



**City of Bellevue  
Development Services Department  
Land Use Staff Report**

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Proposal Name: **Alamo Manhattan Main Street**

Proposal Address: 10505 Main Street

Proposal Description: Mixed-use residential project with 2 buildings, 260 residential units, approximately 7,000 SF of ground level commercial/retail space and underground parking for 319 vehicles on a 1.45 acre site zoned DNTN-MU and located in Subdistrict A of the Downtown Perimeter Design District.

File Number: **11-117760 -LD**

Applicant: Alamo Manhattan Main Street, LLC

Decisions Included: Process II, Combined Design Review Decision and SEPA Determination

Planner: Sally Nichols

State Environmental Policy Act Threshold Determination: Determination of Non-significance (DNS)

Carol V. Helland  
Carol V. Helland, Environmental Coordinator  
Development Services Department

Director's Decision: **Approval with Conditions**  
Michael A. Brennan, Director  
Development Services Department

By: Carol V. Helland  
Carol V. Helland, Land Use Director

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Date of Application: July 13, 2012  
Notice of Application: August 16, 2012  
Public Meeting: September 27, 2012 and November 14, 2012  
Decision: December 27, 2012  
Appeal Deadline: January 10, 2013  
Design Review Expiration: January 10, 2015

For information on how to appeal a proposal, visit the Development Services Center at City Hall, 450 110<sup>th</sup> Avenue NE, or call (425) 452-6800. Comments on State Environmental Act Determinations can be made with or without appealing the proposal within the noted comment period for the SEPA determination. Appeal of the decision must be received in the City Clerk's office by 5 p.m. on the date noted for appeal of the decision.



DEVELOPMENT SERVICES DEPARTMENT  
ENVIRONMENTAL COORDINATOR  
11511 MAIN ST., P.O. BOX 90012  
BELLEVUE, WA 98009-9012

## DETERMINATION OF NON-SIGNIFICANCE

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**PROPONENT:** Alamo Manhattan Main Street

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**LOCATION OF PROPOSAL:** 10505 Main Street

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**DESCRIPTION OF PROPOSAL:** Application for approval to develop a 6-story, multi-family, mixed-use project consisting of 2 buildings. The development will have 260 residential units, approx. 7,000 square feet of retail at the ground level, and underground parking with 319 spaces.

**FILE NUMBERS:** 12-127760-LD

The Environmental Coordinator of the City of Bellevue has determined that this proposal does not have a probable significant adverse impact upon the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(C). This decision was made after the Bellevue Environmental Coordinator reviewed the completed environmental checklist and information filed with the Land Use Division of the Development Services Department. This information is available to the public on request.

- There is no comment period for this DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's office by 5:00 p.m. on \_\_\_\_\_.
- This DNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on the DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's Office by 5 p.m. on January 10, 2012.
- This DNS is issued under WAC 197-11-340(2) and is subject to a 14-day comment period from the date below. Comments must be submitted by 5 p.m. on \_\_\_\_\_. This DNS is also subject to appeal. A written appeal must be filed in the City Clerk's Office by 5 p.m. on \_\_\_\_\_.

This DNS may be withdrawn at any time if the proposal is modified so that it is likely to have significant adverse environmental impacts; if there is significant new information indicating, or on, a proposals probable significant adverse environmental impacts (unless a non-exempt license has been issued if the proposal is a private project); or if the DNS was procured by misrepresentation or lack of material disclosure.

Carol V Holland  
Environmental Coordinator

12/27/12  
Date

**OTHERS TO RECEIVE THIS DOCUMENT:**

State Department of Fish and Wildlife  
State Department of Ecology,  
Army Corps of Engineers  
Attorney General  
Muckleshoot Indian Tribe

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**Attachments**

- A. Project Drawings
- B. SEPA Checklist
- C. Certificate of Concurrency

**I. REQUEST/PROPOSAL DESCRIPTION**

The applicant requests Design Review approval for a mixed-use residential project with 260 residential apartment units, approximately 7,000 SF (square feet) of retail space, and four stories of underground parking with 319 stalls. The site is located in the DNTN-MU (Downtown – Mixed Use) Land Use District and within Subdistrict A of the Downtown Perimeter Design District.

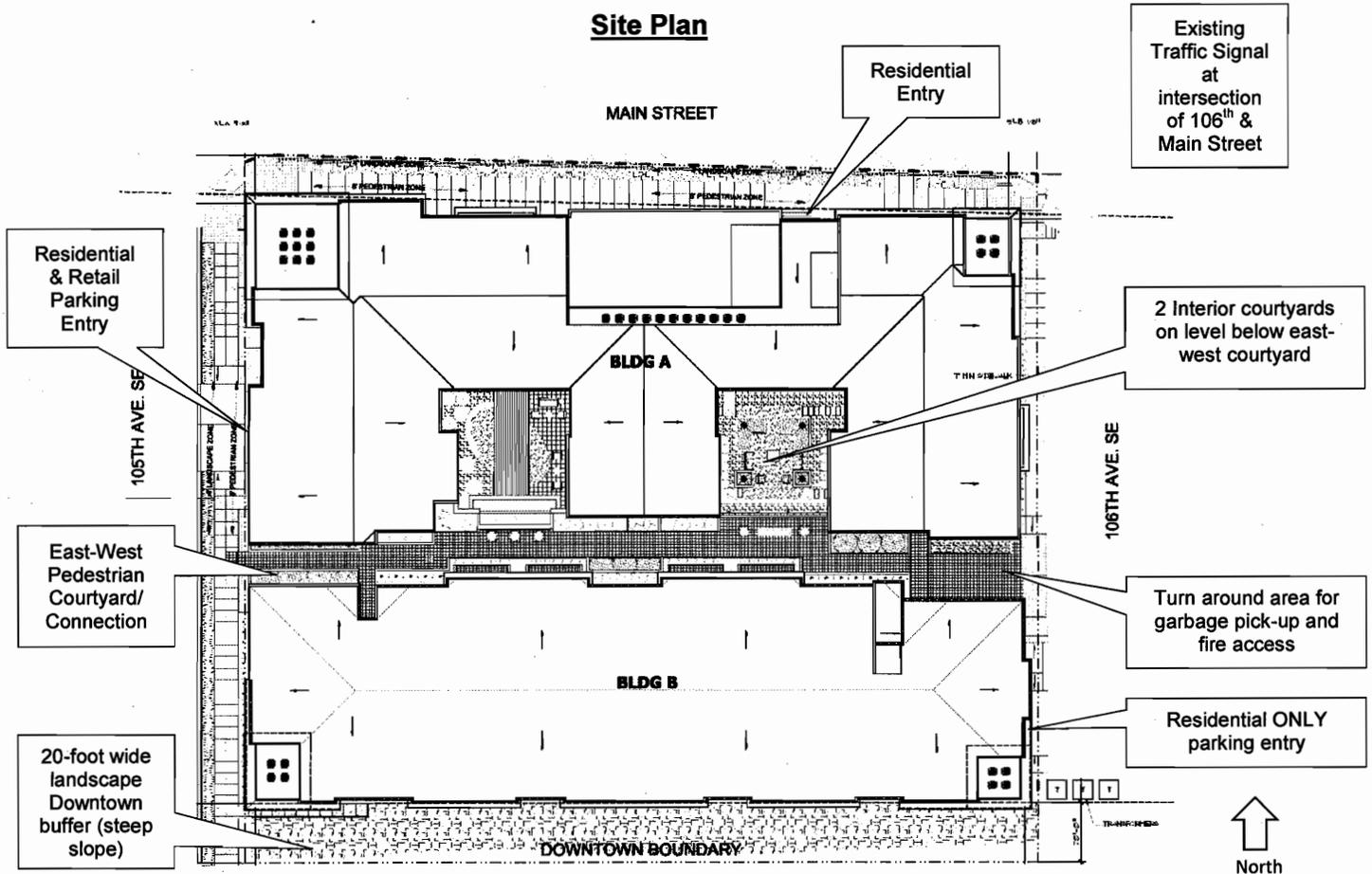


**A. Site Design**

The proposal is for development on what is currently an under-utilized site on the southern perimeter of the Downtown. The primary street frontage will be along Main Street, with secondary frontages on both 105<sup>th</sup> and 106<sup>th</sup> Avenues SE. The proposal will incorporate two buildings (Buildings A and B) that will run parallel to each other with an east-west orientation. The northern building (Building A) will run along the entire Main Street frontage and the second building is directly behind, adjacent to the required 20-foot wide landscaped perimeter buffer at the Downtown boundary. The upper floors will be connected via pedestrian bridges and an internal courtyard at the podium level will connect the lower floors. This landscaped courtyard will extend over a common base garage and will also provide a public pedestrian connection between 105<sup>th</sup> Avenue SE and 106<sup>th</sup> Avenue SE.

The retail spaces, the leasing office, the residential lobby, and pedestrian connection to the parking garage will all be located along the Main Street frontage. The pedestrian streetscape will include special paving at the building corners, residential

entry and retail entries, street trees in continuous 4-foot wide planting strips, and street furniture including benches and/or seat walls.



Vehicular access to the parking garage will be off both 105<sup>th</sup> Avenue SE and 106<sup>th</sup> Avenue SE. The entrance on 105<sup>th</sup> Avenue SE will provide access for both residential and retail parking. Access to the residential parking will be restricted through a secure gate inside the garage. There will be no new traffic signals placed at the intersection of 105<sup>th</sup> Avenue SE and Main Street. The vehicular entry to the garage off 106<sup>th</sup> Avenue SE – a dead-end street - will be exclusively for the residents. There is an existing traffic signal at the intersection of 106<sup>th</sup> Avenue SE and Main Street. A turn-around space on 106<sup>th</sup> Avenue SE will also be provided for fire and garbage/recycling pick-up functions (see site plan above).

**B. Building Design**

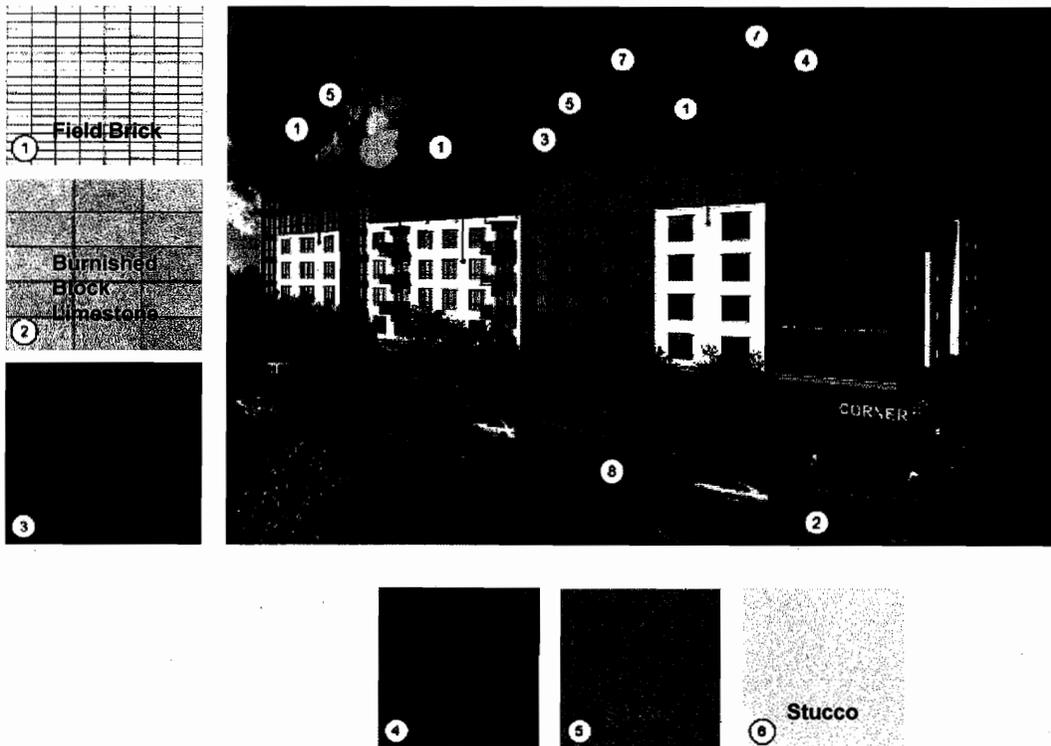
The two proposed buildings will be set over a common underground parking structure. This common base is designed to accommodate a multi-level parking garage that steps with the existing grade of the site. The parking for the retail uses on Main Street will be located directly behind retail spaces on parking garage level 3. This parking will have an accessible pedestrian connection to Main Street through the building.

The layout of the two buildings arranges the residential units around the common internal linear corridor and two smaller interior courtyards of approximately 1,500 square feet each. This layout will allow all of the units to have a direct connection to the exterior for natural light and ventilation.

The Main Street façade has been designed to appear to be multiple buildings along the entire street frontage. The changes in the building façade are created by changes in materials, color, building modulation, height and architectural details. The primary corner at Main Street and 105<sup>th</sup> Avenue SE has been further enhanced by a slightly taller feature, which will completely enclose the mechanical equipment at this location. The corner will also have taller, more prominent windows, an entry to the corner retail that is recessed to create a larger pedestrian realm at the street level, and placement of street furniture and enhanced plantings. A similar, but smaller version of this corner feature will occur on the northeast corner of Building A. A roof top terrace is also provided for the use of residents along the Main Street façade. This terrace further helps to break up the linearity of the building and provides some softening of hard roof edge.

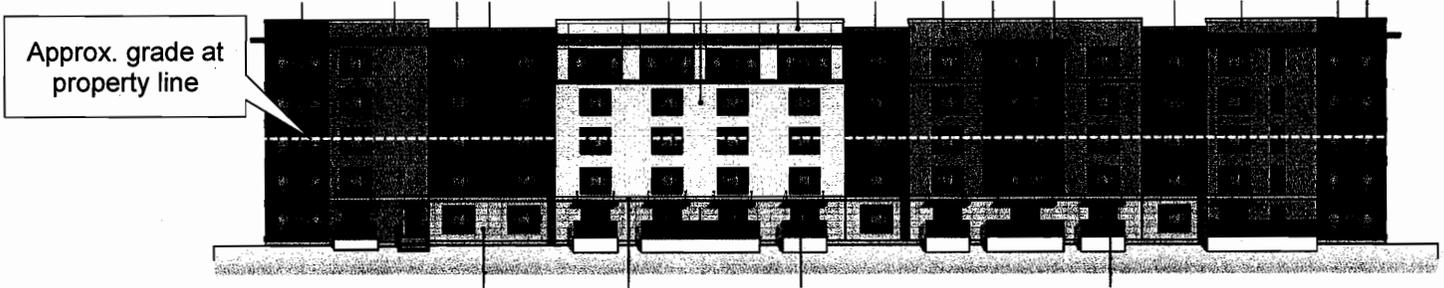
Exterior building materials will include brick veneer, stucco, cementitious panels and siding, exposed cast in-place concrete, metal railings, vinyl windows, aluminum storefront windows and metal louver screens at the towers. All material colors and finishes have been selected to reduce the reflection of light and glare.

### Building Materials and Colors

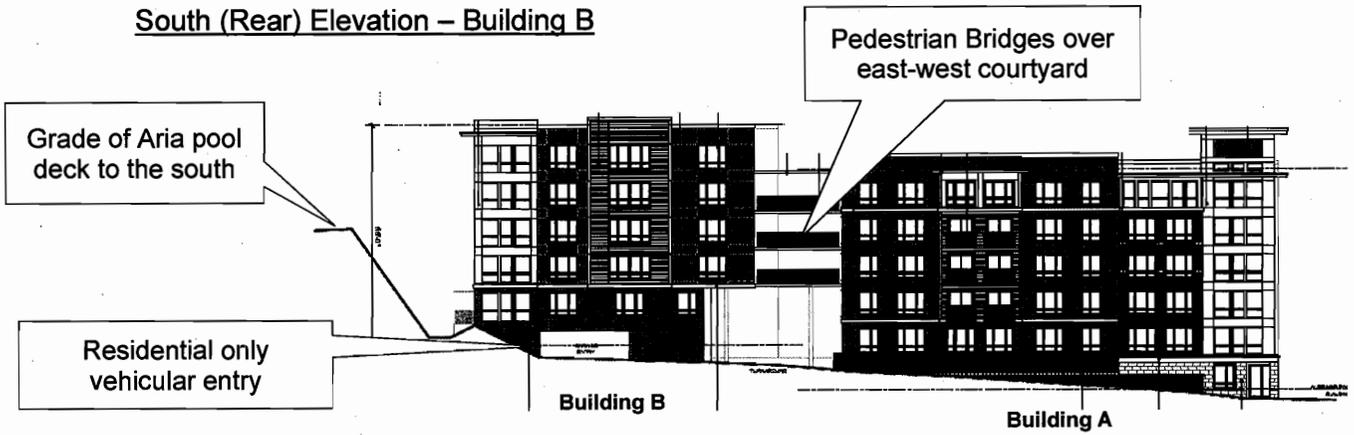


The southern façade of Building B will face the residential neighborhood to the south – particularly the adjacent Aria Condominium building. As a result of public comment submitted during this design review, the applicant redesigned this façade to be more compatible with the residential neighbors to the south through increased modulation and interesting changes in color and materials. Two corner features were removed to reduce the apparent height of the building.

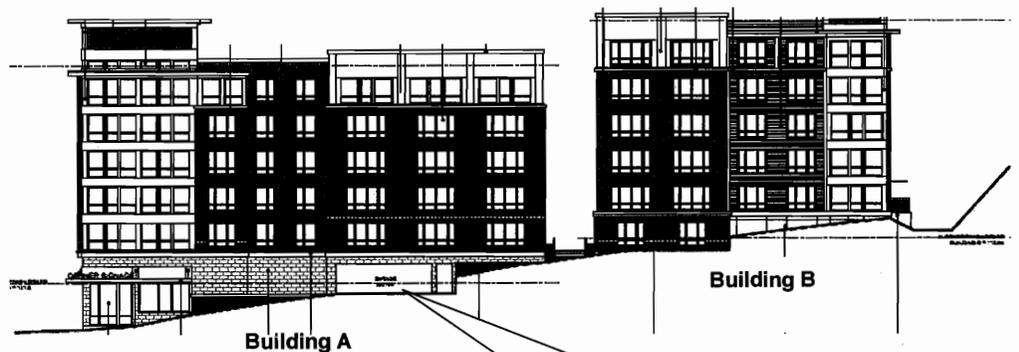
**Building Elevations**



**South (Rear) Elevation – Building B**



**East (106<sup>th</sup> Avenue SE) Elevation**



**West (105<sup>th</sup> Avenue SE) Elevation**

**C. Process**

Design Review is required by Land Use Code (LUC) 20.25A.010.C. The Design Review and SEPA Threshold Determination are both Process II decisions. Process II is an administrative process. The Environmental Coordinator issues the SEPA Threshold Determination and the Director of Development Services issues the Design Review decision. An appeal of any Process II decision is heard and decided upon by the City of Bellevue Hearing Examiner.

**II. SITE DESCRIPTION, ZONING, & CONTEXT**

**A. Site Description (Existing)**

The site is actually composed of two separate parcels. For the purposes of this report, both lots will be referred to as the "site." A Boundary Line Adjustment will be required prior to issuance of the building permit to combine the two parcels into one building lot. Refer to Condition of Approval regarding the boundary line adjustment in Section XI of this report.

Aerial Photograph – Existing Conditions



The proposal site is located on Main Street, bordered to east by 106<sup>th</sup> Avenue SE and to the west by 105<sup>th</sup> Avenue SE. 105<sup>th</sup> Avenue SE is a through street that connects to the residential neighborhood to the south. 106<sup>th</sup> Avenue SE is also a public right of way that dead-ends at the southern property line of the proposal site. It currently functions as a driveway leading to parking behind the existing retail building. None of the streets bordering the site has on-street parking.

The site is currently occupied by retail uses on each of the parcels. Each use has associated surface parking, both in front and behind the existing retail buildings.



Existing Retail Buildings facing Main Street

For a width of approximately 20-feet along the entire southern property line, there is a steep slope with a rise in grade of approximately 20 feet on the proposal property and at least another 20 feet on the Aria property to the south. The slope is heavily vegetated with mature trees (Alder, Madrona, Douglas Fir, Big Leaf Maple and Birch) and understory that includes Western Hazelnut and dense stands of invasive species such as blackberry and ivy. There are also stands of significant mature trees at the top of the slope on the Aria Condominium project. Note that steep slopes are not categorized as critical areas in the Downtown.



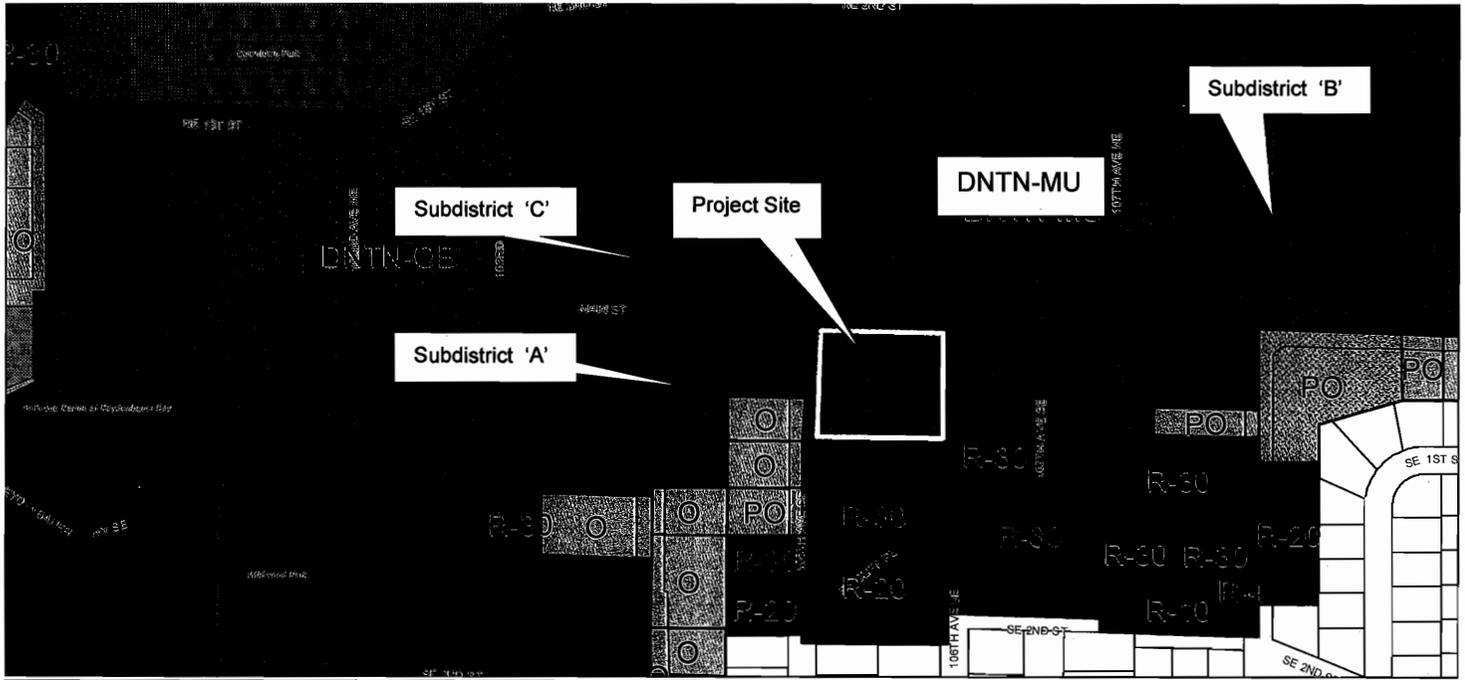
Vegetated Steep Slope along the Southern Property Line

#### B. Site Zoning

A site zoning map is provided below. The site is zoned Downtown-MU (Mixed Use) and located in Subdistrict 'A' of the Downtown Perimeter Design District. The site

was rezoned in 1981 through Ordinance 3013 to implement the 1979 Downtown Subarea Plan Map. The Perimeter Design District was initially adopted in 1985 (Ord. 3553) and amended in 1991 (Ords. 4235 & 4268). The proposed retail and residential uses are allowed outright in Subdistrict A of the Downtown-MU zone.

**Land Use Districts**



**C. Site Context**

The proposed site fronts Main Street and lies between 105<sup>th</sup> Avenue SE and 106<sup>th</sup> Avenue SE. Main Street is designated as a Type “D” streetscape per the LUC’s Design Guidelines Building/Sidewalk Relationships (DG-B/SR). The surrounding properties to the east, north, and west are all located in the same land use district, DNTN-MU (Downtown – Mixed Use) - Subdistrict ‘A’. Surrounding properties that are *outside* of the Downtown limits/boundary line include a small O (Office) parcel to the west and a multi-family residential parcel to the south in the R-30 land use district. Specific uses on the surrounding properties are as follows:

- North:** DNTN-MU, Subdistrict B, retail uses with surface parking
- East:** DNTN-MU, Subdistrict A; currently a retail use with surface parking
- West:** DNTN-MU, Subdistrict A and O; Two-story mixed-use office and retail with surface parking on the O parcel.
- South:** R-30, Multi-Family Residential; residential; four-story (three floors of residential over one story of parking) condominium buildings with pool area and limited surface parking.

### III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS

#### A. General Provisions of the Land Use Code

##### 1. Use

The proposed residential and commercial/retail uses are permitted in the Downtown-MU, Perimeter Design District, Subdistrict 'A'.

##### 2. Dimensional Requirements

The dimensional/area requirements that apply in DNTN-MU, Subdistrict A, are listed below and the proposal is evaluated for consistency.

