



DEVELOPMENT SERVICES DEPARTMENT
ENVIRONMENTAL COORDINATOR
450 110th Ave NE., P.O. BOX 90012
Bellevue, WA 98009-9012

MITIGATED DETERMINATION OF NON-SIGNIFICANCE

PROPONENT:

Michael Chen, Group Mackenzie

LOCATION OF PROPOSAL:

15015 Main Street, Bellevue, WA

DESCRIPTION OF PROPOSAL:

Application for Design Review approval and a SEPA determination to redevelop the existing 16 acre commercial center. The proposal includes renovation of the two existing buildings and construction of two new buildings, along with parking lot and landscape improvements. This application does not include the small parcel at the corner of Main Street and 148th Avenue SE, which is under separate ownership.

FILE NUMBER: 10-121806-LD

The Environmental Coordinator of the City of Bellevue has determined that this proposal does not have a probable significant adverse impact upon the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(C). This decision was made after the Bellevue Environmental Coordinator reviewed the completed environmental checklist and information filed with the Development Services Section. This information is available to the public on request.

- There is no comment period for this MDNS. There is a 14-day appeal period. Only persons who submitted written comments before the MDNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's office by 5:00 p.m. on _____.
- This MDNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on the DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's Office by 5 p.m. on 4/14/11.
- This MDNS is issued under WAC 197-11-340(2) and is subject to a 14-day comment period from the date below. Comments must be submitted by 5 p.m. on _____. This MDNS is also subject to appeal. A written appeal must be filed in the City Clerk's Office by 5 p.m. on _____.

This MDNS may be withdrawn at any time if the proposal is modified so that it is likely to have significant adverse environmental impacts; if there is significant new information indicating, or on, a proposal's probable significant adverse environmental impacts (unless a non-exempt license has been issued if the proposal is a private project); or if the MDNS was procured by misrepresentation or lack of material disclosure.

Carole V. Holland
Environmental Coordinator

3/31/11
Date

OTHERS TO RECEIVE THIS DOCUMENT:

State Department of Fish and Wildlife
State Department of Ecology, Shoreline Planner N.W. Region
Army Corps of Engineers
Attorney General
Muckleshoot Indian Tribe



**City of Bellevue
Development Services Department
Land Use Staff Report**

Proposal Name: **Kelsey Creek Center**

Proposal Address: 15015 Main Street

Proposal Description: Application for Design Review approval and a SEPA determination to redevelop the existing 16 acre commercial center. The proposal includes renovation of the two existing buildings and construction of two new buildings, along with parking lot and landscape improvements. This application does not include the small parcel at the corner of Main Street and 148th Avenue SE, which is under separate ownership.

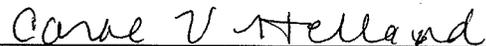
File Number: **10-121806-LD**

Applicant: Michael Chen, Group Mackenzie

Decisions Included: Process II, Combined Design Review and SEPA Determination

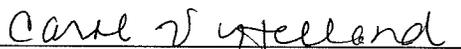
Planner: Mike Upston, AICP 425/452-2970

State Environmental Policy Act Threshold Determination: **Mitigated Determination of Non-Significance (MDNS)**



Carol V. Helland, Environmental Coordinator
Development Services Department

Director's Decision: **Approval with Conditions**
Michael A. Brennan, Director
Development Services Department

By: 

Carol V. Helland, Land Use Director
Development Services Department

Notice of Application: 10/21/10
Decision Publication Date: 3/31/11
Appeal Deadline: 4/14/11
Expiration of Vesting for Decision: 3/31/13

For information on how to appeal a proposal, visit Development Services at City Hall or call (425) 452-6800. Comments on State Environmental Policy Act (SEPA) Determinations can be made with or without appealing the proposal within the noted comment period for a SEPA Determination. Appeal of the Decision must be received in the City's Clerk's Office by 5 PM on the date noted for appeal of the decision.

