



DEVELOPMENT SERVICES DEPARTMENT
 ENVIRONMENTAL COORDINATOR
 450 110th AVENUE NE
 BELLEVUE, WA 98009-9012

DETERMINATION OF NON-SIGNIFICANCE

PROPONENT: First Bellevue Properties, LLC

LOCATION OF PROPOSAL: 25 102nd Avenue NE

DESCRIPTION OF PROPOSAL: To demolish an existing two story office/retail building to construct a 9 story, 108 room hotel with associated restaurant, banquet, support facilities and underground parking. Total parking provided will be 115 stalls on-site with 50 off-site overflow stalls.

FILE NUMBER: 07-143899 LD

The Environmental Coordinator of the City of Bellevue has determined that this proposal does not have a probable significant adverse impact upon the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(C). This decision was made after the Bellevue Environmental Coordinator reviewed the completed environmental checklist and information filed with the Land Use Division of the Development Services Department. This information is available to the public on request.

- There is no comment period for this DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's office by 5:00 p.m. on _____.
- This DNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on the DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's Office by 5 p.m. on October 14, 2010.
- This DNS is issued under WAC 197-11-340(2) and is subject to a 14-day comment period from the date below. Comments must be submitted by 5 p.m. on _____. This DNS is also subject to appeal. A written appeal must be filed in the City Clerk's Office by 5 p.m. on _____.

This DNS may be withdrawn at any time if the proposal is modified so that it is likely to have significant adverse environmental impacts; if there is significant new information indicating, or on, a proposals probable significant adverse environmental impacts (unless a non-exempt license has been issued if the proposal is a private project); or if the DNS was procured by misrepresentation or lack of material disclosure.

Carle Vysellend
 Environmental Coordinator

9/30/10
 Date

OTHERS TO RECEIVE THIS DOCUMENT:

- State Department of Fish and Wildlife
- State Department of Ecology,
- Army Corps of Engineers
- Attorney General
- Muckleshoot Indian Tribe



**City of Bellevue
Development Services Department
Land Use Division Staff Report**

Proposal Name: Bellevue Park Hotel

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File Number: **07-143899 LD**

Applicant: First Bellevue Properties LLC

Decisions Included: Design Review with SEPA, Process II

Planner: Antoinette Pratt, Senior Planner

State Environmental Policy Act Threshold Determination: **Determination of Non-significance**
Carol V. Helland
Carol V. Helland
Environmental Coordinator

Director's Decision: **Approval with Conditions**

Michael A. Brennan, Director
Development Services Department

By: Carol V. Helland
Carol V. Helland, Land Use Director

Application Date: January 11, 2010
Notice of Application Date: February 4, 2010
Minimum Comment Period: February 25, 2010
Bulletin Publication Date: September 30, 2010
Appeal Deadline: October 14, 2010

For information on how to appeal a proposal, visit Development Services at City Hall or call (425) 452-4570. Appeal of the Decision must be made by 5 p.m. on the date noted for appeal of the decision.

CONTENTS

I.	Request/Review Process.....	2
II.	Site Zoning, Context & Character.....	3
III.	Consistency with Land Use Code/Zoning Requirements.....	6
IV.	Public Notice & Comment.....	19
V.	Technical Review.....	20
VI.	State Environmental Policy Act (SEPA).....	24
VII.	Changes to Proposal Due to City Review.....	27
VIII.	Decision Criteria.....	27
IX.	Decision.....	32
X.	Conditions of Approval.....	32

I. REQUEST/PROPOSAL DESCRIPTION

A. Request

First Bellevue Properties LLC requests Design Review approval to demolish an existing two story retail/office building and construct a nine story, 108 room building with hotel lobby, restaurant and bar at grade over 3 levels of underground parking for a total of 115 parking stalls. The applicant has proposed 75,269 GSF of heated building area with 40,637 GSF of below grade parking. Included is 1,232 SF of hotel lobby, 2,054 SF restaurant/bar, 5,662 SF banquet and 1,598 SF administration space.

The project includes street frontage improvements along NE 1st Street and 102nd Avenue NE.

**View Looking West—North
and East Elevations**



See attached Project Plans & Drawings for more information.

B. Review Process

Design Review is required by Land Use Code (LUC) 20.30F. The Design Review, and SEPA Determinations are both Process II decisions. Process II is an administrative process. The Environmental Coordinator issues the SEPA Threshold Determination and the Director of the Development Services Department issues the Design Review decision. An appeal of any Process II decision is heard and decided upon by the City of Bellevue Hearing Examiner.

II. SITE, ZONING & LAND USE CONTEXT

A. Site



Total site size for this project is .355 acres or 15,483 square feet.

The site is located at southwest corner of NE 1st Street and 102nd Avenue NE. To the immediate west are a series of mid-rise residential buildings. To the east, across 102nd Avenue NE, are underdeveloped parcels that contain low rise structures. To the north, across NE 1st Street is the Downtown Park.

As part of the Old Bellevue neighborhood, the site is convenient to the Downtown Park and the Old Bellevue Shopping District. The Old Bellevue neighborhood is characterized by low to medium density development and acts as a transition zone between the Downtown Park and its peripheral areas.

The building is addressed off 102nd Avenue NE via a double height corner related entry. A ground floor bar/restaurant will activate NE 1st Street while the hotel lobby will activate 102nd Avenue NE, which is a Type "A" street. The vehicular entry is located south of the lobby. This will also function as the vehicular drop off and port cochere for the hotel.

Building servicing, loading and access to dumpsters is provided with a 13'-6" clear ceiling height and will be located at the southeast corner of the building. The loading area is sized to accommodate a 30' long vehicle that can enter on and back off within the site. Pickup for garbage will be staged near the garage entry.

At grade the building will be set back five feet off the west property line to provide light and air at the ground level of the neighboring property which is the McKee Condominium. At level three, the entire tower will be set back 20' from the west property line. To the east, the building is set 4'4" off the property line to provide the required 12 foot sidewalk. At level three the east façade of the tower will set back an additional 15'. Zero setbacks are provided at the north and south property lines at grade. At level three the tower will set back 20' from the south property line.

Street trees in grates line both NE 1st and 102nd Avenue NE reinforcing the urban character of the street. At the street corner, on-grade planters include a mix of shrubs, grasses and perennials consistent with existing plantings along NE 1st. A large planter at the SE corner of the building includes an evergreen Magnolia with grasses and low shrubs below.

B. Zoning



The property is located within the Downtown-Old Bellevue (DNTN-OB) land use district, Subdistrict B. No concomitant agreement applies to the property.

C. Building Design

The project's primary urban design goals are to reinforce the architectural backdrop to Bellevue's Downtown Park, provide a strong, well defined corner at 102nd and NE 1st and create an updated version of the memorable architectural street wall along Manhattan's Central Park. The base of the building is comprised of the more public program such as a restaurant, bar, lobby and banquet spaces and will activate NE 1st Street and 102nd Avenue NE.

The building massing is conceived as a series of thick masonry walls articulated by vertical glazing. This is emphasized by the cornice height being offset between the glazed and masonry portions. The exterior will be a punch window masonry exterior with masonry accents at window sills, plinths, cornices and string courses.

North Elevation (NE 1st Street—See Sheet A3.01): The North façade is the most significant because of its unique relationship to the Bellevue Downtown Park. The façade is vertically proportioned into three central glazed bays articulated by brick pilasters and capped by a projecting metal accent cornice at the roof line. This façade is the most articulated and is intended to present an iconic, memorable façade to the Park.

A warm antiqued brick blend will be the typical exterior cladding. A darker brick will be used at accent string courses to provide more detail at street level. Accent brick detailing at level 3 window sills will differentiate the building's podium. Double height glazing at the restaurant, lobby and banquet spaces will provide more transparency into the podium. The two story base wraps the NE corner from 102nd Avenue NE and weaves into the 9 story building elevation. At the sidewalk, a painted cast-in-place concrete plinth will ground the building and provide human scale. At the NW corner, the building steps down in scale to a one story elevation. The brick clad first floor portion steps back from the north property line 3 feet and is held 5 feet off the west property line in order to make a more graceful transition to the neighboring property.

The window openings at levels 8 and 9 are grouped together to create a more vertical pattern unique to the building's top. A continuous band of accent brick at the 8th floor will also help differentiate the top two floors. The northwest and northeast corners of the building will be glazed to reduce the perceived building mass and create a more distinctive silhouette. At the rooftop, translucent glass and metal screen walls will add a level of filigree and interest to the

building's silhouette. The associated mechanical screen wall panels are located to complement building massing and will be painted to complement the overall building palette. Mechanical equipment will be entirely screened from all vantage points.

West Elevation (Façade facing McKee—See sheet A3.04): This façade is comprised of a lower one story portion that steps back 5' from the property line and is predominantly blank except for exhaust grilles. The setback allows daylight to penetrate closer to ground plane and provides an exit path for stair discharge. Above level two the building steps back 20' from the property line. The façade is glazed at the NW corner from levels 3 through 9. The primary elevation is separated into two masses by vertical glazing to proportion the façade more vertically. The fenestration is a punched window expression with an accent brick string course at level 8 along with double height punch windows at level 8 and 9 to help accentuate the building's top.

South Elevation (See Sheet A3.03): The first two levels of the south façade are on the property line and comprised of CMU block as infill within the concrete frame. Above level 3 the tower will set back 20' from the south property line. This wall plane continues to the building roof and is stepped back at the mechanical screen wall. The top two floors are differentiated by a continuous band of accent brick string course at the 8th floor. Large double height windows express the 8th and 9th floors and continue vertically to help further articulate the south façade. The façade is separated into two masses by a vertical glazing to proportion the façade more vertically.

East Elevation (102nd Avenue NE—See Sheet A3.02): The two story podium is set back from the property line 4'-4" to accommodate the required 12' sidewalk. The wall is recessed south of the garage entry, to accommodate a planter located at the SE corner. The fuel fill for the emergency generator is located within the planting area and is accessed by pavers. The heat rejection louver for the emergency generator is located behind the landscaping. The garage entry will conform to all site triangle dimensions. The podium is fenestrated with large windows to connect the activity of lobby, prefunction and banquet room to the street. A series of canopies to promote pedestrian scale will be integrated into the vertically proportioned windows along with a painted cast-in-place concrete plinth and masonry accent texture at the spandrels.

The building will step back 15' from the east property line above level 3. At the southeast and northeast corner, the building will be glazed to reduce the perceived building mass and create a more distinctive silhouette. The window openings at the two penthouse levels are grouped together to create a more vertical pattern unique to the building's top. The top two penthouse floors are differentiated by a continuous band of accent brick at the 8th floor. A central bay of vertical glazing will proportion the east façade more vertically. Custom lighting sconces, up lights, and hardware will be provided at all pilasters between glazed openings.

Site Amenities:

Various amenities will be offered to guests. They include a well designed entry lobby, building prefunction spaces, and banquet spaces.

III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS

A. General Provisions of the Land Use Code

Uses are regulated by Section 20.10.400 (Use Charts) and 20.25A (Downtown-Old Bellevue zoning district). In addition, the uses proposed for this project are permitted with Design Review approval subject to meeting the Design Review criteria of Land Use Code (LUC) 20.25A.110 and the Design Guidelines – Building/Sidewalk Relationships of LUC section 20.25A.115, summarized as follows:

Item	Permitted/Required	Proposed	Comments/Conditions
Project Limit	No minimum	15,483 SF	None
Building Height	90 FT	114 FT	Max. Height + 10% (9' plus 15' for mechanical) See Section III B for additional height analysis.
Floor Area Ratio (FAR)	2.0 Base / 5.0 Max	4.75	The project FAR is 73,588 sf
Total Gross Square Feet (GSF)	No minimum	75,269 GSF (Building) 40,637 GSF (Garage)	None
Exempt Retail/Ped-Oriented Frontage Floor Area	Maximum 1.0 FAR	2,041 SF (.13 FAR)	Level 1 Restaurant/Bar/Lobby proposed
GSF for FAR	77,415 GSF	73,588GSF	Exempt restaurant not included

Item	Permitted/Required	Proposed	Comments/Conditions
Floor Area per Floor Above 40 Feet	20,000 GSF / Floor	8,072 GSF / Floor	Levels 4-7
Floor Area per Floor Above 80 Feet	12,000 GSF / Floor	8,072 GSF / Floor	Levels 8-9
Setbacks			
Fronts (North and East) 102 nd Avenue NE NE 1 st Street	0 FT	4'-4" FT	LUC 20.25A.020 (0' setback from back of required sidewalk)
Side (South)	0 FT	0 FT	LUC 20.25A.020
Rear (West/McKee)	0 FT	5 FT	LUC 20.25A.020
Stepbacks			
Upper level Stepback in Perimeter Design District West/South Elevations East Elevation	20 FT 15 FT	20 FT 15 FT	LUC 20.25A.020, note 25 LUC 20.25A.090.D.5.a
Sidewalk Width	12'-6"	12'-6"	8' Sidewalk + 4' Planting Strip + 6" Curb
Street Tree/ Species & Caliper	3", except 4" on Main Street, 100 th Avenue, 12 th Street in Perimeter District	Scarlet Oak (QC) and Skyline Honey Locust (GTS) / 3"	Species meets LUC requirements.
Mid-Block Walkways	Not Required	None	LUC 20.25A.D60.C.4.a

Parking and Loading

Item	Required		Proposed	Comments/ Conditions
Parking	Minimum and Maximum Permitted Ratios		Proposed Parking Stalls	Maximum Parking Stalls per Use
Hotel Rooms (108 Rooms)			30 Min 41 Max	41
Banquet	NA	NA	15 Min 76 Max	72
Restaurant	NA	NA	13 Min 22 Max	22
Employees	NA	NA	19 Min 24 Max	24
Total Parking Demand	NA	NA	77 Min 163 Max	163
	Required/Permitted		Proposed	
Striped Parking Stalls	NA		86	
Stacked Valet Parking Stalls	NA		29	
Off-Site/Overflow Parking	NA		50	
Compact Stalls	75 Max		53= 46%	Based on 65% Max
Loading Area	10' x 30' within building + 40' for maneuvering clearance		Proposed location on A2.04 – Level P1	
Recycling & Solid Waste	(LUC 20.20.725)		285 SF	Email from Wes Smith, commercial supervision for Allied Waste of Bellevue dated 10/15/07.

B. Analysis of Height Modification

The applicant has requested a height increase beyond the underlying zoning district. LUC 20.25A.090.D.2, subnote 2 states the following regarding height modifications:

In Subdistrict B, the maximum height may be exceeded by no more than 10 percent, and only if the applicant can demonstrate that the additional height accommodates architecturally integrated mechanical equipment, interesting roof forms, significant floor plate modulation, significant facade modulation, or other such unique architectural features, and that the resulting design exceeds the quality and design requirements of LUC 20.25A.110.

The basic height for the DT-OB, Subdistrict B is 90 feet. Height may be increased to 99 feet per LUC 20.25A.090.D.2, subnote 2 as noted above. An additional 15 feet is permitted for mechanical equipment. Total maximum height is 114 feet. Building height proposed is 114 feet which includes the increase for mechanical equipment.

The applicant should note that if the design of the mechanical equipment increases in height, a building height reduction will be necessary to conform to this standard. Changes to the shown roof details below are subject to approval by staff and consistency with the code during building permit review. If modifications are made after building permit approval, the Land Use Exemption (LUX) process may be used. Modification to the roof design/mechanical equipment without such approval may place the height increase gained at risk for reduction to the normal maximum building height of 90 feet plus 15 mechanical or 105 feet. See Section X.C for related condition.

A height modification is granted because the applicant has proposed the following:

- Placement of the elevator overrun in the center of the building.
- A 11 foot glass and metal screen wall to screen the elevator overrun and to provide additional area for placement of associated equipment.
- Translucent glass screen walls provide an iconic park-facing element atop the north façade.
- Vertical stepping between glass and masonry portions of the roof parapet articulates the building mass.
- Rooftop screen walls are located to complement building massing to provide continuity between building top and main body.
- Provision of a roof cornice at the north elevation to further define building identity at this elevation while providing a unique architectural feature.
- The colors and materials used for the mechanical equipment screen walls will match the building body creating an architecturally integrated system.

See image below:



As conditioned, the proposal is eligible for a modification of height per LUC 20.25A.090.D.2, subnote 2.

C. Special District Requirements

1. FAR Amenity System Requirements

Subject to LUC 20.25A.020C.2, the amount of bonus floor area in square feet generated by the amenities provided to meet the Basic Floor Area Ratio (FAR) requirement must at a minimum be equal to 20% of the Project Limit area times the Basic FAR permitted for a non-residential building in the land use district. The Basic FAR requirement in square feet = $0.20 \times \text{the Project Limit area} \times \text{the Basic Non-residential FAR}$ ($0.20 \times 15,483 \times 0.5 = 1,548.3 \text{ SF}$).

The amount of "Basic" FAR Amenity Earned (30,583 SF), calculated below, exceeds the minimum required 1,548.3 SF by 29,034.7 SF.

Table 1: Bonus Amenity Area Earned

Project Gross Floor Area (GFA)	72,682 SF
Basic Permitted Floor Area (Basic FAR X Project Limit)	30,966 SF (2.0 X 15,483 SF)
Additional Floor Area Requested (Project GFA – Basic Permitted Floor Area)	41,716 SF (72,682 – 30,966)
“Basic” FAR Amenity Earned	30,583 SF (Refer to Table 2 below)
Remaining (“Bonus”) FAR Amenity to Earn (Project GFA - Basic Permitted Floor Area – “Basic” FAR Amenity Earned)	11,133 SF (72,682 - 30,966 - 30,583)
Total FAR Amenity Earned	111,857 SF (Refer to Table 2 below)
Excess FAR Amenity Earned (Total FAR Amenity Earned – Basic FAR Amenity Earned – Bonus FAR Amenity to Earn)	70,141 SF (111,857 – 30,583 – 11,133)

Table 2: Bonus Amenities

Subject to LUC 20.25A.030.D, the total amount of bonus floor area earned through the Amenity Incentive System for a project, and the total amount of bonus floor area to be utilized on-site for that project, must be recorded with the King County Division of Records and Elections and with the Bellevue City Clerk. Therefore, a condition is included in Section X of this report requiring that the applicant record a copy of the approved bonus point calculations, project drawings and conditions of this Design Review approval. See Section X.D for related condition.

Amenity		Units of Measure	Bonus Ratio	Bonus Floor Area Earned	How it Meets the Description and Design Criteria? Public Benefit?
Basic Amenities	Pedestrian Oriented Frontage (Level 1)	LF	200:1	(148'-1" x 200') 29,617 SF	Stimulates sidewalk pedestrian activity.
	Marquee (east elevation)	SF	2:1	(483 SF x 2) 966 SF	Pedestrian cover from inclement weather.
	Sub-Total Basic			30,583 SF	

Non-Basic	Underground Parking	SF	2:1	(40,637 SF x 2) 81,274 SF	Hides unsightly parking area underground.
	Sub-Total Bonus			81,274 SF	
	Combined Total			111,857 SF	

2. Design Review Criteria and Design Guidelines

The proposal meets the Design Review criteria (LUC 20.25A.110) as follows:

Site Design Criteria

Vehicular Circulation and Parking

- a. *Provide efficient vehicular access to parking and service areas which is coordinated on a superblock basis.* **Finding:** The superblock configuration does not exist in Old Bellevue. Vehicular access will be available via one access point: the southeast corner of 102nd Avenue NE. This access point will lead vehicles into the interior of the garage with porte cochere provided for guests to drop their cars for valet service. A loading dock will be provided in this same vicinity.
- b. *Coordinate the location of vehicular and pedestrian mid-block connections, considering opportunities for mid-block crossings.* **Finding:** No midblock connections are required for this project. A midblock connection exists west of the McKee development.
- c. *Provide for safe and pleasant movement within the Downtown for the pedestrian and bicyclist.* **Finding:** The NE 1st Street sidewalk will remain the same size at 12 feet. The 102nd Avenue NE sidewalk will increase from 8 to 12 feet as required by the LUC. A 12 foot sidewalk with a two foot scoring pattern will be provided along with vegetation and benches. Since this is a Type "A" right-of-way along 102nd Avenue NE, the addition of these amenities to this area will further define the pedestrian connection to the Downtown Park, Main Street and Meydenbauer Bay.
- d. *Maximize the separation of vehicular traffic from pedestrian areas by means of level changes, space and distance, or landscaping.* **Finding:** The sidewalk is separated from the roadway by a six-inch curb and four-foot tree grates.
- e. *Incorporate retail shopping space at ground level into parking structures whenever practical and appropriate.* **Finding:** Ground level public spaces such as restaurant/bar, hotel lobbies and main building entry will be located fulfilling standards for a Type "A" street.

f. *Minimize the location of parking adjacent to pedestrian connections.* Finding: All parking is located within the building. On-street parking exists east on 102nd Avenue NE in the public right-of-way. These parking stalls will continue to exist after this proposal is complete.

g. *Limit the number of driveway openings and the number of access lanes in each opening.* Finding: Only one driveway opening will be proposed at the southeast corner of the site.

h. *Where appropriate, fit garages into the topography and make use of garage roofs for public open space, recreation or landscaped areas.* Finding: Hotel Lobby, restaurant and bar are proposed over the P1 level of the garage.

i. *Maximize the use of underground parking.* Finding: All parking will be contained within 3 levels of underground parking.

Pedestrian Circulation and Amenities

Pedestrians can circulate around the building by way of sidewalks at the north and east edges of the site. Street trees, weather protection, benches, landscaping, and special paving are used to provide a more pleasant pedestrian experience.

Wind and Sun

The building is designed to step down to the pedestrian level. Required building setbacks on the west, south and east elevations define the building podium. These setbacks allow sun penetration into the project. Level one will meet building setbacks which are zero for this district on the north, south and east sides. The applicant has chosen to provide a five foot building setback at the west elevation. This setback will provide further sun penetration and air circulation in the area between the Essex and the McKee to the west.

Sheet L1.01, landscaping also makes provision for trees along the north and east edges of this site. Additionally, the presence of the building wall will block prevailing winds from the south.

Light and Glare

The project will minimize light spill and provide cut-off fixtures to reduce glare whenever possible. The project will respect all existing and future adjacent residential projects with no exterior building illumination to the west or south except as needed to provide exit pathway lighting.

Downtown Patterns and Context

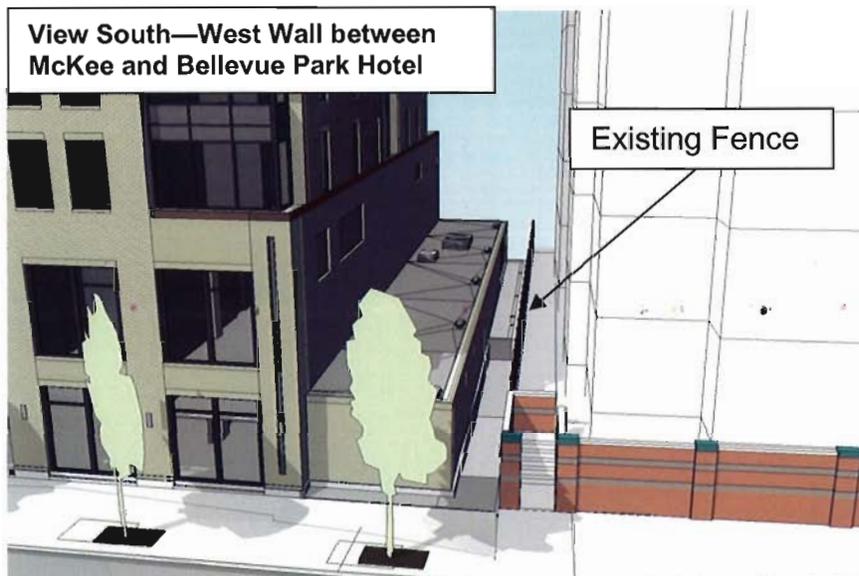
Natural Setting and Topography

This site is almost rectangular in shape and measures approximately 137 feet along NE 1st Street and 115 feet along 102nd Avenue NE. Topography on this site is generally level with a slight drop in elevation from the north property line to the south property line of about four feet. There is a drop of only one foot between the west and east property lines.

Landscape Design

Landscaping will be provided along NE 1st Street and 102nd Avenue NE. The site does not contain significant existing vegetation. There is an existing street tree in a grate along NE 1st Street. See Section II. A (Site) above for further discussion regarding landscaping.

Views



McKee

Residents at the very lower levels of the east elevation will have views of an existing six foot fence located on McKee property. The applicant has provided a five foot setback to allow better sun penetration and minimized the parapet height along that wall. The building will set back an overall 20' from the McKee above the first level.

building materials in this vicinity have been upgraded from the initial submission of concrete to a colored masonry as shown on Sheet A3.04.

In addition to the above,

Bellevue Park Hotel

Floors 7 through 9 will have views westerly to Seattle over the McKee. The windows have been designed to off-set, as much as possible, to provide privacy between the two buildings. There are no windows at the west façade of Level 2 to provide greater privacy to the residents of the McKee.

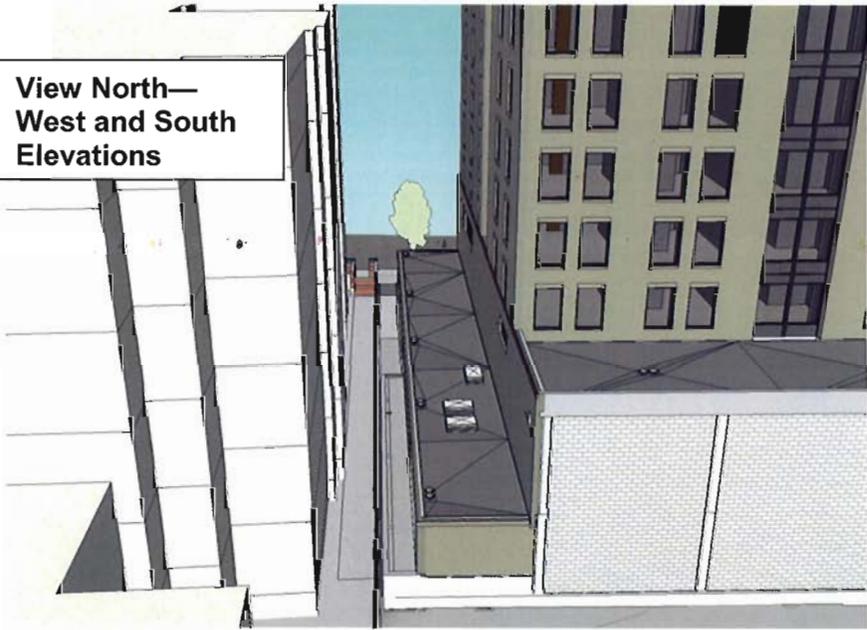
The northern hotel rooms of the Bellevue Park Hotel will have views of the Downtown Park and beyond into the downtown core.

Building Height and Bulk

The building base has been designed to maximize the permitted zero setbacks for this zoning district with exception of the west property line where a five foot setback is provided. Required building setbacks of 20 feet on the west and south property lines cause the tower portion of the structure to diminish in size and square footage. A required 15 foot setback further defines the building tower at the east elevation. No such setback is required for the north elevation. Provision of these building setbacks at the upper levels creates space that will provide light and air between structures upon future redevelopment. Diminishing floor plates of the tower potentially may allow views north to the Downtown Park from properties south of the park.