**Table 1: Dimensional Requirements**

Item	Permitted/Required	Proposed	Comments/Conditions
<b>Project Limit</b>	No minimum	63,235 SF	Located in DNTN-MU, Perimeter Design Dist. A
<b>Building Height</b>	55 FT – measured from average <i>finished</i> grade	55 FT (includes approx. 13.5 FT for rooftop mechanical equip at northwest corner of Building A and approx. 11 FT at the northeast corner of Building A)	Measured from average finish grade to the rooftops, excluding 15-FT allowed for rooftop mechanical equip. LUC 20.25A.090 & 20.50.012 Meets requirement.
<b>Building Coverage</b>	75%	68%	LUC 20.25A.090.D and LUC 20.25A.020.B.5 Meets requirement.
	47,426 SF	42,973 SF	
<b>Floor Area Ratio (FAR)</b>	Residential: 2.0 Min. 3.5 Max.	3.5	Meets requirements through FAR Amenity Incentive System. Refer to Section III.B below.
<b>Exempt Retail/Ped-Oriented Frontage (POF) Floor Area</b>	Maximum 1.0 FAR (63,235 SF)	0.11 FAR 7,000 SF	Meets the criteria of LUC 20.25A.020.B.3 and LUC 20.25A.115
<b>Total GSF for FAR</b>		7,000 SF < 63,235 SF  216,130 SF	Note per LUC 20.50.022 , GFA <u>excludes</u> vent shafts, stairwells, parking, mechanical areas, outdoor courts and balconies.
<b>Floor Area per Floor Above 40 Feet</b>	20,000 GSF/FLR	19,209.5 SF (Averaged)*	

Item	Permitted/Required	Proposed	Comments/Conditions
<b>Floor Area per Floor Above 40 Feet (cont'd)</b>		Building A: 20,132 GSF (less than 1% increase over 20,000 SF) Building B: 18,287 GSF	Floorplates on residential building may be increased by 10%. Floorplates may be averaged on buildings less than 70 FT. LUC 20.25A.020.B.1 Meets requirement
<b>Setbacks – Building less than 75 feet</b>	<u>Front:</u> Main Street = 0 FT  105 <sup>th</sup> Ave. NE = 0 FT 106 <sup>th</sup> Ave. NE = 0 FT  <u>Rear :</u> 20 FT from rear property line – which is also DNTN boundary	<u>Front:</u> Main Street = Varies, with minimum width 12.5 FT 105 <sup>th</sup> Ave. NE = .5 FT 106 <sup>th</sup> Ave. NE = Varies, building placed at back of sidewalk <u>Rear:</u> 20 FT Rear setback also landscaped per LUC 20.25A.090.D.4	Meets requirement
<b>Refuse &amp; Recycling Residential:</b>  <b>Retail:</b>	Required:  1.5 SF/unit @ 160 units = 240 SF  5 SF/1000 SF with  10,594 SF POF = 53 SF Total Min. Area Required: 293 SF	Provided:  923 SF trash room, exceeding minimum by 630 SF.	LUC 20.20.725 Meets requirement.  Allied Waste reviewed the plans and provided written support for the size of and the access to the refuse and recycling area. <b><u>Refer to letter from Republic Services dated Nov. 7, 2012 in the project file and to Condition of Approval regarding solid waste/recycling recording in Section XI of this report.</u></b>
<b>Sidewalk Width</b>	Main Street: 12.5 FT (includes 4-foot planting strip & 6" curb)  105 <sup>th</sup> Ave. NE: 12.5 FT (includes 4-foot planting strip & 6" curb)  106 <sup>th</sup> Ave. NE: 6.5 FT (includes 6" curb)	12.5 FT (includes 4-foot planting strip & 6" curb)  12.5 FT (includes 4-foot planting strip & 6" curb)  7.5 FT (includes 6" curb)	LUC 20.25A.060 <b><u>Refer to Condition of Approval regarding alternative sidewalk designs in Section XI of this report.</u></b> As conditioned, meets requirement

Item	Permitted/Required	Proposed	Comments/Conditions
<b>Perimeter Landscaping</b>	20-foot wide planted setback (buffer) along Downtown perimeter boundary	Landscaped 20-foot setback provided along the entire southern property line on existing steep slope. Existing vegetation will be enhanced with species per LUC 20.25A.090.D.4.b.iii	LUC 20.25A.090.D.4.b.iii Meets requirement  Note: There is no requirement for tree retention in this land use district. Applicant will retain as many trees as possible. Refer to arborists report, dated Nov. 6, 2012 in the project file.
<b>Landscaping - Street Tree Caliper &amp; Species</b>	<p><b>Planting Strip:</b> 4 FT-wide planting strip along Main Street and 105<sup>th</sup> Ave. SE</p> <p><b>Street Trees:</b> Main Street: Fraxinus ornus 'Raywood' – Raywood Ash 4" Caliper</p> <p>105<sup>th</sup> Ave. SE: No specific species required 3" Caliper</p>	<p>4 Ft- wide planting strip with street trees, shrubs, groundcover, and irrigation</p> <p>Main Street: Fraxinus pennsylvanica 'Summit' – Summit Ash 4" Caliper</p> <p>105<sup>th</sup> Ave. SE: Tilia cordata 'Greenspire' – Greenspire Linden 3" Caliper</p>	<p>LUC 20.25A.060.B, Plate B Street Tree Map, and LUC 20.25A.090.D.4.c (DNTN Perimeter Dist.)</p> <p>Note: Applicant requests a change to Fraxinus species per Alternative Landscape Option. <b>Refer to Section IV.C.1.b of this report.</b></p> <p>Meets requirement <b><u>Refer to Conditions of Approval regarding the final landscape plan, planting strip/right-of-way irrigation, planting in right-of-way/streetscape, landscape installation and maintenance assurance devices, and maintenance agreement with the Parks Department in Section XI of this report.</u></b></p>
<p><b>Parking Residential Parking</b> – 260 units</p> <p><b>Retail Parking</b> – 7,000 nsf</p> <p><b>Total Parking</b></p> <p>Compact Stalls</p>	<p>Min. 1/unit: 260 Max. 2/unit: 520</p> <p>Min. 2/1000 SF: 14 Max. 4/1000 SF: 28</p> <p>Total Min. Stalls Required: 274</p> <p>No Compact stalls required. Up 65% of required parking stalls may be compact = 207 stalls</p>	<p>300 Stalls (40 more than minimum)</p> <p>19 stalls = 2.7/1000 SF</p> <p>319 stalls</p> <p>92 stalls = 29%</p>	<p>Meets LUC 20.25A.050</p> <p>Meets LUC 20.25A.050.C</p> <p>Meets LUC 20.25A.050</p> <p>Meets LUC 20.25A.050.F.2</p>

Item	Permitted/Required	Proposed	Comments/Conditions
<b>Loading Area/Refuse Collection Area</b>	One 10 FT x 55 FT dedicated loading space	One 20 FT x 40 FT (approx.) dedicated loading & refuse pick-up area on 106 <sup>th</sup> Ave. SE	Modification allowed per LUC 20.20.590K.4 <u><b>Refer to Condition of Approval regarding provisions for loading in Section XI of this report.</b></u>

**B. Special Requirements**

**Floor Area Ratio (FAR) and Amenity Incentive System Requirements**

**Table 2: Bonus Amenity Area Earned**

Project Area (Site) in DNTN-MU		<b>63,235 SF</b>
Project Gross Floor Area (GFA) Proposed GFA – Floor Area Exempt from FAR		<b>216,130 SF</b> 221,312 SF – 5,182 SF
<b>BASIC</b> Permitted Floor Area (FAR) for Residential DNTN-MU Basic Residential FAR (2.0) X Project Area		<b>126,470 SF</b> $2.0 \times 63,235 \text{ SF} = 126,470 \text{ SF}$
<b>MAXIMUM</b> Permitted Floor Area (FAR) for Residential DNTN-MU Maximum Residential FAR (3.5) X Project Area		<b>221,323 SF</b> $3.5 \times 63,235 \text{ SF} = 221,323 \text{ SF}$
<b>BASIC FAR</b>	<b>BASIC FAR Amenity Required</b>	<b>6,324 SF</b>
	Basic Non-Residential FAR (.5) x 20% of the Project Area	$0.5 \times (0.2 \times 63,235) = 6,324 \text{ SF}^*$ * 6,324 “buys” 126,470 SF
	<b>BASIC FAR Amenity Earned</b> (See Table 4 below)	<b>25,960 SF</b> 25,960 SF > 6,324 SF – Meets requirement of LUC 20.25A 020.C
	<b>Excess BASIC</b>	<b>19,636 SF</b> $25,960 \text{ SF} - 6,324 \text{ SF} = 19,636 \text{ SF}$
<b>Non-BASIC FAR</b>	<b>NON-BASIC Earned</b> (see Table 4 below)	<b>496,661 SF</b>
	Remaining <b>NON-BASIC FAR Amenity to Earn</b> GFA – Basic Permitted FAR of 2.0	<b>89,660 SF</b> $216,130 \text{ SF} - 126,470 \text{ SF} = 89,660 \text{ SF}$
	Remaining FAR Available after <b>BASIC</b> <b>Excess BASIC Points + NON-BASIC Earned</b>	<b>516,297 SF</b> $19,636 \text{ SF} + 496,661 \text{ SF} = 516,297 \text{ SF}$ 516,297 SF > 89,660 SF – Meets requirement of LUC 20.25A.030
<b>Total FAR Amenity Earned</b> BASIC FAR Amenity Earned + NON-BASIC FAR Amenity Earned		<b>522,621 SF</b> (Refer to Table 4 below) $25,960 \text{ SF} + 496,661 \text{ SF} = 522,621 \text{ SF}$
<b>Excess FAR Amenity Earned</b> (Total FAR Earned – BASIC FAR Amenity Required – Remaining FAR Amenity to Earn)		<b>426,637 SF</b> $522,621 \text{ SF} - 6,324 \text{ SF} - 89,660 \text{ SF} = 426,637 \text{ SF}$

- a. **BASIC FAR:** The BASIC FAR requirement equals: 20% of the project limit in square feet (SF) multiplied by the BASIC FAR permitted for a non-residential building in the DNTN-MU, Subdistrict A:  $(0.2 \times 63,235 \text{ SF}) \times 0.5 = 6,324 \text{ SF}$ . The amount of BASIC FAR Amenity Earned is 25,960 SF (see Table 3 below). This amount exceeds the minimum BASIC requirement of 6,324 SF.
- b. **NON-BASIC FAR:** In order to develop to a maximum FAR of 3.5, the proposed project is required to include amenities that support 221,323 SF of building floor area. The total amount of points (BASIC and NON-BASIC) required are 95,984 SF (6,324 SF + 89,660 SF). The proposed amenity package has earned 522,621 SF of FAR amenity points as shown in Table 3 below. There will be an excess of 426,637 SF and the proposal meets all FAR amenity requirements.

**Table 3: FAR BONUS AMENITIES**

	Amenity	Units of Measure	Bonus Ratio	Bonus Floor Area Earned	How it Meets the Design Criteria & Benefits the Public
BASIC Amenities	Pedestrian-Oriented Frontage (POF) Retail	248 LF	100:1	24,800 (248 x 100)	The space is visually and physically connected to Main Street and the corners at 105 <sup>th</sup> and 106 <sup>th</sup> Avenues NE
	Marquees	580 SF	2:1	1,160 (580 x 2)	Height varies, but is less than 12 FT & is coordinated with the building design
	<b>Sub-Total BASIC</b>			<b>25,960 SF</b>	
Non-BASIC Amenities	Underground Parking	68,685 SF	2:1	137,370 (68,685 x 2)	All of the parking area is below Average Finish Grade
	Residential Use	116,324 SF	2:1	232,648 SF	
	Active Recreation Area	1,082 SF	1:1	1,082 SF	
	<b>Sub-Total Non-BASIC</b>			<b>496,661 SF</b>	
	<b>COMBINED TOTAL (BASIC + Non-BASIC)</b>			<b>522,621 SF</b>	

As part of the building permit application, the applicant shall submit a detailed design for each proposed FAR amenity with a design component and a statistical summary for each amenity. Prior to TCO, the floor area earned through the Amenity Incentive System, and the total bonus floor area actually utilized for the project, project drawings and conditions of approval must be recorded with King County, Division of Records (LUC 20.25A.030.D). **Refer to Condition of Approval regarding amenity designs and statistics and bonus system and pedestrian-oriented frontage recording in Section XI of this report.**

#### IV. DESIGN GUIDELINES & DESIGN CRITERIA

##### A. Design Guidelines Building/Sidewalk Relationships - LUC 20.25A.115

Main Street and the corner of Main Street and 105<sup>th</sup> Avenue SE are designated as 'D' right-of-way.

*"Rights-of-way designated 'D' shall have low to moderate orientation to pedestrians. This shall be achieved by designing some relationship between exterior and interior activities with respect to visual access. Design attention should be given to sidewalk-related activities and amenities."*

The proposal includes pedestrian-oriented commercial/retail spaces that will span the entire Main Street frontage and continue around the corner on 105<sup>th</sup> Avenue SE. Clear storefront glazing will provide visual access into these spaces from the sidewalk. The ground floor retail space at the corner has a recessed entry door, thereby providing a more inviting pedestrian space at the corner.

The streetscape will be focused on the pedestrian experience. The sidewalk itself will be 12-foot wide with a 4-foot wide planting strip that will include shrubs and designated street trees. Benches will be placed along the façade to encourage pedestrians to linger. Note, however, that benches will not be allowed to encroach into the required sidewalk. The proposed improvements at the frontage will provide a pedestrian-friendly experience that does not currently exist on Main Street east of Bellevue Way. This development will provide a connection with Old Bellevue that will become increasingly important as a pedestrian route to the proposed Sound Transit station to the east. **Refer to Condition of Approval regarding ground-mounted mechanical equipment, furniture and screening in Section XI of this report.**

##### B. Perimeter District Design

###### 1. Special Standards – LUC 20.25A.090.D.5

The proposal meets the applicable Special Design Standards for a development within the Perimeter Design District:

###### a. Lighting:

All exterior lighting fixtures will be required to have cutoff shield to minimize impacts to adjacent properties. **Refer to Condition of Approval regarding exterior lighting in Section XI of this report.**

**b. Signage:**

This decision excludes review and/or approval of any building signage. The applicant will be required to submit a complete sign package for City Review and approval prior to the issuance of any occupancy permits, tenant improvement permits, or individual sign permits for the commercial/retail spaces. All signs must be an integral part of the architectural design, and scaled to the pedestrian environment. Each sign will require a sign permit, which will be reviewed according to the design criteria of the sign package and this design review. Refer to Condition of Approval regarding the sign permit package and individual sign permits in Section XI of this report.

**2. Perimeter Design District Design Guidelines – LUC 20.25A.090.E**

The proposal meets the following applicable Downtown Perimeter Design District Design Guidelines:

**a. Buildings should incorporate interior arcades, open courtyards, enclosed plazas or combinations thereof which offer mid-block pedestrian connections between perpendicular and/or parallel streets.**

The proposal will provide an open courtyard between Building A and B that will become an east-west public pedestrian connection between 105<sup>th</sup> Avenue SE and 106<sup>th</sup> Avenue SE. Refer to Condition of Approval regarding public access signage for the pedestrian mid-block connection in Section XI of this report.

**b. Buildings should be clad with materials which minimize reflected light. Overhangs, awnings, sunscreens, and other devices should be considered to minimize conditions of glare.**

The specific materials proposed for the building – brick veneer, stucco, cementitious panels and siding, cast in place concrete, storefront windows, and metal accents including louvered screens – have been chosen for their ability to reduce the reflection of light and glare.

Additionally, in the pedestrian realm at ground level, the sidewalk will have marquees which will provide weather protection while at the same time diffusing light.

**c. Building facades should be divided into increments through the use of bay windows, offsets, angled facets, recesses and other architectural features, which serve to break down the scale.**

The applicant has used numerous architectural elements to break down the scale of the building, including the following:

- To break down the overall scale of the project, the applicant proposes two smaller, multi-level buildings. The difference in the building heights, due in large part to the change in topography, also helps to break down the mass of the building.
- The public facades of the building, particularly on Main Street, will have an interesting visual rhythm through the use of plane modulation, change in materials and colors, variety in the width of each building segment,

and modulation of the roofline, including the provision of a rooftop terrace with a canopy.

- The corner features on Building A along Main Street will provide visual interest, help to identify the development, emphasize the important corner of 105<sup>th</sup> Avenue SE and Main Street, and break up what could have been an uninteresting rectilinear profile.
- The southern façade facing the adjacent residential neighborhood will have a variety of colors, materials, modulation and window placement to help break up the linearity of the façade.

**d. Rooftops should incorporate features such as pitched or sloped forms, terraces, and perimeter planting to soften an otherwise rectilinear profile.**

- Multiple roof treatments and modulation of the roof plane will provide interest on all sides of the buildings – particularly those facing public streets.
- The rooftop outdoor terrace along Main Street will incorporate overhead screening, which will further help to soften the hard edges of the building.

**e. Surface parking should be concealed from street level views by berms, hedges, walls or combinations thereof.**

There will be no surface parking. All parking will be within the underground garage.

**f. Special attention should be given to the provision of elements at or near the ground level such as awnings, recessed entries, water features, address signs, seasonal flower beds, seating, pedestrian oriented uses and display kiosks.**

The applicant will provide weather protection via marquees along the Main Street façade and provide approximately 248 linear feet of pedestrian oriented frontage. Landscaping, including street trees, shrubs and/or groundcover, will be provided in 4-foot wide planting strips, a raised planter at the northwest corner, and along the building wall at the northeast corner of Building A. Benches will be placed along the Main Street building façade and special paving will be used to enhance entry points to the building. Design review of the project signage will take place under sign design package, which must be submitted to Land Use prior to application for any sign permits. **Refer to Condition of Approval regarding the sign permit package in Section XI of this report.**

**C. Design Review Criteria – LUC 20.25A.110**

The proposal meets the Design Review Criteria of LUC 20.25A.110 for ***SITE DESIGN*** and ***DOWNTOWN PATTERNS AND CONTEXT***.

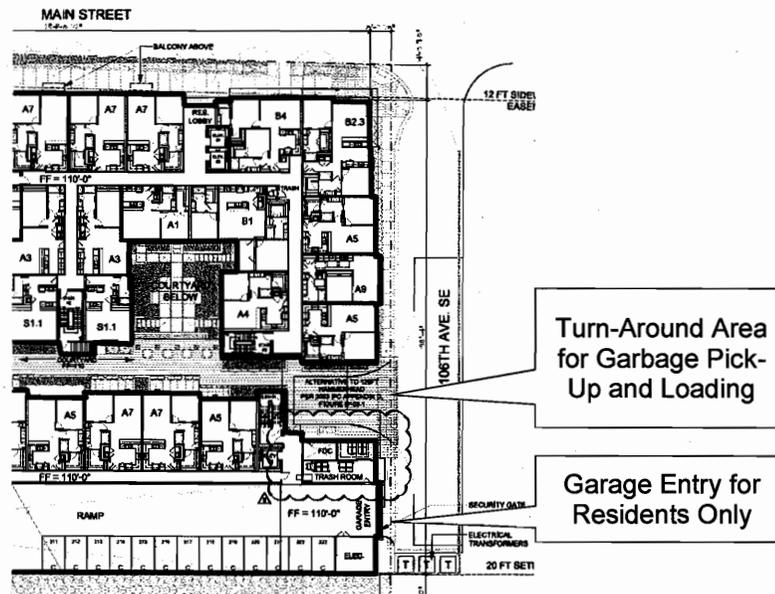
**1. SITE DESIGN (LUC 20.25A.110.A)**

**a. Vehicular Circulation and Parking**

Parking & Service Areas: All required parking will be accommodated within the underground garage, which has three full levels of parking under both buildings and a partial level under Building B. Access to the garage will be

from two locations. The entrance on 105<sup>th</sup> Avenue SE will provide access to residential AND retail parking; however the entry to the residential portion of the garage will be restricted to residents only by an internal gate. The garage entry off 106<sup>th</sup> Avenue SE, a dead end street, will be exclusively for residential parking. Both exits from the parking garage will be required to meet the requirements for sight distance.

Solid waste and recycling containers will be stored in a trash room in the northeast corner of Building B. Adjacent to this room and north of the parking garage entry, the designers have used the space between Buildings A and B to create a hammerhead/turn-around space off of 106<sup>th</sup> Avenue SE to support all garbage pick-up and loading functions for both buildings. Trash containers will be rolled out to the trash staging area adjacent to the trash room and will not need to be placed within a public right of way. Garbage and recycling trucks will then pick-up the trash from this location and will be able to turn around in 106<sup>th</sup> Avenue SE. **Refer to Condition of Approval regarding solid waste/recycling containers and recording in Section XI of this report.**



**b. Pedestrian Circulation and Amenities (LUC 20.25A.060)**

**Frontage Sidewalks:** The frontage sidewalks on Main Street and 105<sup>th</sup> Avenue SE will be 12-foot wide, including 4-foot planting strips. The sidewalk along 106<sup>th</sup> Avenue SE will be 7-foot wide and will not have street trees due to the narrowness of the sidewalk and setbacks. However, landscaping will be planted between the sidewalk and the building to soften the interface between the building wall and the pedestrian. Approximately 248 linear feet of ground floor pedestrian-oriented retail/commercial space, as well as other pedestrian-generating spaces such as the residential entry and leasing office, are proposed next to the public sidewalk on Main Street. Clear glazing in the storefront windows will provide visual access from the sidewalk into the commercial spaces. Benches will be placed along the

building façade to provide space for pedestrians to linger. **Refer to Condition of Approval regarding storefront glazing in Section XI of this report.**

**Street Trees:** Per LUC 20.25A.060 - Plate B, the street tree to be used along Main Street is *Fraxinum ornus* 'Raywood'. However, the applicant has requested that the Ash species be changed to *Fraxinus pennsylvanica* 'Summit' – Summit Ash. This change is proposed to provide a more robust and successful street tree and is consistent with yet unadopted changes being considered by the City to be included in comprehensive code updates for the Downtown. The 'Raywood' Ash has been experiencing severe dieback syndrome throughout the United States and is therefore not recommended as an appropriate street tree. The general appearance and form of the 'Summit' species is similar to that of the 'Raywood' Ash. The applicant has also proposed to use *Tilia cordata* 'Greenspire' – Greenspire Linden along 105<sup>th</sup> Avenue SE, although there is no specified tree for this location in the LUC. The street trees along Main Street will have a minimum 4-inch caliper and those along 105<sup>th</sup> Avenue SE will have a minimum 3-inch caliper.

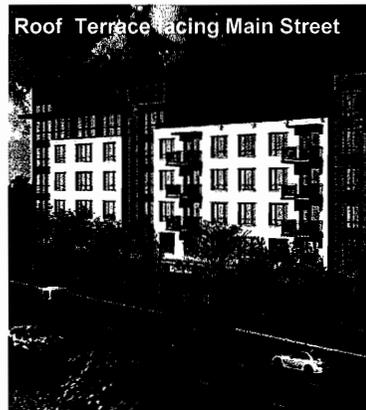
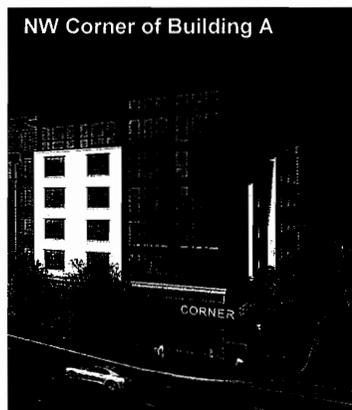
**c. Wind and Sun**

By constructing two separate buildings versus one, the applicant was able to create a linear open space and two smaller courtyards in the interior of the project. All of the residential units will be able to take advantage of light and air provided by these spaces.

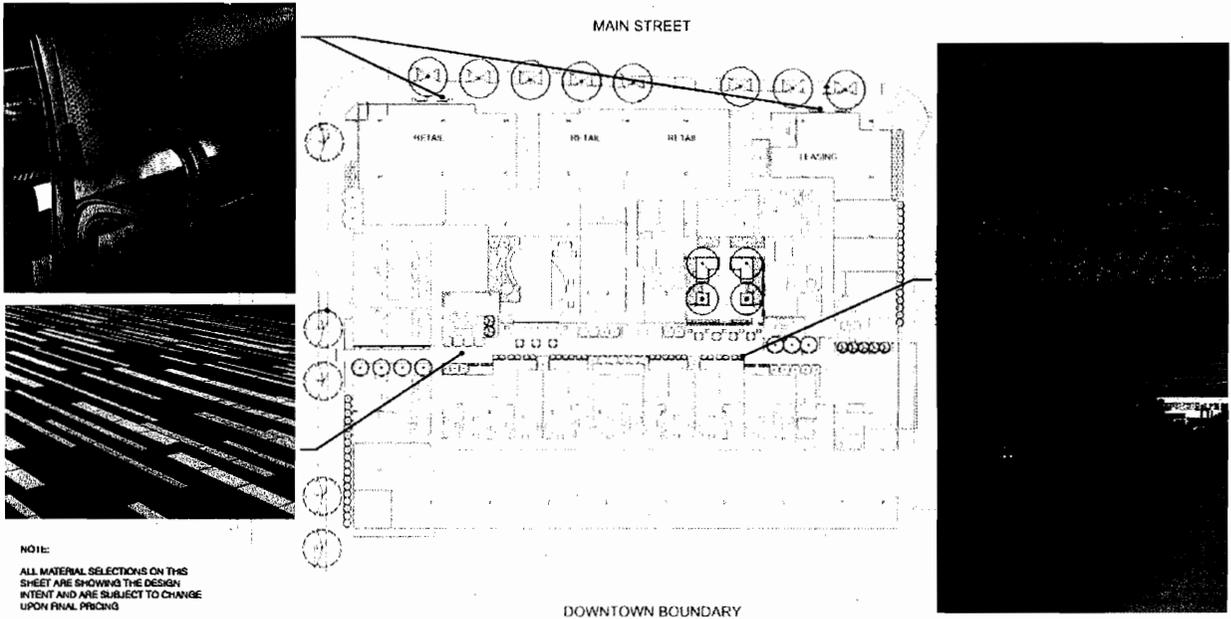
Pedestrian scaled marquees will provide year-round weather protection along Main Street.

**d. Open Space**

Public open space along Main Street will be provided at the northwest corner by notching the building and along the building façade where benches have been placed to encourage pedestrian activity. Additional open space that can be used by the public as well as by the residents will be provided along the interior east-west courtyard that connects 105<sup>th</sup> Avenue SE with 106<sup>th</sup> Avenue SE. The building will also have a rooftop outdoor terrace on Building A for use by the residents.



**Street Furnishings along Streetscapes and in Courtyard**



**e. Light and Glare**

The specific materials proposed for the building – brick veneer, stucco, cementitious panels and siding, exposed cast-in-place concrete, storefront windows, and metal accents such as louver screens and railings – have been chosen for their low reflectivity. Additionally, at the ground level, the sidewalk will have marquees which will provide weather protection as well as diffuse light. The applicant will also use landscaping (street trees, shrubs and groundcover) to mitigate against potential glare on the public streets and provide pleasant pedestrian spaces. These street trees will also help to mitigate the heat island effect from paving on the public sidewalk.