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Attachments:

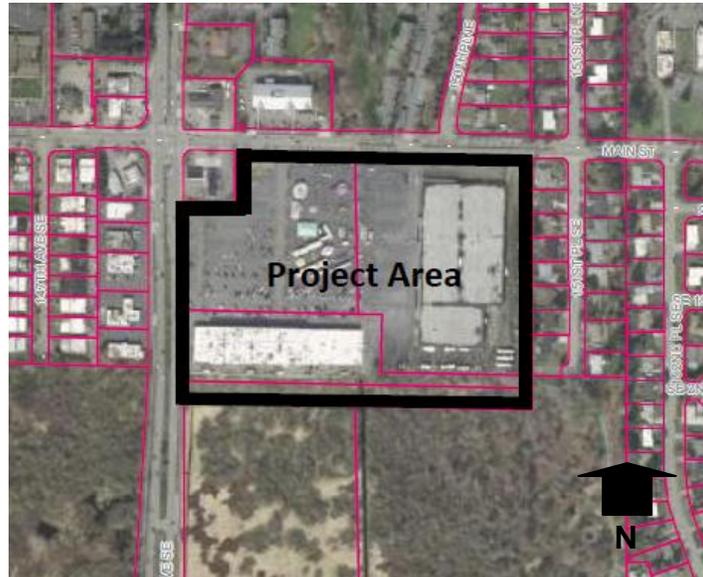
Attachment A: Comprehensive Plan Consistency

Attachment B: Project Plans & Drawings

I. REQUEST/PROPOSAL DESCRIPTION

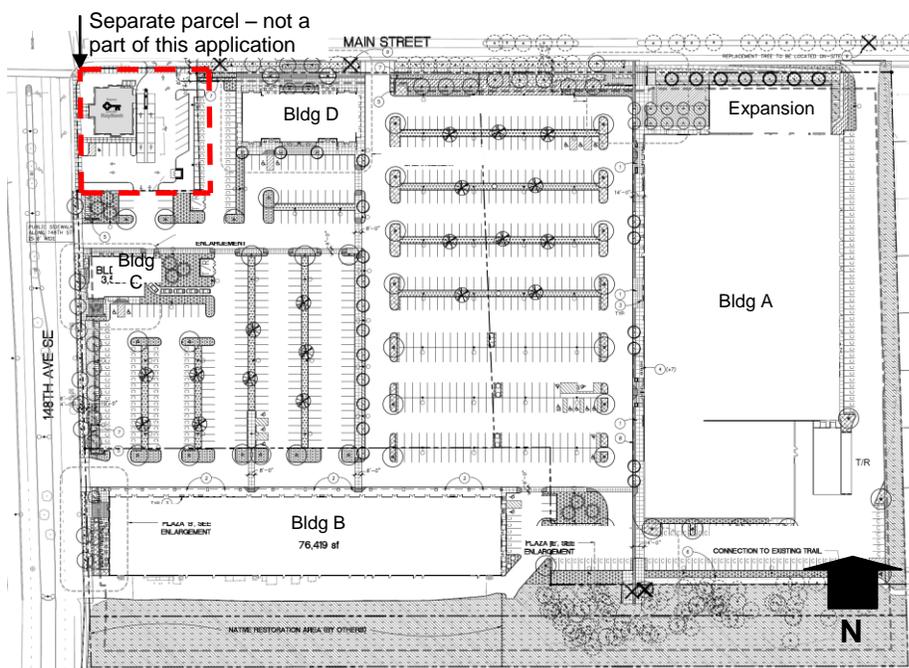
A. Design Intent

The application proposes redevelopment of Kelsey Creek Center. The existing K-Mart building (Building A on the site plan below) would be renovated and a 9,175 square foot expansion on its north side is proposed, bringing the total square footage to 115,634 square feet. Multiple tenants are planned to occupy Building A, with tenant size limits dictated by the concomitant zoning agreement referenced in Sub-section III.A.1 of this report. The proposed renovation of the existing 76,419 square foot Kelsey Creek Center building