Initially, the proposed building will be larger than the surrounding neighborhood. As redevelopment occurs, buildings of similar height may be produced using current zoning codes. This structure will be 114 feet including mechanical equipment. The adjacent building, the McKee has a building height of 69 feet for building A and a height of 54 feet for building B. To the west of the McKee is the Seasons at 55 feet in height for the west building and a height of 90 feet for the east building.

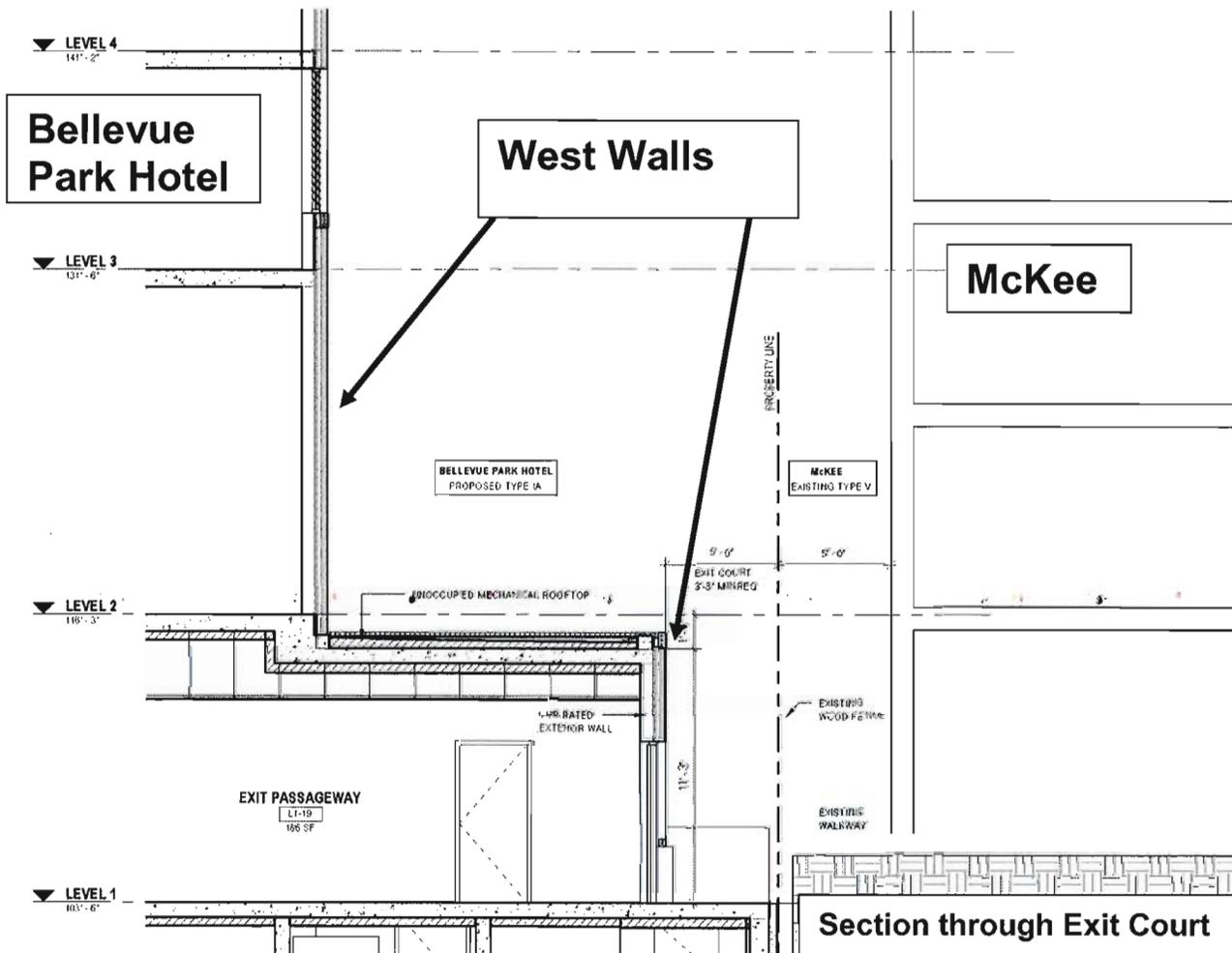
The roof top of this structure has been designed as an integrated form of this structure. The roof contains modulation and integrated building materials to conform to the building body.



Transitions

The project has been designed to maximize open space at the west elevation with the provision of a five foot setback from the west property line. The applicant has provided five feet to create a larger open space between properties and sun penetration into this area.

See diagram below:



Patterns of Activity

The building provides various opportunities for pedestrian-level activity including ground level retail, residential lobby and leasing spaces. At the northeast corner of this site, the pedestrian areas will be enhanced with benches, landscaping, and scored paving materials.

Signage

Signage information has not been provided at this time. General signage information noted on the building will be required prior to building permit issuance with a formal sign package following such submittal. See Sections X.C and E.

Design Guidelines – Building/Sidewalk Relationships (LUC 20.25A.115)

NE 1st Street is classified as a “C” Right-of-Way while 102nd Avenue NE is classified as an “A” Right-of-Way. These rights-of-way have been given these designations as it is anticipated that they shall have moderate to the highest orientation to pedestrians respectively. Because of these street classifications, the applicant has oriented the main restaurant frontage towards the

north face of the site for direct views from street level due to the moderate pedestrian use in this area. Conversely, 102nd Avenue NE has been given the highest pedestrian orientation and accessibility due to its direct connection to Main Street.

Long term development plans are underway for the Meydenbauer area with linkages between this area and the downtown park, 102nd Avenue NE increases in importance as a pedestrian connection. Provision of additional retail will further activate this area of the Old Bellevue.

Downtown-Old Bellevue District

1. Street Improvements. The applicant shall provide half-street and sidewalk improvements including paving, street trees, lighting and other street furniture comparable to the existing Main Street streetscape between 102nd Avenue and Bellevue Way on:

- b. 102nd and 103rd Avenues between SE 1st Street and NE 1st Street; and

Response: The applicant will provide sidewalk improvements on NE 1st Street and 102nd; thus, complying with the standard above.

2. Mid-Block Connections.

Response: Not required for this site.

3. Parking. An off-street parking area may not be located between the buildings and streets along:

- a. 102nd Avenue NE between Main Street and NE 1st Street; and

Response: All associated parking for this project will be provided underground to comply with the standard above.

4. Minor Publicly Accessible Spaces.

Response: Not required for this project.

5. Pedestrian-Oriented Frontage and Marquees or Awnings.

- a. The applicant shall provide pedestrian-oriented frontage and marquees or awnings on:
 - ii. 102nd Avenue frontage.

Response: The applicant has complied with the standard above by providing 52'-8" of marquees along 102nd Avenue NE.

- b. Pedestrian-oriented frontage, marquees, and awnings must comply with the design criteria of LUC 20.25A.030.C, and the FAR amenity bonus may be utilized.

Response: The applicant complies with the standards detailed above for marquees.

- c. Pedestrian-oriented frontage must include display windows having mullions that are spaced two to six feet apart.

Response: Windows have been provided along 102nd Avenue NE to allow pedestrians visibility into the hotel lobby spaces.

- d. The use of exposed concrete, metal or plastic for storefront facades is not permitted. This does not preclude the use of metal and acrylic glazing on marquees or vinyl fabric on awnings, nor the use of brass, copper or aluminum for decorative trim.

Response: The applicant has proposed a painted cast-in-place concrete base as shown on Sheet A3.02.

Perimeter Design District

The Perimeter Design District shall include the following property west of the centerline of 112th Avenue NE within the Downtown as defined in LUC 20.50.016. This site is located within Subdistrict B.

1. Development projects should include a mid-block street, where feasible, to provide more convenient circulation within the perimeter of the Downtown and to promote development with a human scale.

Response: This standard is not applicable as mid-block connections are not required for this site either through pedestrian or vehicular connections.

2. Buildings should incorporate interior arcades, open courtyards, enclosed plazas or combinations thereof which offer mid-block pedestrian connections between perpendicular and/or parallel streets.

Response: As discussed above, mid-block connections are not required for this project.

3. Buildings should be clad with materials which minimize reflected light. Overhangs, awnings, sunscreens and other devices should be considered in order to minimize conditions of glare.

Response: The building will be clad with one masonry building color as shown on Sheet A3.01. These colors are a weathered tan in color with an accent brick at the top of the building and at various intervals throughout the building body. A dark brown/grey metal panel material will be used for the cornices and mechanical screen. All materials have been proposed to minimize reflected light to comply with the standard above.

4. Building facades should be divided into increments through the use of bay windows, offsets, angled facets, recesses and other architectural features which serve to break down the scale.

Response: The proposed building fulfills the standards above. See Section III.C.2 above.

5. Rooftops should incorporate features such as pitched or sloped forms, terraces, perimeter planting to soften an otherwise rectilinear profile.

Response: The applicant has proposed a flat roof with a glass and metal screen wall to provide a sculpted building top. See Section III.B above for further roof discussion.

6. Surface parking should be concealed from street level views by berms, hedges, walls or combinations thereof.

Response: No surface parking is proposed for this project.

7. Special attention should be given to the provision of elements at or near the ground level such as awnings, recessed entries, water features, address signs, seasonal flower beds, seating, pedestrian-oriented uses and display kiosks.

Response: The applicant has complied with the above by providing benches, a marquee on the east elevation, street trees, and pedestrian oriented uses on 102nd Avenue NE.

IV. PUBLIC NOTICE AND COMMENT

Public Correspondence: To date, staff has not received any written correspondence regarding this project.

Outreach Meetings

The applicant held one documented outreach meeting on this proposal as summarized below:

The applicant provided public notice to owners within 500 feet a letter that described the project and that it was converting from condominium to hotel. Then on March 17, 2009, the owner and his consultants met with two residents of the McKee to discuss their project. A general overview was given by the project architect. Residents who attended asked questions regarding the conversions of this proposal. The residents requested that small modifications be made to the kitchen exhaust that it not vent from the sidewall but to the roof and that the podium be modified so that it would limit interference with light, air, and view. Modifications made by the applicant were satisfactorily accepted by the two McKee residents.

See project file for meeting minutes sent by Shugart Bates.

City Public Meetings

In addition to the above outreach meetings, the Land Use Code (LUC) provides for an optional public meeting for design review applications. Staff determined that such a meeting was necessary and held this meeting on February 25, 2010. There were approximately eight individuals who attended this meeting. See project file for list of speakers.

Many of the attendees of this meeting were from the McKee. Comments from speakers on this proposal were wide ranging. See the following comments:

- Questions were raised regarding an external exit path along the west portion of the site. Some of the adjacent residents had security questions regarding about this exit. The applicant replied that the area would be monitored and that a security camera will be mounted on the building wall exterior
- Concerns were expressed about the proposed exhaust fans. The applicant responded that a noise study would be done to ensure that the fans would meet City requirements and also their hotel standard of luxury.
- A few questions were raised about construction schedule and timing.
- Positive comments were received from adjacent residents regarding the proposed restaurant. Many felt that this would enhance the south edge of the Downtown Park.

V. TECHNICAL REVIEW

A. Utility Department

Utility review has been on a conceptual basis only; consequently there are no implied approvals of the engineering specifications for the water, sewer and storm drainage components of the project. Engineering review of the water, sewer and storm drainage infrastructure will be performed under the Utility Developer Extension Agreements, and will coincide with the clearing and grading permit review. Final civil engineering may require changes to the site layout to accommodate the utilities. See Section X.A for related condition.

B. Fire Department

The site development plans for this application generally conform to Fire Code requirements. However, there are a number of conditions that must be met prior to issuance of building permits. See Attachment C for related conditions.

C. Transportation Department

Site Access: Access for this site will be provided via a 24 foot wide driveway off of 102nd Avenue NE at the southeast corner of the building. This location will allow for:

- Maximum clear distance from the nearest intersection;
- Adequate sight distance.

Deliveries and loading will be required to take place entirely on site or through the legal use of on-street parking spaces. All other on-street loading will not be allowed.

Street Frontage Improvements: In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, the construction of street frontage improvements is required as a condition of development approval. The design of the improvements must conform to the requirements of the Americans with Disabilities Act, the Transportation Development Code (BCC 14.60), the Transportation Department Design Manual, and requirements stated elsewhere in this document.

1. Analysis of street lighting levels shows that the developer is required to install a

streetlight on 102nd Avenue in order to achieve minimum standard lighting levels on the street near the proposed driveway. A streetlight location and light fixture type identified in the clearing and grading plans received (from a previous application) at the city on August 6, 2008 are satisfactory for the Design Review. Complete engineering details and specifications for the streetlight and all related equipment must be included in the project's final engineering plans. The identified streetlight location is on the eastern side of 102nd Avenue, opposite the site's southern boundary. Installation across the street is necessary because the project's driveway and the nearby driveway on adjacent property to the south preclude installation of a streetlight in the optimum portion of the site's frontage. Streetlight installation across the street will likely require asphalt restoration and replacement of some sidewalk panels on the eastern side of 102nd Avenue. The identified light location does not conflict with any street trees.

2. The Americans with Disabilities Act (ADA) requires that sidewalk cross slopes not exceed two percent. The sidewalk cross slope may be less than two percent if the sidewalk has a longitudinal slope sufficient to provide adequate drainage. Bellevue's standard for curb height is six inches, except where curb ramps are needed. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm compliance. New curb and sidewalk shall be constructed in compliance with these requirements. Building elevations shall be consistent with the required curb and sidewalk elevations. Spot elevations must be included in the building plans in a manner that proves that building elevations are designed to correspond to the sidewalk elevations shown in the engineering plans, especially at entrances and other key points. Curb and sidewalk elevations will not be revised to fit the building, and city inspectors may require spot surveys during construction in order to confirm the required elevations. ADA also requires provision of a consistent travel path for visually handicapped pedestrians. Potential tripping hazards are not allowed in the main pathway. Any planter boxes installed in the sidewalk must be designed to reduce the tripping potential and, if attached or adjacent to the building, must not extend more than two feet from the building. Traffic signal controller boxes, streetlight contactor cabinets, and fire hydrants must be located so as not to interfere with the main pedestrian path. Buildings shall be designed so that doors do not swing out into the pedestrian path.

ADA-compliant curb ramps shall be installed where needed, consistent with standard drawings TE-12 or TE-13. Two existing ramps at the southwestern corner of the adjacent street intersection must be replaced if the Transportation Department inspector determines that they do not meet current standards. Installation of Polara-type pedestrian pushbuttons, "countdown" pedestrian signal heads, and related equipment is required to update the developer's end of both crosswalks at the street intersection. Plan details and specifications for such equipment must be included in the final engineering plans.

3. The sidewalk on both street frontages shall be completely removed and reconstructed with a standard public sidewalk at least twelve feet wide. This width does not include the curb or any bulb-outs. Planters or tree wells are included in the

twelve-foot width. Existing driveways shall be closed with installation of standard curb and gutter in addition to standard sidewalk. On NE 1st Street, some existing curb and gutter may remain in place, if the Transportation Department inspector determines that it is in satisfactory condition and it is not damaged during construction.

4. The sidewalk and landscaping on both street frontages must comply with the standards and drawings in the Transportation Department Design Manual, including standard drawings TE-11 and DEV-3. The sidewalk shall be constructed of standard concrete with a broom finish and a two-foot by two-foot score pattern, with four-foot by six-foot tree wells. However, the applicant has proposed non-standard paving for each frontage. If the Transportation Department and the Development Services Department agree to accept non-standard paving, the approval process must conform to the Alternative Paving Materials section below.

Any non-standard features or vegetation in the public sidewalk area will be allowed only if approved by both the Transportation Department and the Development Services Department, and if an agreement is recorded against the property to hold the landowners responsible for maintenance and replacement of all such non-standard sidewalk features. Such features shall not create a sight obstruction within any required sight triangle, shall not create a tripping or slipping hazard in the sidewalk, and shall not create a raised fixed object in the street's clear zone. The materials and installation methods must meet typical construction requirements.

5. Tree wells and other landscaping within the sidewalk on either adjacent city street shall be irrigated with a metered water source. Electrical connections for lighting in tree wells or planter strips may be allowed, if installed in compliance with the electrical code and subjected to an electrical inspection. Irrigation devices and electrical components shall not create a tripping hazard in the sidewalk.
6. The driveway on 102nd Avenue shall have an approach width, as defined in standard drawing DEV-5, of at least 24 feet. The driveway apron design and construction shall be consistent with a modified version of standard drawing DEV-5, as shown on the engineering plans received at the city on August 6, 2008.
7. No new building or garage structure, including pilings, shall be constructed under a street right of way or under an existing public sidewalk/utility easement if there would be any conflict with utilities. In some conditions (to be finalized during engineering and building plan review), new structure may be allowed under a new sidewalk/utility easement. No soil nailing is allowed under a street right of way or sidewalk/utility easement without an indemnification agreement that protects the city.
8. No new utility vaults that serve only one development will be allowed within a public sidewalk. Vaults serving a broader public purpose may be located within a public sidewalk, if a non-slip surface is installed.
9. Any awning, marquee, balcony, or other building fixture over a sidewalk or utility easement or over city right of way must be at least 16 feet above the sidewalk, or be

removable (with an agreement regarding removal and replacement); and must have at least 3 feet horizontal clearance from any streetlight or traffic signal pole. No supports shall be installed within the public sidewalk. Installation of building fixtures over city right of way requires a long-term lease of air rights (see below).

10. No fixed objects, including fire hydrants, trees, and streetlight poles, are allowed within ten feet of a driveway edge, defined as Point A in standard drawing Dev-6. Fixed objects are defined as anything with breakaway characteristics stronger than a 4-inch by 4-inch wooden post. An exception is allowed for a fire hydrant south of the site's driveway, due to the limited space between that driveway and the driveway on the adjacent property.
11. No new overhead utility lines will be allowed within or across any right of way or sidewalk easement, and existing overhead lines must be relocated underground.

Easements: The property owner shall provide sidewalk and utility easements to the City as needed to encompass the full required width of any sidewalks located outside the city right of way fronting this site. At a minimum, that requires granting an easement roughly four or more feet wide along 102nd Avenue. Any utility easements on this site that will be negatively affected by this development must be mitigated or easements relinquished. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.

Lease of Air Space above City Right of Way on NE 1st Street: Building fixtures that are proposed to extend over the existing city right of way on NE 1st Street cannot be allowed unless the developer obtains a long-term lease of air space to accommodate such fixtures. The developer and the Transportation Department have discussed an exchange by which, as compensation for use of the city's air space, the developer would give the city use of and access to part of the building structure to mount a city traffic camera. City Council approval is required for any long-term lease of property rights, including a lease of city air space. City Council approval of any such lease must be obtained prior to issuance of any building permit for this development. If such approval is not obtained, then the building design must be revised prior to building permit issuance to eliminate any use of the city's air space.

Holiday Construction & Traffic Restrictions: From November 15th to January 5th, construction activities such as hauling and lane closures will be allowed only between the hours of 10:00 p.m. and 6:00 a.m. due to holiday traffic. The dates and times of these restrictions are subject to change. The applicant shall contact the Transportation Department Right-of-Way Section to confirm the specifics of this restriction prior to applying for a Right-of-Way Use Permit, which is issued directly by the Transportation Department.

Use of the Right of Way During Construction: Applicants often request use of the right of way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right of Way Use Permit for such activities must be acquired prior to issuance of any construction permit,

including any demolition permit. Sidewalks may not be closed except as specifically allowed by a Right of Way Use Permit.

Pavement Restoration: The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration Program, every street in the City of Bellevue has been examined and placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are, "No Street Cuts Permitted," "Overlay Required," and "Standard Trench Restoration." Each category has different trench restoration requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that often comes with small strip patching.

Near the development site, NE 1st Street is presently classified as "Standard Trench Restoration" and 102nd Avenue is classified as "Overlay Required." Any trench or pavement restoration in 102nd Avenue will require a grind and overlay at least 50 feet long for the full width of any affected lane. Pavement conditions change over time. The exact extent of pavement restoration will be specified in the right-of-way permit for this project, based on field conditions at the time of construction.

Alternative Paving Materials: The developer has proposed a non-standard concrete color and scoring pattern for the public sidewalk and driveway apron area on 102nd Avenue NE. Prior to approval of final construction plans, the developer must submit a color sample for city review. The materials and installation methods must meet typical construction requirements. Approval of the alternative material or color is not guaranteed. If the alternative material or color is approved, then the property owner must sign and record an indemnification agreement which states that all future maintenance and replacement is the responsibility of the property owner. Work within the alternative material area by City, franchise or other workers as a result of either emergency, normal maintenance or new installation will result in replacement of the surface by standard materials. Advance notification of such work will not be provided to the property owner. In such a circumstance, should the property owner wish to replace or repair the surface with the alternative material, a Right of Way Use Permit may be required.

VI. STATE ENVIRONMENTAL POLICY ACT

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the proposal. Therefore, issuance of a Determination of Non-Significance (DNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements, with incorporation by reference of the *2009-2020 Transportation Facilities Plan Final Environmental Impact Statement* (TFP EIS), dated November 2006. This document is available in the Records Office at City Hall, 450 110th Avenue NE. Transportation-related impacts associated with this project are consistent with the potential projected impacts analyzed within this document.

Adverse impacts which are less than significant are subject to City Codes or Standards which are intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code Sec. 22.02.140 provides substantive authority to mitigate impacts disclosed through the environmental review process.

Amendments to Environmental Checklist

The Environmental Checklist submitted with the application was revised per the revised design concept and it adequately discloses expected environmental impacts associated with the proposed project. The City codes and requirements, including the Clear and Grade Code, Utility Code, Land Use Code, Noise Ordinance, Building Code and other construction codes adequately mitigate expected environmental impacts except as noted below. The following amendments to the Environmental Checklist are needed. These impacts will be mitigated through exercise of code authority as well as through project-specific conditions of approval, contained in Section X.A and C.

Noise

Exterior Noise: As conditioned, short term impacts related to noise generation as a result of the construction will be minimized. Normal hours for allowed generation of noise related to construction are from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. Exceptions to the construction noise hour limitation contained in the Noise Control Code MAY NOT be granted pursuant to 9.18.020C.1 & 2. However, prolonged exposure to noise created by extended hour construction activity is likely to have a significant impact on inhabitants of surrounding commercial and residential properties during the proposed timeline for construction. If expanded hours are necessary to accommodate a specific component of construction, a noise permit shall be required for exemption from the Noise Control Code must be submitted in advance of the scheduled onset of extended hour construction activity. In accordance with the Bellevue City Code – BCC 9.10 – Noise Control, the City will only be providing construction noise exemptions for the following work:

- Work in the ROW and essential public facilities (i.e., Water connections that require a main shut off are required to be done at night by the Utility Dept, public school construction)
- Work to accommodate transportation mitigation
- Required evening haul routes

Work that has been previously determined by sound level monitoring to not exceed the maximum permissible noise levels. Utility/site work on private projects/property is not essential public facilities.

Interior Noise Levels: The Bellevue City Code, BCC 9.18, limits interior noise levels within residential structures to 40 dBA in sleeping areas and 45 dBA in non-sleeping areas. Special construction is generally necessary to meet these thresholds. Prior to the issuance of any occupancy permits, the applicant shall verify that this threshold has been met.

See Section X.A and C for related conditions.

TRANSPORTATION

Long-Term Impacts and Mitigation

The long-term impacts of development projected to occur in the City by 2020 have been addressed in the City's Transportation Facilities Plan EIS. The impacts of growth which are projected to occur within the City by 2020 are evaluated on the roadway network assuming that all the transportation improvement projects proposed in the City's current Transportation Facilities Plan are in place. The Transportation Facilities Plan EIS divides the City into several Mobility Management Areas (MMAs) for analysis purposes. The Bellevue Park Hotel lies within MMA 3 (Downtown) which has a 2020 total growth projection of 780,258 square feet of new hotel use. This development proposes 74,736 square feet of Hotel Use (108 rooms with ground level restaurant and bar). Therefore, the volume of proposed development is within the assumptions of the Transportation Facilities Plan EIS.

Traffic impact fees are used by the City to fund street improvement projects to alleviate traffic congestion caused by the cumulative impacts of development throughout the City. Payment of the transportation impact fee, as required by BCC 22.16, contributes to the financing of transportation improvement projects in the current adopted Transportation Facilities Plan, and is considered to be adequate mitigation of long-term traffic impacts. Fee payment is required at the time of building permit issuance.

Mid-Range Impacts and Mitigation

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more p.m. peak hour trips undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained.

This development will generate approximately 65 new p.m. peak hour trips; however, 55 trips were used to check for concurrency (10 trip credit applied for previous use). City staff distributed and then assigned project-generated trips to the street network using the City's EMME-2 travel forecasting model with the current Capital Investment Program network. By adding the expected project-generated trips to the traffic volumes in the model, the area average levels of service were determined. To create a baseline condition for comparison, the levels of service were also determined using traffic volumes without the project-generated trips. In this project analysis, 4 system intersections received 20 or more p.m. peak hour trips. Additional analysis was performed to determine area-average levels of service. The analysis showed that the no congestion allowances were exceeded as a result of traffic generated from this proposal. (The concurrency analysis spread sheet is available in the project file.) Therefore, the proposed development passes the concurrency test. The concurrency test results are included in the Transportation Department file for this development.

The rules of concurrency reservation are outlined in the Traffic Standards Code Director's Rules, updated May 23, 2001. A concurrency determination is issued on the date of issuance of the land use decision. This project complies with the Traffic Standards Code and is receiving a Certificate of Concurrency. See project file for the Certificate of Concurrency.

The concurrency determination is reserved to this project at the land use decision date. The concurrency reservation expires one year from the land use decision date unless a complete building permit application is filed (BCC 14.10.010.D).

Short Term Operational Impacts and Mitigation

City staff analyzed the short-term operational impacts of this proposal in order to recommend mitigation. These impacts included driveway operations, proximity of driveways on adjacent sites, and evaluation of vehicle and pedestrian sight distance. It was determined that short-term traffic impacts can be mitigated by appropriate site design and street frontage improvements, as described in other sections of this document and the project file.

See Sections X: A, B, C, and D for related Conditions of Approval.

VII. CHANGES TO PROPOSAL DUE TO CITY REVIEW

Building Design

- Concern was expressed regarding the light coloring of the proposed brick material. The material was so light that staining could become a problem. The applicant responded by deepening the color of the brick and selecting a blend with a variety of shades.
- The applicant was requested to increase the width of the marquee along 102nd Avenue NE as the initial proposal was too shallow at three feet wide. This has since been increased to 4'-4" feet wide.
- The applicant was requested to review the location of the generator combustion exhaust such that it was not visible from 102nd Ave NE, but also maintained compliance with all applicable mechanical codes. The duct's previous location was above the garage entry at the south-east corner of the building adjacent to the parapet. It has since been relocated to the middle of the south roof on L3. Once it penetrates the roof, it bends 90 degrees and exhausts towards the west.

Site Design

- The Transportation Department required a light level analysis for 102nd Avenue NE. The results of this study require the applicant to place an additional street light on the east side of 102nd Avenue NE.