All exterior building lighting will be required to include cut-off shields to minimize the impacts of light to the future residents and to off-site properties. **Refer to Condition of Approval regarding exterior lighting in Section XI of this report.**

**1. DOWNTOWN PATTERNS AND CONTEXT (LUC 20.25A.110.B)**

**a. Natural Setting and Topography**

The bulk of the proposed project is on a gently sloping site within the urban context. However, there is a very steep slope approximately 25 to 30 feet wide along the entire southern property lines. The proposed building will take advantage of this grade by tucking the building close to the toe of the slope in order to bury parking and lessen the impact of the building on the neighbors to the south. Note that steep slopes are not categorized as critical areas in the Downtown.

Per the submitted arborist's report prepared by Gilles Consulting and dated November 6, 2012, some of the existing mature trees and shrubs at the base of the slope will need to be removed for construction of the proposed building and drainage features. These trees are relatively small and do not currently provide screening between the proposal site and the properties to the south. The existing vegetation at the top of the slope on the proposal site will remain and will continue to provide the most significant screening of the proposed building from the properties to the south. None of the trees and vegetation on the Aria site will be affected and, when combined with the preserved trees on the proposal site, they will help to create a robust landscaped buffer along the southern edge of the Downtown. A copy of the arborist's report is available for public viewing in the project file.

**b. Landscape Design**

Street Frontage: The proposed street frontage development will include continuous 4-foot wide planting strips between the sidewalk curb. The plant materials, which include street trees, shrubs and groundcover, will visually soften the building and improve separation between vehicles and pedestrians. The planting strips will also be required to have irrigation on a separate system with its own meter to facilitate future maintenance by the City of Bellevue. **Refer to Condition of Approval regarding the planting strip/ right-of-way irrigation system and planting in the right-of-way/streetscape in Section XI of this report.**



Residential Courtyards and Rooftop Terrace: In addition to the plantings within the city right-of-way and public sidewalk, the applicant will provide landscaped outdoor open spaces. The open courtyard, which connects 105<sup>th</sup> Avenue SE and 106<sup>th</sup> Avenue SE, will also provide a pleasant public mid-block connection.

Downtown Landscape Buffer/Steep Slope: The applicant will retain as much existing vegetation as possible on the steep slope along the southern property line. The existing vegetation will be enhanced with native vegetation, including trees, shrubs and groundcover to help stabilize the slope, provide restoration for areas disturbed by the construction of the building, and provide screening of Building B from the neighborhood to the

south. A revised planting plan for this specifically targeted at this area will be required to direct planting efforts. **Refer to Condition of Approval regarding landscape plant types/Downtown buffer in Section XI of this report.**

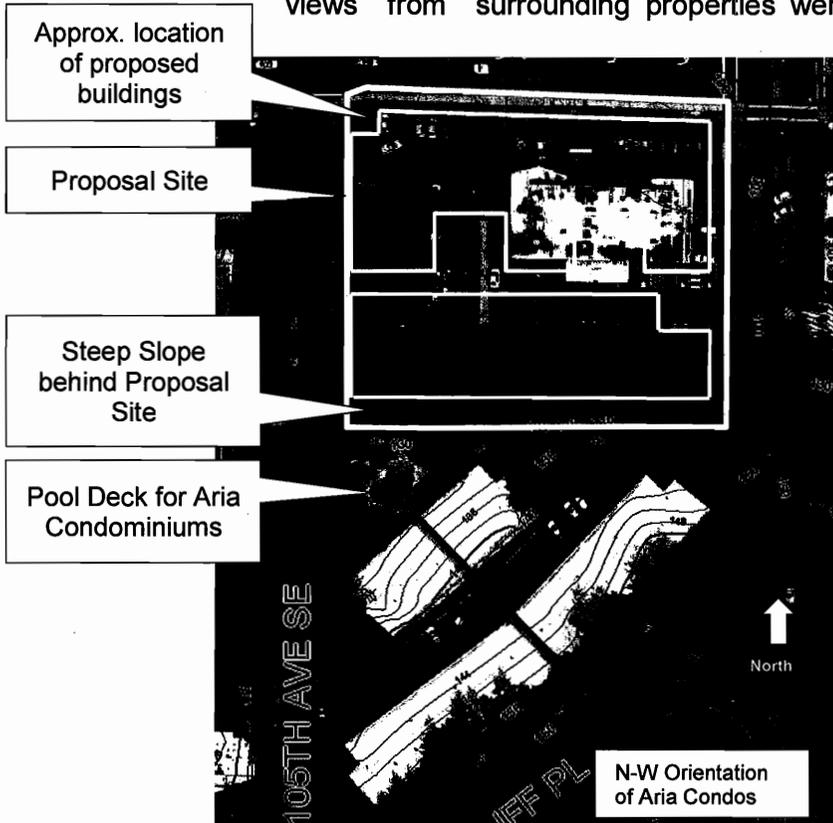
**Utility Vaults:** Above-ground mechanical equipment (boxes and vaults) will not be allowed in any public sidewalks. If they are placed in planting areas, they shall be adequately screened with vegetation. **Refer to Condition of Approval regarding ground-mounted mechanical equipment screening in Section XI of this report.**

**c. Views**

Private views and the impact of proposals on those views from adjacent properties are not regulated by the Land Use Code. However, the private views from surrounding properties were considered when designing the proposed development.

The proposal has incorporated the required 20-foot wide landscaped buffer along the southern property line and will provide substantial screening by retaining as many of the existing trees as possible. The nearest building in the Aria condominium complex to the south is approximately 45-50 feet from the back of Building B and more importantly, the Aria buildings (and decks for all units) are angled to the northwest instead of facing directly north toward the proposed buildings. Lastly, the proposed buildings were able to take advantage of the elevation change along the southern property line such that the buildings could be placed at least 20-feet lower than the properties to the south. This will

help to reduce the height of Building B that extends above the elevation of development to the south.



Southwest Corner of Proposed Building B



Northwesterly view of Building B from Aria Condominiums' pool deck near the southern property line

**d. Building Height and Bulk**

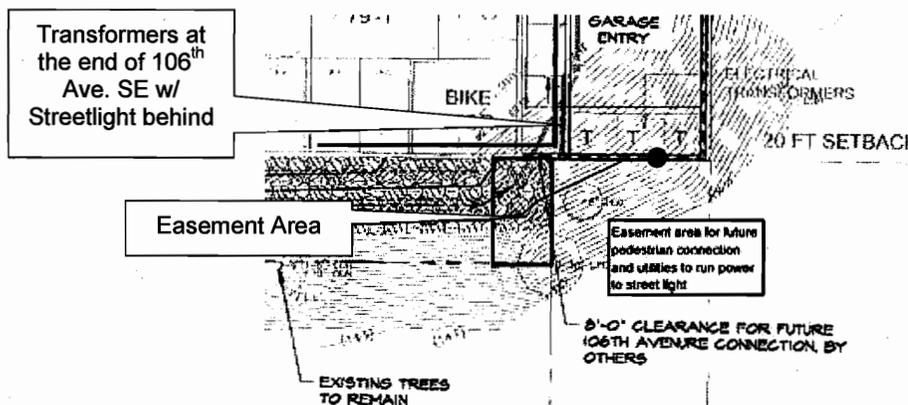
- i. **Mass & Void:** Although the shape and square footage of the proposal site dictated rectangular buildings, the proposed design has used building articulation and layers of architectural detail and materials to break down the hard, rectilinear edges of the structure. Breaking the project into two buildings has further helped to reduce the apparent bulk of the building. There will be almost 50-feet between the proposed building and the corner of the nearest existing building to the south, due in large part to the required 20-foot wide landscaped buffer. This will provide a feeling of openness between the proposed buildings and the development to the south. Because the buildings will lie at a lower elevation to the north of the surrounding residential neighborhood, they will not directly cast shadows onto adjacent properties.

Breaking the project into two buildings has further helped to reduce the apparent bulk of the building. The buildings will also step slightly with the topography, thereby reducing the overall visual mass of the buildings and providing an opportunity to create a mid-block pedestrian connection that will bring light and air to the residents of the development.

- ii. **Rooftop Modulation:** Rooftop features, including the projections at northwest and northeast corners of Building A, add architectural interest and modulation and also serve to completely hide rooftop mechanical units that are incorporated within the features.

**e. Transitions**

The proposed design of the ground level, including the provision of pedestrian-oriented spaces within the building and development of a landscaped streetscape along all street frontages will provide a pedestrian friendly environment along Main Street where one does not exist today. In the future, this will be a vital pedestrian connection from Old Bellevue and the residential neighborhood to the south to the light rail station to the east. **Refer to Condition of Approval regarding public access signage for pedestrian mid-block connection in Section XI of this report.**



In addition, the applicant will be required to provide an easement to the City in the southeast corner of the site. This easement will allow the City to get utilities to the proposed street light. In the future, it will also provide room for the City to design and construct a

pedestrian-only connection up the steep slope to connect the residential neighborhood to the south with Main Street. **Refer to Condition of Approval regarding utility and pedestrian access easement at the end of 106<sup>th</sup> Avenue SE in Section XI of this report.**

**f. Patterns of Activity**

The proposal will include commercial/retail spaces along Main Street and frontage sidewalks will be a minimum of 12.5-foot wide (including curb) with 4-foot wide planting strips to separate autos and pedestrians. Mechanical equipment in above-ground cabinets will not be allowed within any pedestrian travel zone. Benches will be provided along Main Street frontage. All garage exhaust vent(s) are required to be located and designed to avoid impacts to pedestrian areas/connections and pedestrian activity. An east-west pedestrian connection will be provided between Buildings A and B to connect 105<sup>th</sup> Avenue SE with 106<sup>th</sup> Avenue SE. **Refer to Condition of Approval regarding garage exhaust vents and certification and ground-mounted mechanical equipment and screening in Section XI of this report.**

**g. Signage**

The applicant must submit sign permits and a complete sign package for City review and approval prior to the issuance of any occupancy permits or sign permits. All signs must be an integral part of the architectural design, and scaled to the pedestrian environment. **Refer to Condition of Approval regarding sign permit package in Section XI of this report.**

**V. PUBLIC NOTICE AND COMMENT**

Application Date:	July 13, 2012
Notice of Application:	August 16, 2012
Public Meetings:	September 27, 2012 and November 14, 2012
Minimum Comment Period:	August 30, 2012

The minimum required public comment period ended on August 30, 2012, but comments were accepted up to the date of this decision. There were a total of 24 Parties of Record. 12 citizens submitted written comments on the proposal. In addition, two public meetings were held at Bellevue City Hall – one on September 27, 2012 and one on November 14, 2012. The two meetings were attended by 13 and 14 citizens respectively. The comments from the meetings as well as the written comments are summarized below, followed by a response from staff.

- 1. Comment: There already is a lot of traffic on 105<sup>th</sup> Avenue SE and it is hard to make a left turn onto Main Street. This project will exacerbate this situation. Why can't parking be accessed off of Main Street? Can you place a traffic signal at the intersection of 105<sup>th</sup> Avenue SE and Main Street?**

**Response:** The applicant has provided a Traffic Impact Analysis for the proposal and the Transportation Department has reviewed the project for compliance with City codes and standards. The Transportation Department has determined that a new traffic signal cannot be placed at the 105<sup>th</sup> Avenue SE and Main Street intersection due to its proximity to the intersection of Main Street and Bellevue Way to the west.

The only people using 106<sup>th</sup> Avenue SE (a dead end) will be the apartment residents *and* there is an existing traffic light at the intersection of Main Street and 106<sup>th</sup> Avenue SE. Therefore, it is anticipated that the majority of the residents will use the 106<sup>th</sup> Avenue SE parking entry/exit because it will be their quickest and easiest option. **Refer to discussion of traffic impacts by the Transportation Department in Section VII (State Environmental Policy Act) of this report.**

Through application of codes and standards, access to parking off Main Street is not consistent with City policy or code. Main Street is an arterial, and access off of an arterial is not permitted when other options exist. Per the Comprehensive Plan and Land Use Code, the Main Street frontage is also identified as pedestrian-oriented with retail spaces and a robust pedestrian environment. A garage entry would adversely affect the pedestrian realm and would not be consistent with existing codes and policies. **Refer to Section IX.1 of this report for discussion of how this proposal meets Comprehensive Plan policies.**

2. **Comment: There is concern about the amount of traffic that this proposal will generate.**

**Response:** See comments above.

3. **Comment: Walking on 105<sup>th</sup> Avenue SE is dangerous. The improved streetscape, including a new sidewalk, on this proposal might give pedestrians a false sense of security. The applicant should install sidewalks all along 105<sup>th</sup> Avenue NE into the residential neighborhood to the south.**

**Response:** Streetscape improvements are required with each individual development. Developers are responsible for improvements in the rights of way adjacent to their property, but cannot be required to correct a pre-existing substandard condition that is not related to their project impacts. As adjacent properties develop, the developers will be required to make streetscape improvements - which most likely would include sidewalks and/or additional street lighting. Currently, the residential neighborhood to the south of the Alamo Manhattan proposal does not have curbs and gutter and sidewalks. If the residents of the neighborhood would like to explore options regarding new sidewalks, they can contact the Neighborhood Traffic Safety Services at [http://www.bellevuewa.gov/traffic\\_services.htm](http://www.bellevuewa.gov/traffic_services.htm) or 425-452-4560 and refer to the Streets Are For Everyone (SAFE) blog at <http://bellevuentss.wordpress.com/contact/> for more helpful information.

The City has just completed a sidewalk improvement project on the west side of the 105<sup>th</sup> Avenue SE from Main Street to the driveway to 123 105<sup>th</sup> Avenue SE (For the Future Foundation building). With the completion of the Alamo Manhattan frontage improvements on the opposite side of the street that include the provision of two new streetlights and an 8-foot wide sidewalk with a 4-foot wide planting strip, the overall pedestrian experience is anticipated to be safer.

4. **Comment: Residents of the project will park on the public streets in the residential neighborhood. There already is not enough on-street parking for the neighborhood.**

**Response:** It is up to each development and homeowner to provide enough parking as required by the Land Use Code on their own property. Alamo has provided 40

more spaces than the 260 spaces required by application of the Land Use Code. The Code requirements are provided to ensure that new developments are able to accommodate their residents' and visitors' parking needs without using parking on a public street. Note that construction workers working on this project will be required to park in a designated off-site location per conditions of the right of way permit. **Refer to Condition of Approval regarding the right-of-way use permit in Section XI of this report.**

The proposed building is within an urban environment and most of the units are very small, with studios that are approximately 550 square feet and one-bedroom units tend to range from 650 to 750 square feet. It is expected that a majority of the units will have only one occupant and therefore will only need parking for one car and that there will be some residents who do not own cars.

**5. Comment: There are too many units.**

**Response:** The applicant has built to the requirements and guidelines of the Land Use Code for a development within the Downtown Mixed-Use (MU), Perimeter 'A' design district. Within this land use district, the number of residential units within a project is not restricted. Instead, the Land Use Code places restrictions on the size of the development via height limits, floor area ratio requirements, floor plate maximums, etc. This proposal complies with all of these dimensional requirements. **Refer to Section III of this report.**

As part of the Growth Management Act, Bellevue is expected to accommodate the population growth that is projected over the next 20 years. Downtown Bellevue is planned to accommodate over 80 percent of these new housing units in the next 20 years (refer to the Housing Element of the City of Bellevue Comprehensive Plan, Volume 1: General Elements). Per the Comprehensive Plan, "higher densities are encouraged" within the Downtown.

**6. Comment: The building will block views to the north of downtown from the Aria condominiums.**

**Response:** The policy direction regarding view preservation was developed when Downtown regulations were adopted in the early 1980's. Views from public places such as certain types of pedestrian open space were specifically protected, but views from private property were not afforded these same protections in order to achieve desired urban development.

The building is being built to maximum height allowed in the Perimeter District A (55-feet as measured from average *finish* grade), which is significantly less than heights allowed within the Downtown Core. The restricted lower height was placed on properties in the Downtown perimeter because they are adjacent to residential neighborhoods. The properties to the south of the proposal site have historically had the benefit of relatively open views of Downtown due to the fact that the site is currently under-utilized with one story buildings. A significant drop in grade of approximately 20-feet from the property to the south and the proposal site will help lessen the overall impact of the proposal since the average finished grade is significantly lower than properties up the hill. In addition, the majority of the plant material on the steep slope between the proposal and the property to the south will remain and continue to provide significant screening of the proposed building. Lastly,

the Aria condominiums are angled such that their primary view is to the northwest and not oriented directly towards the proposal buildings. **Refer to Sections III.A and IV.C.2.c for additional information and photos regarding the views of the proposal from adjacent properties.**

**7. The building is too tall.**

**Response:** The two buildings on the site will each conform to the height requirements in the Land Use Code, which allows a building to be 55 feet high as measured from *average finish grade*. The applicant has worked with the City to ensure that the height of the building conforms to the Land Use Code requirements for buildings within the Downtown Perimeter A land use district. This included breaking what was originally one large, blocky building into two buildings that step with the grade.

The building height definition that applies to Subdistrict A is found in LUC 20.50.012. Rooftop mechanical screening up to 15-feet above the permitted rooftop elevation is specifically excluded from the overall height calculation. Mechanical screening may be in the form of a parapet wall around the rooftop perimeter or a structural element that encloses the mechanical equipment. In this case the applicant has chosen to use a parapet to screen the rooftop of Buildings A and B. The two taller corner elements on Building A along Main Street provide screening by completely enclosing the mechanical units. The elements are integral to the overall architecture of the building as directed by Urban Design Policy UD-8 of the Comprehensive Plan.

**8. How will safety be handled during construction?**

**Response:** Safety issues will be handled under the right of way permit, including the location of construction signing and pedestrian detour routes and the provision of pedestrian access during construction. **Refer to Condition of Approval regarding the right-of-way use permit in Section XI of this report.**

## VI. TECHNICAL REVIEW

### A. Transportation

#### Site Access

The primary access to the proposed development's residential garage and loading will be provided from the existing signalized intersection of Main Street/ 106<sup>th</sup> Avenue SE. In addition, a single 26-foot wide driveway off of 105<sup>th</sup> Avenue SE will be mainly used to access the building's retail parking. However, the building residents may also utilize the 105<sup>th</sup> Avenue SE driveway since the building garage is all connected.

#### Street Frontage Improvements

In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, the construction of street frontage improvements is required as a condition of development approval. The design of the improvements must conform to the requirements of the Americans with Disabilities Act and the Transportation Development Code (BCC 14.60), and the provisions of the Transportation Department Design Manual. **Refer to Condition of Approval regarding street**

**frontage improvements, civil engineering plans for transportation and building and site plans for transportation in Section XI of this report.**

1. A combined street tree and street light plan is required for review and approval prior to completion of engineering and landscape plans for their installation. The goal is to provide the optimum number of street trees while not compromising the light and safety provided by streetlights. Street trees and streetlights must be shown on the same plan sheet with the proper separation (generally 25 feet apart) and the proper spacing from driveways (ten feet from Point A in standard drawing DEV-7A).

2. The Americans with Disabilities Act (ADA) requires that sidewalk cross slopes not exceed two percent. The sidewalk cross slope may be less than two percent only if the sidewalk has a longitudinal slope sufficient to provide adequate drainage. Bellevue's standard for curb height is six inches, except where curb ramps are needed. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm compliance. New curb and sidewalk shall be constructed in compliance with these requirements. Building elevations shall be consistent with the required curb and sidewalk elevations. Spot elevations must be included in the building plans in a manner that proves that building elevations are designed to correspond to the sidewalk elevations shown in the engineering plans, especially at entrances and other key points. Curb and sidewalk elevations will not be revised to fit the building, and city inspectors may require spot surveys during construction in order to confirm the required elevations.

ADA also requires provision of a consistent travel path for visually handicapped pedestrians. Potential tripping hazards are not allowed in the main pathway. Buildings shall be designed so that doors do not swing out into the pedestrian path. Installations of colored or textured bands to guide pedestrians in the direction of travel are advisable, subject to the requirements for non-standard sidewalk features. ADA-compliant curb ramps shall be installed where needed, consistent with standard drawings TE-12 or TE-13.

3. As part of street pavement widening, the curb, gutter, and sidewalk on both Main Street and 105<sup>th</sup> Avenue SE shall be constructed with a sidewalk width of at least 12 feet, not including the curb. Any planters or tree wells are included in the 12-foot width.

The 105<sup>th</sup> Avenue SE street frontage must be improved to current Downtown Standards to include curbs, gutters, 12-foot wide concrete sidewalk, street lights, and street trees with tree wells and grates. The face of curb must be placed 15 feet from the right-of-way centerline. A street profile must be submitted with construction plans. The curb return at the intersection of Main Street/ 105<sup>th</sup> Avenue SE shall be 25-foot in radius.

Main Street is designated for a future bike route, which will require the face of curb to be placed 31 feet from the street centerline. (This requires moving the curb approx. 1.5 ft. south of the present location.) The 12-foot wide concrete sidewalk and planter area must be designed and located so that they will be properly located and have proper width when the face of the curb is moved back to the 31-foot location. A street profile must be submitted with construction plans.

The 106<sup>th</sup> Avenue SE roadway section will have a 26-foot wide travel surface and include curb and gutter and 6-foot wide concrete sidewalk and pedestrian connection to the end of 106<sup>th</sup> Avenue SE at the site's property line. A street profile must be submitted with construction plans.

4. The design and appearance of the sidewalk and landscaping on Main Street, 105<sup>th</sup> Avenue SE and 106<sup>th</sup> Avenue SE shall comply with the standards and drawings in the Transportation Department Design Manual, including standard drawings TE-11 and DEV-3. The sidewalk shall be constructed of standard concrete with a broom finish and a two-foot by two-foot score pattern, with four-foot by six-foot tree wells, unless both the Transportation Department and the Development Services Department agree to accept any non-standard pattern, color, or other features. Alternative paving samples must be submitted for review. If approved, any non-standard patterns, colors, or other features may be installed only if an agreement is recorded against the property to hold the landowners responsible for maintenance and replacement of all such non-standard sidewalk features.

Any non-standard features or vegetation shall not create a sight obstruction within any required sight triangle, shall not create a tripping or slipping hazard in the sidewalk, and shall not create a raised fixed object in the street's clear zone. The materials and installation methods must meet typical construction requirements. Any non-standard features or vegetation shall not create a sight obstruction within any required sight triangle and shall not create a tripping or slipping hazard in the sidewalk. **Refer to Condition of Approval regarding alternative streetscape designs in Section XI of this report.**

5. Tree wells and other landscaping within the sidewalk on either adjacent city street shall be irrigated with a private metered water source. Electrical connections for lighting in tree wells or planter strips may be allowed, if installed in compliance with the electrical code and subjected to an electrical inspection. Irrigation devices and electrical components shall not create a tripping hazard in the sidewalk. **Refer to Condition of Approval regarding the planting strip/right-of-way irrigation in Section XI of this report.**

6. The driveway on 105<sup>th</sup> Avenue SE shall have an approach width, as defined in standard drawing DEV-7F, of at least 26 feet. The driveway apron design shall be consistent with standard drawing DEV-7F.

7. No new building structure or garage shall be constructed under a street right of way or existing public sidewalk/utility easement. In some conditions (to be finalized during engineering and building plan review), new structure may be allowed under a new sidewalk/utility easement. No soil nailing is allowed under a street right of way or sidewalk/utility easement without an indemnification agreement that protects the city.

8. No new utility vaults that serve only one development will be allowed within a public sidewalk. Vaults serving a broader public purpose may be located within a public sidewalk.

9. Any awning, marquee, balcony, etc. over a sidewalk or utility easement must be at least 16 feet above the sidewalk, or be removable (with an agreement regarding removal and replacement); and must have at least 3 feet horizontal clearance from any streetlight or traffic signal pole. **Refer to Condition of Approval regarding the removable marquees in Section XI of this report.**

10. No fixed objects, including fire hydrants, trees, and streetlight poles, are allowed within ten feet of a driveway edge, defined as Point A in standard drawing Dev-6. Fixed objects are defined as anything with breakaway characteristics stronger than a typical 4 by 4 wooden post.

11. No new overhead utility lines will be allowed within or across any right of way or sidewalk easement, and existing overhead lines must be relocated underground.

#### **Easements**

The applicant shall provide sidewalk and utility easements to the City as needed to encompass the full required width of any sidewalks located outside the city right of way fronting this site. There are some utility easements contained on this site which are affected by this development. Any negative impact that this development has on those easements must be mitigated or easements relinquished. **Refer to Conditions of Approval regarding existing utility easements, easements for traffic signal, street light boxes and vaults, and pedestrian and utilities easements in Section XI of this report.**

Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.

#### **Right of Way Dedication**

Main Street is designated for a future bike route, which will require the face of the curb to be placed 31 feet from the street centerline. To incorporate street improvements which are reasonably necessary to mitigate the direct results of the development, the developer is required to dedicate 1.5 feet such that street surface to back of curb is accommodated within the public right of way. **Refer to Condition of Approval regarding dedication of the right of way in Section XI of this report.**

#### **Holiday Construction & Traffic Restrictions**

From November 15<sup>th</sup> to January 5<sup>th</sup>, construction activities such as hauling and lane closures will be allowed only between the hours of 10:00 p.m. and 6:00 a.m. due to holiday traffic. The dates and times of these restrictions are subject to change. The applicant shall contact the Transportation Department Right-of-Way Section to confirm the specifics of this restriction prior to applying for a Right-of-Way Use Permit, which is issued directly by the Transportation Department. **Refer to Condition of Approval regarding holiday construction and traffic restrictions in Section XI of this report.**

#### **Use of the Right of Way During Construction**

Applicants often request use of the right of way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right of Way Use Permit for such activities must be acquired

prior to issuance of any construction permit including demolition permit. Sidewalks may not be closed except as specifically allowed by a Right of Way Use Permit. **Refer to Condition of Approval regarding the right of way use permit in Section XI of this report.**

#### **Pavement Restoration**

The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration Program, every street in the City of Bellevue has been examined and placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are, "No Street Cuts Permitted," "Overlay Required," and "Standard Trench Restoration." Each category has different trench restoration requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that often comes with small strip patching.

105<sup>th</sup> Avenue SE and Main Street are classified as "Overlay Required" with the City's trench restoration program; therefore, a full grind and overlay from center of the roadway to the new curb line along the entire site's frontage will be required. Details of any trench restoration must be shown on the engineering plans. **Refer to Condition of Approval regarding pavement restoration in Section XI of this report.**

#### **Transportation Management Program**

In order to reduce single occupant vehicle trips and provide enhanced options to employees and infrastructure users, the City has adopted code provisions for a transportation management program. The owner of this development shall, prior to any initial occupancy of the building structure, sign and record an agreement approved by the City of Bellevue to establish a transportation management program to the extent required by BCC14.60.070. and 14.60.080. Included is the requirement to post information regarding transit and ridesharing within a public area of the building. **Refer to Condition of Approval regarding the transportation management program and the implementation of the transportation management program in Section XI of this report.**

#### **B. Utilities**

The Utility Department approval of the design review application is based on the conceptual design only. The utility engineering review, approval and inspection will be completed through the Utility Developer Extension agreement (DEA) – the UE permit. The UE permit should be submitted as early in the process as possible and prior to either the Clearing and Grading and/or Building permits. **Refer to discussion in Section VII.1 & 2 and Condition of Approval regarding the utilities conceptual approval in Section XI of this report.**

#### **C. Clearing and Grading**

The Clear and Grade reviewer has reviewed the plans and materials submitted for this project and determined that clearing and grading portion of this land use application can be approved. The future Clearing and Grading Permit application for

this development must comply with City of Bellevue Clearing and Grading Code (BCC 23.76).

