>New Office Max Height 150' Max FAR 4.0</b>	2 Buildings 430,000 GSF	4 Buildings 1,104,000 GSF	- Buildings - GSF	5 Buildings 1,316,000 GSF	1 Buildings 11,000 GSF	3 Buildings 561,000 GSF	1 Buildings 293,000 GSF	16 Buildings 3,715,000 GSF
<b>New Retail Max Height 150' Max FAR 4.0</b>	Buildings 13,000 GSF	Buildings 18,000 GSF	Buildings - GSF	Buildings 63,000 GSF	Buildings 18,000 GSF	Buildings 41,000 GSF	Buildings 13,000 GSF	Buildings 166,000 GSF
<b>Hotel Max Height 150' Max FAR 4.0</b>	- Buildings - GSF	- Buildings - GSF	- Buildings - GSF	- Buildings - GSF	1 Buildings 199,000 GSF	- Buildings - GSF	- Buildings - GSF	1 Buildings 199,000 GSF
<b>Cumulative FAR</b>	2.10	3.13	NA	3.35	3.29	3.26	3.27	3.27
<b>Parks</b>	130,000 sf	(54,000) sf	- sf	26,000 sf	- sf	- sf	- sf	102,000 sf
<b>Sound Transit 120th LRT Station</b>	-	-	Yes	-	-	-	-	
<b>Net Impervious Surface Increase (Decrease)</b>	(91,000) sf	1,000 sf	(26,000) sf	(39,000) sf	(1,000) sf	(29,000) sf	(38,000) sf	(223,000) sf
<b>Parking Stalls</b>	1,500	3,000	-	2,800	400	1,200	1,200	10,100
<b>New Added Streets</b>	127,500 sf	12,200 sf	- sf	57,600 sf	- sf	3,400 sf	24,500 sf	225,200 sf
<b>Acres</b>	11.2	5.5	0.0	8.8	1.9	5.4	3.2	36.01
<b>Estimated Time Range* (+/-)</b>	2013 - 2015*	2015-2018*	2015-2023*	2020-2022*	2022-2024*	2024-2026*	2026-2028*	15 years*

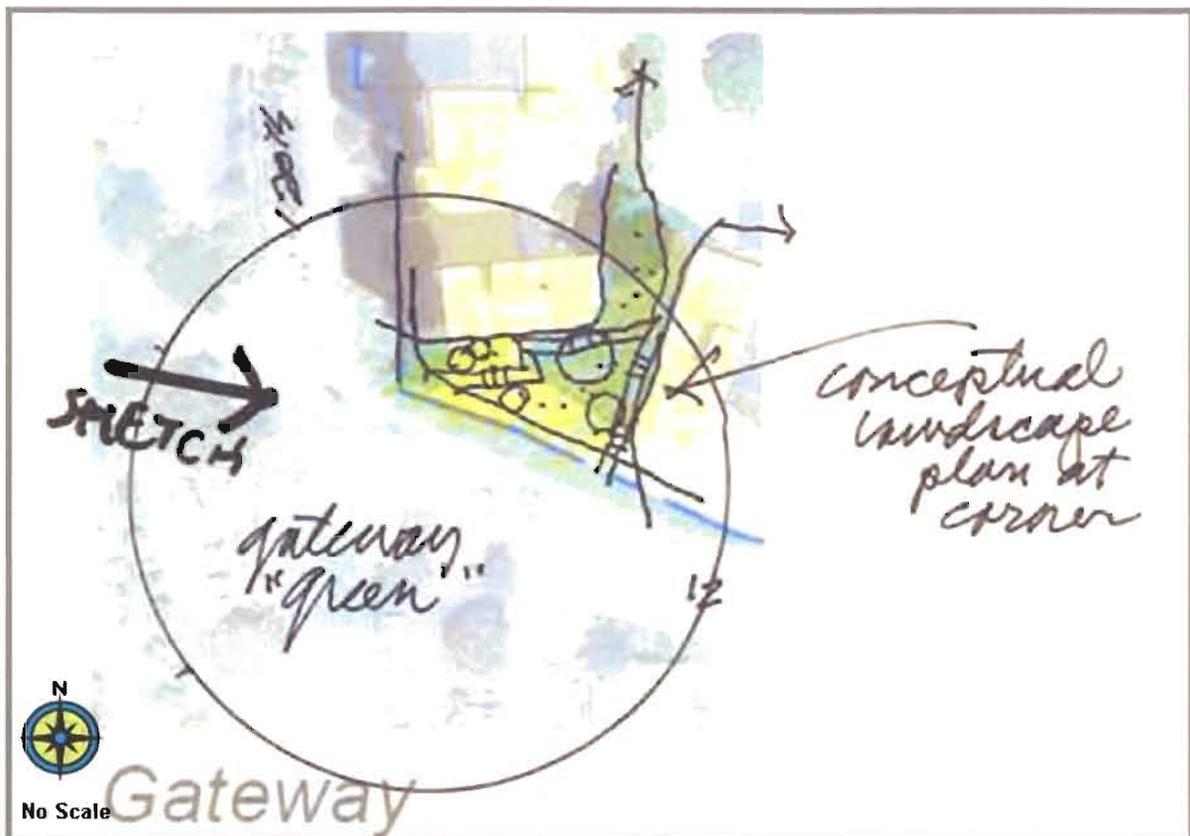
\*Dependent on market timing

### Other Project Elements:

In addition to the site elements noted above, the project will include the following:

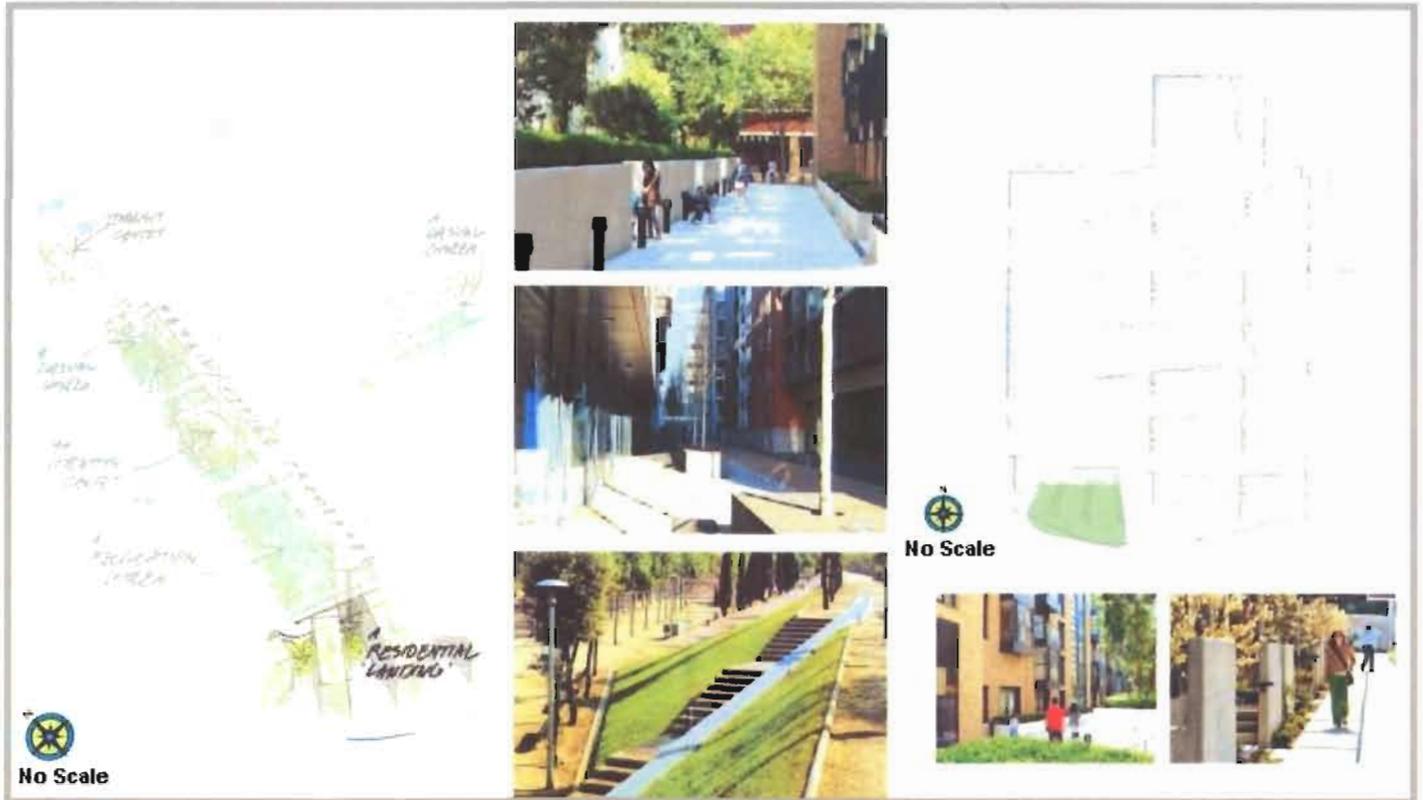
- Frontage Improvements. As each Design Review application is submitted for individual buildings/phases, frontage improvements will be provided along 124<sup>th</sup> Avenue NE, 120<sup>th</sup> Avenue NE, NE 12<sup>th</sup> Street, and NE 15<sup>th</sup> Street.
- "Pedestrian" Gateway Element. A pedestrian gateway element will be located at the southwest corner of the property between Buildings 18 & 19. The gateway will provide pedestrian access from the site (NE 13<sup>th</sup> Street and 121<sup>st</sup> Avenue NE) to the city streets at NE 12<sup>th</sup> Street and 120<sup>th</sup> Avenue NE. The pedestrian connection will be designed and permitted during the Design Review process for the first building or phase developed adjacent to the area. Refer to Conditions of Approval regarding Gateway in Section X.A.

### Gateway Corner 120<sup>th</sup> Avenue NE and NE 12<sup>th</sup> Street



Graphic representation

### Gateway Images



Graphic representation

- Limited surface parking areas. The majority of the parking stalls are located in underbuilding parking garages or above-ground parking structures out of public view.
- Pedestrian and vehicular circulation within the site. There will be clear pedestrian connections within The Spring District. The applicant will provide sidewalks along all city/private streets within the project limits. Residents can walk to Sound Transit from their residential units past the park areas to the station. There will be the pedestrian gateway element (as discussed above) which will provide pedestrian access from NE 12<sup>th</sup> Street to the site. Vehicular circulation is planned to provide clear connections for residents, office workers and visitors throughout the site. Refer to Conditions of Approval regarding Pedestrian Amenities and Street Development in Section X.A.
- Sidewalk level retail uses (below office and residential). The sidewalk level uses will provide more pedestrian activity to create a vibrant neighborhood.

## **B. Review Processes**

A Master Development Plan is a Process II application (LUC 20.35.200) with an administrative decision by the Director of Development Services (LUC 20.30V). The SEPA Determination is also a Process II decision with administrative decision by the Environmental Coordinator. Appeals are heard and decided by the Hearing Examiner for Process II applications. Minor changes to the approved MDP may be processed as a Land Use Exemption. Refer to Conditions of Approval regarding Modification to MDP in Section X.A.

## **II. SITE CONTEXT and DESCRIPTION**

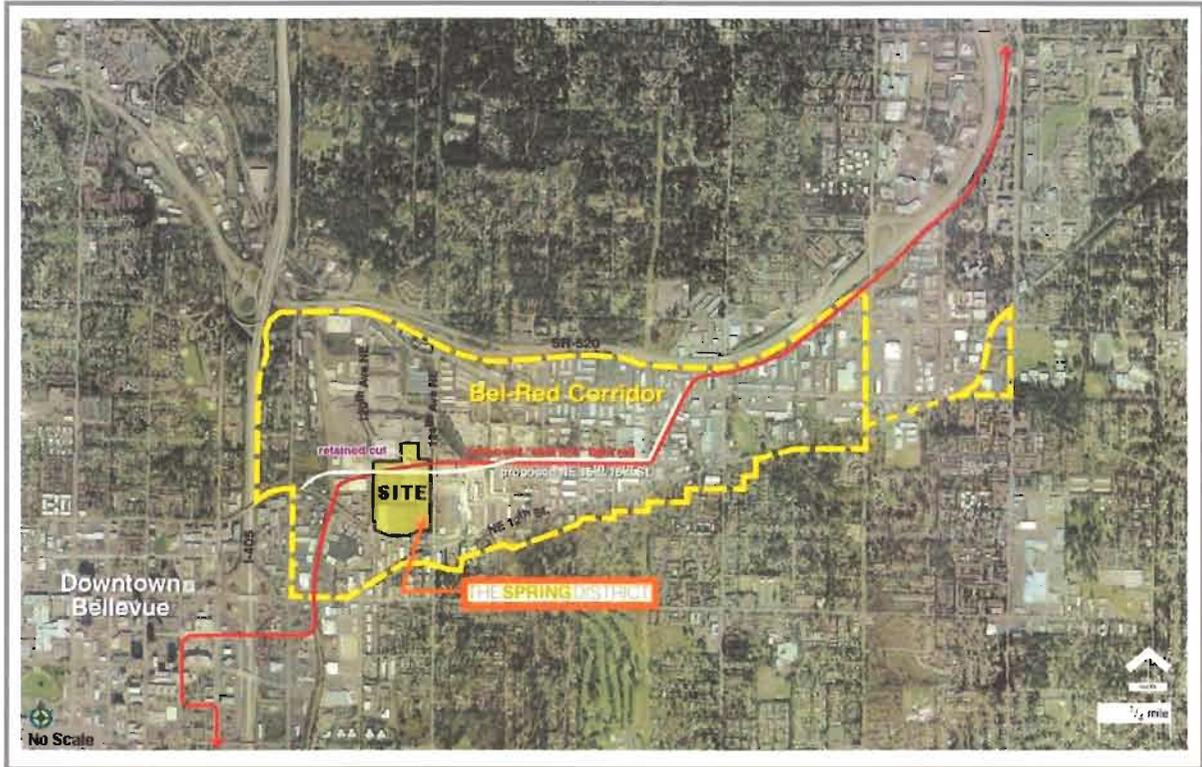
### **A. Site Context**

The Spring District site is located in the Bel-Red Corridor of Bellevue. It is located between 120<sup>th</sup> Avenue NE, NE 12<sup>th</sup> Street, and 124<sup>th</sup> Avenue NE. The site is close to two freeway systems: State Route 520 (about 0.3 miles to the north) and Interstate 405 (0.5 miles to the west). The site's current uses are industrial in nature and include dry/cold warehouse storage with distribution, vehicle storage, manufacturing, and accessory uses. Several current tenants include Orca Bay Seafoods, AmazonFresh, Coca-Cola and MV Transportation. The existing Bellevue Distribution Center will remain onsite and continue in operation in the early phases of the project until eventually replaced.

The existing Safeway Ice Cream and Milk production buildings (north of the site) are owned by Safeway and are anticipated to remain in place for the duration of the project. These buildings are not within the boundary of The Spring District.

Office and light industrial uses are located to the northeast and west of the site. A car dealership (Barrier Motors) is located to the west at NE 15<sup>th</sup> Street. Retail uses are located south of NE 12th Street. The Lake Bellevue neighborhood (restaurants/retail, office, multi-family use) is located to the southwest of the site. Residential neighborhoods (Wilburton neighborhood, multi-family units) are located to the south /southeast of the site.

Vicinity Map



Aerial Photograph



## B. Site Description

The Spring District site is approximately 36 acres in size and is currently being used for light industrial uses (warehouse, distribution, bus storage). About 89% of the site is covered with impervious surface (buildings, driveways, paved parking lots). There are currently 6 structures on the site, totaling 700,000 gross square feet. Over the phasing of this development, these structures will be demolished. There is no native vegetation onsite. As viewed from the adjacent streets, the site appears as a typical industrial site with large box concrete buildings (up to 2 stories in height), truck loading docks, roll-up warehouse doors, chain link fences with barbed wire, wide expanses of paved driving surfaces, and semi-trucks maneuvering onsite.

### Existing Site Conditions



History of Site: Beginning in the 1910s, The Spring District property and adjacent areas contained several single-family homes with the balance of the land being used for agricultural purposes. Beginning in the 1950s, Safeway Inc. substantially graded the site to flatten and facilitate development of distribution and processing plants. Additions and improvements to the warehouses continued into the 1970s. In 2005, Safeway relocated much of their operation to a new plant in Auburn, WA. Also in 2005, the City began a process to rezone the 900-acre Bel-Red corridor from industrial to mixed-use. In 2007, Wright Runstad & Company in partnership with Shorestein Properties purchased 36 acres of surplus Safeway land located around the future 122<sup>nd</sup> development node. In 2009, the City completed its rezone and executed a Development Agreement with The Spring District to be implemented through a new Master Development Plan for the property.

“The Spring District” Naming Process: The applicant purchased the former Safeway Distribution Site in May 2007 and subsequently embarked on a process to select a name compatible with the future mixed-use development vision. The applicant subsequently went through a process of talking with dozens of stakeholders including Eastside natives and others familiar with the property and industry to explore historical uses, natural

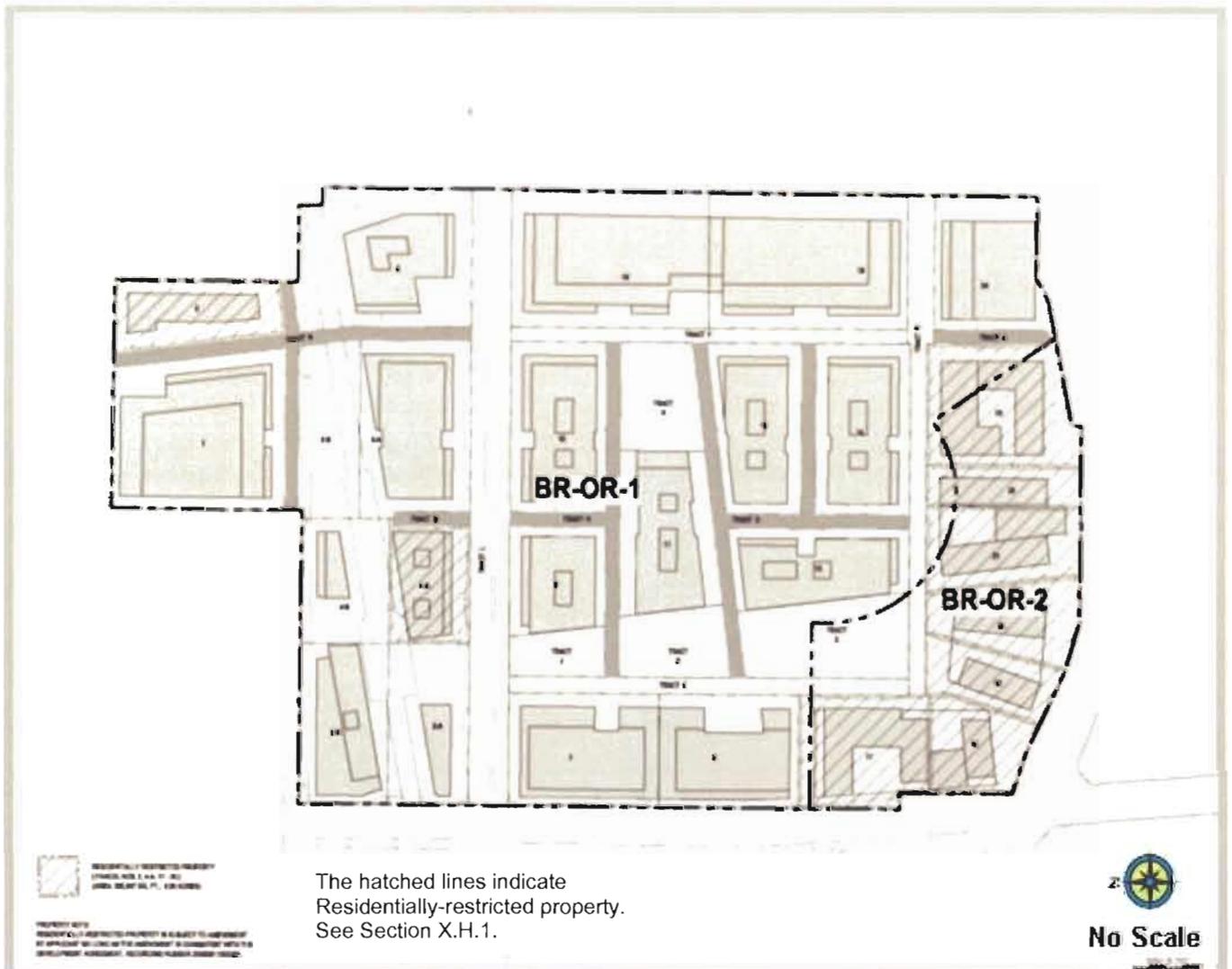
features, and attributes or characteristics common in the Northwest. The Spring District emerged as the preferred name for several reasons: 1) Spring represents a time of year when there is rebirth and new beginnings, and 2) District represents something larger than an individual parcel. District is used in other neighborhoods that have successfully transitioned from industrial to mixed-use including the Pearl District in Portland, Oregon. The applicant's goal was to create a name that is memorable and easily compatible with the naming of the future high capacity transit station.

### III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS

#### A. General Provisions of the Land Use Code

The site is located in the Bel-Red Land Use district. As such, the Master Development Plan is subject to the Bel-Red requirements of Land Use Code 20.25D. The proposal meets the requirements of Land Use Code 20.25D as follows:

#### Zoning Map



**TABLE 2  
 DIMENSIONAL REQUIREMENTS**

ITEM	PERMITTED/REQUIRED	PROPOSED	COMMENTS
<b>ZONING</b>	Zoning districts Bel-Red OR-1 and OR-2.	No change.	LUC 20.10.375.A.4, 5
<b>ALLOWED LAND USES</b> LUC 20.25D.070	Office, residential, hotel, retail, restaurant and parks.	Office, residential, hotel, retail, restaurant and parks.	Proposed land uses meet the Land Use Code.
<b>BUILDING HEIGHT</b> LUC 20.25D.080	Maximum 150' within BR-OR- 1 and maximum 125' within BR-OR-2.	Maximum 150' within BR-OR-1 and maximum 125' within BR-OR-2.	Massing studies show that building heights will comply with the Land Use Code. Building heights will be confirmed with each new Design Review application for individual buildings.
<b>MAXIMUM IMPERVIOUS SURFACE AREA /LOT COVERAGE</b> LUC 20.25D.080	Maximum 75%.	89% existing.	The overall project will not retain any existing buildings. The maximum impervious surface/lot coverage is 75% MDP area-wide. Refer to Conditions of Approval regarding <u>Impervious Surface/Lot Coverage</u> in Section X.B.
<b>BUILDING SETBACKS</b> - Front - Rear - Side LUC 20.25D.080	0' setback subject to stepbacks/setbacks per Notes (2) and (14) of LUC 20.25D.080.A.	Proposed building locations are conceptual at this MDP stage.	Building setbacks will be confirmed with each new Design Review application for individual buildings.
<b>PARKING STALLS</b> LUC 20.25D.120	Parking stall calculations must meet LUC 20.25D.120.	Approximately 10,000 parking stalls are proposed for the entire MDP.	Each building must have required parking to meet code requirements. Parking will be calculated at each Design Review application. Refer to Conditions of Approval regarding <u>Parking Stall Calculations</u> in Section X.B.

ITEM	PERMITTED/REQUIRED	PROPOSED	COMMENTS
<p><b>SITE LANDSCAPING</b>            - Street frontage            - Interior boundaries            - Surface Parking area            LUC 20.25D.110            LUC 20.20.520.</p>	<p>Site landscaping required per LUC 20.25D.110:            6' sidewalks, 5' planting strip with street trees and groundcover.            Type V surface parking area landscaping.</p>	<p>None shown at the MDP stage. Artist renderings show conceptual landscape designs.</p>	<p>Site landscaping will be confirmed with each new Design Review application for individual buildings.</p> <p>The overall MDP must have a coordinated perimeter landscape plan along each right-of-way of 120<sup>th</sup> Avenue NE and 124<sup>th</sup> Avenue NE. Refer to Conditions of Approval regarding <u>Coordinated Landscape Plan along Site Perimeter</u> in Section X.B.</p>
<p><b>REFUSE &amp; RECYCLING AREA</b>            LUC 20.20.725</p>	<p>Refuse and recycling areas may be not located within adjacent public/private streets or adjacent rights-of-way. These activities must be located on each individual building site and screened from public view.</p>	<p>None shown at the MDP stage.</p>	<p>Refuse and recycling areas will be confirmed with each new Design Review for individual buildings. Refer to Conditions of Approval regarding <u>Recycling and Solid Waste Collection</u> in Section X.B.</p>
<p><b>BINDING SITE PLAN</b>            LUC 20.30V.140</p>	<p>The applicant may request approval of a Binding Site Plan with the MDP approval.</p>	<p>Applicant is preparing a Binding Site Plan submittal.</p>	<p>Required for sale or lease of lots/tracts. Refer to Conditions of Approval regarding <u>Binding Site Plan</u> in Section X.A.</p>
<p><b>SIGN MASTER PLAN</b></p>	<p>A Sign Master Plan is required for the entire MDP project.</p>	<p>Not shown at the MDP stage.</p>	<p>Required Sign Master Plan with the first Design Review application. Signs must be consistent within the MDP project. Refer to Conditions of Approval regarding <u>Sign Master Plan</u> in Section X.A.</p>

**Further Land Use Code Topics:**

**1. Art Concept**

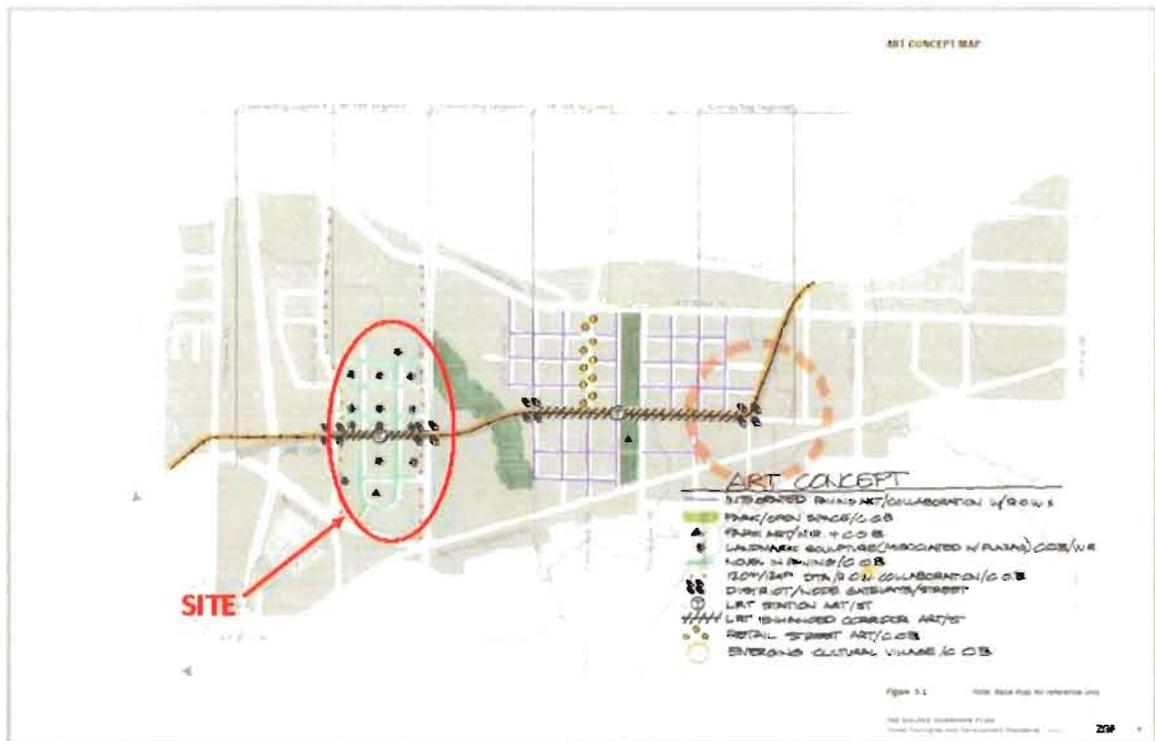
Public art is an important element to The Spring District. It places a role in creating unique identities for neighborhoods. Public art can help create a cohesive sense of place. It can serve as a wayfinding tool for the public. And, public art can help the growth of a cultural district.

The Design Guidelines of LUC 20.25D.150 state that each development within a Bel-Red land use district *must* comply with the provisions of the Bel-Red Subarea Design Guidelines contained within this section. The provisions of the Design Guidelines *will* be applied pursuant to the review requirements of LUC 20.25D.030. (*emphasis added*)

LUC 25D.150.B.5.a states that the intent of art in the Bel-Red Subarea should complement the character of a site, building or district as a whole. Art should be integrated into the design of the building or outdoor space.

Page 1 of the Bel-Red Corridor Plan (effective date of September 13, 2011), states that the plan is intended to supplement Part 20.25D - Bel-Red. Within this Corridor Plan the applicable pages are Figure 3.1 (see below) and Appendix C “Public Art Considerations.”

**Figure 3.1 of the Bel-Red Corridor Plan**



The above graphic from the Bel-Red Corridor Plan shows the location of art concepts within The Spring District. The plan presents a menu of options which illustrate the types, sizes and locations of public art within neighborhoods. Within The Spring District, the plan shows the location of approximately a dozen landmark sculptures (associated with a plaza). It also portrays NE 15<sup>th</sup> Street as a district/node gateway/street. And at the southern end of The Spring District, the plan shows a park with art. Appendix C of the Bel-Red Corridor Plan has specific recommendations for The Spring District.

The Bel-Red Corridor Plan lays out a number of opportunities for private, public and community stakeholder partnerships, some of which are between the City of Bellevue and Wright-Runstad & Company.

As an integral part of The Spring District Project, an art concept must be installed within one of the public parks/plaza/gateways and reviewed under the subject Design Review application. Review/approval by the City of Bellevue Arts Commission is not required. The art concept shall be installed prior to completion/final certificate of occupancy of The Spring District. Refer to Conditions of Approval regarding Art Concept in Section X.A.

## **2. Multi-Purpose Trail**

A multi-purpose trail (12-14 feet in width) shall be provided along the entire length of The Spring District's property along 124<sup>th</sup> Avenue NE to connect to the multi-purpose trail on NE 16<sup>th</sup> Street as shown on the MDP plans. See Attachment A. For Phase 1a, the applicant shall design the entire trail (for both Phase 1a and Phase 1b) and construct the Phase 1a trail adjacent to any Phase 1a development. For Phase 1b, the applicant shall complete construction of the entire trail. Refer to Conditions of Approval regarding Pedestrian Amenities in Section X.A.

## **3. Landscaping along 124<sup>th</sup> Avenue NE**

The area within the Seattle City Light easement (adjacent to 124<sup>th</sup> Avenue NE) shall be landscaped along with a multi-purpose trail. Vehicular parking areas, vehicular lanes or load/unload areas will be prohibited within this easement area. This landscaping shall be installed as a part of Phase 1. Refer to Conditions of Approval regarding Pedestrian Amenities in Section X.A.

## **4. Mid-block Public Pedestrian Connection**

A mid-block public pedestrian connection (12-14' wide) between Buildings 15 & 16 (as shown on the MDP plans, Attachment A) shall be provided from 123<sup>rd</sup> Avenue NE to 124<sup>th</sup> Avenue NE and shall meet ADA accessibility requirements and the intent of the Green Streets Development Standards. The location and design shall meet the intent of LUC 20.25D.140.D. This

mid-block public pedestrian connection with its associated easement and signage shall be installed as a part of Phase 1. Refer to Conditions of Approval regarding Pedestrian Amenities in Section X.A.

**5. Outward Focus of Buildings**

During the Design Review of individual buildings on the perimeter (124<sup>th</sup> Avenue NE, NE 12<sup>th</sup> Street, 120<sup>th</sup> Avenue NE), the applicant shall provide building designs that convey an outward focus toward the city streets as well as toward the interior of the development. The use of blank walls or flat nondescript walls that are not articulated by any visual interest is not consistent with applicable design criteria. Refer to Conditions of Approval regarding Outward Focus of Perimeter Buildings in Section X.B.

**6. “Pedestrian” Gateway Connection**

A “Pedestrian” Gateway Connection shall be located at the southwest corner of the property between Buildings 18 & 19. The gateway shall provide pedestrian access from the site (approximately NE 13<sup>th</sup> Street and 121<sup>st</sup> Avenue NE) to the adjacent city street at NE 12<sup>th</sup> Street (near 120<sup>th</sup> Avenue NE). The pedestrian connection shall be designed and permitted during the Design Review process for the first adjacent building to the gateway. Refer to Conditions of Approval regarding Pedestrian Amenities in Section X.A.

**7. Other Gateway Opportunities**

The Land Use Code provides Design Guidelines within LUC 20.25D.150 for “Character and Site Guidelines.” The purpose of these guidelines is to address the qualities that make the Bel-Red Subarea unique. They consider what makes an area a special, distinct “place,” not simply a group of individual buildings and streets. One of the elements of this chapter is to “Establish and Strengthen Gateways.” The intent is to use architectural and landscape elements to mark transitions and entrances. The guideline states that entrances into and within the Bel-Red Subarea should be celebrated at many levels. Pedestrians, cyclists, transit passengers, and motorists should experience a sense of “entering” or moving into the area as well as unique districts or neighborhoods in the subarea.

The applicant will be required to provide a gateway entrance at the street level for pedestrians, motorists, and those passing by the site. The location and timing of the construction of this gateway entrance at street level shall be determined during Phase 1. Likely locations would be the entrance to the site at NE 13<sup>th</sup> Street (from 124<sup>th</sup> Avenue NE) or NE 15<sup>th</sup> Street (from 120<sup>th</sup> Avenue NE or 124<sup>th</sup> Avenue NE). Refer to Conditions of Approval regarding Other Gateway Opportunities in Section X.A.

**8. Public Plaza at the Sound Transit 120<sup>th</sup> East Link Station**

As a part of the master planning process for the Sound Transit 120<sup>th</sup> East Link Station, the applicant should coordinate with the City of Bellevue and Sound Transit regarding the design of a public plaza adjacent to Sound Transit's 120<sup>th</sup> East Link subterranean light rail station. The applicant should designate the plaza as a "public plaza" on the recorded Master Development Plan. Refer to Conditions of Approval regarding Public Plaza at the Light Rail Station in Section X.A.

**9. Amenities**

According to LUC 20.30V.130, the phasing plan for the MDP shall provide for proportionate installation of amenities that must be included when each phase of development is constructed. As such, the city will require amenities for each Design Review issued for a building/phase. Amenities may not be "borrowed" from future phases.

The Amenity Incentive System specifies tiers for amenities for residential development and non-residential development (LUC 20.25D.090). The applicant will need to meet the amenities outlined in the Development Agreement as well as the Land Use Code.

For the purpose of complying with the FAR Amenity Incentive System (LUC 20.25D.090 and 20.25D.035.B), the applicant shall calculate FAR amenities using a rolling average across the approved Master Development Plan, establishing an initial FAR for non-residential development of 3.1 and residential development of 4.0 (for a cumulative FAR of 3.27), as illustrated in Attachment B. With each Design Review application submitted under the MDP, the applicant shall include the physical FAR of the proposed project, as well as the calculated FAR of the project as part of the overall MDP rolling average. At no time shall the averaged MDP FAR fall below 2.5 or exceed 4.0. Use of an alternate MDP FAR calculation or a change to the target MDP FAR must be reviewed and approved by the City of Bellevue as a modification, processed as a Land Use Exemption application to Design Review or as a modification request with the subject Design Review application. Refer to Conditions of Approval regarding Amenities and FAR for each Design Review in Section X.B.

**10. Request for Modification to Bel-Red Street Development Standards per LUC 20.25D. 140**

Due to the new site configuration (from when the Bel-Red Land Use Code amendments were adopted), the following modifications are requested by the applicant.

See Attachment H for maps portraying locations of modification requests.

**(a) Required Local Streets per LUC 20.25D.140.A.** See Attachment H.

The Land Use Code requires The Spring District to meet the Required Local Streets per LUC 20.25D.140.A. The intent of the local streets grid is to introduce a public right-of-way system that improves mobility by increasing access for local vehicular and pedestrian traffic through-out the Bel-Red District.

Figure 20.25D.140.A identifies the general location of new local streets, including a local street connection from the site to NE 12<sup>th</sup> Street (near 120<sup>th</sup> Avenue NE). The applicant requests a modification of this local street connection.

The applicant requests that this local street connection be deleted due to topographic considerations. There is more than a 20-foot drop in grade from the site to NE 12<sup>th</sup> Street at this location. It is not practical to build a local street at this location as it would be very steep. The steepness of the grade would make it difficult for both vehicles and pedestrians. A flat vehicular landing area at the intersection with NE 12<sup>th</sup> Street would not be feasible. Meeting pedestrian routes that meet ADA guidelines are also difficult along this street alignment. In order to provide alternate vehicular routes to city streets, the applicant will provide two vehicular access points to underground parking garages, one along NE 12<sup>th</sup> Street and the other along 120<sup>th</sup> Avenue NE (signalized). The exact location of these garage driveways will be determined at the Design Review stage of individual buildings/phases. Refer to Conditions of Approval regarding Access to Underground Parking Garages in Section X.A.

According to LUC 20.25D.140.A.2, the Director may approve modifications to the local street grid to respond to specific site conditions, property ownership, and phasing considerations; provided, that the modified local street grid satisfies the intent of subsection A.1 of this section and meets the applicable standards below.

The Director finds that the requested modification to delete the local street connection from the site to NE 12<sup>th</sup> Street meets the criteria of LUC 20.25D.140.A.2 and therefore approves the modification request with the subject MDP decision as follows:

- This modification approval to delete the local street connection from the site to NE 12 Street is applicable to future reviews of Design Review applications.
- In order to provide alternate vehicular routes from the city streets to the site, the applicant will provide 2 vehicular access points to underground residential parking garages, one along NE 12<sup>th</sup> Street and the other along 120<sup>th</sup> Avenue NE. The exact location of these parking garage driveways will be determined at the Design Review stage of individual buildings/phases.

Refer to Conditions of Approval regarding Access to Underground Parking Garages in Section X.A.

**(b) Green Streets per LUC 20.25D.140.D.** See Attachment H.

The Land Use Code requires The Spring District site to meet the Green Streets requirement per LUC 20.25D.140.D. The purpose of the Green Street is to utilize natural drainage systems, to the extent feasible, to improve and reduce the amount of stormwater runoff at its source. Specific plant materials are designated for Green Streets (LUC 20.25D.110.B). The applicant must fulfill the intent of “Green Streets” per LUC 20.25D.140.D and be consistent with the Bel-Red Corridor Plan Streetscape Character, Guidelines and Standards

Based on site reconfiguration and street realignment (from when the Bel-Red Land Use Code amendments were adopted), the applicant has identified the need to modify the identified streets to in order to meet the intent of the Bel-Red code. The original design included a park in the “horseshoe” between approximately NE 14th, NE 13th, 121st and 123rd Avenue. The park was intended to be integral to the green street. The redesigned park runs north-south, anchored by the high capacity transit station just north of NE 15th at 121st Avenue. Shifting the green street north to the loop roads will have the opportunity to more effectively incorporate the green elements of the project to the green street network and proposed parks within the MDP. The shifting of the Green Street north to the loop roads will provide a more pedestrian-friendly environment (since the loop roads will have less traffic than NE 13<sup>th</sup> Street), and will provide twice the amount of Green Streets since the applicant will be shifting from one street (NE 13<sup>th</sup> Street) to two streets (the loop roads). See portrayal in Attachment H.

According to LUC 20.25D.140.D.2, the Director may approve the final location of Green Streets to respond to specific site conditions, property ownership, and phasing considerations; provided that the final location satisfies the intent of subsection D.1 of this section and meets the applicable standards below.

The Director finds that the requested modification of the Green Streets meets the criteria of LUC 20.25D.140.D.2 and therefore approves the modification request with the subject MDP decision as follows:

- This modification approval to relocate the Green Street from NE 13<sup>th</sup> Street to the loop roads is applicable to future reviews of Design Review applications.
- The applicant shall provide a mid-block public pedestrian connection between Buildings 15 & 16.

Refer to Conditions of Approval regarding Pedestrian Amenities in Section X.A.

**11. Addressing of Buildings/Tracts**

The applicant shall contact Jami Carter, Information Technology Department, phone 425-452-4310 regarding the addressing of buildings/tracts. The Planning and Community Development Department shall review and approve such addressing.

Refer to Conditions of Approval regarding Addressing of Buildings/Tracts in Section X.A.

**12. City Council Approval of NE 15<sup>th</sup> Street/NE 16<sup>th</sup> Street and portrayal on the Binding Site Plan**

The final NE 15<sup>th</sup> Street/16<sup>th</sup> Street alignment (horizontal/vertical) has not been approved by the City Council as of the publication date of this staff report (April 26, 2012). And, the applicant wishes to record the submitted Binding Site Plan (12-110450-LF) with the NE 15<sup>th</sup> Street/NE 16<sup>th</sup> Street alignment as soon as possible after the final decision of the Master Development Plan. Therefore, a condition will be imposed with this MDP approval to allow flexibility in the location and width of the NE 15<sup>th</sup> Street/16<sup>th</sup> Street alignment (horizontal/vertical).

If shifting of the NE 15<sup>th</sup> Street/NE 16<sup>th</sup> Street alignment (horizontal/vertical) at a later date is in conflict with the recorded BSP, the applicant shall amend the BSP to match the City Council approved alignment (horizontal/vertical) of NE 15<sup>th</sup> Street/NE 16<sup>th</sup> Street. Such amendment of the BSP shall occur prior to submittal of the next Design Review (DR) application for the MDP site after the final alignment is approved. The vested status of final street locations does not occur until an adjacent DR is issued.

Refer to Conditions of Approval regarding City Council Approval of NE 15<sup>th</sup> Street/NE 16<sup>th</sup> Street and portrayal on the Binding Site Plan in Section X.A.

**13. BROTS Interlocal Agreement Limitation**

According to LUC 20.25D.040, development in the Bel-Red land use districts is subject to the provisions of the BROTS Interlocal Agreement. This Agreement between the cities of Bellevue and Redmond addresses land use planning and the funding and construction of transportation improvements in the Bel-Red/Overlake Transportation Study Area. In the case of The Spring District, compliance with BROTS and this Land Use Code requirement will be applied with each new Design Review application.

## **B. Development Agreement**

On July 10, 2009, a Development Agreement (DA) was signed between the City of Bellevue and WR-SRI 120<sup>th</sup> LLC. The Development Agreement was recorded on September 11, 2009. This agreement required specific items for the development of the property known as "The Spring District." See Attachment D.

Note: Items are listed in the order of the Development Agreement:

### **A. Spring District Development Components**

#### **1. Must Meet Catalyst Project Criteria (Attachment D, page 4) Bel-Red Code 20.25D.035: Catalyst Project Definitions:**

- a. The project must be located in a Bel-Red Land Use District node.

Finding: The Spring District is located in the 122<sup>nd</sup> Avenue NE node (Policy S-BR-87).

- b. The project limit is greater than 4 acres in size.

Finding: The Spring District is 36.01 acres, thus exceeding the minimum 4 acres for a Catalyst Project.

- c. FAR within the project limit is proposed to exceed 2.0.

Finding: The proposed FAR is 3.27 which exceeds the minimum 2.0 FAR.

- d. A complete Master Development Plan (MDP) application is submitted within eighteen (18) months of Federal Transit Authority issuance of a Record of Decision on the Sound Transit East Link Project, or prior to December 31, 2011, whichever is later.

Finding: The Master Development Plan was submitted November 7, 2011 in advance of the December 31, 2011 deadline. Sound Transit's Record of Decision was issued November 2011 setting the outside deadline for MDP submission at approximately May 2013.

- e. The Construction Value associated with Developer Funded Infrastructure exceeds \$300,000 per acre of land included within the project limit.

Finding: The Development Agreement references the Catalyst Project criteria outlined in the Bel-Red code (20.25D.035.A) which requires Developer Funded Infrastructure to exceed \$300,000 per acre within

the project limit in order to qualify as a Catalyst Project. At \$300,000 per acre x 36.01 acres, the applicant would need to spend \$10,803,000 for the entire project limit of The Spring District. Within the first phase alone, the applicant estimates costs of infrastructure to be approximately \$11 million dollars (Attachment G) which exceeds the \$10.8 million required for the entire site. Therefore, the applicant will have complied with this code requirement within the first phase. The applicant estimates that total Spring District infrastructure costs will exceed \$50 million dollars or +/- \$1,400,000 per acre upon completion of all phases.

**2. Minimum Contents of MDP (Attachment D, page 4)**

- a. Owner agrees to include in its application for an MDP development across the Property with a minimum average FAR of 2.5.

Finding: The proposed FAR is 3.27 which exceeds the minimum FAR of 2.5.

- b. The MDP shall include the Residentially-restricted Property (as defined in Section H.)

Finding: The proposal meets this requirement. See Section H.1 below for response.

- c. The first phase of any MDP approved under this Agreement shall include development of a public mini-park a minimum of one acre in size (designated as project M-3 in the Bel-Red Parks and Open Space Project List in the Comprehensive Plan) and an activated park or recreation space of at least 30,000 contiguous square feet.

Finding: The Bel-Red Subarea Plan indicates a mini-park, M-3 to be located between 120<sup>th</sup> Avenue NE and 124<sup>th</sup> Avenue NE, south of NE 15<sup>th</sup>/16<sup>th</sup> Street. The Plan further states that park facilities serve residential and commercial mixed use development. The proposal meets this requirement.

At the end of Phase 1, the applicant will complete the park area requirement with approximately 1 acre and 32,440 square feet in total which exceeds the DA requirement of 1 acre and 30,000 square feet. Of the 1 acre and 32,440 square feet, approximately 32,440 square feet are located within an activated park or recreation space which meets the DA requirement of at least 30,000 contiguous square feet.

Review and approval of the park areas shall be conducted under Design Review approval, with input by the City of Bellevue into

the design of the park areas. The applicant has indicated that they wish to own and maintain the subject parks. If so, the applicant must record a public easement over the park tracks. Refer to Conditions of Approval regarding Parks under the Development Agreement and Review Process for Parks/Plaza/Gateways in Section X.A.

**B. Subsequent Land Use Review** (Attachment D, page 4)

1. Governing regulations are specified in Sections A and C of the DA.

Finding: See Attachment D.

2. If the MDP application is approved, then such approval shall be vested for a period of fifteen (15) years from the date of the final decision (as defined in Section 20.35.045 of the LUC) on the MDP (the "Vesting Period"). During the Vesting Period and subject to Section T (of the DA), the City shall not impose any modification of or new or additional Governing Regulations on the MDP or any Land Use Code approvals required for The Spring District consistent with this Agreement and the MDP. To the extent that neither this Agreement nor the Governing Regulations specified below address a certain subject, element or condition of the Project, then the Project shall be governed by the City's then-existing code.

Finding: The MDP was submitted per the requirements of the DA. As such, the MDP will vest for 15 years upon approval. Refer to Conditions of Approval regarding Vested Status of the Master Development Plan in Section X.A.

3. The vesting period shall only apply to Catalyst Projects on the Property.

Finding: The Spring District is a Catalyst Project in its entirety.

**C-G. Other Stipulations** (Attachment D, pages 5-8)

Finding: See Attachment D. These stipulations include amenity rates, concurrency, timing and vesting.

**H. Catalyst Project Residential Requirements and Delay Penalty**

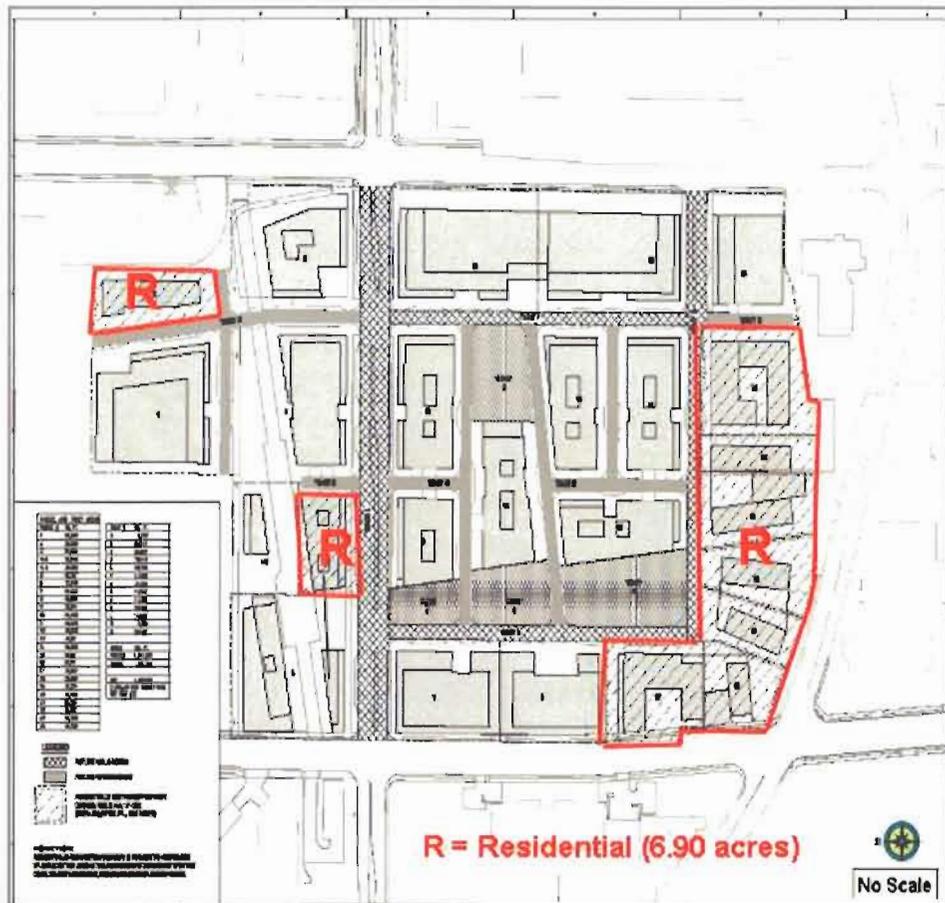
**1. Designation of Residentially-restricted Property**

The Master Development Plan must designate at least 5.8 acres within the MDP for residential use and associated required ground floor commercial or retail uses, exempt ground floor retail and exempt childcare or nonprofit space (as described in LUC Section 20.25D.090). Such area shall be designated as the "Residentially-restricted Property." The MDP shall further establish a requirement that a minimum of 784,000 square feet of residential development, exclusive of the associated uses described above,

be developed within the Residentially-restricted Property. A covenant restricting use shall be recorded against the property and run with the land.

Finding: The applicant has proposed 6.90 acres of Residentially-restricted property per the Development Agreement which exceeds the DA requirement of at least 5.8 acres. A condition of approval of this MDP will require the applicant to record the appropriate covenant with each Design Review for Residentially-restricted property. Refer to Conditions of Approval regarding Residential Property under the Development Agreement in Section X.A. The applicant anticipates amending the Residentially-restricted Property with each Design Review; the covenant will be updated accordingly.