VIII. APPLICABLE DESIGN REVIEW DECISION CRITERIA

The Director may approve, or approve with modifications, an application for Design Review if:

- 1. The proposal is consistent with the Comprehensive Plan.**

Following are land use policies from the Urban Design Element of the City's Comprehensive Plan that are pertinent to, and supportive of, the project proposal:

Policy UD-1: Encourage high-quality, attractive, architecturally appealing designs for major buildings in order to create distinctive visual reference points in the community. Finding: This project will create an architecturally attractive building in its massing, materials, and detail at the intersection of NE 1st Street and 102nd Avenue NE that is consistent with the Old Bellevue Community.

Policy UD-2: *Support Designs for the built environment that are visually stimulating and thoughtful and which convey excellence in architecture and workmanship, and durability in building materials.* Finding: This project proposes high quality durable materials, including two types of masonry, metal panels for cornice and mechanical screening, as well as pavement materials. The building design incorporates careful attention to detail, variety in building heights and elements, integration of landscape, and building forms to create a visually stimulating, cohesive project.

Policy UD-3: *Encourage a variety of site and building designs which are compatible and consistent with surrounding development and that implement the policies of this plan.* (See finding below for UD-4).

Policy UD-4: *Ensure that the development relates, connects, and continues design quality and site functions from site to site.* Finding: This project will be compatible with the high quality high-rise and mid-rise residential development that exists now in Old Bellevue to the west and south along Main Street as well as the lower rise retail development along 102nd Avenue NE to the east and south of this project.

Policy UD-6: *Design buildings located on the edge of public spaces using materials, forms, details and other architectural elements that will enrich the appearance of the places and encourage people to use them.* Finding: The east side of the building faces 102nd Avenue NE will provide pedestrian interest through the use of windows and a view to a monumental stair in the hotel lobby to create visual interest to passersby while increasing amenities such as benches, façade articulation, and overhead weather protection for retail activities. The proposed building materials will enrich this intersection to create further visual interest. Additionally, this site is located directly south of the Downtown Park. The north elevation contains a restaurant along the first floor and ballrooms along the second floor which both will front towards this park. Occupants of these spaces will have views towards the park and pedestrian access view the main building entry at the north east corner of the building.

Policy UD-8: *Design rooftop mechanical screening so that it is integral with building architecture. Consider the visual effects of technical advances such as satellite dishes, on building design.* Finding: All roof-top mechanical equipment will be enclosed within a penthouse that has been designed to integrate with the building design. See discussion above in Section III.B

POLICY UD-10: *Encourage public and private development to incorporate access to sunlight.* Finding: This proposal has been designed to allow light penetration at the tower level by its diminishing floor plate above the podium. The podium setback of five feet along the west property line further allows light penetration between this site and the McKee to the west. See Section III.C.2 Sun and Wind above for further discussion.



Policy UD-11: *Encourage architectural elements that provide for both rain cover and access to sunlight in pedestrian areas.*

Finding: Transparent/ translucent marquees will be provided on 102nd Avenue NE at the back of sidewalk at the east elevation. A canopy will be provided at the

northeast corner to provide weather protection at the building entry.

Policy UD-14: Encourage seasonal color plantings in public and semi-public areas.

Policy UD-15: Use landscape design that is urban in character in more urban settings

Finding: The landscape design incorporates both hardscape and landscape to respond to the urban setting, and includes street trees along all public streets. Seasonal color will be provided as well.



North Elevation

Policy UD-20: Preserve and encourage open space as a dominant element of the community's character. Finding: A small, at grade, gathering space will be provided at the northeast corner of the site. It will offer benches, interesting pavement materials, and landscaping. The placement of the plaza in this vicinity will create a focal point for the intersection of NE 1st Street and 102nd Avenue NE.

POLICY UD-71. Permit high intensity residential development subject to design criteria which assures a livable urban environment. Finding: The applicant has applied for design review approval per the Land Use Code and is therefore; subject to all of the design criteria and applicable plan policies for this project. By submittal of this application and project design, the applicant has complied with this policy.

This proposal is also consistent with the following policies from the Downtown Sub-area of the Comprehensive Plan:

POLICY S-DT-1 promotes Downtown livability, with provisions made for the needs, activities, and interests of Downtown residents, employees, shoppers, and visitors. Finding: The restaurant will provide additional activities for area residents.

POLICY S-DT-2 encourages a variety of land uses to occur in mixed-use buildings or complexes where appropriate. Finding: This is a mixed use building with a dining and drinking establishment at the base with residential above.

POLICY S-DT-3 promotes development of Downtown as an aesthetically attractive area. Finding: This proposal will add to the aesthetic environment of the Downtown.

POLICY S-DT-4 states that the highest intensity development shall be located in the core of Downtown, with diminishing intensities towards the edges of Downtown. Finding: By its location, this site lies outside the Downtown core within Subdistrict B which requires lower building heights, etc.

POLICY S-DT-5 sets out to organize Downtown to provide complementary functional relationships between various land uses. Finding: This proposal will complement the mixed use environment of Downtown-Old Bellevue by adding to the restaurant/residential base.

POLICY S-DT-7 encourages the Downtown to continue to serve surrounding residential areas as a neighborhood retail district. Finding: Residents of this project will be served by the higher intensity retail/service/entertainment uses within the Downtown.

POLICY S-DT-9 promotes use of bonus incentives (related to permitted intensity, height, etc.) for private developments to accomplish the public objectives. Finding: See Section III.B above for discussion on how this project qualifies for the bonus incentive for height.

POLICY S-DT-24 promotes density incentives to encourage urban residential development throughout Downtown. Finding: See Section III.A above for review of project density.

POLICY S-DT-25 supports for a range of Downtown urban residential types and densities. Finding: Complies with standard.

POLICY S-DT-26 encourages residential uses to occur in mixed-use structures or complexes. Finding: Complies with standard.

POLICY S-DT-36 seeks to utilize development standards for building bulk, heights, setbacks, landscaping requirements, stepbacks, floor area ratios, open space requirements, and development incentives. Finding: See Section III.A to review dimensional standards.

POLICY S-DT-37 supports linking building intensity to design guidelines relating to building appearance, amenities, pedestrian orientation and connections, impact on adjacent properties, and maintenance of view corridors. These guidelines will seek to enhance the appearance, image, and design character of the Downtown. Finding: See Section III.C above for project attributes.

POLICY S-DT-40 seeks to enhance the appearance of all types of streets and adjoining sidewalks with street trees, landscaping, water features, pedestrian-scaled lighting, street furniture, paving treatments, medians, or other softening treatments as appropriate. Finding: The project complies with this standard. See Sheet L1.01 for details.

POLICY S-DT-57 promotes the creation of pedestrian linkages within and between the Downtown Districts as well as to surrounding residential areas outside Downtown. Finding: The redevelopment of the surrounding sidewalks will provide connections to the Downtown Park through Old Bellevue to Meydenbauer Bay.

POLICY S-DT-87. Provide a graceful pedestrian connection from Downtown Park through Old Bellevue to Meydenbauer Bay. Finding: Complies with standard.

POLICY S-DT-91. Reinforce the unique character of the Old Bellevue area by encouraging residential development, specialty retail, and other development with an emphasis on pedestrian activity. Finding: The applicant has proposed restaurant activities to face NE 1st Avenue and ballroom functions on Level 2 of the north and east elevation to further define the pedestrian oriented frontage along 102nd Avenue Ne.

POLICY S-DT-105. Provide a visual and physical connection from Downtown to Meydenbauer Bay that terminates in a significant waterfront presence. The connection will provide unique recreation, retail, and tourism opportunities. Finding: Redevelopment of the sidewalks in this area will create the opportunity to connect to Meydenbauer Bay.

2. The proposal complies with the applicable requirements of this Code.

As conditioned, the proposal complies with applicable requirements of the Land Use Code and Bellevue City Code with exception of the items detailed below. Additionally, see Section III.A above for further Code discussion.

- More information was required regarding the parking lot layout and loading dock operation. The applicant will need to provide this information prior to Certificate of Occupancy. See Section X.D for related condition.
 - More information was requested regarding building and site lighting. The applicant will need to provide more information regarding actual cut sheets for fixtures for the building and site use. See Section X.C for related condition.
 - More information was requested regarding exhaust noise. The applicant has hired an acoustical engineer who is drafting a report. Review and approval of such report shall occur prior to building permit issuance to ensure proper dba of the proposed equipment. See Section X.C for related condition.
 - More information is necessary regarding proposed ventilation and air vents as they have not been finalized for this project. See Section X.C for related condition.
 - The applicant was requested to provide general information on where project signage will be portrayed per building elevation. See Sections X.C and E for related condition.
 - Encroachment from cornices and marquees over the property line were noted with a subsequent submittal. The metal accent cornice will project 3'-6" over the property line. The applicant was required by the Transportation Department to obtain a Right-of-Way Use permit for lease of air rights. This requires City Council approval under a separate process from this application. Final review and approval will be determined by Council and is not appealable. Additionally, the Director of Land Use must also approve such encroachment per LUC 20.25A.020, subnote 15. This was discussed with the Director who has determined that such encroachment is acceptable. However, if approval is denied by Council, the applicant must revise the north elevation to maintain architectural integrity.
- 3. The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent.**

See Section III.C.2 of this report for discussion relating to how the applicable design guidelines and criteria are met. Additionally, see Section III.C.2, Downtown Old Bellevue and Perimeter design criteria.

- 4. The proposal is compatible with, and responds to, the existing or intended character, appearance, and quality of development and physical characteristics of the subject property and immediate vicinity.**

The project is designed to respond to existing site conditions and existing surrounding development. Internal work spaces along with a small gym facility will be provided for guest use. Finally, the street trees proposed will be consistent with the type, pattern and size required by the Land Use Code. However, a condition of approval is necessary to ensure that required landscaping is installed correctly along with a maintenance assurance device. See Section X.D for related condition.

- 5. The proposal will be served by adequate public facilities including streets, fire protection, and utilities.**

All required public services and facilities are available to the site. The applicant has received an email from Allied Waste dated October 15, 2007, stating that they see no service issues for this

project. The owner will provide for the return of receptacles and trash not removed from the property back into the building the day of pick-up. See Section X.C for related condition.

IX. DECISION

After conducting the various administrative reviews associated with the proposal, including applicable Land Use consistency, SEPA and City Code & Standard compliance reviews, the Director of the Development Services Department does hereby **APPROVE WITH CONDITIONS** the subject proposal.

X. CONDITIONS OF APPROVAL

The following conditions are imposed under the authority referenced:

A. GENERAL CONDITIONS

1. **Compliance with Bellevue City Codes and Ordinances:** The applicant shall comply with all applicable Bellevue City Codes and Ordinances including but not limited to:

Clearing and Grading Code - BCC 23.76	Savina Uzunow	425/452-7860
Bellevue Development Standards	"	
Transportation Code - BCC 14.60	Ray Godinez	425/452-4228
Trans. Improvement Program - BCC.22.16	"	
Right-of-Way Use Permit - BCC 14.30	Jon Regalia	425/425-4599
Bellevue Utilities Code - BCC Title 24	Robert Hutchinson	425/452-7903
Construction Codes - BCC Title 23	Sheri Crawford	425/452-2843
Land Use Code - BCC Title 20	Antoinette Pratt	425/452-5374
Sign Code - BCC Title 22B	"	
Noise Control - BCC 9.18	"	
International Fire Code - BCC 23.11	Travis Ripley	425/452-6042

2. **Conceptual Approval:** Utility Department approval of the design review application (07-143899 LD) is based on the conceptual design only. Changes to the site layout may be required to accommodate the utilities after utility engineering is approved.

Authority: BCC Title 24.02, 24.04, 24.06
Reviewer: Rob Hutchinson

3. **City Code and Standards:** The water, sewer, and storm drainage systems shall be designed per the current City of Bellevue Utility Codes and Utility Engineering Standards. All design review, plan approval, and field inspection shall be performed under the Utility Developer Extension Agreements.

Authority: BCC Title 24.02, 24.04, 24.06
Reviewer: Rob Hutchinson

4. **Construction Hours:** Noise related to construction is allowed from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. Construction noise is prohibited on all Sundays and WA state holidays. Posting of construction hours is

required. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY ONLY be granted pursuant to 9.18.020C.1&2. Prolonged exposure to noise created by extended hour construction activity is likely to have a significant impact on inhabitants of surrounding residential properties during the proposed timeline for construction. In order to minimize detriment on residential uses in the immediate vicinity of the project, the Contractor shall not rely on City issuance of a blanket exemption from the Noise Control Code during the construction period. To vary construction noise hours, the applicant shall apply for a noise permit that must be submitted two weeks prior to the scheduled onset of extended hour construction activity. Such requests may be required to include a noise analysis prepared by a noise consultant, including recommendations for achieving the noise limitations of the Noise Ordinance for new residential construction.

Authority: BCC 9.18.040
Reviewer: Antoinette Pratt

5. **Holiday Construction and Traffic Restrictions:** Construction activities such as hauling and lane closures between November 15th and January 5th will be allowed only between the hours of 10:00 pm and 6:00 am due to holiday traffic. The Transportation Department will be monitoring traffic and may modify this moratorium accordingly.

Authority: BCC 14.30.060
Reviewer: Jon Regalia

6. **Provisions for Loading:** The property owner shall provide an off-street loading space which can access a public street. After construction is complete, on-street loading and unloading will not be permitted, except for vehicles which can legally use available on-street parking spaces.

Authority: LUC 20.20.590.K.4
Reviewer: Ray Godinez

- B. **The following conditions are imposed to ensure compliance with the relevant decision criteria and Code requirements and to mitigate adverse environmental impacts not addressed through applicable Code provisions. These conditions must be complied with on plans submitted with the Clearing & Grading or Demolition permit application:**

1. **Right-of-Way-Use Permit:** Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

- a) Designated truck hauling routes.
- b) Truck loading/unloading activities.
- c) Location of construction fences.

- d) Hours of construction and hauling.
- e) Requirements for leasing of right of way or pedestrian easements.
- f) Provisions for street sweeping, excavation and construction.
- g) Location of construction signing and pedestrian detour routes.
- h) All other construction activities as they affect the public street system.

In addition, the applicant shall submit for review and approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevents access. General materials storage and contractor convenience are not reasons for preventing access.

The applicant shall secure sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.

Authority: BCC 11.70 & 14.30

Reviewer: Jon Regalia

2. **Civil Engineering Plans - Transportation:** Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all street frontage improvements and driveway accesses must be in conformance with the requirements of the Americans with Disabilities Act, the Transportation Development Code, the provisions of the Transportation Department Design Manual, and specific requirements stated elsewhere in this document. All relevant standard drawings from the Transportation Department Design Manual shall be copied exactly into the final engineering plans. Specific requirements for the engineering plans include, but are not limited to:

- a) Traffic signs and markings, if any.
- b) Curb, gutter, sidewalk, and driveway approach design. (The final engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as needed.)
- c) Handicapped ramps, crosswalk revisions, and crosswalk equipment such as pushbuttons and pedestrian signal heads.
- d) Installation or relocation of streetlights and related equipment.
- e) Sight distance. (Show the required sight triangles and include any sight obstructions, including those off-site.)
- f) Location of fixed objects in the sidewalk or near the driveway approach.
- g) Trench and pavement restoration within any right of way or access easement.

Authority: BCC 14.60; Transportation Department Design Manual

Reviewer: Ray Godinez

C. The following conditions are required by City Code. These conditions must be complied with prior to Building Permit Issuance:

1. **Transportation Impact Fee:** Payment of the transportation impact fee will be required at the time of building permit issuance for the primary structure. The net impact fee will be \$82,452.50 with credit for removal of the existing miscellaneous retail square footage. This fee is subject to change on January 1, 2013 and the fee schedule in effect at the time of building permit issuance will apply.

Authority: BCC 22.16
Reviewer: Ray Godinez

2. **Building and Site Plan – Transportation:** The building grade and elevations shall be consistent with the curb and sidewalk grade shown in the approved civil engineering plans. During construction, city inspectors may require additional survey work at any time in order to confirm proper elevations. Building plans, landscaping plans, and architectural site plans must accommodate on-site traffic markings or signs and driveway design as specified in the engineering plans. Any gate across the driveway or parking ticket station in the driveway must be located so that stopped vehicles do not block the sidewalk. Building plans, landscaping plans, and architectural site plans must comply with vehicle and pedestrian sight distance requirements, as shown on the engineering plans.

Authority: BCC 14.60.060, 110, 120, 150, 180, 181, 190, 200, 210, 240, 241
Reviewer: Ray Godinez

3. **Existing Utility Easements:** The developer shall identify existing utility easements on the site. Any negative impact that this development has on those easements must be mitigated or easements relinquished.

Authority: BCC 14.60.100
Reviewer: Jon Regalia

4. **Lease of Air Space, City Council Approval:** Building fixtures that the developer has proposed to extend over the existing city right of way on NE 1st Street will not be allowed unless the developer obtains a long-term lease of air space to accommodate such fixtures, prior to issuance of any building permit for this development. City Council approval is required for any long-term lease. If such approval is not obtained, then the building design must be revised prior to building permit issuance to eliminate any use of the city's air space while maintaining the architectural integrity of the north elevation. Granting a lease of city property rights, including air space, requires compensation for fair market value from the lessee. Some or all of such compensation may be provided by the property owner allowing the city use of and access to part of the building structure to mount a city traffic camera. Details of the proposed air space lease and any rights granted to the city in exchange will be established via a separate legal document.

Authority: BCC 14.30, 14.60.100, LUC 20.25A.090.E.3 and 4
Reviewer: Ray Godinez and Antoinette Pratt

5. **Sidewalk/Utility Easements:** The applicant shall provide sidewalk and utility easements to the City, using a city-approved form, such that the required width of public sidewalks outside of the City right of way along the property frontages is located within a sidewalk/utility easement area.

Authority: BCC 14.60.100
Reviewer: Ray Godinez

6. **General Signage Information:** General signage information noted on the building will be required prior to building permit issuance.

Authority: LUC 20.25A.110, LUC 20.30F, BCC 22B
Reviewer: Antoinette Pratt

7. **Solid Waste/Recycling:** The owner shall designate staff to take out and provide for the return of receptacles and trash established with Allied Waste at a defined time. All rights of way and public easements shall not be occupied by trash receptacles, dumpsters, recycling bins or other such items. Additionally, the applicant must show approval of area by Allied Waste.

Authority: LUC 20.20.720
Reviewer: Antoinette Pratt

8. **Residential Noise Levels:** Residential noise levels shall not exceed the standard below:

a. Noise Levels: Noise levels shall not to exceed 40 dBA in sleeping areas and 45 dBA in non-sleeping areas. The applicant shall measure the noise levels in a random sample of the rooms and submit the findings to the City. If the maximum threshold is exceeded, additional noise mitigation will be required to meet the threshold prior to the issuance of any Occupancy Permit.

Authority: BCC 9.18
Reviewer: Antoinette Pratt

b. Garage Exhaust: The applicant shall provide certification from the mechanical engineer that the air flow from the garage exhaust fans will not exceed 60 dBA.

Authority: BCC 9.18.030
Reviewer: Antoinette Pratt

9. **Exterior and Retail Lighting:** The applicant shall submit a detailed design for all building exterior lighting. Lighting shall be contained to this site and not be permitted to spillover to adjacent sites. In addition to this, information on retail lighting along the pedestrian oriented frontage (102nd Avenue NE) shall be supplied as well to determine appropriate use for façade.

Authority: LUC 20.20.522, LUC 20.25A.110.5.a
Reviewer: Antoinette Pratt

10. **Mechanical Equipment/Ventilation:** The current maximum mechanical roof height is 114 feet. No changes shall be permitted to the shown roof details for this project without consent of staff during building permit review. Additional height is based on the details provided herein. Modification of the building design may place the additional height gained at risk for reduction to normal maximum building height of 105 feet (includes mechanical). Additionally, all venting shall be designed so that it does not detract from the architectural integrity of the building and submitted for staff review prior to building permit issuance.

Authority: LUC 20.20.525
Reviewer: Antoinette Pratt

- D. **The following conditions are required by City Code and supported by City Policy. The conditions shall be complied with prior to issuance of the Temporary Certificate of Occupancy (TCO):**

1. **Parking Lot Striping/Operable Loading Bay:** All parking stalls shall be striped and the loading bay shall be operable prior to Temporary Certificate of Occupancy.

Authority: LUC 20.20.590.K
Reviewer: Antoinette Pratt

2. **Bonus System Recording:** The applicant shall record a copy of the approved bonus point calculations, a statement that 100% of the project frontage shall remain pedestrian-oriented retail, and the project drawings & conditions of this Design Review with the King County Records office.

Authority: LUC 20.25A.020.D.3
Reviewer: Antoinette Pratt

3. **Landscape Installation Assurance Device:** If a Temporary Certificate of Occupancy is requested prior to completion of the landscaping installation the applicant shall file with DSD a landscape installation assurance device equal to 150% of the cost of labor and materials for any landscaping that has not yet been installed.

Authority: LUC 20.40.490
Reviewer: Antoinette Pratt

4. **Landscape Maintenance Device:** The applicant shall file with the Development Services Department a landscape maintenance assurance device for a one-year period in the form of an assignment of savings or letter of credit for 20% of the cost of labor and materials for all required landscaping.

Authority: LUC 20.40.490
Reviewer: Antoinette Pratt

5. Street Frontage Improvements: All street frontage improvements and other required transportation elements, including landscaping and streetlight and traffic signal revisions, must be constructed by the applicant and accepted by the City Inspector. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible. Bonding or other types of assurance devices will not be accepted in lieu of construction. Specific requirements include the following:

- a) Curb, gutter, sidewalk, ramps, crosswalks, and driveway approaches shall be constructed per the approved engineering plans. ADA compliance must be achieved. Any non-standard materials or features in the public sidewalk must be acceptable to the City.
- b) Driveway approaches shall not exceed a slope of 7% for a distance of 30 feet behind the sidewalk on either street. Any grade changes in the driveways must be designed to prevent vehicles from bottoming out. Any gate or parking ticket station in the driveway shall be located so that stopped vehicles do not block the sidewalk.
- c) Traffic signs and markings, if any, shall be installed per the approved engineering plans, with possible modifications per Transportation Department inspectors.
- d) Vehicle and pedestrian sight triangles shall be achieved per BCC 14.60.240 and 14.60.241, including consideration of all fixed objects and mature landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance.
- e) Streetlights, pedestrian signals, and related equipment shall be installed per the approved streetlight and signal plan and specifications. ADA compliance must be achieved.
- f) Location of fixed objects within any right of way or near the driveway approaches shall meet Transportation Department standards per the Transportation Department Design Manual and as described elsewhere in this Design Review Staff Report.
- g) Landscaping and irrigation within any city right of way or sidewalk easement shall be installed per the approved landscaping plans.

Authority: BCC 14.60.060, 100, 110, 120, 150, 181, 190, 200, 210, 240, 241;
Transportation Department Design Manual; and Transportation Department Design
Manual Standard Drawings.

Reviewer: Ray Godinez

6. Trench and Pavement Restoration: Pavement and trench restoration associated with street frontage improvements or to repair damaged street surfaces shall be provided as follows:

- a) NE 1st Street: This street requires standard trench restoration, per the Transportation Design Manual.
- b) 102nd Avenue NE: Based on this street's excellent condition, it is classified in the City's overlay program as "Overlay Required". Street cutting is permitted only with a grind and overlay at least 50 feet long for the full width of any affected lane.

Pavement conditions change over time. The exact extent of pavement restoration will be specified in the right-of-way permit for this project, based on field conditions at the time of construction.

Authority: BCC 14.60. 250; Design Manual Design Standard #21
Reviewer: Jon Regalia

7. **Fire Department:** The Fire Department has identified a number of conditions that must be met prior to issuance of building permits. See Attachment C for related conditions.

E. PRIOR TO ISSUANCE OF SIGN PERMITS

1. **Signs:** All signage shall be pedestrian oriented. A comprehensive sign package shall be submitted to DSD for review and approval prior to issuance of any sign permits. The sign package plans, elevations, and other sketches shall include but are not limited to:
 - A. Location
 - B. Lighting
 - C. Color Palate
 - D. Material
 - E. Design (No cabinet and freestanding signs are permitted)

Signage shall be the minimum size necessary to convey information and shall be architecturally compatible and integrated with the building. See Sign Code limitations or other design criteria. Design review of individual signs and compliance with the approved sign package will occur through review of each sign permit application.