**D. Parks Department**

All new street trees are required to be planted per the Parks Department Best Management Practices in place at time of construction. The irrigation for the right-of-way plantings shall be on a separate meter to allow accessibility for any necessary maintenance work by the City of Bellevue. Lastly, prior to the release of the landscape maintenance assurance device, the applicant and the City of Bellevue shall enter into an agreement regarding future maintenance of the streetscape and right-of-way. **Refer to Conditions of Approval regarding planting strip/right-of-way irrigation, planting in the right-of-way/streetscape, and maintenance agreement with the City of Bellevue in Section XI of this report.**

**E. Fire**

The Fire Department has reviewed the proposal for compliance with applicable codes and standards. As conditioned, this proposal will conform to these requirements. **Refer to Conditions of Approval regarding Fire Department requirements in Section XI of this report.**

**F. Building**

The plans for this decision have not been sufficiently developed for a thorough review under the International Building Code requirements. This review will occur during review of the Building Permit. The plans generally conform to the requirements applicable to this stage of the design process.

**VII. STATE ENVIRONMENTAL POLICY ACT**

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the proposal (see Environmental Checklist in the project file at City Hall Records Office). Therefore, issuance of a Determination of Non-Significance (DNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements with the incorporation by reference of the 2009-2020 Transportation Facilities Plan Final Environmental Impact Statement (TFP EIS), dated November 2008 and adopted April 2009 (available in the Records Office at City Hall). This document analyzes the transportation and air quality impacts of the City's Traffic Task Force recommendations to meet the Comprehensive Plan, Transportation Element, and Mobility Management goals.

This section of the staff report is an addendum to the adopted EIS referenced above. Adverse impacts which are less than significant are usually subject to City Code or Standards which are intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code Section 22.02.140 provides substantive authority to mitigate impacts disclosed through the environmental review process. A discussion of the impacts is noted below together with specific conditions of approval. These impacts will be mitigated through exercise of Code authority as well as through project-specific conditions of approval, contained in Section XI of this report.

**A. Surface Water**

The site is located within the Meydenbauer Drainage Basin. Storm water from the site currently drains to Main Street and is conveyed in catch basins and pipes along the road frontage and eventually discharges to Lake Washington. The site is located in the Meydenbauer No Detention Zone and no detention is required as long as the site can maintain historical drainage patterns on the property. The site does not create enough pollution generating surface to trigger water quality requirements.

The proposed storm drainage outfall for the entire site is to connect to the existing drainage system located in Main Street. The existing system has capacity for the flow expected from this site.

The Storm and Surface Water Engineering Standards provide adequate direction for mitigating this condition.

**B. Utilities**

Domestic water for the site will connect to a 12" cast iron main in Main st. an 8" ductile iron water main located in 105<sup>th</sup> Ave SE. There is sufficient capacity in the water mains to supply the site with domestic water.

Domestic sewer for the site is available in either Main St. or 105<sup>th</sup> Ave SE. If the development proposes greater than 8 units then the side sewer will need to connect into a manhole will an 8" size sewer stub.

**C. Noise**

**Construction Noise:** The Bellevue Noise Control Ordinance BCC 9.18 limits noise levels at the property line to 60 dBA (A-weighted sound level), except from 7:00 a.m. to 6:00 p.m. on weekdays and 9:00 a.m. to 6:00 p.m. on Saturdays that are not legal holidays. Expanded hours may be approved by the Land Use Director under two conditions: to accommodate traffic mitigation and/or for construction of essential public facilities. The site is located near residential uses to the east and west. Restricting the construction hours will reduce noise impacts to neighboring properties. Expanded construction hours during evening or early morning hours should be avoided to minimize noise impacts to nearby residents. In addition, the contractor must use the best available noise abatement technology consistent with feasibility during construction. **Refer to Condition of Approval regarding construction hours and use of best available noise abatement technology in Section XI of this report.**

**Interior Noise:** Bellevue City Code, 9.18, prohibits the approval of new residential structures where the exterior noise level exceed Ldn (day-night average sound level) of 65 dBA anywhere along the site boundary, unless the construction can achieve *interior* noise levels of 40 dBA in sleeping areas and 45 dBA in non-sleeping areas. *Prior to the issuance of any building permit*, the applicant must submit an Acoustical Engineer's report on the proposed construction and the anticipated maximum noise thresholds inside the units facing a street frontage. Before any occupancy permits are issued, the noise levels must be measured inside a random sample of the residential units and the report revised to reflect the results. If the actual noise levels exceed the maximum required thresholds, the acoustical report must include recommendations to modify the construction to meet the interior noise thresholds. **Refer to Conditions of Approval regarding the acoustical engineer's report and noise measurements prior to TCO**

**in Section XI of this report.**

**Garage Exhaust Noise/Air:** The garage exhaust vents must be designed and located to prevent adverse impacts to the pedestrian environment and to the people living in or near the project. The applicant must provide certification by a noise consultant that the operation of the garage exhaust fans will not exceed 60 dBA at the public sidewalk and along the southern property line, and that the velocity and direction of airflow will not adversely affect the pedestrian environment or the residents of the project/neighborhood. In addition, odors from any proposed use may not escape from the building, thereby adversely affecting residents, adjacent neighbors or pedestrians. **Refer to Conditions of Approval regarding garage exhaust and certification in Section XI of this report.**

**D. Transportation**

**Long Term Impacts and Mitigation**

The long-term impacts of development projected to occur in the City by 2020 have been addressed in the City's Transportation Facilities Plan EIS. The impacts of growth which are projected to occur within the City by 2020 are evaluated on the roadway network assuming that all the transportation improvement projects proposed in the City's current Transportation Facilities Plan are in place. The Transportation Facilities Plan EIS divides the City into several Mobility Management Areas (MMAs) for analysis purposes. The Alamo Manhattan Main Street development lies within MMA # 3, which has a 2020 total growth projection of 7,043 multi-family units and 1,259,253 Gross Square Feet (GSF) of retail. This development proposes 260 units of apartments and approximately 7,000 GSF of retail. Therefore, the volume of proposed development is within the assumptions of the Transportation Facilities Plan EIS.

Traffic impact fees are used by the City to fund street improvement projects to alleviate traffic congestion caused by the cumulative impacts of development throughout the City. Payment of the transportation impact fee, as required by BCC 22.16, contributes to the financing of transportation improvement projects in the current adopted Transportation Facilities Plan, and is considered to be adequate mitigation of long-term traffic impacts. Fee payment is required at the time of building permit issuance. **Refer to Condition of Approval regarding the transportation impact fee in Section XI of this report.**

**Mid-Range Impacts and Mitigation**

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more p.m. peak hour trips undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained.

This development will generate approximately 76 new p.m. peak hour trips. City staff distributed and then assigned project-generated trips to the street network using the City's EMME-2 travel forecasting model with the current Capital Investment Program network. By adding the expected project-generated trips to the traffic volumes in the model, the area average levels of service were determined. To create a baseline condition for comparison, the levels of service were also determined using traffic

volumes without the project-generated trips. In this project analysis, 2 system intersections received 20 or more p.m. peak hour trips. Neither the maximum area-average levels of service nor the congestion allowances were exceeded as a result of traffic generated from this proposal. Therefore, the proposed development passes the concurrency test. The concurrency test results are included in the Transportation Department file for this development. The report is available for review in the project file.

The rules of concurrency reservation are outlined in the Traffic Standards Code Director's Rules, updated May 23, 2001. A concurrency determination is issued on the date of issuance of the land use decision. This project complies with the Traffic Standards Code and is receiving a Certificate of Concurrency. See Attachment B for this certificate.

The concurrency determination is reserved to this project at the land use decision date. The concurrency reservation expires one year from the land use decision date unless a complete building permit application is filed (BCC 14.10.010.D). At the time of a complete building permit application, the Certificate of Concurrency will remain in effect for the life of the building permit application, pursuant to BCC 23.05.090H. At issuance of building permit, the Certificate of Concurrency will be extended and remain in effect for one additional year (with the possibility of up to two one-year extensions) as provided for in BCC 23.05.100.

#### **Short Term Operational Impacts and Mitigation**

City staff directed the applicant's traffic consultant, Gibson Traffic Consultants (GTC), to analyze the short term operational impacts of this proposal in order to recommend mitigation if necessary. These impacts included traffic operations conditions during the p.m. peak hours. Issues that were analyzed in the Traffic Impact Analysis, dated October, 2012, included Level of Service (LOS) analysis at nearby intersections, LOS and vehicle queues at the site driveway, site vehicular and pedestrian circulation, transit availability, and accident history analysis for the past five years. Adverse operational impacts due to the proposed development are not anticipated. The GTC analysis is available for review in the project file.

### **VIII. CHANGES TO PROPOSAL DUE TO CITY REVIEW**

- The overall height of the buildings was reduced to comply with the 55-foot height requirement in the Perimeter Design District.
- In order to meet the standards for floorplate and still accommodate the applicant's intent to provide 260 residential units, the project was redesigned as two separate buildings. By splitting the project into two buildings, the applicant was also able to get more light and activity into interior open spaces where previously there were only small, dark interior light wells.
- The Main Street façade was redesigned to add more architectural interest and break up the long expanse of the façade.
- The garbage and pick-up area was relocated off of 106<sup>th</sup> Avenue SE so the trucks did not have to enter the underground garage, nor would they need to stop within a public right-of-way.
- The corner at 105<sup>th</sup> Avenue SE and Main Street is important due to its proximity to

Bellevue Way and Old Bellevue. This corner was enhanced by pulling the retail under the building and through the use of an interesting roof form and a change in siding material at the corner.

- The southern elevation was revised and the corner tower elements were removed to provide additional architectural detail and modulation, which will result in greater architectural interest and a more pleasing view from adjacent properties to the south.

## IX. DECISION CRITERIA

***The Director may approve, or approve with modifications, an application for Design Review if:***

### **1. The proposal is consistent with the Comprehensive Plan.**

This project is consistent with the Comprehensive Plan, which includes policies for the provision of housing along with policies that address the goals of site and building design in the Downtown. The addition of new residential units is consistent with the City's goal of creating housing for its population and the region under the State's Growth Management Act. The most relevant Comprehensive Plan Policies related to this proposal are outlined below:

***Policy LU-9:*** *Maintain compatible use and design with surrounding built environment when considering new development or redevelopment within an already developed area.*

***Response:*** The residential and pedestrian-oriented uses proposed are consistent with the newer development occurring in this part of the Downtown.

***Policy HO-14:*** *Encourage housing development Downtown including innovative, affordable housing.*

***Policy HO-17:*** *Encourage infill development on vacant or under-utilized sites that have adequate urban services and ensure that the infill is compatible with the surrounding neighborhoods.*

***Response:*** The proposed project adds housing within the Downtown – an area that is expected to accommodate a large share of the projected housing growth anticipated over the next 20 years. The existing two parcels are under-utilized sites, with existing one to two story structures, with an overall development density that is significantly less than allowed by code on the site. This proposal seeks to use all of the allowed development opportunity on the site. A mixed-use development that combines residential and retail uses is compatible with the emerging surrounding development.

***Policy UD-2:*** *Support designs for the built environment that are visually stimulating and thoughtful and which convey excellence in architecture and workmanship, and durability in building materials.*

***Response:*** The design of the building, using articulation, varying planes, and a thoughtful mix of durable, high-quality building materials will result in building that will fit well within the Downtown Perimeter context.

**Policy S-DT-26.** *Encourage residential uses to occur in mixed-use structures or complexes.*

**Policy UD-71:** *Permit high intensity residential development subject to design criteria which assures a livable urban environment.*

**Response:** The proposal is a mixed-use residential building with retail uses occupying a large percentage of the ground floor, Main Street frontage. The 260 units are being designed for a Downtown workforce and is consistent with densities found in other Downtown residential projects.

**Policy S-DT-40.** *Enhance the appearance of all types of streets and adjoining sidewalks with street trees, landscaping, water features, pedestrian-scaled lighting, street furniture, paving treatments medians, or other softening treatments as appropriate.*

**Policy UD-75.** *Use urban design features to soften the public right-of-way and sidewalk environment as appropriate. These features include, but are not limited to, street trees, landscaping, water features, raised planter boxes, potted plantings, pedestrian-scaled lighting, street furniture, paving treatments, medians, and the separation of pedestrians from traffic.*

**Policy S-DT-124.** *Utilize sidewalk, landscaping, and green space treatments within Perimeter Areas to provide a transition from Downtown to surrounding residential neighborhoods.*

**Response:** The proposal enhances the pedestrian right-of-way along Main Street, thereby helping to create a pleasant pedestrian experience from Old Bellevue. This has been accomplished by the provision of pedestrian-oriented frontage, continuous marquees, landscaped planting strips with street trees and benches, and interesting paving patterns in the sidewalk. Eventually, this will also be an important pedestrian route to the new light rail station to the east. An upgraded streetscape on 105<sup>th</sup> Avenue SE will also provide a safer and more pleasant pedestrian route for the residential neighborhoods to the south. The applicant will also provide easements along the southern end of 106<sup>th</sup> Avenue SE such that the City may, in the future, provide a pedestrian connection up the slope to the residential neighborhoods to the south. **Refer to Condition of Approval regarding the pedestrian and utility easement at the end of 106<sup>th</sup> Avenue SE in Section XI of this report.**

**Policy UD-8.** *Design rooftop mechanical screening so that it is integral with building architecture. Consider the visual effects of technical advances such as satellite dishes, on building design.*

**Policy UD-11.** *Encourage architectural elements that provide for both rain cover and access to sunlight in pedestrian areas.*

**Policy UD-68.** *Encourage rooflines which create interesting and distinctive forms against the sky within the Downtown.*

**Response:** The proposal provides an interesting roof form that will create an interplay of varying heights, materials, colors and architectural detailing. In addition, the roofline and corner elements provide screening of rooftop mechanical units. Marquees along Main Street will provide year-round weather protection.

**2. The proposal complies with the applicable requirements of this Code.**

**Response:** The tables in Section III.A and B of this report summarize the applicable requirements, and analyze the proposed project for consistency with the applicable requirements. The proposal complies with the LUC requirements for building height, lot coverage, floor area ratio and parking. The proposal also conforms to the applicable design guidelines and decision criteria, as described in Section IV. A-C.

**3. The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent.**

**Response:** Refer to Section IV of this report, which includes information on how the design guidelines and criteria are met.

**4. The proposal is compatible with, and responds to, the existing or intended character, appearance, and quality of development and physical characteristics of the subject property and immediate vicinity.**

**Response:** Most of the development near the proposal site was constructed before the current Downtown dimensional requirements were codified. The proposal is compatible with the scale and character of development that is just beginning to emerge in this neighborhood. The proposed exterior materials are visually harmonious with the surrounding development and the proposed frontage development is consistent with the character of Main Street.

**5. The proposal will be served by adequate public facilities including streets, fire protection, and utilities.**

**Response:** The proposal site has access to water, sewer and electrical services.

**X. DECISION**

After conducting the various administrative reviews associated with the proposal, including applicable Land Use consistency, City Code & Standard compliance reviews, and SEPA, the Director does hereby **APPROVE WITH CONDITIONS** the subject proposal.

**XI. CONDITIONS OF APPROVAL:**

The following conditions are imposed on the applicant under the authority referenced:

**A. GENERAL:**

The following conditions are per each phase of the development:

**1. Compliance with Bellevue City Codes and Ordinances**

Compliance with all applicable Bellevue City Codes and Ordinances including but not limited to the following is required:

Clearing and Grading Code - BCC 23.76	Janney Gwo,	425-452-6190
Bellevue Development Standards	Abdy Farid,	425-452-7698
Transportation Code - BCC 14.60	Abdy Farid,	425-452-7698
Trans. Improvement Program - BCC.22.16	Abdy Farid,	425-452-7698

Right-of-Way Use Permit - BCC 14.30	Tim Stever,	425-452-4294
Bellevue Utilities Code - BCC Title 24	Mark Dewey,	425-452-6179
Construction Codes - BCC Title 23	Lee Kranz,	425-452-2732
Code - BCC Title 20	Sally Nichols,	425-452-2727
Sign Code - BCC Title 22B	Sally Nichols,	425-452-2727
Noise Control - BCC 9.18	Sally Nichols,	425-452-2727
Uniform Fire Code - BCC 23.11	Kevin Carolan,	425-452-7832
Parks Department	Tom Kuykendall,	425-452-7924

**2. Construction Hours**

Noise related to construction is allowed from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020C.1 when necessary to accommodate construction which cannot be undertaken during exempt hours. Prolonged exposure to noise created by extended hour construction activity would likely have a significant impact on the surrounding residents. In order to minimize detriment to nearby residential uses, the contractor shall not rely on City issuance of a blanket exemption from the Noise Control Code during the construction period. Allowances for short term work outside of normal construction hours shall be limited and will be reviewed on a case by case basis to verify necessity and ensure appropriate noise mitigation is utilized to protect surrounding uses and properties. Requests for exemption from the Noise Control Code must be submitted in writing two weeks prior to the scheduled onset of extended hour construction activity. Such request shall include a noise analysis prepared by a noise consultant, including recommendations for achieving the noise limitations of the Noise Ordinance for new residential construction.

AUTHORITY: BCC 9.18.040  
REVIEWER: Sally Nichols, Land Use

**3. Use of Best Available Noise Abatement Technology**

The use of best available noise abatement technology consistent with feasibility is required during construction to mitigate construction noise impacts to surrounding uses.

AUTHORITY: BCC 9.18.020F  
REVIEWER: Sally Nichols, Land Use

**4. Utilities Conceptual Approval**

Utility Department approval of the design review application is based on the conceptual design only. Changes to the site layout may be required to accommodate the utilities after utility engineering is approved. The water, sewer, and storm drainage systems shall be designed per the current City of Bellevue Utility Codes and Utility Engineering Standards. Utilities Department design review, plan approval, and field inspection is performed under the Utility Developer Extension Agreement (DEA) and Utilities Permit Processes. A water, sewer and storm Developer Extension Agreement will be required for the project. All connection charges will be due with the Developer Extension Agreement prior to issuance of the permit.

AUTHORITY: BCC 24.02, 24.04, 24.06  
REVIEWER: Mark Dewey, Utilities

**5. Holiday Construction & Traffic Restrictions**

Construction activities such as hauling and lane closures between November 15<sup>th</sup> and January 5<sup>th</sup> will be allowed only between the hours of 10:00 pm and 6:00 am due to holiday traffic. The Transportation Department will be monitoring traffic and may modify this moratorium accordingly.

AUTHORITY: BCC 14.30.060  
REVIEWER: Tim Stever, Right-of-Way/Transportation

**6. Boundary Line Adjustment**

Prior to the issuance of any construction permits, the applicant shall submit and receive City approval of a Boundary Line Adjustment (LW Permit) to create one project site.

AUTHORITY: LUC 20.45B.260  
REVIEWER: Sally Nichols, Land Use

**7. Fire Department Conditions**

The following shall be addressed in all future permit submissions:

- a) Provide automatic fire sprinklers per NFPA 13. (IFC 903)
- b) The Fire Department Connection (FDC) shall be located at the front entrance with a fire hydrant located within 50 ft of the FDC. (IFC 903/912)
- c) Provide fire alarm system per NFPA 72. (IFC 907)
- d) Provide carbon monoxide alarms for each separate sleeping area if fuel fire appliances are present. (IFC 907.2.9)
- e) Provide complete information for the generator. (IFC Chapters 27 & 34) Need specific information on intake and exhaust for generator room and refueling station.
- f) Provide standpipes for the building. (IFC 905)
- g) Provide an elevator that will accommodate 24 inch by 84 inch ambulance stretcher. (IBC 3002.4)
- h) Provide a Knox Box with keys to the building at an approved location. (IFC 506)
- i) Provide a smoke control system for the building meeting the requirements of Bellevue amended IBC 503 (e).

AUTHORITY: International Fire Code (IFC) 506, 903, 905, 907, & 912, IFC Chapters 27 and 34, IBC 503 (e) & 3002.4, and BCC 5749, Bellevue Fire Department Development Standards (BFDDS)  
REVIEWER: Kevin Carolan, Fire

**8. Provisions For Loading**

The property owner shall provide an off-street loading space which can access a public street. On-street loading and unloading will not be permitted.

AUTHORITY: LUC 20.20.590.K.4  
REVIEWER: Sally Nichols, Land Use & Abdy Farid, Transportation

**B. PRIOR TO CLEARING & GRADING PERMIT:**

The following conditions are imposed to ensure compliance with the relevant decision criteria and Code requirements and to mitigate adverse environmental impacts not addressed through applicable Code provisions. These conditions must be complied with on plans submitted with the Clearing & Grading or Demolition permit application:

**1. Right-Of-Way Use Permit**

Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

- a) Designated truck hauling routes.
- b) Truck loading/unloading activities.
- c) Location of construction fences.
- d) Hours of construction and hauling.
- e) Requirements for leasing of right of way or pedestrian easements.
- f) Provisions for street sweeping, excavation and construction.
- g) Location of construction signing and pedestrian detour routes.
- h) All other construction activities as they affect the public street system.

In addition, the applicant shall submit for review and approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevent access. General materials storage and contractor convenience are not reasons for preventing access.

The applicant shall secure sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.

AUTHORITY: BCC 11.70 & 14.30

REVIEWER: Tim Stever, Transportation

**9. Civil Engineering Plans – Transportation**

Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all street frontage improvements and driveway access and the turnaround must be in conformance with the requirements of the Americans with Disabilities Act, the Transportation Development Code, the provisions of the Transportation Department Design Manual, and specific requirements stated elsewhere in this document. All relevant standard drawings from the Transportation Department Design Manual shall be copied exactly into the final engineering plans. Requirements for the engineering plans include, but are not limited to:

- a) Traffic signs and markings.  
Curb, gutter, sidewalk, and driveway approach design. (The engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as

- needed.)
- b) Handicapped ramps, crosswalk revisions, and crosswalk equipment such as pushbuttons.
  - c) Installation or relocation of streetlights and related equipment.
  - d) Sight distance. (Show the required sight triangles and include any sight obstructions, including those off-site.)
  - e) Location of fixed objects in the sidewalk or near the driveway approach.
  - f) Trench restoration within any right of way or access easement.
  - g) Street lighting plan and specifications.
  - h) Traffic signal modification plan and specifications.

Specific requirements are detailed below.

- a) Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.
- b) Miscellaneous:
  - ♦ Driveway aprons must be constructed in accordance with Design Manual Standard Drawing DEV-7F for 105<sup>th</sup> SE access and DEV-7A for 106<sup>th</sup> SE garage access.
  - ♦ Landings on sloping approaches are not to exceed a 10% slope for a distance of 20 feet approaching the back edge of sidewalks. Driveway grades must be designed to prevent vehicles from bottoming out due to abrupt changes in grade.
  - ♦ Vehicle and pedestrian sight distance must be provided per BCC 14.60.240 and 14.60.241. Sight distance triangles must be shown at all driveway locations and must consider all fixed objects and mature landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance.

AUTHORITY: BCC 14.60, Transportation Department Design Manual  
REVIEWER: Abdy Farid, Transportation

#### **10. Alternative Streetscape Designs**

Any sidewalk design that includes non-standard pattern, color, or other features such as street furnishings not required by transportation Department standards (benches, raised planters, etc.) will be allowed only if it meets Transportation and ADA requirements, and is reviewed and approved by both the Transportation Department and Land Use. If alternative paving materials are proposed, samples must be submitted for review. If approved, any non-standard patterns, colors, or other features may be installed only if an agreement is recorded against the property to hold the landowners responsible for maintenance and replacement of all such non-standard sidewalk features.

AUTHORITY: BCC 14.30  
REVIEWER: Abdy Farid, Transportation

**C. PRIOR TO BUILDING PERMIT:**

The following conditions are required by City Code. Unless specified otherwise below, these conditions must be complied with on plans submitted with the Building permit application:

**1. Transportation Impact Fee**

Payment of the traffic impact fee will be required at the time of building permit issuance. This fee is subject to change and the fee schedule in effect at the time of building permit issuance for the above ground building permit (i.e., the podium, the tower) will apply.

AUTHORITY: BCC 22.16  
REVIEWER: Abdy Farid, Transportation

**2. Building and Site Plans – Transportation**

The building grade and elevations shall be consistent with the curb and sidewalk grade shown in the approved civil engineering plans. During construction, city inspectors may require additional survey work at any time in order to confirm proper elevations. Building plans, landscaping plans, and architectural site plans must accommodate on-site traffic markings and signs and driveway design as specified in the engineering plans. Building plans, landscaping plans, and architectural site plans must comply with vehicle and pedestrian sight distance requirements, as shown on the engineering plans.

AUTHORITY: BCC 14.60.060, 110, 120, 150, 180, 181, 190, 240, 241  
REVIEWER: Abdy Farid, Transportation

**3. Existing Utility Easements**

There are utility easements contained on this site which are affected by this development. Any negative impact that this development has on those easements must be mitigated or easements relinquished.

AUTHORITY: BCC 14.60.100  
REVIEWER: Tim Stever, Transportation

**4. Easements for Traffic Signal, Street Light Boxes & Vaults**

The applicant shall provide easements to the City for location of traffic signal and street light facilities such as above-grade boxes and below-grade vaults between the building and sidewalk within the landscape area.

AUTHORITY: BCC 14.60.100  
REVIEWER: Abdy Farid, Transportation

**5. Pedestrian and Utilities Easements**

The applicant shall provide sidewalk and utility easements to the City such that sidewalks outside of the City right of way along the property frontage are located within a pedestrian easement area.

AUTHORITY: BCC 14.60.100  
REVIEWER: Abdy Farid, Transportation

**6. Dedication of Right of Way**

The applicant shall dedicate 1.5 feet of right of way on Main Street to the City along the property frontage such that street improvements to and including the back of new curb are located within the public right of way.