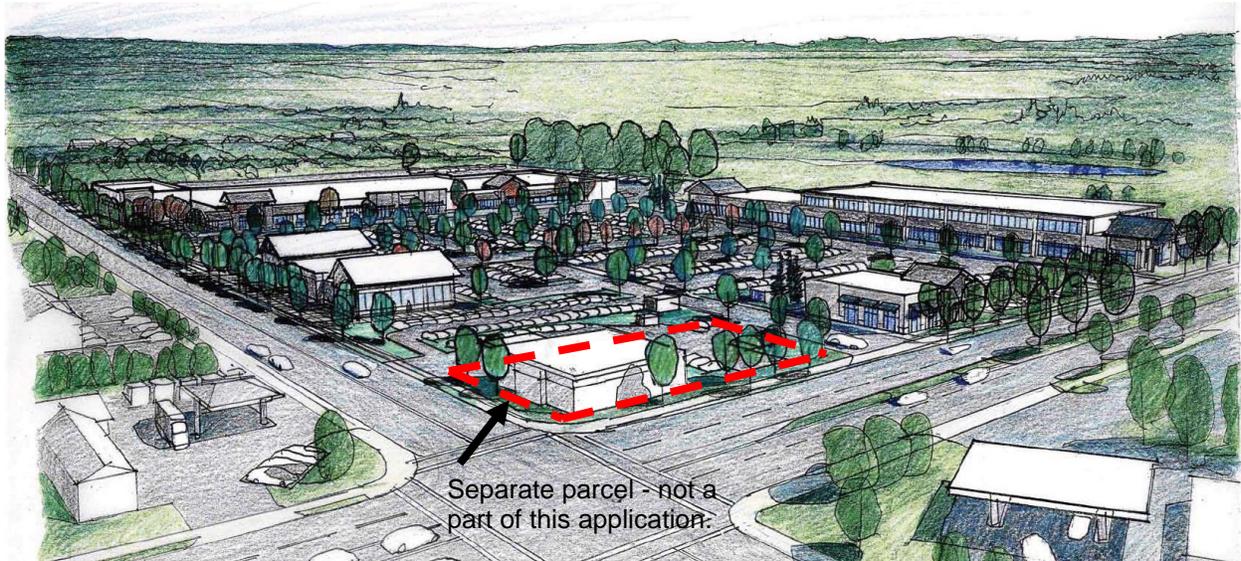
(Building B) includes a new façade and enhancement of the pedestrian pathway along the north elevation. Building C is proposed as a stand-alone 3,526 square foot fast food restaurant with drive-thru. Building D would be an 8,362 square foot retail building planned to accommodate two future tenants. The total of all existing and proposed floor area is 203,941 square feet.

B. Site Design



The proposed site renovation includes a new parking lot, storm water detention, and pedestrian facilities that connect 148th Avenue SE, Main Street and the project to Lake Hills greenbelt and the neighborhood. Buffer landscaping per the Land Use Code is proposed around all portions of the Center. Plantings within those buffers will consist mainly of native shrubs and columnar trees. A majority of the existing perimeter buffers along the east and south

property lines will remain. See the site plan above and the illustration at the top of the next page.



C. Building Design

The renovations to the two existing buildings and the two proposed new buildings are designed to reflect what's commonly referred to as a Pacific Northwest style, with masonry block, horizontal wood siding, rough cut timber, corrugated metal sheet panels, and standing seam metal roof panels. Anticipated sign placement is depicted with dashed lines on the drawings below. A sign permit which specifies design details, size, and structural details consistent with this Design Review decision would have to be approved before any signs could be installed.



Bldg A (Proposed Renovation & Addition)



Bldg B (Proposed Renovation)



Bldg C (Proposed New)



Bldg D (Proposed New)

II. SITE DESCRIPTION, ZONING, & LAND USE CONTEXT

A. Site Description

The existing commercial center was constructed in the late 1960's. Building A has been vacant for a number of years. Building B is a fully occupied two story building with office and retail tenants situated along the south property line. The remainder of the site contains parking, plazas, walkways, and landscaping.

B. Zoning

The project site's base zoning is Community Business (CB) with the Transition Area Design District (TADD), Community Retail Design District (CRDD), and the concomitant zoning agreement (CZA) as zoning overlays (see maps on this page). Zoning surrounding the project site is:

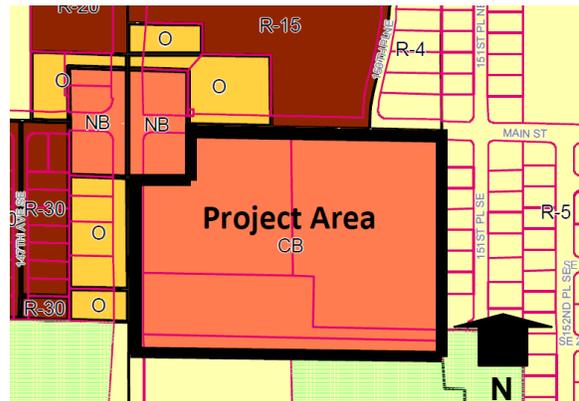
East: Single Family Residential (R-5).

South: Single Family Residential (R-1), developed as park/open space.

West: Office (O) and Neighborhood Business (NB).