### Residentially-restricted Properties



## 2. Required Timing of Development

The Development Agreement (DA) stipulates timing of the Residentially-restricted property in regard to the Sound Transit station. The Residentially-restricted property shall either be sold, developed or proportionally developed prior to the date of Sound Transit's regularly scheduled passenger-carrying service or be subject to the penalties as described in the DA.

Finding: The applicant shall meet the DA requirement that Residentially-restricted property shall either be sold, developed or proportionally developed prior to the date of Sound Transit's regularly scheduled passenger-carrying service or be subject to delay penalties as described in the DA. Refer to Conditions of Approval regarding Residential Property under the Development Agreement in Section X.A.

### H.3.-4. Other items in the Development Agreement (Attachment D, page 8)

Finding: See Attachment D. These items discuss the Residentially-Restricted property.

The applicant shall meet all DA requirements, including any not noted specifically in this staff report. Refer to Conditions of Approval regarding Other DA Requirements under the Development Agreement in Section X.A.

## IV. PUBLIC NOTICE AND COMMENTS

Application Dates: November 07, 2011  
Notice of Application: December 15, 2011  
Minimum Comment Period: December 29, 2011

The minimum required public comment period ended December 29, 2011. However, comments were accepted up to the date of this decision. The City received email communication from about a dozen people. The applicant and the city held individual meetings with the public to discuss and clarify the proposal and address concerns.

The following is a list of neighbor concerns and city response:

**Comment #1:** As the flagship of Bel-Red planning, a project of this scope warrants a public hearing before Master Development Plan approval, since there have been many economic and demographic changes in the five years since the Bel-Red Corridor Project Draft Environmental Impact Statement (DEIS) public hearing. The development needs to be re-evaluated in light of current conditions and constraints. Future projections, particularly for traffic and transportation systems, need to be updated for sound decision making.

Response:

See LUC Part 20.30V and 20.35.227. As a Process II application, public hearings are not required for Master Development Plan approval. Extensive public meetings were held before adoption of the Bel-Red Subarea Plan. In addition, there was significant public involvement prior to the public hearing on the Bel-Red Corridor Project DEIS. See Appendix B of the Final Environmental Impact Statement. (FEIS).

The proposal is being evaluated under current code provisions as required by law using the existing environmental documents for the Bel-Red Subarea along with additional information submitted by the applicant. Development allowed under the Master Development Plan will be subject to traffic concurrency analysis, design review, environmental review, and other permit requirements during design review for individual phases or buildings.

**Comment #2:** Please extend the public comment period through January 2012 with an open house and public hearing in February.

Response:

See LUC Part 20.30V, 20.35,225, and 20.35.227. The minimum required public comment period (14 days) ended December 29, 2011. That is just the “minimum” comment period before which the City of Bellevue can make a decision. Comments on The Spring District project were accepted up to the date of this MDP publication. As a Process II application, public hearings are not required for Master Development Plan approval. The City received email communication from about a dozen people. The applicant and the city held individual meetings with the public to discuss and clarify the proposal and address concerns. By the time of publication of this staff report, the public had approximately four months of time to comment on the proposal.

**Comment #3:** The Development should include enough tree canopy to offset its carbon footprint.

Response:

The Spring District will be developed using Leadership in Energy and Environmental Design (LEED) standards where feasible for the construction. Although the applicant has provided an estimate of the project’s greenhouse gas emissions in the environmental review documentation, there is no legal requirement for a project to offset its carbon footprint.

**Comment #4:** Will the buildings meet LEED certification standards?

Response:

There is no requirement for the buildings to meet LEED standards, but the applicant is currently planning to do so. The applicant also intends the site design to meet Leadership in Energy and Environmental Design (LEED-ND) standards.

**Comment #5:** In public comments from the January 31, 2008, Bel-Red Open House, a citizen recommended that the City mandate rooftop gardens for buildings over a certain number of stories. Will The Spring District showcase green roofs, as the City of Vancouver, British Columbia has done so brilliantly?

Response:

The applicant may include rooftop gardens but it is not a requirement under the Land Use Code or Development Agreement that was ultimately adopted by City Council in response to the referenced public comment. Individual buildings will be subject to design review under LUC 20.25D.030.C.

**Comment #6:** How will storm water and surface runoff be handled to protect the environment?

Response:

See Bellevue City Code (BCC) Chapters 24.06 and 23.76. An on-site stormwater management plan will be required prior to issuance of building permits and must be consistent with the cited chapters and the City's Surface Water Engineering Standards. Section D6-01.1 and other portions of the City's Surface Water Engineering Standards describe storm and surface water management provisions required for new development.

**Comment #7:** Fifteen years is a long development cycle. What mitigation is planned for noise, dust, and runoff during construction? We often see dump trucks with uncovered loads of fill dirt driving on the road. Will the developer agree to cover those loads?

Response:

The City will require appropriate construction dust mitigation measures during all stages of development, as imposed by issued permits. While construction is underway, city inspectors will visit the site, sometimes daily, for inspection of progress onsite per the issued permits and any inspect for possible violations.

According to the Clearing & Grading Code BCC 23.76.095 under "Dust Suppression," dust from clearing, grading, and other construction activities shall be minimized at all times. Impervious surfaces on or near the construction area shall be swept, vacuumed, or otherwise maintained to suppress dust. Watering the site to suppress dust is also prohibited unless it can be done in a way that keeps sediment out of the storm and surface water system and receiving waters.

The State law contained in Revised Code of Washington Section 46.61.655 also prohibits unsecured loads on public highways. No vehicle shall be driven or moved on any public highway unless such vehicle is so constructed or loaded as to prevent any of its load from dropping, sifting, leaking, or otherwise escaping therefrom, except that sand may be dropped for the purpose of securing traction.

If any neighbor observes an unsecured load, please call Code Compliance at 425-452-4570.

**Comment #8:** Since this is a transit-based development, does it require that the City's Mobility and Infrastructure Initiative projects be completed first?

Response:

The Mobility and Infrastructure Initiative is a group of transportation road investments in the downtown, west Wilburton and Bel-Red areas intended to respond to increased traffic spurred by downtown development and growth anticipated in the Bel-Red area. The City Council selected these investment projects after analyzing various alternatives because they will provide the infrastructure capacity needed to support planned development throughout the Downtown and Bel-Red, and were demonstrated to have the greatest effect in reducing congestion at full build-out.

This group of projects includes the widening of 120th Avenue NE (from NE 4<sup>th</sup> Street to Northup Way), the construction of a new surface street connection between 116<sup>th</sup> Ave NE and 120<sup>th</sup> Ave NE (NE 4<sup>th</sup> Street extension), construction of NE 15<sup>th</sup>/16<sup>th</sup> Streets (from 116<sup>th</sup> Avenue NE to 124<sup>th</sup> Avenue NE), and the expansion of 124<sup>th</sup> Ave NE north of Bel Red Road to Northup Way. Construction of these projects will be phased and implemented in response to actual build-out throughout the city. Construction funding is still being finalized. Consistent with the Mobility and Infrastructure Initiative, The Spring District also has a phasing plan that extends for approximately 15 years to full build-out. Accordingly, all development allowed under the Master Development Plan for The Spring District will be subject to traffic concurrency analysis, transportation mitigation, and additional environmental review at the time of building or phase-specific permitting. The timing and completion of the Mobility and Infrastructure Initiative projects will be taken into consideration during the transportation analysis and transportation mitigation for each building or phase during the Design Review process. To comply with concurrency requirements, funding for appropriate transportation infrastructure must be committed with each building/phase of the Spring District buildout.

**Comment #9:** Realistically, can the development be supported by planned infrastructure if light rail does not materialize?

Response:

Planned infrastructure for The Spring District includes not only new roads, but also bicycle lanes, sewer service, water service, storm water detention, public parks and parking stalls, all per city codes and standards. All of these improvements are planned for and will be phased in over the 15 year phasing period. If the East Link light rail does not materialize, the required infrastructure (such as improved roadway capacity and utilities) will still be in place per the approved phasing plan.

**Comment #10:** As a major trip generator, The Spring District will affect the surrounding community. If development proceeds before transportation improvements are in place, how does the City plan to handle the traffic burden?

Response:

Trip generation, as well as traffic concurrency analysis (pursuant to the Development Agreement) and environmental review, will be addressed on a building or phase-specific basis during the Design Review process. The results of the analysis will determine which transportation improvements will be required to be funded for that specific Design

Review application.

**Comment #11:** Will the eastbound on-ramp to 520 from 124<sup>th</sup> Avenue be complete before The Spring District is fully developed? This seems essential for a development with such a dense footprint.

Response:

Construction of an eastbound on-ramp to State Route 520 from 124<sup>th</sup> Avenue NE is under the jurisdiction of the Washington State Department of Transportation (WSDOT). Construction and timing of this ramp will be coordinated with the City of Bellevue Transportation Department (contact: Nancy LaCombe, phone 425-452-4382). At this time, the project is in the planning stages. An actual construction date has not been established. It is a high priority project in the corridor and is subject to funding by the state Legislature.

Review of The Spring District permits and construction according to the phasing plan will continue, regardless of the timing the future on-ramp construction. As noted earlier, traffic concurrency analysis and traffic mitigation will be addressed on a building or phase-specific basis during the Design Review process.

**Comment #12:** Will 120<sup>th</sup> Avenue NE improvements be complete in time for the development? The Wilburton neighborhood already experiences effects of cut-through traffic on 124<sup>th</sup> Avenue NE, where traffic volumes are projected to increase between 11% and 65% (according to the environmental impact statement). Without the 12<sup>th</sup> Avenue NE projects to improve flow from downtown and the freeway to The Spring District, 124<sup>th</sup> will be the road of least resistance. Safeway semi-trucks do not use 124<sup>th</sup> south of NE 8<sup>th</sup> Street; can the City assure the neighborhood that construction trucks working in The Spring District take other routes? Pedestrian safety is a major concern, since we have children and elderly citizens in the neighborhood, as well as Three Cedars School on 125<sup>th</sup> at 5<sup>th</sup> Street.

Response:

The Bel-Red Corridor Project Environmental Impact Statement (EIS) issued July 19, 2007 identified transportation improvements for redevelopment of the Bel-Red Corridor, which includes The Spring District property. The EIS acknowledges the concern expressed by residents for the potential for increased traffic from Bel-Red Corridor redevelopment intrusion into surrounding residential neighborhoods. The potential for neighborhood traffic intrusion was evaluated for each alternative (identified in the EIS) based on forecasts of 2030 PM peak-hour traffic volumes. These traffic volumes were used to estimate traffic volumes, including traffic volumes entering and leaving the Bel-Red Corridor (screenline traffic volumes). The 2030 PM peak-hour screenline traffic volumes for the action alternatives were then compared with the No-Action Alternative to identify the potential for neighborhood traffic intrusion based on the development level and proposed transportation system improvements included in each alternative. The EIS provides detailed analysis regarding planned projects, traffic volumes, and intersection traffic operations. This analysis includes an analysis of improvements to Bel-Red & 124<sup>th</sup> Avenue NE (3 alternatives), NE 12<sup>th</sup> Street and NE 16<sup>th</sup> Street. There is further discussion on the effect on adjacent neighborhoods suggesting that capacity would be constrained along many major roadways entering Wilburton, East Bellevue, and

Overlake neighborhoods.

As stated above, the traffic impacts of development of the Bel-Red Corridor, including The Spring District project, were identified and evaluated with the Bel-Red Corridor Project EIS.

Improvements to 120<sup>th</sup> Avenue NE are in the planning/design phase (contacts: Paul Krawczyk, phone 425-452-7905 and Steve Costa, phone 425-452-2845). There are a total of 6 phases of construction. Only one is actually funded at this time – Phase 1 - 120<sup>th</sup> Ave from NE 3<sup>rd</sup> Street to NE 8<sup>th</sup> Street. The other 5 phases are not funded and the schedule is subject to funding per City Council consideration and approval. The other projects are planned for construction based on funding and inclusion in future CIP plans.

The timing and completion of the 120<sup>th</sup> Avenue NE improvement project will be taken into consideration during The Spring District transportation analysis and transportation mitigation for each building or phase during the Design Review process. To comply with concurrency requirements, funding for appropriate transportation infrastructure, as identified in the Mobility and Infrastructure Initiative, must be committed prior to each phase of The Spring District buildout.

Bellevue truck routes are identified by ordinance in Bellevue City Code 11.76.010, and specific haul routes for development projects can be further defined through the conditions of the Right of Way Use Permit issued to the developer/contractor. This will be the case for the Spring District. 124<sup>th</sup> Avenue NE south of NE 8<sup>th</sup> Street is not an approved truck route. (Contact: Tim Stever, phone 425-452-4294.) If neighbors notice any violation, please call Tim Stever or call 911.

**Comment #13:** When the development is fully populated, some of the projected 37,000 daily trips generated by The Spring District will filter onto the neighborhood street. Since the road surface on 124<sup>th</sup> is deteriorating, does the City plan resurfacing? As traffic increases, will the City add a crosswalk on 124<sup>th</sup> at NE 5<sup>th</sup> for the schoolchildren at Three Cedars? Since 124<sup>th</sup> turns into Main Street on the way to 116<sup>th</sup> Avenue, the planned crosswalk for Bellevue Botanical Garden at approximately 12001 Main Street needs to be in place, as well.

Response:

Installation of crosswalks at Three Cedars School and the Botanical Gardens is a separate issue from The Spring District project. If you are interested in having crosswalks installed, please contact Karen Gonzalez, phone 425-452-4598.

Resurfacing for this portion of 124<sup>th</sup> Avenue NE is not in the current City Overlay Project. If overlay is needed, please call Tim Stever, phone 425-452-4294.

**Comment #14:** For a development of this size and density, does Bellevue plan to have on-site community policing in The Spring District neighborhood?

Response:

The City's capital planning process will determine the needs for new equipment and facilities as the Bel-Red Corridor is redeveloped. The Spring District's impacts on police

services will be evaluated during the individual phase/building permitting processes. At the Master Development Plan stage, there are no plans for an on-site police presence; however, the Bellevue Police Department operates in a community based policing model to provide exceptional continuity of police services. Patrol officers are assigned specific geographical areas to work for a full calendar year. These officers are also assigned to work the same work hours to enhance community relations. (Contact: Lt Tony Dempsey, phone 425-452-6986.)

**Comment #15:** Thank you for considering the impacts of the proposed Spring District development project on neighboring properties and the City's infrastructure. We trust the city will protect these properties and public assets from the negative impacts of this huge development.

While The Spring District offers the opportunity to accommodate a significant amount of Bellevue's future growth, as well as provide a catalyst for the planned redevelopment of the Bel-Red Corridor, this should not be allowed at the expense of surrounding properties or public assets. It is important to understand that The Spring District's preferential zoning, which give it greater building heights and floor area ratios than surrounding properties, effectively limit these other properties to operating in their current configuration until The Spring District project is largely built out. This will take decades. In other words, it is unreasonable to expect redevelopment of properties without the same preferential zoning The Spring District enjoys.

Therefore, we request the City meet its responsibility to ensure the project's proponents provide complete mitigation of all negative impacts from their proposed project.

The most significant impacts are to Bellevue's road network. According to reports from the City, a massive amount of new traffic will be generated by The Spring District project. The vast majority of these trips will not be accommodated by Sound Transit's proposed East Link light rail project. Therefore, The Spring District should be held fully responsible for increasing the capacity of the transportation system (e.g. roads) needed to serve their trip generation demand, including the construction and acquisition costs.

According to the Sound Transit East Link Final Environmental Study (July 2011), ridership at the 120<sup>th</sup> light rail station will be only 500 trips in 2020, and 1,000 trips in 2030, when the system is fully operational and utilized.

Chapter 3, page 31 states:

*"The 120<sup>th</sup> Station, which would be included in all alternatives except Alternative D5, would generate 500 daily boardings in year 2020. In 2030, daily boardings at the 120<sup>th</sup> station would range between 500 and 1,000."*

With a boarding being one person either embarking or disembarking East Link, the FEIS states light rail will only accommodate 500 round trips at the very maximum. This is a very small percentage of the trips The Spring District is forecast to generate. The vast majority of The Spring District's trip generation is expected to significantly increase the demand on Bellevue's road system that serves the proposed development site.

Therefore, full mitigation of these impacts must be made. Furthermore, the full cost to mitigate the impacts of The Spring District should be paid for by the developer creating the need for, and benefitting from, the improvements. These improvements would not be necessary but for the new development.

It is patently unfair for the City of Bellevue to allow a significant deterioration of the road system to be caused by the Spring District. Nor would it be fair for neighboring properties to pay for something that is primarily designed for The Spring District's benefit.

With The Spring District's preferential zoning, including building heights and densities that put nearby properties at a severe competitive disadvantage, the least Bellevue can do is attribute the benefits from capital improvement projects to the development that receives the massive increase in property value The Spring District would enjoy.

Response:

As detailed in the Bel-Red Corridor Project DEIS, FEIS, and addenda, the preferred alternative's land use changes and transportation improvement projects are intended to provide for redevelopment of the Bel-Red Corridor (not just The Spring District) while minimizing negative impacts. These decisions were made by the City Council when the Bel-red zoning and associated development regulations were adopted in 2009. Project level review evaluates the consistency of a project with adopted regulations.

Re-evaluation of previously made policy decisions is not appropriate or legal, and would also undermine the City goals of certainty and predictability in the permitting process.

Development allowed under The Spring District Master Development Plan will be required to undergo additional review for traffic concurrency, potential traffic mitigation, and impact fees, as well as other environmental review when individual phases or buildings are submitted for Design Review approval. See the Development Agreement between the city and applicant; BCC Chapters 14.10, 14.60, and 22.16.

**Comment #16:** Our property, Brierwood Center, is slated to lose nearly half the parking in our lower parking lot. The planned road expansion to serve The Spring District will also eliminate two access points. Another major impact will be the actual demolition of part of a building do to the reconfiguration of the Old Bel-Red Road east of 120<sup>th</sup>.

Existing businesses and homes will be harmed. Some will be physically displaced, while most will suffer loss of business and/or access. Business revenues and property values will be diminished. At least one long-term business will be forced to move, and many more might due to economic necessity. We anticipate a measurable increase in vacancies, and loss of rental income.

Both the construction and permanent negative impacts from these projects must be mitigated. It is the City of Bellevue's responsibility to ensure properties neighboring The Spring District project are protected from its impacts.

Response:

Physical impacts, if any, to private property from public road projects identified to support redevelopment when the Bel-Red corridor was rezoned, will be addressed in the

property acquisition phase of the city road project. For the 120<sup>th</sup> Avenue NE, the city contacts are Paul Krawczyk, phone 425-452-7905 and Steve Costa, phone 425-452-2845.

As noted in #15 above, it is not appropriate to re-visit previously made policy decisions at the project level review stage. Development allowed under The Spring District Master Development Plan will be required to undergo additional project specific review for traffic concurrency, potential traffic mitigation, and impact fees, as well as other environmental review when individual phases or buildings are submitted for Design Review approval.

**Comment #17:** Overall, mitigation measures should ensure that The Spring District's growth pays for itself. Specific mitigation steps should include:

- Cost of the expansion of 120<sup>th</sup> Avenue NE, north of NE 8<sup>th</sup> Street. Including compensation for taking of adjacent properties and loss of operating income.
- Cost of maintaining the current operating capacity in the transportation system (i.e. preventing the current delays at intersections from increasing).
- Cost of signaling the intersections servicing The Spring District, including 120<sup>th</sup> and the Old Bel-Red Road, and the Old Bel-Red road and 124<sup>th</sup> Avenue NE.

Response:

Physical impacts, if any, to private property from public road projects identified to support redevelopment when the Bel-Red corridor was rezoned, will be addressed in the property acquisition phase of the city road project. For the 120<sup>th</sup> Avenue NE, the city contacts are Paul Krawczyk, phone 425-452-7905 and Steve Costa, phone 425-452-2845.

As noted in #15 above, it is not appropriate to re-visit previously made policy decisions at the project level review stage. Development allowed under The Spring District Master Development Plan will be required to undergo review for traffic concurrency, potential traffic mitigation, and payment of transportation impact fees. This review determines potential project-related impacts to the road system and required mitigation as conducted by the City of Bellevue Transportation Department, under Bellevue City Code 14.10, 14.60, 22.16.

**Comment #18:** Due to the non-competitive zoning designation the City gave Brierwood Center during the Bel-Red Corridor Study, our property must remain in its current physical configuration for the foreseeable future. Since The Spring District is allowed to build at twice the height, and nearly twice the density, it will capture the next 15-20 years of market demand for new development in the area. Therefore, any changes City projects will have on Brierwood Center's value must be based on the Property's net operating income as it is currently built.

We believe it will be difficult for the city to just compensate us for the eminent domain condemnation planned. However, we do expect the City to meet this responsibility.

Response:

The City works with property owners whose property will be acquired for any public project and compensation is determined pursuant to applicable code and law. For the 120<sup>th</sup> Avenue NE project, the city contacts are Paul Krawczyk, phone 425-452-7905 and

Steve Costa, phone 425-452-2845.

**Comment #19:** If, at some point in the future Brierwood Center is redeveloped, we suggest the City consider some form of “latecomer” agreement. It would be fair and reasonable for properties that redevelop, and directly benefit from new road capacity paid for by The Spring District, to contribute to the project cost at that time. In no case is it fair for properties that don’t redevelop to be assessed a “special benefit” charge if no true special benefit occurs.

Response:

Latecomer agreements, local improvement districts, and other financing mechanisms that may be used to pay for transportation projects and other improvements will be implemented and managed according to the applicable legal requirements. For any financing mechanism, including LIDs, that require a special benefit analysis, the City will take into account all appropriate legal and economic factors in determining special benefit and applicable assessments. For the 120<sup>th</sup> Avenue NE, the city contacts are Paul Krawczyk, phone 425-452-7905 and Steve Costa, phone 425-452-2845.

**Comment #20:** What is/are “concurrency analyses,” and what is the basis for deciding that these analyses are not required to approve Wright Runstad’s master development plan?

Response:

The 2009 Development Agreement between the city and the applicant allows for traffic concurrency analysis under BCC Chapter 14.10 to be conducted at the time a phase or building (allowed under the Master Development Plan) undergoes Design Review or any other Process II permit review. BCC 14.10.020 provides the authority allowing phased development, e.g. The Spring District, is to undergo concurrency analysis for each individual phase rather than during review of the Master Development Plan.

Concurrency analysis is a review to determine if the proposed development would cause the level of service of a transportation facility to fall below the city’s adopted standards. BCC 14.10.010. If so, mitigation will be required.

**Comment #21:** I am encouraged by your commitment to a walkable neighborhood and LEEDS for the buildings and neighborhood development. As you know, most of our neighborhood’s concerns focus on weak links in the transportation network.

Response:

The proposal includes pedestrian-friendly features, such as pedestrian connections to adjacent streets, sidewalks along all city streets within the project limits, and a mid-block pedestrian connection from 123<sup>rd</sup> Avenue NE to the multi-purpose trail along 124<sup>th</sup> Avenue NE. The gateway corner at NE 12<sup>th</sup> Street and 120<sup>th</sup> Avenue NE will provide further pedestrian connection and encourage adjacent neighbors to visit the parks, restaurants and other amenities within The Spring District. Refer to Conditions of Approval regarding Pedestrian Connections in Section X.A.

**Comment #22:** Surely 124<sup>th</sup> Avenue NE has a different designation *south of* NE 8<sup>th</sup> from than north of it. The traffic generated from The Spring District’s development is utterly

incompatible with the residential character of the neighborhood. And certainly its construction traffic should NOT use 124<sup>th</sup> as a route: those trucks will be too heavy and the noise and disruption of a neighborhood street is too intense.

Response:

As noted in comment #15 above, it is not appropriate to re-visit previously made policy decisions at the project level review stage. Development allowed under The Spring District Master Development Plan will be required to undergo review for traffic concurrency, potential traffic mitigation, and payment of transportation impact fees. This review determines potential impacts to the road system and required mitigation.

As noted in comment #12 above, Bellevue truck routes are identified by ordinance in Bellevue City Code 11.76.010. 124<sup>th</sup> Avenue NE *south* of NE 8<sup>th</sup> Street is not an approved truck route. If neighbors notice any violation, please call Tim Stever at 425-452-4294 or call 911.

**Comment #23:** My expectation as a citizen and resident is that the City of Bellevue will protect its existing residential neighborhoods, essentially holding them harmless, at the same time that it encourages creation of new ones. Given the density planned north of the Bel-Red, doing that will require substantial efforts at mitigation of traffic. I will be most interested to learn how they intend to do that. Please keep me up to date.

Response:

As noted in comment #15 above, development allowed under The Spring District Master Development Plan will be required to undergo review for traffic concurrency, potential traffic mitigation, and payment of transportation impact fees. This review determines potential impacts to the road system and required mitigation.

**Comment #24:** What is the current status for Mobility and Infrastructure Initiative (MII) project priorities and funding? On the project homepage, we see that "Funding limitations delay projects." Is there an update to this information? Will the transportation network be in place to support development in The Spring District? Any information you can share is greatly appreciated.

Response:

As noted in comment #8 above, the group of MII projects includes the widening of 120<sup>th</sup> Avenue NE (from NE 4<sup>th</sup> Street to Northup Way), the construction of a new surface street connection between 116<sup>th</sup> Ave NE and 120<sup>th</sup> Ave NE (NE 4<sup>th</sup> Street extension), construction of NE 15<sup>th</sup>/16<sup>th</sup> Streets (from 116<sup>th</sup> Avenue NE to 124<sup>th</sup> Avenue NE), and the expansion of 124<sup>th</sup> Ave NE north of Bel Red Road to Northup Way. Construction of these projects will be phased; construction funding is still being finalized. For more information about status, please contact Paul Krawczyk, phone 425-452-7905, for 120<sup>th</sup> Avenue NE (south of NE 12 Street); Steve Costa, phone 425-452-2845 for 120<sup>th</sup> Avenue NE (north of NE 12 Street); and Nancy LaCombe, phone 425-452-4382 for 124<sup>th</sup> Avenue NE.

Accordingly, all development allowed under the Master Development Plan for The Spring District will be subject to traffic concurrency analysis, transportation mitigation, and additional environmental review at the time of building or phase-specific permitting. The timing and completion of the Mobility and Infrastructure Initiative projects will be

taken into consideration during the transportation analysis and transportation mitigation for each building or phase during the Design Review process. To comply with concurrency requirements, funding for appropriate transportation infrastructure will be committed with each building/phase of the Spring District buildout.

**Comment #25:** The Spring District MDP refers to 124<sup>th</sup> Avenue as a major arterial (MDP environmental checklist 7b (1) on page 8 under noise). Has the designation changed from secondary arterial?

Response:

124<sup>th</sup> is classified as a Minor Arterial. The SEPA checklist has been annotated to correct this classification. See Attachment D.

**Comment #26:** Are there plans to restore the sign limiting gross vehicle weight for traffic entering 124<sup>th</sup> Avenue *south* of NE 8<sup>th</sup> Street?

Response:

According to the Transportation Department, this sign currently exists with a gross truck weight limit of 32,000 lbs. There are no plans to remove it. It appears that neighbors are concerned about large trucks using 124<sup>th</sup> Avenue NE as a haul route and the negative impact from the trucks (noise, dust, traffic congestion, safety). See comment #12 above which states that truck haul routes are defined through the Right of Way Use Permit. According to the Transportation Department, for The Spring District, 124<sup>th</sup> Avenue NE *south* of NE 8<sup>th</sup> Street is not an approved truck route. See comment #22 for phone numbers for any violations.

**Comment #27:** The MDP indicated 1,630,000 cubic yards of earth movement during the development cycle. Is the Transportation Department coordinating with the developer to use some fill dirt on the MMI projects in the vicinity? It would be quite an impact to our neighborhood if the removal route involves 124<sup>th</sup> Avenue *south* of NE 8<sup>th</sup>.

Response:

The SEPA environmental checklist indicates that there will be approximately 1.5 million cubic yards of earthwork. Of this approximate 1.5 million cubic yards of earthwork, the applicant anticipates that 130,000 cubic yards will be used for on-site fill. (Note: The numbers should not be added together.) This earthwork will occur over the 7 phases and next 15 years. So, the impact of hauling dirt on an individual building/phase basis would be significantly less than all at one time.

The applicant intends to use the excess earth on-site wherever possible. Any excess will be recycled and could be used for other projects (city or private). The Spring District contractor is responsible for legal disposal of the dirt. The contractor could contact the Transportation Department separately about the city using excess dirt for city projects. The city does not have code authority to require this as a condition of approval for this Master Development Plan.

As noted in comment #22 above, Bellevue truck routes are identified by ordinance in

Bellevue City Code 11.76.010. 124<sup>th</sup> Avenue NE *south* of NE 8<sup>th</sup> Street is not an approved truck route. If neighbors notice any violation, please call Tim Stever at 425-452-4294 or call 911.

**Comment #28:** What changes do you foresee on 124<sup>th</sup> Avenue NE *south* of NE 8<sup>th</sup> during and after construction of The Spring District in terms of traffic volume and the integrity of the road surface?

Response:

It is anticipated The Spring District and Bel-Red Corridor generally will not be complete until nearly 2030, as discussed in the Bel-Red Corridor DEIS, FEIS and addenda. Impacts on 124<sup>th</sup> Avenue NE *south* of NE 8<sup>th</sup> Street and the integrity of the road surface will be issues the city continues to revisit through its transportation and capital planning processes.

## V. TECHNICAL REVIEW

### A. Clearing & Grading

The Clearing & Grading Division will conduct a detailed review under future clearing & grading permit applications. Refer to Conditions of Approval regarding Clearing and Grading Permit in Section X.C.

### B. Utilities

Utility review of the Spring District Master Plan was completed on a conceptual basis only. Specific building site utility engineering review and approval will be completed under a Developer Extension Agreements (UE) permit. Refer to Conditions of Approval regarding Preliminary Design, Utility Code and Engineering Standards in Section X.A.

Water, sewer and storm water infrastructure can be extended onto and through the proposed master plan development.

#### 1. Water

Water mains have been conceptually sized based on projected maximum build out for multifamily, office, hotel and retail buildings proposed for the site. Adequate capacity in City of Bellevue water mains in 120<sup>th</sup> Ave NE and 124<sup>th</sup> Ave NE is available to serve the development. Final sizing of the mains will occur during engineering review of construction drawing submitted under a Developer Extension Agreement.

#### 2. Sewer

Sewer is available at the south end of the project through an existing 15" sewer main and to the north through a 12" sewer main. Capacity in the 15" main is limited to 848 GPM and only a small portion of the north site will drain to the 12" sewer main. Additional sewer capacity will be needed. Several options for additional sewer capacity are available to the development and are all feasible options to serve the site.

Option 1: Upgrade the existing south 600 feet of 15" vitrified clay sewer main to a larger diameter pipe to meeting the sewer demand for The Spring District development.

Option 2: Extend sewer main in 120<sup>th</sup> Ave NE from the existing downstream sewer. The main extension is approximately 700 feet.

Option 3: Connect to a sewer main extension in 120<sup>th</sup> Ave NE to be constructed by the City of Bellevue Transportation 120<sup>th</sup> St. CIP. The applicant will be required to pay a connection charge based on the amount of sewer main physically constructed.

### **3. Storm Water**

The development proposes to meet all the minimum requirements triggered for the site. Based on information submitted with this permit the site will trigger storm drainage minimum requirements 1-9 of the Storm and Surface Water Engineering Standards. Appendix VI Page 7 of the Master Development Plan comment responses addresses the storm water impacts to the site and how onsite storm water, water quality and flow control will be addressed for the site. After a review of the materials submitted it is feasible for the development to mitigate storm water impacts to the site. There will be an overall decrease in impervious area for the site and enough room exists on site to implement onsite and water quality Best Management Practices. Flow control will not be required because the site will not create new impervious surface that will trigger the requirement.

## **C. Transportation**

### **1. Use of the Right of Way**

Applicants often request use of the right of way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right of Way Use Permit for such activities must be acquired prior to issuance of any construction permit including demolition permit.

### **2. Traffic Impact Fee**

Traffic impact fees are used by the city to fund street improvement projects to alleviate traffic congestion caused by the cumulative impacts of the development throughout the City. Payment of the transportation impact fee, as required by BBC 22.16, contributes to the financing of transportation improvement projects in the current adopted Transportation Facilities Plan, and is considered to be adequate mitigation of long-term traffic impacts. Fee payment is required at the time of building permit issuance.

### **3. Easements**

The applicant shall provide all necessary easements to the City for location of signal and street light facilities consisting of above-grade boxes and/or below-grade vaults between the building and sidewalk within the landscape area as necessary along all street frontages. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible. The applicant shall also grant any necessary construction, pedestrian, landscaping and utilities easements within and along the entire master development site to the City for location of future sidewalks and utilities outside of the City right-of-way.

### **4. Street Frontage Improvements**

In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, the construction of street frontage improvements is required as a condition of development approval with each new building or phase. The design of the improvements must conform with the requirements of the Americans with Disabilities Act and the Transportation Development Code (BCC 14.60); The Bel-Red Corridor Plan Streetscape Character, Guidelines, and Standards; and the provisions of the Transportation Department Design Manual.

### **5. Local Improvement District (LID)**

The applicant shall not protest the city's formation of an LID or Transportation Improvement District when it is one of the multiple funding alternatives having the purposes of constructing arterial street improvements on 120<sup>th</sup> Avenue NE north of NE 8<sup>th</sup> Street and south of Northup Way, 124<sup>th</sup> Avenue NE north of NE 12<sup>th</sup> Street and south of Northup Way, NE 15<sup>th</sup> Street or other internal public street segments including pavement, widening, curbs, gutters, landscaping and medians, on-street public parking, landscape strips, sidewalks, urban design amenities, gateway amenities, bike lanes, multi-purpose pathways, street lighting, underground utilities, traffic signal system elements, storm drainage flow control and water quality treatment and associated appurtenances. The property owner does not waive the right to protest the method of assessment or the amount of assessment or identified special benefits attached to the property. This no protest provision shall be recorded and attach to the properties or any future properties through segregation or aggregation of the property.

### **6. Right of Way Dedication**

To incorporate street improvements which are reasonably necessary to mitigate the direct results of the development, the developer is required to dedicate property such that street surface including on street parking to back of curb is accommodated within the public right of way.

## 7. Site Access

Access to the proposed project will be provided via a number of new signalized and un-signalized public streets and/or private roads and driveways as follows:

### 120<sup>th</sup> Avenue NE:

First, a signalized driveway access based on the following factors:

- Adequate southbound left turn storage at NE 12<sup>th</sup> Street,
- Aligned with the access for land uses to the west side of 120<sup>th</sup> NE,
- To minimize grades for vehicles entering the garage, and
- Consistent with the city's traffic signal design policies and practices.

Second, a new driveway between the new signalized driveway and NE 15<sup>th</sup> Street.

### NE 15<sup>th</sup> Street (new):

- A new public road (121<sup>st</sup> Avenue NE) with an "All-Way Pedestrian Crossing" signal,
- A new signalized public road (123<sup>rd</sup> Avenue NE) with left-turn vehicular movement restrictions and pedestrian crosswalks,
- A private road between 121<sup>st</sup> Avenue NE and 123<sup>rd</sup> Avenue NE, called 122<sup>nd</sup> Place NE, or as otherwise designated by the city.

### 124<sup>th</sup> Avenue NE:

- A new public road (NE 13<sup>th</sup> Street) with traffic signal; the west leg will be a minimum of 48 feet from curb to curb to allow for one inbound lane and three outbound lanes (left, shared left/right, right) if warranted by traffic volumes generated during later phases of development. During the initial phases, the eastbound lanes will be channelized with separate left and right turn lanes and an eleven foot wide planted median provided; and a new driveway between NE 13<sup>th</sup> Street and NE 15<sup>th</sup> Street.

### NE 12<sup>th</sup> Street:

- A single garage driveway to be at least 30-foot wide.

All non-signalized driveways and private roads accessing the above streets (120<sup>th</sup> Avenue NE, NE 15<sup>th</sup> Street, 124<sup>th</sup> Avenue NE, and NE 12<sup>th</sup> Street) will be restricted to right in/right out. No left turns will be allowed at the signalized intersection of NE 15<sup>th</sup> Street/ 123<sup>rd</sup> Avenue NE.

## 8. Pavement Restoration

The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration Program, every street in the City of Bellevue has been examined and placed in

one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are "No Street Cuts Permitted," "Overlay Required," and "Standard Trench Restoration." Each category has different trench restoration requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that often comes with small strip patching.

Pavement restoration will be evaluated at the DR application for each new building or phase.

## **9. Transportation Management Program**

In order to reduce single occupant vehicle trips and provide enhanced options to employees and infrastructure users, the City has adopted code provisions for a transportation management program. The owner of this development shall, prior to any initial occupancy of the building structure, sign and record an agreement approved by the City of Bellevue to establish a transportation management program to the extent required by BCC14.60.070. Future additions to the master development site will be subject to code requirements in effect at the time of Design Review applications.

Refer to Conditions of Approval regarding Transportation Department in Sections X.A, X.C, X.D, X.E.

### **D. Building**

Building code issues will be covered during the Building Permit application review.

### **E. Fire**

The Fire Department has reviewed this proposal and recommends approval subject to conditions including fire hydrants, fire apparatus access and adequate fire flow. Refer to Conditions of Approval regarding Fire Department in Section X.D.

### **F. Parks & Community Services**

The Parks & Community Services Department reviewed the proposal with regard to the Development Agreement. Future design and development of the one acre public park (M-3), per the Development Agreement, must be approved by the Parks & Community Services Department and be consistent with BCC 3.43.

## VI. STATE ENVIRONMENTAL POLICY ACT (SEPA)

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the Master Development Plan approval that are beyond those identified in the Bel-Red Corridor EIS. The Environmental Checklist together with information submitted (in the official file) adequately discloses expected environmental impacts associated with the Master Development Plan approval. Additional project level analysis will be conducted as a part of the Design Review process for each new building or phase. The City codes and requirements, including SEPA, Land Use Code, Noise Ordinance, Building Code and other construction codes adequately mitigate expected environmental impacts. Therefore, issuance of a Determination of Non-Significance (DNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements, with incorporation by reference of:

- The Bel-Red Corridor Project Draft and Final Environmental Impact Statement, issued July 19, 2007 and addendum.

This document is available in the Development Services Department, Records Room, Lobby Floor, Bellevue City Hall, 450 110<sup>th</sup> Avenue NE under file #05-127994-LE.

Adverse impacts which are less than significant are typically subject to City Codes or Standards which are intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code, Section 22.02.140, provides substantive authority to mitigate impacts disclosed through the environmental review process.

The following amendments to the Environmental Checklist are provided by the City of Bellevue.

### **Transportation**

The proposed Master Development Plan (The Spring District) includes demolition of six buildings and construction of 29 buildings on a 36-acre site. This mixed used development will contain office buildings, neighborhood retail, multi-family dwellings, a hotel, parks and new roads/ infrastructure facilities. The master development plan is anticipated to be implemented over 15 years in seven different phases.

The long-term impacts of development projected to occur in the City by 2020 have been addressed in the City's Final Environmental Impact Statement (TFP EIS) 2009-2020 Transportation Facilities Plan published March 5, 2009, and through the future payment of traffic impact fees. The impacts of growth which are projected to occur within the City by 2020 are evaluated on the roadway network assuming that all the transportation improvement projects proposed in the City's current Transportation Facilities Plan are in place. The Transportation Facilities Plan EIS divides the City into several Mobility Management Areas (MMAs) for analysis purposes. The Spring District lies within MMA 12, which has a 2020 total growth projection of 3,324,252 Gross Square Feet (GSF) of office; 521,400 GSF of retail; 3,300 multi-family units; and a 400-room hotel. The master plan development proposes 3,712,000 GSF of office; 163,000 of retail; 1,220 multi-family dwelling units; and 248-room hotel. Therefore, the volume of proposed development is within the assumptions of the Transportation Facilities Plan EIS. In addition, the City's Final Environmental Impact Statement (The Bel-Red Corridor Project FEIS) 2007-2030, issued July 19, 2007, has also evaluated the long-term 2030 impacts within MMA 12.

City staff directed the applicant's traffic consultant, TSI, Inc., to analyze operational impacts of the proposal at full build out in order to recommend mitigation if necessary. This analysis included 2030 with-project conditions during p.m. peak hours at the proposal access points and intersections adjacent to the entire site. In addition, the evaluation included intersection LOS, roadway channelization assumptions, and intersection controls as well as consistency with the transportation assumptions and findings of the Bel-Red Corridor Project FEIS and current transportation planning efforts. Particular issues were: 1) the effect of converting the site's access on NE 12<sup>th</sup> Street from a local street (as assumed in the Bel-Red Corridor Project FEIS) to a driveway serving a residential garage, 2) evaluating turning restrictions on NE 15<sup>th</sup> Street and 3) overall traffic operations.

The TSI, Inc. report provided the analysis that illustrates the effects of two trip generation scenarios that incorporate the City of Bellevue Downtown trip generation rates as well as rates generated by a Transit Oriented Development (TOD) such as The Spring District. The findings for these scenarios focused on problematic areas and identified the transportation infrastructure that would be needed to support the build-out of The Spring District while providing both the City and the applicant with flexibility to effectively address evolving transportation conditions as the site is developed in the next 15 years. The TSI, Inc. analysis dated April 2, 2012, is included in the city file for this development. Key conclusions of the Traffic Impact Analysis include the following:

1. No adverse operational impact due to the proposed Master Development Plan is anticipated.
2. Trip generation characteristics for the project may be lower than projected due to the proximity of the light rail station and "Transit Oriented Development" mixed-use nature of the development. This provides a conservative analysis for this review.
3. Projected vehicle volume forecasts on NE 15th Street necessitate back to back left turn lanes between 120th Ave NE and 124th Ave NE. This would preclude all left turn movements at site access points on NE 15th Street. However, a break in the landscaped median/ left turn pocket at NE 123rd would allow for northbound/southbound through and right turn movements as well as a protected pedestrian crossing.

**Traffic Standards Code**

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more p.m. peak hour trips must undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained. For the purpose of the concurrency analysis, the City has been divided into 14 MMAs.

However, the code allows a Development Agreement to adjust the timing of traffic

concurrency analysis and the expiration date of concurrency approval. The City and the applicant agreed to amend analysis time and concurrency expiration date through a Development Agreement recorded September 11, 2009. Therefore, a concurrency analysis and determination will not be required at the time of the Master Development Plan application.

A new concurrency analysis must be performed for each building application as the site develops.

### **Earth**

The affected geographic area is generally flat, with the exception of a man-made steep slope located at the western edge of the site along 120th Avenue NE. There are some slopes over 40% along the west perimeter adjacent to 120<sup>th</sup> Avenue NE.

Fill consisting of moist to wet, medium dense, sand to silty sand, and medium stiff to hard sandy silt was encountered beneath the pavement in depth ranging between 3 to 21 feet. Soils beneath the fill are classified as lacustrine deposits (3 to 10 ft in depth) underlain by glacial till consisting of dense to very dense silty sand (see geotechnical report by Kleinfelder, Inc. dated November 11, 2005 and 2012 updated letter). According to the geotechnical engineer of record, the slope is stable and capable of the proposed development.

Proposed earthwork includes the excavation of approximately 1.5 million cubic yards of material for construction of underground parking garages. Of this material approximately 130,000 cubic yards will be used for fill within the site, if suitable. Fill trucked in from off-site will come from approved suppliers.

There is potential for erosion and sedimentation impacts as a result of the earth movement activities. Applicants and permittees are responsible for preventing erosion and discharge of sediment and other pollutants into the storm and surface water system and receiving waters. A construction stormwater pollution prevention plan (CSWPPP) prepared according to the Washington State Department of Ecology Stormwater Management minimum technical requirement # 2, will be required at time of Clearing and Grading permit application for each of the development phases. (Bellevue City Code 23.76)

By incorporating during the Design Review process for each building or phase, the requirements of existing codes and standards, the recommendations included in the geotechnical report (including site management techniques), use of Best Management Practices for temporary erosion and sedimentation control, and rainy season restrictions, it is expected that anticipated impacts to earth resources will be adequately mitigated.

### **Water**

The existing 36 acres is a light industrial site with approximately 89% existing impervious surface. At full build-out, The Spring District development's overall impervious surface will be reduced to 75% (maximum allowed under the Land Use Code). This results in a reduction of impervious surface of approximately 5 acres.

The Spring District development area is approximately 400 feet northeast of Lake Bellevue. There is one Category III wetland located directly to the southwest of the property along 120th Avenue NE. (This is in an area of work for the 120<sup>th</sup> Avenue NE Phases 2 and 3 road widening project.) The wetland flows to Lake Bellevue, which flows to Sturtevant Creek to the southwest. Kelsey Creek is located approximately 300 feet northeast of the site. The development will not require any work over, in, or adjacent to any body of water.

Stormwater from rooftops and roadways will be collected, treated, and conveyed through approved systems that eventually discharge to Lake Bellevue. To reduce the amount of stormwater runoff, natural drainage practices will be implemented, including rain gardens and pervious concrete where appropriate. Internal, private roadways are narrower than standard street sections, reducing the use of asphalt pavement and therefore reducing runoff. During construction, contractors will be required to have a Spill Prevention Control and Countermeasure plans and a Stormwater Pollution Prevention Plan (SWPPP) in place. Stormwater systems will be designed and operated in accordance with relevant standards and requirements and will be treated prior to discharge into an approved system.

The full build-out footprint of The Spring District is within an area currently used for light industry. This area is already paved for automobile storage and dry/cold storage. The proposed development is expected to decrease overall adverse impacts to the quality of surface water leaving the site. See Appendix VI of the Utilities Design Intent submitted with the application materials dated February 10, 2012 by Justin Jones P.E. of Parametrix Inc. The City's Utility Codes and Engineering Standards provide adequate direction to mitigate for both runoff control and water quality treatment for conventional pollutants for any impacts related to the proposal.

The site is located in the Sturtevant Drainage Basin. All site work during the rainy season (November 1<sup>st</sup> through April 31<sup>st</sup>) is required to be approved by the Clearing & Grading Section based on a specific request to the Clearing & Grading reviewer. Any approval to perform clearing & grading activities during the rainy season will be subject to conditions requiring site erosion and sedimentation to be minimized. An augmented temporary erosion and sedimentation control plan may also be required at the time of Clearing & Grading permit review. (Bellevue City Code 23.76)

### **Air Quality**

During construction, emissions to the air will be released by construction vehicles and heavy equipment. Following construction, emissions from residents' and office workers vehicles will be released.

Construction would temporarily increase dust and vehicle emissions near the construction area. Mitigation will include using BMPs to control dust, covering exposed soils, and requiring idling vehicles to be shut off. Construction vehicles will be fitted with required, factory-installed emission control devices. To reduce the potential of dust, construction accesses will be covered with rock or aggregate. Dust emissions will also be reduced during construction through the use of spray water as necessary during dry weather conditions and planting disturbed areas with erosion control seed mix as soon as is practical. Material stockpiles will also be covered or watered as necessary to

control dust. These are standard practices imposed on the Clearing & Grading permit. (Bellevue City Code 23.76)

The Bel-Red Corridor DEIS states that to the extent that pollutant-generating land uses (i.e. repair shops, drycleaners) redevelop over time within the Bel-Red Corridor, overall emissions would be lower in the future than they are now. And, that despite the predicted increase in traffic volumes and emissions, the Bel-Red Corridor redevelopment is not likely to result in any exceedance of the air quality standards. Maintaining traffic flow will reduce vehicles idling and, therefore, reduce pollutant emissions from vehicles. With the future LTR trips, pollutant emissions from vehicles are assumed to be further reduced.

The applicant provided a technical memorandum from Parametrix Inc. dated February 10, 2012 and updated March 28, 2012 (see Attachment E), which describes the anticipated greenhouse gas emissions associated with the construction and operation of The Spring District project.

The Spring District development includes 29 buildings, parks, and roadways within a 36.01 acre site. The project will be constructed in phases over the next 15 years. Since the SEPA review considers potential environmental impacts project-wide, the report also calculates the Greenhouse Gas (GHG) emissions on a project-wide basis. However, it should be noted that individually the proposed buildings will be below the 25,000 metric tons of carbon dioxide equivalents (MTCO<sub>2e</sub>) per year threshold set by the State Department of Ecology. The City of Bellevue does not have code authority to regulate air quality. Air quality is under the jurisdiction of the State Department of Ecology.

Mitigation measures for potential impacts to air quality due to the proposed for the development include:

1. Transit-Oriented Development: These mitigation measures include transit-oriented development (light rail station, walkable community, multi-use path with regional connection, and mixed-use development) and low impact development techniques (i.e. rain gardens, bioretention, porous concrete, and LEED® certified buildings). These techniques can reduce the emissions by at least 11% compared to the no-mitigation option, according to the technical memorandum by Parametrix, Inc.
2. Building Lifespan: These mitigation measures include applicant commitment to ecological construction, green building management and a practice of energy conservation. Other mitigation measures include adaptive building reuse, sustainably grown and regionally produced products (i.e. extracted, harvested, or recovered within 500 miles of the project) and high-performance systems (i.e. ventilation, HVAC systems, water conservation strategies). Additional mitigation measures include applicant commitment to waste reduction, as well as, reuse and recycling in all buildings. By selecting durable and less energy consuming building components (i.e. durable shell, permanent interior systems), the applicant has a proven history of sustainable 100-year buildings. With the mitigation measures related to building lifespan, emissions can be reduced by another 31% compared to the no-mitigation option.

This is a total of 42% potential reduction (11% and 31%) in Greenhouse Gas Emissions compared to the no-mitigation option.

### **Plants and Animals**

The existing 36 acres is a light industrial site with approximately 89% existing impervious surface. Upon completion of the MDP, the overall impervious surface will be reduced to 75%, as required by the Land Use Code. This results in a reduction of impervious surface of approximately 5 acres. With the installation of future public parks and other site landscaping (i.e. plaza, perimeter/interior, streetscape), there will be the opportunity for new plants (native/ornamental), animals (typical urban animals such as squirrels, moles), and native birds to occupy The Spring District.