AUTHORITY: BCC 14.60.090  
REVIEWER: Abdy Farid, Transportation

**7. Transportation Management Program**

The owner of the property being developed shall sign and record at the King County Office of Records and Elections an agreement to establish a Transportation Management Program to the extent required by Sections 14.60.070 and 14.60.080.

AUTHORITY: BCC 14.60.070, 14.60.080  
REVIEWER: Abdy Farid, Transportation

**8. Removable Marquees**

Any awning, marquee, balcony, etc. over a sidewalk or utility easement must be at least 16 feet above grade, or be removable (with an agreement regarding removal and replacement); and must have at least 3 feet horizontal clearance from any streetlight or traffic signal pole. No supports will be allowed within a public sidewalk. No structure will be allowed above a city right of way without a long-term lease of airspace.

AUTHORITY: BCC 24.04.160.B4, LUC 20.25A.030  
REVIEWER: Abdy Farid, Transportation

**9. Ground-Mounted Mechanical Equipment, Street Furniture and Screening**

No mechanical equipment (including power, telephone, traffic control, etc.) shall be located in above ground cabinets in a "pedestrian travel zone." Such equipment shall be located in underground vaults, in a building, or substantially screened in planting areas per the approval of the Transportation Department and Land Use. No street furniture shall be located within the "pedestrian travel zone."

AUTHORITY: LUC 20.20.650, 20.25A.110.B.6  
REVIEWER: Sally Nichols, Land Use

**10. Garage Exhaust and Certification**

Garage vents shall be located such that exhaust and odors are directed away from pedestrians. All vents shall be architecturally integrated to the building.

Provide certification by a noise consultant or mechanical engineer prior to issuance of any mechanical permits that the noise from the exhaust fans will not exceed 60 dBA at the public sidewalk and the southern property line and a determination by the City's Mechanical Plans Examiner that the velocity and direction of airflows from the exhaust system within pedestrian areas not exceed 500 CFM.

AUTHORITY: BCC 9.18.045  
REVIEWER: Sally Nichols, Land Use

**12. Amenity Designs and Statistics**

The building permit submittal plans and drawings shall include a detailed design for each proposed FAR amenity with a design component and a statistical summary for each amenity.

AUTHORITY: LUC 20.25A.030  
REVIEWER: Sally Nichols, Land Use

**13. Final Landscape Plan**

A Final Landscape Plan shall be submitted with the Building Permit application for review by Land Use and the Parks Department. Provide details for any electrical outlets that are proposed to be located within the planting strips.

AUTHORITY: LUC 20.20.520.I  
REVIEWER: Sally Nichols, Land Use

**14. Landscape Plant Types/Downtown Buffer**

The landscape plan submitted with the Design review application shall be refined for the 20-foot wide landscaped Downtown buffer. The plan shall be comprised of native and adapted plant species, including trees, to minimize irrigation demands and reduce maintenance requirements.

AUTHORITY: LUC 20.20.520.I  
REVIEWER: Sally Nichols, Land Use

**15. Planting Strip/Right-of-Way Irrigation**

The irrigation system for all street trees and landscaping within the right-of-way shall include a separate water meter and controller that can be accessed at all times by the City of Bellevue. Include automatic operation and rain sensors to override the automatic cycle if needed. Where feasible, drip irrigation should be used to reduce water loss due to over-spray and evaporation.

If the irrigated area exceeds 500 square feet then the landscape irrigation budgeting section of the Water Code applies.

Prior to building permit approval, the final irrigation plan must be reviewed and approved by the City of Bellevue Utilities and Parks Departments.

Electrical connections for lighting in tree wells or planter strips may be allowed, if installed in compliance with the electrical code and subjected to an electrical inspection. As-built drawings shall be submitted to the City of Bellevue Parks Department and Land Use. Irrigation devices and electrical components shall not create a tripping hazard in the sidewalk.

AUTHORITY: BCC 24.02.205, LUC 20.25A.060.B  
REVIEWER: Sally Nichols, Land Use

**16. Planting in Right-of-Way/Streetscape**

a) Planting shall be done according to the Parks Department Best Management Practices and Design Standards in place at the time of construction.

- b) A Parks Department representative shall be on-site to inspect street trees prior to planting and at the time of planting to observe the installation. Contact Parks Department Resource Management at (425) 45206855 at least 24 hours before planting to schedule the inspection.

AUTHORITY: LUC 20.25A.060.B  
REVIEWER: Tom Kuykendall, Parks Department  
Sally Nichols, Land Use

**17. Exterior Lighting**

The applicant shall submit a detailed design for all building exterior lighting. Lighting shall be contained to this site and not be permitted to spill-over to adjacent sites. In addition, information on retail lighting along the pedestrian-oriented frontages along Main Street shall be supplied as well. All exterior building lighting is required to include cut-off shields to minimize impacts of light and glare.

AUTHORITY: LUC 20.25A.110.A.5  
REVIEWER: Sally Nichols, Land Use

**18. Acoustical Engineer's Report**

The applicant shall submit an Acoustical Engineer's report on the proposed construction and the anticipated maximum noise thresholds inside the units facing each street frontage, including the alleys.

AUTHORITY: BCC 9.18  
REVIEWER: Sally Nichols, Land Use

**19. Storefront Glazing**

To ensure visibility from the sidewalk into the retail use spaces, clear (non-tinted, non-reflective) window glass shall be used. A sample of the glass shall be submitted to the Land Use Division of DSD for review and approval prior to building permit issuance. The storefront windows shall not be obstructed with devices such as curtains, blinds, etc. to allow continuous visual access into the spaces.

AUTHORITY: LUC 20.25A.030.C.1, 20.25A.090.E, 20.30F.145  
REVIEWER: Sally Nichols, Land Use

**D. PRIOR TO ANY TCO:**

The following conditions are required by City Code and supported by City Policy. The conditions shall be complied with prior to issuance of the Temporary Certificate of Occupancy (TCO):

**1. Street Frontage Improvements**

All street frontage improvements and other required transportation elements, including pavement widening, street light revisions, must be constructed by the applicant and accepted by the City Inspector. All existing street light and apparatus affected by this development, including power sources, must be relocated as necessary. All required improvements must be constructed as per the approved

plans or as per direction of the Transportation Department inspector. Bonding or other types of assurance devices will not be accepted in lieu of construction. Specific requirements are detailed below:

- a) The applicant is responsible for half-roadway improvements on 105<sup>th</sup> SE and pavement widening on Main Street including a new 25-foot radius curb at the intersection of Main Street/ 105<sup>th</sup> SE. The frontage improvements call for 15.5 feet half-roadway section on 105<sup>th</sup> SE and 31.5 feet half-roadway section on Main Street. Half-roadway section is measured from the centerline of the right of way to the new back of curb.
- b) The applicant is responsible for traffic signal pole and/or equipment relocation at the southwest corner of the Main Street/ 106<sup>th</sup> SE intersection, and any associated modifications to signal equipment and conduit necessary as a result of this work. In addition, the developer will be required to purchase from the SCATS consultant a new SCATS intersection personality (i.e. controller software modifications).
- c) The applicant is responsible for street channelization, marking and signage at the Main Street/ 105<sup>th</sup> SE and Main Street/ 106<sup>th</sup> SE intersections and along the 105<sup>th</sup> SE, Main Street and 106<sup>th</sup> SE roadways.
- d) Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.
- e) Landings on sloping approaches are not to exceed a 10% slope for a distance of 20 feet approaching the back edge of sidewalks. Driveway grades must be designed to prevent vehicles from bottoming out due to abrupt changes in grade.
- f) Vehicle and pedestrian sight distance must be provided per BCC 14.60.240 and 14.60.241. Sight distance triangles must be shown at all driveway locations and must consider all fixed objects and mature landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance.

AUTHORITY: BCC 14.60.090, 110, 120, 150, 181, 200, 210, 240, 241; Transportation Department Design Manual Sections 9, 12, 14, 19, 20; and Transportation Department Design Manual Standard Drawings DEV-2, DEV-3, DEV-7A, DEV-7F, DEV-10, TE-4, TE-5, TE-7, TE-10, TE-11, TE-12 and TE-21.

REVIEWER: Abdy Farid, Transportation

## **2. Pavement Restoration**

Pavement restoration associated with street frontage improvements or to repair damaged street surfaces shall be provided as follows:

- 105th Avenue SE and Main Street: Based on these streets' pavement classifications, they are designated as "Overlay Required." Street cutting is permitted only with extraordinary pavement restoration. A full grind and overlay from center of the roadway to the new curb line along the entire site's frontage will be required.

AUTHORITY: BCC 14.60.250; Design Manual Design Standard #21  
REVIEWER: Tim Stever, Transportation

**3. Implementation of the Transportation Management Program**

The Transportation Management Program required by Bellevue City Code Sections 14.60.070 and 14.60.080 per a condition of approval above must be functional prior to issuance of the initial certificate of occupancy.

AUTHORITY: BCC 14.60.070, 14.60.080  
REVIEWER: Abdy Farid, Transportation

**4. Solid Waste/Recycling Containers & Recording**

The applicant shall sign and record with King County an agreement that runs with the property which requires all recycling bins, refuse containers and similar items to be moved out of the building immediately prior to pick up by building staff, and moved back into the building immediately upon completion of pick-up. No trash containers will be allowed to remain in and/or block the public right-of-way/alley.

AUTHORITY: LUC 20.20.725 and 20.25A.110.B  
REVIEWER: Sally Nichols, Land Use

**5. Bonus System & Pedestrian-Oriented Frontage Recording**

The applicant shall record 1) a copy of the approved bonus point calculations (Tables 2, 3 and 4 in Section III.B of this report), 2) an agreement to maintain only pedestrian-oriented frontage uses in the retail tenant spaces on NE 1<sup>st</sup> Street and 102<sup>nd</sup> Avenue NE, and 3) the project drawings and conditions of this Design Review with the King County Office of Records and Elections.

AUTHORITY: LUC 20.25A.020.D.3  
REVIEWER: Sally Nichols, Land Use

**6. Landscape Installation Assurance Device**

All site landscaping shall be 100% complete per the plan approved by the City. Alternatively, the following may be submitted: 1) a red-marked plan identifying which landscape areas are incomplete; 2) an estimate for the total cost to complete these areas; and 3) a notarized Assignment of Savings dedicated to the City for 150% of the estimated cost to complete these areas per the approved Landscape Plan. The assurance device will be released upon complete installation, inspection and approval by the land use reviewer for the project.

AUTHORITY: LUC 20.40.490  
REVIEWER: Sally Nichols, Land Use

**7. Landscape Maintenance Assurance Device**

File with the Development Services Department a landscape maintenance assurance device for a one-year period in the form of an assignment of savings or letter of credit for 20% of the cost of labor and materials for all required landscaping.

AUTHORITY: LUC 20.20.520.K and 20.40.490  
REVIEWER: Sally Nichols, Land Use

**8. Maintenance Agreement with the City of Bellevue**

After one-year, the landscape shall be inspected by Land Use and the Parks Department. Prior to the release of the Landscape Maintenance Assurance Device, the applicant and the City of Bellevue shall enter into an agreement to determine future maintenance responsibilities for the streetscape and streetscape plantings.

AUTHORITY: LUC 20.25A.060  
REVIEWER: Sally Nichols, Land Use

**9. Sign Permit Package**

The applicant shall submit a complete sign design package for City review and approval prior to the issuance of any occupancy permits for the building, tenant improvement permits for the commercial spaces, or sign permits. All signs shall be an integral part of the architectural design and signs at or near the street shall be scaled to the pedestrian environment.

AUTHORITY: LUC 20.25A.110.B.7.a-c, BCC 22B.10 (Sign Code)  
REVIEWER: Sally Nichols, Land Use

**10. Pedestrian and Utility Access Easement at the end of 106<sup>th</sup> Avenue SE**

The applicant will be required to grant a 10 ft. (east-west) x 20 ft. (north-south) utility and pedestrian access easement in the southeast corner of the proposal site to the City to provide a connection to the proposed street light and to allow for a future, City-constructed pedestrian path up the hill to the south of the project site within the 106<sup>th</sup> Avenue SE right-of-way. Refer to Section IV.C.2.e of this report.

AUTHORITY: LUC 20.25A.060, BCC 14.60.100  
REVIEWER: Sally Nichols, Land Use  
Abdy Farid, Transportation

**11. Public Access Signage for Pedestrian Mid-Block Connection**

Two "Public Access" signs (one at each entrance to the courtyard connecting 105<sup>th</sup> Avenue SE to 106<sup>th</sup> Avenue SE), as depicted in the Bellevue Wayfinding Manual, shall be prominently located to convey the message that pedestrians may use the alleys as mid-block pedestrian connections. The exact location of these signs (generally placed on building or planter walls) shall be coordinated with and approved by Land Use. At a minimum, the hours that the connection shall be open are 7:00 am to 9:00 pm.

AUTHORITY: LUC 20.25A.060 and 20.30F.145  
REVIEWER: Sally Nichols, Land Use

**12. Noise Measurements**

The noise levels must be measured inside a random sample of the residential units facing each street and the original acoustical report shall be revised to reflect the results. If the actual noise levels exceed the maximum required thresholds, the

acoustical report must include recommendations to modify the construction to meet the interior noise thresholds.

AUTHORITY: BCC 9.18  
REVIEWER: Sally Nichols, Land Use

## **E. PRIOR TO ISSUANCE OF SIGN PERMITS**

### **1. Individual Sign Permits**

All signage shall be pedestrian oriented. Design review of individual signs and compliance with the approved sign package will occur through review of each sign permit application. The sign package plans, elevations, and/or sketches shall include but are not limited to:

1. Location
2. Illumination
3. Color and Materials
4. Design (no cabinet or freestanding signs are permitted)

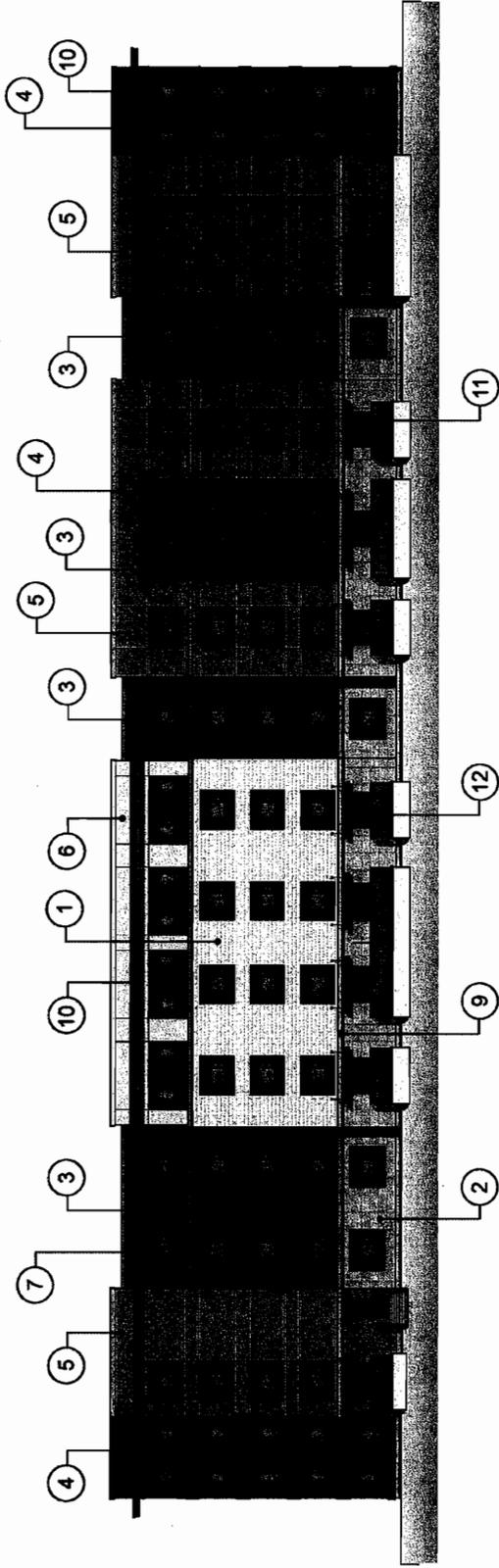
AUTHORITY: LUC 20.25A.110, LUC 20.30F, BCC 22B.10.025.B.4  
REVIEWER: Sally Nichols, Land Use

#### **Attachments:**

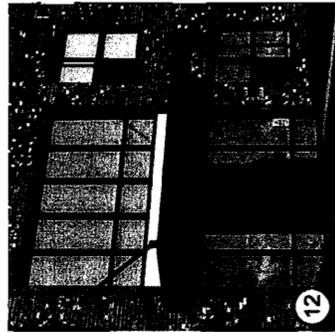
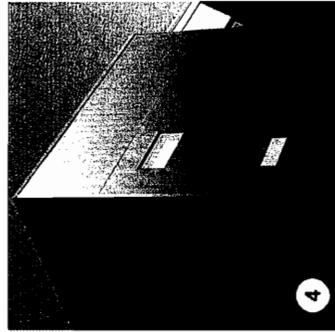
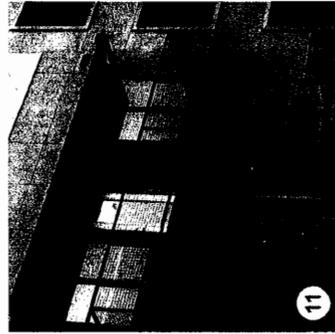
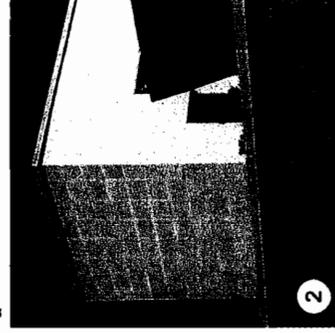
- A. Design Review Submittal Drawings
- B. Certificate of Concurrency
- C. Environmental Checklist







ELEVATION ALONG DOWNTOWN BOUNDARY



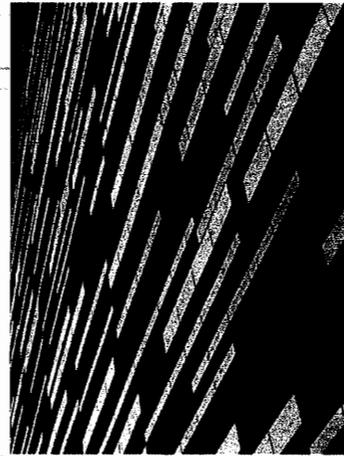
CONTEXT IMAGES

ALAMO MANHATTAN

MAIN STREET BELLEVUE - MATERIAL SELECTIONS

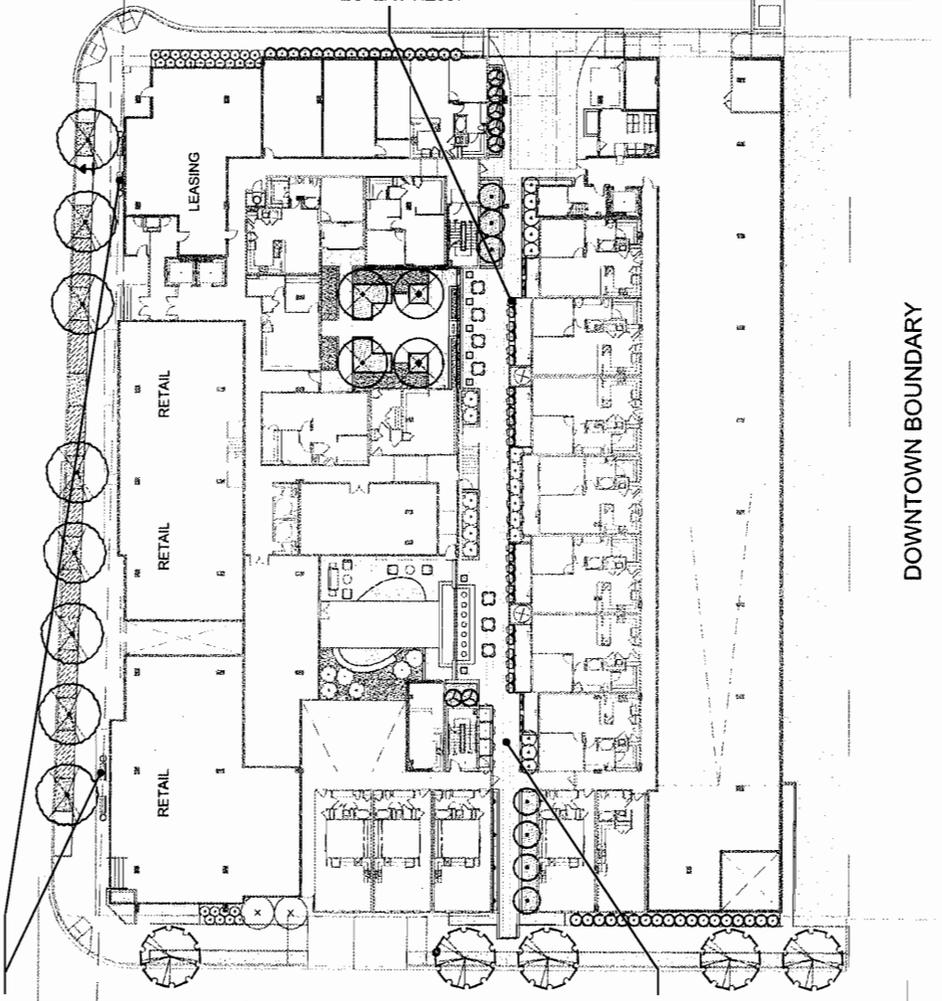
**Hensley Lamkin Rachel, Inc.**  
ARCHITECTURE AND PLANNING





NOTE:  
ALL MATERIAL SELECTIONS ON THIS SHEET ARE SHOWING THE DESIGN INTENT AND ARE SUBJECT TO CHANGE UPON FINAL PRICING

MAIN STREET



DOWNTOWN BOUNDARY



Case 137

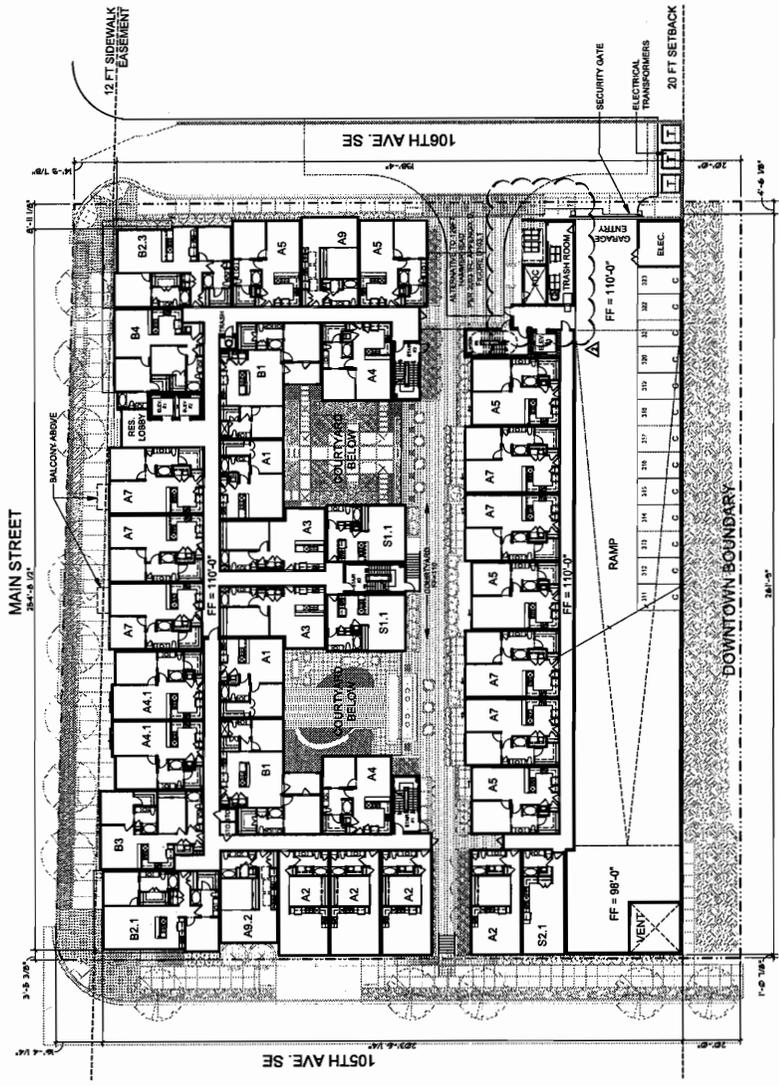
# BELLEVUE ALAMO APARTMENTS

Bellevue, WA

CITY SUBMITTAL: November 9, 2012

studioOutside





**FLOOR PLAN**  
**GARAGE LEVEL 1 / RES. LEVEL 2 (411-UL2)**  
SCALE: 1/8" = 1'-0"

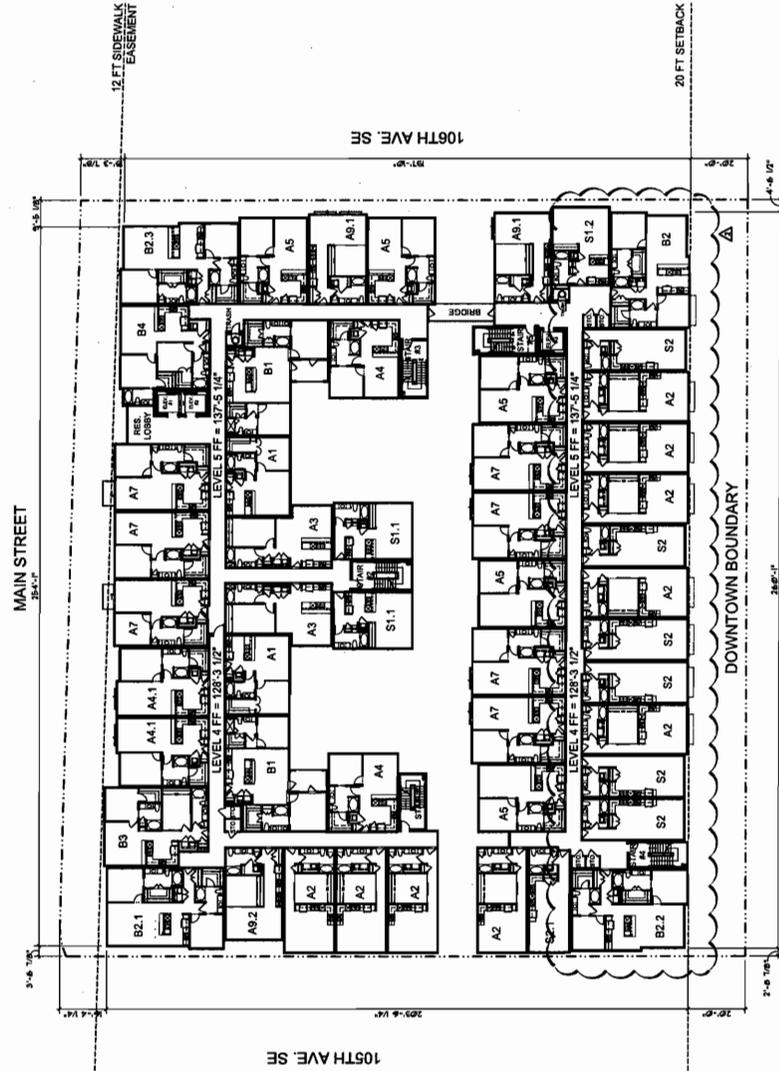




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PROJECT NO.	02
DRAWING DATE	1/15
CHECKED BY	WJL
DATE	1/15/15
PROJECT BY	WJL
CONTRACTOR BY	WJL
DATE	1/15/15
REVISIONS	
1	REVISION 1 - ISSUE
2	REVISION 2 - ISSUE