Northwest: Neighborhood Business (NB) and Office (O).

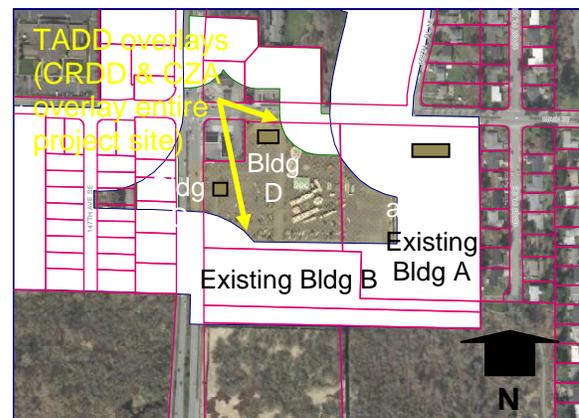
Northeast: Multifamily Residential (R-15) and Single Family Residential (R-4).



C. Land Use Context

The parcel at the southeast corner of Main Street and 148th Avenue SE is an abandoned Shell gas station that is planned for redevelopment as a KeyBank branch office (refer to Design Review application 10-121139-LD for more information).

A variety of office and retail uses are located across 148th Avenue SE west of the project site.



The north side of Main Street contains office, retail and social service uses, along with multi-family and single family residential uses beyond. A well established single family residential neighborhood is located to the east and northeast. Lake Hills greenbelt is to the immediate south.

III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS

A. General Provisions of the Land Use Code

1. Use

Uses are regulated by LUC 20.10.400 (Use Charts), LUC 20.20 (General Development Requirements), and the concomitant zoning agreement created under Rezone 09-113801-LQ adopted by City ordinance number 5946. The project proposal is permitted subject to Design Review approval as required by the concomitant zoning agreement and by virtue of its underlying Community Business (CB) zoning designation, as well as the Transition Area Design District (TADD, LUC 20.25B) and Community Retail Design District (CRDD, LUC 20.25I) zoning area overlays.

2. Dimensional Requirements

All applicable dimensional requirements of the Land Use Code (LUC 20.20 and LUC 20.25B.040) and concomitant zoning agreement are met. Refer to the table below and following paragraphs for specific information.

	Permitted/Required	Proposed
Building Height	30 feet basic maximum height. Up to 45 feet with bonuses. (LUC 20.20 010 & 20.25B.040.A)	35 feet for the Building A & B remodels, with use of LUC 20.25B.040.A.3.c & d to earn additional building height above 30 feet. Buildings C & D will be less than 30 feet tall. See further discussion on page 7, Building Height.
Setbacks	Front/148th Avenue SE: None required for Building C which is sited outside the TADD. 20 feet required for existing Building B inside the TADD. Front/Main Street: None required for Building D proposed outside the TADD. 20 feet required for the Building A addition inside the TADD. East property line: 20 feet required for the existing Building A. South property line: 20 feet required for Buildings A & B inside the TADD. (LUC 20.20.10 & 20.25B.040.C where the Transition Area Overlay District applies) (LUC 20.20 010 & 20.25B.040.B)	Front/148th Avenue SE Bldg C: 10 feet Bldg B: 25 feet (existing) Front/Main Street Bldg D: 25 feet Bldg A Addition: 20 feet East property line Bldg A: 70 feet (existing) South property line Bldg A: 180 feet (existing) Bldg B: 110 feet (existing)

Lot Coverage	No limit (LUC 20.20.010)	27%
Impervious Surface	85% maximum (LUC 20.20.010)	77%
Parking	4 stalls/1,000 square feet 203,941 SF = 204 x 4 = 816 stalls (LUC 20.20.590)	816 stalls
Landscape	1) Street frontage and perimeter property lines: 20 feet width minimum. 2) Parking Areas: Type V 35 SF/stall x 816 stalls = 28,560 SF (LUC 20.20.520 & 20.25B.040.C)	1) 20+ feet wide, including public-oriented plazas. 2) Type V, 36,780 SF <u>Conditions of approval are included in Section X of this report addressing street tree damage, tree protection, and landscape installation & maintenance assurance requirements.</u>
Queue at Drive-Up	7 car lengths x 12 feet/car = 84 feet (LUC 20.20.590.K.5)	84 feet
Recycling & Solid Waste Collection Areas	At least one area of a size measuring at least 929 SF: Retail: 869 SF Office: 60 SF Total Required = 929 SF (LUC 20.20.725 & LUC 20.25B.040.F)	Total proposed: 1,400 SF See further discussion on page 7, Recycling & Solid Waste Collection Area.
Mechanical Equipment	Locate on the roof or below grade and visually screen, unless requirement is modified by the City for projects requiring discretionary approval. (LUC 20.20.525.C.5 & 6, and 20.25B.050.E)	All mechanical equipment is proposed to be located on the rooftops and screened per LUC requirements. See further discussion on page 7, Mechanical Equipment.
Loading	Off-street, at least 10'x55' plus maneuvering length and accessible from a public street. (LUC 20.20.590.K.4)	Provided as required. See further discussion on page 7, Loading.