### **Noise**

As conditioned, short term impacts related to noise generation as a result of the construction will be minimized. Noise related to construction is allowed from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday, except for Federal holidays and as further defined by the Bellevue City Code. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020C.1 when necessary to accommodate construction which cannot be undertaken during exempt hours. Written requests for exemption from the Noise Control Code must be submitted two weeks prior to the scheduled onset of extended hour construction activity. The use of best available noise abatement technology consistent with feasibility is required during construction to mitigate construction noise impacts to surrounding uses. These standard requirements are imposed with the construction permits. (Bellevue City Code 9.18)

### **Light and Glare**

There is the potential for light and glare with the lights at night from any above ground parking garages, office buildings and residential units. Since this site is approximately 400 feet from the closest multi-family units (and approximately 1500 feet from the closest single family units), the impact will be on adjacent businesses that operate after dark and the vehicular/pedestrian traffic along adjacent streets. In order to mitigate potential impacts to adjacent businesses/residents, any light source emitting from the project area shall be designed so as not to provide light and glare and spillover offsite. This is a standard requirement that will be imposed with individual Design Review applications. (Land Use Code 20.20.522)

## **VII. CHANGES TO PROPOSAL DUE TO CITY REVIEW**

### **A. Transportation Department**

As a result of review from the Transportation Department, the applicant will at the minimum provide the following:

- Traffic mitigation, including new traffic signals at 124<sup>th</sup> Avenue NE & NE 13<sup>th</sup> Street, 120<sup>th</sup> Avenue NE & the first site access north of the NE 12<sup>th</sup> Street intersection, all-way pedestrian signals at NE 15<sup>th</sup> Street & 121<sup>st</sup> Avenue NE, and NE 15<sup>th</sup> Street & 123<sup>rd</sup> Avenue NE;
- Identification of a pedestrian only crossing at NE 15<sup>th</sup> Street & 121<sup>st</sup> Avenue NE

- (to the future light rail station);
- Identification of restricted vehicular lanes onto city streets; and

## **B. Land Use Division**

The Land Use Division reviewed the proposal and requested the following:

- Public plaza at the East Link light rail station;
- Art Concept per Bel-Red Corridor Plan Figure 3.1;
- Outward focus of buildings to relate to adjacent perimeter public streets and future pedestrian activity on 124<sup>th</sup> Avenue NE, NE 12<sup>th</sup> Street and 120<sup>th</sup> Avenue NE;
- Reduction of the maximum impervious surface/lot coverage from 89% (existing) to 75% (per Code). This results in a difference of approximately 5 acres (converted to pervious surface);
- Multi-purpose trail along 124<sup>th</sup> Avenue NE;
- Mid-block pedestrian connection from 123<sup>rd</sup> Avenue NE to 124<sup>th</sup> Avenue NE; and
- More detail about the pedestrian gateway connection from the site to NE 12<sup>th</sup> Street and 120<sup>th</sup> Avenue NE.

## **VIII. DECISION CRITERIA**

### **MASTER DEVELOPMENT PLAN**

The Director may approve or approve with modifications an application for a Master Development Plan if (LUC 20.30V.150):

- A. The proposed Master Development Plan is consistent with the Comprehensive Plan.

Finding: The Spring District Master Development Plan is consistent with the Comprehensive Plan, including the following goal/policies from the Bel-Red Subarea Plan.

1) **Vision:**

- The Bel-Red corridor in 2030 will be an area that is unique within the city of Bellevue and the entire Puget Sound region. It will be an area where thriving businesses will be adjacent to, and sometimes mixed with, livable neighborhoods, all served by a multi-modal transportation system that connects the area to the greater city and region. The area will also be distinguished by environmental and community amenities that will serve residents and employees in the area, as well as residents from surrounding neighborhoods and the entire city. The area will transition gracefully over time, with existing businesses being accommodated while new types of development will occur as conditions warrant.

- Vibrant, diverse and walkable neighborhoods: Bel-Red will contain a broad range of housing types to meet the needs of a diverse population of varied income levels. Neighborhoods will have a pedestrian friendly and walkable character, with convenient access to shopping, jobs, and community amenities, and will also be well connected to the larger city and region.

Finding:

The Spring District's Master Development goals are consistent with the vision of the Bel-Red Subarea Plan. As stated in Section I.A, the applicant has 6 goals for The Spring District:

- (a) to develop a new urban community;
- (b) to establish neighborhood identity;
- (c) to create a catalyst project;
- (d) to be complementary to downtown Bellevue;
- (e) to create a new business environment; and
- (f) to provide vibrant open spaces.

Since The Spring District is a phased project over 15 years, it will have time to transition gracefully. At full build-out, The Spring District will be an urban village, served by light transit, with a public plaza at the entrance to the light rail station, public parks and a vibrant residential neighborhood, close to centers of employment (i.e. office buildings within the project scope).

- 2) **Land Use Goal:** To develop a land use pattern that is environmentally sustainable and economically vibrant, and that creates distinctive new commercial and residential neighborhoods for the Eastside.

**POLICY S-BR-6.** Concentrate the majority of future Bel-Red growth into a series of mixed use, pedestrian-friendly and transit-oriented development nodes, with higher density and height therein, as enabled through a land use incentive system. Within each node, provide for tiered building heights, with maximums at the center.

**POLICY S-BR-8.** Encourage mixed use development, promoting opportunities to live, work, shop, and recreate within close proximity.

Finding:

The Spring District meets the Land Use goals and policies of the Bel-Red Subarea plan noted above. The project is located within a designated transit-oriented development node. Within 36 acres, The Spring District will create a distinctive urban village with commercial/residential neighborhoods and public parks. The applicant intends to build LEED/LEED-ND. Although not a requirement by the Land Use Code, LEED certification is an amenity within the FAR amenity system.

- 3) **Urban Design Goal:** To achieve a design character that results in aesthetically beautiful, distinctive, and long-lasting places that evoke a strong sense of Bellevue and the Northwest, and a dynamic public realm that encourages social interaction.

**POLICY S-BR-14.** Use design guidelines to promote pedestrian-friendly and transit-oriented design, ensure quality and a sense of permanence, promote environmental sustainability, and create a distinct sense of place. Conduct design review for all mixed use, office and residentially designated areas of the Subarea. Apply additional depth and attention to the details of design review within transit-oriented development nodes.

*Discussion: Design review should pay special attention to creating a pedestrian-friendly environment, by helping to create vibrant, interesting, safe, walkable and interconnected sites.*

**POLICY S-BR-15.** Integrate transit in the design of public and private developments, so that the form and connectivity of the built environment support travel choices.

*Discussion: Features such as transit stops near major buildings, building entrances oriented toward transit stops, and direct pedestrian connections between buildings and transit help to integrate transit and land use, improving the pedestrian environment and supporting travel choices.*

**POLICY S-BR-16.** Encourage place-making and a dynamic public realm by integrating publicly accessible plazas, open spaces and other gathering places with development, in public and private projects.

**POLICY S-BR-18.** Encourage diversity in the built environment through a variety of building heights and forms, building articulation and modulation. Encourage building rooflines and floorplates that break down the scale of buildings, help to differentiate Bel-Red from Downtown, and enhance the architectural variety of the area.

**POLICY S-BR-23.** Reinforce neighborhood character and identity through the use of gateways and neighborhood signage. Prohibit signage that is out of scale with or detracts from the public realm.

**POLICY S-BR-24.** Encourage private and public use of public art to enrich design aesthetics and add character, identity and a sense of place.

**POLICY S-BR-25.** Design and develop an outstanding street environment that promotes streets as key urban places, sensitive to their context and providing an interesting and aesthetically rich experience. Apply a street hierarchy with design guidelines and street standards that provides an appropriate combination of the following elements:

- a. Strong consideration of character and aesthetics in the design and implementation of all street projects;
- b. Integration of open space and landscaping, including street trees;
- c. Environmentally sensitive practices, including natural drainage systems where appropriate;
- d. Sidewalk development standards that promote pedestrian functionality and interest, and avoid obstructions;
- e. Ground floor differentiation, including preferred uses, visual and physical access;
- f. Mid-block pedestrian crossings; and
- g. On-street parking, where it contributes to pedestrian convenience and safety.

Finding:

The Urban Design goals and policies of the Bel-Red Subarea are incorporated in The Spring District Master Development Plan. The Spring District includes 29 buildings as an urban village. Prior Design Review approval will be required for future buildings/parks/development. The architectural guidelines of the Land Use Code will be implemented at the time of future Design Review applications. These guidelines include high quality building materials, interesting building massing (including building articulation), interesting rooflines and welcoming residential entries. Public parks will create “a walk in the park” to the light rail station. A public plaza at the station will provide further place-making. Pedestrian connectivity will occur throughout the site and at the gateway corner to the public sidewalk at NE 12<sup>th</sup> Street and 120<sup>th</sup> Avenue NE. A master sign permit will ensure architectural consistency with new signs placed on buildings. The Spring District will create a pedestrian-friendly street environment which will include landscaping, street trees, on-street parking, bicycle opportunities, and pedestrian-oriented uses at the sidewalk level. These improvements will be reviewed through future Design Review applications.

- 4) **Parks and Open Space Goal:** To create a robust, aesthetically beautiful, and functional parks and open space system that serves the needs of Bel-Red and the broader community, and that connects with and complements the larger Bellevue parks and open space system.

**POLICY S-BR-39.** Promote the development of “green streets” throughout the corridor, with an abundance of street trees and areas of landscaping to improve and reduce the amount of stormwater runoff, be aesthetically pleasing, and provide an attractive pedestrian experience.

Finding:

Along the streets within The Spring District, the applicant will provide the required street landscaping: street trees/low plantings. This will provide for a pleasing pedestrian experience.

- 5) **Housing Goal:** To encourage Bel-Red redevelopment to result in a diversity of housing types and prices, including a significant share of

“workforce housing.”

**POLICY S-BR-40.** Encourage a diversity of housing types, from high density, multistory housing in transit nodes, to medium density housing outside nodes, to other innovative housing forms, such as live/work and work/live units.

Finding:

Within The Spring District, approximately 1200 residential units will be constructed. The applicant assumes that units will average 1000 square feet, generally priced at levels consistent with current market. The Amenity Incentive Systems requires that the first 1.25 FAR above the base of 1.0 be earned through affordable housing. If rental, affordable housing will be provided at the 80% median income. If ownership, affordable housing will be provided at 100% median income. Exact number of affordable units will be determined during future Design Review applications. Land Use Code 20.25D.090.C provides amenity points for affordable housing or the applicant can pay a fee-in-lieu of for the affordable housing. The applicant has shown an interest in the option to pay a fee-in-lieu of for affordable housing, as defined in the Development Agreement.

- 6) **Transportation Goal:** To create a more complete, connected, and well balanced transportation system, while protecting neighborhoods from spillover traffic impacts and while ensuring that transportation investments contribute to the area’s sense of place and sustainability.

**POLICY S-BR-68.** Work with Sound Transit to realize the City’s preferred light rail route, alignment and station locations, as shown in Figure S-BR.2. Support the development of light rail stations in the vicinity of Overlake Hospital Medical Center, 122nd Avenue NE/NE 16th Street, and 130th Avenue NE/NE 16th Street.

Finding:

The Spring District has anticipated the Sound Transit station along NE 15<sup>th</sup> Street. A separate parcel and access is provided on the Master Development Plan (see Phasing Plan Graphic Representation, Section I.A).

Potential impacts and mitigation from traffic associated with The Spring District are discussed in Section V “Transportation” and Section VI “State Environmental Policy Act (SEPA).”

The applicant shall meet the intent of the “Arterial Street Design Standards within the Bel-Red Subarea.” Refer to Conditions of Approval regarding Arterial Street Design Standards in Section X.A.

- 7) **Neighborhoods/Districts Goal:** To support the development of new Bel-Red neighborhoods that achieve distinctive and high quality character.

**Node at 122nd Avenue NE**

**POLICY S-BR-87.** Provide for a mix of office, housing and retail uses in this area, with office as the predominant use. Potential height in the center of this node may reach 150 feet, and up to 125 feet in the perimeter.

*Discussion: This office-supportive node is located on the west side of the subarea, where it is better served by regional transportation infrastructure and has lesser potential for traffic impacts on East Bellevue neighborhoods. Housing and retail uses should be part of the mix of this neighborhood, to avoid a sterile office-only area and to create a balance of activities. Parks, open spaces, and recreational opportunities are envisioned as part of this urban environment. The Master Development Plan process will be used as a review mechanism for larger developments.*

Finding:

The Spring District meets the Neighborhoods/Districts goal, specifically Policy S-BR-87. The applicant proposes a Master Development Plan to layout the different elements of the project: residential to the south, office throughout, transit station north of NE 15<sup>th</sup> Street, hotel adjacent to the transit station, and a connected park system (running from the residential neighborhood to the transit station).

- B. The Master Development Plan complies with the applicable requirements of the Bellevue City Code.

Finding: A detailed review of city codes and standards will occur when individual Design Review applications and construction permit applications are submitted. As conditioned, the proposed Master Development Plan complies with the applicable requirements of the Bellevue City Code.

The proposal complies with the requirements of the Transportation Code as modified by the Development Agreement contained in Attachment D, and as conditioned in Section X. (Bellevue City Code 14.60)

The Utilities Department will require a Utility Extension (UE) Agreement for review and approval of the utility design as conditioned in Section X. (Bellevue City Code 24.02, 24.04, 24.06)

The proposed Master Development Plan complies with the requirements of the Fire Code as conditioned in Section X. (International Fire Code 503)

And, the proposal complies with the requirements of the Land Use Code as outlined in Section VIII.C below and as conditioned in Section X. (Land Use Code 20)

The applicant shall submit a sign master plan for the entire MDP project. The sign master plan may be submitted with the first Design Review application or separately as a Land Use Exemption to the approved Master Development Plan. Proposed signs onsite for individual buildings will not be approved until found

consistent with the approved sign master plan. Refer to Conditions of in Approval regarding Sign Master Plan in Section X.A

- C. The proposed Master Development Plan addresses all applicable standards, guidelines or criteria of this Code in a manner which fulfills their purpose and intent.

Finding: As conditioned, the proposal addresses all applicable standards, guidelines or criteria of the Land Use Code in a manner which fulfills its purpose and intent.

The purpose of a Master Development Plan is as follows: a mechanism by which the City can ensure that site development including structure placement, vehicular and pedestrian mobility and necessary amenities are developed and phased to conform to the terms of the Land Use Code and other applicable City codes and standards. (Land Use Code 20.30V.120)

The proposal meets this criteria. The Spring District is a 36 acre site development which occurs over 7 phases and 15 years. The Spring District Master Development Plan shows site development including phasing, the number and type of buildings, road location, sidewalks and pedestrian connections to adjacent streets. The proposal also includes an integrated public park system (approximately 1.74 acres) which will connect the residential buildings to the Sound Transit station. Other amenities include public art, pedestrian-oriented uses (such as restaurants) and a public gateway with a graceful entry to the project.

See Section III for detailed discussion regarding consistency with Land Use Code and Zoning requirements, including consistency with the Bel-Red chapter of the Land Use Code 20.25D.

- D. The Master Development Plan depicts features of and relationships and connectivity between required site features for the underlying Land Use District.

Finding: The subject zoning of the site is Bel-Red OR-1 and OR-2. See Section III for zoning map. The Spring District Master Development Plan shows features of and relationships and connectivity between required site features of the underlying Land Use District.

The Master Development Plan shows connectivity between the residential portions of the project, through a “walk in the park,” to the light right station. Connectivity is also shown through the pedestrian gateway connection in the southwest corner of the site connecting to NE 12<sup>th</sup> Street and 120<sup>th</sup> Avenue NE. A multi-use trail along the west side of 124<sup>th</sup> Avenue NE provides further connectivity along the perimeter of the project. A pedestrian connection will be provided between Buildings 15 & 16 to connect from 123<sup>rd</sup> Avenue NE to 124<sup>th</sup> Avenue NE. Refer to Conditions of Approval regarding Pedestrian Amenities in Section X.A.

The LTR station planned along NE 15<sup>th</sup> Street will provide light rail transportation to residents and office workers. This station will also bring others to visit The Spring District as a “destination” for public parks, restaurants, etc.

**IX. DECISION**

After reviewing the proposal for consistency with applicable City of Bellevue requirements, policies, development standards and SEPA, the Director hereby APPROVES WITH CONDITIONS THE SPRING DISTRICT MASTER DEVELOPMENT PLAN.

**X. CONDITIONS OF APPROVAL**

**COMPLIANCE WITH BELLEVUE CITY CODES AND ORDINANCES:**

The applicant shall comply with all applicable Bellevue City Codes and Ordinances including but not limited to:

Clearing and Grading Code - BCC 23.76	Savina Uzunow	425-452-7860
Bellevue Development Standards	Savina Uzunow	425-452-7860
Transportation Code - BCC 14.60	Abdy Farid	425-452-7698
Trans. Development Review - BCC.22.16	Abdy Farid	425-452-7698
Right-of-Way Use Permit - BCC 14.30	Tim Stever	425-425-4294
Bellevue Utilities Code - BCC Title 24	Mark Dewey	425-452-6179
Construction Codes - BCC Title 23	Mark Chang	425-452-6997
Structural Codes – BCC Title 23	Mark Chang	425-452-6997
Land Use Code - BCC Title 20	Carol Hamlin	425-452-2731
Sign Code - BCC Title 22B	Carol Hamlin	425-452-2731
Noise Control - BCC 9.18	Carol Hamlin	425-452-2731
Uniform Fire Code - BCC 23.11	Adrian Jones	425-452-6032
Transportation Department Design Manual	Abdy Farid	425-452-7698
The Bel-Red Corridor Plan Streetscape Character, Guidelines, and Standards	Abdy Farid	425-452-7698
Addressing	Jami Carter	425-452-4310

**A. GENERAL CONDITIONS:** The following conditions apply to all phases of development.

The following conditions are imposed under authority referenced:

1. Vested Status of the Master Development Plan: The vested status of the Master Development Plan shall be for a period of 15 years from the date of final decision, as defined in LUC 20.35.045 per the Development Agreement B.2.

Authority: Development Agreement B, signed July 10, 2009 and recorded September 11, 2009 recording number #200909110002269

2. Development Agreement (DA) between the City and Applicant:

(a) Residential Property:

- The applicant shall meet the DA requirement that Residentially-restricted property shall either be sold, developed or proportionally developed prior to the date of Sound Transit's regularly scheduled passenger-carrying service or be subject to delay penalties as described in the DA.
- The applicant shall record the appropriate covenant with each Design Review approval for "Residentially-restricted Property."

Authority: Development Agreement, signed July 10, 2009 and recorded September 11, 2009 recording number #200909110002269, paragraph H.

(b) Parks:

- Within Phase 1, the project shall include development of a public mini-park a minimum of one acre in size (designated as project M-3 in the Bel-Red Parks and Open Space Project List in the Comprehensive Plan) and an activated park or recreation space of at least 30,000 contiguous square feet.
- All park tracts developed as a condition of the Development Agreement shall, if owned and maintained by the applicant, record an easement securing public access over the park tracks.
- Future design and development of the one acre public park (M-3), per the Development Agreement, shall be approved by the Parks & Community Services Department and be consistent with BCC 3.43.

Authority: Development Agreement, signed July 10, 2009 and recorded September 11, 2009 recording number #200909110002269, paragraph A.2

(c) Minimum FAR: Refer to Conditions of Approval regarding Amenities and FAR for each Design Review in Section X.B.

Authority: Development Agreement, signed July 10, 2009 and recorded September 11, 2009 recording number #200909110002269, paragraph A.2

(d) Other DA Requirements: The applicant shall meet all DA requirements, including any not noted specifically in this staff report.

Authority: Development Agreement, signed July 10, 2009 and recorded September 11, 2009 recording number #200909110002269, paragraph A

Reviewer: Carol Hamlin, phone 425-452-2731  
Camron Parker, phone 425-452-2032

Authority: Development Agreement B.2, dated September 11, 2009  
recording number #200909110002269; BCC 3.43

3. Review Process for Parks/Plaza/Gateways: Review and approval of all park/plaza/gateway areas shall be conducted under Design Review approval, with input by the City of Bellevue into the design of these areas.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.25D.030

4. Phasing Plan: The Phasing Plan shall be followed per the Conceptual Phasing Plan (Attachment F). Modifications to the phasing plan may occur, per LUC 20.30V.160.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.30V.160

5. City Council Approval of NE 15<sup>th</sup> Street/NE 16<sup>th</sup> Street and portrayal on the Binding Site Plan:  
If shifting of the NE 15<sup>th</sup> Street/NE 16<sup>th</sup> Street alignment (horizontal/vertical) at a later date is in conflict with the recorded BSP, the applicant shall amend the BSP to match the City Council approved alignment (horizontal/vertical) of NE 15<sup>th</sup> Street/NE 16<sup>th</sup> Street. Such amendment of the BSP shall occur prior to submittal of the next Design Review application for the MDP site after the final alignment is approved. The vested status of final street locations does not occur until an adjacent DR is issued.

Reviewers: Abdy Farid, phone 425-452-7698  
Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.30V.140, RCW 58.17

6. Public Plaza at the Light Rail Station: As a part of the master planning process for the Sound Transit 120<sup>th</sup> East Link Station, the applicant shall coordinate with the City of Bellevue and Sound Transit regarding the design of a public plaza adjacent to Sound Transit's 120<sup>th</sup> East Link subterranean light rail station. The applicant shall designate the plaza as a "public plaza" on the recorded Master Development Plan.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.25D.150

7. Pedestrian Amenities:

- **Multi-purpose trail:** A multi-purpose trail (12-14 feet in width) shall be provided along the entire length of The Spring District's property along 124<sup>th</sup> Avenue NE to connect to the multi-purpose trail on NE 16<sup>th</sup> Street as shown on the MDP plans. See Attachment A. For Phase 1a, the applicant shall design the entire trail (for both Phase 1a and Phase 1b) and construct the Phase 1a trail adjacent to any Phase 1a development. For Phase 1b, the applicant shall complete construction of the entire trail.
- **Landscaping along 124<sup>th</sup> Avenue NE:** The area within the Seattle City Light easement (adjacent to 124<sup>th</sup> Avenue NE) shall be landscaped along with a multi-purpose trail. Vehicular parking areas, vehicular lanes or load/unload areas will be prohibited within this easement area. This landscaping shall be installed as a part of Phase 1.
- **Mid-block Public Pedestrian Connection:** A mid-block public pedestrian connection (12-14' wide) between Buildings 15 & 16 (as shown on the MDP plans, Attachment A) shall be provided from 123<sup>rd</sup> Avenue NE to 124<sup>th</sup> Avenue NE and shall meet ADA accessibility requirements and the intent of the Green Streets Development Standards. The location and design shall meet the intent of LUC 20.25D.140.D. This mid-block public pedestrian connection with its associated easement and signage shall be installed as a part of Phase 1.
- **Green Streets:** The modification approval to relocate the Green Street from NE 13<sup>th</sup> Street to the loop roads is applicable to future reviews of Design Review applications. The applicant shall provide a mid-block public pedestrian connection between Buildings 15 & 16. See the condition above: **Mid-block Public Pedestrian Connection.**
- **Sidewalks:** The applicant shall provide sidewalks along all city/private streets within the project limits.
- **"Pedestrian" Gateway Connection:** A "Pedestrian" Gateway Connection shall be located at the southwest corner of the property between Buildings 18 & 19 as shown on the MDP plans. See Attachment A. The gateway shall provide pedestrian access from the site (approximately NE 13<sup>th</sup> Street and 121<sup>st</sup> Avenue NE) to the adjacent city street at NE 12<sup>th</sup> Street (near 120<sup>th</sup> Avenue NE). The pedestrian connection shall be designed and permitted during the Design Review process for the first adjacent building to the gateway.

Reviewers: Carol Hamlin, phone 425-452-2731  
Abdy Farid, phone 425-452-7698

Authority: LUC 20.30V.150.D, LUC 20.25D.140.A.3.a, LUC 20.25D.140.D, LUC 20.25D.150.C, Bel-Red Subarea Policies S-BR-14, 16, 23, 25

8. Other Gateway Opportunities:

The applicant will be required to provide a gateway entrance at the street level for pedestrians, motorists, and those passing by the site. The location and timing of the construction of this gateway entrance at street level shall be determined during Phase 1. Likely locations would be the entrance to the site at NE 13<sup>th</sup> Street (from 124<sup>th</sup> Avenue NE) or NE 15<sup>th</sup> Street (from 120<sup>th</sup> Avenue NE or 124<sup>th</sup> Avenue NE). Such location shall be reviewed and approved under Design Review and approved by the Transportation Department.

Reviewer: Carol Hamlin, phone 425-452-2731

Abdy Farid, phone 425-452-7698

Authority: LUC 20.25D.150.B .3, LUC 20.25D.140.B

9. Sign Master Plan: The applicant shall submit a sign master plan for the entire MDP project. The sign master plan may be submitted with the first Design Review application or separately as a Land Use Exemption to the approved Master Development Plan. Proposed signs onsite for individual buildings will not be approved until found consistent with the approved sign master plan.

Reviewer: Carol Hamlin, phone 425-452-2731

Authority: LUC 20.25D.150.F

10. Recycling and Solid Waste Collection: With each Design Review application, the applicant shall document how recycling and solid waste will be collected. Recycling and solid waste receptacles may not be pulled out to the adjacent street/sidewalk. Refuse and recycling areas may be not located within adjacent public/private streets or adjacent rights-of-way. These activities must be located on each individual building site and screened from public view. The applicant shall coordinate the location of receptacles with Allied Waste or any successor in interest to the Bellevue Waste Hauling franchise.

Reviewer: Carol Hamlin, phone 425-452-2731

Authority: LUC 20.20.725

11. Street Development: Future street development must be approved by the Transportation Department and be consistent with LUC 20.25D.140.

Reviewer: Carol Hamlin, phone 425-452-2731

Abdy Farid, phone 425-452-7698

Authority: LUC 20.25D.140

12. Access to Underground Parking Garages: In order to provide alternate vehicular routes to city streets (due to the modification of the NE 12<sup>th</sup> Street local street), the applicant shall provide two vehicular access points to the underground parking garages, one along NE 12<sup>th</sup> Street and the other along

120<sup>th</sup> Avenue NE (signalized). The exact location of these parking garage driveways shall be determined at the Design Review stage of the subject buildings/phases.

Reviewer: Carol Hamlin, phone 425-452-2731  
Abdy Farid, phone 425-452-7698  
Authority: LUC 20.25D.140.A.2

13. Modification to MDP: The applicant shall submit a modification to the Master Development Plan for minor changes to the approved MDP. Modifications can be processed as a Land Use Exemption application (LUC 20.30V.160.B). Minor changes include updates to road configurations, FAR calculations, the Amenity Chart, impervious surface/lot coverage and the Sign Master Plan. It will still be necessary to record the updated MDP.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.30V.160; Development Agreement, signed July 10, 2009 and recorded September 11, 2009 recording number #200909110002269, paragraph C.3.

14. Binding Site Plan: The Binding Site Plan shall be recorded upon the final decision of the Master Development Plan before the sale or lease of any lot, tract or parcel. The BSP shall include all required dedications and easements per the MDP conditions of approval at the time of each DR application for each project.

Additional amendments may be required based on future phases of development. Any noted discrepancies between the public and private street cross sections as portrayed on the MDP plans (Attachment A) and the initial BSP plan submittal on March 29, 2012 (12-110450-LF) shall be reconciled prior to recording of the BSP.

Modifications shall be processed as an amendment to the Binding Site Plan, per RCW 58.17.

Reviewer: Carol Hamlin, phone 425-452-2731  
Abdy Farid, phone 425-452-7698  
Authority: LUC 20.30V.140, RCW 58.17

15. Preliminary Design, Utility Codes and Engineering Standards: Utility review has been completed on the preliminary information submitted at the time of this application. The review of this application has no implied approvals for water, sewer and storm drainage components of the project. Final plan approval will occur under a Utility Extension Agreement which will be required for review and approval of the utility design. Submittal of the utility extension will coincide with future clearing and grading permit review. Final civil engineering may require some changes to the site layout to accommodate the utilities.

Reviewer: Mark Dewey, phone 425-452-6179  
Authority: BCC Title 24.02, 24.04, 24.06

16. Art Concept: An art concept is required to be consistent with LUC 25D.150.B.5.a. Such art shall be installed within one of the public parks/plaza/gateways and reviewed under the subject Design Review application. Review/approval by the City of Bellevue Arts Commission is not required. The art concept shall be installed prior to completion/final certificate of occupancy of The Spring District.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.25D.150, LUC 20.25D.030, Bel-Red Subarea Policy S-BR-24

17. Addressing of Buildings/Tracts: The applicant shall contact Jami Carter, Information Technology Department, phone 425-452-4310 regarding the addressing of buildings/tracts. The Planning and Community Development Department shall review and approve such addressing.

Reviewer: Adrian Jones, phone 425-452-6032  
Authority: Uniform Fire Code 505

18. Restricted Driveway Access: With each new Design Review application, the applicant shall document how the driveway access of that development meets the restricted driveway access intent of LUC 20.25D.140.F.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.25D.140.F

19. Arterial Street Design Standards: The applicant shall meet the intent of the "Arterial Street Design Standards within the Bel-Red Subarea."

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.30V.150

**Transportation Department:**

20. Right of Way and Easements: The applicant shall dedicate to the City all necessary right of way such that street improvements including on-street parking to the back of curb are located within the fee public right of way. The applicant shall also grant any necessary construction, pedestrian, landscaping and utility easements. The applicant shall provide easements to the City for location of signal and street light facilities such as above-grade boxes and below-grade vaults between the building and sidewalk within the landscape area.

Authority: BCC 14.60.100  
Reviewer: Abdy Farid (425) 452-7698

21. Vehicular Access Restrictions: All non-signalized driveways and private roads adjacent to 120<sup>th</sup> Avenue NE, NE 15<sup>th</sup> Street, 124<sup>th</sup> Avenue NE and NE 12<sup>th</sup> Street will be restricted to right in/right out. No left turns will be allowed at the signalized intersection of NE 15<sup>th</sup> Street/ 123<sup>rd</sup> Avenue NE. The applicant will purchase and post “No Left Turn” signs at the subject street and/or driveway locations, when and if necessary as directed by the City. All access is subject to additional restrictions based on traffic operations conditions as determined by the Transportation Department.

Authority: BCC 14.60.150  
Reviewer: Abdy Farid (425) 452-7698

22. Local Improvement District (LID): The applicant shall not protest the city’s formation of a LID or Transportation Improvement District when it is one of multiple-funding alternatives having the purposes of constructing arterial street improvements on 120<sup>th</sup> Avenue NE north of NE 8<sup>th</sup> Street and south of Northup Way, 124<sup>th</sup> Avenue NE north of NE 12<sup>th</sup> Street and south of Northup Way, NE 15<sup>th</sup> Street or other internal public street segments including pavement, widening, curbs, gutters, landscaping and medians, on-street public parking, landscape strips, sidewalks, urban design amenities, gateway amenities, bike lanes, multi-purpose pathways, street lighting, underground utilities, traffic signal system elements, storm drainage flow control and water quality treatment and associated appurtenances. The property owner does not waive the right to protest the method of assessment or the amount of assessment or identified special benefits attached to the property. This no protest provision shall be recorded and attach to the properties or any future properties through segregation or aggregation of the property.

Authority: BCC 14.60.110.C  
Reviewer: Abdy Farid (425) 452-7698

- B. **PRIOR TO ANY DESIGN REVIEW (DR) APPROVAL FOR A BUILDING, PHASE, AND/OR PARK/PLAZA/GATEWAY**: The following conditions must be complied with prior to any Design Review (DR) approval for a building, phase, and/or park/plaza/gateway:

1. Design Review: Each new site development (i.e. building, parks, plaza, gateway) shall require Design Review approval. If appropriate, Design Review applications may be combined.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.25D.030

2. Outward Focus of Perimeter Buildings: During the Design Review of individual buildings on the perimeter (124<sup>th</sup> Avenue NE, NE 12<sup>th</sup> Street, 120<sup>th</sup> Avenue NE), the applicant shall provide building designs that convey an outward focus toward the city streets as well as toward the interior of the development. The applicant shall incorporate the design guidelines of LUC 20.25D.150. The use of blank walls or flat nondescript walls that are not articulated by any visual interest is not consistent with applicable design criteria.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.25D.150, Bel-Red Subarea Policies S-BR-14, 18, 25

3. Coordinated Landscaping Plan along Site Perimeter: With the first perimeter Design Review submittal along 124<sup>th</sup> Avenue NE and the first perimeter Design Review submittal along 120<sup>th</sup> Avenue NE, a coordinated perimeter landscaping plan shall be submitted for the length of The Spring District property along each right-of-way. Note: This coordinated landscaping plan is for on-site landscaping and not off-site landscaping associated with any city right-of-way project.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.30V.150.D,

4. Impervious Surface/Lot Coverage: The maximum impervious surface/lot coverage is 75% MDP area-wide. This calculation shall be provided upon each Design Review application to keep track towards the final phase of the MDP.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.25D.080.A

5. Parking Stall Calculations: Each building shall submit parking stall calculations to document how it meets the Land Use Code requirements for onsite parking.

Reviewer: Carol Hamlin, phone 425-452-2731  
Authority: LUC 20.25D.120

6. Amenities and FAR for each Design Review:

- The phasing plan for the MDP shall provide for proportionate installation of amenities that must be included when each phase of development is constructed.
- Required amenities for each individual building shall be provided at the time of that building construction. In-lieu fees shall be assessed and collected at building permit issuance. Required amenities cannot be "borrowed" from future buildings or phases.
- The applicant shall meet the amenities outlined in the Development Agreement as well as the Land Use Code. The applicant shall use the

attached Amenity calculation sheet (Attachment B) to show compliance with the DA and Land Use Code.

- The calculations shall show that for the overall MDP at full build-out, there is at least a minimum 2.5 FAR overall (per the Development Agreement) with a maximum 4.0 FAR overall (per the Land Use Code).
- With each Design Review application submitted under the MDP, the applicant shall include the physical FAR of the proposed project, as well as the calculated FAR of the project as part of the overall MDP rolling average. At no time shall the averaged MDP FAR fall below 2.5 (DA) or exceed 4.0 (LUC).
- Use of an alternate MDP FAR calculation or a change to the target MDP FAR must be reviewed and approved by the City of Bellevue as a modification, processed as a Land Use Exemption application to Design Review or as a modification request with the subject Design Review application.

Reviewer: Carol Hamlin, phone 425-452-2731

Authority: LUC 20.30V.130, LUC 20.25D.030.B, LUC 20.25D.090, LUC 20.30F.175.C, Development Agreement dated September 11, 2009 recording number #200909110002269

**C. PRIOR TO CLEARING & GRADING (CG) PERMIT:** The following conditions must be complied with on plans submitted with the Clearing & Grading or Demolition permit application:

1. Clearing and Grading Permit: A Clearing and Grading Permit is required for each phase of the development per BCC 23.76.035. If individual buildings are built independent of the particular construction phase, a separate Clearing and Grading Permit will be required. The permit applications must be in accordance with the Clearing and Grading Code, as outlined in the submittal requirements and the Clearing and Grading Development Standards, which is available on the City of Bellevue website at: <http://www.bellevuewa.gov/clearing-grading-standards.htm>.

Reviewer: Savina Uzunow, phone 425-452-7860

Authority: Bellevue City Code 23.76

**2. Transportation Department:**

**a. RIGHT-OF-WAY USE PERMIT**

Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

- 1) Designated truck hauling routes.
- 2) Truck loading/unloading activities.
- 3) Location of construction fences.

- 4) Hours of construction and hauling.
- 5) Requirements for leasing of right of way or pedestrian easements.
- 6) Provisions for street sweeping, excavation and construction.
- 7) Location of construction signing and pedestrian detour routes.
- 8) All other construction activities as they affect the public street system.

In addition, the applicant shall submit for review and approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevents access. General materials storage and contractor convenience are not reasons for preventing access.

As determined by the inspector, damage to any City infrastructure resulting from any work or process done under or associated with this permit will be repaired to as good as or better condition in a manner and with materials as approved by the City. Such repair may require an addendum to the existing permit or may require a new Right of Way Use permit. The City will allow soil nailing, but not within five feet of the existing utilities. The applicant must sign an Indemnification Agreement for the area affected by the nails prior to the issuance of clear and grading permit.

The applicant will secure sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.

The City will require a Right of Way Use License or Agreement that addresses all requested private features in the public right of way. Unless otherwise approved, no private features will be allowed in the public right of way.

Authority: BCC 11.70 & 14.30  
Reviewer: Tim Stever (425) 452-4294

**b. CIVIL ENGINEERING PLANS – TRANSPORTATION**

Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all street frontage improvements and driveway accesses must be in conformance with the requirements of the Americans with Disabilities Act; the Transportation Development Code; the provisions of the Transportation Department Design Manual; The Bel-Red Corridor Plan Streetscape Character, Guidelines, and Standards and specific requirements stated elsewhere in this document. All relevant standard drawings from the Transportation Department Design Manual shall be copied exactly into the

final engineering plans. Requirements for the engineering plans include, but are not limited to:

- 1) Traffic signs and markings.
- 2) Curb, gutter, sidewalk, and driveway approach design. (The engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as needed.)
- 3) Handicapped ramps, crosswalk revisions, and crosswalk equipment such as pushbuttons.
- 4) Installation or relocation of streetlights and related equipment.
- 5) Sight distance. (Show the required sight triangles and include any sight obstructions, including those off-site.)
- 6) Location of fixed objects in the sidewalk or near the driveway approach.
- 7) Trench restoration within any right of way or access easement.

Specific requirements are detailed below.

- 8) The applicant is responsible for the entire cost associated with the design, installation and operation of a new traffic signal at 124<sup>th</sup> Avenue NE/ NE 13<sup>th</sup> Street intersection.
- 9) The applicant is responsible for the entire cost associated with the design, installation and operation of a new traffic signal at 123<sup>rd</sup> Avenue NE/ NE 15<sup>th</sup> Street intersection.
- 10) The applicant is responsible for the entire costs associated with the design, installation and operation of a new traffic signal at the 120<sup>th</sup> Avenue NE/ Site Access (i.e., between Buildings 8 & 17).
- 11) The applicant is responsible for the entire costs associated with the design, installation and operation of a new "All-Way Pedestrian Crossing" traffic signal at the 121<sup>st</sup> Avenue NE/ NE 15<sup>th</sup> Street intersection.
- 12) The applicant is responsible for the cost associated with the design and installation of street lights along the entire site frontage.
- 13) The applicant is responsible to design and construct the site's roadways including public streets such as 121<sup>st</sup> Avenue NE, 123<sup>rd</sup> Avenue NE and NE 13<sup>th</sup> Street.
- 14) The applicant is responsible to design and construct the "Green Streets" (i.e. the loop roads) located within the Spring District MDP, as identified in Attachment H. Such design and construction shall be consistent with the Bel-Red Corridor Plan Streetscape Character, Guidelines, and Standards.
- 15) The applicant is responsible to design and construct:
  - a. The multi-purpose trail along the entire length of 124<sup>th</sup> Avenue NE (i.e., between NE 16<sup>th</sup> Street and the site's most southern property line) and NE 16<sup>th</sup> Street between 120<sup>th</sup> Avenue NE and 124<sup>th</sup> Avenue NE;
  - b. The midblock pedestrian connection between Buildings 15 & 16; and
  - c. The pedestrian connection to NE 12<sup>th</sup> Street between Buildings 18

& 19.

- 16) The applicant is responsible for all street channelization, marking, and signage as necessary under each phase of the master development plan.
- 17) Any damage to the site's frontage during the construction must be repaired or replaced as directed by the Transportation Department Construction Inspector.
- 18) Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.
- 19) Miscellaneous:
  - City standards for driveway widths range from 30 to 36 feet on arterial streets. The site's NE 12<sup>th</sup> Street driveway must be at least 30-foot wide. Aprons must be constructed in accordance with Design Manual Standard Drawing DEV-7A or in accordance with The Bel-Red Corridor Plan Streetscape Character, Guidelines, and Standards. The details of the design must be included in the final engineering plans.
  - Landings on sloping approaches are not to exceed a 7% slope for a distance of 30 feet approaching the back edge of sidewalks. Driveway grades must be designed to prevent vehicles from bottoming out due to abrupt changes in grade.
  - Vehicle and pedestrian sight distance must be provided per BCC 14.60.240 and 14.60.241. Sight distance triangles must be shown at all driveway locations and must consider all fixed objects and mature landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance.

Authority: BCC 14.60.090, 110, 120, 150, 181, 200, 210, 240, 241; Transportation Department Design Manual Sections 9, 12, 14, 19, 20; and Transportation Department Design Manual Standard Drawings DEV-2, DEV-3, DEV-7A, DEV-10, TE-4, TE-5, TE-7, TE-10, TE-11, TE-12 and TE-21, The Bel-Red Corridor Plan Streetscape Character, Guidelines, and Standards.  
Reviewer: Abdy Farid (425) 452-7698

**D. PRIOR TO BUILDING PERMIT (BP):** The following conditions are required by City Code. Unless specified otherwise below, these conditions must be complied with on plans submitted with the Building Permit application:

1. **Fire Department:**

- a. Fire Hydrants: On plan sheets C2.00 to C2.06, relocate fire hydrants per Bellevue requirements and per locations of Fire Department connections.

Reviewer: Adrian Jones, phone 425-452-6032  
Authority Basis: International Fire Code 503

- b. Fire Apparatus Access: On plan sheet A3.10 street sections, the Fire Department will review Fire Apparatus access to the buildings before acceptance of the proposed street sections.

Reviewer: Adrian Jones, phone 425-452-6032  
Authority Basis: International Fire Code 503

- c. Fire Flow: Fire flow for Type 1A & 1B buildings is based on the largest 3 consecutive floors. See 2009 International Fire Code Appendix B Section B104.3.

Reviewer: Adrian Jones, phone 425-452-6032  
Authority Basis: 2009 International Fire Code Appendix B Section B104.3

## 2. Transportation Department:

### a. TRANSPORTATION IMPACT FEE

Payment of the traffic impact fee will be required at the time of building permit issuance. This fee is subject to change and the fee schedule in effect at the time of building permit issuance.

For development with a single building permit for an underground parking garage and above-ground structure, the impact fee must be paid with the issuance of the permit. When there is a shoring permit for an underground garage followed by the building permit for a garage and a tower, or a shoring permit followed by one building permit for a garage and one or more additional building permits for a podium and one or more towers, the payment of the impact fee must occur with the issuance of the building permit for the structure which will generate the trips which trigger the fee.

Authority: BCC 22.16  
Reviewer: Abdy Farid (425) 452-7698

### b. EXISTING EASEMENTS

There are existing utility easements contained on this site that will be affected by future development as discussed in the master plan. Any negative impact that such development has on those easements must be mitigated or easements relinquished.

Authority: BCC 14.60.100  
Reviewer: Tim Stever (425) 452-4294

**c. EASEMENTS FOR SIGNAL CONTROL AND STREET LIGHT BOXES AND VAULTS**

The applicant shall grant easements to the City for location of signal and street light facilities such as above-grade boxes and below-grade vaults between the building and sidewalk within the landscape area.

Authority: BCC 14.60.100  
Reviewer: Abdy Farid (425) 452-7698

**d. PEDESTRIAN EASEMENTS**

The applicant shall grant easements to the City such that sidewalks outside of the City right of way along the property frontage are located within a pedestrian easement area.

Authority: BCC 14.60.100  
Reviewer: Abdy Farid (425) 452-7698

**e. DEDICATION OF RIGHT OF WAY**

The applicant shall dedicate right of way to the City along the property frontage such that street improvements including on street parking to the back of curb are located within the public right of way.

Authority: BCC 14.60.090  
Reviewer: Abdy Farid (425) 452-7698

**f. PUBLIC ACCESS EASEMENT**

The applicant shall provide a public pedestrian easement for the multi-purpose trail along the entire length of 124<sup>th</sup> Avenue NE (i.e., between NE 16<sup>th</sup> Street and the site's most southern property line); NE 16<sup>th</sup> Street between 120<sup>th</sup> Avenue NE and 124<sup>th</sup> Avenue NE; the midblock pedestrian connection between Buildings 15 & 16; and the pedestrian connection to NE 12<sup>th</sup> Street between Buildings 18 & 19. This public access easement shall be provided at the appropriate phase of development.

Authority: BCC 14.60.100  
Reviewer: Abdy Farid (425) 452-7698  
Carol Hamlin (425) 452-2731

**g. TRANSPORTATION MANAGEMENT PROGRAM**

The owner of the property being developed shall sign an agreement to establish a Transportation Management Program to the extent required by Sections 14.60.070.

Authority: BCC 14.60.070  
Reviewer: Abdy Farid (425) 452-7698

**h. ENGINEERING PLANS**

A road plan, street lighting plan, traffic signal plan, and site (civil engineering) plan produced by a qualified engineer must be approved by the City prior to building permit approval. The design of all street

frontage improvements must be in conformance with the requirements of the Americans with Disabilities Act and the Transportation Development Code; the provisions of the Transportation Department Design Manual; and The Bel-Red Corridor Plan Streetscape Character, Guidelines, and Standards.

Authority: BCC 14.60; Transportation Department Design Manual

Reviewer: Abdy Farid (425) 452-7698

**E. PRIOR TO ISSUANCE OF ANY CERTIFICATE OF OCCUPANCY:**

**1. STREET FRONTAGE IMPROVEMENTS**

All street frontage improvements and other required transportation elements for each phase of the master development plan, including but not limited to: street light and traffic signal installation and/or revisions, must be constructed by the applicant and accepted by the City Inspector. All existing street light and traffic signal apparatus affected by this development, including traffic controllers, pedestrian signal poles, traffic signal poles, and power sources, must be relocated as necessary. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible. Bonding or other types of assurance devices will not be accepted in lieu of construction. Specific requirements are detailed below.

Authority: BCC 14.60.090, 110, 120, 150, 181, 200, 210, 240, 241; Transportation Department Design Manual Sections 5, 6, 9, 11, 12, 13, 14, 15, 19, 20, 21; and Transportation Department Design Manual Standard Drawings TE-7, TE-10, TE-11, TE-12, DEV-7A, DEV-10. The Bel-Red Corridor Plan Streetscape Character, Guidelines, and Standards.

Reviewer: Abdy Farid (425) 452-7698

**2. PAVEMENT RESTORATION**

Pavement restoration associated with street frontage improvements or to repair damaged street surfaces will be evaluated at the Design Review application for each new building or phase.