Authority: LUC 20.25A.110, LUC 20.30F, BCC 22B.10.025.B.4
Reviewer: Antoinette Pratt

ATTACHMENTS

- A. Project Plans and Drawing
- B. Concurrency Documentation
- C. Fire Review Comments

ATTACHMENT A
(Plans and Drawings)

SPECIAL EXCEPTIONS

- RIGHT TO MAKE NECESSARY SLOPES FOR CUTS OR FILLS UPON PROPERTY HEREIN DESCRIBED AS GRANTED IN DEED:
GRANTEE: KING COUNTY
RECORDED: MAY 29, 1941
RECORDING NUMBER: 3168360
- AGREEMENT AND THE TERMS AND CONDITIONS THEREOF:
BETWEEN: OWNERS
AND: CITY OF BELLEVUE
RECORDED: DECEMBER 16, 1981
RECORDING NUMBER: 19812160552
REGARDING: CONDITIONS TO ISSUANCE OF BUILDING PERMIT
- TERMS AND CONDITIONS OF NOTICE OF CHARGES BY WATER, SEWER, AND/OR STORM AND SURFACE WATER UTILITIES, RECORDED UNDER RECORDING NUMBER 9612200938.
- MATTERS DISCLOSED BY SURVEY RECORDED UNDER RECORDING NUMBER 9903299001, IN VOLUME 128 OF SURVEYS, PAGE(S) 224, AS FOLLOWS:
INTEREST OF SOUTHERLY ADJOINING OWNERS, AS EVIDENCED BY WOOD GUARD RAIL ONTO PROPERTY
- PAYMENT OF THE REAL ESTATE EXCISE TAX, IF REQUIRED.
THE PROPERTY DESCRIBED HEREIN IS SITUATED WITHIN THE BOUNDARIES OF LOCAL TAXING AUTHORITY OF 0330 BELLEVUE. ANY CONVEYANCE DOCUMENT MUST BE ACCOMPANIED BY THE OFFICIAL WASHINGTON STATE EXCISE TAX AFFIDAVIT. THE APPLICABLE EXCISE TAX MUST BE PAID AND THE AFFIDAVIT APPROVED AT THE TIME OF THE RECORDING OF THE CONVEYANCE DOCUMENTS.
- GENERAL AND SPECIAL TAXES AND CHARGES, PAYABLE FEBRUARY 15, DELINQUENT IF FIRST HALF UNPAID ON MAY 1, SECOND HALF DELINQUENT IF UNPAID ON NOVEMBER 1 OF THE TAX YEAR (AMOUNTS DO NOT INCLUDE INTEREST AND PENALTIES):
YEAR: 2007
TAX ACCOUNT NUMBER: 154510-0128-07
LEVY CODE: 0330 BELLEVUE
ASSESSED VALUE-LAND: \$ 1,238,600
ASSESSED VALUE-IMPROVEMENTS: \$ 168,600
GENERAL & SPECIAL TAXES:
BILLED: \$ 10,900.49
PAID: \$ 5,450.25
UNPAID: \$ 5,450.24
- ASSESSMENT:
AMOUNT: \$ 2,112.63
INTEREST: 7.59%
COMMENCING 30 DAYS AFTER: NOVEMBER 17, 1989
ANNUAL INSTALLMENTS: 20
INSTALLMENTS PAID: 17
INSTALLMENTS DELINQUENT: 0
NEXT INSTALLMENT DUE: DECEMBER 17, 2007
LEVIED BY: CITY OF BELLEVUE
FOR: STREET IMPROVEMENTS
L.I.D. NUMBER: 277
ACCOUNT NUMBER: 654
- DEED OF TRUST AND THE TERMS AND CONDITIONS THEREOF:
GRANTOR: CENTENNIAL ASSOCIATES, A WASHINGTON LIMITED PARTNERSHIP
TRUSTEE: STEWART TITLE COMPANY OF WASHINGTON, INC.
BENEFICIARY: PACIFIC FIRST FEDERAL SAVINGS BANK, A FEDERALLY CHARTERED STOCK SAVINGS BANK
AMOUNT: \$ 600,000.00
DATED: MARCH 26, 1987
RECORDED: MARCH 26, 1987
RECORDING NUMBER: 8703261086
- LOAN NUMBER: 452600-0
THE AMOUNT NOW SECURED BY SAID DEED OF TRUST AND THE TERMS UPON WHICH THE SAME CAN BE DISCHARGED OR ASSUMED SHOULD BE ASCERTAINED FROM THE HOLDER OF THE INDEBTEDNESS SECURED.
P MODIFICATION OF DEED OF TRUST AND THE TERMS AND CONDITIONS THEREOF:
DATED: 1ST DAY OF APRIL, 1997
RECORDED: APRIL 11, 1997
RECORDING NUMBER: 970411055
- COLLATERAL ASSIGNMENT OF RENTS AND/OR LEASES AND THE TERMS AND CONDITIONS THEREOF:
ASSIGNOR: CENTENNIAL ASSOCIATES, A WASHINGTON LIMITED PARTNERSHIP
ASSIGNEE: PACIFIC FIRST FEDERAL SAVINGS BANK, A FEDERALLY CHARTERED STOCK SAVINGS BANK
DATED: MARCH 26, 1987
RECORDING DATE: MARCH 26, 1987
RECORDING NUMBER: 8703261087
- FINANCING STATEMENT AND THE TERMS AND CONDITIONS THEREOF:
SECURED PARTY: PACIFIC FIRST FEDERAL SAVINGS BANK
DEBTOR: CENTENNIAL ASSOCIATES, A WASHINGTON LIMITED PARTNERSHIP C/O PACIFIC GROUP PROPERTIES
COVERS: PERSONAL PROPERTY AND FIXTURES LOCATED ON PROPERTY HEREIN DESCRIBED
RECORDED: MARCH 26, 1987
RECORDING NUMBER: 8703261088
A CONTINUATION OF SAID FINANCING STATEMENT HAS BEEN RECORDED UNDER RECORDING NUMBER(S): 9606030594.
- UNRECORDED LEASE, INCLUDING THE TERMS AND CONDITIONS THEREOF:
LESSOR: CENTENNIAL ASSOCIATES, A WASHINGTON LIMITED PARTNERSHIP
LESSEE(S): GALAXY TRAVEL, INC.
DISCLOSED BY: RECORDING NO. 8703261090
- SUBORDINATION, NONDISTURBANCE AND ATTORNEY AGREEMENT, AND THE TERMS AND CONDITIONS THEREOF:
LENDER: PACIFIC FIRST FEDERAL SAVINGS BANK, A FEDERALLY CHARTERED STOCK SAVINGS BANK
TENANT: GALAXY TRAVEL, INC.
LANDLORD: CENTENNIAL ASSOCIATES, A WASHINGTON LIMITED PARTNERSHIP
RECORDED: MARCH 26, 1987
RECORDING NUMBER: 8703261090
- UNRECORDED LEASE, INCLUDING THE TERMS AND CONDITIONS THEREOF:
LESSOR: CENTENNIAL ASSOCIATES, A WASHINGTON LIMITED PARTNERSHIP
LESSEE(S): MCDONOUGH'S JEWELRY, INC.
DISCLOSED BY: RECORDING NO. 8703261091 AND KING COUNTY SUPERIOR COURT CAUSE NO. 87-2-05319-9

SPECIAL EXCEPTIONS-CONTINUED

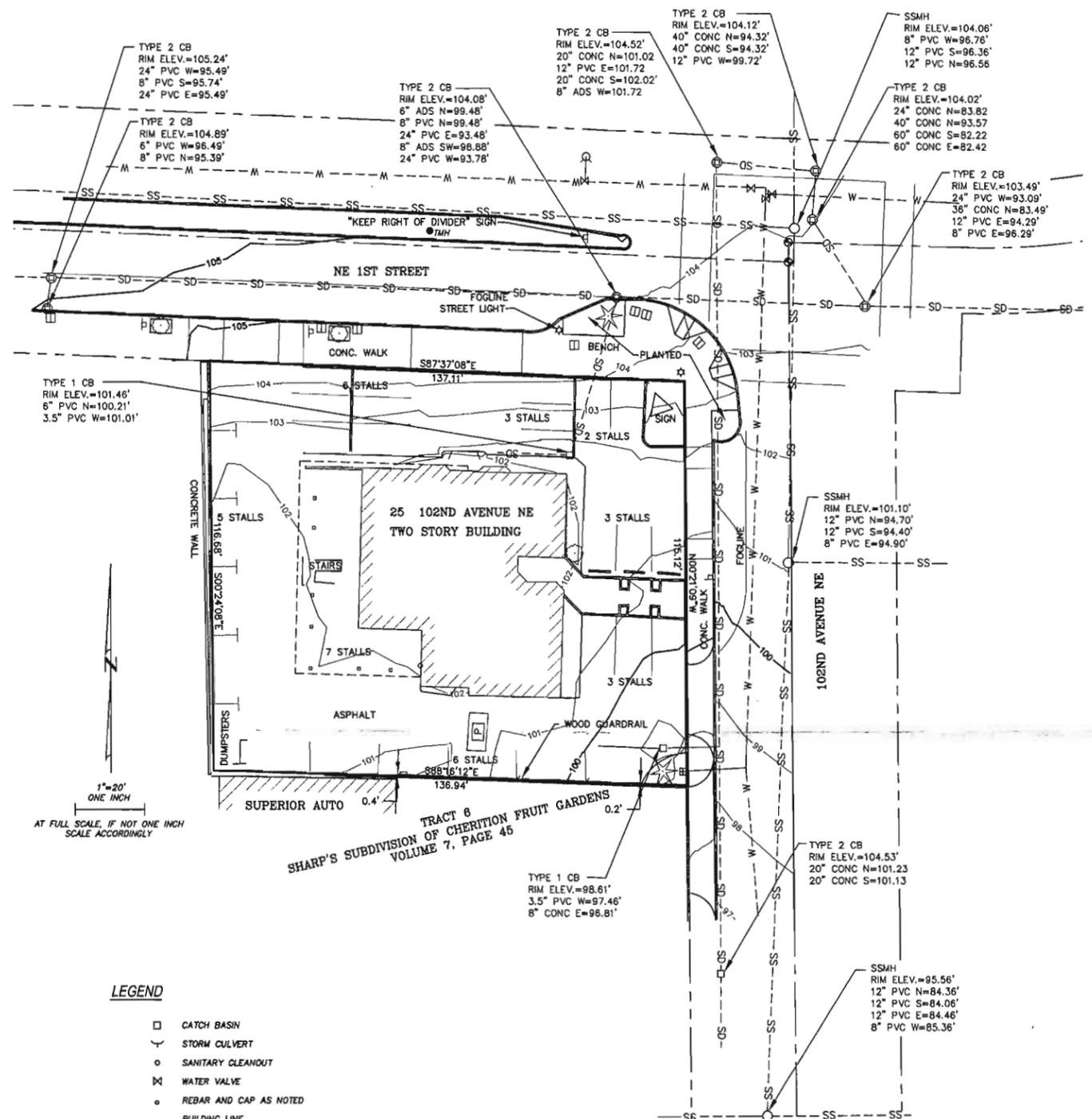
- SUBORDINATION, NONDISTURBANCE AND ATTORNEY AGREEMENT, AND THE TERMS AND CONDITIONS THEREOF:
LENDER: PACIFIC FIRST FEDERAL SAVINGS BANK
TENANT: MCDONOUGH'S JEWELRY, INC.
LANDLORD: CENTENNIAL ASSOCIATES, A WASHINGTON LIMITED PARTNERSHIP
RECORDED: MARCH 26, 1987
RECORDING NUMBER: 8703261091
- UNRECORDED LEASEHOLDS, IF ANY, RIGHTS OF VENDORS AND HOLDERS OF SECURITY INTERESTS ON PERSONAL PROPERTY INSTALLED UPON SAID PROPERTY AND RIGHTS OF TENANTS TO REMOVE TRADE FIXTURES AT THE EXPIRATION OF THE TERM.
- TERMS AND CONDITIONS OF THE PARTNERSHIP AGREEMENT FOR CENTENNIAL ASSOCIATES.
NOTE: A COPY OF THE PARTNERSHIP AGREEMENT, AND AMENDMENTS THERETO, IF ANY, MUST BE SUBMITTED.
- ANY CONVEYANCE OR MORTGAGE BY CENTENNIAL ASSOCIATES, MUST BE EXECUTED IN ACCORDANCE WITH THE PARTNERSHIP AGREEMENT AND BY ALL THE GENERAL PARTNERS AND THEIR RESPECTIVE SPOUSES AS OF THE DATE OF ACQUISITION, OR EVIDENCE MUST BE SUBMITTED THAT CERTAIN DESIGNATED PARTNERS HAVE BEEN AUTHORIZED TO ACT FOR THE PARTNERSHIP.
- MATTERS OF RECORD, IF ANY, AGAINST THE NAMES OF THE UNDISCLOSED GENERAL PARTNERS OF CENTENNIAL ASSOCIATES.
- THE LEGAL DESCRIPTION IN THIS COMMITMENT IS BASED ON INFORMATION PROVIDED WITH THE APPLICATION AND THE PUBLIC RECORDS AS DEFINED IN THE POLICY TO ISSUE. THE PARTIES TO THE FORTHCOMING TRANSACTION MUST NOTIFY THE TITLE INSURANCE COMPANY PRIOR TO CLOSING IF THE DESCRIPTION DOES NOT CONFORM TO THEIR EXPECTATIONS.
- TERMS AND CONDITIONS OF THE LIMITED LIABILITY COMPANY AGREEMENT FOR FIRST BELLEVUE PROPERTIES, LLC.
NOTE: A COPY OF THE LIMITED LIABILITY COMPANY AGREEMENT, AND AMENDMENTS THERETO, IF ANY, MUST BE SUBMITTED.
- ANY CONVEYANCE OR MORTGAGE BY FIRST BELLEVUE PROPERTIES, LLC, MUST BE EXECUTED IN ACCORDANCE WITH THE LIMITED LIABILITY COMPANY AGREEMENT AND BY ALL THE MEMBERS, OR EVIDENCE MUST BE SUBMITTED THAT CERTAIN DESIGNATED MANAGERS/MEMBERS HAVE BEEN AUTHORIZED TO ACT FOR THE LIMITED LIABILITY COMPANY.
- PRIOR TO ISSUING AN EXTENDED FORM OWNER'S POLICY THIS COMPANY REQUIRES THAT THE PARTIES TO THE TRANSACTION PROVIDE AN ALTA/ACSM CERTIFIED, AS-BUILT SURVEY.
- TO PROVIDE AN EXTENDED COVERAGE OWNER'S AND LENDER'S POLICY GENERAL EXCEPTIONS A THROUGH D WILL BE CONSIDERED WHEN OUR INSPECTION AND/OR REVIEW OF SURVEY, IF REQUIRED, IS COMPLETED. A SUPPLEMENTAL COMMITMENT WILL FOLLOW.

LEGAL DESCRIPTION CHICAGO TITLE INSURANCE COMPANY A.L.T.A. COMMITMENT Order No.: 001238131

TRACT 6 IN SHARP'S SUBDIVISION OF CHERITON FRUIT GARDENS, ACCORDING TO THE PLAT THEREOF, RECORDED IN VOLUME 7 OF PLATS, PAGE 45, IN KING COUNTY, WASHINGTON;
EXCEPT THE WEST 466 FEET;
AND EXCEPT THE SOUTH 200 FEET;
AND EXCEPT THAT PORTION CONVEYED TO KING COUNTY FOR NORTHEAST 1ST STREET BY DEED RECORDED UNDER RECORDING NUMBER 3168360;
AND EXCEPT THAT PORTION CONVEYED TO KING COUNTY FOR 102ND AVENUE NORTHEAST BY DEED RECORDED UNDER RECORDING NUMBER 1617971.

NOTES

- THIS SURVEY COMPLIES WITH ALL STANDARDS AND GUIDELINES OF THE SURVEY RECORDING ACT AS PER CHAPTER 58.09 RCW AND CHAPTER 332-130 WAC.
- ALL MONUMENTS VISITED DURING MAY 2007 UNLESS OTHERWISE NOTED.
- EQUIPMENT: THE PRIMARY MEASUREMENT EQUIPMENT UTILIZED IN THE PERFORMANCE OF THIS SURVEY WAS A LEICA TCRA 1101 TOTAL STATION, SN# 624750.
- ALL PRIMARY MEASUREMENT EQUIPMENT UTILIZED HAS BEEN COMPARED TO AND ADJUSTED AGAINST A NATIONAL GEODETIC SURVEY CALIBRATED BASELINE WITHIN THE PAST ONE YEAR.
- BASIS OF BEARING: WASHINGTON STATE PLANE COORDINATE SYSTEM, NORTH ZONE, NAD 83(91)
- VERTICAL DATUM: NAVD 88
- SPECIAL EXCEPTION 4 IS THE ONLY PLOTTABLE REFERENCE. GUARDRAIL ENCROACHMENT AS MEASURED THIS SURVEY.

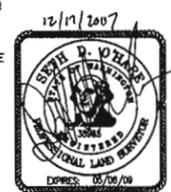


- LEGEND**
- CATCH BASIN
 - Y STORM CULVERT
 - SANITARY CLEANOUT
 - ⊗ WATER VALVE
 - REBAR AND CAP AS NOTED
 - ▨ BUILDING LINE
 - ROAD R.O.W. LINE
 - DITCH CENTERLINE
 - PROPERTY LINE
 - R.O.W. CENTERLINE
 - X- FENCE LINE

TO FIRST BELLEVUE PROPERTIES, LLC, A WASHINGTON LIMITED LIABILITY COMPANY, AND CHICAGO TITLE INSURANCE COMPANY:

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE "MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS," JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS IN 2005, AND INCLUDES ITEMS 1, 2, 4, 5, 7A, 7C, 8, 9, 10, 11A, 11B, AND 14 OF TABLE A THEREOF. PURSUANT TO THE ACCURACY STANDARDS AS ADOPTED BY ALTA AND NSPS AND IN EFFECT ON THE DATE OF THIS CERTIFICATION, UNDERSIGNED FURTHER CERTIFIES THAT IN MY PROFESSIONAL OPINION, AS A LAND SURVEYOR REGISTERED IN THE STATE OF WASHINGTON, THE RELATIVE POSITIONAL ACCURACY OF THIS SURVEY DOES NOT EXCEED THAT WHICH IS SPECIFIED THEREIN.

DATE: 6/8/2007
SETH D. O'HARE, PLS
REGISTRATION NO. 38985



SY#	REVISION	BY	APPROVED	DATE	SY#	REVISION	BY	APPROVED	DATE

DUANE HARTMAN & ASSOCIATES, INC.
Surveyors

16928 WOODINVILLE-REDMOND ROAD, B-107 (425) 483-5355
WOODINVILLE, WASHINGTON 98072 FAX (425) 483-4650

ACCEPTABILITY
THIS DESIGN AND/OR SPECIFICATION IS APPROVED

APPROVED BY: _____ DEPT.: _____ DATE: _____

CHECKED BY: _____ ENGR.: _____ DATE: 05/29/07

CHECKED BY: _____ ENGR.: _____ DATE: 05/29/07

CHECKED BY: _____ ENGR.: _____ DATE: _____

CHECKED BY: _____ ENGR.: _____ DATE: _____

ALTA SURVEY

25 102nd AVENUE NE

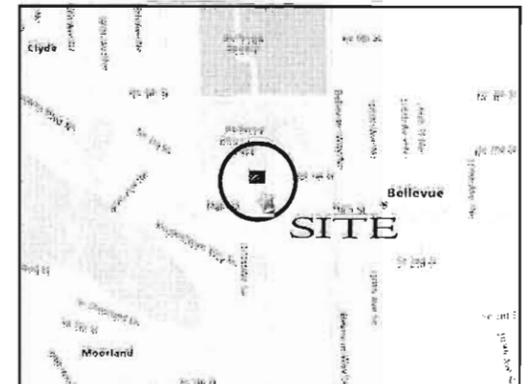
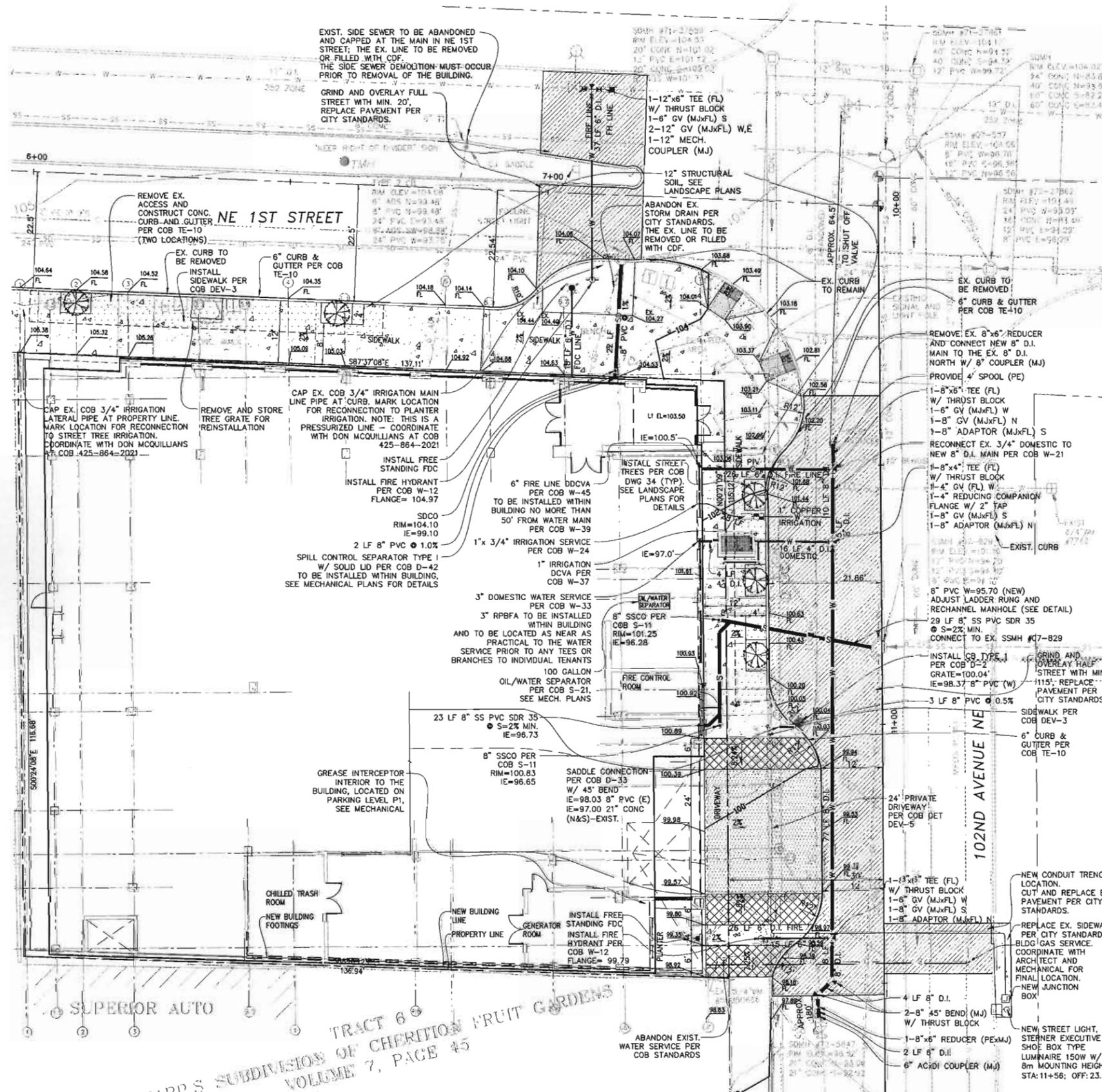
BELLEVUE WASHINGTON

LAST REVISION: 1 OF 1

SHEET: 1 OF 1

JOB NO.: 07-1170

DWG. NO.: 07-1170ALTA.DWG



VICINITY MAP

NOT TO SCALE

DEVELOPER

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CIVIL/STRUCTURAL ENGINEER

DCI ENGINEERS
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SURVEYOR

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WOODVILLE, WA 98072
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FAX (425) 483-4650

VERTICAL DATUM - BASIS OF ELEVATION

NORTH AMERICAN VERTICAL DATUM OF 1988 AS ADOPTED BY THE CITY OF BELLEVUE NAVD 88.

BASIS OF BEARINGS

WASHINGTON STATE PLANE COORDINATE SYSTEM, NORTH ZONE, NAD 83(91)

LEGAL DESCRIPTION

LEGAL DESCRIPTION CHICAGO TITLE INSURANCE COMPANY A.L.T.A. COMMITMENT ORDER NO.: 001238131

TRACT 6 IN SHARP'S SUBDIVISION OF CHERITON FRUIT GARDENS, ACCORDING TO THE PLAT THEREOF, RECORDED IN VOLUME 7 OF PLATS, PAGE 45, IN KING COUNTY, WASHINGTON; EXCEPT THE WEST 486 FEET; AND EXCEPT THE SOUTH 200 FEET; AND EXCEPT THAT PORTION CONVEYED TO KING COUNTY FOR NORTHEAST 1ST STREET BY DEED RECORDED UNDER RECORDING NUMBER 3168360; AND EXCEPT THAT PORTION CONVEYED TO KING COUNTY FOR 102ND AVENUE NORTHEAST BY DEED RECORDED UNDER RECORDING NUMBER 1617971.

CONTRACTOR NOTE

ALL EXISTING UTILITIES SHOWN ON PLANS ARE TO BE VERIFIED HORIZONTALLY AND VERTICALLY PRIOR TO ANY CONSTRUCTION. ALL EXISTING FEATURES INCLUDING BURIED UTILITIES ARE SHOWN AS INDICATED ON RECORD MAPS AND SURVEY FURNISHED BY OTHERS. WE ASSUME NO LIABILITY FOR THE ACCURACY OF THOSE RECORDS AND SURVEY. FOR THE FINAL LOCATION OF EXISTING UTILITIES IN AREAS CRITICAL TO CONSTRUCTION CONTACT THE UTILITY OWNER/AGENCY.

STATISTICAL INFORMATION

NO.	DESCRIPTION	REQUIRED/ALLOWED	PROPOSED
1	LAND USE ZONE		DVNH-CB
2	SITE AREA		15,483 SF (0.36 ACRES)
3	SITE DATA SUMMARY:		
A	NUMBER OF DWELLING UNITS/ACRE	N/A	N/A
B	TOTAL NUMBER OF DWELLING UNITS	N/A	110 HOTEL ROOMS
C	AREA OF EACH PROPOSED STRUCTURE		
	NET (EXCLUDES VERTICAL SHAFTS, COMMON AREAS)	N/A	105,641 NTSF
	NET LEASABLE (FOR SHOPPING CENTERS)	N/A	N/A
	GROSS (EXCLUDES VERTICAL SHAFTS)	N/A	74,758 GSF (HEATED), 40,637 (GARAGE)
D	FLOOR AREA RATIO (PHASE I AND II)	2.0 BASE (5.0 MAX)	4.68 (72,686 SF)
E	AREA OF PROPOSED BUILDING BY USE		
	GARAGE		40,637 NTSF
	RETAIL		N/A
	RESTAURANT		2,972 NTSF
	BANQUET		4,499 NTSF
	BACK OF HOUSE		2,197 NTSF
	KITCHEN/PANTRY		3,046 NTSF
	RESIDENTIAL		42,962 NTSF
	TOTAL NET		95,513 NTSF
	TOTAL GROSS		115,395 GSF
4	PERCENTAGE OF LOT COVERAGE (15,483 SQ. FT.)	100%	100%
5	SITE DISTURBANCE AREA IN SQUARE FEET	15,483 SF	15,483 SF
6	CUT/FILL (CUBIC YARDS)	N/A	16,900 CY / 0 CY
7	BUILDING HEIGHT: MEASURED FROM AVG. EXISTING GRADE IN SHORELINE & TRANSITION AREAS; MEASURED FROM AVG. FINISHED GRADE FOR ALL OTHER AREAS	99' ± 15'	99' ± 15'
8	PARKING TOTAL # OF SPACES FOR THE PROJECT	N/A	112 ON-SITE, 50 OFF-SITE
A	# OF SPACES BY EACH PROPOSED USE		
	HOTEL	N/A	36 14'x14' 44' MAX.
	RESTAURANT	N/A	14 14'x23' MAX.
	BANQUET	N/A	14 14'x23' MAX.
B	PERCENTAGE OF COMPACT	65% MAX	52% (60 STALLS)
C	ACCESSIBLE PARKING STALLS	5	5 (1 VAN)
9	AREA OF PROPOSED LANDSCAPING		
A	ADJACENT TO RIGHT-OF-WAY	4' PLANTING STRIP FOR STREET TREES	4' PLANTING STRIP WITH STREET TREES 25' ON CENTER
B	ADJACENT TO INTERIOR PROPERTY LINES	N/A	N/A
C	WITHIN THE PARKING AREA	N/A	N/A
D	SIGNIFICANT TREES TO BE RETAINED	N/A	N/A



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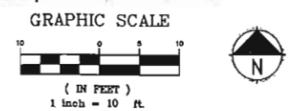


AREAS

TOTAL SITE AREA	15,483 SF
ON SITE AREAS	
DISTURBED	15,483 SF
IMPERVIOUS	15,483 SF

OFF SITE AREAS (PUBLIC R.O.W.)