Building Height

LUC 20.25B.040.A.3.c (pitched roof) is being applied to allow an additional 5 feet of building height above the 30 feet basic maximum permitted. This is for pitched roof accent features which respond to the Community Retail design guidelines by providing strong architectural elements and a sense of place for the Kelsey Creek Center. Further, the proposed addition on the north side of Building A is proposed to be 35 feet tall. Here, LUC 20.25B.040.A.3.f is being applied; the existing grade of Main Street is approximately 15 feet higher than the finished floor of the proposed addition.

Recycling & Solid Waste Collection Areas

Screened recycling and trash collection for Building A will occur on the back/east side. Building B will continue to use its back side as well, but since the area is viewable from the adjacent Larson Lake greenbelt park and no screening is proposed, a condition of approval is included in Section X of this report requiring that screening be provided for all existing and proposed recycling and solid waste collection areas associated with Building B. Buildings C and D both have individual trash enclosures designed to be visually compatible with the center. Both enclosures are 344 square feet, for a total of 688 square feet. Designated areas on the back of Buildings A and B bring the total allocated square footage to 1,400 square feet. In addition, a condition of approval is included in Section X of this report requiring confirmation from Allied Waste prior to building permit approval that they endorse the proposed size and function of the recycling & solid waste collection areas, and that garbage containers will be kept out of view.

Mechanical Equipment

Roof-mounted mechanical equipment will be proposed for Building A when future tenants locate there. No modifications are proposed to the existing rooftop mechanical equipment for Building B, though any future proposals would be reviewed under a Land Use Exemption (LUX) from Design Review. Buildings C and D are designed with parapets to screen future mechanical units, the locations of which will be shown on future construction permit plans. Any future permit plans for rooftop equipment atop any of the buildings will need to show the location of each piece of equipment and demonstrate that it is not visible from below or from the tops of surrounding buildings. See related condition of approval in Section X of this report. Another condition is included in Section X to address ground-mounted mechanical equipment should any be proposed in the future.

Loading

Building A: The tenants in Building A will use a screened loading area at the east/back side of the building, with small deliveries (FedEx, etc.) at the front of the store.

Building B: Building B is an existing building with existing loading area on its south/back side. No modifications are proposed.

Building C: This building is intended to serve as a small fast food restaurant. Loading will be short term and occur at the front (south) side of the restaurant.

Building D: This building is intended to have retail tenants ranging from 1,000 - 2,000 SF each. Loading will be short term and occur at the front (south) side of the building.

In addition to the loading provisions proposed, a condition of approval is included in Section X of this report which expressly prohibits loading from the public right of way.

B. Transition Area Design District Design Guidelines (LUC 20.25.B.050)

Site Design Guidelines

1. *Whenever possible, vehicular access should be designed so that traffic is not directed through an abutting residential district of lower intensity.*

Kelsey Creek Center is an existing development constructed in the 1960's. 148th Avenue SE and Main Street are collector arterials which funnel vehicles through residential districts. Vehicular access to Kelsey Creek Center will be from 148th Avenue SE and Main Street. Access directly from abutting residential neighborhoods is not proposed.

2. *Loading and refuse collection areas should be on the side of a building facing away from an abutting residential district of a lower intensity, but not in a front yard setback.*

Buildings B, C, and D all have loading and refuse collection areas away from neighboring homes. Since loading and refuse collection for Building A already exists at a location facing an abutting residential neighborhood, an area screened behind a new 10 foot tall wall is proposed here. There are no other locations that would not significantly compromise the building's function. In addition, an existing mature landscape buffer consisting of evergreen shrubs and trees runs along the east property line.