Authority: BCC 14.60. 250; Design Manual Design Standard #21

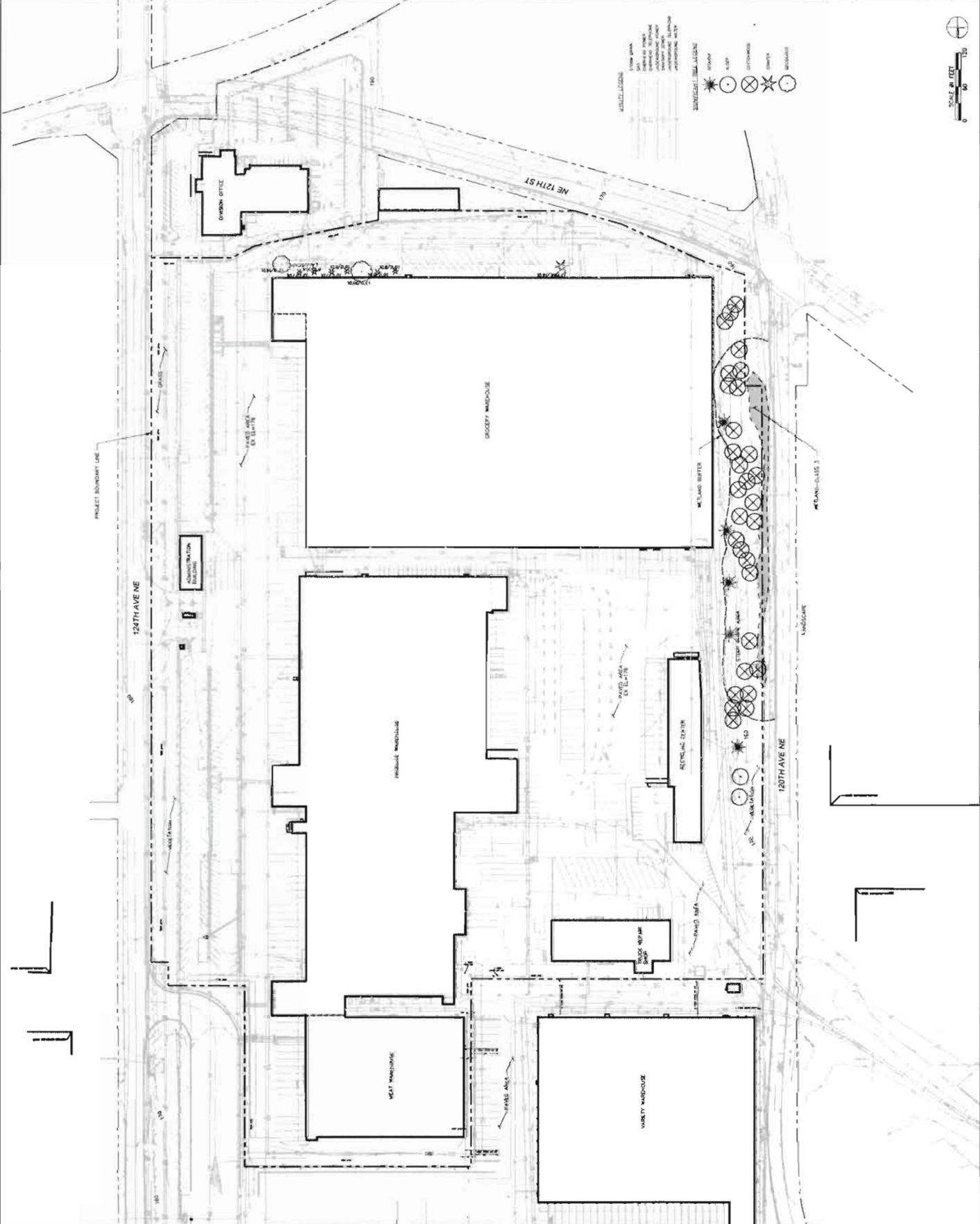
Reviewer: Tim Stever (425) 452-4294





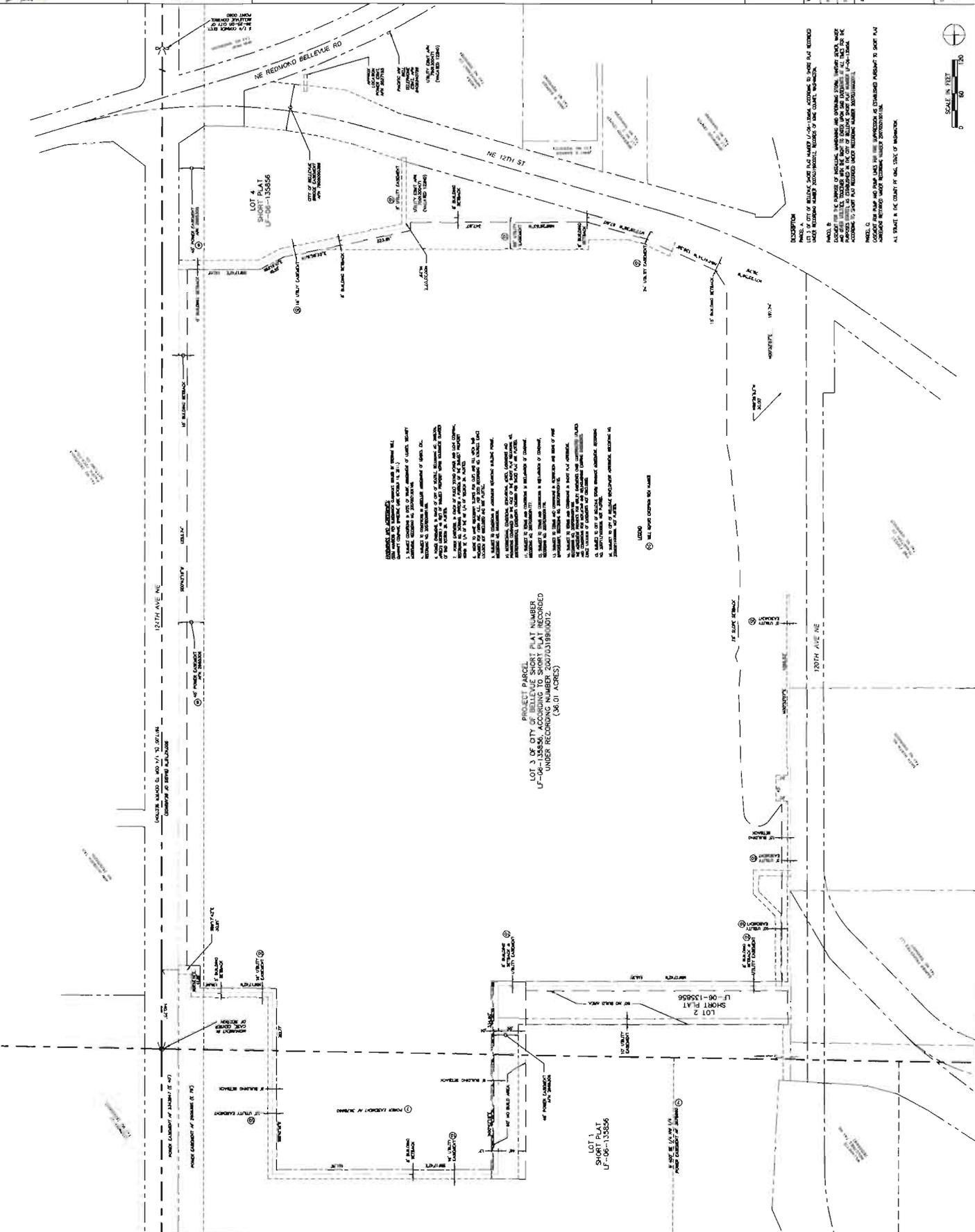
DATE:	11/10/12
DRAWN BY:	W.S.
CHECKED BY:	W.S.
PROJECT NO.:	10019613
DATE:	3/10/09
PROJECT:	SPRING

**EXISTING SITE  
SURVEY**





DATE	3/16/2012
DRAWN BY	DAVID P. HARRIS
CHECKED BY	DAVID P. HARRIS
DATE	10/01/09
PROJECT	3/16/2012



SCALE IN FEET  
 0 60 120



Author	10/18/13
Designer	3/16/2013

Wright Runstad & Company  
1201 3rd Avenue  
Suite 2700  
Seattle, WA 98101

Shorenstein Properties LLC  
235 Montgomery St  
16th Floor  
San Francisco, CA 94104

NBBJ  
223 Yale Avenue North  
Seattle, WA 98109

Parametrix  
1019 39th Avenue SE  
Suite 100  
Puyallup, WA 98374

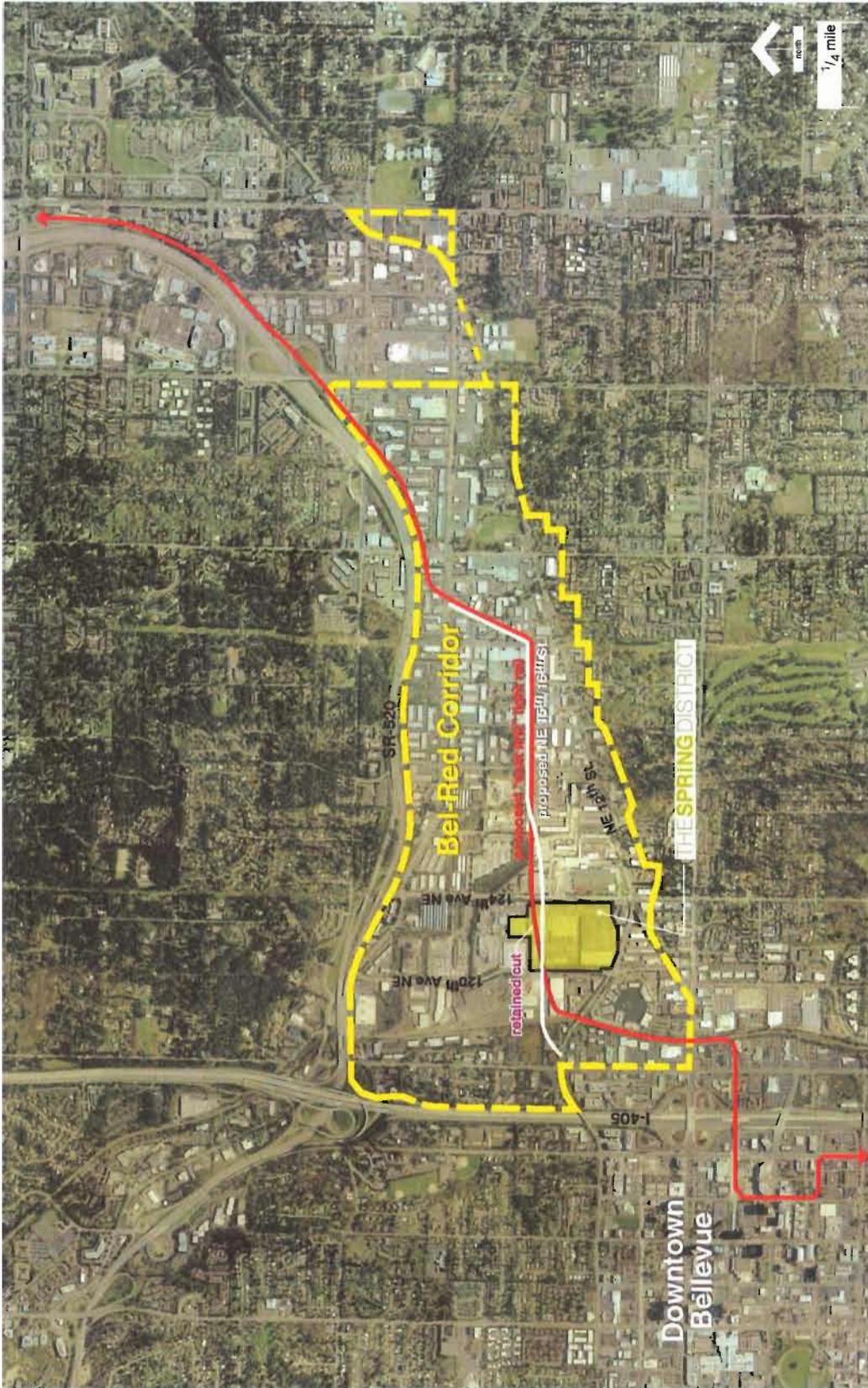
Transportation Solutions, Inc.  
8250 165th Avenue NE  
Suite 100  
Redmond, WA 98052



DRAWING INDEX

SITE PLAN B

AL1	Conform Street Drain
AL2	Conform Street Drain
AL3	Conform Street Drain
AL4	Conform Street Drain
AL5	Conform Street Drain
AL6	Conform Street Drain
AL7	Conform Street Drain
AL8	Conform Street Drain
AL9	Conform Street Drain
AL10	Conform Street Drain
AL11	Conform Street Drain
AL12	Conform Street Drain
AL13	Conform Street Drain
AL14	Conform Street Drain
AL15	Conform Street Drain
AL16	Conform Street Drain
AL17	Conform Street Drain
AL18	Conform Street Drain
AL19	Conform Street Drain
AL20	Conform Street Drain
AL21	Conform Street Drain
AL22	Conform Street Drain
AL23	Conform Street Drain
AL24	Conform Street Drain
AL25	Conform Street Drain
AL26	Conform Street Drain
AL27	Conform Street Drain
AL28	Conform Street Drain
AL29	Conform Street Drain
AL30	Conform Street Drain
AL31	Conform Street Drain
AL32	Conform Street Drain
AL33	Conform Street Drain
AL34	Conform Street Drain
AL35	Conform Street Drain
AL36	Conform Street Drain
AL37	Conform Street Drain
AL38	Conform Street Drain
AL39	Conform Street Drain
AL40	Conform Street Drain
AL41	Conform Street Drain
AL42	Conform Street Drain
AL43	Conform Street Drain
AL44	Conform Street Drain
AL45	Conform Street Drain
AL46	Conform Street Drain
AL47	Conform Street Drain
AL48	Conform Street Drain
AL49	Conform Street Drain
AL50	Conform Street Drain
AL51	Conform Street Drain
AL52	Conform Street Drain
AL53	Conform Street Drain
AL54	Conform Street Drain
AL55	Conform Street Drain
AL56	Conform Street Drain
AL57	Conform Street Drain
AL58	Conform Street Drain
AL59	Conform Street Drain
AL60	Conform Street Drain
AL61	Conform Street Drain
AL62	Conform Street Drain
AL63	Conform Street Drain
AL64	Conform Street Drain
AL65	Conform Street Drain
AL66	Conform Street Drain
AL67	Conform Street Drain
AL68	Conform Street Drain
AL69	Conform Street Drain
AL70	Conform Street Drain
AL71	Conform Street Drain
AL72	Conform Street Drain
AL73	Conform Street Drain
AL74	Conform Street Drain
AL75	Conform Street Drain
AL76	Conform Street Drain
AL77	Conform Street Drain
AL78	Conform Street Drain
AL79	Conform Street Drain
AL80	Conform Street Drain
AL81	Conform Street Drain
AL82	Conform Street Drain
AL83	Conform Street Drain
AL84	Conform Street Drain
AL85	Conform Street Drain
AL86	Conform Street Drain
AL87	Conform Street Drain
AL88	Conform Street Drain
AL89	Conform Street Drain
AL90	Conform Street Drain
AL91	Conform Street Drain
AL92	Conform Street Drain
AL93	Conform Street Drain
AL94	Conform Street Drain
AL95	Conform Street Drain
AL96	Conform Street Drain
AL97	Conform Street Drain
AL98	Conform Street Drain
AL99	Conform Street Drain
AL100	Conform Street Drain

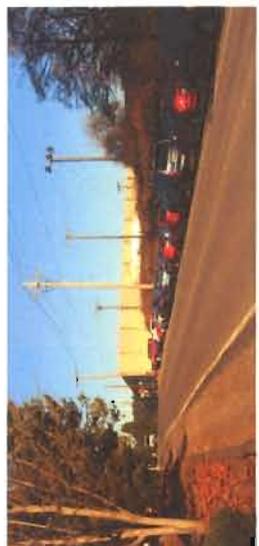




DATE	DESCRIPTION
08/15/13	DS
08/15/13	DS
08/15/13	DS



LOOKING EAST ALONG NE 12TH ST



LOOKING NORTH ALONG NE 120TH

AT INTERSECTION OF NE 12TH AND  
NE 120TH LOOKING EAST AT SITE



AT INTERSECTION OF NE 12TH AND  
NE 120TH LOOKING NORTH AT SITE



ON NE 124TH LOOKING @ SITE



AT INTERSECTION OF NE 120TH AND NE 12TH LOOKING NE @ SITE







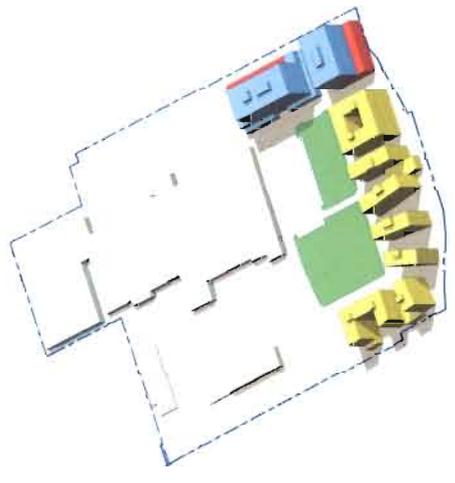
THE SPRING DISTRICT



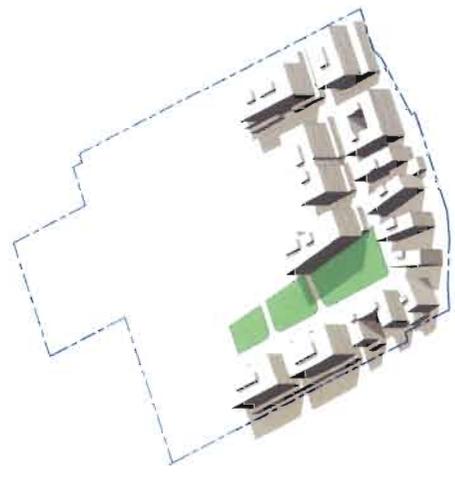
Master Development  
Plan : Conform Set

DATE	3/16/2012
AUTHOR	REV. 001
DESIGNER	1001164.1.3
TITLE	3/16/2012

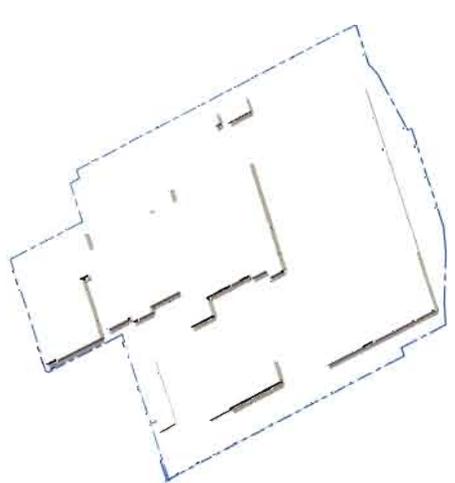
PHASING  
DIAGRAMS



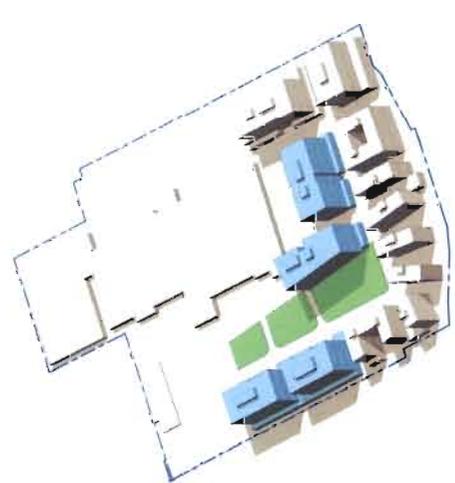
Phase 1



Phase 2



Phase 3



Phase 4



DATE	3/18/2015
PROJECT	THE SPRING DISTRICT
AUTHOR	MM/MS/13
DATE	3/18/2015
PROJECT	THE SPRING DISTRICT

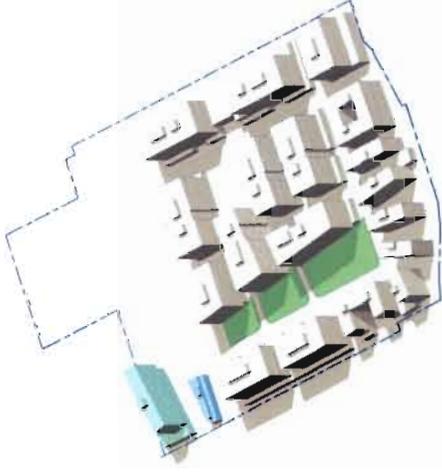


Figure 1

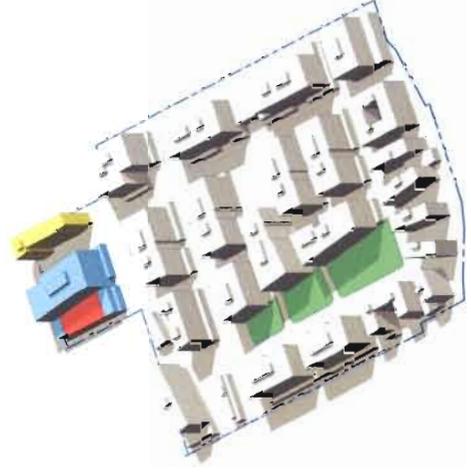


Figure 2

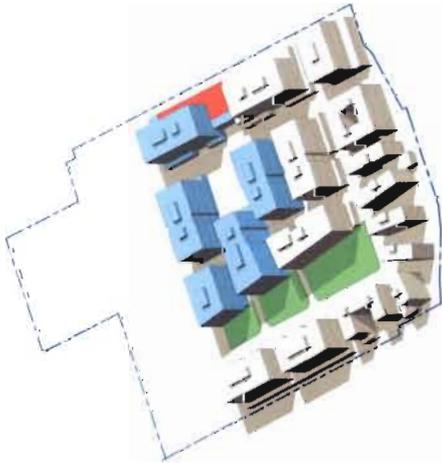


Figure 3

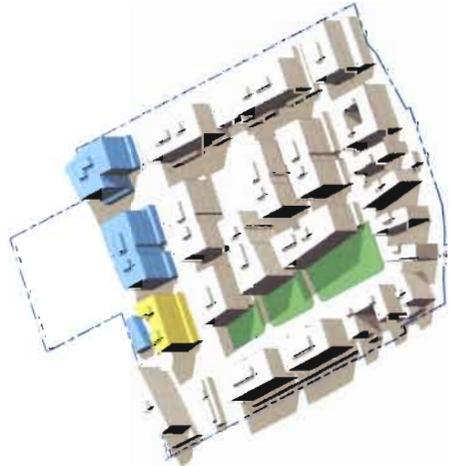
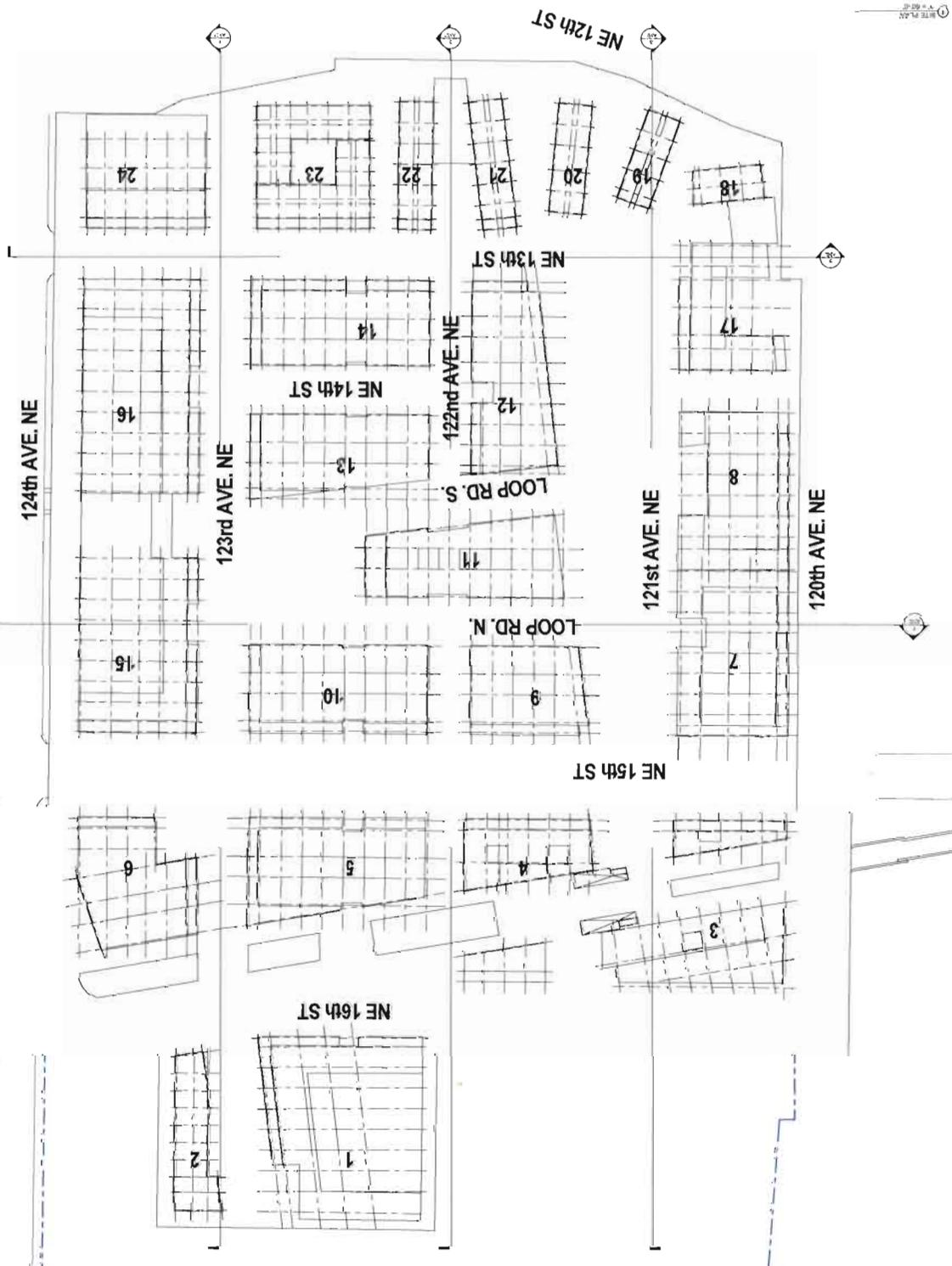


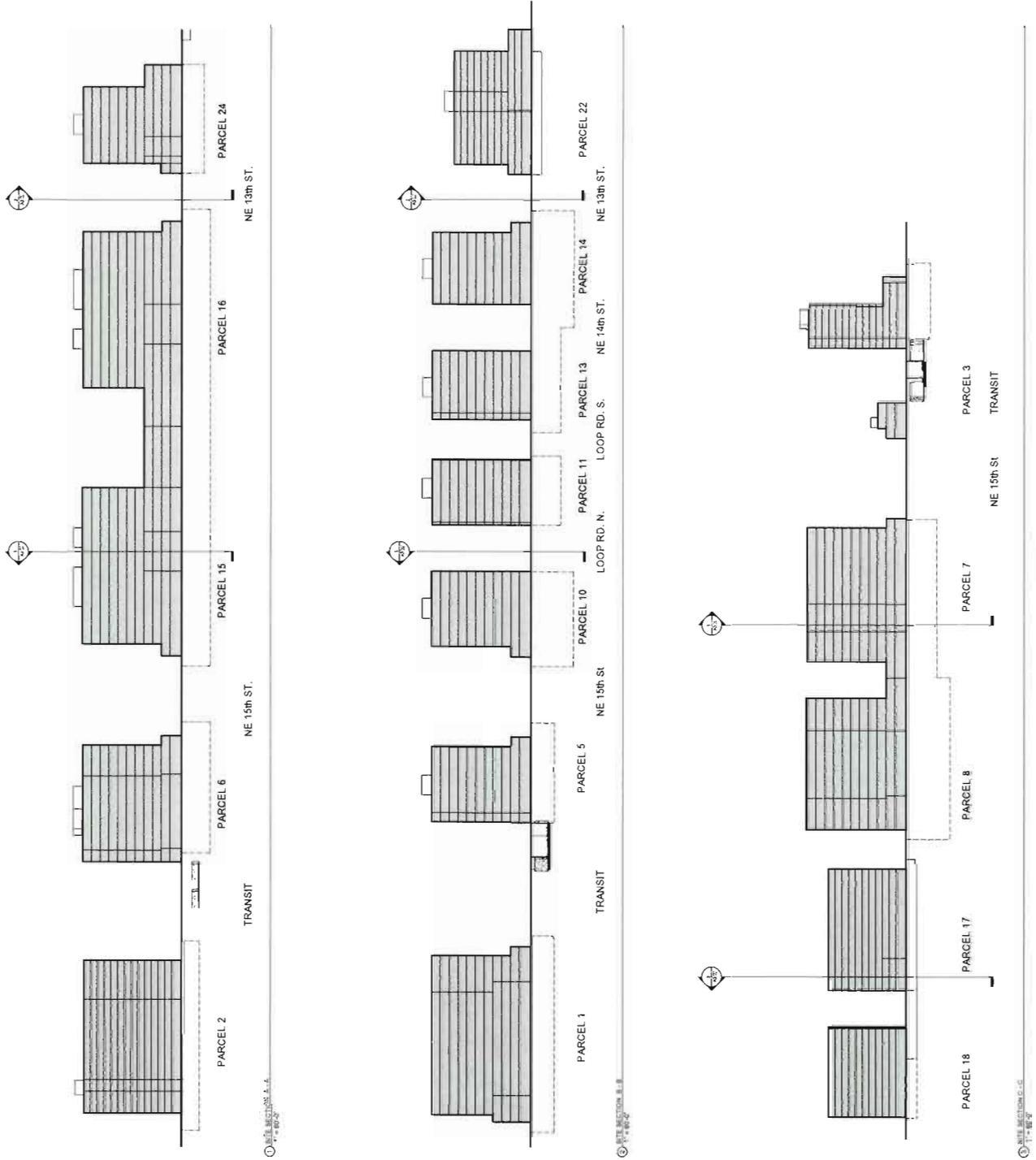
Figure 4

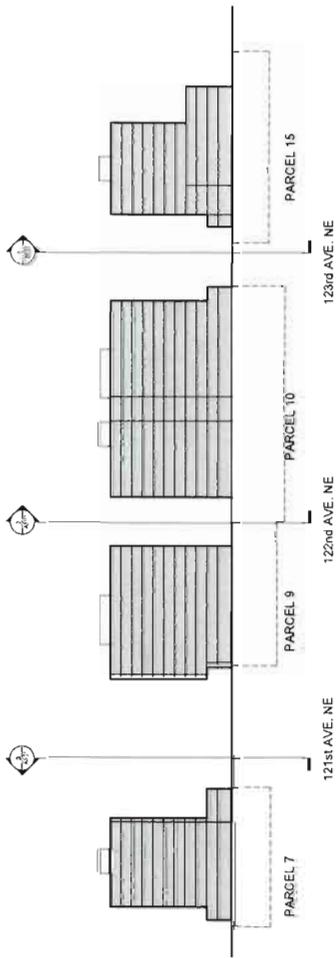




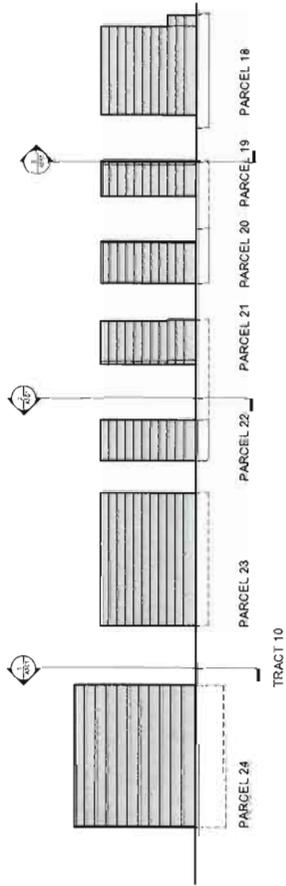


DATE: 3/16/2012	PROJECT: 100018413
AUTHOR: 3/16/2012	DESIGNER: 3/16/2012
DATE: 3/16/2012	DATE: 3/16/2012





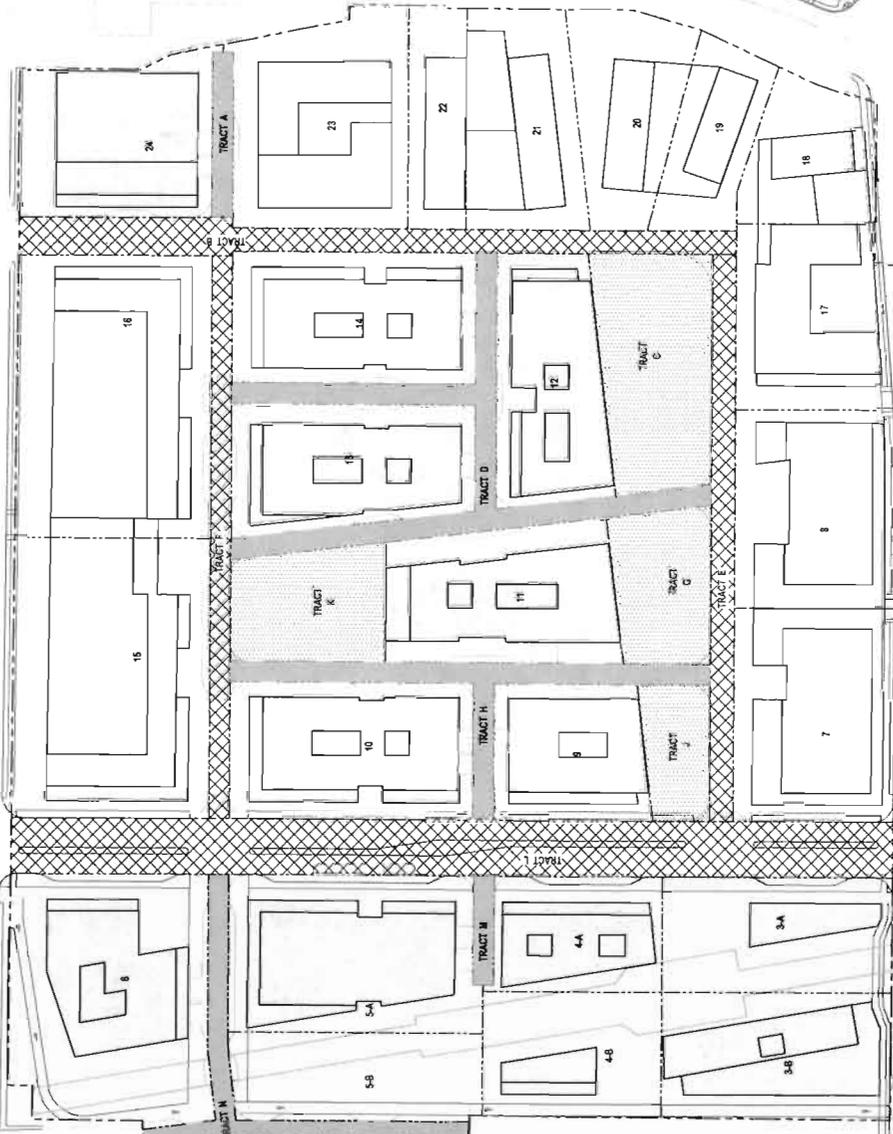
SECTION 2-D  
1" = 60'-0"



SECTION 3-E  
1" = 60'-0"



*Patent*

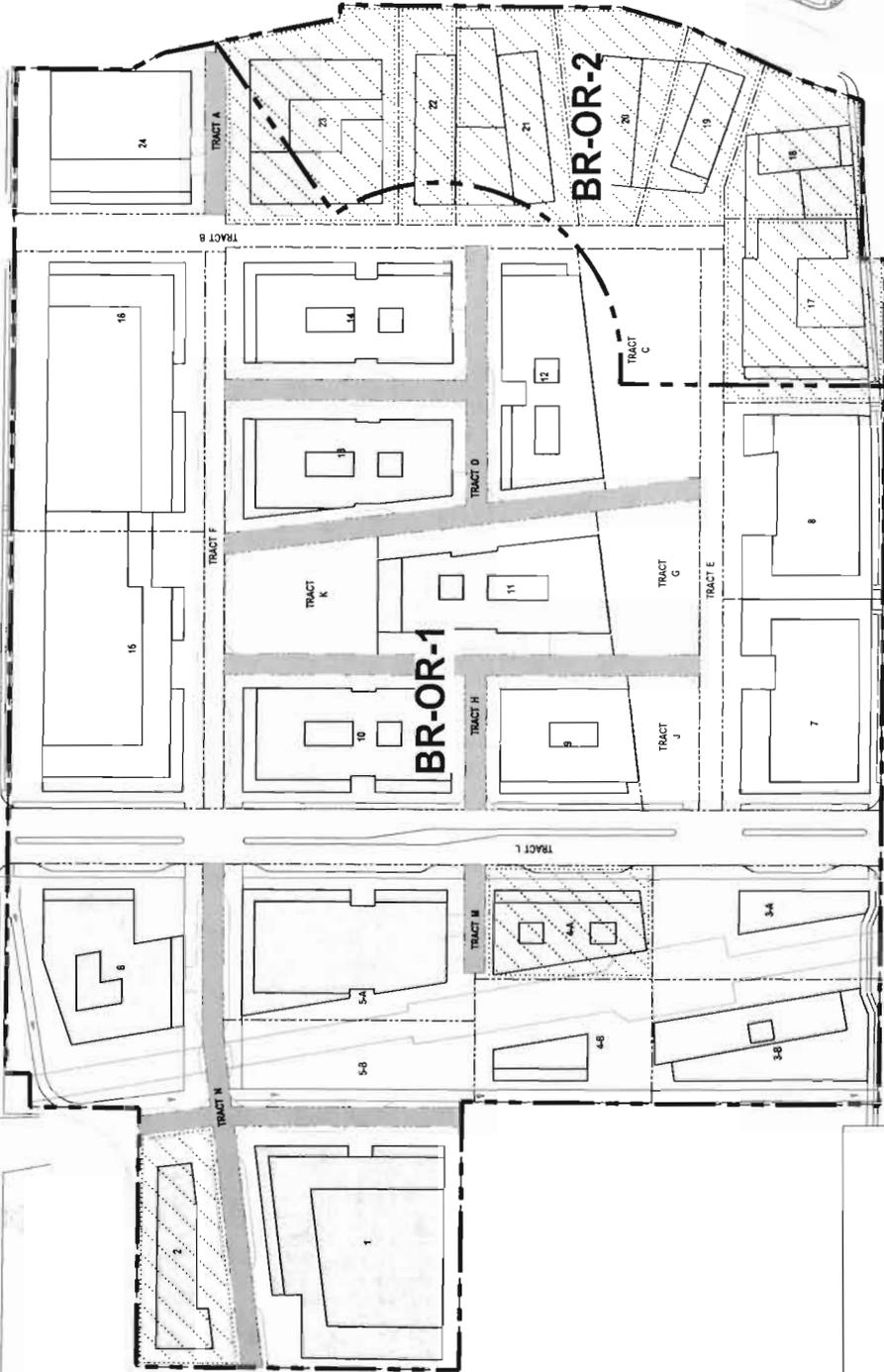


TRACT #	SQ. FT.	TRACT #	SQ. FT.
1	80,155	13	64,978
2	31,085	14	48,898
3	42,411	15	48,898
4	42,411	16	48,898
5	42,411	17	48,898
6	42,411	18	48,898
7	42,411	19	48,898
8	42,411	20	48,898
9	42,411	21	48,898
10	42,411	22	48,898
11	42,411	23	48,898
12	42,411	24	48,898
13	42,411	25	48,898
14	42,411	26	48,898

**LEGEND**  
 FUTURE PUBLIC ROADS  
 FUTURE PRIVATE ROADS



PROJECT NO.	2019-000022
DATE	02/16/2022
FILE NO.	10019613
DATE	02/16/2022

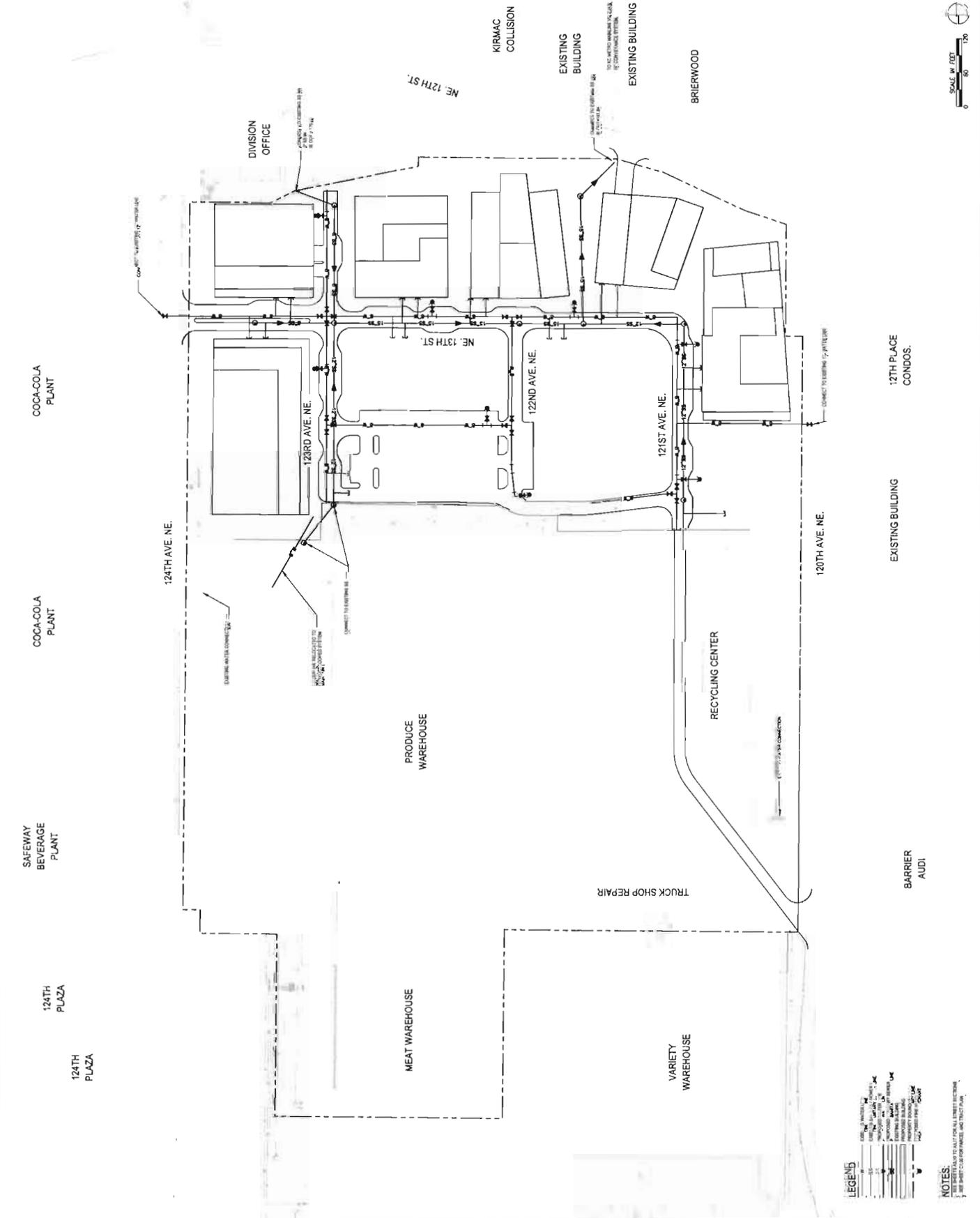


RESIDENTIALLY RESTRICTED PROPERTY  
(PARCEL NOS. 2, 4A, 17, 23)  
(AREA: 300,947 SQ. FT., 6.89 ACRES)

PROPERTY NOTE:  
RESTRICTED PROPERTY IS SUBJECT TO AGREEMENT  
BY APPLICANT SO LONG AS THE AGREEMENT IS CONSISTENT WITH THE  
DEVELOPMENT AGREEMENT, RECORDING NUMBER 2003861100239



DATE	BY	CHKD.
10/08/13		
DATE	BY	CHKD.
01/02/12		

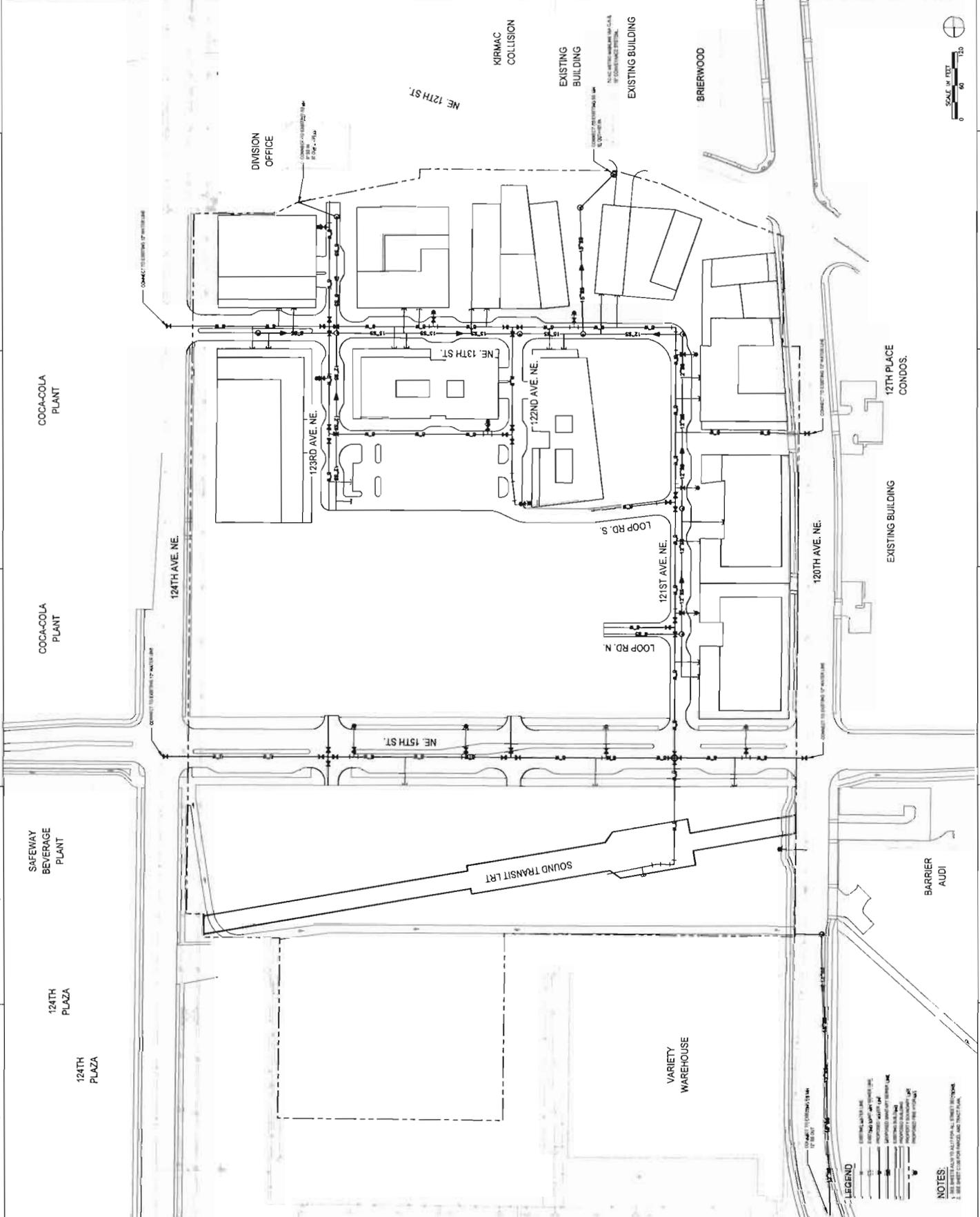


**LEGEND**

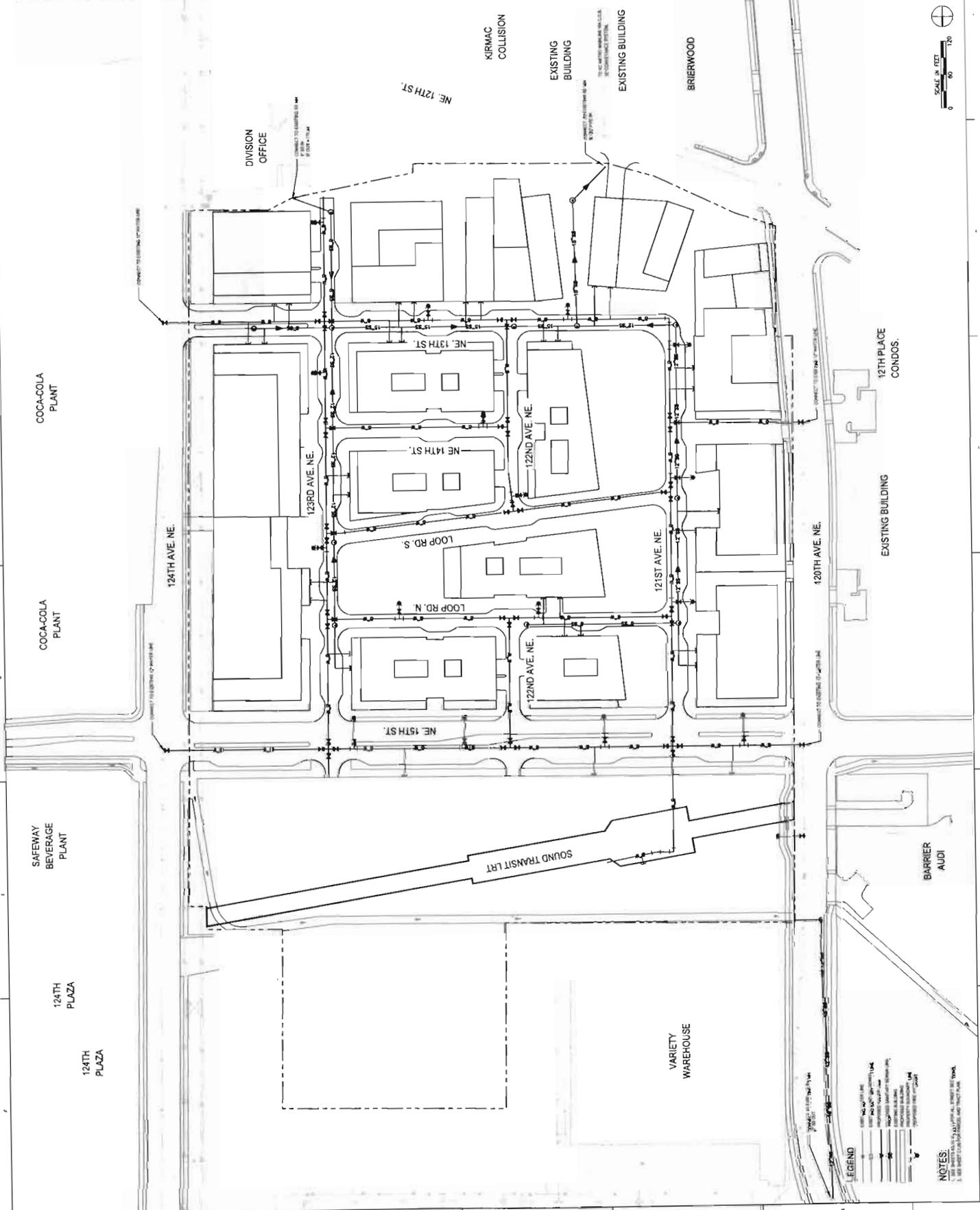
---	EXISTING UTILITY
---	EXISTING WATER CONNECTION
---	EXISTING SANITARY CONNECTION
---	EXISTING GAS CONNECTION
---	EXISTING FIBER OPTIC CONNECTION
---	EXISTING POWER CONNECTION
---	EXISTING TELEPHONE CONNECTION
---	EXISTING CABLE CONNECTION
---	EXISTING SLOTTED DRAINAGE
---	EXISTING CURB
---	EXISTING SIDEWALK
---	EXISTING DRIVEWAY
---	EXISTING DRIVE
---	EXISTING PAVEMENT
---	EXISTING ASPHALT
---	EXISTING CONCRETE
---	EXISTING GRAVEL
---	EXISTING SAND
---	EXISTING SOIL
---	EXISTING VEGETATION
---	EXISTING TREES
---	EXISTING SHRUBS
---	EXISTING GRASS
---	EXISTING MULCH
---	EXISTING COMPOST
---	EXISTING BIOMASS
---	EXISTING WASTE
---	EXISTING DEBRIS
---	EXISTING RUBBER
---	EXISTING GLASS
---	EXISTING METAL
---	EXISTING PLASTIC
---	EXISTING TEXTILE
---	EXISTING PAPER
---	EXISTING FOOD
---	EXISTING OIL
---	EXISTING FUEL
---	EXISTING CHEMICALS
---	EXISTING PHARMACEUTICALS
---	EXISTING COSMETICS
---	EXISTING TOYS
---	EXISTING SPORTS EQUIPMENT
---	EXISTING MUSICAL INSTRUMENTS
---	EXISTING ARTS SUPPLIES
---	EXISTING GARDEN SUPPLIES
---	EXISTING FISHING EQUIPMENT
---	EXISTING HUNTING EQUIPMENT
---	EXISTING CAMPING EQUIPMENT
---	EXISTING BOATING EQUIPMENT
---	EXISTING CYCLING EQUIPMENT
---	EXISTING SKIING EQUIPMENT
---	EXISTING SNOWBOARDING EQUIPMENT
---	EXISTING WATER SPORTS EQUIPMENT
---	EXISTING WINTER SPORTS EQUIPMENT
---	EXISTING SUMMER SPORTS EQUIPMENT
---	EXISTING RECREATION EQUIPMENT
---	EXISTING EDUCATIONAL EQUIPMENT
---	EXISTING HEALTH CARE EQUIPMENT
---	EXISTING PERSONAL CARE EQUIPMENT
---	EXISTING HOME CARE EQUIPMENT
---	EXISTING BUSINESS EQUIPMENT
---	EXISTING INDUSTRIAL EQUIPMENT
---	EXISTING AGRICULTURAL EQUIPMENT
---	EXISTING CONSTRUCTION EQUIPMENT
---	EXISTING MAINTENANCE EQUIPMENT
---	EXISTING REPAIR EQUIPMENT
---	EXISTING STORAGE EQUIPMENT
---	EXISTING TRANSPORTATION EQUIPMENT
---	EXISTING COMMUNICATIONS EQUIPMENT
---	EXISTING SECURITY EQUIPMENT
---	EXISTING DEFENSE EQUIPMENT
---	EXISTING AEROSPACE EQUIPMENT
---	EXISTING MARINE EQUIPMENT
---	EXISTING AVIATION EQUIPMENT
---	EXISTING SPACE EQUIPMENT
---	EXISTING NUCLEAR EQUIPMENT
---	EXISTING BIOTECHNOLOGY EQUIPMENT
---	EXISTING NANOTECHNOLOGY EQUIPMENT
---	EXISTING NANOMATERIALS EQUIPMENT
---	EXISTING NANOELECTRONICS EQUIPMENT
---	EXISTING NANOMEDICINE EQUIPMENT
---	EXISTING NANOBIOLOGY EQUIPMENT
---	EXISTING NANOCHEMISTRY EQUIPMENT
---	EXISTING NANOPHYSICS EQUIPMENT
---	EXISTING NANOMECHANICS EQUIPMENT
---	EXISTING NANOPTICS EQUIPMENT
---	EXISTING NANOTECHNOLOGY EQUIPMENT

**NOTES:**  
1. SEE SHEET 120 FOR PHASE 1A DEVELOPMENT PLAN.  
2. SEE SHEET 121 FOR PHASE 1B DEVELOPMENT PLAN.  
3. SEE SHEET 122 FOR PHASE 1C DEVELOPMENT PLAN.