10025  
**A26**  
FLOOR PLAN  
L41 - L45  
V. 09.12.12



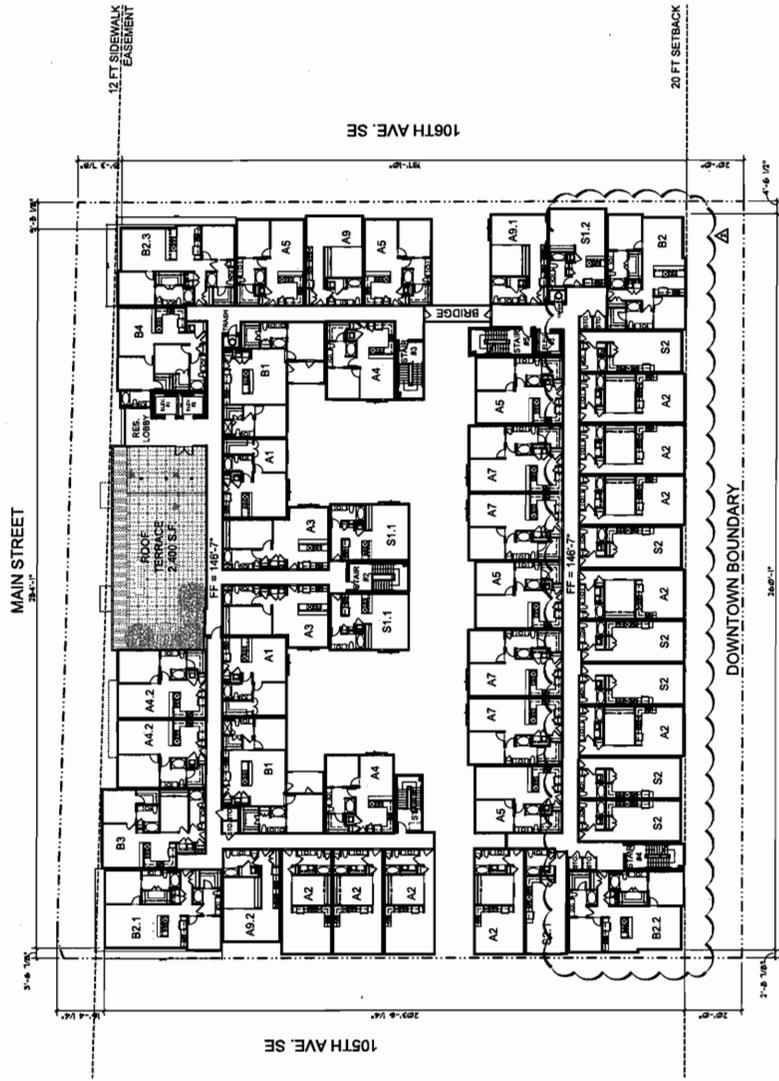
FLOOR PLAN  
RESIDENTIAL LEVELS 415 (UL4-JUL5)  
SCALE: 1/8" = 1'-0"



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PROJECT NO.	01
DATE	11/11/05
DESIGNED BY	—
CHECKED BY	—
SCALE	AS SHOWN
DATE	11/11/05
CONTRACTOR	—
CONSTRUCTION BY	—
REVISIONS	—
DESCRIPTION 1 - GROUND	—
DESCRIPTION 2 - THIRD	—

12025  
**A27**  
FLOOR PLAN  
11/11/05  
V. 05.17.12



FLOOR PLAN  
RESIDENTIAL LEVEL 6 (UL6)  
SCALE: 1/8" = 1'-0"

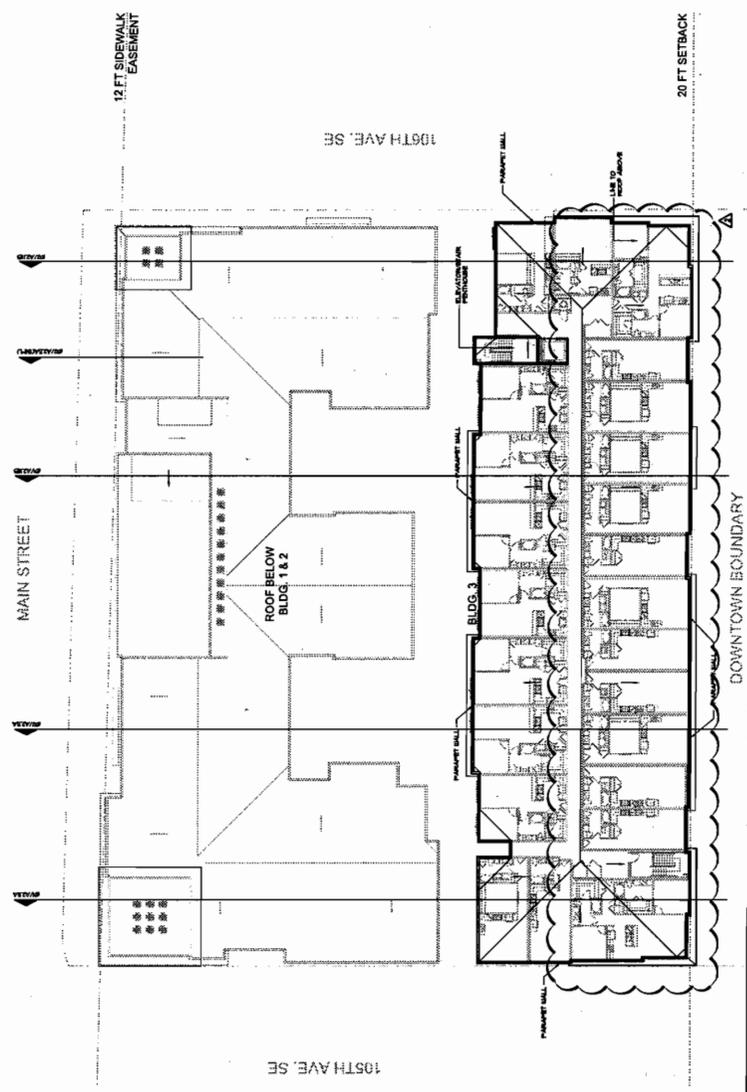




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PROJECT NO:	02
DATE:	7/25/12
DESIGNED BY:	TR
CHECKED BY:	TR
DATE:	7/25/12
PROJECT NAME:	ALAMO MANHATTAN
CONTRACT NO.:	12-0025
CONTRACT DATE:	12/01/11
CONTRACT VALUE:	\$1,000,000
CONTRACT TYPE:	GENERAL CONTRACT
CONTRACTOR:	TR
ARCHITECT:	TR
ENGINEER:	TR
PLUMBER:	TR
ELECTRICIAN:	TR
Mechanical:	TR
Structural:	TR
Other:	TR

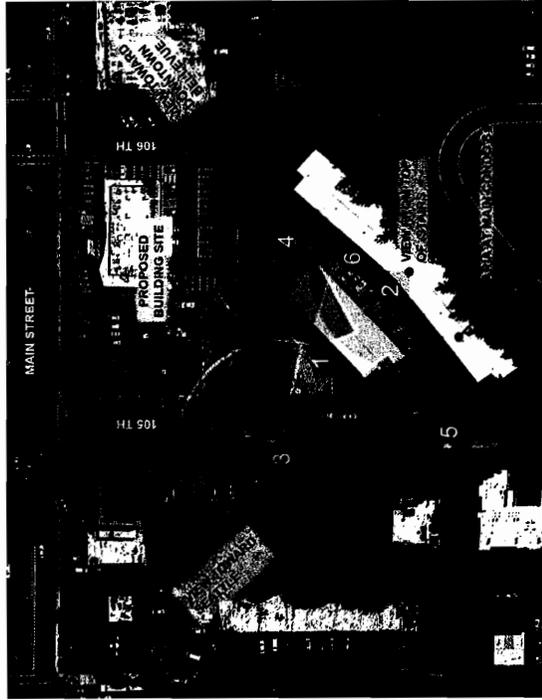
120025  
**A2.9**  
 ROOF PLAN  
 V. 09.17.12



**ROOF PLAN**  
**BLDG. A + BLDG. B**  
 SCALE: 1/8" = 1'-0"



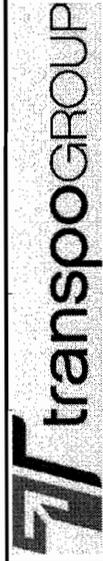
2



**Hensley Lamkin Rachel, Inc.**  
ARCHITECTURE AND PLANNING

**MAIN STREET BELLEVUE**  
3D VIEWS

**ALAMO MANHATTAN**



transpogroup		Package: Alamo Manhattan - Main Street			
Reviewer:		Submittal: Preliminary Signal and Lighting Plans			
Organization:	City of Bellevue	Date: 09/17/2012			
Response Codes: A - Incorporated; B - Open/Under Review; C - Not Incorporated; D - Not In Scope					
E - No Longer Applicable; F - Noted, No Action Required					
Comment No.	Date	Review Comment	Initial	Response	Final Action
1	9/17/12	A traffic signal plan is required for review and approval prior to design review permit issuance.	JEF	Traffic signal plan included in submittal	A
2	9/17/12	A combined street tree and street light plan is required for review and approval prior to completion of engineering and landscape plans for their installation. The goal is to provide the optimum number of street trees while not compromising the light and safety provided by streetlights. Street trees and streetlights must be shown on the same plan sheet with the proper separation (generally 25 feet apart) and the proper spacing from driveways (ten feet from Point A in standard drawing DEV-7A).	JEF	Street light plan showing street trees included in submittal. Due to the 5' spacing between driveways, the street light pole located in the sidewalk on 106th Ave (in the southeast corner of the site) does not meet the 10' minimum spacing. This light pole could possibly be integrated with the proposed retaining wall at the southern edge of the site in order to achieve 10' spacing from driveways.	B
3	9/17/12	Show two access ramps at the corner of Main/ 106th.		Our plans have been revised to show two access ramps at the corner of Main Street and 106th Avenue.	
4	9/17/12	Pedestrian push-button separation at the corner of Main/106th. This will mean at least one additional pole, but could mean more depending on how much the design impacts the current signal pole location.	JEF	Push button separation provided as requested. New mastarm pole and pedestrian signal pole provided on the SW corner of Main/106th.	A
5	9/17/12	Street lighting analysis for all surrounding streets (i.e., 105th, Main, 106th) and the two intersections (Main/105th and Main/ 106th)	JEF	Street lighting analysis provided; see light levels table on illumination plan.	A
5a	9/17/12	Main Street as a Major Arterial	JEF	Meets average and uniformity targets	A

# MAIN STREET

ALAMO MANHATTAN  
BELLEVUE, WA

Hensley Lamkin Rachel, Inc.  
 1401 QUINLAN DRIVE  
 SUITE 300  
 BELLEVUE, WA 98005  
 PH 206 726-9400  
 FAX 206 726-9401

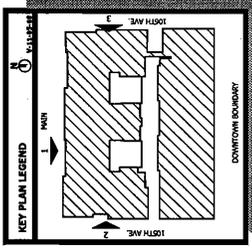


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PROJECT NAME: A2.10A  
 DRAWING TITLE: EXTERIOR ELEVATIONS  
 DATE: 12/02/15  
 DRAWN BY: JAL/JS  
 CHECKED BY: JAL/JS  
 SCALE: AS SHOWN  
 SHEET NO.: 22 OF 22  
 CONTRACTOR: JAL/JS  
 CONSTRUCTION BY: JAL/JS  
 DATE: 12/02/15

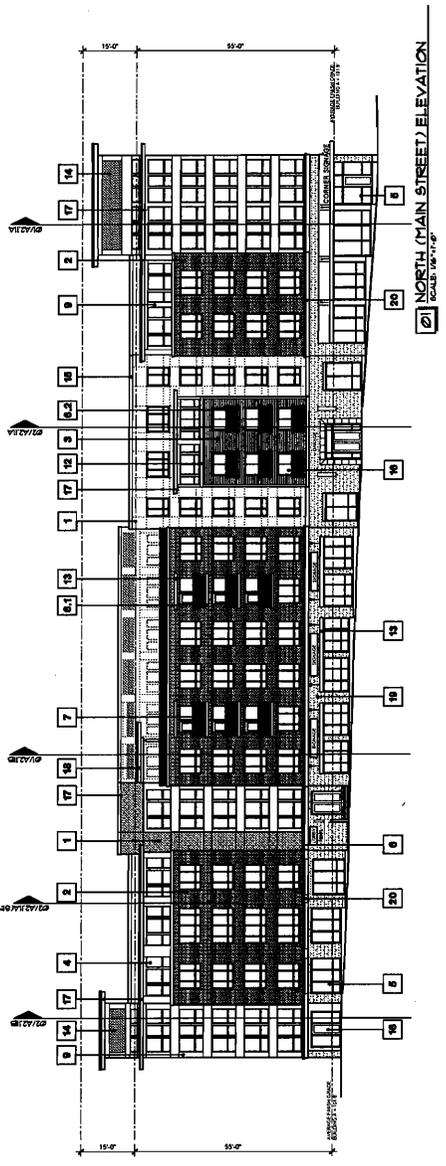
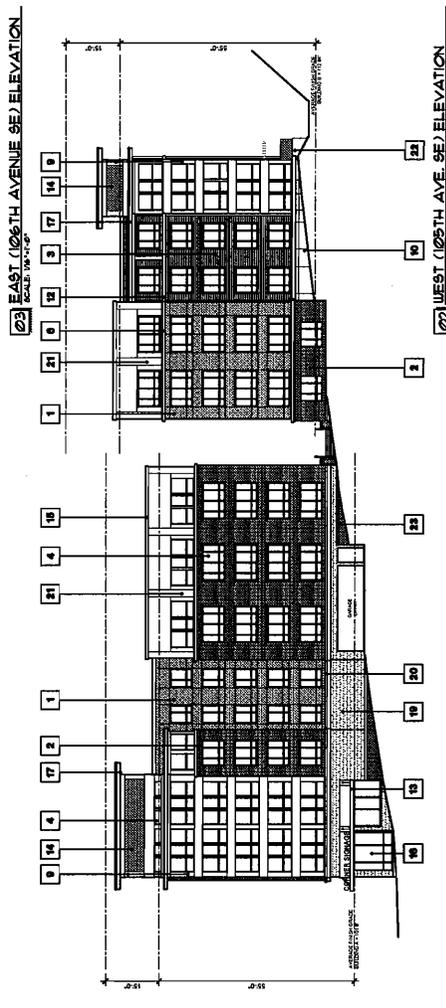
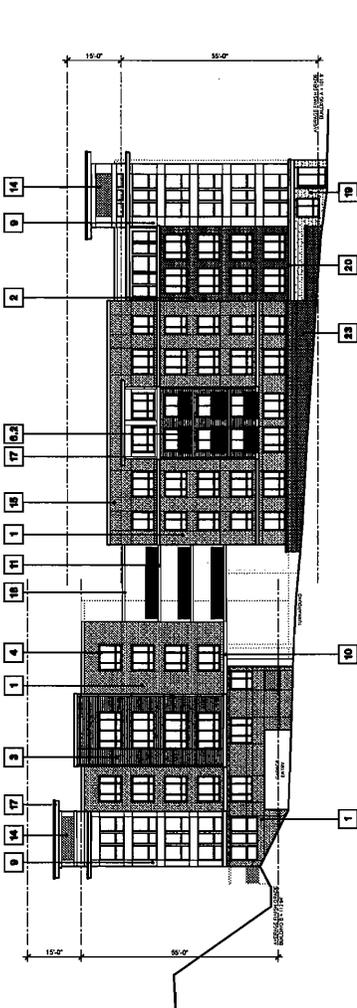
UNITS: METRIC  
 DIMENSIONS: METRIC  
 FINISHES: METRIC  
 MATERIALS: METRIC  
 COLORS: METRIC

NOTE:  
 1. WINDOW AND DOOR SIZES AT UNITS TO BE 4'-1" UNLESS NOTED OTHERWISE.  
 2. RETAIL SPACE TO HAVE A MIN. CEILING HEIGHT OF 12'-0".  
 3. CONCRETE SHALL BE 4000 PSI.



**ELEVATION LEGEND**

1	STOOD FINISH SYSTEM
2	STACK BOND - BRICK VENEER
3	SPRING SYSTEM
4	VINYL WINDOW
5	STAIRWELL SYSTEM
6	COUNTERSINK BALCONY SYSTEM #1
7	APPLIED BALCONY SYSTEM #2
8	METAL CASING/MAIL SYSTEM
9	STUCCO DOWN TURN
10	METAL TRIM
11	GRASSO CONCRETE
12	BRIDGE
13	COMPRESSIBLE TERN
14	METAL CANOPY
15	PAINTED METAL LOUGES
16	PAINTED COPING CAP
17	DOOR SYSTEM
18	PAINTED FACIA BOARD
19	ROOF STRUCTURE
20	BURNISHED BRICK
21	CAST STONE
22	PAINTED WOOD TRIM
23	WOOD DECK PATIO
24	PLANTERS





# MAIN STREET

ALAMO MANHATTAN  
BELLEVUE, WA

Hensley Lamkin Rachel, Inc.

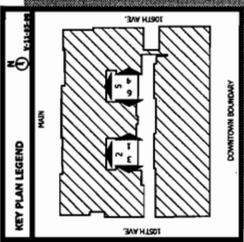
SUITE 200  
1525 QUINCY DRIVE  
DAILY, WA 98004  
PH 920.728.9400  
FAX 920.728.9401



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PROJECT NO: 02  
DRAWN BY: LD  
CHECKED BY:  
DATE: 10/20/10  
CONTRACT NO: 10025  
CONTRACTOR: HENSELY LAMKIN RACHEL, INC.

10025  
**A2.10C**  
INTERIOR COURT-YARD ELEVATION

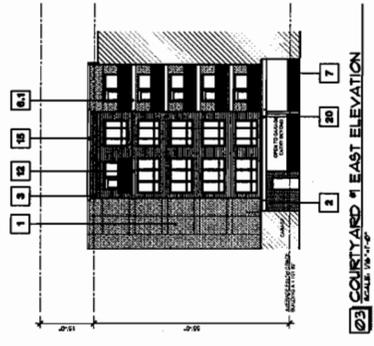
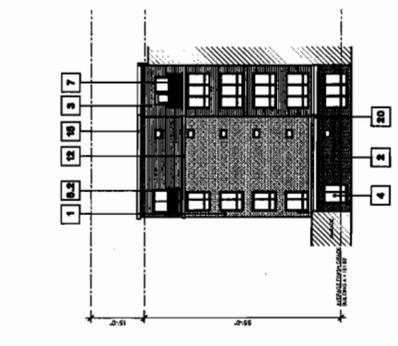
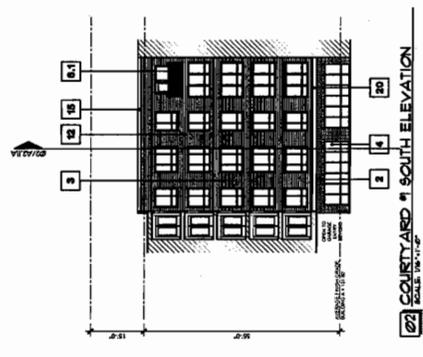
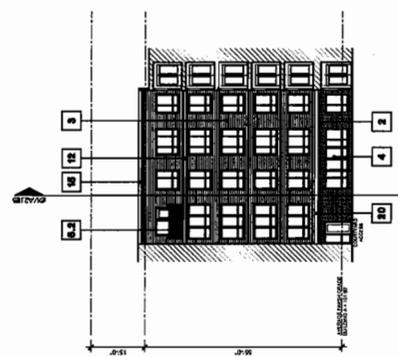
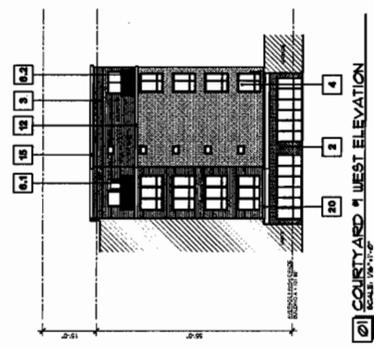
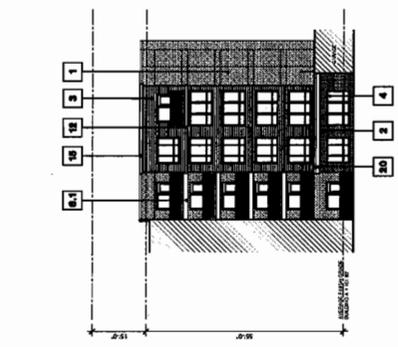


**ELEVATION LEGEND**

- 1 STUCCO FINISH SYSTEM
- 2 STUCCO BOND - BRICK VENEER
- 3 SOUNE SYSTEM
- 4 VINYL WINDOW
- 5 STAINLESS STEEL SYSTEM
- 6 CONTIGERED BALCONY SYSTEM #1
- 7 IMPULSED BALCONY SYSTEM #2
- 8 METAL GUARDRAIL SYSTEM
- 9 STUCCO FORM TEKH
- 10 METAL TEKH
- 11 EXPOSED CONCRETE
- 12 BRICK
- 13 ORIENTAL TILE
- 14 METAL CLAMP
- 15 PAINTED METAL LEAVES
- 16 PAINTED COPING CAP
- 17 DOOR SYSTEM
- 18 PAINTED PANEL BOARD
- 19 ROOF STRUCTURE
- 20 BURNISHED BRICK
- 21 CAST STONE
- 22 PAINTED WOOD TRIM
- 23 WOOD DECK PAINT
- 24 PLANTER

**NOTES:**

1. TYPICAL PANEL HEIGHTS AT UNITS TO BE 6'-1" UP UNLESS NOTED OTHERWISE.
2. METAL SPACES TO HAVE A MIN. CEILING HEIGHT OF 12'-0".
3. CHAMFER BELOW 18'-0" TO BE REMOVABLE.



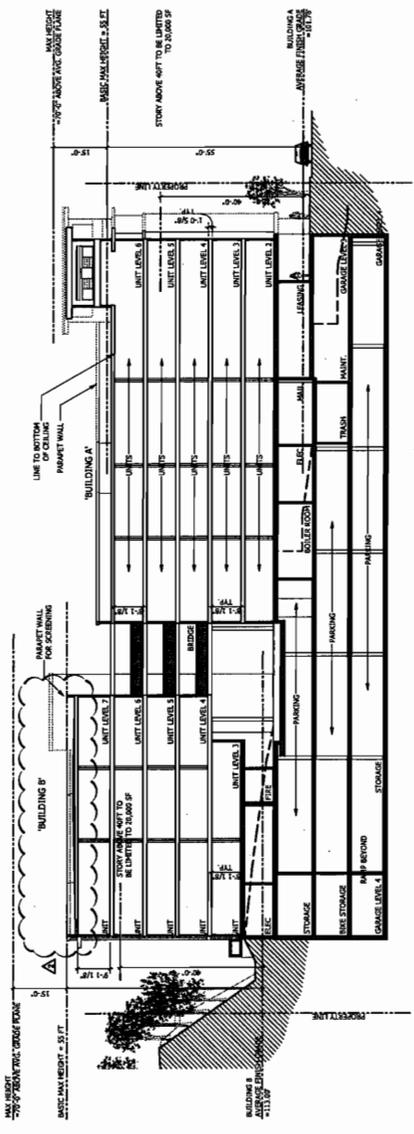
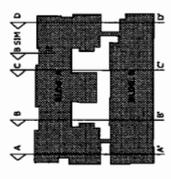




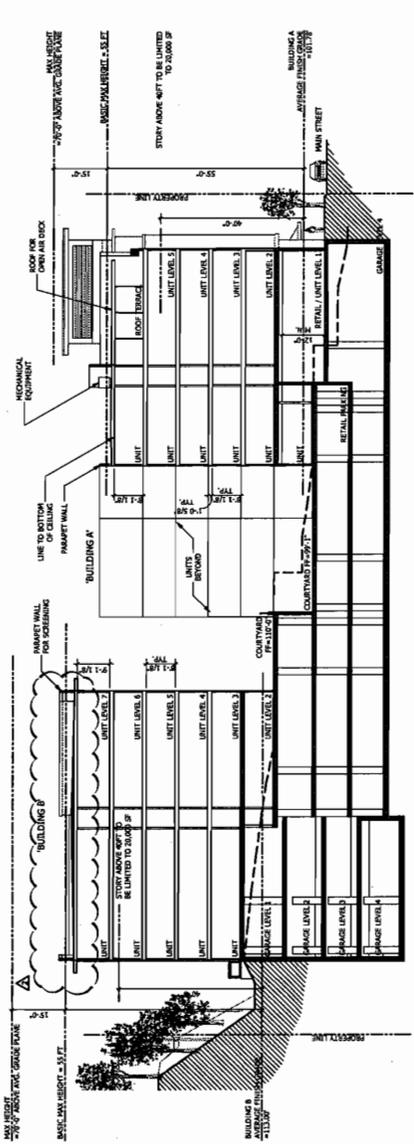
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PROJECT NO:	02
DRAWN BY:	MM
CHECKED BY:	MM
DATE:	07/15/12
PROJECT NAME:	ALAMO MANHATTAN
CONTRACTOR:	MM
CONSTRUCTION BY:	MM
REVISIONS:	
REVISION 1 - ISSUE	
REVISION 2 - ISSUE	

12025  
**A2.1B**  
 BUILDING SECTION



**02** BUILDING SECTION P-P  
 SCALE 1/8" = 1'-0"



**01** BUILDING SECTION C-C  
 SCALE 1/8" = 1'-0"



# MAIN STREET

ALAMO MANHATTAN  
BELLEVUE, WA

Hensley Lamkin Rachel, Inc.