3. *In addition to the minimum requirements of LUC 20.20.520, site development should maximize the retention of existing significant vegetation in order to soften the visual impact on adjacent residential uses.*

The property currently has minimal vegetation. The proposed site plan incorporates landscaping around the perimeter and landscape areas throughout the redesigned parking lot. The new groundcover, shrubs and trees will soften the visual impact of the surface parking area and provide a visually pleasing environment.

4. *Surrounding vegetation, topography, street patterns, parking configuration and building massing should be considered in order to result in a compatible fit between the proposed development and existing residential development.*

The proposed renovation of Kelsey Creek Center has taken the surrounding neighborhoods, street patterns and building massing into consideration. The result is a revised parking lot layout that includes much more landscaping than exists today, improves vehicular flow and is pedestrian friendly. In addition, renovations are proposed for Buildings A and B to make the center a more pleasant experience, along with two new retail buildings (Buildings C and D) nicely oriented to the street.

Building Design Guidelines

1. *Building surfaces facing abutting residential districts should be clad with materials which are similar to or compatible with surrounding uses, and which minimize reflected lighting.*

The proposed renovation of Buildings A and B, as well as the proposed new Buildings C and D, are clad with materials which are compatible with the surrounding area, such as wood and metal panel siding, non-reflective glass, and earth tone paint colors. These materials will create a soft and natural appearance that will mesh well with the nearby residential and commercial areas.

2. Building facades should incorporate elements such as stepbacks, offsets, angled facets, deep roof overhangs, recesses and other architectural features which serve to break down the scale. The larger the building, the greater the number and variety of such elements that may be necessary to achieve the effect of diminishing scale.

The proposed building facades incorporate setbacks and offsets that break down the scale of the buildings and create visual relief. Vertical elements such as pedestrian-scale canopies, stepped roof forms, and pitched roofs are utilized to diminish the scale of the buildings as well.

3. Pitched roof forms are preferred in order to enhance the compatibility with nearby residential areas. However, under certain circumstances, a stepped roof form could achieve a similar effect.

Pitched roofs are proposed for all of the buildings to reflect the surrounding residential neighborhood.

4. Communication dishes greater than one meter (3.28 feet) in diameter should not be visible from adjacent residential districts.

There are existing wireless communication facilities located on the roof of Building B which can be seen from Larson Lake to the south, but are not visible from adjacent residential homes. No new communication dishes are proposed with this application.

5. Materials and colors used on the building facades should be compatible with nearby residential buildings and the surrounding natural environment; however, colors and materials used for the purpose of accent may be approved.

The colors proposed for the buildings are earth tones which compliment nearby building materials and the natural setting of the Larson Lake greenbelt to the south, and rough cut timber is proposed for the canopy columns and structure to introduce a Pacific Northwest aesthetic.

C. Community Retail Design District Design Guidelines (LUC 20.25.I.040)

Building Design Guidelines

1. All buildings within a multi-building complex should achieve a unity of design through the use of similar architectural elements, such as roof form, exterior building materials, colors, and window pattern.

2. Individual buildings should incorporate similar design elements, such as surface materials, color, roof treatment, windows and doors, on all sides of the building to achieve a unity of design.

The buildings achieve a unity of design through the use of similar architectural elements. Pitched roof forms, metal roof material, rough-sawn lumber, masonry, and earth tones are proposed for all of the buildings to weave the site together and blend with the surrounding community. Refer to the perspective and building elevations on page 3 of this report and within the attached plan set for more graphic information.

Site Design Guidelines

- 1. Where a perimeter landscape area required under LUC 20.20.520.F may be relocated under Note (1) of that section, some or all of the relocated area should be used to provide a landscaped courtyard. Any relocated landscape area should be visible from the public street. The courtyard should be protected from wind on two sides and in sun during part of the day. Seating shall be provided.*
- 2. Parking areas should be designed to minimize conflicts between pedestrian and vehicular movements. Parking area landscaping should be used to define and separate parking, access, and pedestrian areas within parking lots.*
- 3. The landscape design for the site should include plantings which emphasize the major points of pedestrian and vehicular access to the site.*

No perimeter landscape areas are proposed to be relocated, and parking areas have been designed to minimize the conflicts between pedestrians and vehicles, with additional landscape areas to enhance the pedestrian experience throughout the site. Further, pedestrian access is provided throughout and plaza spaces have been added to help define the site and create an amenity for the neighborhood.