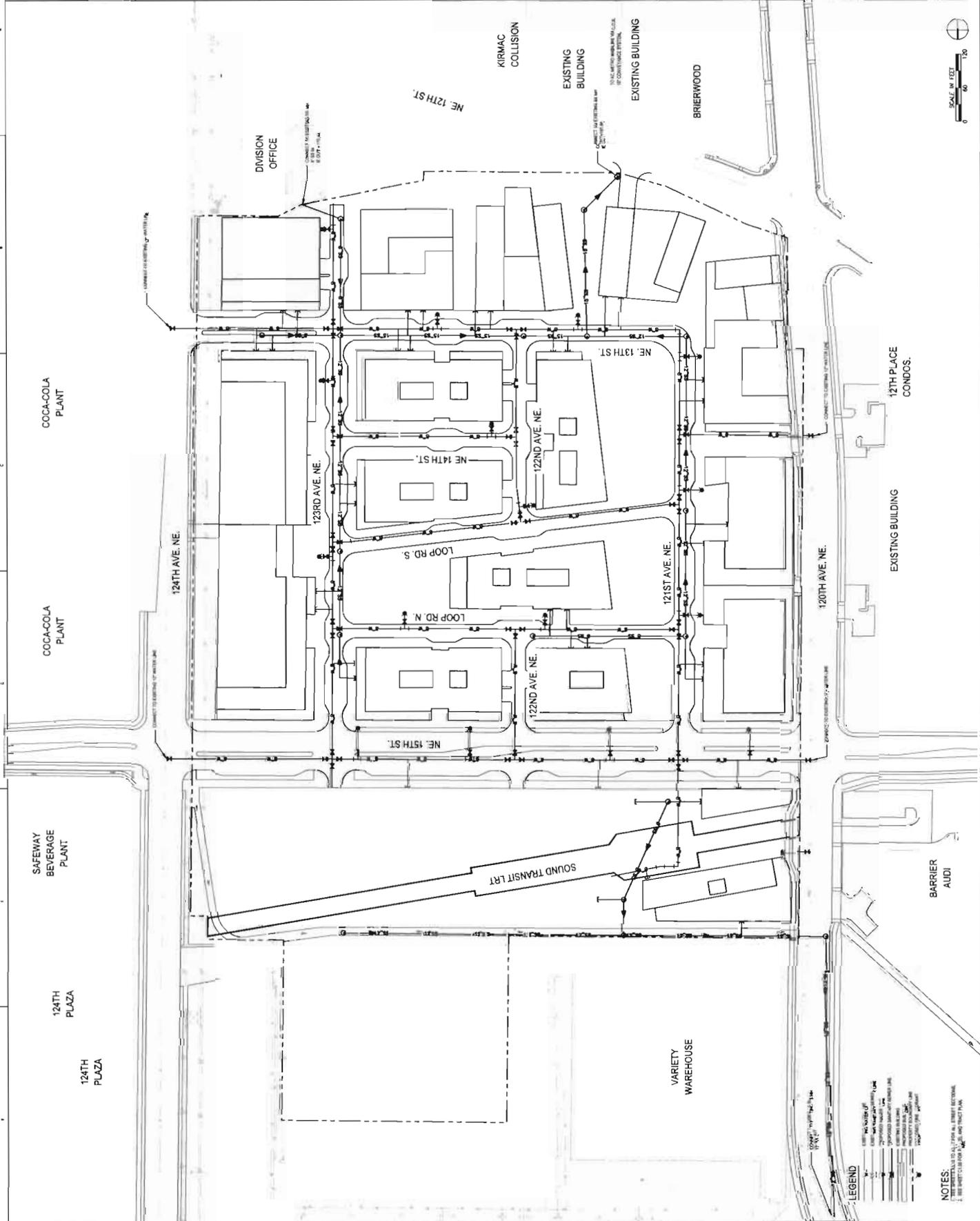
- LEGEND**
- 1. EXISTING BUILDING
  - 2. EXISTING DRIVEWAY
  - 3. EXISTING DRIVEWAY
  - 4. EXISTING DRIVEWAY
  - 5. EXISTING DRIVEWAY
  - 6. EXISTING DRIVEWAY
  - 7. EXISTING DRIVEWAY
  - 8. EXISTING DRIVEWAY
  - 9. EXISTING DRIVEWAY
  - 10. EXISTING DRIVEWAY
  - 11. EXISTING DRIVEWAY
  - 12. EXISTING DRIVEWAY
  - 13. EXISTING DRIVEWAY
  - 14. EXISTING DRIVEWAY
  - 15. EXISTING DRIVEWAY
  - 16. EXISTING DRIVEWAY
  - 17. EXISTING DRIVEWAY
  - 18. EXISTING DRIVEWAY
  - 19. EXISTING DRIVEWAY
  - 20. EXISTING DRIVEWAY
  - 21. EXISTING DRIVEWAY
  - 22. EXISTING DRIVEWAY
  - 23. EXISTING DRIVEWAY
  - 24. EXISTING DRIVEWAY
  - 25. EXISTING DRIVEWAY
  - 26. EXISTING DRIVEWAY
  - 27. EXISTING DRIVEWAY
  - 28. EXISTING DRIVEWAY
  - 29. EXISTING DRIVEWAY
  - 30. EXISTING DRIVEWAY
  - 31. EXISTING DRIVEWAY
  - 32. EXISTING DRIVEWAY
  - 33. EXISTING DRIVEWAY
  - 34. EXISTING DRIVEWAY
  - 35. EXISTING DRIVEWAY
  - 36. EXISTING DRIVEWAY
  - 37. EXISTING DRIVEWAY
  - 38. EXISTING DRIVEWAY
  - 39. EXISTING DRIVEWAY
  - 40. EXISTING DRIVEWAY
  - 41. EXISTING DRIVEWAY
  - 42. EXISTING DRIVEWAY
  - 43. EXISTING DRIVEWAY
  - 44. EXISTING DRIVEWAY
  - 45. EXISTING DRIVEWAY
  - 46. EXISTING DRIVEWAY
  - 47. EXISTING DRIVEWAY
  - 48. EXISTING DRIVEWAY
  - 49. EXISTING DRIVEWAY
  - 50. EXISTING DRIVEWAY
  - 51. EXISTING DRIVEWAY
  - 52. EXISTING DRIVEWAY
  - 53. EXISTING DRIVEWAY
  - 54. EXISTING DRIVEWAY
  - 55. EXISTING DRIVEWAY
  - 56. EXISTING DRIVEWAY
  - 57. EXISTING DRIVEWAY
  - 58. EXISTING DRIVEWAY
  - 59. EXISTING DRIVEWAY
  - 60. EXISTING DRIVEWAY
  - 61. EXISTING DRIVEWAY
  - 62. EXISTING DRIVEWAY
  - 63. EXISTING DRIVEWAY
  - 64. EXISTING DRIVEWAY
  - 65. EXISTING DRIVEWAY
  - 66. EXISTING DRIVEWAY
  - 67. EXISTING DRIVEWAY
  - 68. EXISTING DRIVEWAY
  - 69. EXISTING DRIVEWAY
  - 70. EXISTING DRIVEWAY
  - 71. EXISTING DRIVEWAY
  - 72. EXISTING DRIVEWAY
  - 73. EXISTING DRIVEWAY
  - 74. EXISTING DRIVEWAY
  - 75. EXISTING DRIVEWAY
  - 76. EXISTING DRIVEWAY
  - 77. EXISTING DRIVEWAY
  - 78. EXISTING DRIVEWAY
  - 79. EXISTING DRIVEWAY
  - 80. EXISTING DRIVEWAY
  - 81. EXISTING DRIVEWAY
  - 82. EXISTING DRIVEWAY
  - 83. EXISTING DRIVEWAY
  - 84. EXISTING DRIVEWAY
  - 85. EXISTING DRIVEWAY
  - 86. EXISTING DRIVEWAY
  - 87. EXISTING DRIVEWAY
  - 88. EXISTING DRIVEWAY
  - 89. EXISTING DRIVEWAY
  - 90. EXISTING DRIVEWAY
  - 91. EXISTING DRIVEWAY
  - 92. EXISTING DRIVEWAY
  - 93. EXISTING DRIVEWAY
  - 94. EXISTING DRIVEWAY
  - 95. EXISTING DRIVEWAY
  - 96. EXISTING DRIVEWAY
  - 97. EXISTING DRIVEWAY
  - 98. EXISTING DRIVEWAY
  - 99. EXISTING DRIVEWAY
  - 100. EXISTING DRIVEWAY
- NOTES**
1. SEE SHEET C2.01 FOR ALL OTHER NOTES.
  2. SEE SHEET C2.02 FOR ALL OTHER NOTES.
  3. SEE SHEET C2.03 FOR ALL OTHER NOTES.
  4. SEE SHEET C2.04 FOR ALL OTHER NOTES.
  5. SEE SHEET C2.05 FOR ALL OTHER NOTES.
  6. SEE SHEET C2.06 FOR ALL OTHER NOTES.
  7. SEE SHEET C2.07 FOR ALL OTHER NOTES.
  8. SEE SHEET C2.08 FOR ALL OTHER NOTES.
  9. SEE SHEET C2.09 FOR ALL OTHER NOTES.
  10. SEE SHEET C2.10 FOR ALL OTHER NOTES.
  11. SEE SHEET C2.11 FOR ALL OTHER NOTES.
  12. SEE SHEET C2.12 FOR ALL OTHER NOTES.
  13. SEE SHEET C2.13 FOR ALL OTHER NOTES.
  14. SEE SHEET C2.14 FOR ALL OTHER NOTES.
  15. SEE SHEET C2.15 FOR ALL OTHER NOTES.
  16. SEE SHEET C2.16 FOR ALL OTHER NOTES.
  17. SEE SHEET C2.17 FOR ALL OTHER NOTES.
  18. SEE SHEET C2.18 FOR ALL OTHER NOTES.
  19. SEE SHEET C2.19 FOR ALL OTHER NOTES.
  20. SEE SHEET C2.20 FOR ALL OTHER NOTES.
  21. SEE SHEET C2.21 FOR ALL OTHER NOTES.
  22. SEE SHEET C2.22 FOR ALL OTHER NOTES.
  23. SEE SHEET C2.23 FOR ALL OTHER NOTES.
  24. SEE SHEET C2.24 FOR ALL OTHER NOTES.
  25. SEE SHEET C2.25 FOR ALL OTHER NOTES.
  26. SEE SHEET C2.26 FOR ALL OTHER NOTES.
  27. SEE SHEET C2.27 FOR ALL OTHER NOTES.
  28. SEE SHEET C2.28 FOR ALL OTHER NOTES.
  29. SEE SHEET C2.29 FOR ALL OTHER NOTES.
  30. SEE SHEET C2.30 FOR ALL OTHER NOTES.
  31. SEE SHEET C2.31 FOR ALL OTHER NOTES.
  32. SEE SHEET C2.32 FOR ALL OTHER NOTES.
  33. SEE SHEET C2.33 FOR ALL OTHER NOTES.
  34. SEE SHEET C2.34 FOR ALL OTHER NOTES.
  35. SEE SHEET C2.35 FOR ALL OTHER NOTES.
  36. SEE SHEET C2.36 FOR ALL OTHER NOTES.
  37. SEE SHEET C2.37 FOR ALL OTHER NOTES.
  38. SEE SHEET C2.38 FOR ALL OTHER NOTES.
  39. SEE SHEET C2.39 FOR ALL OTHER NOTES.
  40. SEE SHEET C2.40 FOR ALL OTHER NOTES.
  41. SEE SHEET C2.41 FOR ALL OTHER NOTES.
  42. SEE SHEET C2.42 FOR ALL OTHER NOTES.
  43. SEE SHEET C2.43 FOR ALL OTHER NOTES.
  44. SEE SHEET C2.44 FOR ALL OTHER NOTES.
  45. SEE SHEET C2.45 FOR ALL OTHER NOTES.
  46. SEE SHEET C2.46 FOR ALL OTHER NOTES.
  47. SEE SHEET C2.47 FOR ALL OTHER NOTES.
  48. SEE SHEET C2.48 FOR ALL OTHER NOTES.
  49. SEE SHEET C2.49 FOR ALL OTHER NOTES.
  50. SEE SHEET C2.50 FOR ALL OTHER NOTES.
  51. SEE SHEET C2.51 FOR ALL OTHER NOTES.
  52. SEE SHEET C2.52 FOR ALL OTHER NOTES.
  53. SEE SHEET C2.53 FOR ALL OTHER NOTES.
  54. SEE SHEET C2.54 FOR ALL OTHER NOTES.
  55. SEE SHEET C2.55 FOR ALL OTHER NOTES.
  56. SEE SHEET C2.56 FOR ALL OTHER NOTES.
  57. SEE SHEET C2.57 FOR ALL OTHER NOTES.
  58. SEE SHEET C2.58 FOR ALL OTHER NOTES.
  59. SEE SHEET C2.59 FOR ALL OTHER NOTES.
  60. SEE SHEET C2.60 FOR ALL OTHER NOTES.
  61. SEE SHEET C2.61 FOR ALL OTHER NOTES.
  62. SEE SHEET C2.62 FOR ALL OTHER NOTES.
  63. SEE SHEET C2.63 FOR ALL OTHER NOTES.
  64. SEE SHEET C2.64 FOR ALL OTHER NOTES.
  65. SEE SHEET C2.65 FOR ALL OTHER NOTES.
  66. SEE SHEET C2.66 FOR ALL OTHER NOTES.
  67. SEE SHEET C2.67 FOR ALL OTHER NOTES.
  68. SEE SHEET C2.68 FOR ALL OTHER NOTES.
  69. SEE SHEET C2.69 FOR ALL OTHER NOTES.
  70. SEE SHEET C2.70 FOR ALL OTHER NOTES.
  71. SEE SHEET C2.71 FOR ALL OTHER NOTES.
  72. SEE SHEET C2.72 FOR ALL OTHER NOTES.
  73. SEE SHEET C2.73 FOR ALL OTHER NOTES.
  74. SEE SHEET C2.74 FOR ALL OTHER NOTES.
  75. SEE SHEET C2.75 FOR ALL OTHER NOTES.
  76. SEE SHEET C2.76 FOR ALL OTHER NOTES.
  77. SEE SHEET C2.77 FOR ALL OTHER NOTES.
  78. SEE SHEET C2.78 FOR ALL OTHER NOTES.
  79. SEE SHEET C2.79 FOR ALL OTHER NOTES.
  80. SEE SHEET C2.80 FOR ALL OTHER NOTES.
  81. SEE SHEET C2.81 FOR ALL OTHER NOTES.
  82. SEE SHEET C2.82 FOR ALL OTHER NOTES.
  83. SEE SHEET C2.83 FOR ALL OTHER NOTES.
  84. SEE SHEET C2.84 FOR ALL OTHER NOTES.
  85. SEE SHEET C2.85 FOR ALL OTHER NOTES.
  86. SEE SHEET C2.86 FOR ALL OTHER NOTES.
  87. SEE SHEET C2.87 FOR ALL OTHER NOTES.
  88. SEE SHEET C2.88 FOR ALL OTHER NOTES.
  89. SEE SHEET C2.89 FOR ALL OTHER NOTES.
  90. SEE SHEET C2.90 FOR ALL OTHER NOTES.
  91. SEE SHEET C2.91 FOR ALL OTHER NOTES.
  92. SEE SHEET C2.92 FOR ALL OTHER NOTES.
  93. SEE SHEET C2.93 FOR ALL OTHER NOTES.
  94. SEE SHEET C2.94 FOR ALL OTHER NOTES.
  95. SEE SHEET C2.95 FOR ALL OTHER NOTES.
  96. SEE SHEET C2.96 FOR ALL OTHER NOTES.
  97. SEE SHEET C2.97 FOR ALL OTHER NOTES.
  98. SEE SHEET C2.98 FOR ALL OTHER NOTES.
  99. SEE SHEET C2.99 FOR ALL OTHER NOTES.
  100. SEE SHEET C2.100 FOR ALL OTHER NOTES.



- LEGEND**
- 10' WIDE SIDEWALK
  - 15' WIDE SIDEWALK
  - 20' WIDE SIDEWALK
  - 25' WIDE SIDEWALK
  - 30' WIDE SIDEWALK
  - 35' WIDE SIDEWALK
  - 40' WIDE SIDEWALK
  - 45' WIDE SIDEWALK
  - 50' WIDE SIDEWALK
  - 55' WIDE SIDEWALK
  - 60' WIDE SIDEWALK
  - 65' WIDE SIDEWALK
  - 70' WIDE SIDEWALK
  - 75' WIDE SIDEWALK
  - 80' WIDE SIDEWALK
  - 85' WIDE SIDEWALK
  - 90' WIDE SIDEWALK
  - 95' WIDE SIDEWALK
  - 100' WIDE SIDEWALK
- NOTES:**
1. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.
  2. SEE SHEET C2.04 FOR PHASE 3 AND TRACT PLAN.



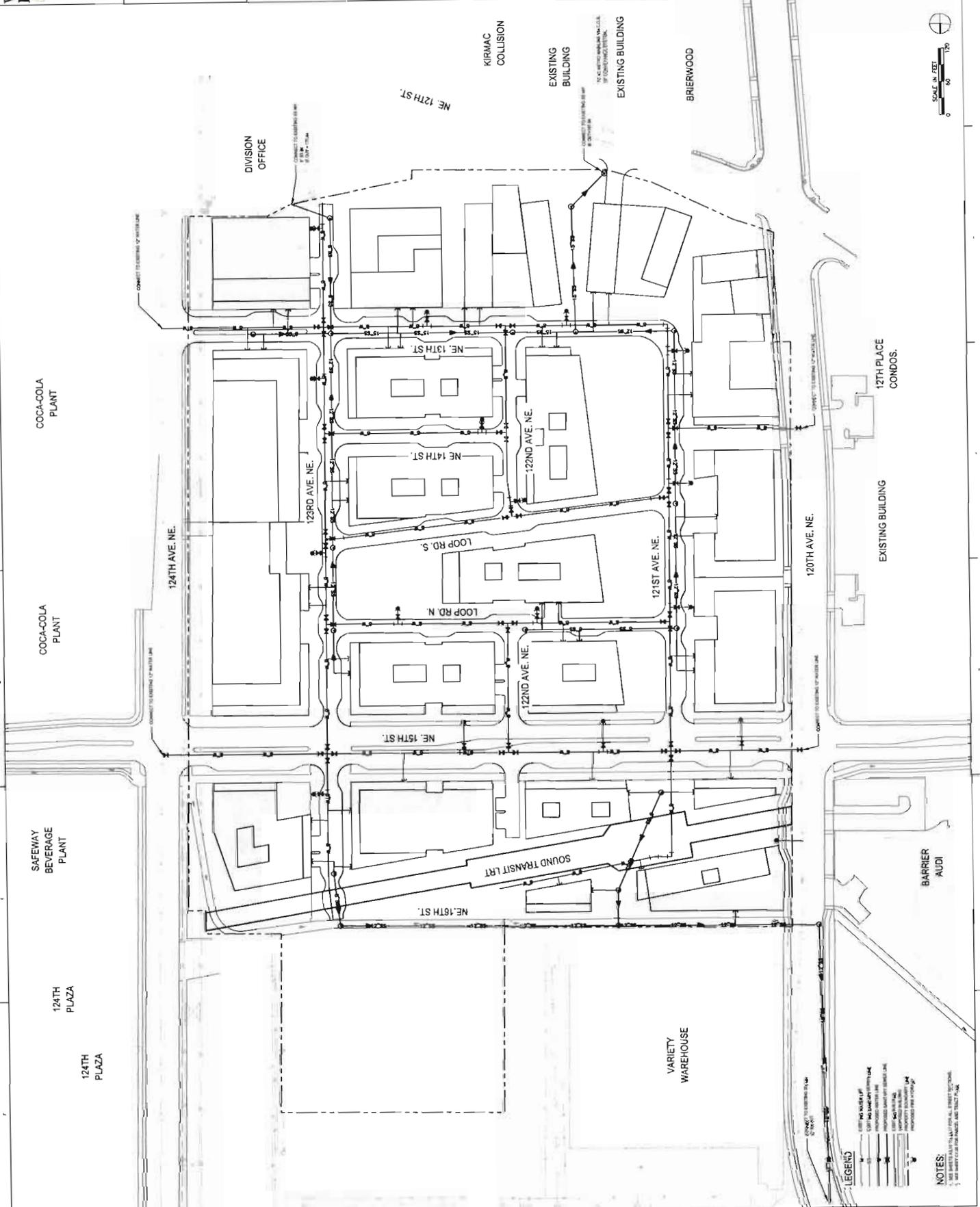
DATE:	3/16/2012
DESIGNER:	S.J.L.
CHECKER:	C.R.
PROJECT NO.:	10018413
CLIENT:	SPURTEC



**LEGEND**

---	EXISTING UTILITY LINE
---	EXISTING WATER MAIN
---	EXISTING SANITARY MAIN
---	EXISTING GAS MAIN
---	EXISTING FIBER OPTIC
---	EXISTING TELEPHONE
---	EXISTING CABLE TV
---	EXISTING POWER
---	EXISTING SEWER
---	EXISTING STORM SEWER
---	EXISTING RAILROAD
---	EXISTING HIGHWAY
---	EXISTING AIRPORT
---	EXISTING CANAL
---	EXISTING DRAINAGE
---	EXISTING FLOOD CONTROL
---	EXISTING OTHER

**NOTES:**  
 1. SEE SHEET C2.01 FOR ALL EXISTING UTILITIES.  
 2. SEE SHEET C2.02 FOR ALL EXISTING BUILDINGS.



**LEGEND**

- EXISTING BUILDING FOOTPRINT
- PROPOSED BUILDING FOOTPRINT
- PROPOSED DRIVEWAY

**NOTES**

- SEE SHEET C2.04 FOR PHASE 4 AND PHASE 5.





Wright Runstad & Company  
1201 3rd Avenue  
Suite 2700  
Seattle, WA 98101

WRIGHT  
RUNSTAD  
& COMPANY



Shorenstein Properties LLC  
235 Montgomery St.  
16th Floor  
San Francisco, CA 94104



NBBJ  
223 Yale Avenue North  
Seattle, WA 98109

Parametrix

Parametrix  
1019 39th Avenue SE  
Suite 100  
Puyallup, WA 98374

TSI

Transportation Solutions, Inc.  
8250 165th Avenue NE  
Suite 100  
Redmond, WA 98052

THE SPRING DISTRICT



Master Development  
Plan - Conform Set

3/16/2012

Project: 1050000  
Author: JRM  
Date: 10/08/11  
Scale: 3/4"=1'-0"

COVER and  
DRAWING INDEX

Sheet  
A0.01c

DRAWING INDEX. PRELIMINARY GRADING & DRAINAGE

Code	Description
AB.01	Cover and Sheet Index
AB.02	Utility Map
AB.03	Existing Site Plan
AB.04	Existing Site Photographs
AB.05	Existing Site Photographs
AB.06	Existing Site Photographs
AB.07	Existing Site Photographs
AB.08	Existing Site Photographs
AB.09	Existing Site Photographs
AB.10	Existing Site Photographs
AB.11	Existing Site Photographs
AB.12	Existing Site Photographs
AB.13	Existing Site Photographs
AB.14	Existing Site Photographs
AB.15	Existing Site Photographs
AB.16	Existing Site Photographs
AB.17	Existing Site Photographs
AB.18	Existing Site Photographs
AB.19	Existing Site Photographs
AB.20	Existing Site Photographs
AB.21	Existing Site Photographs
AB.22	Existing Site Photographs
AB.23	Existing Site Photographs
AB.24	Existing Site Photographs
AB.25	Existing Site Photographs
AB.26	Existing Site Photographs
AB.27	Existing Site Photographs
AB.28	Existing Site Photographs
AB.29	Existing Site Photographs
AB.30	Existing Site Photographs
AB.31	Existing Site Photographs
AB.32	Existing Site Photographs
AB.33	Existing Site Photographs
AB.34	Existing Site Photographs
AB.35	Existing Site Photographs
AB.36	Existing Site Photographs
AB.37	Existing Site Photographs
AB.38	Existing Site Photographs
AB.39	Existing Site Photographs
AB.40	Existing Site Photographs
AB.41	Existing Site Photographs
AB.42	Existing Site Photographs
AB.43	Existing Site Photographs
AB.44	Existing Site Photographs
AB.45	Existing Site Photographs
AB.46	Existing Site Photographs
AB.47	Existing Site Photographs
AB.48	Existing Site Photographs
AB.49	Existing Site Photographs
AB.50	Existing Site Photographs
AB.51	Existing Site Photographs
AB.52	Existing Site Photographs
AB.53	Existing Site Photographs
AB.54	Existing Site Photographs
AB.55	Existing Site Photographs
AB.56	Existing Site Photographs
AB.57	Existing Site Photographs
AB.58	Existing Site Photographs
AB.59	Existing Site Photographs
AB.60	Existing Site Photographs
AB.61	Existing Site Photographs
AB.62	Existing Site Photographs
AB.63	Existing Site Photographs
AB.64	Existing Site Photographs
AB.65	Existing Site Photographs
AB.66	Existing Site Photographs
AB.67	Existing Site Photographs
AB.68	Existing Site Photographs
AB.69	Existing Site Photographs
AB.70	Existing Site Photographs
AB.71	Existing Site Photographs
AB.72	Existing Site Photographs
AB.73	Existing Site Photographs
AB.74	Existing Site Photographs
AB.75	Existing Site Photographs
AB.76	Existing Site Photographs
AB.77	Existing Site Photographs
AB.78	Existing Site Photographs
AB.79	Existing Site Photographs
AB.80	Existing Site Photographs
AB.81	Existing Site Photographs
AB.82	Existing Site Photographs
AB.83	Existing Site Photographs
AB.84	Existing Site Photographs
AB.85	Existing Site Photographs
AB.86	Existing Site Photographs
AB.87	Existing Site Photographs
AB.88	Existing Site Photographs
AB.89	Existing Site Photographs
AB.90	Existing Site Photographs
AB.91	Existing Site Photographs
AB.92	Existing Site Photographs
AB.93	Existing Site Photographs
AB.94	Existing Site Photographs
AB.95	Existing Site Photographs
AB.96	Existing Site Photographs
AB.97	Existing Site Photographs
AB.98	Existing Site Photographs
AB.99	Existing Site Photographs
AB.100	Existing Site Photographs







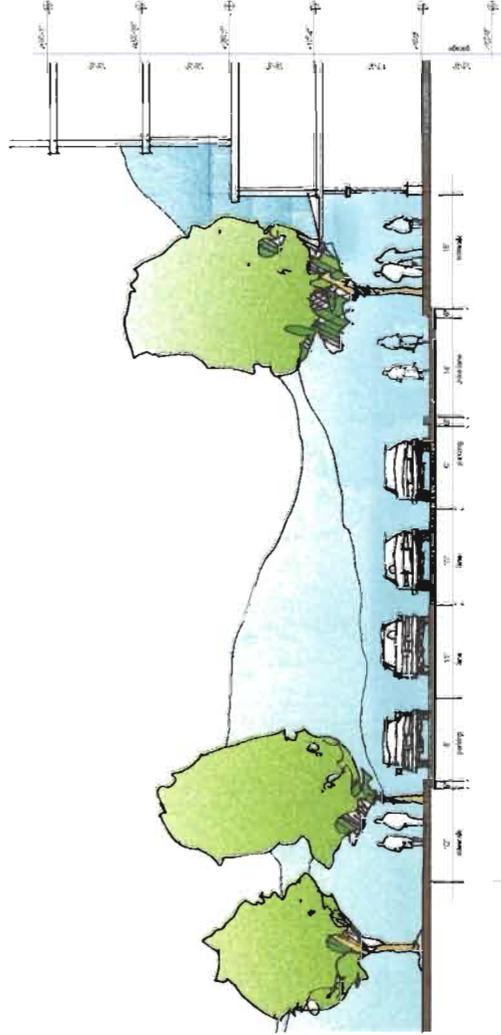




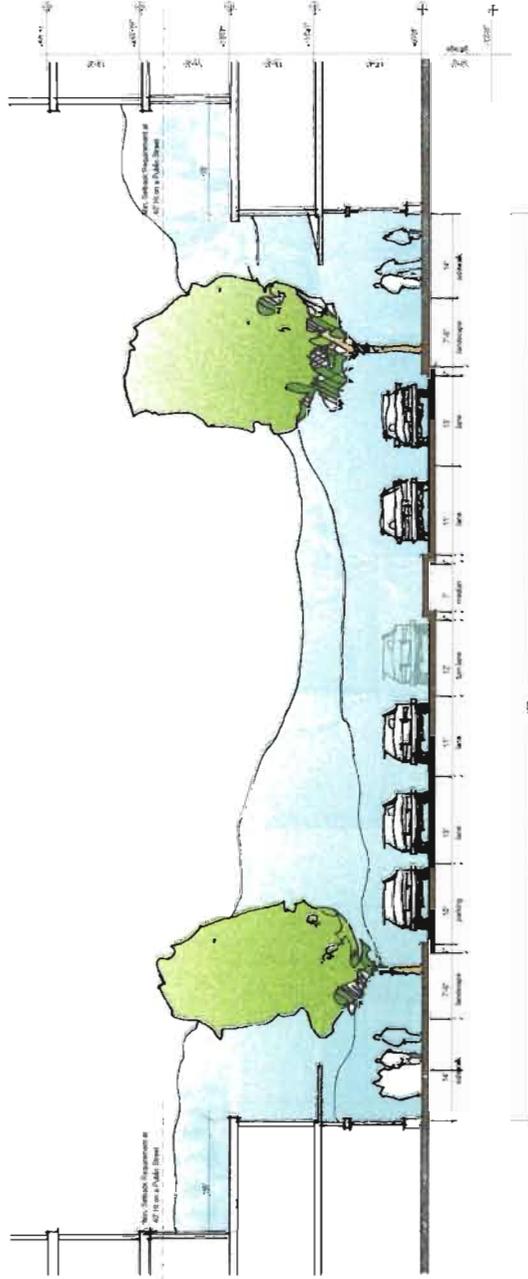








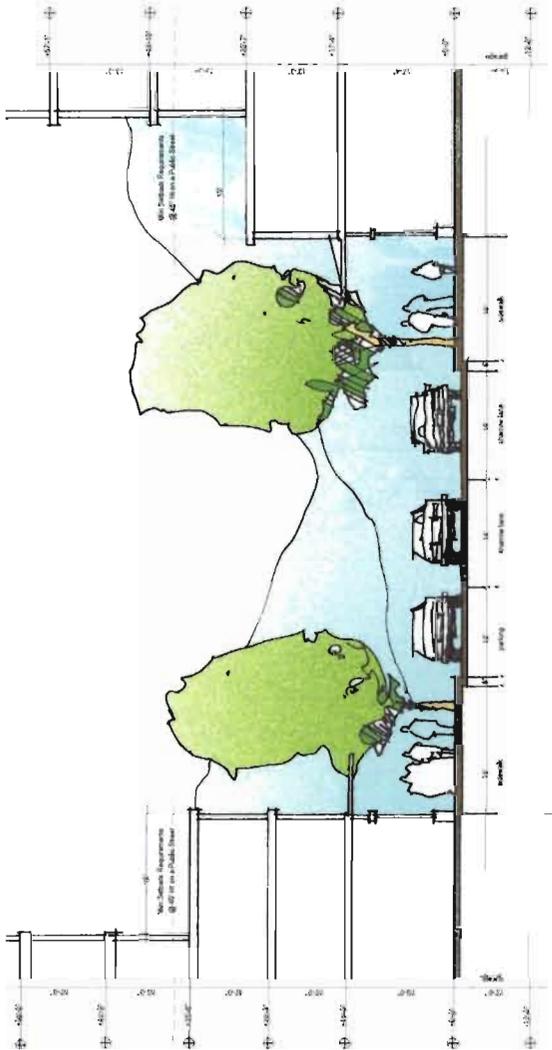
NE 16th Street / Bike Lane



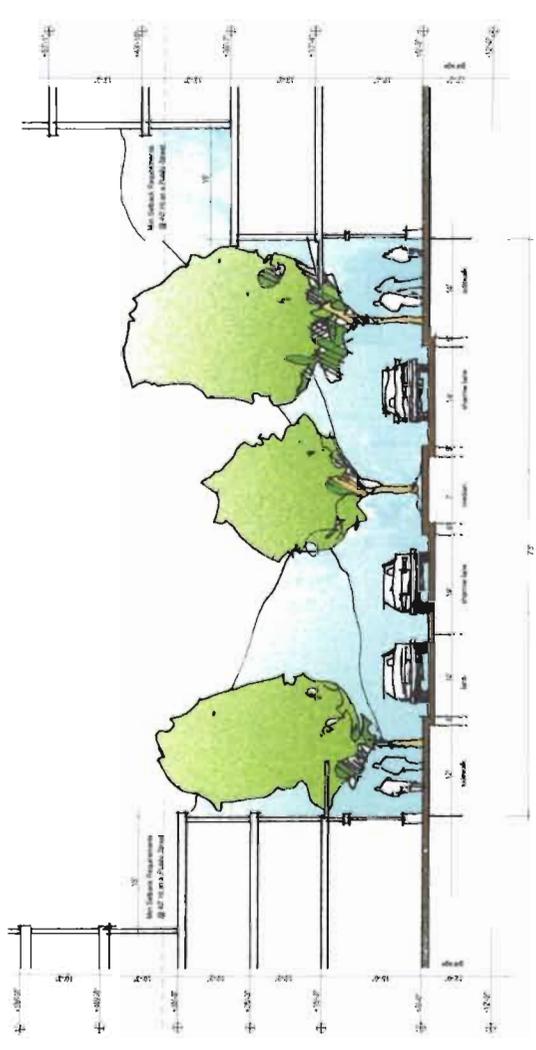
NE 15th Street / BelRed Corridor





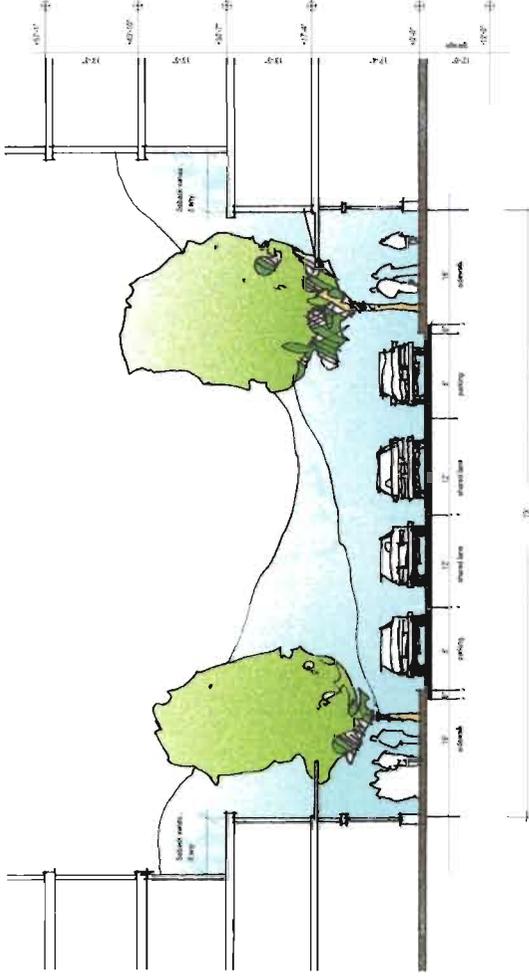


NE 13th Street / Residential Edge



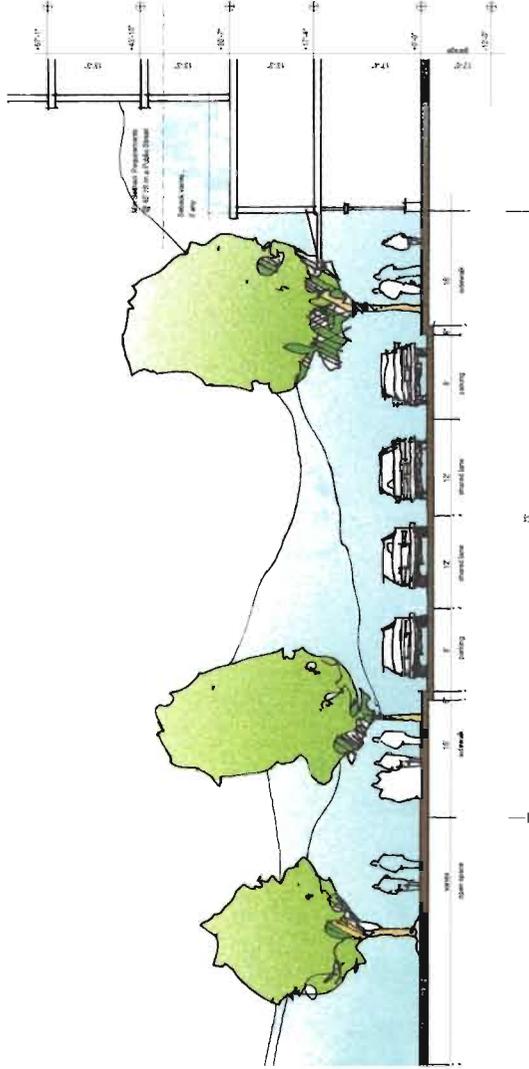
NE 13th Street / Gateway





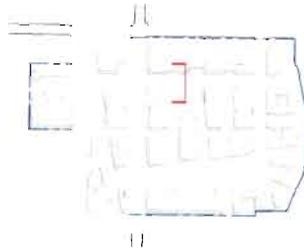
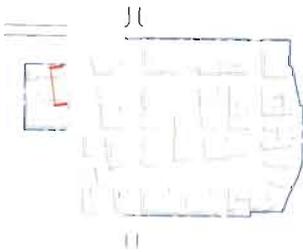
123rd Avenue NE / Private Avenue