SLATE 550  
7524  
FAX 972 728-9401

14881 GOLFVIEW DRIVE  
DALLAS, TEXAS  
972 728-9400



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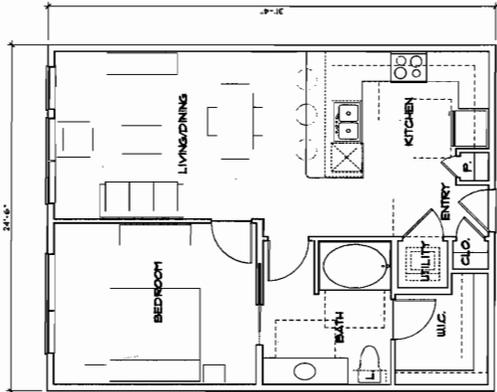
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PROJECT NO.	148
DATE	11/14/12
DESIGNED BY	OC
CHECKED BY	OC
SCALE	AS SHOWN
DATE	07/16/12
PROJECT SITE	
CONTRACTOR BY NAME	
CONTRACTOR ADDRESS	
CONTRACTOR PHONE	
CONTRACTOR FAX	
CONTRACTOR E-MAIL	

12025

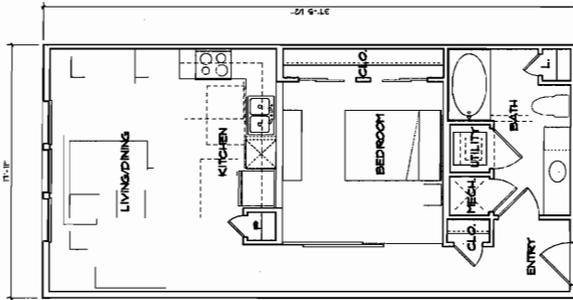
A3.2

FLOOR PLANS

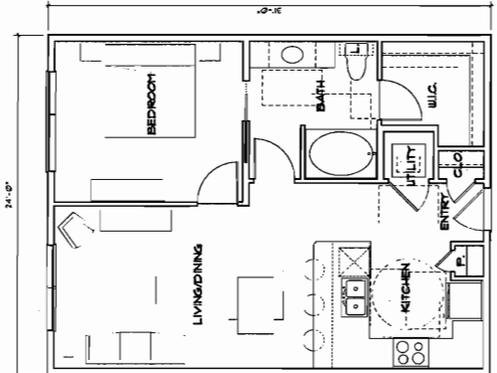
V-09-17-12



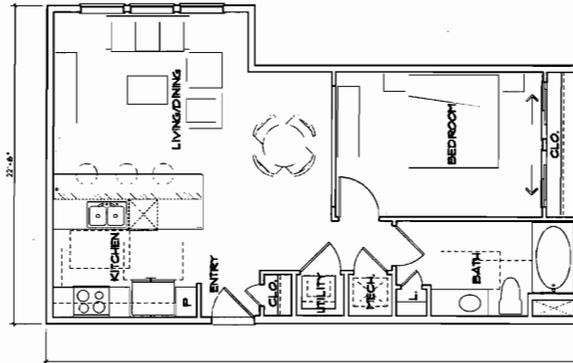
④ UNIT A4.1 FLOOR PLAN  
SCALE: 1/4" = 1'-0"  
NET 34' 0" GROSS 740 SF



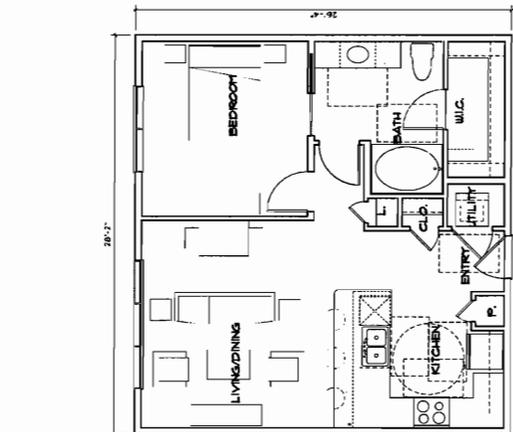
② UNIT A3.2 FLOOR PLAN  
SCALE: 1/4" = 1'-0"  
NET 51' 0" GROSS 918 SF



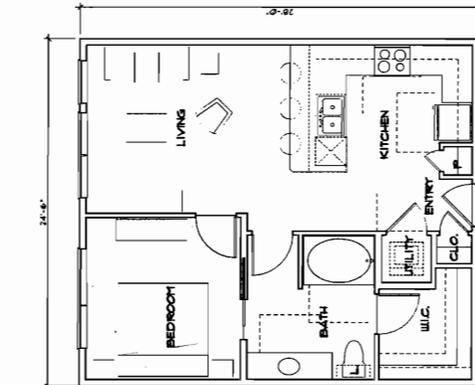
② UNIT A3.5 FLOOR PLAN  
SCALE: 1/4" = 1'-0"  
NET 34' 0" GROSS 744 SF



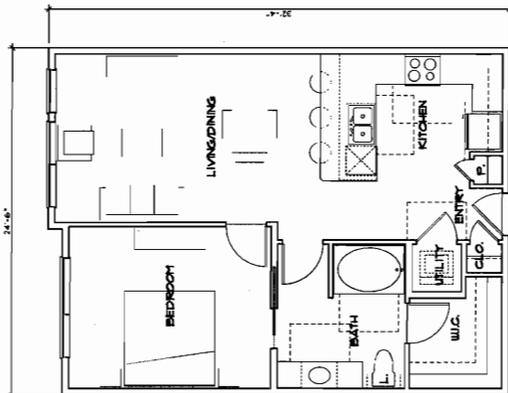
② UNIT A3.3 FLOOR PLAN  
SCALE: 1/4" = 1'-0"  
NET 51' 0" GROSS 918 SF



② UNIT A6 FLOOR PLAN  
SCALE: 1/4" = 1'-0"  
NET 54' 0" GROSS 102 SF



② UNIT A4 FLOOR PLAN  
SCALE: 1/4" = 1'-0"  
NET 448 SF GROSS 848 SF



② UNIT A4.1 FLOOR PLAN  
SCALE: 1/4" = 1'-0"  
NET 348 SF GROSS 740 SF



# MAIN STREET

ALAMO MANHATTAN  
BELLEVUE, WA

Hensley Lamkin Rachel, Inc.

STATE 590  
75254  
FAX 972 726-9401

1881 GOLFVIEW DRIVE  
DALLAS, TEXAS 75248

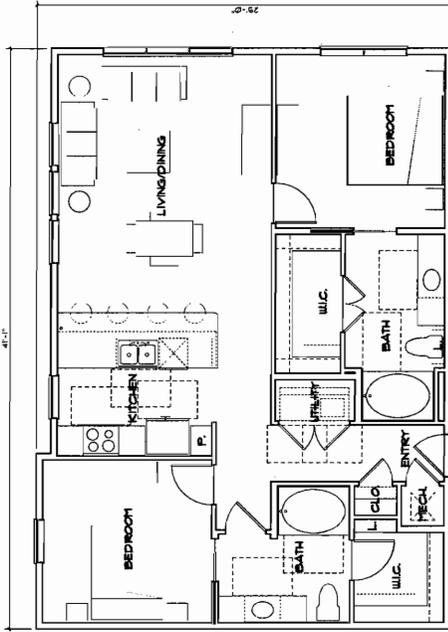


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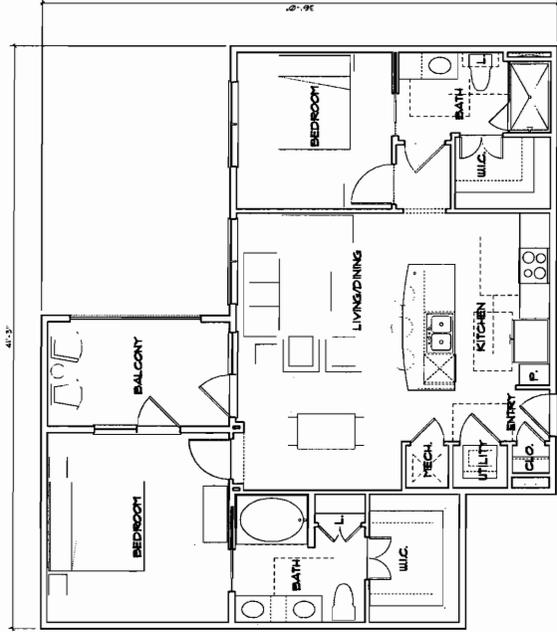
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DATE	07/10/12
SCALE	AS SHOWN
DESIGNED BY	MM
CHECKED BY	MM
DATE	07/10/12
PROJECT NO.	02
DATE	07/10/12
SCALE	AS SHOWN
DESIGNED BY	MM
CHECKED BY	MM
DATE	07/10/12

120025

A3.4  
UNITS  
FLOOR PLANS  
V-09-17-12



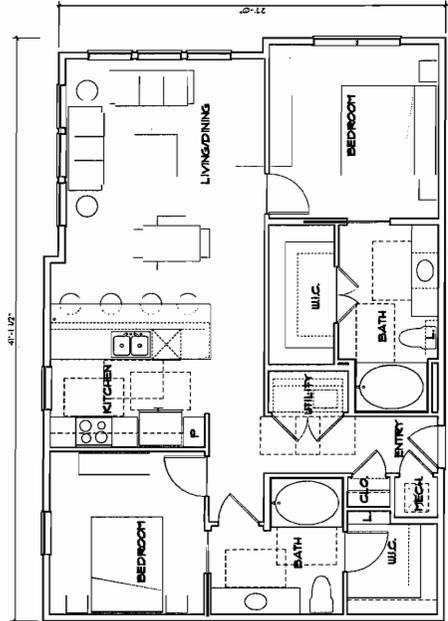
03 UNIT B21 FLOOR PLAN  
NET 108.57 SQ. FEET  
SCALE: 1/4" = 1'-0"



01 UNIT B1 FLOOR PLAN  
NET 107.85 SQ. FEET  
SCALE: 1/4" = 1'-0"



04 UNIT B22 FLOOR PLAN  
NET 108.57 SQ. FEET  
SCALE: 1/4" = 1'-0"



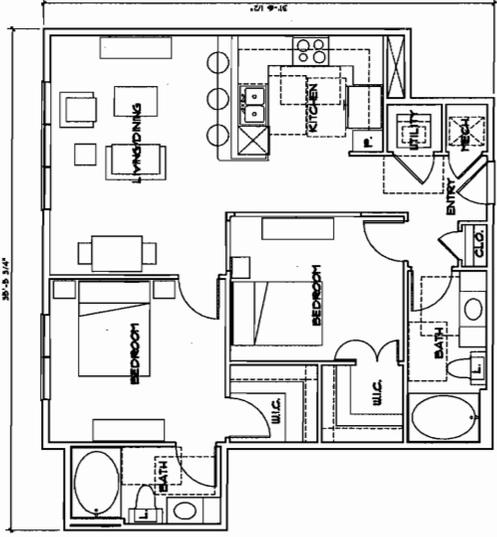
02 UNIT B2 FLOOR PLAN  
NET 102.55 SQ. FEET  
SCALE: 1/4" = 1'-0"



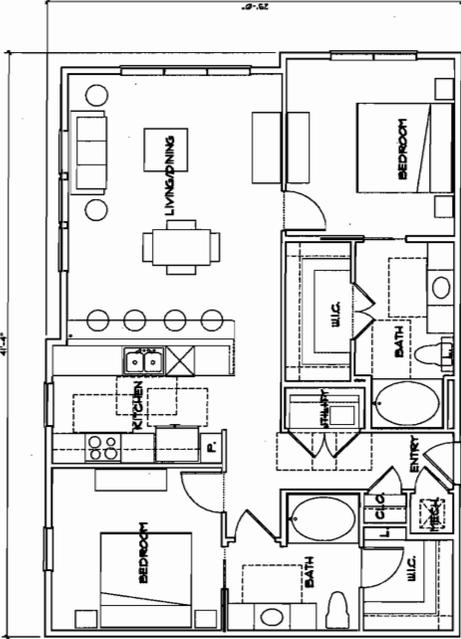
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PROJECT NAME	02
DESIGN BY	HLR
CHECKED BY	HLR
DATE	07/11/05
PRINT BY	HLR
CONTRACTOR BY NAME	
CONTRACTOR BY PHONE	
CONTRACTOR BY EMAIL	
INSTRUMENT TYPE	02

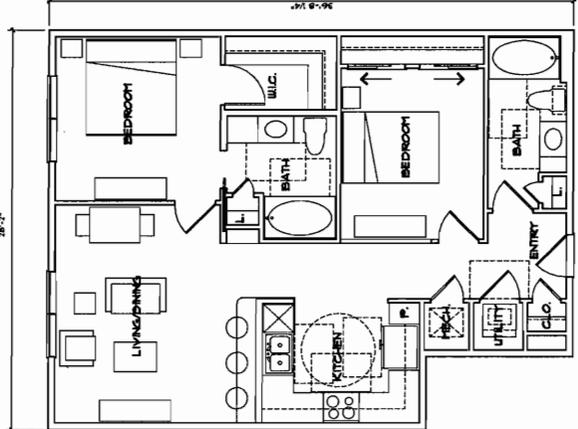
12025  
**A35**  
UNIT FLOOR PLAN  
V-09-17-12



**UNIT B4 FLOOR PLAN**  
NET 158 SQ. FEET, GROSS 199 SQ. FT.  
SCALE: 1/4" = 1'-0"



**UNIT B23 FLOOR PLAN**  
NET 134 SQ. FEET, GROSS 164 SQ. FT.  
SCALE: 1/4" = 1'-0"

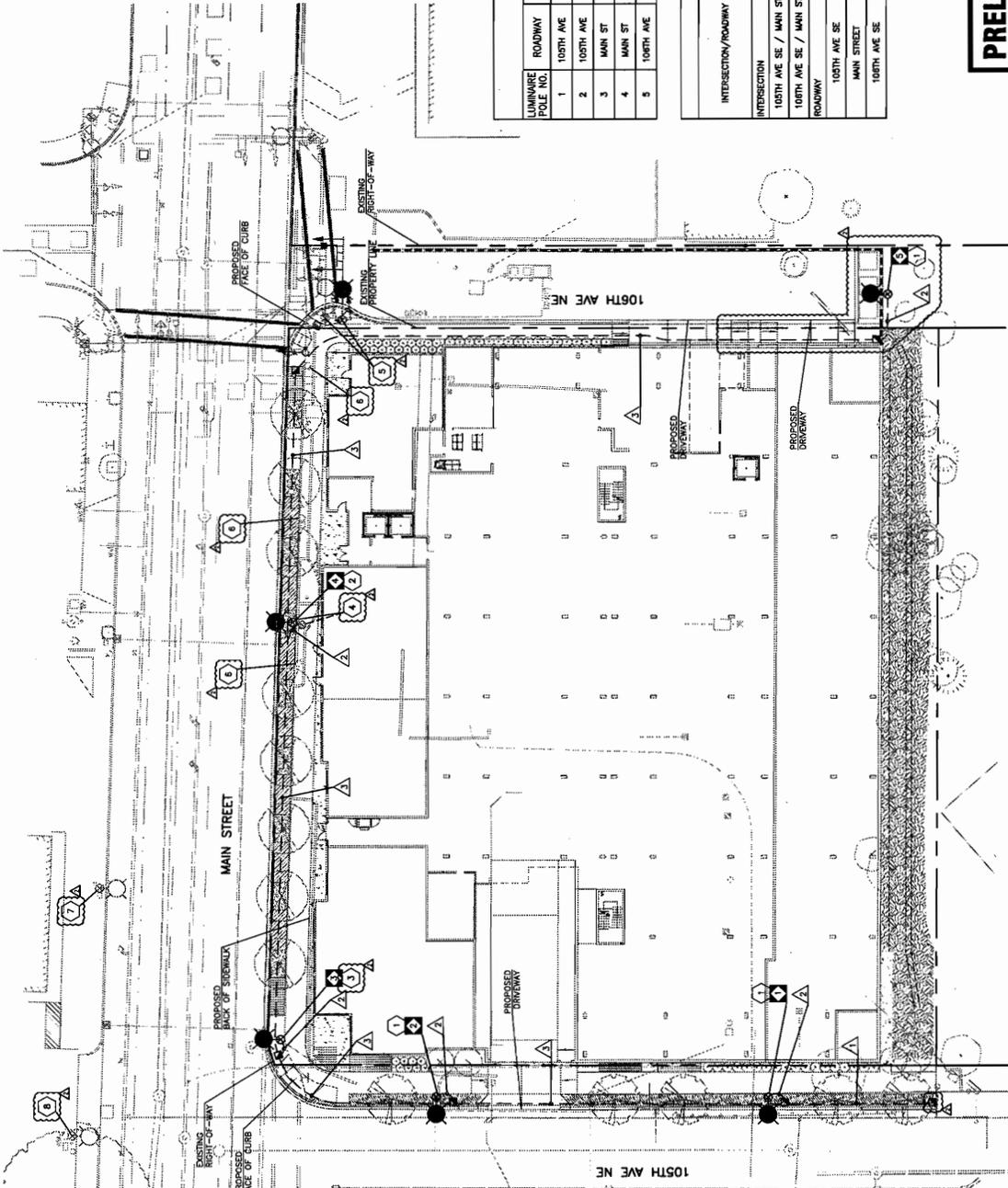


**UNIT B3 FLOOR PLAN**  
NET 161 SQ. FEET, GROSS 191 SQ. FT.  
SCALE: 1/4" = 1'-0"









**LEGEND**

EXISTING: ROADWAY LUMINAIRE AND POLE, JUNCTION BOX, CONDUIT, RIGHT-OF-WAY

PROPOSED: ROADWAY LUMINAIRE AND POLE, JUNCTION BOX, CONDUIT, RIGHT-OF-WAY

LUMINAIRE POLE NUMBER: 1, 2, 3

CONSTRUCTION NOTE: 1

**WIRING SCHEDULE**

NO	BACKWAY CONDUIT SIZE	ILLUM #	GROUND #
1	2" SCH40	SPARE (EMPTY)	1
2	2" SCH40	SPARE (EMPTY)	2
3	2" SCH40	SPARE (EMPTY)	1

\*ALL CONDUIT SHALL BE PVC

**LUMINAIRE POLE SCHEDULE**

LUMINAIRE POLE NO.	ROADWAY	STATION	POLE OFFSET	POLE HEIGHT	RM LENGTH	WATTAGE	SVCS CKT NO.	COMMENT
1	106TH AVE	TBD	TBD	4.5'	66	TBD	TBD	
2	106TH AVE	TBD	TBD	4.5'	66	TBD	TBD	
3	MAIN ST	TBD	TBD	12.2 METERS	8"	200	TBD	
4	MAIN ST	TBD	TBD	4.5'	400	TBD	TBD	
5	106TH AVE	TBD	TBD	9 METERS	4.5'	66	TBD	

**LIGHT LEVELS**

INTERSECTION/ROADWAY	AVG. FOOT CANDLE TARGET	UNIFORMITY (AVG/MIN)
106TH AVE SE / MAIN ST	16	9.9 / 3.1
106TH AVE SE / MAIN ST	16	20.5 / 3.1
106TH AVE SE	5	5.0 / 4.1
MAIN STREET	13	14.3 / 3.1
106TH AVE SE	5	TBD / 4.1

**PRELIMINARY**



- CONSTRUCTION NOTES**
- INSTALL KIM LIGHTING LUMINAIRE (1A-CSS-17-A-3-60-240V-BU) AND CONCRETE POLE STANDARD AT 9M HEIGHT PER CITY OF BELLEVUE STANDARD PLAN TSS-1.
  - INSTALL KIM LIGHTING LUMINAIRE (1A-CSS-25-A-3-60RFS-240V-BU) AND CONCRETE POLE STANDARD AT 9M MOUNTING HEIGHT PER CITY OF BELLEVUE STANDARD PLAN TSS-1.
  - INSTALL KIM LIGHTING LUMINAIRE (1A-CSS-25-A-3-260RFS-240V-BU) AND CONCRETE POLE STANDARD AT 9M MOUNTING HEIGHT PER CITY OF BELLEVUE STANDARD PLAN TSS-1.
  - RELOCATE EXISTING CONCRETE POLE TO REMAIN. REMOVE EXISTING LUMINAIRE AND WIRING.
  - REMOVE EXISTING CONCRETE POLE AND LUMINAIRE TO REMAIN.
  - REMOVE EXISTING CONCRETE POLE AND LUMINAIRE TO REMAIN.
  - REMOVE EXISTING CONCRETE POLE AND LUMINAIRE TO REMAIN.
  - REMOVE EXISTING CONCRETE POLE AND LUMINAIRE TO REMAIN.

- GENERAL NOTES**
- ALL WORK SHALL BE DONE IN ACCORDANCE WITH CITY OF BELLEVUE STANDARDS AND SPECIFICATIONS.
  - UTILITY LOCATIONS SHOWN ARE APPROXIMATE AND SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO CONSTRUCTION.
  - PROPOSED JUNCTION BOXES SHALL BE TYPE 2 PER MSDOT STANDARD PLAN J-40.10-02.

ALAMO MANHATTAN MAIN STREET  
CITY OF BELLEVUE, WASHINGTON  
ILLUMINATION PLAN

**transpoGROUP**  
11726 158TH AVE NE SUITE 600  
REDLAND, WASHINGTON 98073  
(206) 425-8210

**City of Bellevue**  
TRANSPORTATION DEPARTMENT

**Approved By**  
TRANSPORTATION DESIGN MANAGER  
PROJECT MANAGER

DATE	REVISION	BY	DATE
09/17/12	1	KJT	11/09/12
09/17/12	2	KJT	09/17/12
09/17/12			09/17/12



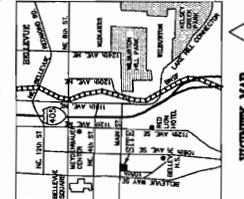




**BUSH, ROED & HITCHINGS, INC.**  
 CIVIL ENGINEERS & LAND SURVEYORS  
 3000 KENNESAW AVE. EAST  
 BELLEVUE, WA 98004  
 (206) 461-7272  
 FAYATSPRINTING.COM

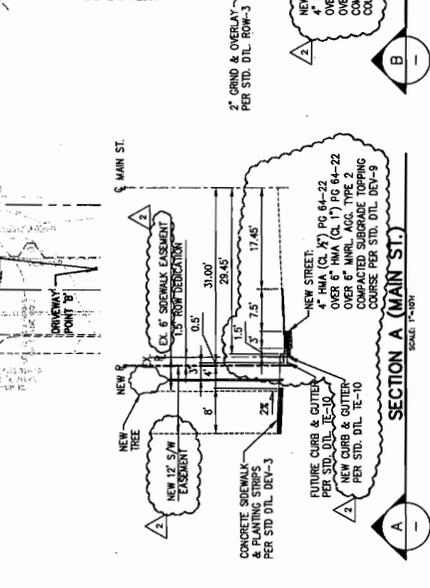
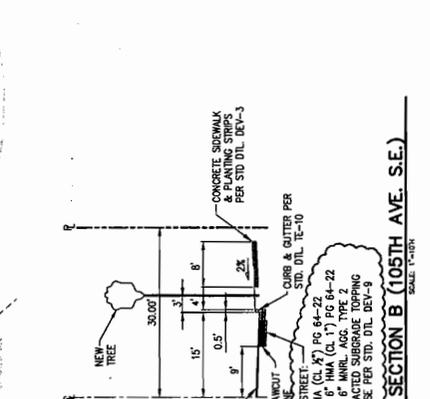
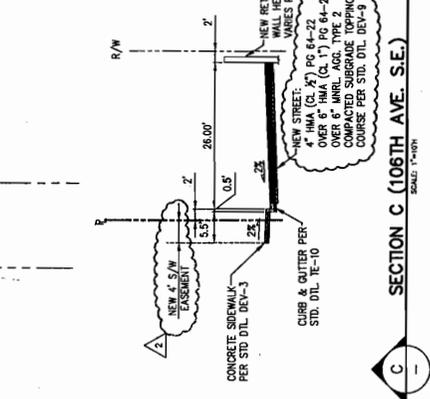
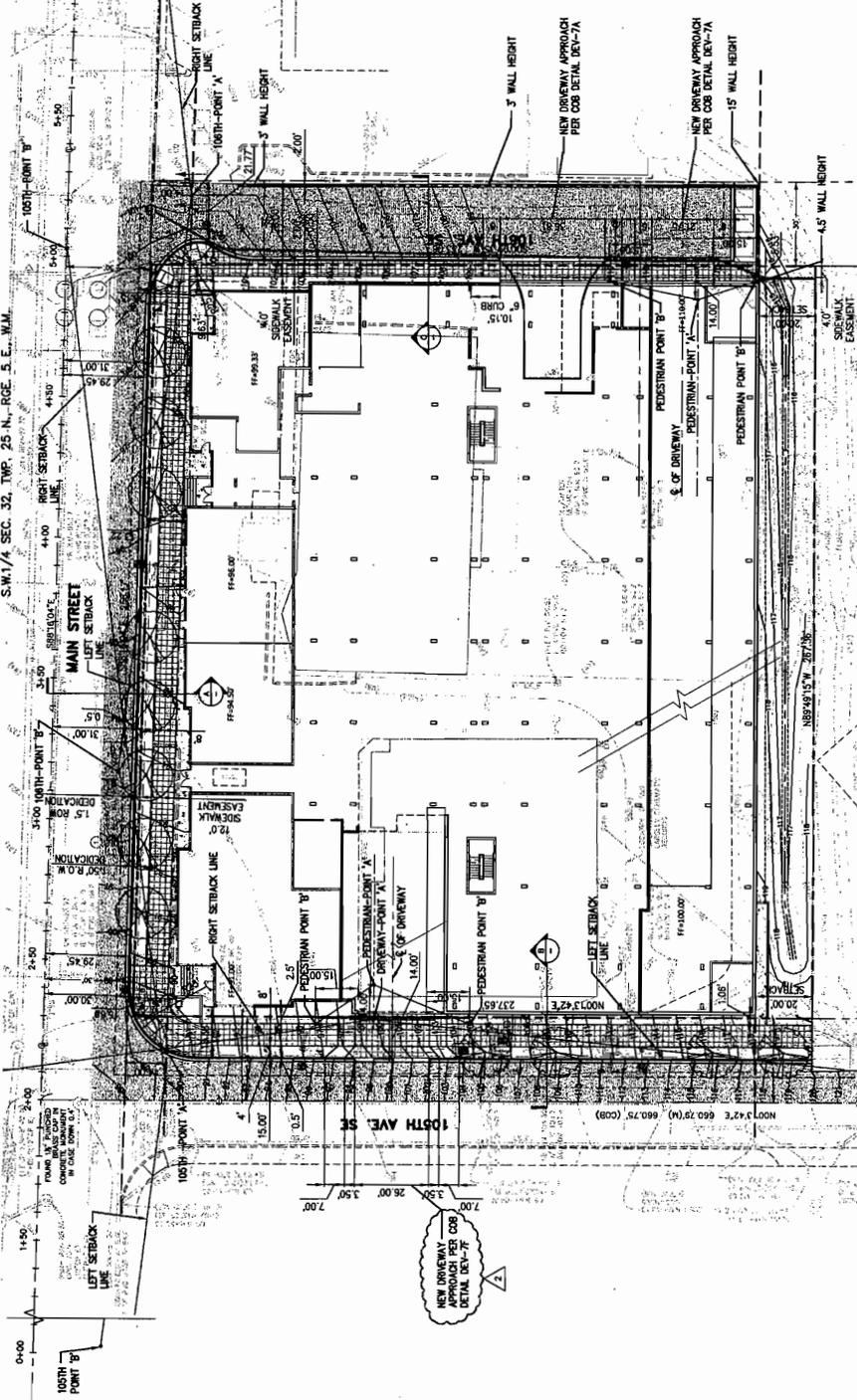
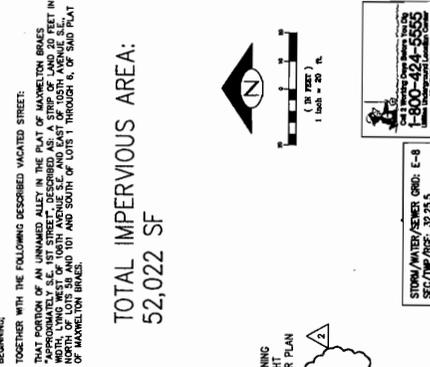
NO.	DATE	DESCRIPTION
1	11/27/23	CITY SUBMITTAL RESPONSE
2	1/17/24	CITY SUBMITTAL RESPONSE