DISTURBED AREA	3,600 SF
IMPERVIOUS	3,290 SF



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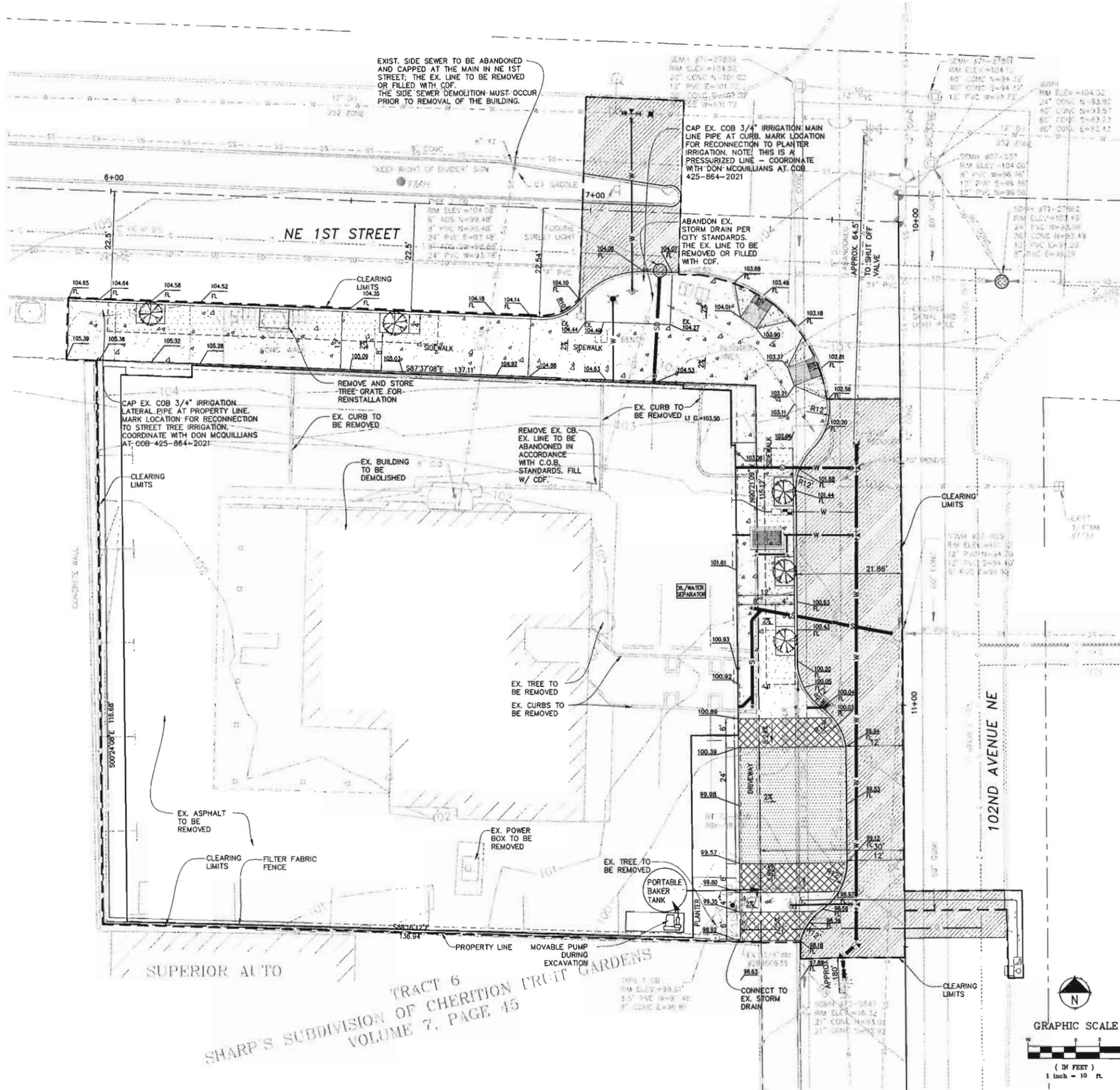
Revisions Date

Design Review

Date: 12.24.2009
Job #: 07-12-039
COB Project #:
Drawing: NKK
Approval: DAS

CIVIL SITE PLAN "B"

DR



CLEARING AND GRADING STANDARD NOTES

- ALL CLEARING & GRADING CONSTRUCTION MUST BE IN ACCORDANCE WITH CITY OF BELLEVUE (COB) CLEARING & GRADING CODE; CLEARING & GRADING EROSION CONTROL STANDARD DETAILS (EC-1 THROUGH EC-23); DEVELOPMENT STANDARDS; LAND USE CODE; UNIFORM BUILDING CODE; PERMIT CONDITIONS; AND ALL OTHER APPLICABLE CODES, ORDINANCES, AND STANDARDS. THE DESIGN ELEMENTS WITHIN THESE PLANS HAVE BEEN REVIEWED ACCORDING TO THESE REQUIREMENTS. ANY VARIANCE FROM ADOPTED EROSION CONTROL STANDARDS IS NOT ALLOWED UNLESS SPECIFICALLY APPROVED BY THE CITY OF BELLEVUE DEPARTMENT OF PLANNING & COMMUNITY DEVELOPMENT (PCD) PRIOR TO CONSTRUCTION.
IT IS THE SOLE RESPONSIBILITY OF THE APPLICANT AND THE PROFESSIONAL CIVIL ENGINEER TO CORRECT ANY ERROR, OMISSION, OR VARIATION FROM THE ABOVE REQUIREMENTS FOUND IN THESE PLANS. ALL CORRECTIONS WILL BE AT NO ADDITIONAL COST OR LIABILITY TO THE CONTRACTOR. DETAILS FOR STRUCTURAL WALLS, ROCKERIES OVER FOUR FEET IN HEIGHT, GEGRID REINFORCED ROCKERIES, AND GEGRID REINFORCED MODULAR BLOCK WALLS MUST BE STAMPED BY A PROFESSIONAL ENGINEER.
- A COPY OF THE APPROVED PLANS MUST BE ON-SITE DURING CONSTRUCTION. THE APPLICANT IS RESPONSIBLE FOR OBTAINING ANY OTHER REQUIRED OR RELATED PERMITS PRIOR TO BEGINNING CONSTRUCTION.
- ALL LOCATIONS OF EXISTING UTILITIES HAVE BEEN ESTABLISHED BY FIELD SURVEY OR OBTAINED FROM AVAILABLE RECORDS AND SHOULD, THEREFORE, BE CONSIDERED ONLY APPROXIMATE AND NOT NECESSARILY COMPLETE. IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INDEPENDENTLY VERIFY THE ACCURACY OF ALL UTILITY LOCATIONS AND TO DISCOVER AND AVOID ANY OTHER UTILITIES NOT SHOWN WHICH MAY BE AFFECTED BY THE IMPLEMENTATION OF THIS PLAN.
- THE AREA TO BE CLEARED AND GRADED MUST BE FLAGGED BY THE CONTRACTOR AND APPROVED BY THE CLEARING & GRADING INSPECTOR PRIOR TO BEGINNING ANY WORK ON THE SITE.
- A REINFORCED SILT FENCE MUST BE INSTALLED IN ACCORDANCE WITH COB EC-5 AND LOCATED AS SHOWN ON THE APPROVED PLANS OR PER THE CLEARING & GRADING INSPECTOR, ALONG SLOPE CONTOURS AND DOWN SLOPE FROM THE BUILDING SITE.
- A HARD-SURFACE CONSTRUCTION ACCESS PAD IS REQUIRED PER CLEARING & GRADING STANDARD DETAIL EC-1 OR EC-2. THIS PAD MUST REMAIN IN PLACE UNTIL PAVING IS INSTALLED.
- CLEARING WILL BE LIMITED TO THE AREAS WITHIN THE APPROVED DISTURBANCE LIMITS. EXPOSED SOILS MUST BE COVERED AT THE END OF EACH WORKING DAY WHEN WORKING FROM OCTOBER 1ST THROUGH APRIL 30TH, FROM MAY 1ST THROUGH SEPTEMBER 30TH, EXPOSED SOILS MUST BE COVERED AT THE END OF EACH CONSTRUCTION WEEK AND ALSO AT THE THREAT OF RAIN.
- ANY EXCAVATED MATERIAL REMOVED FROM THE CONSTRUCTION SITE AND DEPOSITED ON PROPERTY WITHIN THE CITY LIMITS MUST BE DONE IN COMPLIANCE WITH A VALID CLEARING & GRADING PERMIT. LOCATIONS FOR THE MOBILIZATION AREA AND STOCKPILED MATERIAL MUST BE APPROVED BY THE CLEARING & GRADING INSPECTOR AT LEAST 24 HOURS IN ADVANCE OF ANY STOCKPIILING.
- TO REDUCE THE POTENTIAL FOR EROSION OF EXPOSED SOILS, OR WHEN RAINY SEASON CONSTRUCTION IS PERMITTED, THE FOLLOWING BEST MANAGEMENT PRACTICES (BMPs) ARE REQUIRED:
PRESERVE NATURAL VEGETATION FOR AS LONG AS POSSIBLE OR AS REQUIRED BY THE CLEARING & GRADING INSPECTOR.
PROTECT EXPOSED SOIL USING PLASTIC (EC-14), EROSION CONTROL BLANKETS, STRAW OR MULCH (COB GUIDE TO MULCH MATERIALS, RATES, AND USE CHART), OR AS DIRECTED BY THE CLEARING & GRADING INSPECTOR.
INSTALL CATCH BASIN INSERTS AS REQUIRED BY THE CLEARING & GRADING INSPECTOR OR PERMIT CONDITIONS OF APPROVAL.
INSTALL A TEMPORARY SEDIMENT POND, A SERIES OF SEDIMENTATION TANKS, TEMPORARY FILTER VAULTS, OR OTHER SEDIMENT CONTROL FACILITIES. INSTALLATION OF EXPOSED AGGREGATE SURFACES REQUIRES A SEPARATE EFFLUENT COLLECTION POND ON-SITE.
- FINAL SITE GRADING MUST DIRECT DRAINAGE AWAY FROM ALL BUILDING STRUCTURES AT A MINIMUM 2% SLOPE, PER THE UNIFORM BUILDING CODE.
- THE CONTRACTOR MUST MAINTAIN A SWEEPER ON-SITE DURING EARTHWORK AND IMMEDIATELY REMOVE SOIL THAT HAS BEEN TRACKED ONTO PAVED AREAS AS RESULT OF CONSTRUCTION.
- A PUBLIC INFORMATION SIGN LISTING 24-HOUR EMERGENCY PHONE NUMBERS FOR THE CITY AND THE CONTRACTOR MAY BE PROVIDED TO THE APPLICANT AT THE TIME THE CLEARING & GRADING PERMIT IS ISSUED. THE APPLICANT MUST POST THE SIGN AT THE PROJECT SITE IN FULL VIEW OF THE PUBLIC AND THE CONTRACTORS, AND IT MUST REMAIN POSTED UNTIL FINAL SIGN-OFF BY THE CLEARING & GRADING INSPECTOR.
- TURBIDITY MONITORING MAY BE REQUIRED AS A CONDITION OF CLEARING & GRADING PERMIT APPROVAL. IF REQUIRED, TURBIDITY MONITORING MUST BE PERFORMED IN ACCORDANCE WITH THE APPROVED TURBIDITY MONITORING PLAN AND AS DIRECTED BY THE CLEARING & GRADING INSPECTOR. MONITORING MUST CONTINUE DURING SITE (EARTHWORK) CONSTRUCTION UNTIL THE FINAL SIGN-OFF BY THE CLEARING & GRADING INSPECTOR.
- ANY PROJECT THAT IS SUBJECT TO RAINY SEASON RESTRICTIONS WILL NOT BE ALLOWED TO PERFORM CLEARING & GRADING ACTIVITIES WITHOUT WRITTEN APPROVAL FROM THE PCD DIRECTOR. THE RAINY SEASON EXTENDS FROM NOVEMBER 1ST THROUGH APRIL 30TH, AS DEFINED IN SECTION 23.76.093A OF THE CLEARING & GRADING CODE.

STORM DRAINAGE NOTES

THIS PROJECT IS WITHIN MEYDENBAUER DRAINAGE BASIN. STORM DRAINAGE DETENTION IS NOT REQUIRED.
THIS PROJECT'S NEW IMPERVIOUS SURFACE SUBJECT TO HEVICULAR TRAFFIC IS LESS THAN 5,000 SQUARE FEET. STORM DRAINAGE WATER QUALITY IS NOT REQUIRED.

WATER NOTES

- PROVIDE 2" DOMESTIC WATER METER PER COB DTL W-29.
- PROVIDE 2" DOUBLE CHECK VALVE ASSEMBLY ON DOMESTIC SERVICE LINE PER COB DTL W-39 WITHIN THE BUILDING MECHANICAL ROOM.
- PROVIDE 6" FIRE SPRINKLER DCVA PER COB DTL W-45 WITHIN THE BUILDING MECHANICAL ROOM.
- PROVIDE HANDWHEEL INDICATOR VALVE DOWNSTREAM OF THE FIRE DCVA WITHIN THE BUILDING MECHANICAL ROOM.
- FIRE DEPARTMENT CONNECTION (FDC) SHALL BE WALL MOUNTED FACING MAIN STREET BETWEEN 30" AND 42" ABOVE THE SIDEWALK ELEVATION.
- PROVIDE FIRE HYDRANT PER COB DTL W-12, TO BE LOCATED WITHIN 50' OF THE FDC.

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CIVIL / STRUCTURAL



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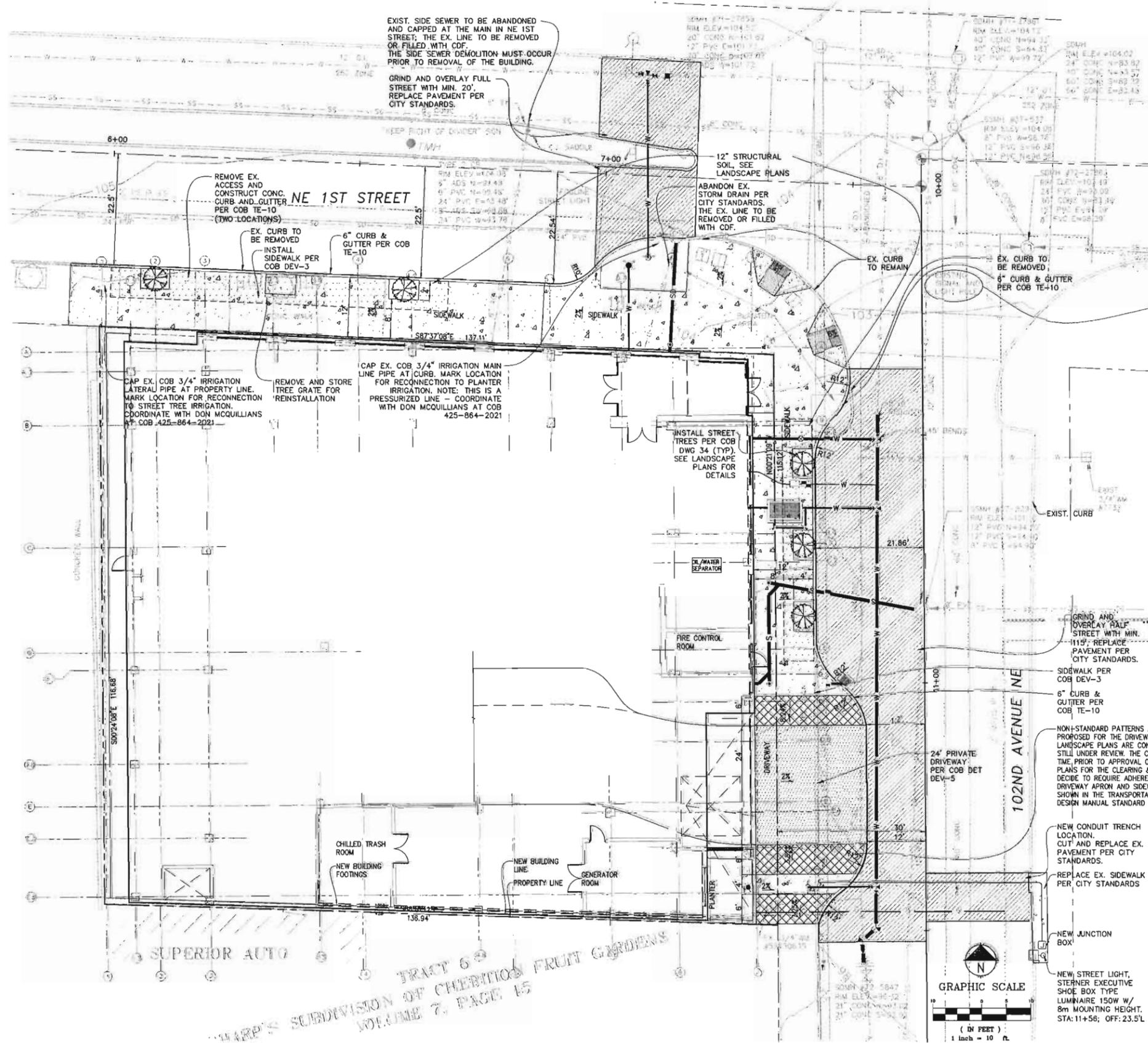
Design Review

Date: 12.24.2009
Job #: 07-12-039
COB Project #:
Drawing: NKK
Approval: DAS

CIVIL
CLEARING AND
GRADING PLAN
CL&GR

TRACT 6
SHARP'S SUBDIVISION OF CHERITON FRUIT GARDENS
VOLUME 7, PAGE 15





EXIST. SIDE SEWER TO BE ABANDONED AND CAPPED AT THE MAIN IN NE 1ST STREET; THE EX. LINE TO BE REMOVED OR FILLED WITH CDF. THE SIDE SEWER DEMOLITION MUST OCCUR PRIOR TO REMOVAL OF THE BUILDING.

GRIND AND OVERLAY FULL STREET WITH MIN. 20" REPLACE PAVEMENT PER CITY STANDARDS.

REMOVE EX. ACCESS AND CONSTRUCT CONC. CURB AND GUTTER PER COB TE-10 (TWO LOCATIONS)

EX. CURB TO BE REMOVED INSTALL SIDEWALK PER COB DEV-3

6" CURB & GUTTER PER COB TE-10

CAP EX. COB 3/4" IRRIGATION LATERAL PIPE AT PROPERTY LINE. MARK LOCATION FOR RECONNECTION TO STREET TREE IRRIGATION. COORDINATE WITH DON MCQUILLIANS AT COB 425-864-2021

REMOVE AND STORE TREE GRATE FOR REINSTALLATION

CAP EX. COB 3/4" IRRIGATION MAIN LINE PIPE AT CURB. MARK LOCATION FOR RECONNECTION TO PLANTER IRRIGATION. NOTE: THIS IS A PRESSURIZED LINE - COORDINATE WITH DON MCQUILLIANS AT COB 425-864-2021

12" STRUCTURAL SOIL, SEE LANDSCAPE PLANS

ABANDON EX. STORM DRAIN PER CITY STANDARDS. THE EX. LINE TO BE REMOVED OR FILLED WITH CDF.

EX. CURB TO REMAIN

EX. CURB TO BE REMOVED, 6" CURB & GUTTER PER COB TE-10

NOTE: THE EXISTING PEDESTRIAN PUSH BUTTON SHALL BE REPLACES WITH NEW PEDESTRIAN PUSH BUTTON TO MEET CURRENT ADA REQUIREMENTS. INSTALL POLARA "BULL DOG" PER POLARA DRAWING # RBDLM2-X-Y. CONTRACTOR SHALL REPLACE WIRES BACK TO SIGNAL CONTROLLER CABINET AND MODIFY CONTROLLER TO MAKE NEW PUSH BUTTONS FUNCTIONAL.

INSTALL STREET TREES PER COB DWG 34 (TYP). SEE LANDSCAPE PLANS FOR DETAILS

DL WATER SEPARATOR

FIRE CONTROL ROOM

GRIND AND OVERLAY FULL STREET WITH MIN. 1115" REPLACE PAVEMENT PER CITY STANDARDS.

SIDEWALK PER COB DEV-3

6" CURB & GUTTER PER COB TE-10

NON-STANDARD PATTERNS AND COLORS PROPOSED FOR THE DRIVEWAY APRON ON LANDSCAPE PLANS ARE CONCEPTUAL AND STILL UNDER REVIEW. THE CITY MAY, AT ANY TIME, PRIOR TO APPROVAL OF THE ENGINEERING PLANS FOR THE CLEARING & GRADING PERMIT, DECIDE TO REQUIRE ADHERENCE TO THE DRIVEWAY APRON AND SIDEWALK STANDARDS SHOWN IN THE TRANSPORTATION DEPARTMENT DESIGN MANUAL STANDARD DRAWINGS.

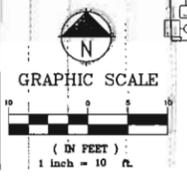
24" PRIVATE DRIVEWAY PER COB DET DEV-5

NEW CONDUIT TRENCH LOCATION. CUT AND REPLACE EX. PAVEMENT PER CITY STANDARDS.

REPLACE EX. SIDEWALK PER CITY STANDARDS

NEW JUNCTION BOX

NEW STREET LIGHT, STERNER EXECUTIVE SHOE BOX TYPE LUMINAIRE 150W W/ 8m MOUNTING HEIGHT. STA: 11+56; OFF: 23.5'L



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Revisions Date

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Design Review

Date: 12.24.2009
Job #: 07 12-039
COB Project #:
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Approval: DAS

CIVIL
ROAD PLAN

SHUGART BATES

RD

TRACT 6
HARP'S SUBDIVISION OF CHERITON FRUIT GARDENS
VOLUME 7, PAGE 15

SUPERIOR AUTO

CHILLED TRASH ROOM
NEW BUILDING FOOTINGS

NEW BUILDING PROPERTY LINE
GENERATOR ROOM

PLANTER

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Revisions	Date
DR Revision 1	04.21.2010

DR Resubmittal

Date:	08.18.2010
Job #:	27017
COB Project #:	07-142899-LD
Drawing:	Author
Approval:	Checker

3D Views



3 Aerial View from 102nd Ave and NE 1st Street



1 View from Main Street



4 Ground View from Main Street



2 View from Bellevue Downtown Park

SHUGART BATES A00.02

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Revisions	Date
DR Revision 1	04.21.2010

100% Design Development Set

Date:	04.21.2010
Job #:	27017
COB Project #:	07-143899-LD
Drawing:	Author
Approval:	Checker

FAR Amenity Plan

FAR CALCULATIONS

PROJECT LIMIT (SITE AREA) = 15,483 SF
(0.2) X (15,483 SF) X (0.5) = 1,548.3 SF BASIC FAR REQUIRED

BASIC PERMITTED FAR = 2.0
(2.0 X 15,483 SF) = 30,966 SF
ALLOWABLE MAXIMUM FAR = 5.0
(5.0 X 15,483 SF) = 77,416 SF

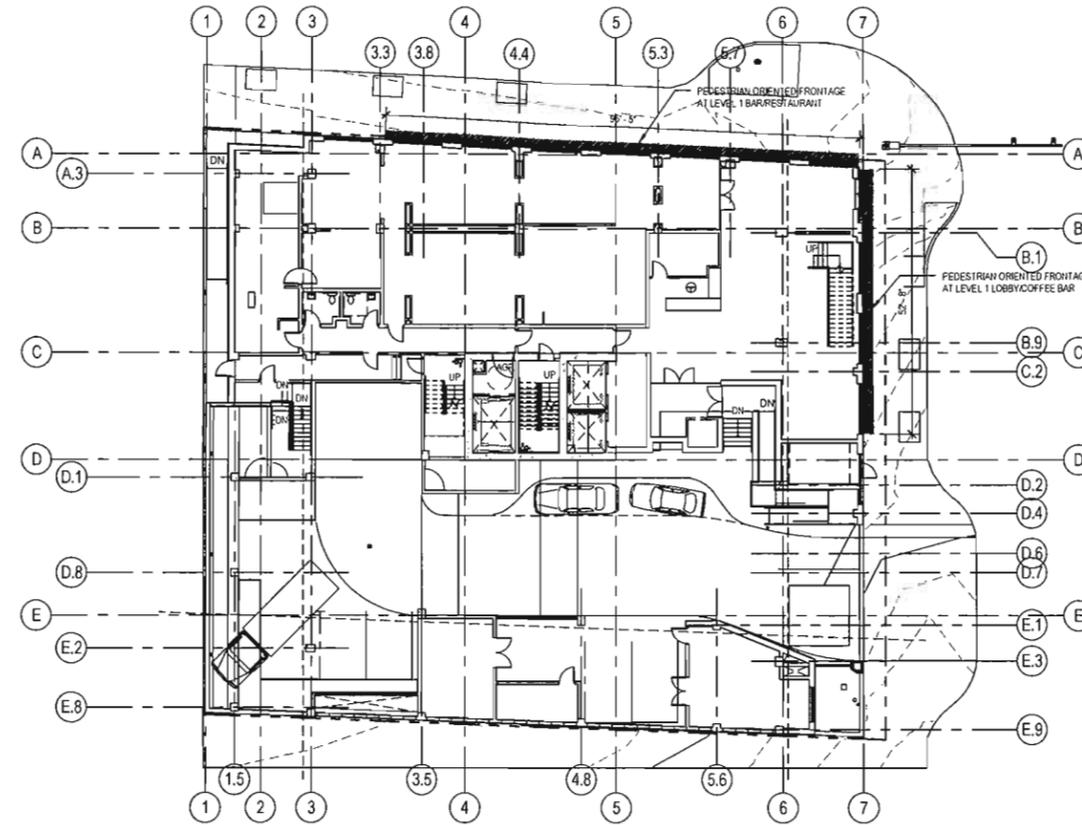
PROJECT GROSS FLOOR AREA (GFA) =	72,682 SF
BASIC PERMITTED FLOOR AREA = (BASIC FAR X PROJECT LIMIT)	30,966 SF
ADDITIONAL FLOOR AREA REQUESTED = (PROJECT GFA - BASIC PERMITTED FLOOR AREA)	41,716 SF
BASIC FAR AMENITY EARNED (TABLE 2) =	30,577 SF
REMAINING (BONUS) FAR AMENITY TO EARN = (PROJECT GFA - BASIC PERMITTED - BASIC EARNED)	11,139 SF
TOTAL FAR AMENITY EARNED (TABLE 2) =	111,857 SF
EXCESS FAR AMENITY EARNED = (TOTAL EARNED - BASIC EARNED - BONUS TO EARN)	70,141 SF