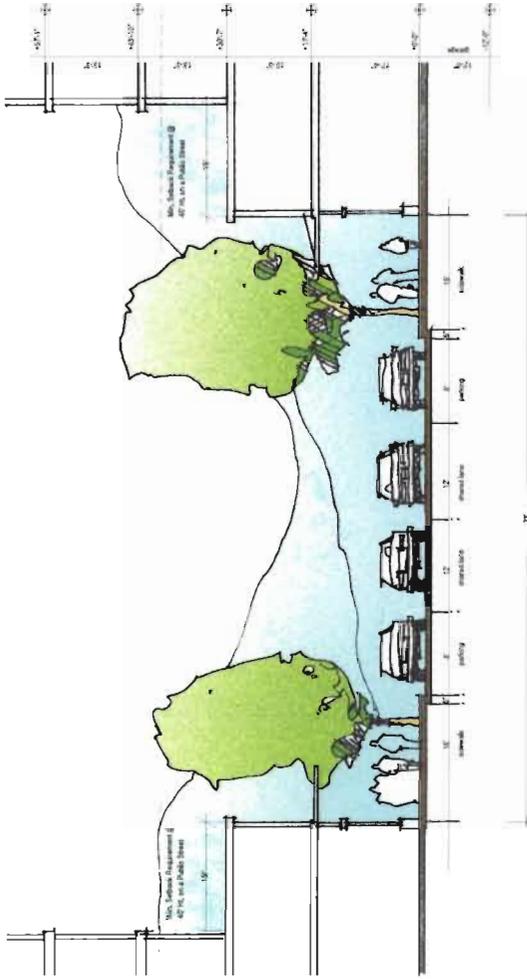
North to Street



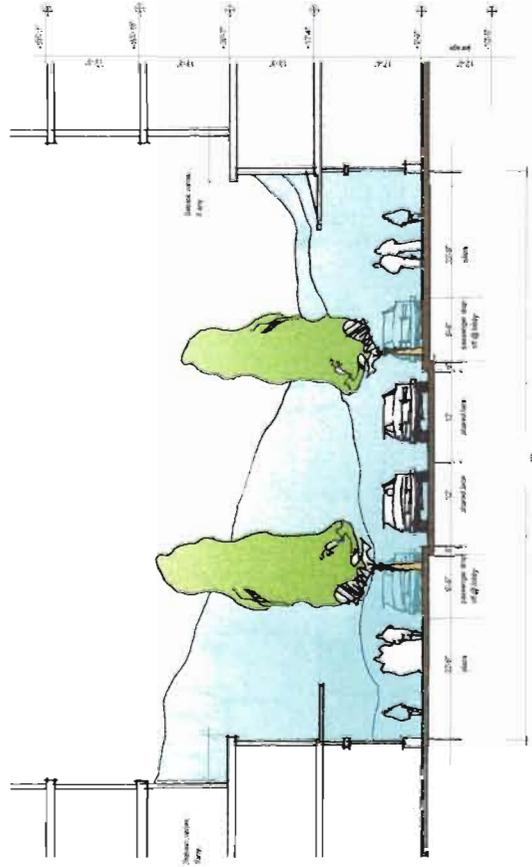
123rd Avenue NE / Pocket Park

North to Street

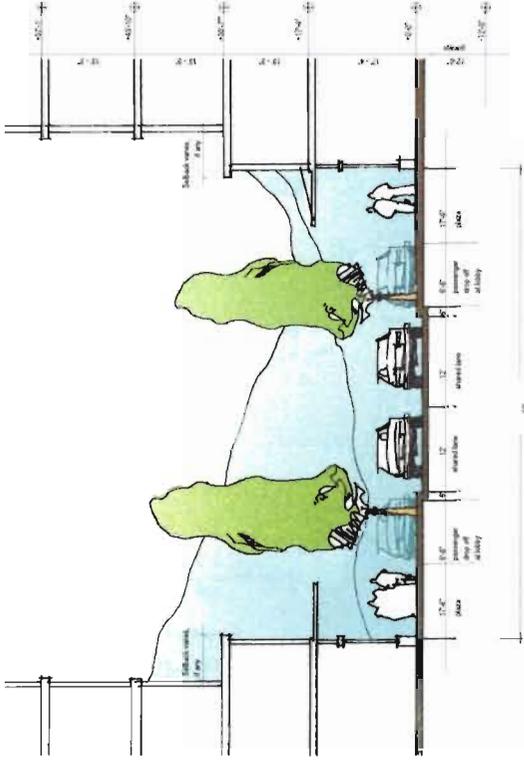




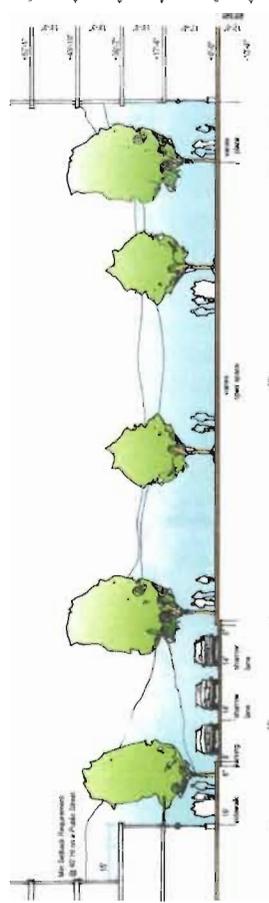
123rd Avenue NE / Urban Frontage



NE 14th Street / Private Street



122nd Avenue NE / Private Avenue  
1/16/12



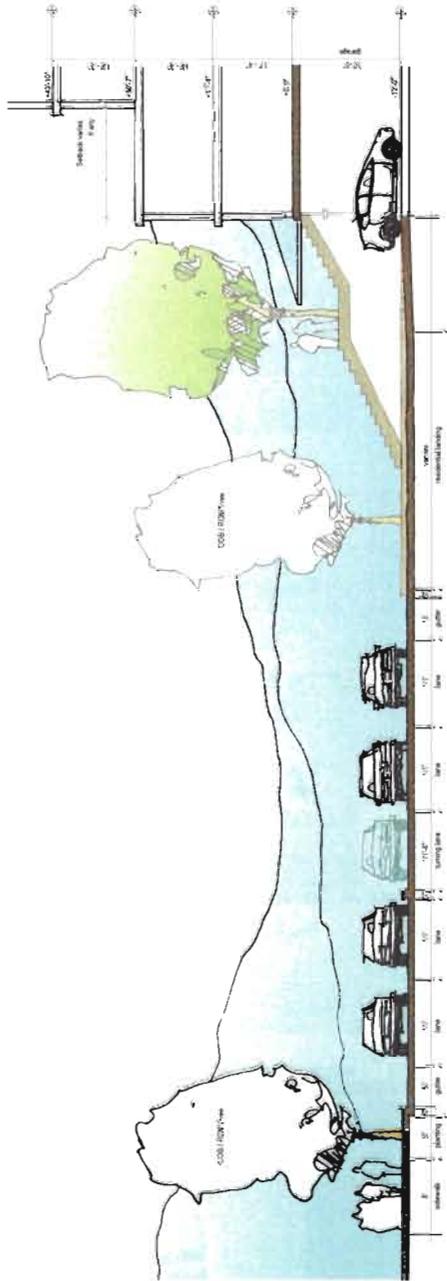
121st Avenue NE / Park Edge  
1/16/12



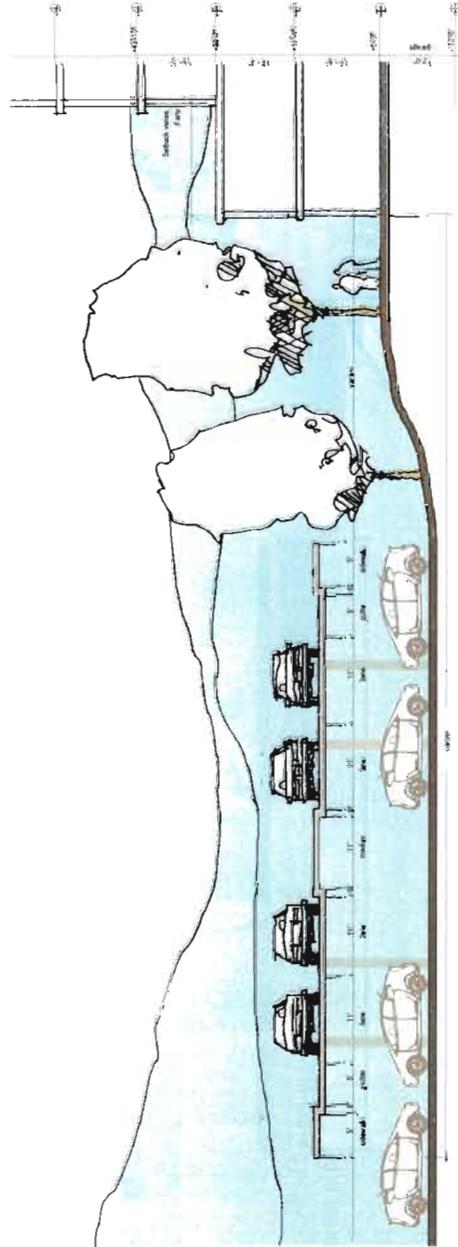




Author	Designer
3/16/2012	3/16/2012
DATE	DATE



NE 12th Street / Public Street



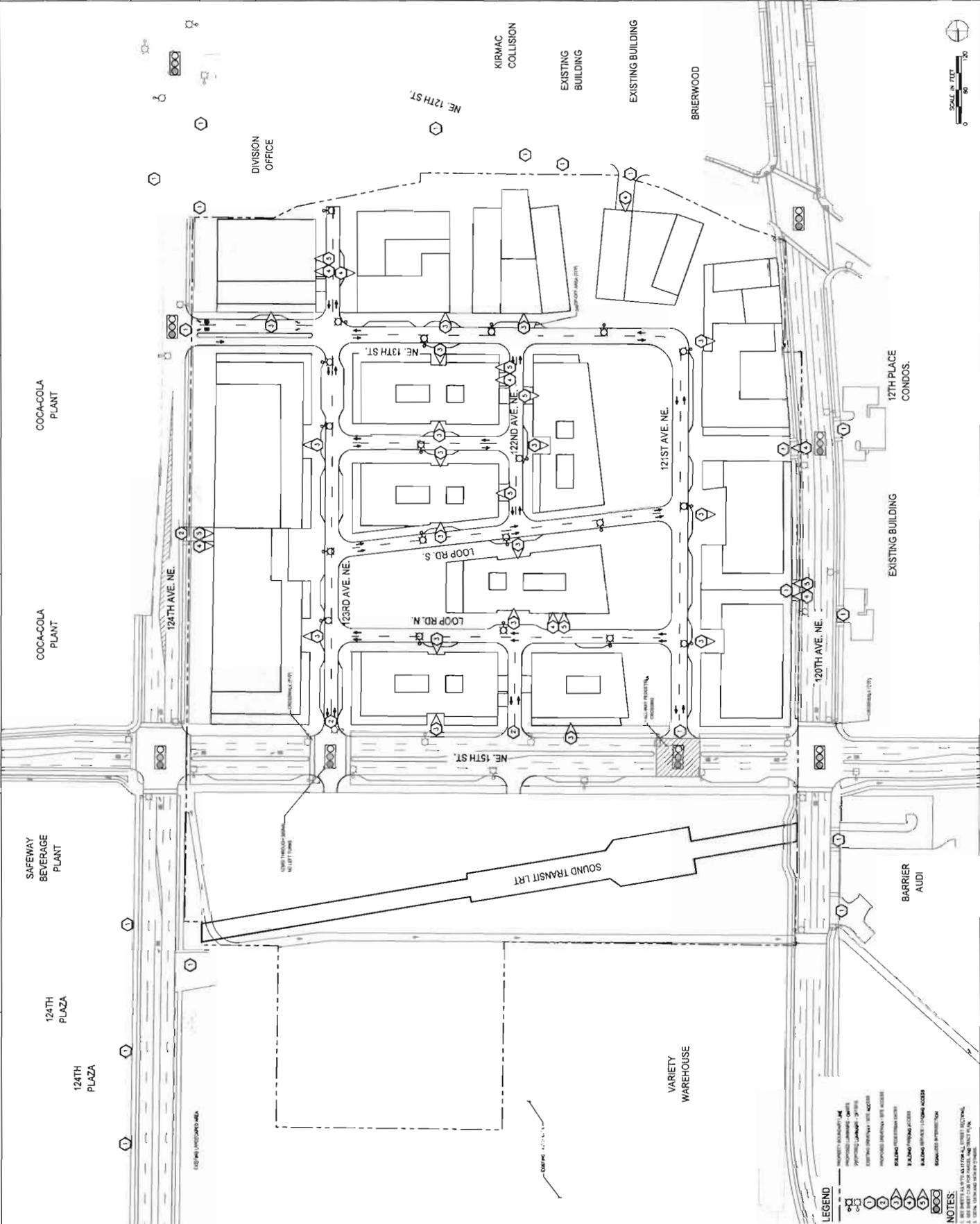
NE 12th Street / Public Street







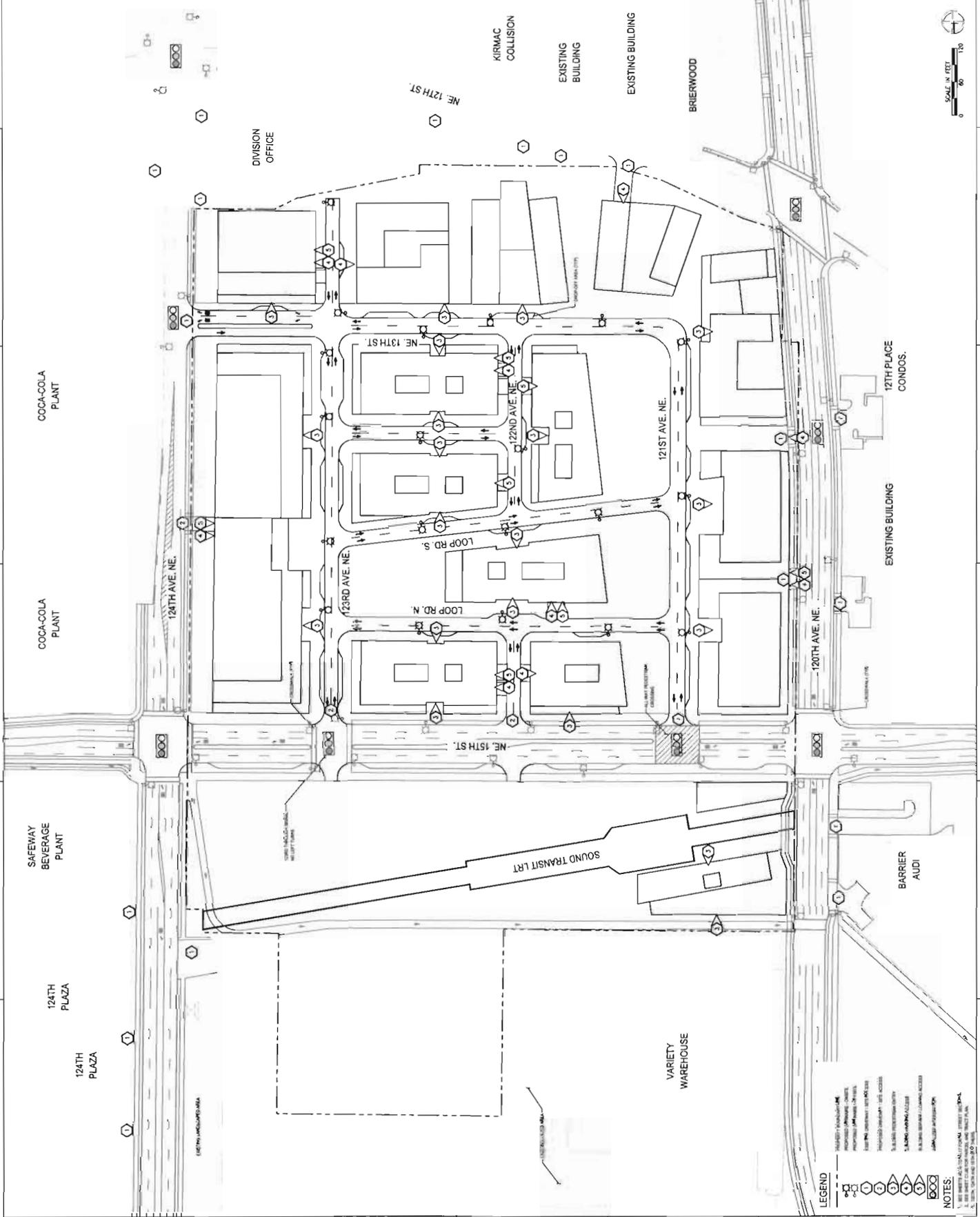




- LEGEND**
- 1. PROPOSED LIGHTING FIXTURE
  - 2. EXISTING LIGHTING FIXTURE
  - 3. PROPOSED STREET LIGHTING
  - 4. EXISTING STREET LIGHTING
  - 5. PROPOSED SIDEWALK LIGHTING
  - 6. EXISTING SIDEWALK LIGHTING
  - 7. PROPOSED BIKEWAY LIGHTING
  - 8. EXISTING BIKEWAY LIGHTING
  - 9. PROPOSED BIKEWAY LIGHTING
  - 10. EXISTING BIKEWAY LIGHTING
- NOTES**
1. SEE SHEET C3.01 FOR LIGHTING PLAN PHASE 1.
  2. SEE SHEET C3.02 FOR LIGHTING PLAN PHASE 2.
  3. SEE SHEET C3.04 FOR LIGHTING PLAN PHASE 4.
  4. SEE SHEET C3.05 FOR LIGHTING PLAN PHASE 5.
  5. SEE SHEET C3.06 FOR LIGHTING PLAN PHASE 6.
  6. SEE SHEET C3.07 FOR LIGHTING PLAN PHASE 7.
  7. SEE SHEET C3.08 FOR LIGHTING PLAN PHASE 8.
  8. SEE SHEET C3.09 FOR LIGHTING PLAN PHASE 9.
  9. SEE SHEET C3.10 FOR LIGHTING PLAN PHASE 10.
  10. SEE SHEET C3.11 FOR LIGHTING PLAN PHASE 11.
  11. SEE SHEET C3.12 FOR LIGHTING PLAN PHASE 12.
  12. SEE SHEET C3.13 FOR LIGHTING PLAN PHASE 13.
  13. SEE SHEET C3.14 FOR LIGHTING PLAN PHASE 14.
  14. SEE SHEET C3.15 FOR LIGHTING PLAN PHASE 15.
  15. SEE SHEET C3.16 FOR LIGHTING PLAN PHASE 16.
  16. SEE SHEET C3.17 FOR LIGHTING PLAN PHASE 17.
  17. SEE SHEET C3.18 FOR LIGHTING PLAN PHASE 18.
  18. SEE SHEET C3.19 FOR LIGHTING PLAN PHASE 19.
  19. SEE SHEET C3.20 FOR LIGHTING PLAN PHASE 20.
  20. SEE SHEET C3.21 FOR LIGHTING PLAN PHASE 21.
  21. SEE SHEET C3.22 FOR LIGHTING PLAN PHASE 22.
  22. SEE SHEET C3.23 FOR LIGHTING PLAN PHASE 23.
  23. SEE SHEET C3.24 FOR LIGHTING PLAN PHASE 24.
  24. SEE SHEET C3.25 FOR LIGHTING PLAN PHASE 25.
  25. SEE SHEET C3.26 FOR LIGHTING PLAN PHASE 26.
  26. SEE SHEET C3.27 FOR LIGHTING PLAN PHASE 27.
  27. SEE SHEET C3.28 FOR LIGHTING PLAN PHASE 28.
  28. SEE SHEET C3.29 FOR LIGHTING PLAN PHASE 29.
  29. SEE SHEET C3.30 FOR LIGHTING PLAN PHASE 30.
  30. SEE SHEET C3.31 FOR LIGHTING PLAN PHASE 31.
  31. SEE SHEET C3.32 FOR LIGHTING PLAN PHASE 32.
  32. SEE SHEET C3.33 FOR LIGHTING PLAN PHASE 33.
  33. SEE SHEET C3.34 FOR LIGHTING PLAN PHASE 34.
  34. SEE SHEET C3.35 FOR LIGHTING PLAN PHASE 35.
  35. SEE SHEET C3.36 FOR LIGHTING PLAN PHASE 36.
  36. SEE SHEET C3.37 FOR LIGHTING PLAN PHASE 37.
  37. SEE SHEET C3.38 FOR LIGHTING PLAN PHASE 38.
  38. SEE SHEET C3.39 FOR LIGHTING PLAN PHASE 39.
  39. SEE SHEET C3.40 FOR LIGHTING PLAN PHASE 40.
  40. SEE SHEET C3.41 FOR LIGHTING PLAN PHASE 41.
  41. SEE SHEET C3.42 FOR LIGHTING PLAN PHASE 42.
  42. SEE SHEET C3.43 FOR LIGHTING PLAN PHASE 43.
  43. SEE SHEET C3.44 FOR LIGHTING PLAN PHASE 44.
  44. SEE SHEET C3.45 FOR LIGHTING PLAN PHASE 45.
  45. SEE SHEET C3.46 FOR LIGHTING PLAN PHASE 46.
  46. SEE SHEET C3.47 FOR LIGHTING PLAN PHASE 47.
  47. SEE SHEET C3.48 FOR LIGHTING PLAN PHASE 48.
  48. SEE SHEET C3.49 FOR LIGHTING PLAN PHASE 49.
  49. SEE SHEET C3.50 FOR LIGHTING PLAN PHASE 50.
  50. SEE SHEET C3.51 FOR LIGHTING PLAN PHASE 51.
  51. SEE SHEET C3.52 FOR LIGHTING PLAN PHASE 52.
  52. SEE SHEET C3.53 FOR LIGHTING PLAN PHASE 53.
  53. SEE SHEET C3.54 FOR LIGHTING PLAN PHASE 54.
  54. SEE SHEET C3.55 FOR LIGHTING PLAN PHASE 55.
  55. SEE SHEET C3.56 FOR LIGHTING PLAN PHASE 56.
  56. SEE SHEET C3.57 FOR LIGHTING PLAN PHASE 57.
  57. SEE SHEET C3.58 FOR LIGHTING PLAN PHASE 58.
  58. SEE SHEET C3.59 FOR LIGHTING PLAN PHASE 59.
  59. SEE SHEET C3.60 FOR LIGHTING PLAN PHASE 60.
  60. SEE SHEET C3.61 FOR LIGHTING PLAN PHASE 61.
  61. SEE SHEET C3.62 FOR LIGHTING PLAN PHASE 62.
  62. SEE SHEET C3.63 FOR LIGHTING PLAN PHASE 63.
  63. SEE SHEET C3.64 FOR LIGHTING PLAN PHASE 64.
  64. SEE SHEET C3.65 FOR LIGHTING PLAN PHASE 65.
  65. SEE SHEET C3.66 FOR LIGHTING PLAN PHASE 66.
  66. SEE SHEET C3.67 FOR LIGHTING PLAN PHASE 67.
  67. SEE SHEET C3.68 FOR LIGHTING PLAN PHASE 68.
  68. SEE SHEET C3.69 FOR LIGHTING PLAN PHASE 69.
  69. SEE SHEET C3.70 FOR LIGHTING PLAN PHASE 70.
  70. SEE SHEET C3.71 FOR LIGHTING PLAN PHASE 71.
  71. SEE SHEET C3.72 FOR LIGHTING PLAN PHASE 72.
  72. SEE SHEET C3.73 FOR LIGHTING PLAN PHASE 73.
  73. SEE SHEET C3.74 FOR LIGHTING PLAN PHASE 74.
  74. SEE SHEET C3.75 FOR LIGHTING PLAN PHASE 75.
  75. SEE SHEET C3.76 FOR LIGHTING PLAN PHASE 76.
  76. SEE SHEET C3.77 FOR LIGHTING PLAN PHASE 77.
  77. SEE SHEET C3.78 FOR LIGHTING PLAN PHASE 78.
  78. SEE SHEET C3.79 FOR LIGHTING PLAN PHASE 79.
  79. SEE SHEET C3.80 FOR LIGHTING PLAN PHASE 80.
  80. SEE SHEET C3.81 FOR LIGHTING PLAN PHASE 81.
  81. SEE SHEET C3.82 FOR LIGHTING PLAN PHASE 82.
  82. SEE SHEET C3.83 FOR LIGHTING PLAN PHASE 83.
  83. SEE SHEET C3.84 FOR LIGHTING PLAN PHASE 84.
  84. SEE SHEET C3.85 FOR LIGHTING PLAN PHASE 85.
  85. SEE SHEET C3.86 FOR LIGHTING PLAN PHASE 86.
  86. SEE SHEET C3.87 FOR LIGHTING PLAN PHASE 87.
  87. SEE SHEET C3.88 FOR LIGHTING PLAN PHASE 88.
  88. SEE SHEET C3.89 FOR LIGHTING PLAN PHASE 89.
  89. SEE SHEET C3.90 FOR LIGHTING PLAN PHASE 90.
  90. SEE SHEET C3.91 FOR LIGHTING PLAN PHASE 91.
  91. SEE SHEET C3.92 FOR LIGHTING PLAN PHASE 92.
  92. SEE SHEET C3.93 FOR LIGHTING PLAN PHASE 93.
  93. SEE SHEET C3.94 FOR LIGHTING PLAN PHASE 94.
  94. SEE SHEET C3.95 FOR LIGHTING PLAN PHASE 95.
  95. SEE SHEET C3.96 FOR LIGHTING PLAN PHASE 96.
  96. SEE SHEET C3.97 FOR LIGHTING PLAN PHASE 97.
  97. SEE SHEET C3.98 FOR LIGHTING PLAN PHASE 98.
  98. SEE SHEET C3.99 FOR LIGHTING PLAN PHASE 99.
  99. SEE SHEET C3.100 FOR LIGHTING PLAN PHASE 100.



DATE	3/18/2012
PROJECT	12TH PLACE CONDOS
SCALE	1" = 60'
DATE	3/18/2012



- LEGEND**
- 1. 120V SINGLE-PHASE TRANSFORMER
  - 2. 240V SINGLE-PHASE TRANSFORMER
  - 3. 480V THREE-PHASE TRANSFORMER
  - 4. 120V SINGLE-PHASE LIGHT FIXTURE
  - 5. 240V SINGLE-PHASE LIGHT FIXTURE
  - 6. 480V THREE-PHASE LIGHT FIXTURE
  - 7. 120V SINGLE-PHASE LIGHT FIXTURE
  - 8. 240V SINGLE-PHASE LIGHT FIXTURE
  - 9. 480V THREE-PHASE LIGHT FIXTURE
  - 10. 120V SINGLE-PHASE LIGHT FIXTURE
  - 11. 240V SINGLE-PHASE LIGHT FIXTURE
  - 12. 480V THREE-PHASE LIGHT FIXTURE
- NOTES**
1. SEE SHEET C3.03 FOR PHASE 3 STREET LIGHTING PLAN.
  2. SEE SHEET C3.05 FOR PHASE 5 STREET LIGHTING PLAN.
  3. SEE SHEET C3.06 FOR PHASE 6 STREET LIGHTING PLAN.
  4. SEE SHEET C3.07 FOR PHASE 7 STREET LIGHTING PLAN.
  5. SEE SHEET C3.08 FOR PHASE 8 STREET LIGHTING PLAN.
  6. SEE SHEET C3.09 FOR PHASE 9 STREET LIGHTING PLAN.
  7. SEE SHEET C3.10 FOR PHASE 10 STREET LIGHTING PLAN.
  8. SEE SHEET C3.11 FOR PHASE 11 STREET LIGHTING PLAN.
  9. SEE SHEET C3.12 FOR PHASE 12 STREET LIGHTING PLAN.







Wright Runstad & Company  
1201 3rd Avenue  
Suite 2700  
Seattle, WA 98101



Shorenstein Properties LLC  
235 Montgomery St  
16th Floor  
San Francisco, CA 94104



NBBJ  
223 Yale Avenue North  
Seattle, WA 98109



Parametrix  
1019 39th Avenue SE  
Suite 100  
Puyallup, WA 98374



Transportation Solutions, Inc.  
8250 165th Avenue NE  
Suite 100  
Redmond, WA 98052



THE SPRING DISTRICT



Master Development  
Plan : Conform Set

Project Number	10000413
Project Name	THE SPRING DISTRICT
Project Location	1019 39th Avenue SE, Puyallup, WA 98374
Project Date	3/16/2012

COVER and  
DRAWING INDEX

DATE  
A00.01e

DRAWING INDEX

PRELIMINARY LANDSCAPE PLAN

AS.01	Cover and Sheet Index
AS.02	Utility Map
AS.03.1	Existing Site Layout
AS.03.2	Existing Site Photographs
AS.04	Existing Site Photographs
AS.05	Existing Site Photographs
AS.06	Existing Site Photographs
AS.07	Existing Site Photographs
AS.08	Existing Site Photographs
AS.09	Existing Site Photographs
AS.10	Existing Site Photographs
AS.11	Existing Site Photographs
AS.12	Existing Site Photographs
AS.13	Existing Site Photographs
AS.14	Existing Site Photographs
AS.15	Existing Site Photographs
AS.16	Existing Site Photographs
AS.17	Existing Site Photographs
AS.18	Existing Site Photographs
AS.19	Existing Site Photographs
AS.20	Existing Site Photographs
AS.21	Existing Site Photographs
AS.22	Existing Site Photographs
AS.23	Existing Site Photographs
AS.24	Existing Site Photographs
AS.25	Existing Site Photographs
AS.26	Existing Site Photographs
AS.27	Existing Site Photographs
AS.28	Existing Site Photographs
AS.29	Existing Site Photographs
AS.30	Existing Site Photographs
AS.31	Existing Site Photographs
AS.32	Existing Site Photographs
AS.33	Existing Site Photographs
AS.34	Existing Site Photographs
AS.35	Existing Site Photographs
AS.36	Existing Site Photographs
AS.37	Existing Site Photographs
AS.38	Existing Site Photographs
AS.39	Existing Site Photographs
AS.40	Existing Site Photographs
AS.41	Existing Site Photographs
AS.42	Existing Site Photographs
AS.43	Existing Site Photographs
AS.44	Existing Site Photographs
AS.45	Existing Site Photographs
AS.46	Existing Site Photographs
AS.47	Existing Site Photographs
AS.48	Existing Site Photographs
AS.49	Existing Site Photographs
AS.50	Existing Site Photographs
AS.51	Existing Site Photographs
AS.52	Existing Site Photographs
AS.53	Existing Site Photographs
AS.54	Existing Site Photographs
AS.55	Existing Site Photographs
AS.56	Existing Site Photographs
AS.57	Existing Site Photographs
AS.58	Existing Site Photographs
AS.59	Existing Site Photographs
AS.60	Existing Site Photographs
AS.61	Existing Site Photographs
AS.62	Existing Site Photographs
AS.63	Existing Site Photographs
AS.64	Existing Site Photographs
AS.65	Existing Site Photographs
AS.66	Existing Site Photographs
AS.67	Existing Site Photographs
AS.68	Existing Site Photographs
AS.69	Existing Site Photographs
AS.70	Existing Site Photographs
AS.71	Existing Site Photographs
AS.72	Existing Site Photographs
AS.73	Existing Site Photographs
AS.74	Existing Site Photographs
AS.75	Existing Site Photographs
AS.76	Existing Site Photographs
AS.77	Existing Site Photographs
AS.78	Existing Site Photographs
AS.79	Existing Site Photographs
AS.80	Existing Site Photographs
AS.81	Existing Site Photographs
AS.82	Existing Site Photographs
AS.83	Existing Site Photographs
AS.84	Existing Site Photographs
AS.85	Existing Site Photographs
AS.86	Existing Site Photographs
AS.87	Existing Site Photographs
AS.88	Existing Site Photographs
AS.89	Existing Site Photographs
AS.90	Existing Site Photographs
AS.91	Existing Site Photographs
AS.92	Existing Site Photographs
AS.93	Existing Site Photographs
AS.94	Existing Site Photographs
AS.95	Existing Site Photographs
AS.96	Existing Site Photographs
AS.97	Existing Site Photographs
AS.98	Existing Site Photographs
AS.99	Existing Site Photographs
AS.100	Existing Site Photographs



Project No.	14-4
Sheet No.	100944-13
Date	3/16/2013

PRELIMINARY  
 LANDSCAPE  
 PLAN - PHASE 1A

SCALE: 1" = 100'



COCA-COLA PLANT

COCA-COLA PLANT

SAFEBWAY BEVERAGE PLANT

124TH PLAZA

124TH PLAZA

124TH AVE. NE.

DIVISION OFFICE

123RD AVE. NE.

NE 13TH ST.

INTERIM PARK

122ND AVE. NE.

INTERIM PARK

121ST AVE. NE.

BRIERWOOD

COLD WAREHOUSE

COLD WAREHOUSE

COLD WAREHOUSE

VARIETY WAREHOUSE

TRUCK SHOP REPAIR

RECYCLING CENTER

KIRMAC COLLISION

EXISTING BUILDING

EXISTING BUILDING

12TH PLACE CONDOS.

EXISTING BUILDING

TELEDESIC

BARRIER AUDI

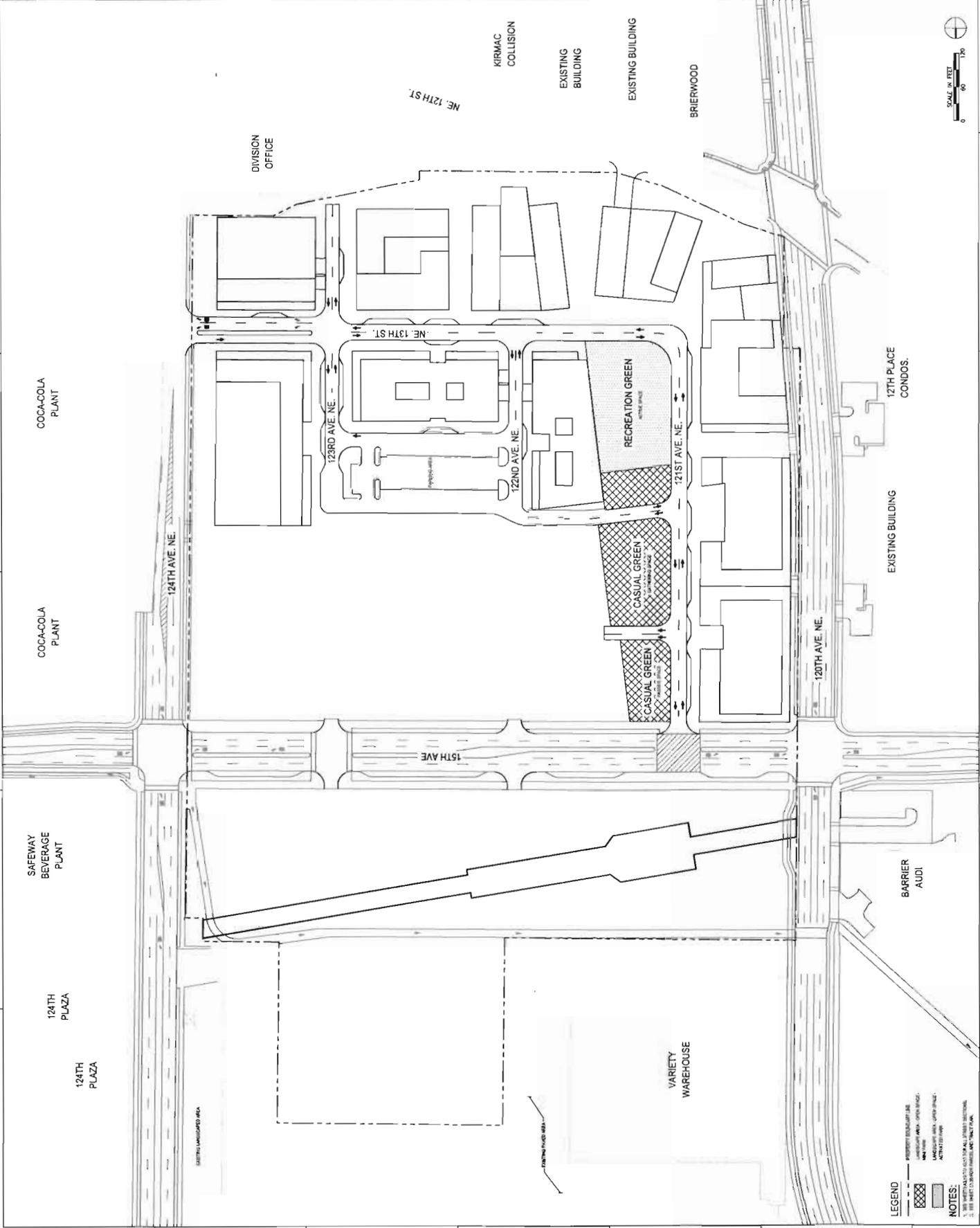
**LEGEND**  
 [Symbol] PROPERTY BOUNDARY  
 [Symbol] EXISTING BUILDING  
 [Symbol] EXISTING DRIVE  
 [Symbol] EXISTING SIDEWALK  
 [Symbol] EXISTING CURB  
 [Symbol] EXISTING DRIVE CURB  
 [Symbol] EXISTING SIDEWALK CURB  
 [Symbol] EXISTING DRIVE CURB  
 [Symbol] EXISTING SIDEWALK CURB

**NOTES:**  
 1. SEE SHEET 100944-12 FOR PRELIMINARY LANDSCAPE PLAN - PHASE 1A.  
 2. SEE SHEET 100944-13 FOR PRELIMINARY LANDSCAPE PLAN - PHASE 1A.





DATE	3/16/2012
BY	3/16/2012
FILE NO.	10019113
OWNER D.S.	3/16/2012



**LEGEND**

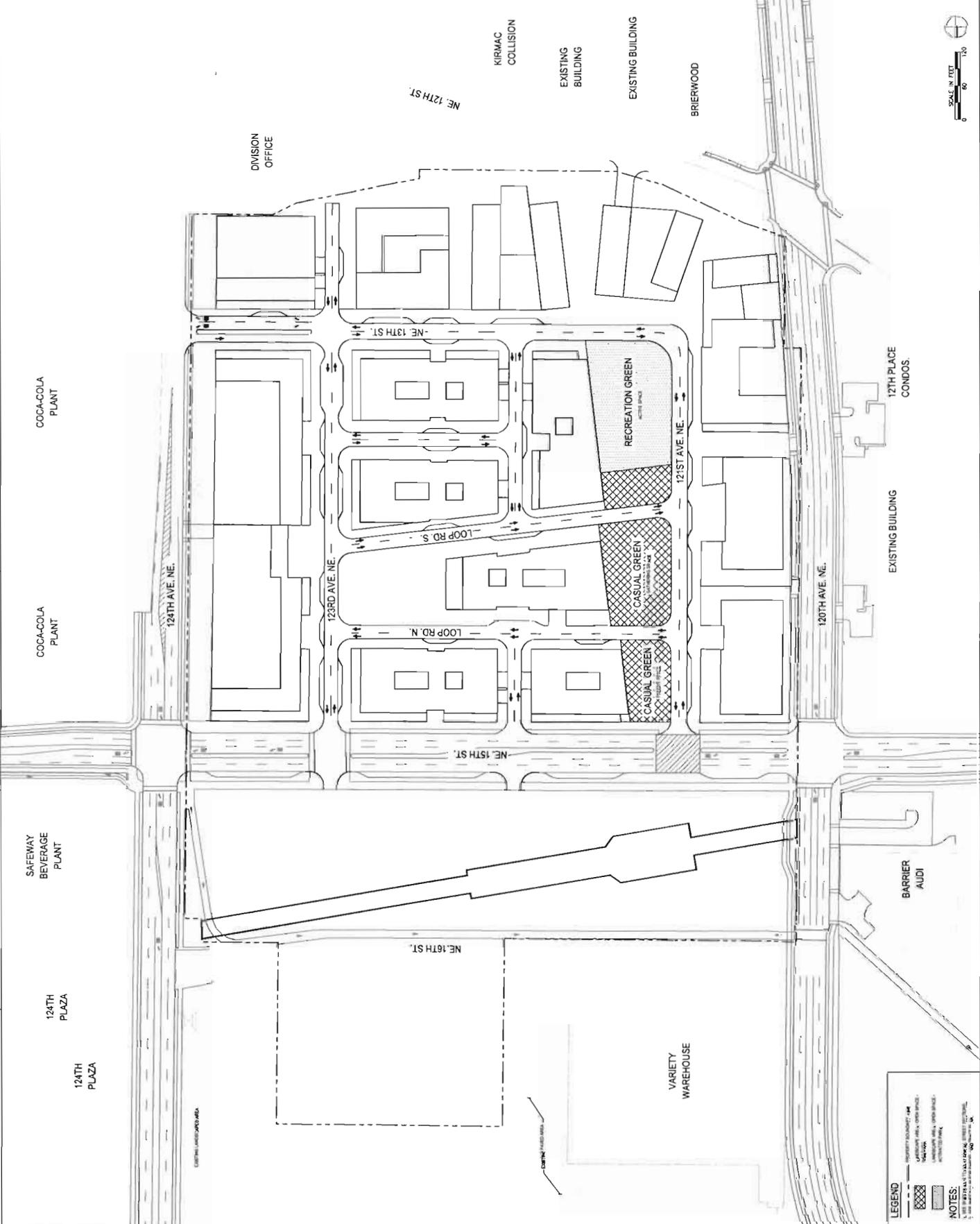
- EXISTING BUILDING LINE
- EXISTING BUILDING
- LANDSCAPE AREA - CASUAL GREEN
- LANDSCAPE AREA - RECREATION GREEN
- ACTIVATED PARK

**NOTES:**

1. SEE SHEET L1.01 FOR ALL OTHER SECTIONS.
2. SEE SHEET L1.03 FOR THE LANDSCAPE PLAN.



DATE	3/14/2012
BY	JM/2012
PROJECT NO.	100000013
FILE NO.	D.A.

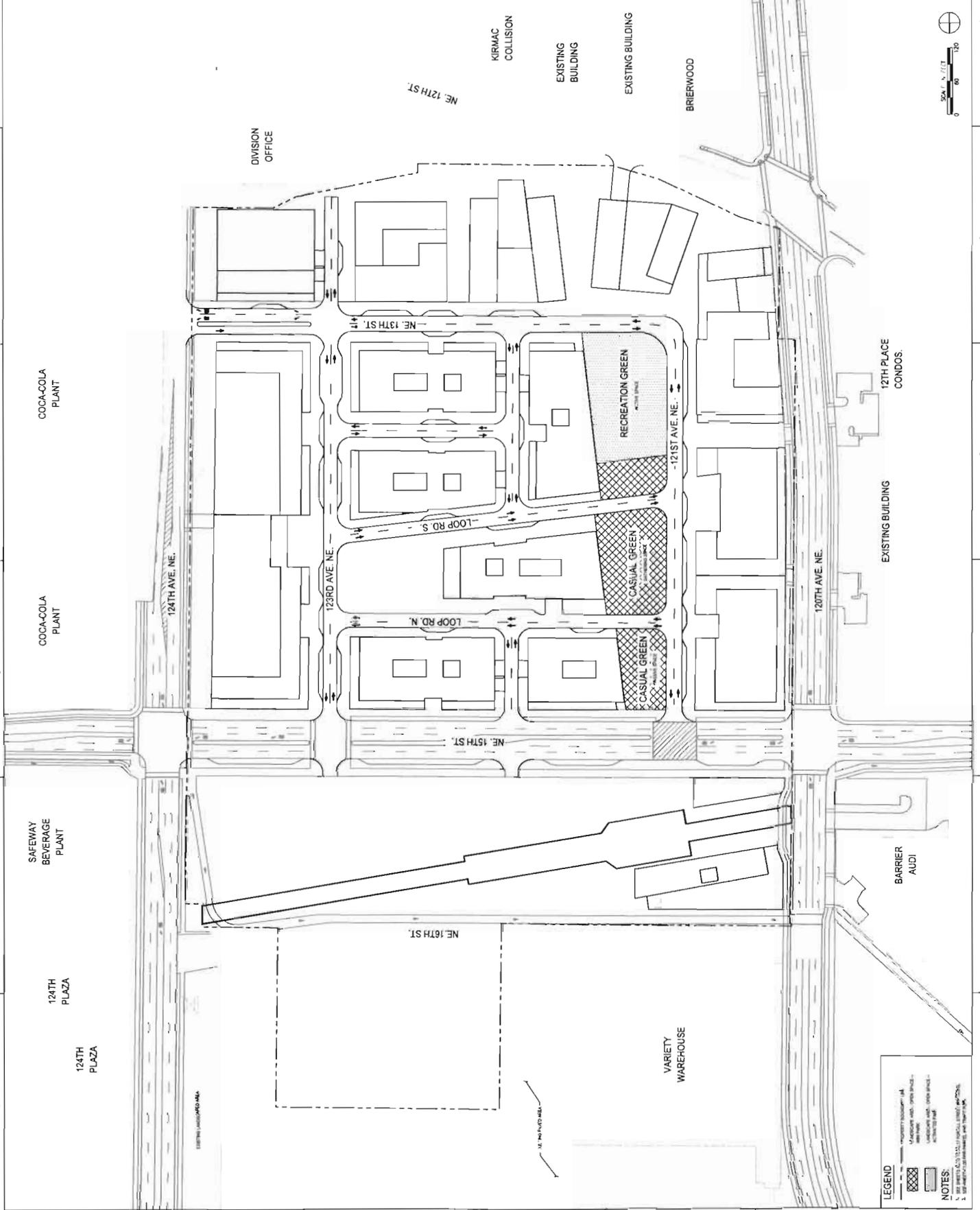


**LEGEND**

- CONCRETE CURB
- CONCRETE DRIVEWAY
- CONCRETE SIDEWALK
- LANDSCAPE WALL
- LANDSCAPE WALL WITH BENCH
- LANDSCAPE WALL WITH SEATING
- LANDSCAPE WALL WITH SEATING AND BENCH

**NOTES:**

- SEE MASTER DEVELOPMENT PLAN FOR GENERAL NOTES.
- SEE MASTER DEVELOPMENT PLAN FOR GENERAL NOTES.
- SEE MASTER DEVELOPMENT PLAN FOR GENERAL NOTES.

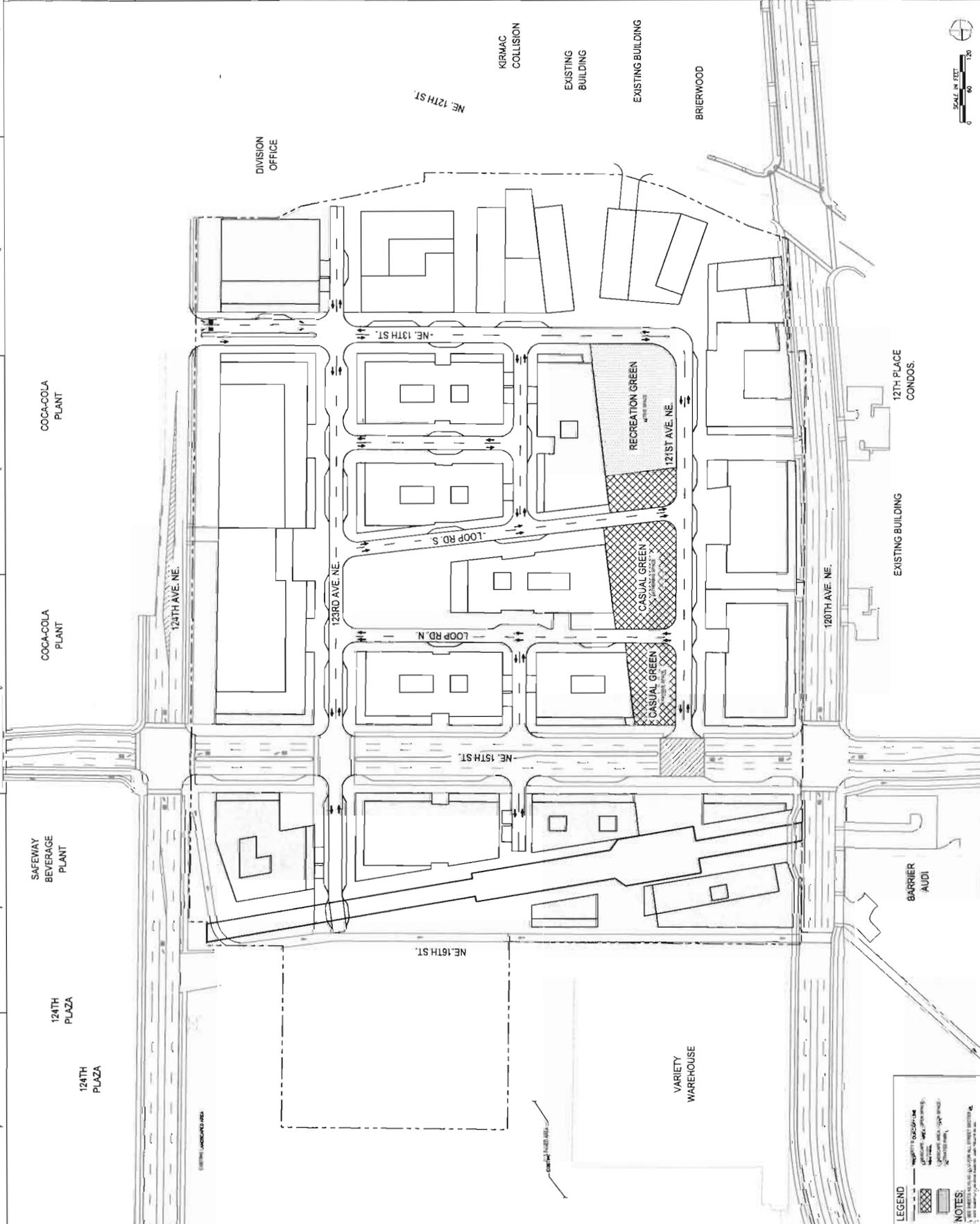


**LEGEND**

- RECREATION GREEN
- CASUAL GREEN
- EXISTING BUILDING
- EXISTING DRIVE
- EXISTING SIDEWALK
- EXISTING CURB
- EXISTING DRIVE
- EXISTING SIDEWALK
- EXISTING CURB

**NOTES**

- SEE SHEET L1.03 FOR PRELIMINARY LANDSCAPE PLAN PHASE 3.
- SEE SHEET L1.05 FOR PRELIMINARY LANDSCAPE PLAN PHASE 5.

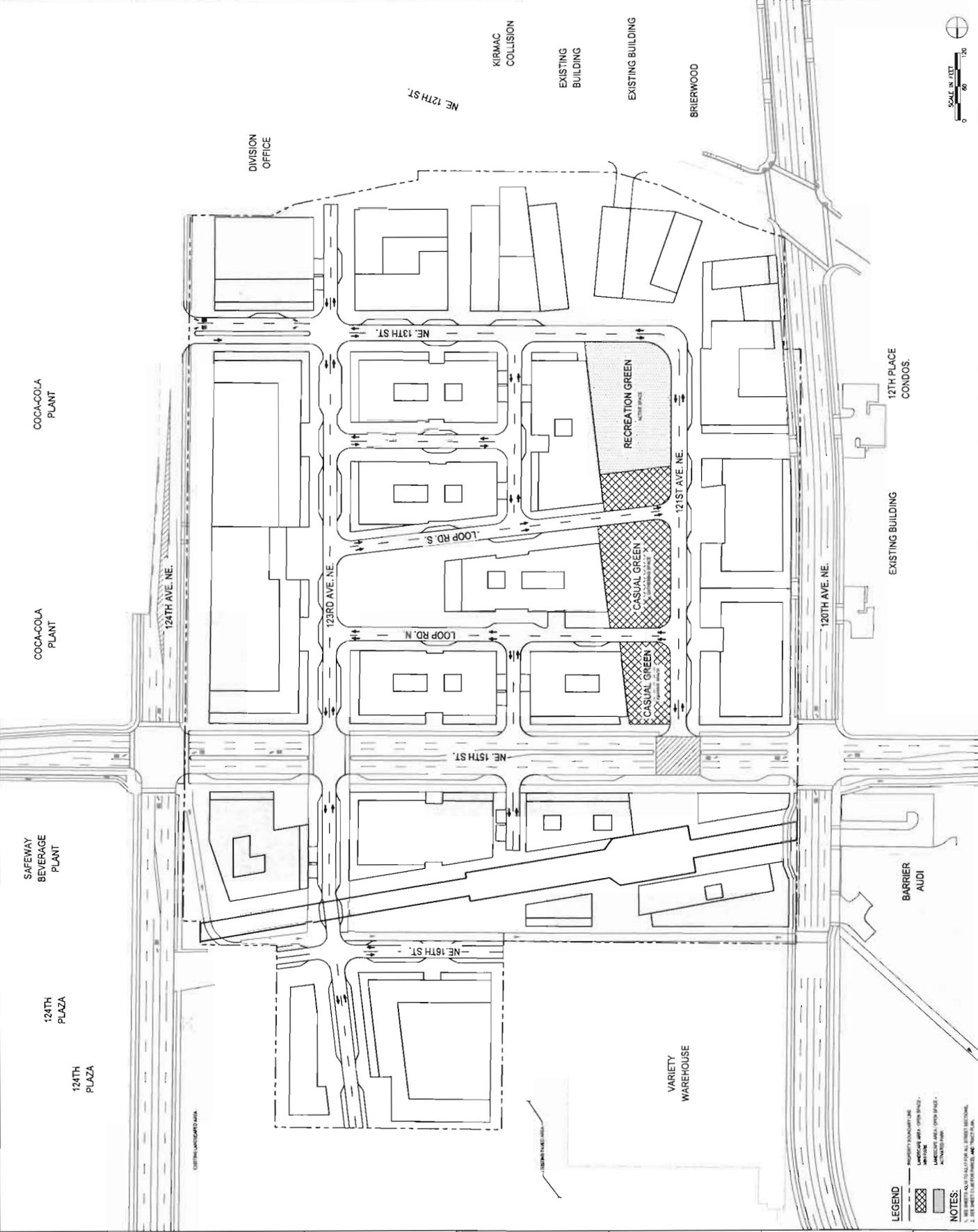


**LEGEND**

- [Symbol] EXISTING BUILDING
- [Symbol] EXISTING GREEN
- [Symbol] EXISTING DRIVE
- [Symbol] EXISTING SIDEWALK
- [Symbol] EXISTING CURB
- [Symbol] EXISTING DRIVE
- [Symbol] EXISTING SIDEWALK
- [Symbol] EXISTING CURB

**NOTES:**

1. ALL EXISTING UTILITIES SHALL BE MAINTAINED.
2. ALL EXISTING UTILITIES SHALL BE MAINTAINED.



**LEGEND**

- EXISTING BUILDING
- LANDSCAPE AREA - CASUAL GREEN
- LANDSCAPE AREA - RECREATION GREEN
- RECREATION GREEN
- CASUAL GREEN

**NOTES:**

1. SEE SHEET 12-01 FOR PHASE 5 LANDSCAPE PLAN.
2. SEE SHEET 12-02 FOR PHASE 6 LANDSCAPE PLAN.



Wright Runstad & Company  
1201 3rd Avenue  
Suite 2700  
Seattle, WA 98101



Shorenstein Properties LLC  
235 Montgomery St.  
16th Floor  
San Francisco, CA 94104



NBBJ  
223 Yale Avenue North  
Seattle, WA 98109



Parametrix  
1019 39th Avenue SE  
Suite 100  
Puyallup, WA 98374



Transportation Solutions, Inc.  
8250 165th Avenue NE  
Suite 100  
Redmond, WA 98052

**DRAWING INDEX**

**PRELIMINARY STREET LIGHTING PLAN**

AL001	Cover and Sheet Index
AL002	Utility Map
AL003	Existing Site Survey
AL004	Existing Site Photographs
AL005	Existing Site Photographs
AL006	Existing Site Photographs
AL007	Existing Site Photographs
AL008	Existing Site Photographs
AL009	Existing Site Photographs
AL010	Existing Site Photographs
AL011	Existing Site Photographs
AL012	Existing Site Photographs
AL013	Existing Site Photographs
AL014	Existing Site Photographs
AL015	Existing Site Photographs
AL016	Existing Site Photographs
AL017	Existing Site Photographs
AL018	Existing Site Photographs
AL019	Existing Site Photographs
AL020	Existing Site Photographs
AL021	Existing Site Photographs
AL022	Existing Site Photographs
AL023	Existing Site Photographs
AL024	Existing Site Photographs
AL025	Existing Site Photographs
AL026	Existing Site Photographs
AL027	Existing Site Photographs
AL028	Existing Site Photographs
AL029	Existing Site Photographs
AL030	Existing Site Photographs
AL031	Existing Site Photographs
AL032	Existing Site Photographs
AL033	Existing Site Photographs
AL034	Existing Site Photographs
AL035	Existing Site Photographs
AL036	Existing Site Photographs
AL037	Existing Site Photographs
AL038	Existing Site Photographs
AL039	Existing Site Photographs
AL040	Existing Site Photographs
AL041	Existing Site Photographs
AL042	Existing Site Photographs
AL043	Existing Site Photographs
AL044	Existing Site Photographs
AL045	Existing Site Photographs
AL046	Existing Site Photographs
AL047	Existing Site Photographs
AL048	Existing Site Photographs
AL049	Existing Site Photographs
AL050	Existing Site Photographs
AL051	Existing Site Photographs
AL052	Existing Site Photographs
AL053	Existing Site Photographs
AL054	Existing Site Photographs
AL055	Existing Site Photographs
AL056	Existing Site Photographs
AL057	Existing Site Photographs
AL058	Existing Site Photographs
AL059	Existing Site Photographs
AL060	Existing Site Photographs
AL061	Existing Site Photographs
AL062	Existing Site Photographs
AL063	Existing Site Photographs
AL064	Existing Site Photographs
AL065	Existing Site Photographs
AL066	Existing Site Photographs
AL067	Existing Site Photographs
AL068	Existing Site Photographs
AL069	Existing Site Photographs
AL070	Existing Site Photographs
AL071	Existing Site Photographs
AL072	Existing Site Photographs
AL073	Existing Site Photographs
AL074	Existing Site Photographs
AL075	Existing Site Photographs
AL076	Existing Site Photographs
AL077	Existing Site Photographs
AL078	Existing Site Photographs
AL079	Existing Site Photographs
AL080	Existing Site Photographs
AL081	Existing Site Photographs
AL082	Existing Site Photographs
AL083	Existing Site Photographs
AL084	Existing Site Photographs
AL085	Existing Site Photographs
AL086	Existing Site Photographs
AL087	Existing Site Photographs
AL088	Existing Site Photographs
AL089	Existing Site Photographs
AL090	Existing Site Photographs
AL091	Existing Site Photographs
AL092	Existing Site Photographs
AL093	Existing Site Photographs
AL094	Existing Site Photographs
AL095	Existing Site Photographs
AL096	Existing Site Photographs
AL097	Existing Site Photographs
AL098	Existing Site Photographs
AL099	Existing Site Photographs
AL100	Existing Site Photographs
AL101	Existing Site Photographs
AL102	Existing Site Photographs
AL103	Existing Site Photographs
AL104	Existing Site Photographs
AL105	Existing Site Photographs
AL106	Existing Site Photographs
AL107	Existing Site Photographs
AL108	Existing Site Photographs
AL109	Existing Site Photographs
AL110	Existing Site Photographs
AL111	Existing Site Photographs
AL112	Existing Site Photographs
AL113	Existing Site Photographs
AL114	Existing Site Photographs
AL115	Existing Site Photographs
AL116	Existing Site Photographs
AL117	Existing Site Photographs
AL118	Existing Site Photographs
AL119	Existing Site Photographs
AL120	Existing Site Photographs
AL121	Existing Site Photographs
AL122	Existing Site Photographs
AL123	Existing Site Photographs
AL124	Existing Site Photographs
AL125	Existing Site Photographs
AL126	Existing Site Photographs
AL127	Existing Site Photographs
AL128	Existing Site Photographs
AL129	Existing Site Photographs
AL130	Existing Site Photographs
AL131	Existing Site Photographs
AL132	Existing Site Photographs
AL133	Existing Site Photographs
AL134	Existing Site Photographs
AL135	Existing Site Photographs
AL136	Existing Site Photographs
AL137	Existing Site Photographs
AL138	Existing Site Photographs
AL139	Existing Site Photographs
AL140	Existing Site Photographs
AL141	Existing Site Photographs
AL142	Existing Site Photographs
AL143	Existing Site Photographs
AL144	Existing Site Photographs
AL145	Existing Site Photographs
AL146	Existing Site Photographs
AL147	Existing Site Photographs
AL148	Existing Site Photographs
AL149	Existing Site Photographs
AL150	Existing Site Photographs
AL151	Existing Site Photographs
AL152	Existing Site Photographs
AL153	Existing Site Photographs
AL154	Existing Site Photographs
AL155	Existing Site Photographs
AL156	Existing Site Photographs
AL157	Existing Site Photographs
AL158	Existing Site Photographs
AL159	Existing Site Photographs
AL160	Existing Site Photographs
AL161	Existing Site Photographs
AL162	Existing Site Photographs
AL163	Existing Site Photographs
AL164	Existing Site Photographs
AL165	Existing Site Photographs
AL166	Existing Site Photographs
AL167	Existing Site Photographs
AL168	Existing Site Photographs
AL169	Existing Site Photographs
AL170	Existing Site Photographs
AL171	Existing Site Photographs
AL172	Existing Site Photographs
AL173	Existing Site Photographs
AL174	Existing Site Photographs
AL175	Existing Site Photographs
AL176	Existing Site Photographs
AL177	Existing Site Photographs
AL178	Existing Site Photographs
AL179	Existing Site Photographs
AL180	Existing Site Photographs
AL181	Existing Site Photographs
AL182	Existing Site Photographs
AL183	Existing Site Photographs
AL184	Existing Site Photographs
AL185	Existing Site Photographs
AL186	Existing Site Photographs
AL187	Existing Site Photographs
AL188	Existing Site Photographs
AL189	Existing Site Photographs
AL190	Existing Site Photographs
AL191	Existing Site Photographs
AL192	Existing Site Photographs
AL193	Existing Site Photographs
AL194	Existing Site Photographs
AL195	Existing Site Photographs
AL196	Existing Site Photographs
AL197	Existing Site Photographs
AL198	Existing Site Photographs
AL199	Existing Site Photographs
AL200	Existing Site Photographs



**Parametrix**

THE SPRING DISTRICT

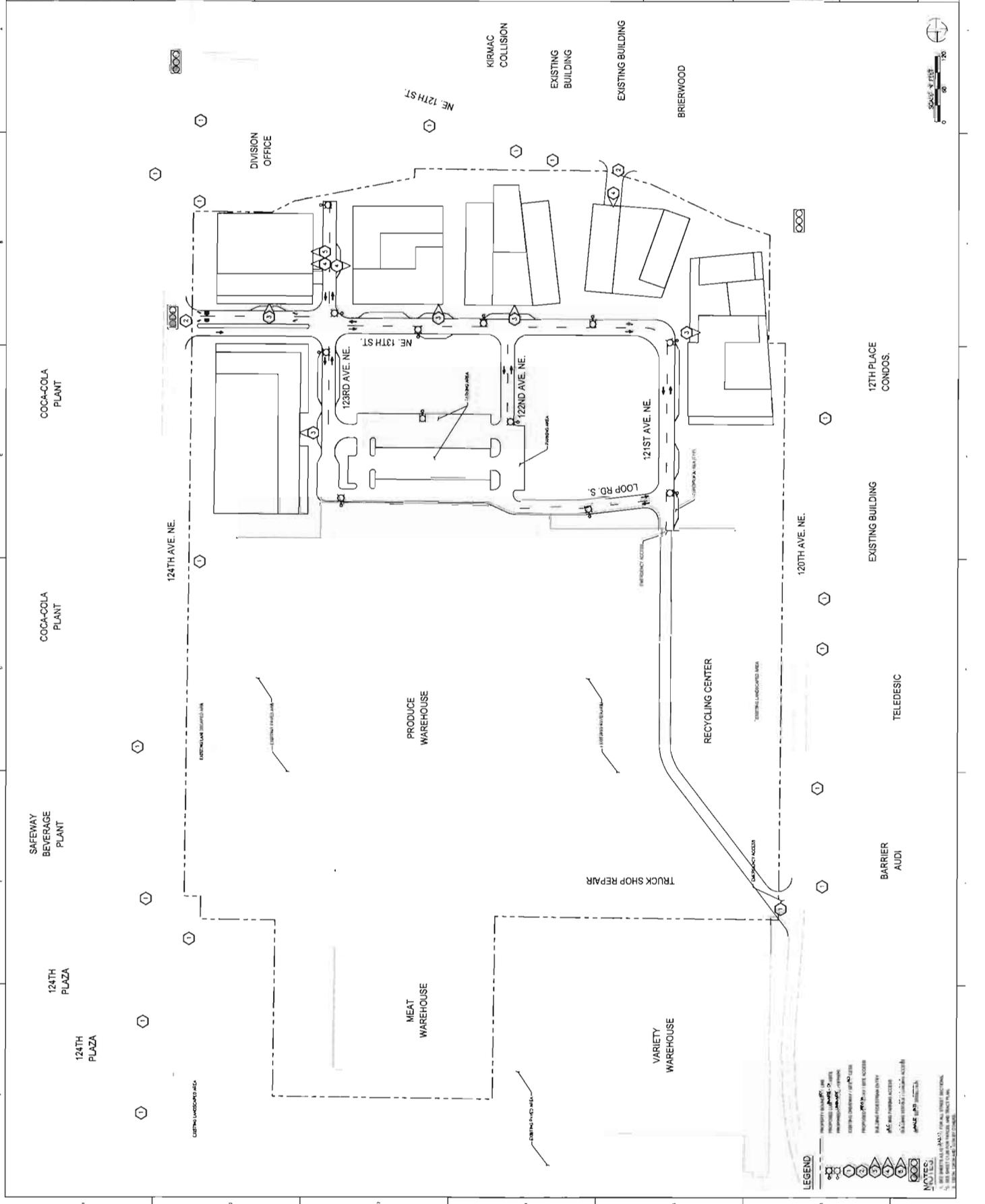


Master Development  
Plan : Conform Set

NO. 001	NO. 001
AUTHOR	10/18/13
DATE	3/16/2012
BY	

COVER and  
DRAWING INDEX

NO. A0.01F



**LEGEND**

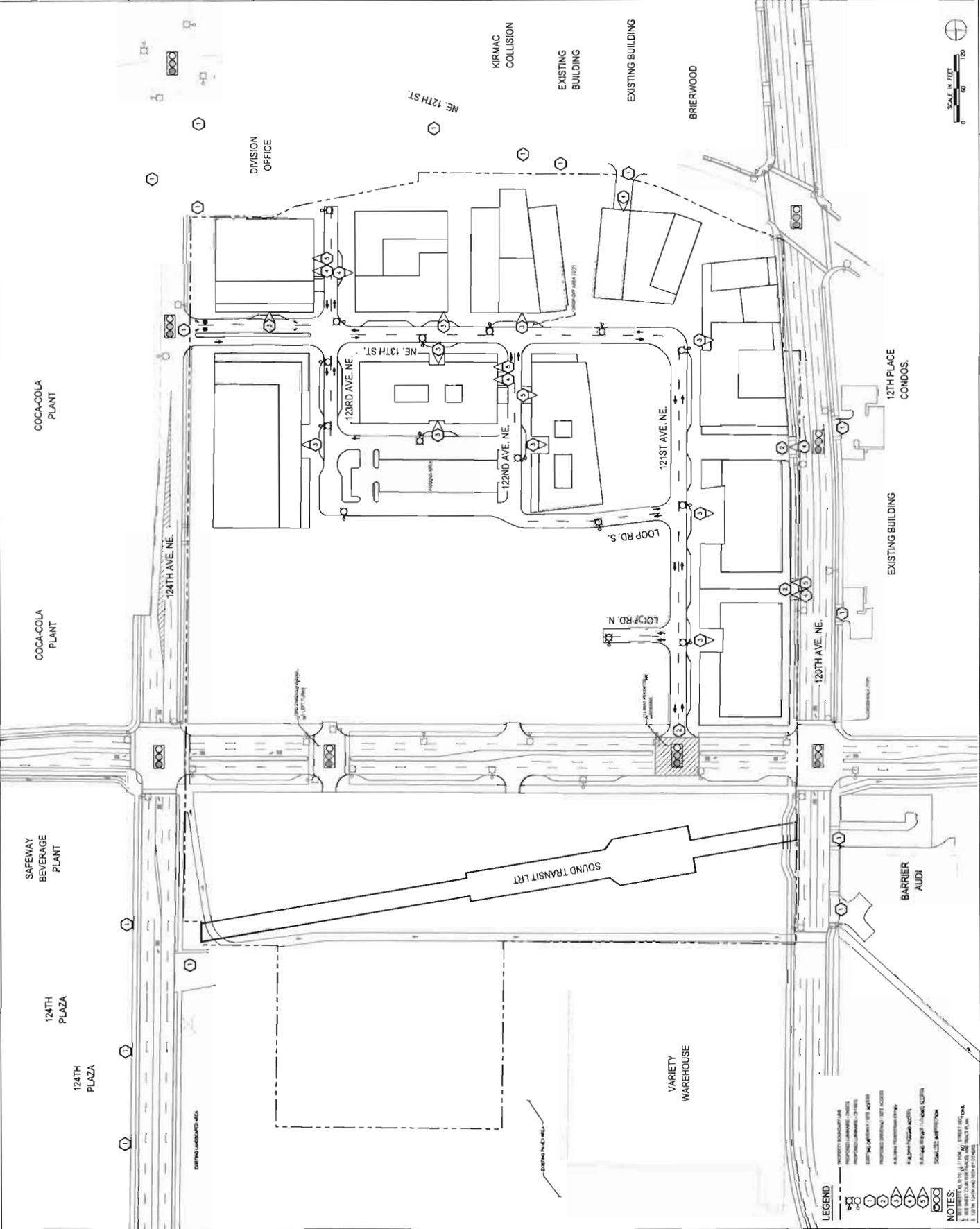
- 1. PROPOSED BUILDING FOOTPRINT
- 2. EXISTING BUILDING FOOTPRINT
- 3. EXISTING DRIVEWAY
- 4. EXISTING DRIVEWAY
- 5. EXISTING DRIVEWAY
- 6. EXISTING DRIVEWAY
- 7. EXISTING DRIVEWAY
- 8. EXISTING DRIVEWAY
- 9. EXISTING DRIVEWAY
- 10. EXISTING DRIVEWAY
- 11. EXISTING DRIVEWAY
- 12. EXISTING DRIVEWAY
- 13. EXISTING DRIVEWAY
- 14. EXISTING DRIVEWAY
- 15. EXISTING DRIVEWAY
- 16. EXISTING DRIVEWAY
- 17. EXISTING DRIVEWAY
- 18. EXISTING DRIVEWAY
- 19. EXISTING DRIVEWAY
- 20. EXISTING DRIVEWAY

**NOTES:**

1. SEE SHEET 1001 FOR ALL STREET LIGHTING INFORMATION.
2. SEE SHEET 1002 FOR ALL STREET LIGHTING INFORMATION.
3. SEE SHEET 1003 FOR ALL STREET LIGHTING INFORMATION.
4. SEE SHEET 1004 FOR ALL STREET LIGHTING INFORMATION.
5. SEE SHEET 1005 FOR ALL STREET LIGHTING INFORMATION.
6. SEE SHEET 1006 FOR ALL STREET LIGHTING INFORMATION.
7. SEE SHEET 1007 FOR ALL STREET LIGHTING INFORMATION.
8. SEE SHEET 1008 FOR ALL STREET LIGHTING INFORMATION.
9. SEE SHEET 1009 FOR ALL STREET LIGHTING INFORMATION.
10. SEE SHEET 1010 FOR ALL STREET LIGHTING INFORMATION.