WASHINGTON  
 BELLEVUE  
 ALAMO MANHATTAN, LLC  
 MAIN STREET SITE PLAN 'B'  
 AS SHOWN 10/17/24  
 2012029.04  
 SHEET NO. 1 OF 1  
 SCALE: 1"=10'

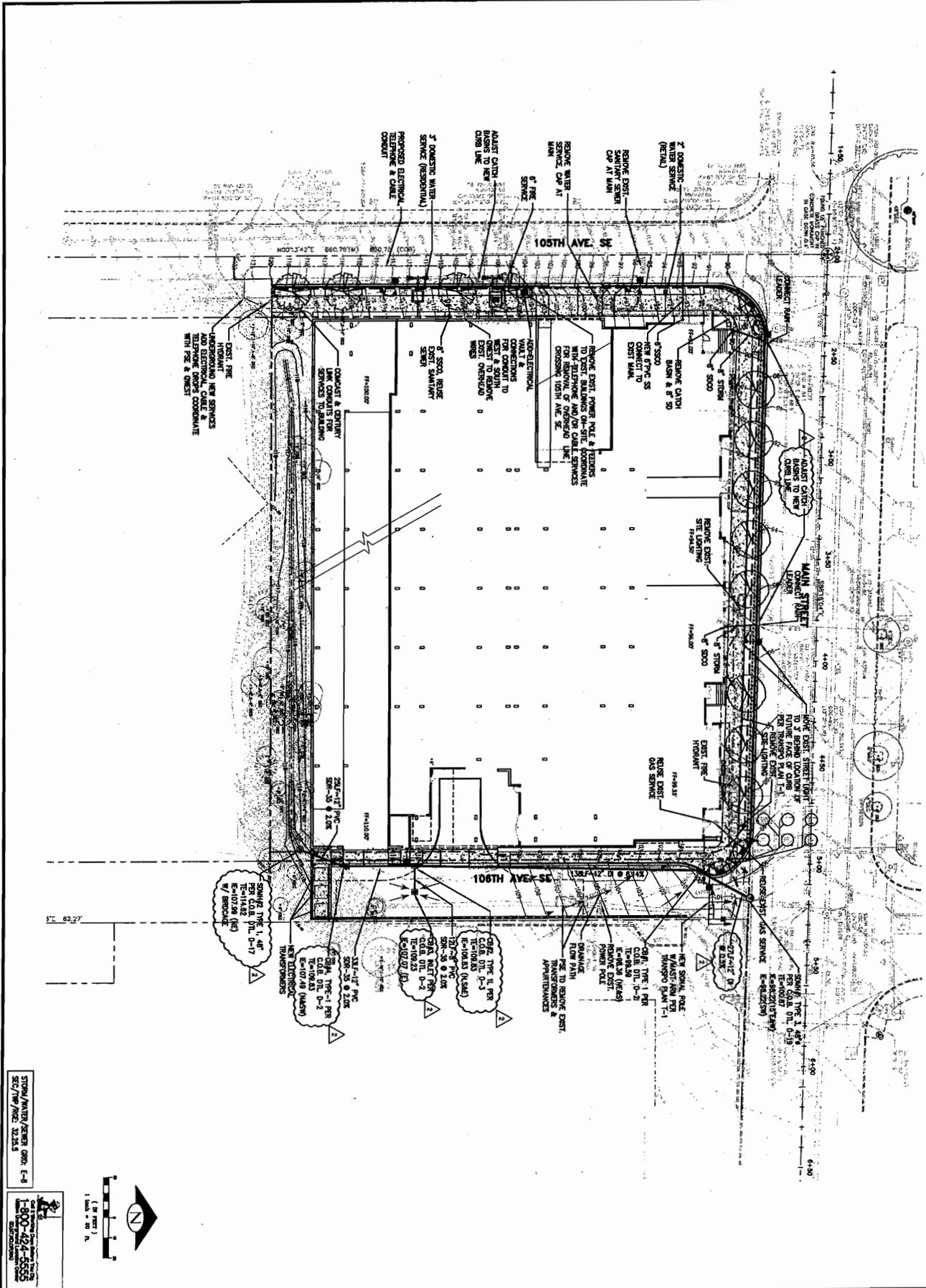


**SITE NOTES**  
 1. SITE ADDRESS: MAIN STREET, BELLEVUE, WA 98004  
 2. TAX ACCOUNT NO.: 522335-0015  
 3. ZONING: DNR-460  
 4. CITY OF BELLEVUE, DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT, PO BOX 90012, BELLEVUE, WA 98009-9012 (425) 462-4848  
 5. CURRENT SETBACK REQUIREMENTS SUBJECT TO SITE PLAN REVIEW. CURRENT SETBACKS MAY DIFFER FROM THOSE IN EFFECT DURING DESIGN/CONSTRUCTION OF THIS PROJECT.  
 6. THIS PROJECT IS THE PROPERTY OF ALAMO MANHATTAN, LLC. THE PROJECT IS BEING DEVELOPED BY ALAMO MANHATTAN, LLC.  
 7. ALL DIMENSIONS ARE IN FEET AND INCHES. DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.  
 8. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.  
 9. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.  
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 19. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.  
 20. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.

**VERTICAL DATUM:**  
 ORIGINAL BENCHMARK: BELLEVUE BENCHMARK #10, BRASS CAP IN CONCRETE AT SOUTHEAST CORNER OF BELLEVUE WAY AND MAIN STREET ON NORTH SIDE OF LOT 1 THROUGH 6, LOTS 98 AND 101, MANWELTON BRASS, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 21 OF PLATS, PAGE(S) 80, IN KING COUNTY, EXCEPT THAT PORTION OF LOT 6 CONVEYED TO THE CITY OF BELLEVUE BY DEED RECORDED UNDER RECORDING NUMBER 00000, DESCRIBED AS FOLLOWS:  
 BEGINS AT THE POINT OF BEGINNING OF SAID LOT 6, 11 FEET THENCE EAST ALONG THE NORTH LINE OF SAID LOT 6, 11 FEET THENCE ALONG THE ARC OF A CURVE TO THE LEFT WITH A RADIUS OF 24'-1/2 FEET TO POINT 'A' ON THE WEST LINE OF LOT 6, 2.60 FEET SOUTH OF THE POINT OF BEGINNING, TO THE POINT OF BEGINNING OF SAID LOT 6 TO THE POINT OF BEGINNING ALONG THE WEST LINE OF SAID LOT 6 TO THE POINT OF BEGINNING.  
 TOGETHER WITH THE FOLLOWING DESCRIBED VICINITY STREET:  
 APPROXIMATELY 4' WIDE CURB AND GUTTER PER PLAN NORTH, LYING WEST OF NORTH AVENUE S.E. AND EAST OF 105TH AVENUE S.E. OF MANWELTON BRASS.  
 APPROXIMATELY 4' WIDE CURB AND GUTTER PER PLAN SOUTH, LYING WEST OF NORTH AVENUE S.E. AND EAST OF 105TH AVENUE S.E. OF MANWELTON BRASS.  
 ELEVATION = 77.845

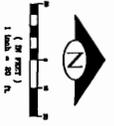






STREET/WATER/SEWER ONLY E-D  
 ECU/MS/AEC 2023.3

100-424-5555  
 201.2029.04



Project No.	100-424-5555
Client	ALAMO MANHATTAN, LLC
City	WASHINGTON
County	BELLEVUE
Map No.	201.2029.04
Scale	AS SHOWN
Sheet	CI.3.3

NO.	REVISION	DATE
3	CITY SUBMITTAL REVISIONS	11/2/22
1	CITY SUBMITTAL REVISIONS	5/19/22

**BUSH, ROED & HITCHINGS, INC.**  
 CIVIL ENGINEERS & LAND SURVEYORS  
 2208 MANOR AVE. EAST  
 SEATTLE, WASHINGTON 98103  
 TEL: 206.465.6444  
 FAX: 206.465.6444  
 WWW.BRHINC.COM



## CERTIFICATE OF CONCURRENCY

### ALAMO MANHATTAN MAIN STREET

This certificate documents the Transportation Department Director's decision that the development project at 10505 Main Street (Design Review File No. 12-117760 LD) complies with the requirements of the Traffic Standards Code (BCC 14.10). This decision reserves 76 net new p.m. peak hour trips to that project, subject to Process II appeal of the concurrency determination. This reservation will expire one year from the effective date of this action unless a complete building permit application is filed prior to that date (BCC 14.10.010D). At the time of a complete building permit application, the concurrency reservation will remain in effect for the life of that application (BCC 23.05.090H). Upon issuance of the building permit, concurrency is reserved for one year; the applicant may request up to two one-year extensions (BCC 23.05.100E).

  
Director, Transportation Department

12/27/2012  
Date

Certificate No. 74

Sally Nichols  
Dec. 6, 2012

## ENVIRONMENTAL (SEPA) CHECKLIST

### BACKGROUND INFORMATION

Property Owner: Stern-Esfeld, LLC ✓  
Proponent: Alamo Manhattan Main Street, LLC ✓  
Contact Person: Robert Lamkin ✓  
Address: 14881 Quorum Drive, Suite 550 Dallas TX, 75243 ✓  
Phone: 972.726.9400 ✓  
Proposal Title: Alamo Manhattan – Main Street ✓  
Proposal Location: 10505 Main Street - Bellevue, WA (see attached vicinity map) ✓

Give an accurate, brief description of the proposal's scope and nature:

1. General description: The proposed project is a multi-family mixed-use development with 260 units, 7,000 s.f. of retail space and a multiple level, sub-grade parking structure.
2. Acreage of site: 1.452 acres
3. Number of dwelling units/buildings to be demolished: no units / two buildings
4. Number of dwelling units/buildings to be constructed: 260 units / two buildings ✓
5. Square footage of buildings to be demolished: Approx. 14,500 s.f. ✓
6. Square footage of buildings to be constructed: 368,015 s.f. ✓
7. Quantity of earth movement (in cubic yards): 39,330 cubic yards ✓
8. Proposed land use: Multi-family Mixed-use ✓
9. Design features, including building height, number of stories and proposed exterior materials: ✓

The proposed project has two 6-story buildings over a common underground parking structure that links vehicular circulation below the two buildings. The common base is designed to work with existing multi-level parking garage that steps with existing grade conditions. Since grading varies along the buildings, the building height varies between 58 ft and 68 ft from the lowest to the highest point. Commercial space is located along Main Street and hides the parking structure behind. <sup>is 58' ave</sup>

The layout of each building has living units on both sides of a common corridor. This layout allows all of the units to have a direct connection to the exterior for natural light and ventilation.

existing use  
retail only

Received  
JUL 13 2012  
Permit Processing

Building ht.  
per LUC 20. 25.8.01  
ht. and LUC  
Max. ht is 58'  
LUC 20.50.  
def.

20

Building exterior materials will include brick veneer, stucco, cementitious panels and siding, exposed cast in-place concrete, metal railing, vinyl windows, aluminum storefront windows in selected locations and metal louver screens at the towers ✓

Estimated date of completion of the proposal or timing of phasing: ✓

Design review: July 2012 - *completed 12/10/12*  
Building permit submittal: December 2012  
Start construction: June 2013

Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. ✓

Not at this time.

List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

A geotechnical report including soil borings is included with the design review submittal. The report is available for review from the City of Bellevue.

*✓ Dated 7/12/12  
Prepared by The  
Riley Group Inc  
in project  
file*

Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. List dates applied for and file numbers, if known.

None known.

List any government approvals or permits that will be needed for your proposal, if known. If permits have been applied for, list application date and file numbers, if known. ✓

None

Please provide one or more of the following exhibits, if applicable to your proposal. ✓  
(Please check appropriate box(es) for exhibits submitted with your proposal):

- Land Use Reclassification (rezone) Map of existing and proposed zoning
- Preliminary Plat or Planned Unit Development  
Preliminary plat map
- Clearing & Grading Permit  
Plan of existing and proposed grading  
Development plans
- Building Permit (or Design Review)  
Site plan  
Clearing & grading plan
- Shoreline Management Permit  
Site plan

A. ENVIRONMENTAL ELEMENTS

1. Earth

- a. General description of the site:  Flat  Rolling  Hilly  Steep slopes  Mountains  Other

The Majority of the site is flat with steep slopes on the south and west portions of the site.

- b. What is the steepest slope on the site (approximate percent slope)? ✓

Approximate 68%.

- c. What general types of soil are found on the site (for example, clay, sand, gravel, peat, and muck)?  
If you know the classification of agricultural soils, specify them and note any prime farmland. ✓

Refer to Geotechnical report for information about the soils found at the site.

- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. ✓

No

- e. Describe the purpose, type, and approximate quantities of any filling or grading proposed. Indicate source of fill.

Approximately 39,330 cubic yards of excavation will be generated for the purpose of constructing underground parking for the proposed development.

- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

Soil erosion could occur from site during clearing and grading activity. ✓

- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

Approximately 92%

- h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any: ✓

Silt fencing, diversion swales and sediment traps will be use to control erosion during clearing and grading activities.

2. AIR

- a. What types of emissions to the air would result from the proposal (i.e. dust, automobile odors, and industrial wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known.

During construction, dust and construction equipment emissions could occur. Increased auto emissions in the vicinity following construction and commencement of operations also could occur.

✓  
Flat area developed.  
steep slopes in DNTN  
not classified as  
critical areas

✓  
Soil report.  
dated 7/12/12 by  
The Riley Co.

✓  
some fill for  
improvements  
106th Ave ST

✓  
For C66 code  
Dec 23.76 - TRES  
reg'd as well  
as CWSPPP

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. ✓

None known.

- c. Proposed measures to reduce or control emissions or other impacts to the air, if any:

The site will be watered down as necessary to control dust. The project will operate within traffic levels of service with the improvements proposed.

Construction dust  
suppression measures  
per BCC 23.76 -  
C&G Code

### 3. WATER

- a. Surface

- (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into. ✓

No

- (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans. ✓

No

- (3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material. ✓

None MM

- (4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known. ✓

No

- (5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan. ✓

No

- (6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge. ✓

No

b. Ground

(1) Will ground water be withdrawn, or will water be discharged to ground water? Give general description.

No

(2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals...; agricultural; etc.) Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

No waste material will be discharged into the ground.

c. Water Runoff (Including storm water)

(1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

The source of runoff from the site is the site storm runoff itself. The onsite storm drainage will be collected via roof drains and bio-retention swale south of the proposed building and will be discharged into the existing storm system in Main Street.

(2) Could waste materials enter ground or surface waters? If so, generally describe.

No

d. Proposed measures to reduce or control surface, ground, and runoff water impacts, if any:

A portion of the rooftop will discharge through a bio-retention swale.

4. Plants

a. Check or circle types of vegetation found on the site:

- deciduous tree: alder, maple, aspen, other
- evergreen tree: fir, cedar, pine, other
- shrubs
- grass
- pasture
- crop or grain
- wet soil plants: cattail, buttercup, bulrush, skunk cabbage, other

} on steep slope

During construction, de-watering may be req'd.  
Per geotech report, groundwater @ depth of 46.5'. Some light seepage @ 26-45'

water only

TESC measure  
Per BCC 23.74

BCC 24.06.  
storm drainage detention  
Per COB & DOE requirements  
CA 9 permit under

water plants: water lily, eelgrass, milfoil, other

other types of vegetation

b. What kind and amount of vegetation will be removed or altered?

All vegetation outside of the landscape buffer will be removed. Vegetation inside the landscape buffer will be left as much as possible.

✓ As much veg. as possible to be retained in 20' landscape buffer, ~~total~~ Buffer req'd in W/C 20.25B.040. Refer to arborist report, dated 11/6/12 prepared by Gilles Consulting

c. List threatened or endangered species known to be on or near the site. ✓

None known.

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

Landscaping will be provided in accordance with the city of Bellevue regulations and codes.

✓ Additional tree planting on slope per arborist report. W/C 20.25P

5. ANIMALS

a. Check or circle any birds and animals which have been observed on or near the site or are known to be on or near the site:

Birds: hawk, heron, eagle, songbirds, other: ✓

Mammals: deer, bear, elk, beaver, other:

Fish: bass, salmon, trout, herring, shellfish, other:

Additional plants w/ streetscape courtyard

b. List any threatened or endangered species known to be on or near the site. ✓

None known.

c. Is the site part of a migration route? If so, explain. ✓

None known.

✓ Pacific coast flyway - entire Puget sound

d. Proposed measures to preserve or enhance wildlife, if any:

None.



## 6. Energy and Natural Resources

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy need? Describe whether it will be used for heating, manufacturing, etc. ✓

Electricity to the project will be provided from the City of Bellevue power grid. The project will consist of electrical heating, lighting, air-conditioning, and other residential uses throughout the building. Natural gas will provide centralized hot water.

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. ✓

Adjacent properties currently do not utilize solar energy strategies. The proposed project will partially shade the adjacent buildings towards the east although all adjacent properties as well as the proposed project have excellent southern exposure. ✓

- c. What kinds of energy conservation features are included in the plans of the proposal? List other proposed measures to reduce or control energy impacts, if any: ✓

The residential units in the building will include high-efficiency appliances and light fixtures. Roof, wall, floor, and slab edge insulation, and quality windows and exterior doors will be utilized as energy conservation features.

*✓  
Bldg lies to n. of  
residential  
bldg. to  
south.*

## 7. Environmental Health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe. ✓

No.

(1) Describe special emergency services that might be required. ✓

None.

(2) Proposed measures to reduce or control environmental health hazards, if any. ✓

None

- b. Noise

(1) What types of noise exist in the area which may affect your project (for example, traffic, equipment, operation, other)? ✓

Traffic noise may impact the project along Main Street.

- (2) What types and levels of noise would be created by or associated with the project on a short-term or long-term basis (for example, traffic, construction, operation, other)? Indicate what hours noise would come from the site. ✓

Short-term noise would include construction conducted in accordance with Washington State Department of Ecology and City of Bellevue noise regulations. Long-term noise would include the anticipated noise levels from a multi-family mixed-use development.

- (3) Proposed measures to reduce or control noise impacts, if any: ✓

The project will comply with Washington State Department of Ecology and City of Bellevue noise regulations. No specific noise reductions measures are proposed at this time but will be provided if required.

Traffic & construction noise per city's Noise Ordinance OCC 9.18 ✓

## 8. Land and Shoreline Use

- a. What is the current use of the site and adjacent properties? ✓

Commercial use

- b. Has the site been used for agriculture? If so, describe. ✓

No

- c. Describe any structures on the site. ✓

Two one story commercial buildings.

- d. Will any structures be demolished? If so, what? ✓

The entire site is to be cleared for the new construction.

no residential properties ✓

- e. What is the current zoning classification of the site? ✓

DNTN-MU Perimeter sub-district 'A'

- f. What is the current comprehensive plan designation of the site? ✓

~~Multi-family mixed-use.~~

Downtown - NRP area - West Bellevue ✓

- g. If applicable, what is the current shoreline master program designation of the site? ✓

Not applicable.

- h. Has any part of the site been classified as an "environmentally sensitive" area? If so, specify. ✓

No

l. Approximately how many people would reside or work in the completed project?

Approximately 400 people.

✓  
260 units -  
most studio/1 bedroom

i. Approximately how many people would the completed project displace?

None, since there are no residences on site.

j. Proposed measures to avoid or reduce displacement impacts, if any:

None at this time.

k. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

The proposed project matches existing and proposed land use patterns. The proposed use is permitted outright in the DNTN-MU zone and coincides with the specific plan intended for the Perimeter district.

✓  
Land Use Decision  
Design Review &  
Ancillary Permits

9. Housing

a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing.

260 market rate apartment units will be provided. There are no low income units provided.

b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.

None. No units are available on current site.

c. Proposed measures to reduce or control housing impacts, if any:

None, because there are no existing housing on site.

10. Aesthetics

a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

55 ft is the height to the tallest point in the building from the average grade plane. Principal exterior materials to include, brick veneer, stucco, and siding.

finish  
per def. of bldg ht. in  
B.L.C. 20.50.01  
& L.U.C. 20.25F  
040  
for Transition

b. What views in the immediate vicinity would be altered or obstructed?

View to Main Street will be block from the north side of the adjacent south property at some parts.

but buildings sit  
approx 20' below  
adj. prop to south

- c. Proposed measures to reduce or control aesthetic impacts, if any:

Modulated massing will mitigate the bulk of the building on pedestrians and adjacent buildings. High quality cladding materials and landscaping will further provide an aesthetically pleasing project.

*Big mail chosen for low reflecting High reflect to DR staff report - DR extends LIC 20.26A*

**11. Light and Glare**

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

The residential units, ground level entry areas, and upper level common deck area will produce light. There will also be some exterior lighting to enhance the exterior landscaping and pathways. It is not anticipated that the light or glare created by the project will be inconsistent with the neighboring apartment buildings or other projects of this scale.

*All fixtures need to have cut-off*

- b. Could light or glare from the finished project be a safety hazard or interfere with views? ✓

The level of light or glare created by the project is not anticipated to create a safety hazard or to interfere with views.

- c. What existing off-site sources of light or glare may affect your proposal? ✓

City of Bellevue street lights and traffic along Main Street will produce some levels of light or glare that may affect the project.

*materials low reflectivity cut-off shield*

- d. Proposed measures to reduce or control light or glare impacts, if any: ✓

Exterior lighting associated with the project will be shielded to limit glare affecting adjacent properties.

**12. Recreation**

- a. What designated and informal recreational opportunities are in the immediate vicinity?

Downtown Park is half a mile from the site and is the closest recreational space.

*Bellevue High school 2 blocks south*

- b. Would the proposed project displace any existing recreational uses? If so, describe.

No

- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: ✓

The project will include landscaped exterior courtyards, some at roof level, which will provide residents and their guests with views and space for leisurely outdoor activities as well as views towards downtown Bellevue.

**13. Historic and Cultural Preservation**

- a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? If so, generally describe.

None known.

- b. Generally describe any landmarks or evidence of historic, archeological, scientific, or cultural importance known to be on or next to the site.

None known.

- c. Proposed measures to reduce or control impacts, if any:

Not applicable.

14. Transportation

- a. Identify public streets and highways serving the site, and describe proposed access to the existing street system. Show on site plans, if any.

The project is surrounded by three public streets, Main Street, 105<sup>th</sup>, and 106<sup>th</sup> avenues. Access to the project will be from 105<sup>th</sup> and 106<sup>th</sup> Avenue both into a parking structure. No access will be provided from Main Street.

main st - arterial existing signal on Main st. / 106th intersection

Retail parking access of 105th only

- b. Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop?

Yes, a bus stop is available on the other side of Main Street right across from the middle of the site.

- c. How many parking spaces would the completed project have? How many would the project eliminate?

319 spaces will be provided, 300 spaces for residents and 19 for retail. All existing private parking spaces on site will be eliminated. No public parking spaces are being eliminated

40 more residential than req'd min.

- d. Will the proposal require any new roads or streets, or improvements to existing roads or streets, not including driveways? If so, generally describe (indicate whether public or private).

No new roads are required but improvements will be performed on all three surrounding streets.

and signal e both upgraded

- e. Will the project use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

No.

70 peak trips - see page 6 of traffic study

- f. How many vehicular trips per day would be generated by the completed project? If known, indicate when peak volumes would occur.

Vehicular trips are unknown at this time. A traffic study might be required during the design review process.

Refer to traffic study prepared by BTC, dated Oct. 20 in project file

- g. Proposed measures to reduce or control transportation impacts, if any:

The project is within walk-able distance to many shops and services. The site is also located on a designated future bicycle route along Main Street. Several bus lines serve the vicinity of the site also

bus stop in front of bldg A

Available transit use combined with available means of alternate transportation should help to reduce vehicular transportation impacts.

*Concurrency determined & Design Review Cert. Issued w/ approval*  
*Access to parking garage from 2nd non-arterial streets one w/ signal*

15. Public Services

- a. Would the project result in an increased need for the public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe.

There will be increased demand for police and fire protection, typical of a multi-family mixed-use development. Ant impact on local schools should be very minimal.

*due to size & units*

- b. Proposed measures to reduce or control direct impacts on public services, if any.

Adequate lighting will be provided for safety, the building will be fully sprinkled, and a building security system will be used.

16. Utilities

- a. Circle utilities currently available at the site: electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other.

- b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

Electricity and natural gas (PSE), water and sanitary sewer (City of Bellevue), telephone (Century Link), Cable (Comcast) and refuse service (City of Bellevue). New water, storm drainage, sanitary sewer, electric, natural gas, telephone, and cable connections for the utilities in the right-of-way will be required.

Signature

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature Robert W. Lamb

Date Submitted 07.13.12

# VICINITY MAP