TABLE 2: BONUS AMENITIES

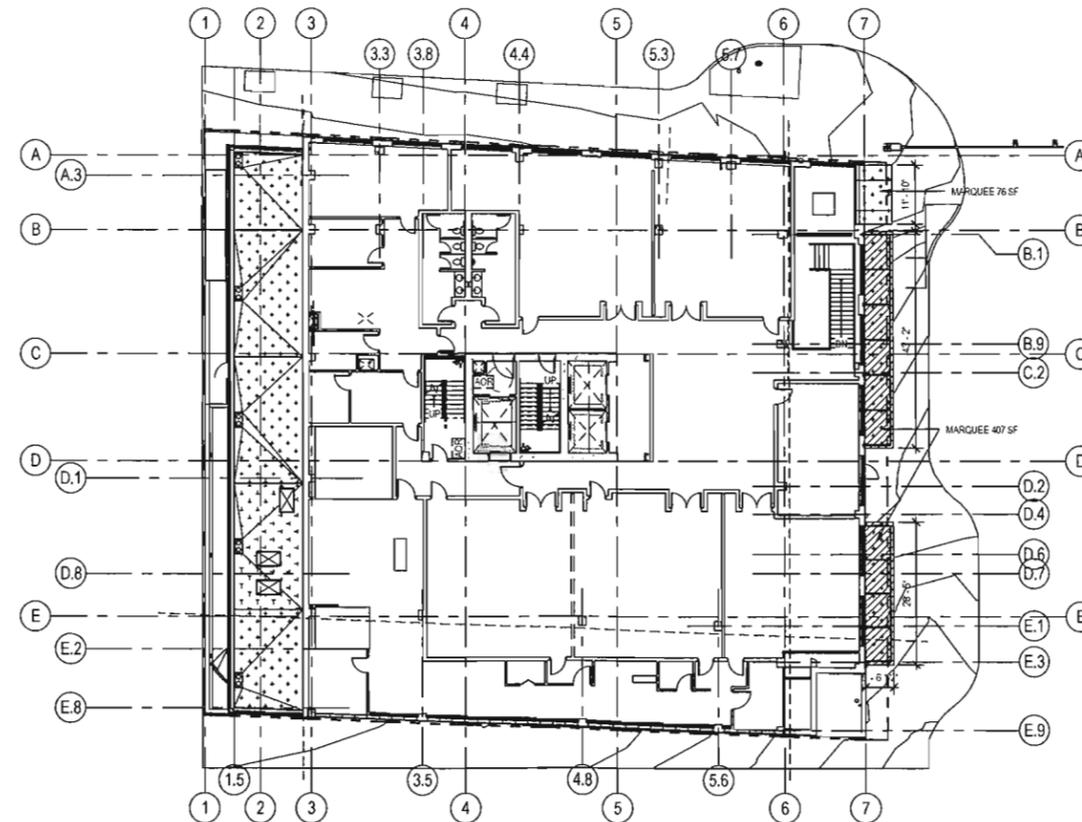
BASIC AMENITIES	
PEDESTRIAN ORIENTED FRONTAGE - LEVEL 1 FRONTAGE = 148'-1" X 200' (200') = 29,617 SF EARNED	
MARQUEE - 483 SF X 2 = 966 SF EARNED	
SUBTOTAL BASIC = 30,583 SF	
NON-BASIC AMENITIES	
UNDERGROUND PARKING - 40,637 SF X 2 = 81,274 SF	
SUBTOTAL BONUS = 81,274 SF	
COMBINED TOTAL = 111,857 SF	

FAR AREA SUMMARY

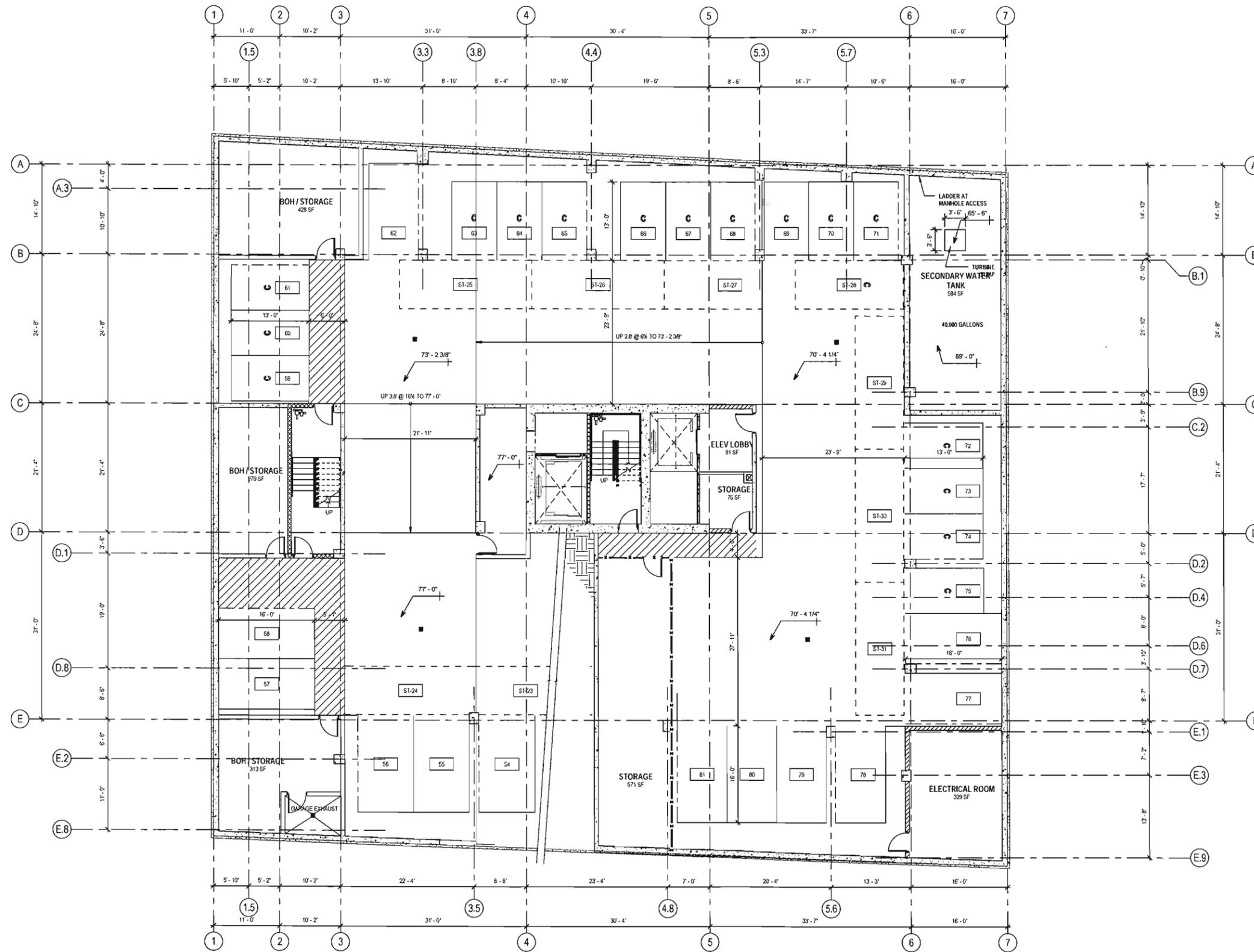
Area Schedule (FAR)	
Level	Area
LEVEL 9	8072 SF
LEVEL 8	8072 SF
LEVEL 7	8072 SF
LEVEL 6	8072 SF
LEVEL 5	8072 SF
LEVEL 4	8072 SF
LEVEL 3	8081 SF
LEVEL 2	11048 SF
LEVEL 1	3956 SF
LEVEL P2	1663 SF
LEVEL P3	411 SF
	73588 SF



1 FAR diagram LEVEL 1
1/16" = 1'-0"



2 FAR diagram LEVEL 2
1/16" = 1'-0"



① LEVEL P3
1/8" = 1'-0"

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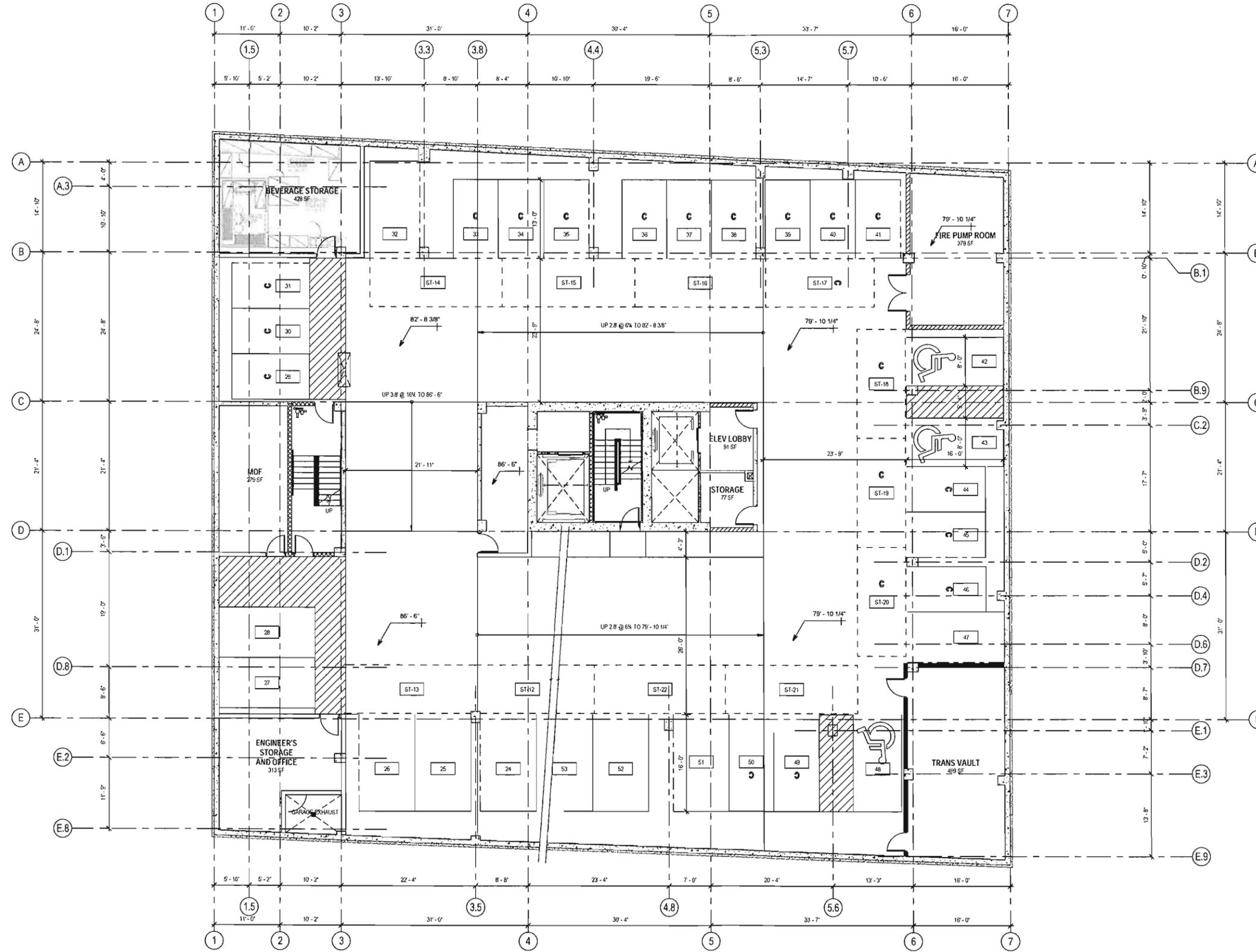
Revisions	Date

Design Review Submittal

Date:	01.11.2010
Job #:	27017
COB Project #:	07-143899-LD
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Approval:	Checker

Parking Level P3 Plan

SHUGART BATES A02.01



① LEVEL P2
1/8" = 1'-0"



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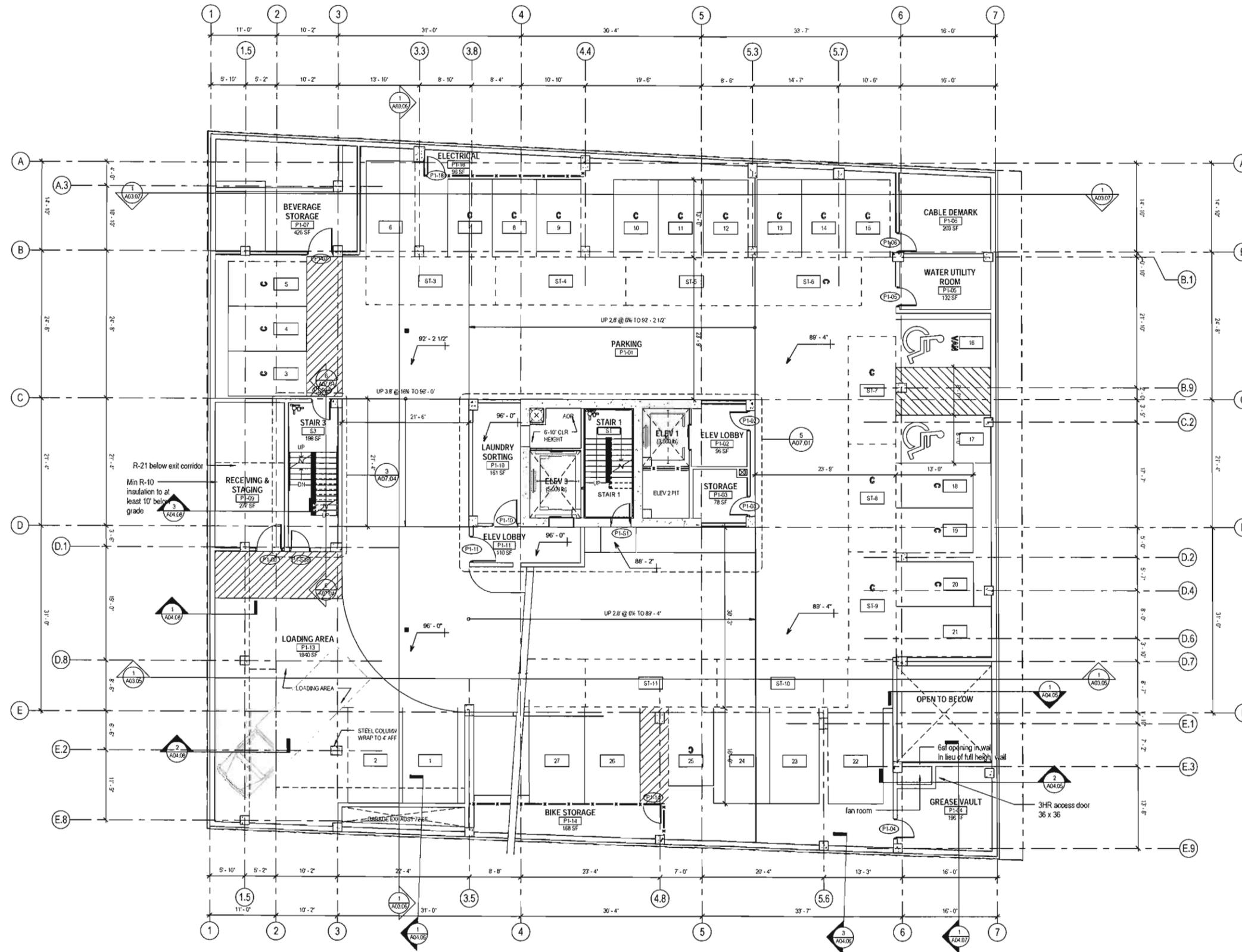
FOR REFERENCE ONLY

Revisions Date

Design Review
Submittal

Date: 01.11.2010
Job #: 27017
COB Project #: 07-143899-LD
Drawing: Author
Approval: Checker

Parking Level P2 Plan



① LEVEL P1
1/8" = 1'-0"

Parking Schedule P3	
Stall Type	Count
Compact	21
Standard 8'-4"	9
Standard 9'-4"	5

Compact Stacked	5
Standard 8'-4" Stacked	6
Total	46

P2	
Stall Type	Count
Compact	17
Handicap	3
Standard 8'-4"	2
Standard 9'-4"	4

Compact Stacked	4
Standard 8'-4" Stacked	6
Total	36

P1	
Stall Type	Count
Compact	15
Handicap	1
Standard 8'-4"	3
Standard 9'-4"	3
Van	1

Compact Stacked	4
Standard 8'-4" Stacked	4
Total	31

L1	
Stall Type	Count
Standard 9'-4"	2
Total	2

Parking Total **115**

Total Parking Counts		
Stall Type	Count	Percentage
Compact	53	46%
Handicap	4	3%
Standard 8'-4"	14	12%
Standard 9'-4"	14	12%
Van	1	1%
Total	86	75%

Compact Stacked	13	11%
Standard 8'-4" Stacked	16	14%
Total	29	25%

Bellevue Park Hotel

25 102nd Ave NE
Bellevue, WA
98004

Owner
First Bellevue Properties LLC
800 5th Ave, Suite 4100
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p: 206.447.1401

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Revisions	Date
DR Revision 1	04.21.2010

100% Design
Development Set

Date: 04.21.2010
Job #: 27017
COB Project #: 07-143899-LD
Drawing: Author
Approval: Checker

Parking Level P1 Plan

SHUGART BATES A02.03

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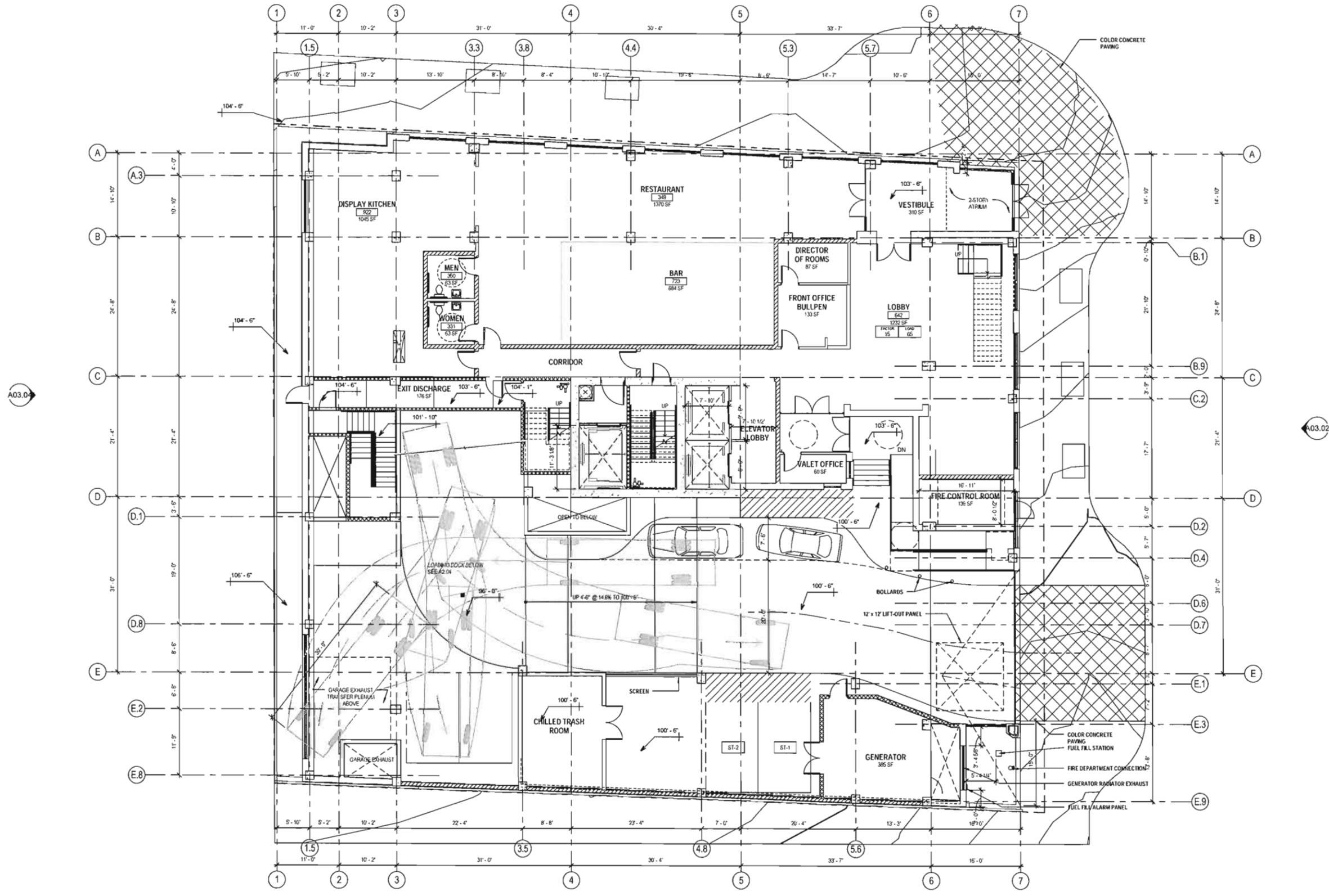
Revisions	Date

Design Review Submittal

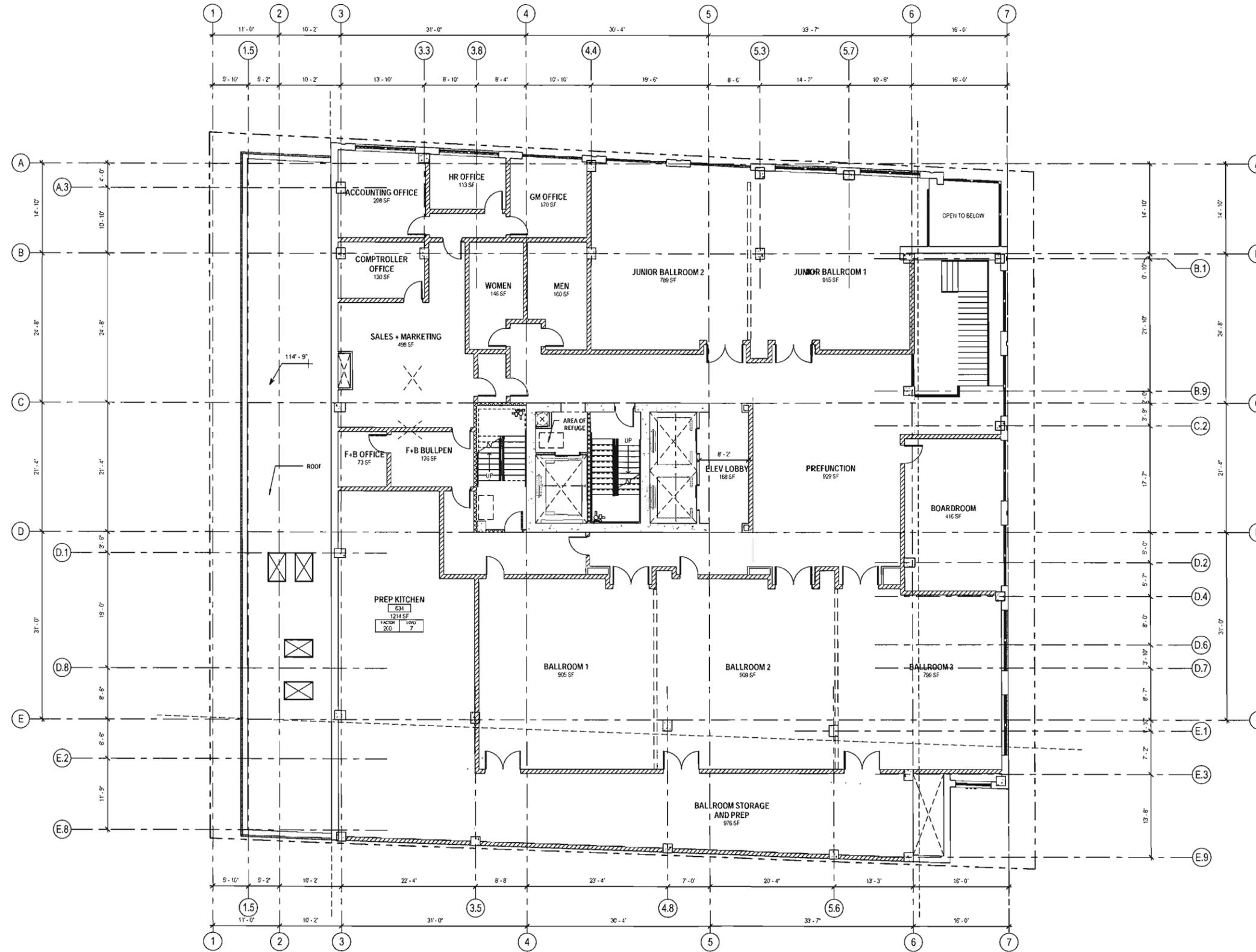
Date: 01.11.2010
Job #: 27017
COB Project #: 07-143899-LD
Drawing: Author
Approval: Checker

Level 1 Floor Plan

SHUGART BATES A02.04



1 LEVEL 1
1/8" = 1'-0"



① LEVEL 2
1/8" = 1'-0"



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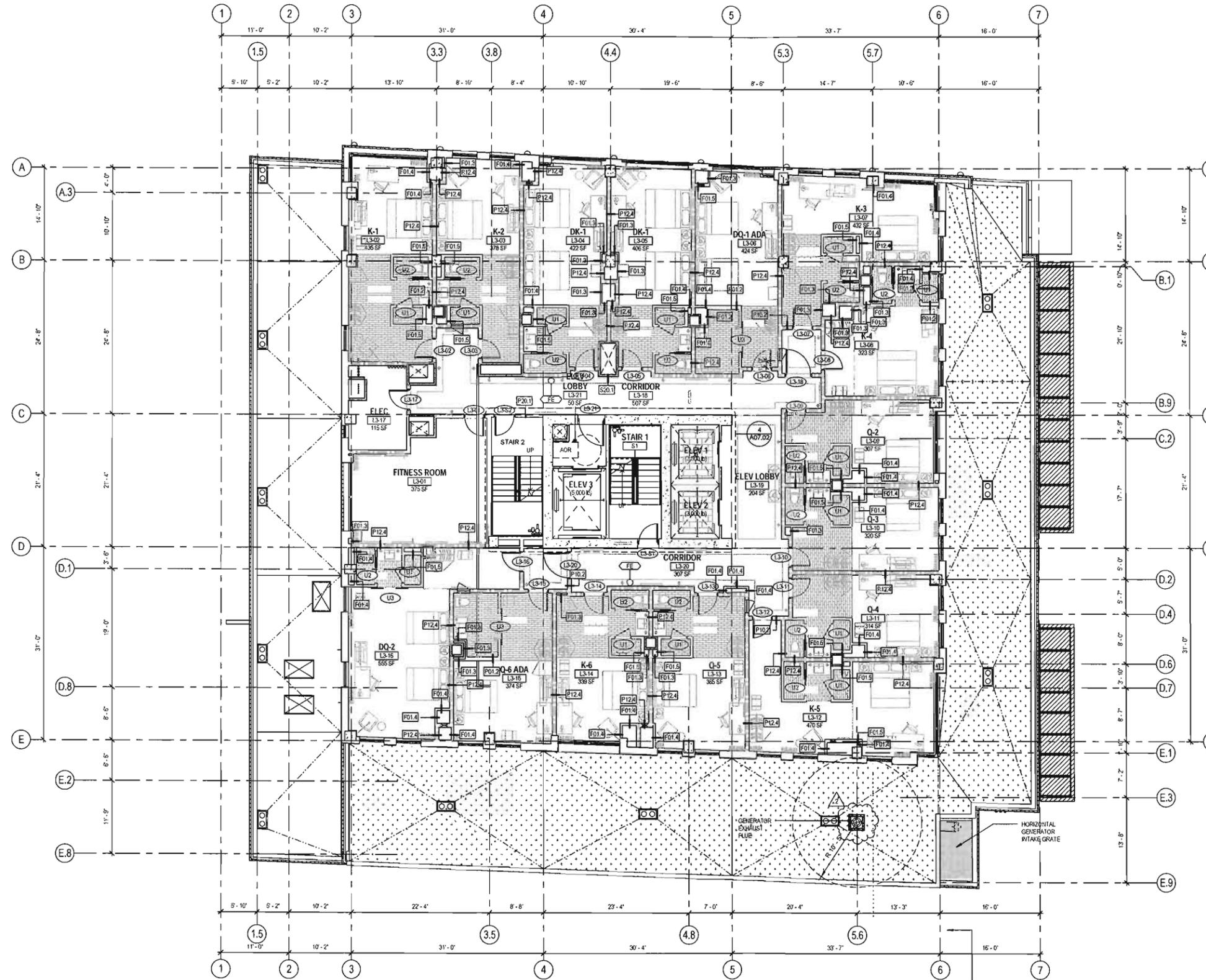
FOR REFERENCE ONLY