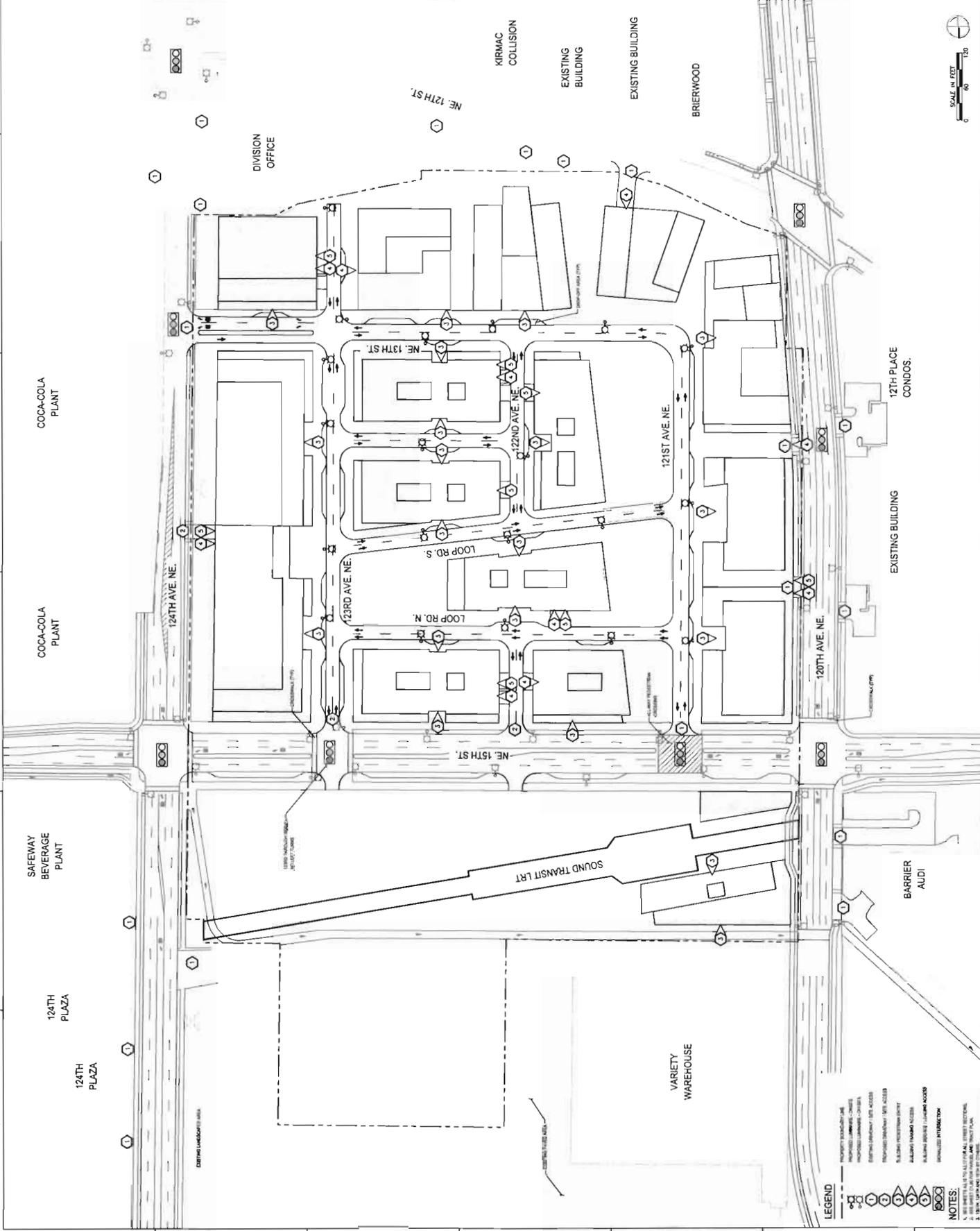




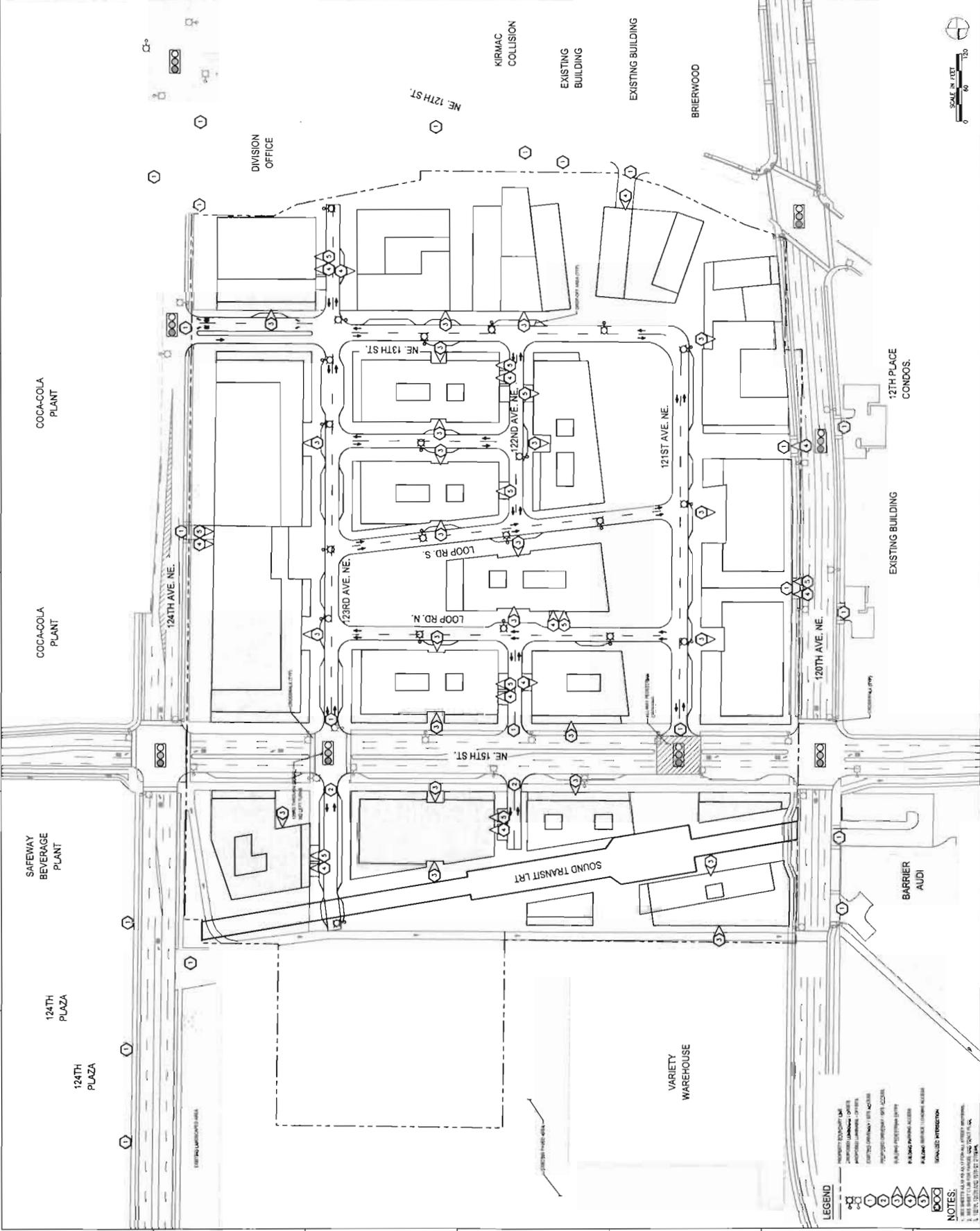


- LEGEND**
- 1. EXISTING BUILDING
  - 2. EXISTING BUILDING
  - 3. EXISTING BUILDING
  - 4. EXISTING BUILDING
  - 5. EXISTING BUILDING
  - 6. EXISTING BUILDING
  - 7. EXISTING BUILDING
  - 8. EXISTING BUILDING
  - 9. EXISTING BUILDING
  - 10. EXISTING BUILDING
  - 11. EXISTING BUILDING
  - 12. EXISTING BUILDING
  - 13. EXISTING BUILDING
  - 14. EXISTING BUILDING
  - 15. EXISTING BUILDING
  - 16. EXISTING BUILDING
  - 17. EXISTING BUILDING
  - 18. EXISTING BUILDING
  - 19. EXISTING BUILDING
  - 20. EXISTING BUILDING
  - 21. EXISTING BUILDING
  - 22. EXISTING BUILDING
  - 23. EXISTING BUILDING
  - 24. EXISTING BUILDING
  - 25. EXISTING BUILDING
  - 26. EXISTING BUILDING
  - 27. EXISTING BUILDING
  - 28. EXISTING BUILDING
  - 29. EXISTING BUILDING
  - 30. EXISTING BUILDING
  - 31. EXISTING BUILDING
  - 32. EXISTING BUILDING
  - 33. EXISTING BUILDING
  - 34. EXISTING BUILDING
  - 35. EXISTING BUILDING
  - 36. EXISTING BUILDING
  - 37. EXISTING BUILDING
  - 38. EXISTING BUILDING
  - 39. EXISTING BUILDING
  - 40. EXISTING BUILDING
  - 41. EXISTING BUILDING
  - 42. EXISTING BUILDING
  - 43. EXISTING BUILDING
  - 44. EXISTING BUILDING
  - 45. EXISTING BUILDING
  - 46. EXISTING BUILDING
  - 47. EXISTING BUILDING
  - 48. EXISTING BUILDING
  - 49. EXISTING BUILDING
  - 50. EXISTING BUILDING
  - 51. EXISTING BUILDING
  - 52. EXISTING BUILDING
  - 53. EXISTING BUILDING
  - 54. EXISTING BUILDING
  - 55. EXISTING BUILDING
  - 56. EXISTING BUILDING
  - 57. EXISTING BUILDING
  - 58. EXISTING BUILDING
  - 59. EXISTING BUILDING
  - 60. EXISTING BUILDING
  - 61. EXISTING BUILDING
  - 62. EXISTING BUILDING
  - 63. EXISTING BUILDING
  - 64. EXISTING BUILDING
  - 65. EXISTING BUILDING
  - 66. EXISTING BUILDING
  - 67. EXISTING BUILDING
  - 68. EXISTING BUILDING
  - 69. EXISTING BUILDING
  - 70. EXISTING BUILDING
  - 71. EXISTING BUILDING
  - 72. EXISTING BUILDING
  - 73. EXISTING BUILDING
  - 74. EXISTING BUILDING
  - 75. EXISTING BUILDING
  - 76. EXISTING BUILDING
  - 77. EXISTING BUILDING
  - 78. EXISTING BUILDING
  - 79. EXISTING BUILDING
  - 80. EXISTING BUILDING
  - 81. EXISTING BUILDING
  - 82. EXISTING BUILDING
  - 83. EXISTING BUILDING
  - 84. EXISTING BUILDING
  - 85. EXISTING BUILDING
  - 86. EXISTING BUILDING
  - 87. EXISTING BUILDING
  - 88. EXISTING BUILDING
  - 89. EXISTING BUILDING
  - 90. EXISTING BUILDING
  - 91. EXISTING BUILDING
  - 92. EXISTING BUILDING
  - 93. EXISTING BUILDING
  - 94. EXISTING BUILDING
  - 95. EXISTING BUILDING
  - 96. EXISTING BUILDING
  - 97. EXISTING BUILDING
  - 98. EXISTING BUILDING
  - 99. EXISTING BUILDING
  - 100. EXISTING BUILDING
- NOTES:**
1. THIS SHEET IS TO BE USED IN CONJUNCTION WITH THE STREET LIGHTING PLAN.
  2. THE STREET LIGHTING PLAN IS THE MASTER DEVELOPMENT PLAN.
  3. THE STREET LIGHTING PLAN IS THE MASTER DEVELOPMENT PLAN.





- LEGEND**
- 1. PROPERTY BOUNDARY LINE
  - 2. PROPOSED LIGHTING FIXTURE
  - 3. PROPOSED LIGHTING POLE
  - 4. PROPOSED TRAFFIC SIGNAL
  - 5. PROPOSED TRAFFIC SIGNAL POLE
  - 6. PROPOSED TRAFFIC SIGNAL HOUSING
  - 7. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 8. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 9. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 10. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 11. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 12. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 13. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 14. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 15. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 16. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 17. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 18. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 19. PROPOSED TRAFFIC SIGNAL HOUSING POLE
  - 20. PROPOSED TRAFFIC SIGNAL HOUSING POLE
- NOTES**
1. SEE SHEET C3.01 FOR PRELIMINARY STREET LIGHTING PLAN.
  2. SEE SHEET C3.02 FOR PRELIMINARY STREET LIGHTING PLAN.
  3. SEE SHEET C3.03 FOR PRELIMINARY STREET LIGHTING PLAN.



**LEGEND**

- 1. NEW STREET LIGHTING
- 2. EXISTING STREET LIGHTING
- 3. NEW BOLLARD
- 4. EXISTING BOLLARD
- 5. NEW BOLLARD INTERSECTION
- 6. EXISTING BOLLARD INTERSECTION
- 7. NEW BOLLARD INTERSECTION
- 8. EXISTING BOLLARD INTERSECTION
- 9. NEW BOLLARD INTERSECTION
- 10. EXISTING BOLLARD INTERSECTION

**NOTES:**

1. SEE SHEET C3.04 FOR ALL OTHER LIGHTING AND BOLLARD PLACEMENTS.
2. SEE SHEET C3.06 FOR ALL OTHER LIGHTING AND BOLLARD PLACEMENTS.
3. SEE SHEET C3.07 FOR ALL OTHER LIGHTING AND BOLLARD PLACEMENTS.





Wright Runstad & Company  
1201 3rd Avenue  
Suite 2700  
Seattle, WA 98101

Shorenstein Properties LLC  
235 Montgomery St.  
16th Floor  
San Francisco, CA 94104

NBBJ  
223 Yale Avenue North  
Seattle, WA 98109

Parametrix  
1019 39th Avenue SE  
Suite 100  
Puyallup, WA 98374

Transportation Solutions, Inc.  
8250 165th Avenue NE  
Suite 100  
Redmond, WA 98052



DRAWING INDEX

BIRD'S EYE PERSPECTIVE

ANL01	Cover and Sheet Index
ANL02	Vehicle Map
ANL03	Existing Site Plan
ANL04	Existing Site Photographs
ANL05	Existing Site Photographs
ANL06	Existing Site Photographs
ANL07	Planning Diagrams
ANL08	Phase 1 Plan
ANL09	Phase 2 Plan
ANL10	Phase 3 Plan
ANL11	Phase 4 Plan
ANL12	Phase 5 Plan
ANL13	Phase 6 Plan
ANL14	Phase 7 Plan
ANL15	Phase 8 Plan
ANL16	Phase 9 Plan
ANL17	Phase 10 Plan
ANL18	Phase 11 Plan
ANL19	Phase 12 Plan
ANL20	Phase 13 Plan
ANL21	Phase 14 Plan
ANL22	Phase 15 Plan
ANL23	Phase 16 Plan
ANL24	Phase 17 Plan
ANL25	Phase 18 Plan
ANL26	Phase 19 Plan
ANL27	Phase 20 Plan
ANL28	Phase 21 Plan
ANL29	Phase 22 Plan
ANL30	Phase 23 Plan
ANL31	Phase 24 Plan
ANL32	Phase 25 Plan
ANL33	Phase 26 Plan
ANL34	Phase 27 Plan
ANL35	Phase 28 Plan
ANL36	Phase 29 Plan
ANL37	Phase 30 Plan
ANL38	Phase 31 Plan
ANL39	Phase 32 Plan
ANL40	Phase 33 Plan
ANL41	Phase 34 Plan
ANL42	Phase 35 Plan
ANL43	Phase 36 Plan
ANL44	Phase 37 Plan
ANL45	Phase 38 Plan
ANL46	Phase 39 Plan
ANL47	Phase 40 Plan
ANL48	Phase 41 Plan
ANL49	Phase 42 Plan
ANL50	Phase 43 Plan
ANL51	Phase 44 Plan
ANL52	Phase 45 Plan
ANL53	Phase 46 Plan
ANL54	Phase 47 Plan
ANL55	Phase 48 Plan
ANL56	Phase 49 Plan
ANL57	Phase 50 Plan
ANL58	Phase 51 Plan
ANL59	Phase 52 Plan
ANL60	Phase 53 Plan
ANL61	Phase 54 Plan
ANL62	Phase 55 Plan
ANL63	Phase 56 Plan
ANL64	Phase 57 Plan
ANL65	Phase 58 Plan
ANL66	Phase 59 Plan
ANL67	Phase 60 Plan
ANL68	Phase 61 Plan
ANL69	Phase 62 Plan
ANL70	Phase 63 Plan
ANL71	Phase 64 Plan
ANL72	Phase 65 Plan
ANL73	Phase 66 Plan
ANL74	Phase 67 Plan
ANL75	Phase 68 Plan
ANL76	Phase 69 Plan
ANL77	Phase 70 Plan
ANL78	Phase 71 Plan
ANL79	Phase 72 Plan
ANL80	Phase 73 Plan
ANL81	Phase 74 Plan
ANL82	Phase 75 Plan
ANL83	Phase 76 Plan
ANL84	Phase 77 Plan
ANL85	Phase 78 Plan
ANL86	Phase 79 Plan
ANL87	Phase 80 Plan
ANL88	Phase 81 Plan
ANL89	Phase 82 Plan
ANL90	Phase 83 Plan
ANL91	Phase 84 Plan
ANL92	Phase 85 Plan
ANL93	Phase 86 Plan
ANL94	Phase 87 Plan
ANL95	Phase 88 Plan
ANL96	Phase 89 Plan
ANL97	Phase 90 Plan
ANL98	Phase 91 Plan
ANL99	Phase 92 Plan
ANL100	Phase 93 Plan
ANL101	Phase 94 Plan
ANL102	Phase 95 Plan
ANL103	Phase 96 Plan
ANL104	Phase 97 Plan
ANL105	Phase 98 Plan
ANL106	Phase 99 Plan
ANL107	Phase 100 Plan



**The Spring District Master Plan**  
**FAR Amenity Calculation**

This represents maximum square feet to be built in MDP.

	Phase 1a Commercial		Phase 1b Commercial		Phase 3 Commercial		Phase 4 Commercial		Phase 5 Commercial		Phase 6 Commercial	
	fn	Remaining MDP SF	Remaining MDP SF	Current SF	Remaining MDP SF	Current SF	Remaining MDP SF	Current SF	Remaining MDP SF	Current SF	Remaining MDP SF	Current SF
Total Site Land Area		1,568,439										
Less: Residential Land Area	(1)	305,005										
Total Commercial Land Area	(1)	1,263,434	1,124,724	356,129	768,595	424,516	344,079	67,961	276,118	181,553	94,565	94,565
Total Commercial Development GSF		4,064,000	3,623,000	1,122,000	2,501,000	1,379,000	1,122,000	228,000	894,000	589,000	305,000	305,000
Less: Retail GSF		150,000	139,000	18,000	121,000	63,000	58,000	18,000	40,000	28,000	12,000	12,000
Commercial GSF Excluding Retail		3,914,000	3,484,000	1,104,000	2,380,000	1,316,000	1,064,000	210,000	854,000	561,000	293,000	293,000
Remaining Site-wide Average FAR		3.10	3.10	3.10	3.10	3.10	3.09	3.09	3.09	3.09	3.10	3.10
Base 1 FAR SF		1,263,434	1,124,724	356,129	768,595	424,516	344,079	67,961	276,118	181,553	94,565	94,565
Balance of SF Subject to Amenity System		2,650,566	2,359,276	747,871	1,611,405	891,484	719,921	142,039	577,882	379,447	198,435	198,435
Total GSF		3,914,000	3,484,000	1,104,000	2,380,000	1,316,000	1,064,000	210,000	854,000	561,000	293,000	293,000

**Footnotes**

(1) Residential land area does not include any allocation of the tract area. Tract area is allocated 100% to commercial land area.



*Callahan  
4/26/12*

## SEPA CHECKLIST

## BACKGROUND INFORMATION

Property owner: WR – SRI 120<sup>th</sup> LLC

Proponent: WRC

Contact person: Cindy Edens, Wright Runstad &amp; Company

Address: 1201 3<sup>rd</sup> Avenue, Suite 2700, Seattle, WA 98101

Phone: (206) 447-9000

Proposal Title: The Spring District Master Development Plan

Proposal Location: BelRed Corridor, Bellevue, WA

Vicinity Map: Attached

General description: The Spring District development encompasses 36.01 acres in the BelRed Subarea. The proposed redevelopment of the industrial complex will transform the area into a transit-oriented, mixed use development containing office space, neighborhood retail space, and housing units.

1. Acreage of site: 36.01
2. Number of dwelling units/buildings to be demolished: 6 buildings
3. Number of dwelling units/buildings to be constructed: 29 buildings
4. Square footage of buildings to be demolished: 700,327 SF
5. Square footage of buildings to be constructed: up to 5,293,764 SF *5.4 million GSF*
6. Quantity of earth movement (in cubic yards): 1,630,000 CY
7. Proposed land use: Mixed use residential, office, retail space, open space
8. Design features, including building height, number of stories, and proposed exterior materials:

Design features of The Spring District include residential, office, a hotel, and retail development, complemented by a network of parks and open spaces. The design will encourage the use of public transit with pedestrian and bicycle facilities and accessibility to the Sound Transit Light Rail Transit (LRT) station at the north end of the site. The residential buildings will likely include five stories of residences over two stories of underground parking. The maximum building height for the development is 150-feet, plus 15-feet for HVAC.

*Building heights range 11-14 stories.*

Individual building permits will be obtained through the administrative design review, during which time the specific design features, building height, stories, and exterior materials will be presented.

✓

**Received****MAR - 2 2012**
**THE SPRING DISTRICT**  
 MASTER DEVELOPMENT
**C**

9. Other  
None.

Estimated date of completion of the proposal or timing of phasing:

Phase 1a construction is expected to begin as early as the first quarter 2012 with full build-out expected in the next 15 years.

Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

The development will be completed in seven distinct phases, as described generally next.

- Phase 1a includes the first residential buildings along the south end of the property, two office buildings with ground-floor retail, and an interim park to meet park space requirements per the Development Agreement.
- Phase 1b includes the addition of more office development and ground-floor retail space;
- Phase 2 includes City roadway improvements, the arrival of the Sound Transit Light Rail Transit (LRT) station, and the permanent placement of the active and passive park spaces;
- Phase 3 includes additional office and retail space;
- Phase 4 adds a landmark hotel that will provide an additional entry to the LRT station;
- Phase 5 adds development north of NE 16<sup>th</sup> Street including residential and office/retail space; and
- Phase 6 adds the final office building and residential complex.

*See  
staff  
report  
page  
8*

Each phase of development will go through administrative design review and will be subject to applicable regulations and policies in effect at the time of application.

List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

An FEIS for the BelRed Corridor Project was issued by the City of Bellevue in July of 2007. The FEIS designates a Preferred Alternative, identified by the BelRed Steering Committee in May 2007, which would increase density in the western half of the BelRed Corridor by including three closely spaced development nodes in the vicinity of Overlake Hospital Medical Center (OHMC), 122<sup>nd</sup>, and 130<sup>th</sup> Avenues NE.

Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.

The Sound Transit East Link project will affect this proposal. The light rail alignment will transect the project site between NE 15<sup>th</sup> Street and NE 16<sup>th</sup> Street.

✓

List any government approvals or permits that will be needed for your proposal, if known. If permits have been applied for, list application date and file numbers, if known.

In addition to the Administration Design Approval in accordance with the Master Development Plan, each phase of development will require local permits, including building, clearing and grading, and utilities as well as coverage under Ecology's stormwater general NPDES permit will also be needed.

Please provide one or more of the following exhibits, if applicable to your proposal. (Please check appropriate box(es) for exhibits submitted with your proposal):

- Land Use Reclassification (rezone) map of existing and proposed zoning
- Preliminary Plat or Planned Unit Development Preliminary plat map
- Clearing and Grading Permit
- Plan of existing and proposed grading
- Development Plans
- Building Permit (or Design Review)
- Site Plan \*See accompanying Master Development Plan submittal documents
- Clearing and Grading Plan
- Shoreline Management Permit
- Site Plan

#### A. ENVIRONMENTAL ELEMENTS

##### 1. Earth

a) General description of the site (circle one): **Flat**, rolling, hilly, steep slopes, mountainous, other....

b) What is the steepest slope on the site (approximate percent slope)?

The affected geographic area is generally flat, with the exception of a man made steep slope located at the western edge of the site along 120<sup>th</sup> Avenue NE. The slope is up to 50-percent in places.



- c) What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck?) If you know the classification of agricultural soils, specify them and note any prime farmland.

A geotechnical engineering report dated November 11, 2005 found significant fill exists on site consisting of medium dense sand to silty sand and medium stiff to hard sandy silt with varying amounts of gravel and organics. Glacial till was encountered at depths of about 13 feet. ✓

- d) Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe.

There are no known indications of or history of unstable soils in the immediate vicinity. The slope along the east side of 120<sup>th</sup> Ave NE at the intersection with NE 12<sup>th</sup> Street is a man-made slope with no visible or known history of instability.

- e) Describe the purpose, type, and approximate quantities of any filling or grading proposed. Indicate source of fill.

Proposed earthwork includes the excavation of approximately 1.5 million CY of material for construction of underground parking garages. Of this material 130,000 CY will be used for fill within the site, if suitable. Fill trucked in from off-site will come from approved suppliers.

- f) Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

The possibility for erosion will be minimized or eliminated through the use of Best Management Practices (BMPs), including an erosion control plan prepared in accordance with City of Bellevue standards and the Stormwater Management Manual for Western Washington. In addition, construction timing, erosion control fencing, and other devices and methods will be employed to ensure erosion potential is minimized.

- g) About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

At full build-out, The Spring District development will be covered with no more than 75% impervious surfaces, such as asphalt and buildings. *per Code*

- h) Proposed measures to reduce or control erosion, or other impacts to the earth, if any:

An erosion control plan will be prepared in accordance with City of Bellevue standards and the Stormwater Management Manual for Western Washington. In addition, construction timing, erosion control fencing, and other devices and methods will be employed to ensure erosion potential is minimized.

*& recommendations per geotech engineer of record*

2. Air

- a) What types of emissions to the air would result from the proposal (i.e., dust, automobile, odors, industrial wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known.

During construction, emissions to the air will be released by construction vehicles and heavy equipment. Following construction, emissions from residents' vehicles will be released. Construction would temporarily increase dust and vehicle emissions near the construction area. Mitigation would include using BMPs to control dust, covering exposed soils, and requiring idling vehicles to be shut off. The BelRed Corridor FEIS predicts that as a result of increased traffic in the study area, carbon monoxide emissions would increase by about 40 percent over No-Action Alternative, and emissions of particulates would increase by about 30 percent. It also states these emissions are not expected to violate air quality standards. The greenhouse gas emissions calculations and mitigation options are described in the Technical Memorandum at the end of this checklist.

*See staff report pages 48-49 re: discussion about anticipated greenhouse gas emissions. Technical*

- b) Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.

There are no known off-site sources of emissions or odor that would affect this proposal.

*memorandum in file by Parametric Inc. dated 2/10/12 and updated 3/28/12*

- c) Proposed measures to reduce or control emissions or other impacts to air, if any:

Construction vehicles will be fitted with required, factory-installed emission control devices. To reduce the potential of dust, construction accesses will be covered with rock or aggregate. Dust emissions will also be reduced during construction through the use of spray water as necessary during dry weather conditions and planting disturbed areas with erosion control seed mix as soon as is practical. Material stockpiles will also be covered or watered as necessary to control dust.

*Dust suppressant measures per BCC 23.76 as imposed on Clearing & Grading Permit.*

3. Water

1. Surface:

- a) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

The Spring District development area is approximately 400 feet northeast of Lake Bellevue. There is one Category III wetland located directly to the southwest of the property along 120th Avenue NE. The wetland flows to Lake Bellevue, which flows to Sturtevant Creek to the southwest. Kelsey Creek is located approximately 300 feet northeast of the site.

*→ This is an area of work for the 120th Avenue NE Phases 2+3 road widening project*

- b) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.

The development will not require any work over, in, or adjacent to any body of water.



- c) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.  
No fill or dredge material will be placed in or removed from surface waters or wetlands as a result of the proposal. The wetland noted above will be filled and mitigated as part of the City's 120<sup>th</sup> Avenue NE Phases 2 and 3 widening project.
- d) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.  
The proposal will not require surface water withdrawals or diversions. A 2005 geotechnical engineering report did not encounter groundwater during boring explorations and concluded the site has a relatively deep groundwater table.
- e) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.  
According to FEMA Flood Insurance Rate Maps, Community Panel numbers 53033C0368F and 53033C0656F (eff. May 16, 1995), the affected geographic area is not within the 100-year floodplain.
- f) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.  
Stormwater from rooftops and roadways will be collected, treated, and conveyed through approved systems that eventually discharge to Lake Bellevue.

## b. Ground:

- 1) Will ground water be withdrawn, or will water be discharged to ground water? Give general description, purpose, and approximate quantities if known.  
This Proposal does not involve withdrawals of or discharges to groundwater.
- 2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.  
This Proposal does not include the discharge of waste materials into the ground from septic tanks or other sources. The residences, offices, and commercial/retail space within the development will be served by the City's public sewer system.

## c. Water runoff (including stormwater):

- 1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow?  
Will this water flow into other waters? If so, describe.  
Stormwater runoff will be generated by rooftops, driveways, and roadways. This runoff will be collected, treated, and will outfall to Lake Bellevue by means of an approved drainage system designed in accordance with the Stormwater Management Manual for Western Washington and City of Bellevue regulations.
- 2) Could waste materials enter ground or surface waters? If so, generally describe.  
It is not anticipated that waste materials will enter ground or surface waters associated with this proposal.



3) Proposed measures to reduce or control surface, ground, and runoff water impacts, if any:

The proposal will comply with all applicable requirements of the Drainage Design & Erosion Control manual and applicable stormwater manual. In addition, the project will include preparation of an IPMP for additional protection against ground and surface water contamination or pollution.

To reduce the amount of stormwater runoff, natural drainage practices will be implemented, including rain gardens and pervious concrete where appropriate. Internal, private roadways are narrower than standard street sections, reducing the use of asphalt pavement and therefore reducing runoff. During construction, contractors will be required to have a Spill Prevention Control and Countermeasure plans and a Stormwater Pollution Prevention Plan (SWPPP) in place. Stormwater systems will be designed and operated in accordance with relevant standards and requirements and will be treated prior to discharge into an approved system.

*The City's Utility Codes and Engineering Standards will be imposed with Developer Extension Agreement BCC 24.02, 24.04, BCC 24.06*

4. Plants

a. Check or circle types of vegetation found on the site:

- deciduous tree: alder, maple, aspen, other
- evergreen tree: fir, cedar, pine, other
- shrubs
- grass
- pasture
- crop or grain
- wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other
- water plants: water lily, eelgrass, milfoil, other
- other types of vegetation

b. What kind and amount of vegetation will be removed or altered?

The majority of the existing vegetation on site, which includes deciduous trees and shrubs, will be removed.

c. List threatened or endangered species known to be on or near the site.

There are no threatened or endangered species known to occur on or near the site.

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

There is limited vegetation currently on the site. While future development of the site will require the removal of deciduous trees and shrubs, the development includes significant landscaping, including mature street trees, open spaces, and parks.

*Landscaping required per the Land Use Code 20.25D*

5. Animals

a. Circle any birds and animals which have been observed on or near the site or are known to be on or near the site:



birds: hawk, heron, eagle, **songbirds**, other:  
 mammals: deer, bear, elk, beaver, other:  
 fish: bass, salmon, trout, herring, shellfish, other:

b. List any threatened or endangered species known to be on or near the site.  
 There are no threatened or endangered species known to occur on or near the site.

c. Is the site part of a migration route? If so, explain.  
 Yes, however, most of Western Washington is generally located in the Pacific Flyway for migratory waterfowl.

d. Proposed measures to preserve or enhance wildlife, if any:  
 As there is no known wildlife on the site, no preservation measures are needed. ~~Future development proposals will be subject to review under applicable regulations and Comprehensive Plan policies in effect at the time of application.~~ *Future landscaping will provide opportunities for small urban animals, such as squirrels, moles, and native birds.*

6. Energy and natural resources

a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.  
 The development will require electricity and natural gas energy for heating/cooling associated with residential, office, and commercial/retail development needs.

b. Would your project affect the potential use of solar energy by adjacent properties?  
 If so, generally describe.  
 It is not likely the development will affect the potential use of solar energy by adjacent properties. The proposal will not produce shadows to the north nor shade other adjacent properties.

c. What kinds of energy conservation features are included in the plans of this proposal?  
 List other proposed measures to reduce or control energy impacts, if any:  
 This proposal is being designed to encourage multimodal transportation, which should reduce the amount of fossil fuels used for transportation. Residential structures will be constructed in accordance with International Building Codes and Washington State Energy Code standards.

7. Environmental health

a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal?  
 If so, describe.  
 There is an ammonia tank on-site for use associated with the cold storage facility. This tank will remain on-site until the demolition of the cold storage facility. Several underground storage sites have been removed and no groundwater contamination was identified. The site received a No Further Action determination from Ecology in 2009.



As with all sites, there may be a risk of spills during construction.

1) Describe special emergency services that might be required.

The need for special emergency services is not anticipated. Non-residential buildings are limited to offices and retail/hotel usage. Facilities storing or processing toxic chemicals are not part of this proposal.

2) Proposed measures to reduce or control environmental health hazards, if any:

Spill Prevention and Control Plans will be utilized by contractors working on-site during construction.

b. Noise

1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

Noise from nearby roadways exists, including freeways I-405 and SR-520 and major arterials 124th Avenue NE and NE 12th Street. Noise from these facilities and other surrounding uses is standard roadway noise and will not affect the proposal.

*124th Ave NE =  
Main Arterial* ←

2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)?

Indicate what hours noise would come from the site.

During the phasing of development, the site will produce short-term construction noise. The BelRed Corridor FEIS states that long-term noise impacts would be similar to the No-Action Alternative (70 to 72 dBA) in areas proposed for residential development.

*per Noise Control BCC 9.18*

3) Proposed measures to reduce or control noise impacts, if any:

Motorized construction equipment will be properly fitted with mufflers to reduce engine noise associated with short-term construction noise. For long-term noise control, the site could employ building and site design measures, including landscaped buffers and other sound-proofing techniques. In addition, the development is designed and oriented for pedestrians and the use of public transit. This focus will reduce the amount of vehicles in The Spring District and noise associated with vehicular traffic.

*Noise control per BCC 9.18*

8. Land and shoreline use

a. What is the current use of the site and adjacent properties?

The development area currently contains warehouse and storage buildings. The uses on-site include manufacturing, warehouse, distribution, and accessory uses. Adjacent properties include warehouse, office, commercial, and residential uses.

b. Has the site been used for agriculture? If so, describe.

Prior to the 1960's when Safeway became the first urban user, the BelRed Corridor area was used for agricultural production. Since that time, the site has been used for light industrial uses.

c. Describe any structures on the site.

There are currently six structures on site, totaling 700,000 SF. The structures are utilized for a mixture of manufacturing, warehouse, distribution, and accessory uses.

d. Will any structures be demolished? If so, what?



All existing buildings on the site will eventually be demolished as part of the site development. The buildings will remain operational until demolition.

e. What is the current zoning classification of the site?

As of 2009, the site was rezoned from light industrial to office/residential.

BR-OR-1  
BR-OR-2

f. What is the current comprehensive plan designation of the site?

The current comprehensive plan designation is mixed-use office/residential.

same ↑

g. If applicable, what is the current shoreline master program designation of the site?

Not applicable.

h. Has any part of the site been classified as an "environmentally sensitive" area? If so, specify.

No, other than a potentially steep slope in the vicinity as described earlier in this checklist.

+ wetland adjacent to 120th Ave NE.

i. Approximately how many people would reside or work in the completed project?

The Spring District will house approximately 3,000 residents and employ approximately 200 retail and hotel workers and 18,560 office workers within the project.

j. Approximately how many people would the completed project displace?

The Proposal will not displace any residents as the current use of the site is industrial with no residences. The current tenants of the site currently employ approximately 350 workers. These workers will be displaced as the buildings are demolished.

k. Proposed measures to avoid or reduce displacement impacts, if any:

The applicant is not proposing any measures to avoid displacement impacts. The Spring District's site and utility design will support the continued use of the industrial buildings until such time as each building is demolished. In the BelRed Corridor FEIS, the City considers potential mitigation for the displacement of industrial workers to include City assistance in finding relocation opportunities in the corridor or elsewhere in Bellevue.

l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

This Proposal is compatible with the City's existing comprehensive plan, the FEIS for the BelRed Corridor Project. Alignment with these plans ensures compatibility with existing and projected land use plans. Any future development that may be proposed within the BelRed Corridor and/or the affected geographic area would be reviewed for compliance with existing regulations in place at the time of the application.

9. Housing

a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing.



The development will construct up to approximately 1,200,000 square feet of multifamily housing. Using as assumption that units will average 1,000 square feet each, this calls for up to 1,200 units and generally priced at levels consistent with current market. The Amenity Incentive System requires the first 1.25 FAR above the base of 1.0 be earned through affordable housing. If rental, affordable housing will be provided at the 80 percent median income, if ownership, affordable housing will be provided at 100 percent median income. Actual number of units will depend on how the Incentive System is used.

b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.

This Proposal will not eliminate any existing housing units as none are currently on-site.

c. Proposed measures to reduce or control housing impacts, if any:

The Proposal will not have an impact on existing housing units, and therefore no housing impact reduction or control is necessary.

## 10. Aesthetics

a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

The building design for the Spring District is not complete. However, land use zoning within the project site allows buildings up to 150 feet tall. The project will follow existing Bellevue Land Use Code along with any future code amendments. There is no proposed building material proposed to date. The development permits will be issued by administrative design review, at which time the exterior building materials will be evaluated for alignment with current regulations and Comprehensive Plan requirements.

b. What views in the immediate vicinity would be altered or obstructed?

The BelRed Corridor FEIS included a view/visual analysis component. The analysis found that taller buildings on the ridgetop location of The Spring District would be prominently visible from several public vantage points. From City Hall and the western terminus of the SR-520 Trail at NE 24th Street, these buildings would intersect the distant ridge lines but would not block significant views, such as of Mount Rainier. Closer to the transit node, at the public vantage points on BelRed Road and on 124th Avenue NE, the buildings would be prominent but would not block significant views.

c. Proposed measures to reduce or control aesthetic impacts, if any:

Streetscapes, buildings, and accesses to and views of specific natural environments on the site have been carefully considered. Approximately 25% of the overall project area will be dedicated to active and passive open space and landscaping.

## 11. Light and glare

a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

New residences and businesses along with street lighting and traffic on the roadway network will increase light and glare at night.

b. Could light or glare from the finished project be a safety hazard or interfere with views?

It is not anticipated that light or glare from this project will be a safety hazard or interfere with views. ✓

c. What existing off-site sources of light or glare may affect your proposal?

There are no known off-site sources of light or glare that would affect the development.

d. Proposed measures to reduce or control light and glare impacts, if any:

Exterior lighting will meet City design standards and cast light downward. Future development proposals will be subject to review under applicable regulations and Comprehensive Plan policies in effect at the time of application.

*Light + glare subject to LUC 20.20.522.*

## 12. Recreation

a. What designated and informal recreational opportunities are in the immediate vicinity?

Wilburton Hill Park and Botanical Gardens and Kelsey Creek Park are located approximately 3/4 miles to 1 mile from the Spring District site.

b. Would the proposed project displace any existing recreational uses? If so, describe.

The development will not displace any existing recreational uses.

c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

The project will create at least 1.68 acres of active and passive open space and parks within the development, which exceeds the City's requirements in accordance with the Development Agreement.

## 13. Historic and cultural preservation

a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? If so, generally describe.

The Washington State Department of Archaeology and Historic Preservation online GIS map tool does not indicate there are any places or objects listed on any registers within the immediate vicinity of the project site.

b. Generally describe any landmarks or evidence of historic, archaeological, scientific, or cultural importance known to be on or next to the site.

None known.

c. Proposed measures to reduce or control impacts, if any:

The development will not have any impact on historical or cultural landmarks.

*Approx. 1.74 A  
See page 58  
of the st.  
rpt.*

## 14. Transportation

a. Identify public streets and highways serving the site, and describe proposed access to the existing street system. Show on site plans, if any.

The project site is generally served by NE 12<sup>th</sup> Street, 124<sup>th</sup> Avenue NE, and 120<sup>th</sup> Avenue NE. At full build-out the streets providing access to the site will include 120<sup>th</sup> Avenue NE, 124<sup>th</sup> Avenue NE, the future NE 15<sup>th</sup> Street, and NE 16<sup>th</sup> Street. These local streets are shown on the site plan and accompanying Master Development Plan documents. Freeway access includes SR-520 located north of the site and I-405 to the west.



b. Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop?

The project site is not served by public transit, but public transit, King County Metro, serves the vicinity with bus service, including:

- Route MT 226-O: - approximately 0.1 miles from the project site
- Route MT 249-O: approximately 0.3 miles from the project site
- Route MT 672-O, MT 889-O: approximately 0.3 miles from the project site
- King County Rapid Ride B-Line: approximately 0.3 miles from the project site

c. How many parking spaces would the completed project have? How many would the project eliminate?

At full build-out, The Spring District will have up to approximately 10,034 parking spaces. The current parking is directly associated with the current uses of the site. As the buildings are taken out of operation and demolished, the need for associated parking for those buildings will also be eliminated.

*Parking required per LUC 20.25D.*

d. Will the proposal require any new roads or streets, or improvements to existing roads or streets, not including driveways? If so, generally describe (indicate whether public or private).

The development of The Spring District will include the addition of private roads classified as local streets internal to the development. The development will also coordinate with planned improvements to 120<sup>th</sup> Avenue NE and 124<sup>th</sup> Avenue NE to provide frontage improvements at access points. One tract will be conveyed to the City for a public street (NE 15<sup>th</sup> Street). It is anticipated the construction of NE 15<sup>th</sup> Street will also be a City-led project.

*See Transportation discussion under Section VII SEPA and Section V Technical Review.*

e. Will the project use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

The development does not use or occur in the immediate vicinity of current water, rail, or air transportation.

f. How many vehicular trips per day would be generated by the completed project? If known, indicate when peak volumes would occur.

The development at build out is forecasted to generate up to 37,000 vehicle trips on a weekday and up to approximately 3,721 trips during the PM peak hour when commuter traffic volumes peak and the potential for congestion is at its greatest."

*See Transportation discussion under Sections VII SEPA & Section V Technical Review*

g. Proposed measures to reduce or control transportation impacts, if any:

The Bellevue City Code (14.60.070) establishes transportation management program requirements that will apply to development within The Spring District. Specific plan elements will be developed as part of the administrative design review for each development.

As a transit-oriented development, there will be additional measures such as signage for non-motorized travel modes and marketing activities to promote vehicle trip reduction within the District. The City's BelRed Corridor FEIS proposes King County Metro Route 233 be routed along NE 15<sup>th</sup> Street through The Spring District, further increasing public transit options.

With the addition of the Sound Transit LRT in Phase 2 of the development, vehicle trips are expected to decline as residents, employees, and visitors take advantage of the proximity of light rail. The development focuses on pedestrian connections to increase accessibility to the station.

The non-motorized experience will include a comprehensive sidewalk and trail system, including

wide sidewalks, pedestrian plazas, shared use lanes, bicycle lanes, and through-block pedestrian connections. The pedestrian/bicycle trail that currently terminates near the project site will be extended along NE 16<sup>th</sup> Street, further increasing non-motorized options for residents and employees within the District.

New traffic associated with the development is expected to impact offsite transportation facilities during the AM and PM weekday peak hours. The City of Bellevue has identified roadway improvements needed to support the BelRed Corridor Plan vision and to accommodate the Sound Transit East Link project.

Roadway improvements adjacent to The Spring District include:

1. 120<sup>th</sup> Avenue NE project - currently in the design stage, this project will widen the roadway along the west side of the project site. This widening project, identified in the City's BelRed Corridor FEIS, will accommodate increased density and vehicle trips associated with new development nodes in the corridor, including The Spring District.
2. 124th Avenue NE project - the project includes improvements to 124th Ave NE between the planned NE 15th/16th Street and Northup Way by widening to a four lane arterial with a two-way left-turn lane, sidewalks, and landscaping. The Spring District will coordinate with this City-led project to provide site access points along the roadway as well as curb, gutter, and sidewalk along The Spring District site. This project is also expected to be complete during Phase 2 of The Spring District development.