Revisions	Date

**Design Review
Submittal**

Date: 01.11.2010
Job #: 27017
COB Project #: 07-143899-LD
Drawing: Author
Approval: Checker

Level 2 Floor Plan



MATERIAL AND EQUIPMENT LEGEND

SEE SHEET A9.01 AND A9.02 FOR ASSEMBLY DETAILS

[Symbol]	NON-RATED CONSTRUCTION PARTITION	[Symbol]	U.O.N.
[Symbol]	1 HOUR CONSTRUCTION PARTITION	[Symbol]	U.O.N.
[Symbol]	2 HOUR CONSTRUCTION PARTITION	[Symbol]	U.O.N.
[Symbol]	3 HOUR CONSTRUCTION PARTITION	[Symbol]	U.O.N.
[Symbol]	2 HOUR SHAFT WALL FIRE BARRIER	[Symbol]	U.O.N.
[Symbol]	UNMARKED FURRING	[Symbol]	
[Symbol]	CONCRETE	[Symbol]	
[Symbol]	FIRE EXTINGUISHER	[Symbol]	
[Symbol]	SITE PROPERTY LINE	[Symbol]	
[Symbol]	FLOOR DRAIN, SEE PLUMBING	[Symbol]	
[Symbol]	INTERNAL ROOF DRAIN AND OVERFLOW, 2" DIA. DRAIN, 8" CLEAR REQ'D FROM CENTER OF PIPE	[Symbol]	
[Symbol]	LINE OF SLOPED INSULATION OR DECK AT RIDGE OR VALLEY	[Symbol]	
[Symbol]	SLOPE TO DRAIN IN DIRECTION OF ARROW, 1/4" 1'-0"	[Symbol]	
[Symbol]	MECHANICAL SHAFT	[Symbol]	
[Symbol]	STANDARD PIPE	[Symbol]	
[Symbol]	HOSE BIB	[Symbol]	
[Symbol]	WALL MOUNTED EXTERIOR LIGHT, SEE LIGHTING	[Symbol]	

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Revisions	Date
DR Revision 2	08.18.2010

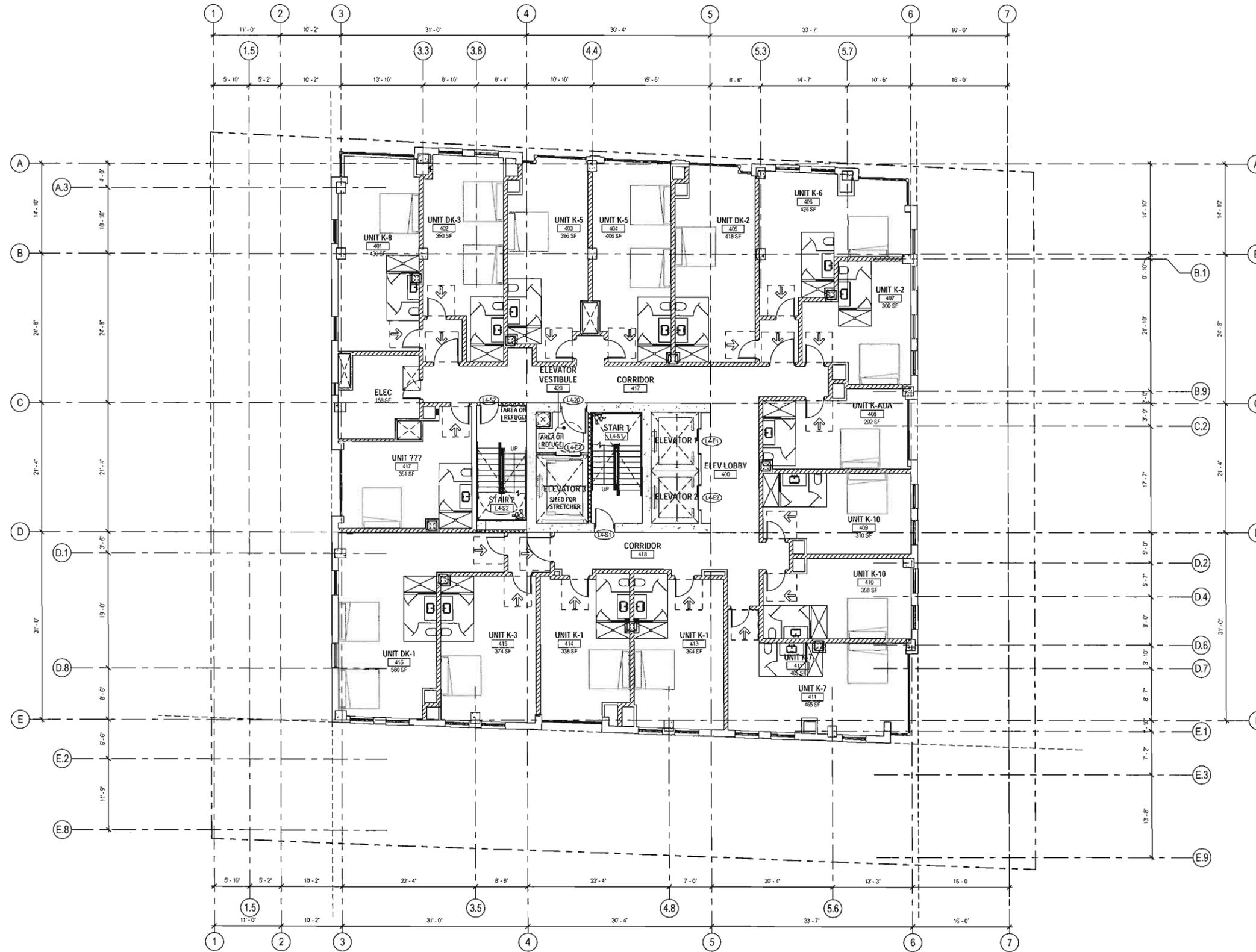
DR Resubmittal

Date: 08.18.2010
Job #: 27017
COB Project #: 07-143899-LD
Drawing:
Approval:

Level 3 Floor Plan

1 LEVEL 3
1/8" = 1'-0"

SHUGART BATES A02.06



① LEVELS 4-8
1/8" = 1'-0"

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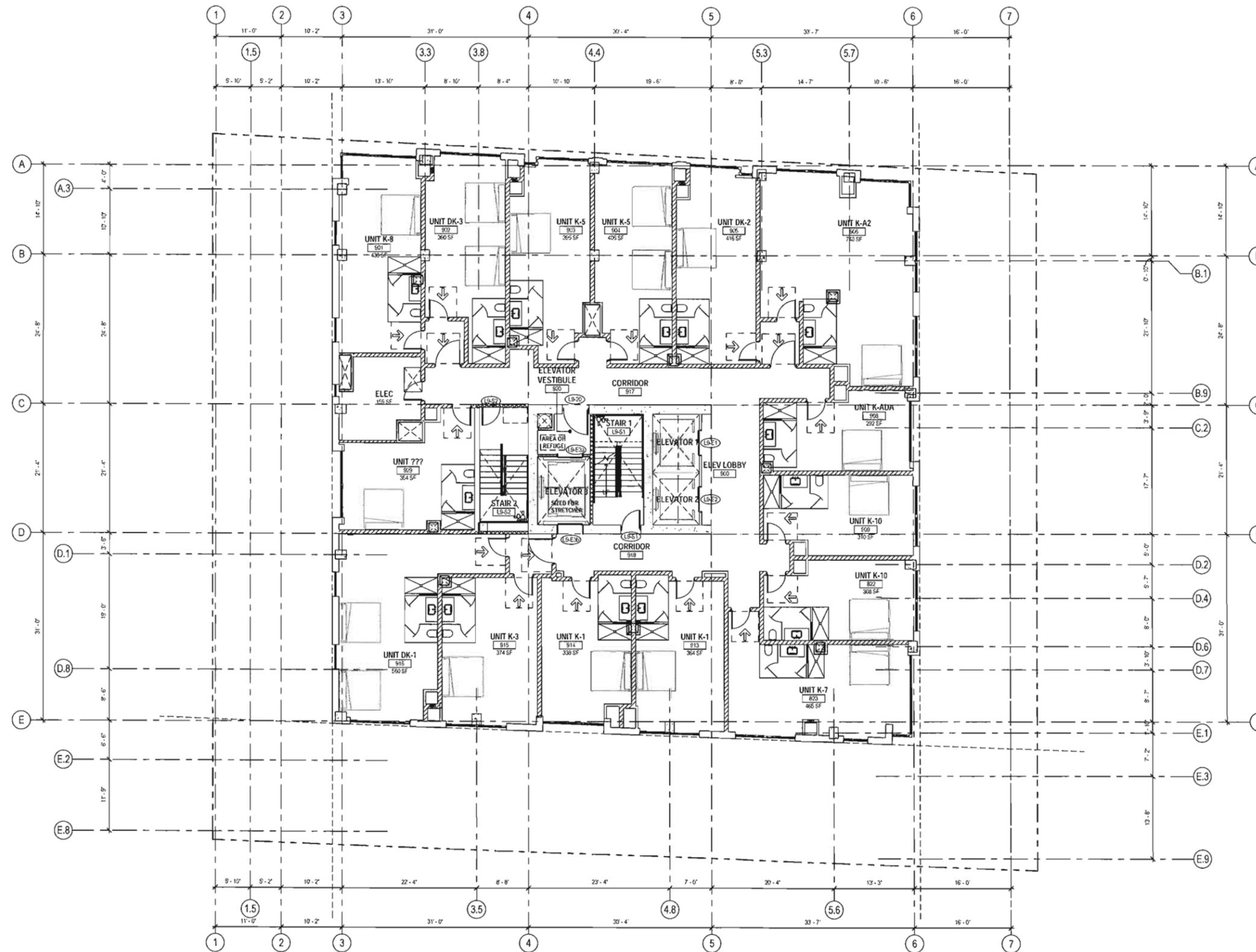
FOR REFERENCE ONLY

Revisions	Date

Design Review
Submittal

Date: 01.11.2010
Job #: 27017
COB Project #: 07-143899-LD
Drawing: Author
Approval: Checker

Levels 4-8 Floor Plan



① LEVEL 9
1/8" = 1'-0"



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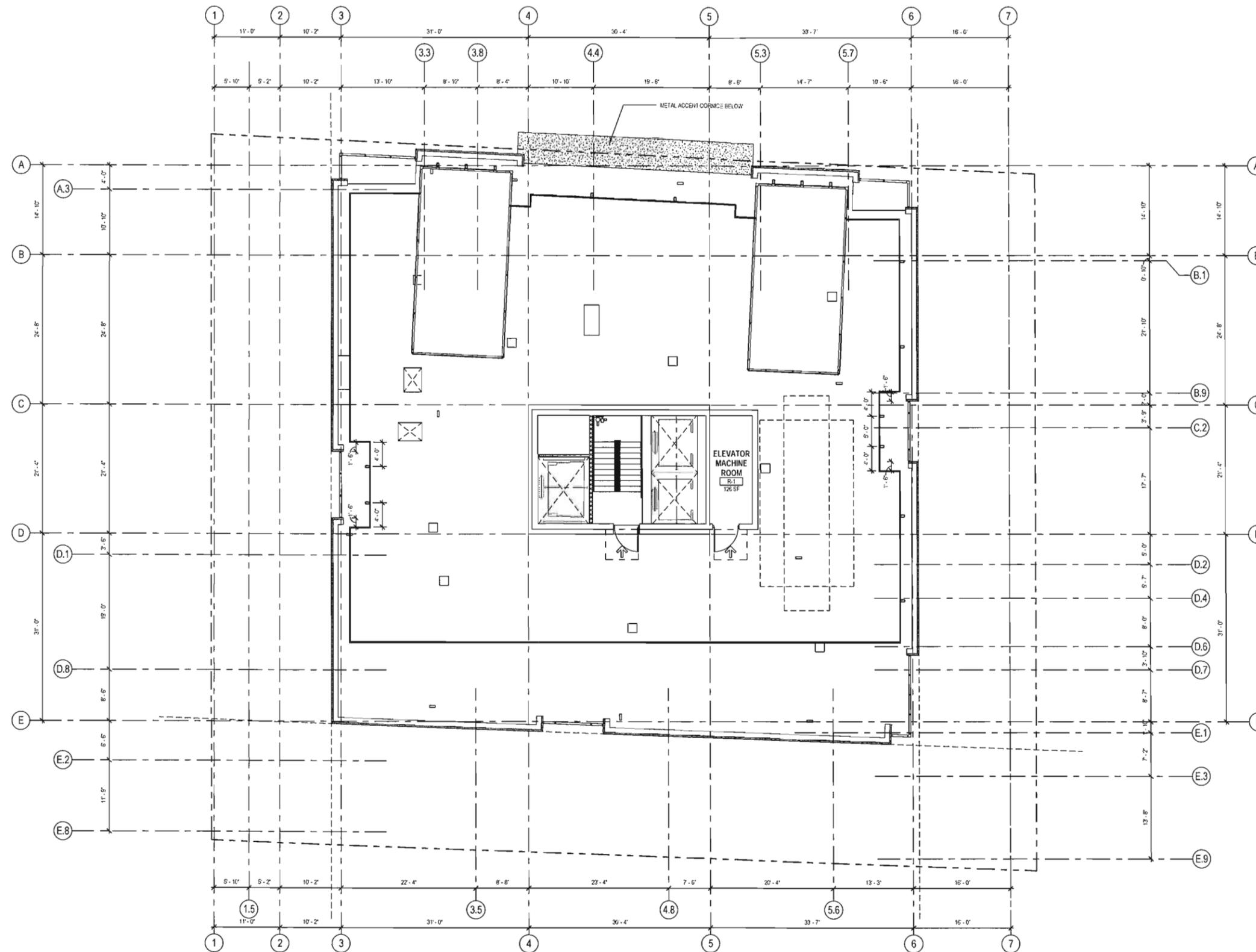
FOR REFERENCE ONLY

Revisions	Date

Design Review
Submittal

Date: 01.11.2010
Job #: 27017
COB Project #: 07-143899-LD
Drawing: Author
Approval: Checker

Level 9 Floor Plan



1 ROOF
1/8" = 1'-0"



Bellevue Park Hotel

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Revisions Date

Design Review
Submittal

Date: 01.11.2010
Job #: 27017
COB Project #: 07-143899-LD
Drawing: Author
Approval: Checker

Roof Plan

SHUGART BATES A02.09



① North
1/8" = 1'-0"

MATERIALS LEGEND:

-  BRICK #1
-  BRICK #2
-  CLEAR, INSULATED, LOW E VISION GLASS IN ANODIZED BRONZE ALUMINUM FRAME
-  SPANDREL GLASS
-  60% OPAQUE CERAMIC FRITTED GLASS LINE PATTERN
-  GLAZED IN METAL PANEL
-  EXPOSED ARCHITECTURAL CONCRETE

Bellevue Park Hotel

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Bellevue, WA
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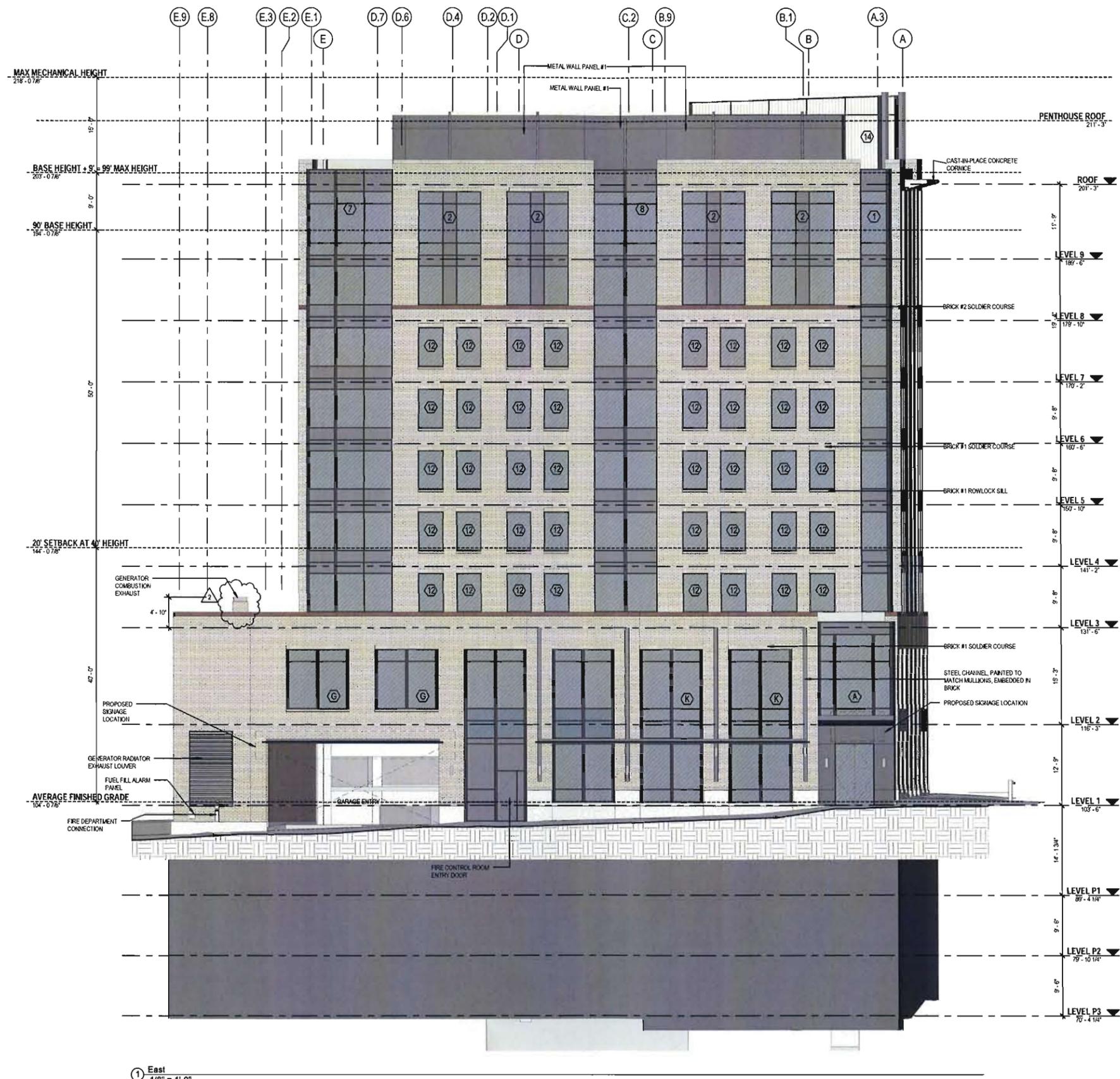
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Revisions	Date

Building Permit Submittal

Date: **06.25.2010**
Job #: **27017**
COB Project #: **07-143899-LD**
Drawing:
Approval:

North Elevation



MATERIALS LEGEND:

-  BRICK #1
-  BRICK #2
-  CLEAR, INSULATED, LOW E MISSION GLASS IN ANNOXIDIZED BRONZE ALUMINUM FRAME
-  SPANDREL GLASS
-  60% OPAQUE CERAMIC FRITTED GLASS LINE PATTERN
-  GLAZED-IN METAL PANEL
-  EXPOSED ARCHITECTURAL CONCRETE

Bellevue Park Hotel

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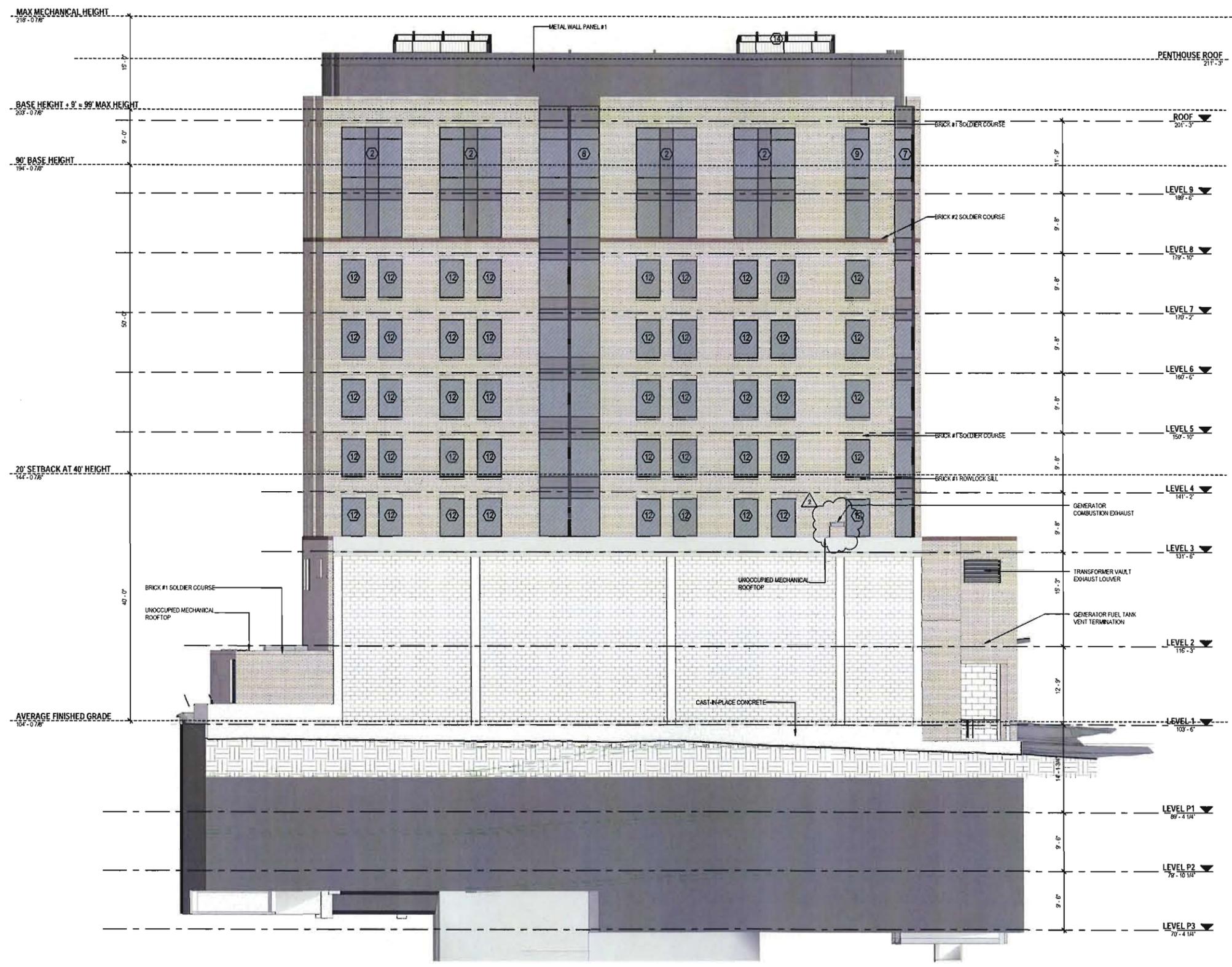
Revisions	Date
DR Revision 2	08.18.2010

DR Resubmittal

Date: **08.18.2010**
Job #: **27017**
COB Project #: **07-143899-LD**
Drawing:
Approval:

East Elevation

1 East
1/8" = 1'-0"



MATERIALS LEGEND:

-  BRICK #1
-  BRICK #2
-  CLEAR INSULATED LOW E VISION GLASS IN ANODIZED BRONZE ALUMINUM FRAME
-  SPANDREL GLASS
-  80% OPAQUE CERAMIC FRITTED GLASS LINE PATTERN
-  GLAZED IN METAL PANEL
-  EXPOSED ARCHITECTURAL CONCRETE

Bellevue Park Hotel

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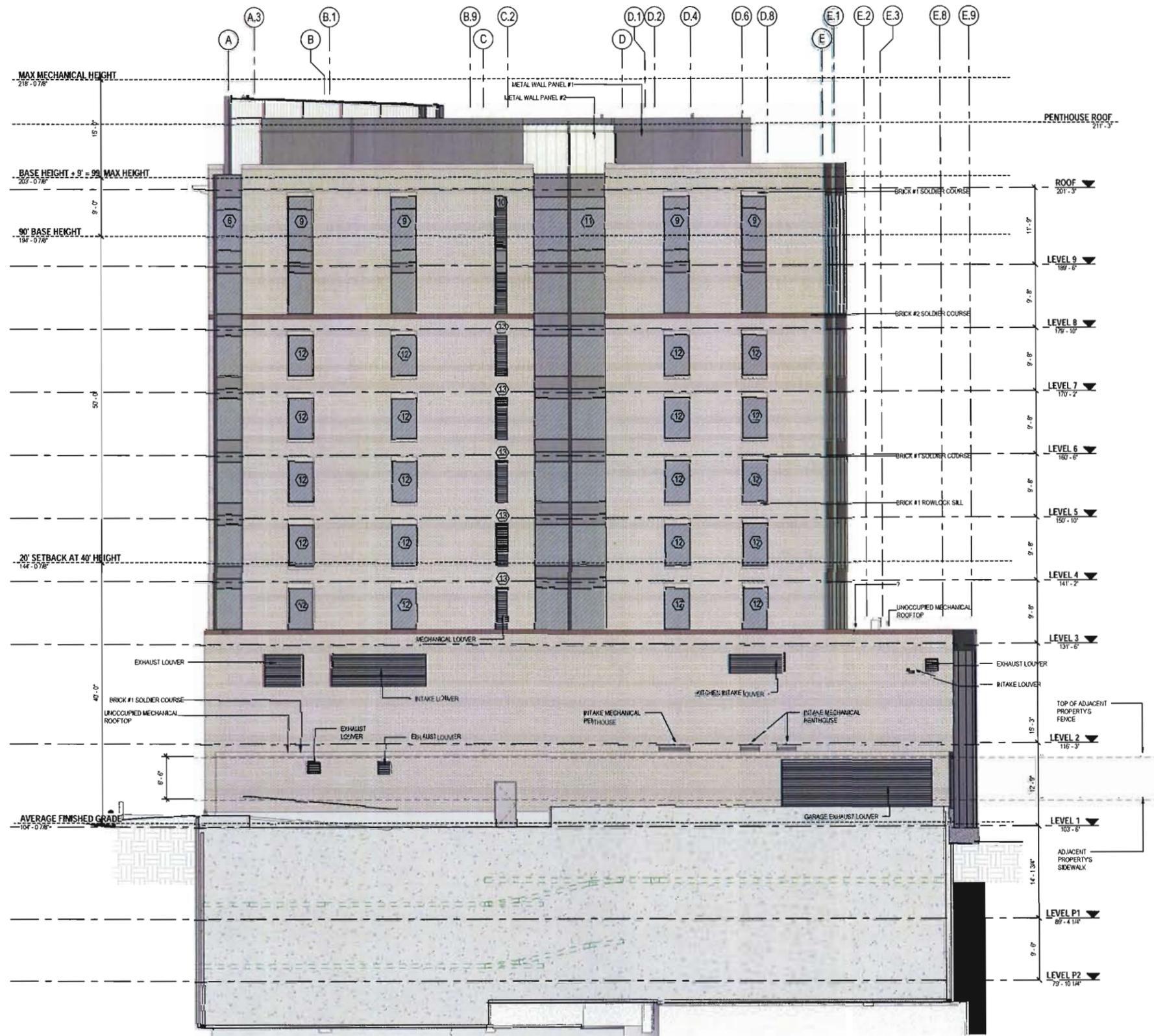
Revisions	Date
DR Revision 1	04.21.2010
DR Revision 2	08.18.2010

DR Resubmittal

Date: **08.18.2010**
Job #: **27017**
COB Project #: **07-143889-LD**
Drawing:
Approval:

South Elevation

1 South
1/8" = 1'-0"



MATERIALS LEGEND:

- BRICK #1
- BRICK #2
- CLEAR INSULATED L.C. E-VIEW GLASS IN ALKOXED BRONZE ALUMINUM FRAME
- SPANDEL GLASS
- 8IN. CERAMIC FRITTED GLASS
- GLAZED IN METAL FRAME
- EXPOSED ARCHITECTURAL CONCRETE

Bellevue Park Hotel

25 102nd Ave NE
Bellevue, WA
98004

Owner:
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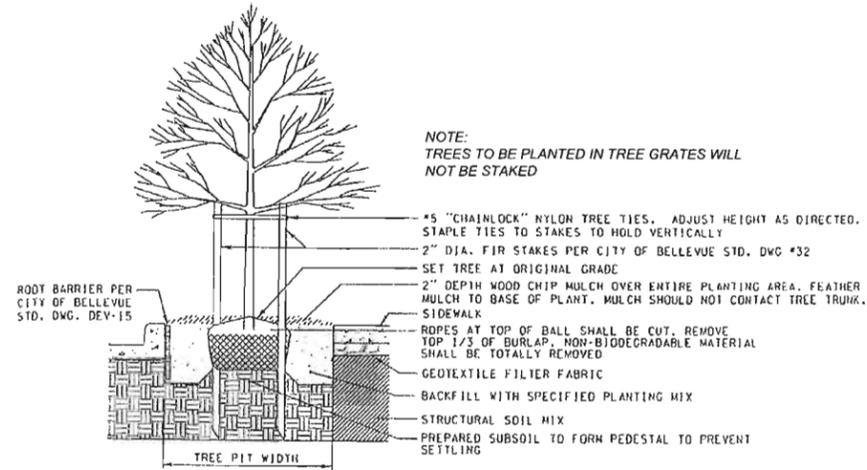
Revisions	Date
DR Revision 1	04.21.2010

Building Permit Submittal

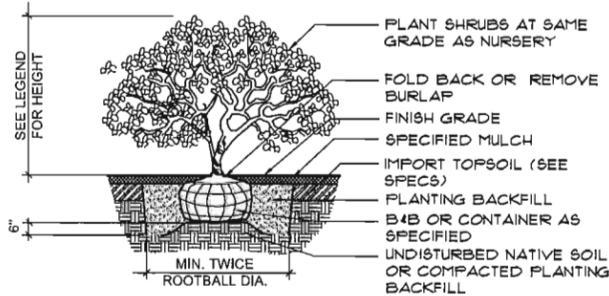
Date: **06.25.2010**
Job #: **27017**
COB Project #: **07-143898-LD**
Drawing:
Approval:

West Elevation

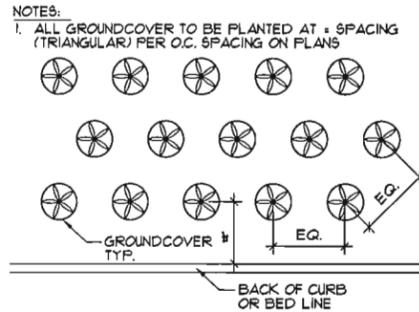
① West
1/8" = 1'-0"



A STREET TREE PLANTING AND STRUCTURAL SOIL
NTS



B SHRUB PLANTING
NOT TO SCALE



C GROUNDCOVER SPACING
NOT TO SCALE

LANDSCAPE SCHEDULE

SYMBOL	BOTANICAL / COMMON NAME	MIN. SIZE / CONDITION / REMARKS
--------	-------------------------	---------------------------------

DECIDUOUS STREET TREES:

	Quercus coccolinea Scarlet Oak	4" cal., full, well-branched, matched, B&B. Select trees to have similar form as existing trees. Prune to match shape if necessary.
	Gleditsia triacanthos 'Shademaster' Shademaster Honey Locust	3" cal., full, well-branched, matched, B&B. Select trees to have similar form as existing trees. Prune to match shape if necessary.
	Magnolia g. 'Little Gem' Little Gem Magnolia	2" cal., full, well-branched, B&B or box

SHRUBS:

	Cornus stolonifera 'Kelsey' Kelsey Red-Twig Dogwood	Min. 15" spr., full foliage, spacing as shown on plan.
	Leucothoe fontanesiana Drooping Leucothoe	18-21" ht., full foliage, spacing as shown on plan.
	Nandina d. 'Wood's Dwarf' Wood's Dwarf Nandina	12-15" ht. and spr., full foliage, spacing as shown on plan.
	Rhododendron x 'Scarlet Wonder' Scarlet Wonder Dwarf Rhododendron	18-21" spr., full foliage, hybrid as approved, spacing as shown on plan.
	Rhododendron sp. Medium Rhododendron	24-30" spr., full foliage, spacing as shown on plan. Cream or white flowering hybrid as approved

GROUNDCOVERS and PERENNIALS:

	Bergenia 'Ruby Elf' Dwarf Bergenia	1 gal. cont., full, spacing as shown on plan.
	Carex flagellifera Orange Sedge	1 gal. cont., full, spacing as shown on plan.
	Carex morrowii exallida Variegated Sedge	1 gal. cont., full, spacing as shown on plan.
	Vinca m. 'Alba' White flowering Periwinkle	4" pots at 12" o.c. triangular spacing, plant throughout planting area where shown

PAVING LEGEND

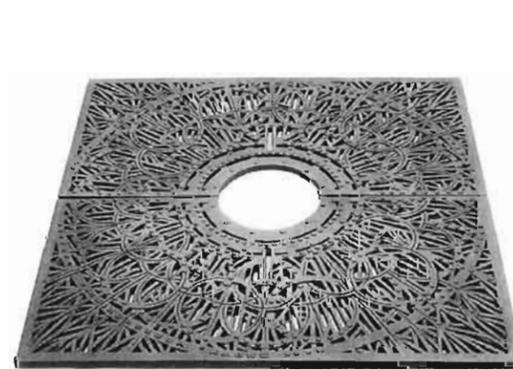
	STRUCTURAL SOIL See detail 'A' on this sheet and specifications for additional information.
	CONCRETE PAVING Broom finish, score lines 2' or 4' o.c. as shown on plans. See civil drawings and Specifications for additional information.
	COLOR CONCRETE PAVING Broom finish, score lines 4' o.c. as shown on plans. Color to be determined. See civil drawings and specifications for additional information.
	STRUCTURAL SOIL See detail 'A' on this sheet and specifications for additional information.
	CONCRETE PAVERS 24" x 24" Concrete Pavers

SITE LIGHTING LEGEND

	Wall Mounted Fixture #1
	Wall mounted Fixture #2
	Landscape Uplight

PLANTING NOTES

- ALL NEW LANDSCAPE AREAS ARE TO BE WATERED WITH AN AUTOMATIC WATER CONSERVING IRRIGATION SYSTEM IN ACCORDANCE WITH CITY WATER CONSERVATION REQUIREMENTS.
- TREES, SHRUBS, AND GROUNDCOVERS SHALL BE GROUPED INTO AREAS OF DISTINCT HYDROZONES (SIMILAR WATER USE NEEDS AND EXPOSURE.)
- ALL NEW SHRUB AND GROUNDCOVER AREAS ARE TO BE MULCHED AS SPECIFIED.
- WHERE GROUNDCOVER IS PROVIDED, IT SHALL BE PLANTED AT THE SPECIFIED SPACING THROUGHOUT THE BED, INCLUDING AREAS UNDERNEATH TREES AND SHRUBS, START FIRST ROW 12" FROM EDGE OF BED.
- PLANTINGS LOCATED WITHIN SIGHT DISTANCE LINE SHALL HAVE A MATURE HEIGHT OF 30" ABOVE SIDEWALK GRADE. SITE FURNITURE AND OTHER NEW OBSTRUCTIONS SHALL BE LOCATED OUTSIDE SIGHT DISTANCE LINE.
- STREET TREES ALONG NE 1ST STREET SHALL BE LIMBED UP TO 7 1/2'. STREET TREES ALONG 102ND AVENUE NE SHALL BE LIMBED UP TO 6'.
- REFER TO SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.



4'x6' grate proposed along both streets



bench to match Bellevue Downtown Park benches

SITE FURNITURE

Bellevue Park Hotel

25 102nd Ave NE
Bellevue, WA
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First Bellevue Properties LLC
800 5th Ave. Suite 4100
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206.632.1981

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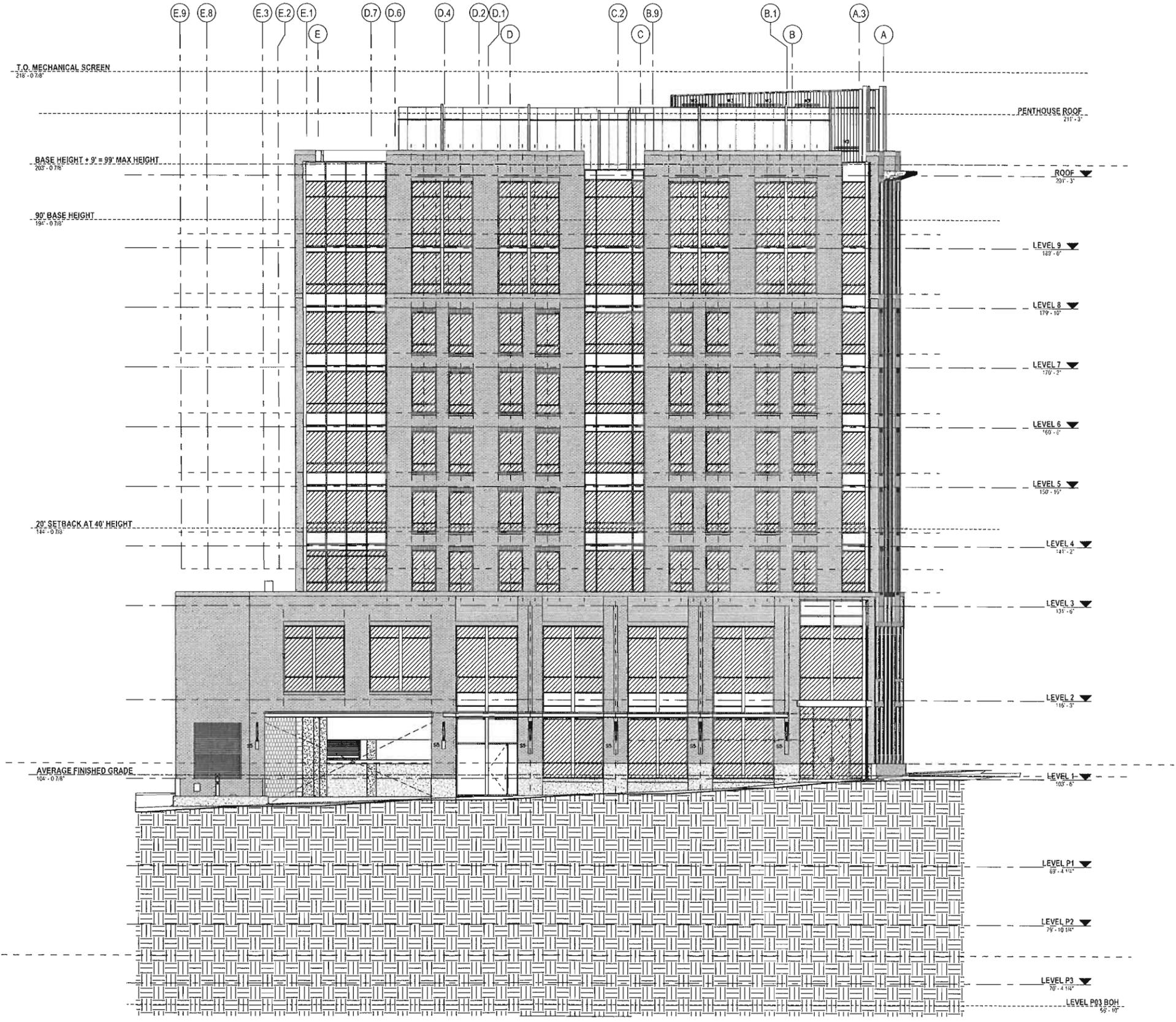
Revisions	Date

Name:	Design Review
Date:	12.24.2009
Job #:	27017
Permit #:	-
Drawing:	-
Approval:	-

Landscape Schedule and Details

L1.02

SHUGART BATES



1 EXTERIOR LIGHTING ELEVATION - EAST
SCALE: 1/8" = 1'-0"

FOR COORDINATION/REVIEW ONLY

SHUGART BATES LD3.02

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Revisions	Date

100% Design Development Set

Date: 02.19.2010
Job #: 27017
COB Project #: -
Drawing:
Approval:

EXTERIOR LIGHTING ELEVATION EAST

ATTACHMENT B
(Certificate of Concurrence)

CERTIFICATE OF CONCURRENCY

BELLEVUE PARK HOTEL

This certificate documents the Transportation Department Director's decision that the development project at 25 102ND Avenue (Design Review File No. 07-143899 LD) complies with the requirements of the Traffic Standards Code (BCC 14.10). This decision reserves 55 p.m. peak hour trips to this project, subject to Process II appeal of either the concurrency determination or the Design Review decision. A building permit application (File No. 10-116982 BB) was filed for the project on June 25, 2010, and deemed complete on June 29, 2010. This concurrency reservation will remain in effect for the life of the building permit application (BCC 23.05.090.H). Upon issuance of the building permit, concurrency is reserved for one year; the applicant may request up to two one-year extensions (BCC 23.05.100.E).



Director, Transportation Department

9/25/10

Date

Certificate No. 60

ATTACHMENT C
(Fire Review Comments)

ATTACHMENT C

Fire Prevention Division, Fire Department

(Staff contact-Travis Ripley, 425-452-6042, TRipley@bellevuewa.gov)

1. Provide life safety systems to include fire alarm, automatic fire sprinklers, emergency voice/alarm communication systems, fire communications system, standpipes, smoke control and shaft pressurization systems, and building radio coverage system.
2. Demolition and construction shall conform to Chapter 14 of the International Fire Code (IFC).
3. Provide Standby and Emergency power systems and fuel systems in accordance with the requirements of the International Building Code (IBC/IFC), in particular city of Bellevue (COB) Amended IBC/IFC 604.
4. Provide a communication amplification (Building radio coverage) system in accordance with COB Amended IFC 511 (23.11.511).
5. The Fire Command Center shall have direct exterior access at an approved location near the front address of the building.
6. Provide information on the size of the generator fuel tank and refueling operation.
7. Bellevue Amendments to the IFC are in Bellevue City Code (BCC) 5749 and IBC are in BCC 5750.
8. Provide distinct addressing for access to the building entrance.
9. Review Bellevue Fire Amendments to the International Fire Code at Bellevue City Code Ordinance 5749.
10. Review Bellevue Fire Department Development Standards at www.cityofbellevue.org – go to Departments, Fire Department, Fire Prevention, and Development Standards.
11. Fire truck and ladder truck outrigger loading shall be required over any of the parking garage that has a road over it or vaults and tanks in the roadway. See Structural Slab Design Loading dated 6/08 in development services handouts.

Automatic Sprinkler Systems

1. The sprinkler and standpipe system is addressed in Chapter 7 of the Bellevue Fire Department Development Standards (BFDDS). Bellevue has adopted the 2007 edition of NFPA-13, but does not grant a reduction in remote area for quick response sprinklers. Wet systems in commercial spaces must be designed as a minimum to a criterion for Light Hazard (0.10 gpm/sq. ft. over the remote 1,500 sq. ft. of floor area).
2. Two separate water supplies are required for the sprinkler/standpipe system in accordance with COB Amended IFC 903.3.5.2. One must be a permanent City water main connection and the second must be a dedicated reservoir. The reservoir must be sized in accordance

with IFC 903.3.5.2 and must comply with NFPA-22 (Water Tanks) and be equipped with an automatic fill assembly that will refill the tank within 4 hours.

3. Two independently driven fire pumps shall be provided and sized for the sprinkler demand and standpipe operations. One pump shall provided for the city water supply and one shall be provided for the onsite reservoir.
4. The onsite water supply shall be 40,000 gallons net usable water to the centerline of the fire pump. See Bellevue Amendment 23.11.903.3.5.2.
5. When large diameter pressure reducing valves are utilized to control zone pressures, two valves shall be installed in parallel, each capable of handling the required water flow.
6. Fire pumps per NFPA 20- 2007 edition. Backup power and primary and secondary wiring shall be provided, routed and protected per NFPA 20- 2007 edition requirements.
7. Pressure Reducing Valves must be set in accordance with COB Amended IFC 905.3.8.
8. Sprinkler piping on floors up to the 2nd floor shall be sized for street pressure only.
9. Adequate heat to maintain 40 deg. F must be provided. Heat tracing is not a substitute other engineering options shall be used. Heat trace will only be allowed in small areas.
10. A Post Indicator Valve shall be located outside of the building for control of the fire protection water supply.

Standpipes / FDC

1. Standpipe systems shall be provided in accordance with Amended IFC 905 and Chapter 7 of the BFDDS. In particular:
 - a. The standpipe risers shall be interconnected and have an isolation valve for each standpipe.
 - b. Two four-way fire department connections shall be provided on separate streets, piped to separate standpipe risers.
 - c. At least one fire department connection shall be piped to the standpipe side of an isolation valve.
 - d. FDC's must be located at an approved location.
 - e. A Fire hydrant must be located within 50ft of the FDC
 - f. FDC must be labeled in accordance with Amended IFC 912.4
 - g. Roof and floor remote areas must be within 200 feet of hose travel distance from a protected standpipe hose connection in accordance with Amended IFC 905.4.
2. The design of the standpipes shall meet three design points: 1) Provide 750 gpm at a minimum residual pressure of 100 psi. 2) Provide 300gpm at 175psi at the outlet of the hydraulically most remote hose connection. 3) Sprinkler system demand with required hose streams. COB Amended 905.3.8 and Chapter 7 of BFDDS.
3. Location of Class I standpipe hose connections must be in accordance with COB Amended IFC 905.4.

Fire Alarms

1. Fire Alarm systems shall be provided in accordance with IFC 907, Chapter 8 of the BFDDS, and NFPA 72 – 2007 edition. In particular:
 - a. Smoke detectors shall be installed to comply with IFC 907. They shall be connected to the Fire Alarm system.
 - b. Sprinkler zoning by floor is required.
 - c. Wiring for the fire alarm systems shall meet the requirements of NFPA 72 with regards to Survivability, and IBC 909.20.6 as relates to shaft pressurization control wiring, and BFDDS chapter 11.
2. This project will require one Fire Command Center (FCC) with direct exterior access at an approved location near a core stairwell.
3. The FCC is required to contain the following equipment per COB Amended IFC 509 and BFDDS.
 - a. The voice alarm and public address system panel
 - b. Fire Department communications panel and 8 portable phone handsets
 - c. The fire-detection and alarm system annunciator panel
 - d. Annunciator visually indicating the location of the elevators and whether they are operational
 - e. Status indicators and controls for the air-handling systems
 - f. Controls for unlocking all stairway and electronic doors simultaneously
 - g. Sprinkler valve and water-flow detector display panels
 - h. Emergency and standby power status indicator, manual start and transfer features
 - i. Telephone for Fire Department use with controlled access to the public telephone system
 - j. Fire pump status indicators and remote manual start must be provided separate of the fire alarm panel.
 - k. Schematic building plans indicating the typical floor plan and detailing the building core, means of egress, fire protection systems, firefighting equipment, and fire department access suitable for marking with a grease pencil
 - l. Work table or counter
 - m. Shaft pressurization systems status indicators and controls
 - n. Annunciation identification
 - o. Emergency Operational Plan
 - p. Smoke control documents required per Chapter 11 of the BFDDS.
4. An approved two-way, fire department communication system designed and installed in accordance with NFPA 72 shall be provided for fire department use as required by IFC 907.2.12.3 and 1007.6.3.

Smoke Control

1. A smoke control system that complies with IBC 909 and the BFDDS must be provided. A Smoke Control system is required for high rise buildings (IBC 403.1), underground buildings (IBC 405.5), and Atriums (IBC 404.4).

2. A separate permit is required for a smoke control system. Submittal requirements are detailed in Chapter 11 of the BFDDS and Number Sheet 42. Drawings must include all zones, all required equipment, definitions of fans and dampers, positive indication supervisory switches, wiring in raceways, a listed smoke control panel (SCP), and a smoke control functional matrix. Note that the BFDDS include many details and standard features applicable to the Smoke Control Panel and procedures applicable to the Special Inspector.
3. Provide 2hr protection of control and power wiring for stair and elevator pressurization systems per COB Amended 909.20.6.1. Wiring for the fire alarm systems shall meet the requirements of NFPA 72 on Survivability, and IBC 909.20.6 as relates to shaft pressurization.
4. Pressurization of all stairways leading into a parking garage that descend more than 30 ft. below grade is required, as well as pressurization of any pressurized stair tower above grade that extends below grade, and the reverse combination.

Generator

1. The installation of generators, fuel tanks and fuel piping shall require a review and approval by both the fire and mechanical departments.
2. Storage of more than 660 gallons of diesel fuel for emergency generators shall be in a Group H occupancy.
3. The requirements of IFC 3404 must be met. In particular:
 - a. Section 3404.2.9.6:
 - i. Fill connections shall be located outside buildings, at least 10 feet from building openings and property lines, provided with a liquid-tight cap, and protected from possible vehicular damage. A metallic pipe shall extend to within 6 inches of the bottom of the tank to minimize the generation of static electricity.
 - ii. Tank overfill prevention shall be provided, including an audible or visual alarm signal at the fill location to indicate when the tank is 85 percent full and automatic shut off of the flow of fuel when the quantity of liquid in the tank reaches 90 percent of tank capacity.
 - iii. The fill pipe shall be provided with a means for making a direct connection to the supply tank vehicle's fuel-delivery hose so that the delivery of fuel is not exposed to the open air during the fueling operation. When any portion of the fill pipe exterior to the tank extends below the level of the top of the tank, a check valve shall be installed in the fill pipe not more than 12 inches from the fill hose connection.
 - iv. An approved spill container having a capacity of not less than 5 gallons shall be provided for the fill connection for above ground tanks.
 - b. 3404.2.7.3:

- i. Normal vents shall discharge to the outside, not less than 12 feet above the adjacent ground level, at least 5 feet from building openings and property lines, and not under eaves or building overhangs.
 - ii. Approved flame arrestors shall be installed in normal vents.
4. One or more portable fire extinguishers having a rating of not less than 20-B shall be located not less than 10 feet or more than 50 feet from the tank storage area.
5. A fire department permit is required to store, handle or use Class II or Class III-A combustible liquids in excess of 25 gallons inside a building.

Additional Requirements

1. Provide a construction standpipe as required by Chapter 14 of the IFC. A construction standpipe system shall be installed when the progress of construction is not more than 40 feet in height above the lowest level of fire department access or two or more levels below grade. Such standpipe systems shall be extended as construction progresses to within one floor of the highest point of construction having secured decking or flooring.
2. Provide fire extinguishers in accordance with IFC 1415 and 23.11.906.1. Verify that all portions of the building are within 75 feet of foot travel of a fire extinguisher.
3. This project will require the services of a designated, full-time Safety Officer at the discretion of the Fire Department at any time during construction. This job assignment may be mobilized for intermittent periods during the course of construction. The Safety Officer shall be an employee of the Developer or the General Contractor, with qualifications approved by the Fire Department. This person's primary responsibility shall be the supervision of the safety conditions on the project site. They will maintain a daily log, recording all related events and personnel involved, including safety system shutdowns and startups. This person will regularly report his findings and observations to the Fire Inspector.
4. This high rise building is required to have a Fire Safety and Evacuation Plan in accordance with IFC 404 reviewed and approved by the Fire Department prior to final occupancy.
5. Before the parking garage can be used for storage, vehicular parking or construction shacks the permanent automatic sprinklers, fire alarms, emergency lighting and exit lighting shall be installed. The Fire Marshal may allow temporary water supplies or other protection systems but specific prior approval is required.
6. Before any floor of the building can be occupied for use the following shall be completed.
 - a. Permanent automatic sprinkler protection shall be installed on the occupied floor and all floors below.
 - b. The permanent fire alarm system shall be installed on the occupied floor and all floors below.
 - c. The elevator recall system shall be installed on all floors
 - d. The emergency generator shall be in service.
 - e. The smoke control system shall be complete and functional
 - f. An approved evacuation and life safety plan shall be in place

Other requirements may be imposed by the fire department as needed to insure life safety.

BFDDS- Bellevue Fire Department Development Standards-
http://www.bellevuewa.gov/fire_prevention_development_standards.htm

IFC 2007 Amendments -
http://www.bellevuewa.gov/pdf/Fire/2007_Bellevue_Fire_Code_Amendments_.pdf

A fire department permit is required to store, handle or use Class II or Class III-A combustible liquids in excess of 25 gallons inside a building.

Provide a construction standpipe as required by Chapter 14 of the IFC. A construction standpipe system shall be installed when the progress of construction is not more than 40 feet in height above the lowest level of the department access or two or more levels below grade. Such standpipe systems shall be extended as construction progresses to within one floor of the highest point of construction having secured decking or flooring.

Provide fire extinguishers in accordance with IFC 1415 and 23.11.008.1. Verify that all portions of the building are within 75 feet of foot travel of a fire extinguisher.

This project will require the services of a designated, full-time Safety Officer at the discretion of the Fire Department at any time during construction. This job assignment may be modified for intermittent periods during the course of construction. The Safety Officer shall be an employee of the Developer or the General Contractor, with qualifications approved by the Fire Department. This person's primary responsibility shall be the supervision of the safety conditions on the project site. They will maintain a daily log, recording all related events and personnel involved, including safety system shutdowns and status. This person will regularly report his findings and observations to the Fire Inspector.

This high rise building is required to have a Fire Safety and Evacuation Plan in accordance with IFC 404 reviewed and approved by the Fire Department prior to final occupancy.

Before the parking garage can be used for storage, vehicular parking or construction stacks the permanent automatic sprinkler, fire alarm, emergency lighting and exit lighting shall be installed. The Fire Marshal may allow temporary water supplies or other protection systems but specific prior approval is required.

5. Before any floor of the building can be occupied for use the following shall be completed:

The elevator recall system shall be installed on all floors
The emergency generator shall be in service
The smoke control system shall be complete and functional
An approved evacuation and life safety plan shall be in place