*See Transportation discussion under Section VI. SEPA & Section V Technical Review.*

15. Public services

a. Would the project result in an increased need for public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe.  
 There will be an increase in demand for all public services including additional students for local schools.

b. Proposed measures to reduce or control direct impacts on public services, if any.  
 Increased tax base for the District will offset additional public services needed for the district. Future development proposals will be subject to review under applicable regulations and Comprehensive Plan policies in effect at the time of application.

16. Utilities

a. Circle utilities currently available at the site: **electricity, natural gas, water, refuse service, telephone, sanitary sewer**, septic system, other.

b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

City sewer and water will be extended to serve the demands of the proposal. Telephone service will be provided by CenturyLink Communications and electricity will be provided by Puget Sound Energy. The BelRed Corridor FEIS predicts that demand for utilities would increase substantially over the No-Action Alternative; however, the increases are not expected to result in the need for significant capacity increases by utility providers.

*See discussion under Utilities - Section V Technical Review.*

✓

C. Signature

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature: Cindy Edens

Date Submitted: 3/2/12



RECORDED AT THE REQUEST OF:  
AND AFTER RECORDING RETURN TO:

FOSTER PEPPER PLLC  
Attention: Tayloe J. Washburn  
1111 Third Avenue, Suite 3400  
Seattle, WA 98101-3299



**WASHINGTON STATE RECORDER'S Cover Sheet (RCW 65.04)**

<b>DOCUMENT TITLE(S)</b> (or transactions contained therein):  Development Agreement
<b>REFERENCE NUMBER(S) OF DOCUMENTS ASSIGNED OR RELEASED:</b>  N/A <input type="checkbox"/> Additional reference #s on page ___ of document(s)
<b>GRANTOR(S)</b> (Last name first, then first name and initials)  WR-SRI 120th LLC, a Delaware Limited Liability company  <input type="checkbox"/> Additional names on page ___ of document
<b>GRANTEE(S)</b> (Last name first, then first name and initials)  City of Bellevue, a municipal corporation <input type="checkbox"/> Additional names on page ___ of document
<b>LEGAL DESCRIPTION</b> (abbreviated: i.e., lot, block, plat or section, township, range)  Lot 3, City of Bellevue Short Plat No. LF06-135856  <input type="checkbox"/> Additional legal is on Exhibit A of document
<b>ASSESSOR'S PROPERTY TAX PARCEL/ACCOUNT NUMBER</b>  1099100102 <input type="checkbox"/> Assessor Tax # not yet assigned

D

## DEVELOPMENT AGREEMENT

**THIS DEVELOPMENT AGREEMENT** (“Agreement” or “Development Agreement”) is entered into as of the effective date, by and between the **CITY OF BELLEVUE**, a Washington municipal corporation (“City”) and **WR-SRI 120th LLC**, a Delaware limited liability company.

### RECITALS

- A. Intent. This Development Agreement is for the purposes of setting forth the applicable development standards and other provisions related to the development described herein, and is adopted pursuant to the authority provided in RCW 36.70B.170 *et seq.*
- B. The City Council, in Ordinance 5858, adopted the Bel-Red Subarea Plan establishing a vision for conversion of the historic pattern of light-industrial land use to office, retail and residential development at densities supportive of high capacity transit. The Bel-Red Subarea Plan is implemented through a package of Land Use Code regulations, adopted in Ordinances 5874 and 5876, and an area-wide rezone of the Subarea, adopted in Ordinance 5875.
- C. WR-SRI 120th LLC (“Owner”), owns a 36.01 acre parcel, King County Parcel No.1099100102 (the “Property”) located in a Bel-Red Sub-Area office/residential development node zoned BR-OR-1 and BR-OR-2, as legally described on Exhibit A attached hereto and incorporated herein. This site is within one of the areas identified for concentrated development in a Bel-Red Subarea “node.” The Owner proposes to develop what it has named “The Spring District” (“TSD”) as a Catalyst Project under LUC 20.25D.035 of the City’s Bel-Red Subarea provisions aimed at promoting urban revitalization through timely initial redevelopment. To be considered a Catalyst Project, TSD must be submitted as a phased master development plan (“MDP”) for approval and permitting purposes by the later of 18 months after the Federal Transit Authority issues a Record of Decision on the Sound Transit East Link Project or December 31, 2011. A mixed-use urban community, The Spring District will include office, residential, and retail components. The Owner proposes to include in The Spring District other major public amenities, such as open spaces, and pedestrian plazas.
- D. The Spring District proposal has the potential to embody many of the principles and vision for the corridor articulated by the Bel-Red Steering Committee in its September 2007 Bel-Red Corridor Project Final Report and the City Council in adopting the Bel-Red Subarea Plan and implementing Bel-Red development regulations. The City Comprehensive Plan puts an emphasis on dense nodal development in order to promote energy efficiency and reduce reliance on cars.
- E. While the Owner believes that a market will come to exist in the next several years for both commercial and multifamily development at the Bel-Red project site, the transitional nature of the Bel-Red Corridor, coupled with the scope and duration of TSD, introduces significant additional risk and uncertainty. The Owner must make a large up-front capital investment in

project design and planning in order to prepare an MDP under LUC Part 20.30V and subsequent development applications. Over its full term, the Owner projects the development will cost well in excess of \$1 Billion. Accordingly, and in order to proceed, the Owner needs to have a high level of certainty regarding regulatory approvals and project costs over an extended period of time, particularly given the lengthy approval and development process for Sound Transit's East Link Project.

- F. The City has issued the Bel-Red Corridor EIS and Addenda (collectively referred to as "EIS"). The EIS analyzes at a programmatic level the impacts of substantial new office, residential, and retail development throughout the Bel-Red corridor. The EIS also assesses the impacts of significant office development in two development nodes within the corridor, including the area in and around the Property.
- G. The City and the Owner acknowledge the importance of developing TSD as a mixed-use neighborhood with significant residential development. To that end, the City and the Owner recognize that the involvement of a residential developer may help in creating a successful project under this Development Agreement.
- H. Development of TSD will meet key objectives of the City embodied in the Comprehensive Plan, the Bel-Red Subarea Plan, the Land Use Code, and other existing city regulations. Specifically, the development of TSD will provide many benefits to the City and the public including, but not limited to, 1) encouraging further redevelopment of the Bel-Red Subarea by providing a central neighborhood and absorbing the substantial risk of being a Catalyst developer in the Sub-Area; 2) enhancing public improvements and infrastructure in an underutilized and underdeveloped area of the city; 3) strengthening the city's economic base with a variety of long-term jobs and near- and long-term construction jobs; and 4) generating substantial City revenues in the form of fees, increased property tax base, and sales tax generation. The City Council therefore finds significant public benefit results from execution of this Development Agreement including, among other things: 1) providing certainty to encourage the required substantial private investment in the planning and development of TSD in years earlier than may otherwise occur; 2) securing orderly development and progressive fiscal benefits for public services, improvements, and facilities planning in the city; 3) ensuring development of certain public amenities in early phases of TSD; 4) providing greater certainty surrounding the timing and amount of residential development in TSD, recognizing that the viability of significant residential development relies on high capacity transit service through the area; and 5) fulfilling and implementing adopted City plans, goals, policies and objectives, including, among others, those embodied in the City's Bel-Red Subarea Plan.
- I. The Development Agreement component of the Legislature's 1995 Regulatory Reform legislation provides a flexible tool with which the City can enter into agreements with property owners for a variety of purposes, broadly authorized in the statute. The legislative finding to this state law, RCW 36.70B. 170-200, emphasizes the challenges posed by lack of predictability in the permitting of development projects: *"The legislature finds that the lack of certainty in the approval of development projects can result in a waste of public and private resources, escalate housing costs for consumers and discourage the commitment to*

*comprehensive planning which would make maximum use of resources at the least economic cost to the public....".*

- J. A Development Agreement can commit the City for the duration of the Development Agreement to vesting review procedures and standards for implementing decisions, phasing, mitigation measures, development conditions, permitted uses, residential and commercial intensities and "any other appropriate development requirement or procedure."

**NOW, THEREFORE**, in consideration of the mutual agreements contained herein, as well as other valuable consideration, the receipt and sufficiency of which are hereby mutually acknowledged, the City and WR-SRI 120th LLC hereby agree as follows:

## **AGREEMENT**

### **A. Spring District Development Components.**

1. **Must Meet Catalyst Project Criteria.** In order to be eligible for the modifications to the Land Use Code and other regulations described in this Agreement, Owner must submit an MDP within the time frame required for "catalyst projects" as set forth in the Land Use Code, and such MDP shall demonstrate that the development proposed meets the definition of catalyst project set forth in LUC Section 20.25D.035.

2. **Minimum Contents of MDP.** Owner agrees to include in its application for an MDP development across the Property with a minimum average FAR of 2.5, which shall include the Residentially-restricted Property (as defined in Section H.) In addition, the first phase of any MDP approved under this Agreement shall include development of a public mini-park a minimum of one acre in size (designated as project M3 in the Bel-Red Parks and Open Space Project List in the Comprehensive Plan) and an activated park or recreation space of at least 30,000 contiguous square feet.

### **B. Development Standards and Vesting Period.**

1. **Master Development Plan Application.** As of the Effective Date of this Agreement, until issuance of the MDP approval consistent with the minimum requirements of Section A above, the provisions of this Agreement, and the Governing Regulations specified in Section C below, shall apply to and govern and vest the review and approval, including associated State Environmental Policy Act (SEPA) review, of the MDP application.

2. **Subsequent Land Use Review.** If the MDP application is approved, then such approval shall be vested for a period of fifteen (15) years from the date of the final decision (as defined in Section 20.35.045 of the LUC) on the MDP (the "Vesting Period"). During the Vesting Period and subject to Section T, the City shall not impose any modification of or new or additional Governing Regulations on the MDP or any Land Use Code approvals required for TSD consistent with this Agreement and the MDP. To the extent that neither this Agreement nor the Governing Regulations specified below address a certain subject, element or condition of the Project, then the Project shall be governed by the City's then-existing code.

3. **Approvals Eligible for Extended Vesting.** The Vesting Period shall only apply to Catalyst Projects on the Property, as defined in LUC 20.25D.035, and associated applications, decisions, and permits. Associated applications, decisions, and permits include those submitted with or during a Catalyst Project MDP application and those related applications submitted subsequent to Catalyst Project MDP approval. All other projects, development, and uses on the Property shall be governed without the benefit of this Development Agreement except as indicated in Section E, below.

**C. Governing Regulations.**

1. **Designation of Governing Regulations.** Except as specified otherwise herein, the existing city development regulations that govern development of the Property and shall be considered vested pursuant to the provisions of this Agreement, include and are limited to the following as they exist on the Effective Date of this Agreement (collectively, the "Governing Regulations"):

- Title 20 – Land Use Code (except process-related provisions, including Part 20.30 and Part 20.35)
- Title 21 – Comprehensive Plan to the extent applied to TSD through the provisions of the LUC

The Parties agree that SEPA applies to permits that will be issued during the Vesting Period. The City shall not exercise its substantive SEPA authority to impose conditions on Land Use Code approvals issued during the Vesting Period in a manner that is inconsistent with the Governing Regulations.

Subject to Section T, these Governing Regulations shall be applied to the Property and TSD during the Vesting Period, except as indicated below.

Except as otherwise specifically provided herein, Owners shall comply with all city ordinances, regulations, development standards and policies in effect at the time of application or issuance of an approval, as the case may be.

2. **Revised Governing Regulations After Sound Transit Operational.** On or after the date Sound Transit's East Link Project or other high-capacity transit operating within a dedicated transit-only right-of-way begins regularly scheduled passenger-carrying service to a transit station within the Property, any application for design review or other required Process II permit under the LUC on the Property shall be subject to the following code provisions (or substitute code sections specifically designated as such by city ordinance) as they exist on the date of issuance of the design review or other Process II decision: LUC 20.25D.030.C (Design Review), LUC 20.25D.110 (Landscape Development), LUC 20.25D.120 (Parking/Circulation), LUC 20.25D.130 (Bel-Red Development Standards), LUC 20.25D.140 (Bel-Red Street Development Standards), LUC 20.25D.150 (Design Guidelines) (collectively referred to as the "Revised Governing Regulations").

3. **Revisions to the Master Development Plan.** Owner acknowledges that it may be required to modify the MDP in order to remain consistent with the Revised Governing

Regulations. Review of such modification shall be based on the Governing Regulations and Revised Governing Regulations. Notwithstanding the provisions of Land Use Code Section 20.30V.160, any other modification, revision or amendment proposed to the MDP during the Vesting Period shall be reviewed against the Governing Regulations and Revised Governing Regulations, so long as the proposed modification, revision or amendment is consistent with the general scope, purpose and intent of this Agreement and the original MDP.

Approval of any modification or revision to the MDP shall not extend the expiration date of the Vesting Period.

**D. No Approval of Project-related Actions**

The Owner has not made any development proposal relating to The Spring District, and plans to do so consistent with the Catalyst Project provisions of Chapter 20.25D and other applicable provisions of the Bellevue Land Use Code. The execution of this Development Agreement does not, in and of itself, permit any development at this time.

Nothing in this Agreement shall be interpreted to limit the exercise by City of its regulatory powers with respect to any development proposal on the Property, including The Spring District MDP or other regulatory matters in accordance with applicable law. Nor shall this Agreement be interpreted as: a) a determination as to the consistency of The Spring District MDP with applicable plans, codes and ordinances, b) an agreement or commitment to approve any or all development on the Property, nor c) any commitment whatsoever by City with respect to any future City discretionary decisions that may be required for development of the Property. A permit or approval issued by the City after the execution of this Development Agreement shall be consistent with this Development Agreement.

**E. Proportional Compliance.**

Any proportional compliance requirements in LUC 20.25D.060.G that would otherwise apply to the Property as a result of application of the Existing Development provisions of LUC 20.25D.060.G shall be deferred from the period of the Effective Date of this Agreement through a final decision on the MDP, so long as the MDP is applied for within the timeframe established for catalyst projects in LUC Section 20.25D.035.A. Any deferred proportional compliance obligation shall be waived if the MDP is approved by the City. If Owner fails to apply for the MDP within the required timeframe under this Agreement, or if the MDP is denied, then the proportional compliance obligations deferred under this Section D shall become immediately due and Owner shall apply for any necessary permits or approvals to perform such proportional compliance with 60 days of expiration of the timeframe or issuance of the denial. Notwithstanding any provision in the associated permits to the contrary, work to complete the proportional compliance obligation shall be completed within 180 days of issuance of the required permits or approvals, unless extended by the City to accommodate any conditions or restrictions on timing of the work.

**F. FAR Amenity Bonus System.**

1. **Adjustment of Tier 1 Fee-in-lieu Rate.** For a Catalyst Project on the Property, the Owner may choose to comply with the LUC 20.25D.090 requirements for Tier 1 amenities

by paying a fee-in-lieu at a rate of \$3.75 for each square foot of floor area for the first 750,000 square feet of development under the MDP, and by paying a fee-in-lieu rate of \$4.00 for each square foot of floor area above 750,000 square feet. In the alternative, this Development Agreement provision shall not bar the Owner, at its choice, from utilizing the LUC 20.25D.090 standards as of the Effective Date to provide required amenities.

**2. Amenity Rate for Certain Required Open Space.** Owner may receive credit towards required amenities for the mini-park and activated park or recreation space required to be included in Phase 1 of the MDP under Section A above as provided in this Section F.

a) **Mini-Park:** Owner shall receive credit towards required amenities for dedication of the M-3 mini-park at the bonus rate set forth in 20.25D.090.C.7 Tier 1b.2 (Park Dedication) and/or Tier 1b.3 (Park Improvements).

b) **Other Activated Park or Recreation Space:** Owner may receive credit towards required amenities for the activated green space as follows:

- at the Tier 2 bonus rate so long as it is developed according to the design criteria set forth in 20.25D.090.C.7 Tier 2.12 (Active Recreation Area); or
- at the Tier 1b bonus rate set forth in 20.25D.090.C.7 Tier 1b.2 (Park Dedication) if the entire area is dedicated to the City, and it meets the design criteria 2 through 4 in that section; and/or
- at the Tier 1b bonus rate set forth in 20.25D.090.C.7 Tier 1b.3 (Park Improvements) if improvements are made according to the design criteria 1 through 5 set forth in such section.

Unless the activated park or recreation space meets one of the provisions above, it shall not receive credit towards required amenities.

**3. Eligibility for Other City Credits, Bonuses or Offsets.** Notwithstanding any provision of city code to the contrary, any public infrastructure required as a condition of approving the MDP that meets the criteria of LUC 20.25D.035.A.3.a through c shall not be eligible for and shall not earn any of the credits, bonuses, or offsets described in LUC 20.25D.035.A.3.d through f.

#### **G. Concurrency.**

The Bellevue Traffic Standards Code, Chapter 14.10 BCC, allows a development agreement to adjust the timing of traffic concurrency analysis and the expiration date of concurrency approval. The City and the Owner agree to the amended analysis time and concurrency expiration date specified below.

**1. Timing of Concurrency Analysis and Determination.** For purposes of approving the MDP, the concurrency analysis pursuant to Chapter 14.10 BCC (the Traffic Standards Code or TSC) shall not be required at the time of the master development plan application. Owner acknowledges that approval of the MDP is not a guarantee, assurance,

acknowledgement or statement of any kind about whether all or any part of the development included in the MDP would satisfy the requirements of Chapter 14.10 BCC, with or without mitigation. Owner acknowledges that approval of the MDP does not limit the City's ability to require compliance with Chapter 14.10 BCC, as modified in this Agreement, which compliance may include denial or conditioning of phases of the MDP. For purposes of compliance with the TSC, the required concurrency analysis and determination shall be conducted on each phase of the MDP, which analysis and determination shall be made at the time of application for the first design review or other required Process II approval for development within such phase.

2. **Vesting of Concurrency Approval.** The concurrency approval issued for each phase shall expire five years from the date of issuance of the Land Use Code approval with which it was issued, provided that such five year period shall be extended consistent with the provisions of BCC 14.10.040.F.1; and further provided that in no event may development consisting of more than 1,000,000 square feet of commercial development be vested at any given time. For purposes of this Paragraph, hotels shall not be considered commercial development.

#### **H. Catalyst Project Residential Requirements and Delay Penalty.**

1. **Designation of Residentially-restricted Property.** The master development plan must designate at least 5.8 acres within the MDP for residential use and associated required ground floor commercial or retail uses, exempt ground floor retail and exempt childcare or nonprofit space (as described in LUC Section 20.25D.090). Such area shall be designated as the "Residentially-restricted Property." The MDP shall further establish a requirement that a minimum of 784,000 square feet of residential development, exclusive of the associated uses described above, be developed within the Residentially-restricted Property. A covenant restricting use shall be recorded against the property and run with the land.

2. **Required Timing of Development.** The Residentially-restricted Property shall be developed with residential uses in an amount proportional to the amount of project limit area developed with commercial uses on the Property by no later than the date that Sound Transit's East Link Project or other high-capacity transit operating within a dedicated transit-only right-of-way begins regularly scheduled passenger-carrying service to a transit station within the Property. For purposes of determining compliance with this Section H, "developed with" residential or commercial use means that such development shall be constructed or underway with an issued building permit.

3. **Sale of Residentially-restricted Property.** It is acknowledged that Owner intends to sell the Residentially-restricted Property to a third party. In order to avoid the penalty described in Paragraph 4 below, any sale of Residentially-restricted Property must be to an unrelated third party, and must be closed at least three years prior to Sound Transit's East Link Project or other high-capacity transit operating within a dedicated transit-only right-of-way beginning regularly scheduled passenger-carrying service to a transit station within the Property. Owner acknowledges that additional approvals, including subdivision or binding site plan approvals, may be required in order to create a parcel or parcels of residentially-restricted property for sale to third parties.

4. **Penalty for Failure to Develop Residentially-restricted Property.** A penalty for delay in developing the Residentially-restricted Property by the time set forth in Paragraph 2 above shall be imposed at the time of any application for any approval of further commercial development on the Property if both of the following conditions are satisfied on or after the date that Sound Transit's East Link Project or other high-capacity transit operating within a dedicated transit-only right-of-way begins regularly scheduled passenger-carrying service to a transit station within the Property:

a) WR-SRI 120th LLC is the fee owner of any portion of the Residentially-restricted Property. For purposes of this Development Agreement, WR-SRI 120<sup>th</sup> LLC will be considered the fee owner of the Property if the fee owner of the Property is an entity related to Shorenstein Properties LLC or Wright Runstad & Company or subsidiaries or affiliates; or if WR-SRI 120<sup>th</sup> LLC is not the fee owner, the sale of the Residentially-restricted Property was not closed at least three years prior to Sound Transit's East Link Project or other high-capacity transit operating within a dedicated transit-only right-of-way beginning regularly scheduled passenger-carrying service to a transit station within the Property.

b) That portion of the Residentially-restricted Property that is proportional to the amount of project limit area developed with commercial uses is not developed with a residential use.

Penalty to be applied if both of the two above conditions are met: The next application for design review on the Property must include a proposal to develop that portion of Residentially-restricted Property necessary to be proportional to the total of: a) the amount of commercial development developed on the Property; and b) the amount of commercial development proposed in any active building permit on the Property.

#### **I. Term, Amendment, and Termination**

This Development Agreement shall go into effect on the date it is executed by the Owner and the City ("Effective Date"). This Development Agreement shall be effective until the later of 18 months after the Federal Transit Authority issues a Record of Decision on the Sound Transit East Link Project or December 31, 2011; provided that the term shall automatically be extended for an additional two years (or such different period agreed to by the parties as a negotiated permit review timeline) so long as an MDP consistent with this Agreement is filed prior to expiration and is diligently pursued by Owner; and further provided that the term of the Development Agreement shall be automatically extended for the effective life of any MDP approved consistent with this Agreement. Upon expiration of such period, as may be extended above, this Development Agreement shall automatically terminate.

Other than as set forth in Section T, no amendment to this Development Agreement shall be effective unless approved by both parties in writing.

#### **J. Binding Effect; Assignability.**

This Development Agreement shall bind and inure to the benefit of the Parties hereto and their respective successors, heirs, legatees, representatives, receivers, trustees, successors, transferees

and assigns. Prior to submittal of an MDP consistent with this Agreement, Owner may not assign its interest or obligations under this Agreement without the City's prior written consent, which shall not be unreasonably withheld. If an MDP consistent with this Development Agreement is submitted, Owner may assign its interest and obligations under this Agreement without the City's consent. Notwithstanding the foregoing, Owner may assign its interest and obligations hereunder to an affiliate or related entity owned, controlled by or under common control with Owner without necessity of City's consent but only following delivery of written notice of such assignment, together with such documents as are reasonably necessary to confirm the relationship between Owner and such affiliated entity.

**K. Representations and Warranties.**

Each signatory to this Development Agreement represents and warrants that he or she has full power and authority to execute and deliver this Development Agreement on behalf of the Party for which he or she is signing, and that he or she will defend and hold harmless the other Parties and signatories from any claim that he or she was not fully authorized to execute this Development Agreement on behalf of the person or entity for whom he or she signed. Upon proper execution and delivery, this Development Agreement will have been duly entered into by the Parties, will constitute as against each Party a valid, legal and binding obligation that shall run with the land, and will be enforceable against each Party in accordance with the terms herein.

**L. Specific Performance and Enforcement.**

The Parties specifically agree that damages are not an adequate remedy for breach of this Development Agreement and that the Parties are entitled to compel specific performance of all material terms of this Development Agreement by any Party in default hereof. All terms and provisions of this Development Agreement are material. Nothing in this Agreement modifies the City's ability to pursue its otherwise applicable enforcement provisions for violations of any permits issued for TSD.

**M. Governing Law and Venue.**

This Development Agreement shall be governed by and construed in accordance with the laws of the State of Washington. Jurisdiction over and venue for any action arising out of or relating to this Development Agreement shall be exclusively in the state and federal courts of King County, Washington. In the event of any apparent conflicts between the provisions of the city code or ordinances and this Development Agreement, this Development Agreement shall prevail.

**N. Full Understanding.**

The Parties each acknowledge, represent and agree that they have read this Development Agreement; that they fully understand the terms thereof; that they have had the opportunity to be fully advised by their legal counsel and any other advisors with respect thereto; and that they are executing this Development Agreement after sufficient review and understanding of its contents.

**O. Counterparts; Facsimile Signatures.**

This Agreement may be executed in more than one counterpart, each of which shall be deemed an original, and all of which shall constitute one and the same instrument. Facsimile signatures on this Agreement shall constitute original signatures of the Parties.

**P. Attorneys' Fees.**

Should it be necessary for any Party to this Development Agreement to initiate legal proceedings to adjudicate any issues arising hereunder, the Party or Parties to such legal proceedings who substantially prevail shall be entitled to reimbursement of their attorneys' fees, costs, expenses, and disbursements (including the fees and expenses of expert and fact witnesses) reasonably incurred or made by the substantially prevailing Parties in preparing to participate in mediation or arbitration, to bring suit, during suit, on appeal, on petition for review, and in enforcing any judgment or award, from the other Party or Parties.

**Q. Waiver.**

The waiver by a party of a breach of any provision of this Development Agreement by the other party shall not operate or be construed as a waiver of any subsequent breach by that party. No waiver shall be valid unless in writing and signed by the party against whom enforcement of the waiver is sought.

**R. Severability.**

This Development Agreement is expressly made and entered into under the authority of RCW 36.70B.170 *et seq.* This Development Agreement does not violate any federal or state statute, rule, regulation or common law known; but any provision which is found to be invalid or in violation of any statute, rule, regulation or common law shall be considered null and void, with the remaining provisions remaining in full force and effect.

**S. Equal Opportunity to Participate in Drafting.**

The Parties have participated and had an equal opportunity to participate in the drafting of this Development Agreement. No ambiguity shall be construed against any Party based upon a claim that the Party drafted the ambiguous language.

**T. Reservation of City Authority.**

As required by RCW 36.70B.170(4) and notwithstanding any other term of this Development Agreement, the City reserves the right to establish and impose new or different additional regulations to the extent required to address a serious threat to public health and safety.

**U. Notice.**

All correspondence and any notice required in this Development Agreement shall be delivered to the following parties:

**City of Bellevue**

Attention: Ms. Carol Helland  
Land Use Director  
450 110th Avenue NE  
PO Box 90012  
Bellevue, WA 98009  
Email: chelland@bellevuewa.gov

**with a copy to:**

Lori Riordan, City Attorney  
450 110th Avenue NE  
PO Box 90012  
Bellevue, WA 98009  
Email: LRiordan@bellevuewa.gov

**WR-SRI 120th LLC**

c/o Shorenstein Realty Services  
Attention: Mr. Todd Sklar  
235 Montgomery Street, 16<sup>th</sup> Floor  
San Francisco, CA 94104  
Email: tsklar@shorenstein.com

c/o Wright Runstad & Company  
Attention: Mr. Greg Johnson  
1201 Third Avenue  
Suite 2700  
Seattle, WA 98101  
Email: gjohnson@wrightrunstad.com

**with a copy to:**

Mr. Tayloe Washburn  
Foster Pepper PLLC  
1111 Third Avenue, Suite 3400  
Seattle, WA 98101  
Email: washt@foster.com

**V. Final and Complete Agreement.**

This Development Agreement constitutes the final and complete expression of the Parties on the development standards governing the Owner's development of the Property. This Development Agreement may not be modified, interpreted, amended, waived or revoked orally, but only by a writing signed by all Parties. This Development Agreement supersedes and replaces all prior agreements, discussions and representations on all subjects discussed herein, without limitation. No Party is entering into this Development Agreement in reliance on any oral or written promises, inducements, representations, understandings, interpretations or agreements other than those contained in this Development Agreement.

**W. Recording Required.**

This Agreement shall be recorded with King County at Owner's expense.

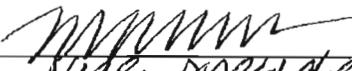
**X. Execution and Effective Date.**

The Effective Date of this Agreement is the date on which it is fully executed by the City and Owner representatives. The following representatives of the Parties are authorized to, and do hereby, execute on behalf of the party so indicated.

**OWNER:**

**WR-SRI 120<sup>TH</sup> LLC**, a Delaware limited liability company

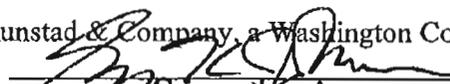
By: **SRI EIGHT 120<sup>th</sup> LLC**,  
A Delaware limited liability company,  
Its Managing Member

By:   
Its: vice president  
Date: 9 July 09

By: **WRC BEL-RED LLC**,  
A Washington limited liability company

By: **Wright Runstad Associates Limited Partnership**, a Washington limited partnership,  
its Manager

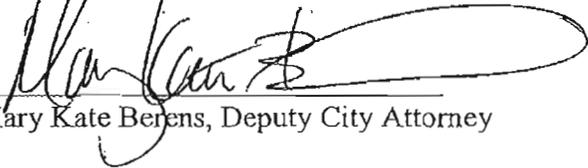
By: **Wright Runstad & Company**, a Washington Corporation, its general partner

By:   
Its: President

**CITY OF BELLEVUE:**

  
\_\_\_\_\_  
Steven R. Sarkozy, City Manager  
Date: 7/10/2009

APPROVED AS TO FORM:  
Lori M. Riordan, City Attorney

  
\_\_\_\_\_  
Mary Kate Berens, Deputy City Attorney

STATE OF WASHINGTON )  
 ) ss.  
COUNTY OF KING )

I certify that I know or have satisfactory evidence that Todd Sklar is the person who appeared before me and said person acknowledged that he/she signed this instrument, on oath stated that he/she was authorized to execute the instrument and acknowledged it as the Vice President of SRI EIGHT 120<sup>th</sup> LLC, Managing Member of WR-SRI 120<sup>th</sup> LLC to be the free and voluntary act of such party for the uses and purposes mentioned in the instrument.

DATED: July 9, 2009.

Notary Seal



Judith K Hoyle  
JUDITH K HOYLE (Print Name)  
Notary Public Residing at Chimacum WA  
My appointment expires: 11-6-11

STATE OF WASHINGTON )  
 ) ss.  
COUNTY OF KING )

I certify that I know or have satisfactory evidence that Gregory K Johnson is the person who appeared before me and said person acknowledged that he/she signed this instrument, on oath stated that he/she was authorized to execute the instrument and acknowledged it as the President of Wright Runstad & Company, general partner of Wright Runstad Associates Limited Partnership, Manager of WRC BEL-RED LLC, to be the free and voluntary act of such party for the uses and purposes mentioned in the instrument.

DATED: July 9, 2009.

Notary Seal



Judith K Hoyle  
JUDITH K HOYLE (Print Name)  
Notary Public Residing at Chimacum WA  
My appointment expires: 11-6-11

STATE OF WASHINGTON )  
 ) ss.  
COUNTY OF KING )

I certify that I know or have satisfactory evidence that Steven R. Sarkozy is the person who appeared before me and said person acknowledged that he/she signed this instrument, on oath stated that he/she was authorized to execute the instrument and acknowledged it as the City Manager of the CITY OF BELLEVUE to be the free and voluntary act of such party for the uses and purposes mentioned in the instrument.

DATED: July 10, 2009

Notary Seal



Janna Dee Steedman  
JANNA DEE STEEDMAN (Print Name)  
Notary Public  
Residing at Summerville WA  
My appointment expires: 12/20/12

**Exhibit A**  
**Legal Description of Property**

Lot 3 of City of Bellevue Short Plat No. LF-06-135856, Recorded under King County Recording No. 20070319900012

## TECHNICAL MEMORANDUM

Project:	The Spring District	From:	Joleen Fuson Justin Jones, PE
Reviewer:	City of Bellevue	Date:	March 28, 2012

---

### Purpose

The purpose of this memorandum is to describe the anticipated Greenhouse Gas Emissions associated with the construction and operation of The Spring District in Bellevue, WA.

This technical memorandum references information provided in the Environmental Checklist prepared in November 2011 for The Spring District Master Development Plan. The information in this memorandum is intended to satisfy draft guidance for lead agencies for use when evaluating proposals under the State Environmental Policy Act (SEPA) that will result in GHG emissions.

The Spring District development includes 29 buildings, parks, and roadways within a 36.01 acre site. The project will be constructed in phases over the next 15 years. Since the SEPA review considers potential environmental impacts project-wide, we are also calculating the GHG emissions on a project-wide basis. However, it should be noted that individually the proposed buildings will be below the 25,000 MTCO<sub>2e</sub> per year threshold set by Ecology.

### Qualitative Disclosure

Ecology guidance suggests projects expected to produce an average estimate of 10,000 to 25,000 MTCO<sub>2e</sub> annually should provide a qualitative disclosure of emissions associated with the project. The expected sources of emissions for the Spring District include embodied emissions created through the manufacturing, transportation, construction, and disposal of building materials as well as emissions created through landscape disturbance.

Emissions associated with the operation of the project include building energy usage and maintenance, vehicle trips accessing the site.

The mitigation measures proposed for the development include transit-oriented development (light rail station, walkable community, multi-use path with regional connection, and mixed-use development), low impact development techniques, which may include: rain gardens, bioretention, porous concrete, and LEED® certified buildings. These techniques can reduce the emissions by at least 11% compared to the no-mitigation option.

### GHG Screening Table

Ecology provides a GHG Screening Table to determine the level of additional greenhouse gas emissions analysis that may be needed. For each category, the table estimates the size of a project that would be expected to produce emissions at annual levels of 10,000 and 25,000 MTCO<sub>2e</sub> during operation. Projects with new emissions that are near or exceed the 25,000 MTCO<sub>2e</sub> threshold may require additional project-specific analysis to determine if emissions may trigger GHG analysis. "New" emissions are described by

Received  
APR 03 2012  
Permit Processing

E

Ecology as emissions that result from a project that are “above and beyond” existing emissions levels. Therefore, the existing uses of the site are considered in this analysis as a baseline.

Using the GHG Screening Table, the Spring District team evaluated the project’s estimated GHG emissions annually (including transportation and operation) based on the proposed building uses. The results of the screening are shown below:

**Ecology GHG Screening Table**

Building Type	Units or SF	10,000 MTCO <sub>2</sub> e Annually	25,000 MTCO <sub>2</sub> e Annually
Multi-family housing	1,200 units	575 Units	1,438 Units
General Retail	166,000 SF	185,000 SF	463,000 SF
Office Space	3,715,000 SF	399,000 SF	998,000 SF
Hotel	200 Units	565 Units	1,411 Units

**Screening Results**

The proposed project uses exceed the 25,000 MTCO<sub>2</sub>e annual emissions threshold set by Ecology. Ecology states in its *Guidance for Ecology including Greenhouse Gas Emissions in SEPA Review* document that projects exceeding the 25,000 MTCO<sub>2</sub>e threshold are required to perform a quantitative analysis and implement mitigation measures to reduce CO<sub>2</sub> emissions by 11%. This analysis and mitigation measures are described next.

**Quantitative Analysis**

Using the King County Department of Development and Environmental Services Greenhouse Gas Emissions Calculator (King County GHG Calculator), updated in March 2011, we have estimated the quantity of “new” greenhouse gases that will be emitted during the lifespan of this project. These calculations encompass both construction as well as ongoing maintenance and operations of these facilities.

The building lifespans assumed in the King County GHG Calculator are as follows:

- Lodging                      62.5 years
- Retail                        62.5 years
- Office                         62.5 years
- Multi-family housing      80.5 years
- Industrial                    62.5 years

Based on the type of use of each building and its square footage, the spreadsheet makes assumptions regarding the occupancy of the buildings and their Embodied, Energy, and Transportation emissions.

The lifespan greenhouse gas emissions using the King County GHG Calculator are shown in the worksheet below:

**King County Dept. of Development and Environmental Services  
SEPA GHG Emissions Worksheet**

**Section I: Buildings**

Type (Residential) or Principal Activity (Commercial)	# Units	Square Feet (in thousands of square feet)	Emissions Per Unit or Per Thousand Square Feet (MTCO2e)			Lifespan Emissions (MTCO2e)
			Embodied	Energy	Transportation	
Single-Family Home.....	0		98	672	792	0
Multi-Family Unit in Large Building .....	1200		33	357	766	1,386,833
Multi-Family Unit in Small Building .....	0		54	681	766	0
Mobile Home.....	0		41	475	709	0
Education .....		0.0	39	646	361	0
Food Sales .....		0.0	39	1,541	282	0
Food Service .....		0.0	39	1,994	561	0
Health Care Inpatient .....		0.0	39	1,938	582	0
Health Care Outpatient .....		0.0	39	737	571	0
Lodging .....		198.0	39	777	117	184,759
Retail (Other Than Mall).....		166.0	39	577	247	143,217
Office .....		3,715.0	39	723	588	5,012,818
Public Assembly .....		0.0	39	733	150	0
Public Order and Safety .....		0.0	39	899	374	0
Religious Worship .....		0.0	39	339	129	0
Service .....		0.0	39	599	266	0
Warehouse and Storage .....		0.0	39	352	181	0
Other Cold Storage/Warehouse/Bus Depot.....		0.0	39	1,500	600	0
Vacant .....		0.0	39	162	800	0

**Section II: Pavement.....**

Pavement.....		225.00				11,250
---------------	--	--------	--	--	--	--------

**Total Project Emissions:**

**6,738,878**

The King County GHG Calculator provides **lifespan** MTCO2e emissions of 6,738,878 for the project. Factoring the building lifespans assumed, the emissions for the project are 103,100 MTCO2e per year.

Using the same King County GHG Calculator for the existing site uses, the existing site emits 17,900 MTCO2e annually. To find the “new” emissions associated with this project, we subtracted the existing site uses from the proposed site uses, as follows:

Existing Site Uses (MTCO2e/year)	Proposed Site Uses (MTCO2e/year)	Net Increase (MTCO2e/year)
17,900	103,100	85,200

## GHG Emissions Mitigation Options

The Spring District is committed to providing environmentally responsible, low impact development techniques as a standard practice. In order to conform to Ecology's requirement to require CO<sub>2</sub>e by 11%, the Spring District has identified several low impact development techniques it is using that will also qualify as mitigation options to reduce project emissions. The mitigation measures are described next, as well as the approximate percent reduction in new emissions that each option can achieve.

### Mitigation #1: Transit-Oriented Development

The Spring District will be a transit-oriented developed with a future light rail station, bus lines, a walkable community layout and mixed use development. The estimated MTCO<sub>2</sub> reduction associated with the reduction of vehicle dependency in a transit-oriented development is 25%\*. The elements of a transit oriented development that result in reductions of CO<sub>2</sub> emissions include the following:

- High employment concentrations
- Mixed-use development (retail, office, residential)
- Significant public transportation accessibility

A 25% MTCO<sub>2</sub> reduction for transit-oriented development equates to approximately 9,000 MTCO<sub>2</sub>e annually. This equates to a CO<sub>2</sub>e reduction of approximately 11% annually for the project.

*\* Source: The Transportation Research Board, Special Report 298: Driving and the Built Environment – The Effects of Compact Development on Motorized Travel, Energy Use, and CO<sub>2</sub> Emissions, 2009*

### Mitigation #2: Building Lifespan

While the GHG Calculator used in this evaluation has lifespan estimates for buildings based on use, the developer (Wright Runstad & Company) has a large portfolio of built work that shows their commitment to ecological construction, green building management and a practice of energy conservation. By employing adaptive building reuse, sustainably grown and regionally produced products and high-performance systems, Wright Runstad & Company promotes waste reduction as well as reuse and recycling in all of their new and existing projects. By selecting durable and less energy consuming building components, they have a proven history of sustainable 100-year buildings.

Using the GHG Calculator with the 100-year lifespan for just the new Office buildings, this mitigation is able to reduce the greenhouse gas emissions associated with the project. Annual emissions can be reduced by an additional 26,800 MTCO<sub>2</sub>e per year, or 31%, with the building lifespan mitigation.

### Other Mitigation Measures

While it is difficult to quantify the MTCO<sub>2</sub>e reductions of many low impact development techniques, these features will certainly provide additional emissions reductions. The Spring District is considering the following measures for this project:

*Bike and Pedestrian Facilities* – bike and pedestrian facilities throughout the Spring District will encourage non-motorized transportation that will further reduce the number of vehicle miles trips.

*LEED®-ND Building Design* – buildings and site design will be designed to LEED-ND standards resulting in energy efficiencies and possible water reuse. During construction, materials will be recycled where possible. This mitigation reduces stationary combustion emission sources.

*Low Impact Development (LID)* – the site will include several low impact development techniques to reduce land use change impacts. These techniques may include, but are not limited to:

- Rain gardens
- Bioretention
- Porous surfaces
- Roof gardens

### Mitigation Summary

Mitigation Measure	Reduction in MTCO <sub>2</sub> e	New Emissions (MTCO <sub>2</sub> e/year)	Percent Reduction
No mitigation option	0	85,200	0
#1: Transit-oriented development	9,000	76,200	11%
#2: Building Lifespan	26,800	58,400	31%
<b>Cumulative Totals After Mitigation Measures #1 &amp; #2</b>	<b>35,800</b>	<b>49,400</b>	<b>42%</b>

## The Spring District Conceptual Phasing Plan

Item	Phase 1a	Phase 1b	Phase 2	Phase 3	Phase 4	Phase 5	Phase 6	Totals
Demolition	3	1	2	-	-	-	-	6
	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings
	358,310	18,575	323,263	-	-	-	-	700,148
	GSF	GSF	GSF	GSF	GSF	GSF	GSF	GSF
New Residential Max Height 125' - 150' Max FAR 4.0	10	-	-	-	-	1	1	12
	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings
	883,000	-	-	-	-	170,000	169,000	1,222,000
	GSF	GSF	GSF	GSF	GSF	GSF	GSF	GSF
New Office Max Height 150' Max FAR 4.0	2	4	-	5	1	3	1	16
	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings
	430,000	1,104,000	-	1,316,000	11,000	561,000	293,000	3,715,000
	GSF	GSF	GSF	GSF	GSF	GSF	GSF	GSF
New Retail Max Height 150' Max FAR 4.0	-	-	-	-	1	-	-	1
	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings
	13,000	18,000	-	63,000	18,000	41,000	13,000	166,000
	GSF	GSF	GSF	GSF	GSF	GSF	GSF	GSF
Hotel Max Height 150' Max FAR 4.0	-	-	-	-	1	-	-	1
	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings	Buildings
	-	-	-	-	199,000	-	-	199,000
	GSF	GSF	GSF	GSF	GSF	GSF	GSF	GSF
Cumulative FAR	2.10	3.13	NA	3.35	3.29	3.26	3.27	3.27
Parks	130,000	(54,000)	-	26,000	-	-	-	102,000
	sf	sf	sf	sf	sf	sf	sf	sf
Sound Transit 120th LRT Station	-	-	Yes	-	-	-	-	-
Net Impervious Surface Increase (Decrease)	(91,000)	1,000	(26,000)	(39,000)	(1,000)	(29,000)	(38,000)	(223,000)
	sf	sf	sf	sf	sf	sf	sf	sf
Parking Stalls	1,500	3,000	-	2,800	400	1,200	1,200	10,100
New Added Streets	127,500	12,200	-	57,600	-	3,400	24,500	225,200
	sf	sf	sf	sf	sf	sf	sf	sf
Acres	11.2	5.5	0.0	8.8	1.9	5.4	3.2	36.01
Estimated Time Range* (+/-)	2013 - 2015*	2015-2018*	2015-2023*	2020-2022*	2022-2024*	2024-2026*	2026-2028*	15 years*

\*Dependent on market timing

November 2011, Master Development Plan

	Total Budget	% of Total	Notes
<b>On-Site Utilities</b>			
Sewer	650,000	5.8%	
Storm	1,000,000	8.9%	
Water	850,000	7.6%	
Power/Com Duct & Vault System	1,400,000	12.5%	Each building budget has its own utility connection charges.
City Permits and Connections	350,000	3.1%	
On-Site Utilities Contingency	280,000	2.5%	
Sales Tax	<u>453,000</u>	4.0%	
<b>On-Site Utilities Total</b>	4,983,000	44.4%	
<b>Off-Site Utilities</b>			
Contingency for Off-Site Utilities	<u>1,000,000</u>	8.9%	
<b>Off-Site Utilities Total</b>	1,000,000	8.9%	Contingency for extension of PSE from 12th site.
<b>Road Improvements</b>			
Grading and Pre-Utility Work	75,000	0.7%	
Subgrade and 1st Lift Pavement	525,000	4.7%	
Curb & Gutter	135,000	1.2%	
Finish Roads	275,000	2.4%	
Road Contingency	100,000	0.9%	
Sales Tax	<u>111,000</u>	1.0%	
<b>Road Improvements Total</b>	1,221,000	10.9%	
<b>Off-Site Road Improvements</b>			
Contingency for off-Site Road Imp.	<u>250,000</u>	2.2%	
<b>Off-Site Road Improvements Total</b>	250,000	2.2%	One offsite traffic signal assumed in this budget.
<b>Design &amp; Engineering</b>			
Architect	400,000	3.6%	
Traffic Engineer	200,000	1.8%	
Civil Engineer	500,000	4.5%	
Soils Engineer	200,000	1.8%	
Environmental Engineer	20,000	0.2%	
Structural Engineer	50,000	0.4%	
Electrical Engineer	40,000	0.4%	
Wayfinding/Signage Design	40,000	0.4%	
Survey	100,000	0.9%	
Landscape Architect	30,000	0.3%	
Testing & Inspection	70,000	0.6%	
Reimbursables	<u>50,000</u>	0.4%	
<b>Design &amp; Engineering Total</b>	1,700,000	15.1%	
<b>Miscellaneous</b>			
RE Taxes During Constr./Other	<u>50,000</u>	0.4%	
<b>Miscellaneous Total</b>	50,000	0.4%	
<b>Project Administration</b>			
Developer's Fee	479,000	4.3%	
Developer's Reimbursables	<u>50,000</u>	0.4%	
<b>Project Administration Total</b>	529,000	4.7%	
<b>Project Contingency</b>	1,500,000	13.4%	
<b>Total Project Costs</b>	<u>\$ 11,233,000</u>	100.0%	



Existing Figure 20.25D.140A: Required New Local Streets

Based on site reconfiguration, the relocation of the high capacity transit and the creation of a parks & open space, we have identified the need to modify the identified streets to in order to meet the intent of the Bel-Red code.

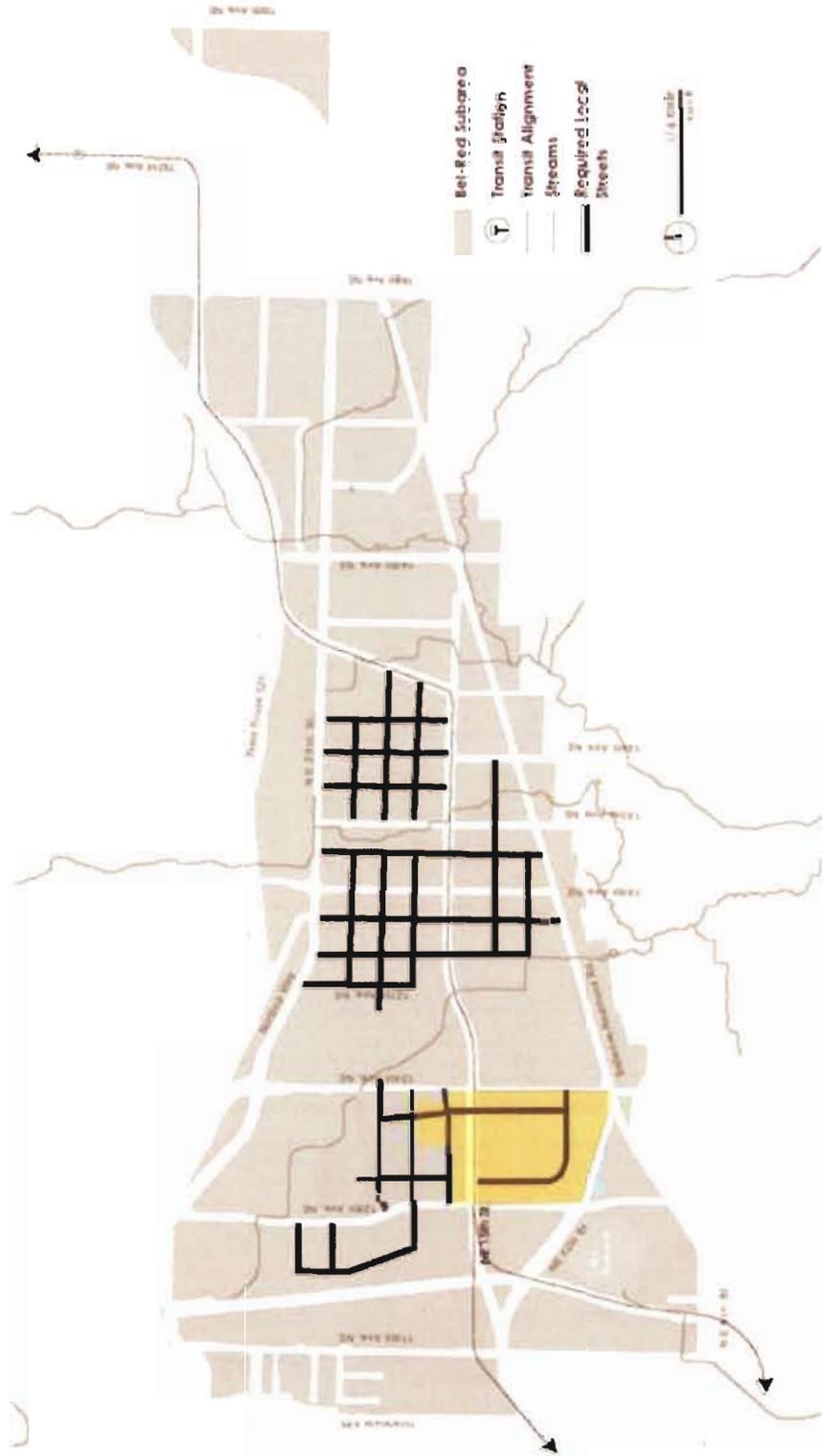
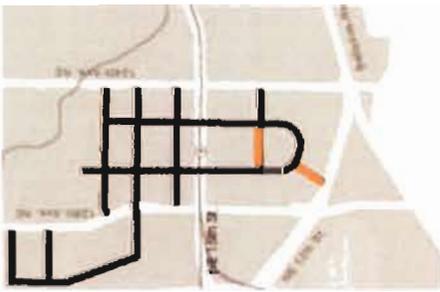


Figure 20.25D.140A: Required New Local Streets

Existing Figure 20.25D.140.D: Green Streets

Based on site reconfiguration and street realignment, we have identified the need to modify the identified streets to in order to meet the intent of the Bel-Red code. The original design included a park in the "horseshoe" between approximately NE 14th, NE 13th, 121st and 123rd Avenue. The park was intended to be integral to the green street. The redesigned park runs north-south, anchored by the high capacity transit station just north of NE 15th at 121st Avenue. Shifting the green street north to the loop roads will have the opportunity to more effectively incorporate the green elements of the project to the green street network. Additionally, a public access will be provided between the green streets west of 123rd Ave NE and the trail connection east of 124th Ave NE.



Figure 20.25D.140.D: Green Streets