- 4. Site features such as fences, walls, refuse and recycle enclosures, and light fixtures should be designed to be consistent with the scale and architectural design of the primary structure(s). Such site features should be designed and located to contribute to the pedestrian environment of the site development.*

Low masonry walls placed along select segments of the project perimeter and masonry trash enclosures will be consistent with the scale and architecture of the proposed buildings. Proposed lighting is scaled appropriately, with tall poles for the parking areas and lower fixtures along walkways and buildings. Exterior building lighting will be from the same style throughout the site to maintain cohesiveness. However, the existing chain link fence along the south property line is in poor condition, is unsightly, and would not be consistent with the balance of the improvements proposed for the property. Therefore, a condition of approval is included in Section X of this report requiring a new fence along the south side of the property. Further, the refuse and recycling area behind Building B is currently exposed to the adjacent Larson Lake greenbelt park. Screening is addressed under Section III.A.2 of this report.

- 5. Loading areas should not be located between the building and the street unless there is no alternative location possible. Loading areas, if located between the building*

and the street, should be oriented away from the street and screened to minimize views of the loading area from the street and sidewalk. Loading areas should not be located on the side of a building which faces toward a residential use.

Loading and refuse collection areas for Buildings A and B are already located behind the buildings, away from Main Street and 148th Avenue SE. In addition, a new 10 foot tall screen wall is proposed to be added behind Building A, and the existing landscape screening just beyond along the east property line will remain since it provides a thick buffer between the existing residential neighborhood and Building A. Finally, while the loading and refuse collection areas for Buildings B, C, and D are away from homes, the existing area behind Building B faces the Larson Lake greenbelt park; refer to discussion for how this is addressed in Section III.A.2 of this report.

6. In multiple-building complexes, buildings should be located to facilitate safe and comfortable pedestrian movement between buildings. On sites which are adjacent to other properties within the Community Retail Design District, building location should be chosen to facilitate pedestrian and vehicular connections to buildings on those adjacent properties.

7. In locations and districts specifically described in the Comprehensive Plan Urban Design Element Figure UD.1, consideration should be given to locating buildings closer to the public street with entrances to the buildings from the public sidewalk, with no intervening parking or driving area. Corner locations are particularly appropriate for this treatment.

8. Opportunities should be found for safe, convenient, and pleasant pedestrian connections to existing transit facilities. Where needed, shelters and lay-bys for transit vehicles should be incorporated into the site development.

Building placement is influenced by the property owner's desire to reuse the existing Buildings A and B, the City's desire for buildings to be near and facing the public sidewalk, and the ability to achieve better circulation and parking. As stated for items 1 – 3 above, parking areas have been designed to minimize the conflicts between pedestrians and vehicles, with additional plaza, walkway, and landscape areas to enhance the pedestrian experience throughout the site. Further, pedestrian access is provided to and from the public sidewalks where existing transit services are provided.

D. Community Retail Design District Design Standards (LUC 20.25.1.050)

Special Corner Feature

If the property is located at the intersection of two arterial streets, the site development shall incorporate a special corner feature at the corner of the site. A special corner feature can be a landscape feature, seasonal color planting area, sculpture or water feature. The feature should provide a visual landmark and some amount of seating area. If the property is not located at an intersection, a similar feature should be considered in conjunction with a transit stop or at the primary access point to the site.

While the project site is oriented to the intersection of 148th Avenue SE and Main Street, the Key Bank property is a separate parcel on the actual street corner. Still, the site plan

proposed for this Kelsey Creek Center project incorporates four street-oriented plaza spaces to enhance the public/pedestrian experience and call attention to this prominent location. One plaza is located on 148th Avenue SE adjacent to the remodeled corner of Building B, a second plaza is oriented to 148th Avenue SE in front of Building C, a third plaza is oriented to Main Street in front of Building D, and a fourth plaza is located on Main Street adjacent to the addition to Building A. Plaza features include seating, landscaping, community bulletin boards, bike racks, and water features. In addition, both plazas serve as entry points/ pedestrian nodes into the site.

Building Design Standards

- 1. The design of buildings shall incorporate elements such as special architectural details, distinctive color schemes, special art and other features, which are sensitive to and enhance the surrounding area and serve to distinguish the complex from other retail complexes in the City.*
- 2. The sides of a building which face toward a public street shall include public entrances to the building and windows to provide visual access to the activity within the building.*
- 3. The sides of a building which face toward an adjoining property, but not toward a public street, shall include elements such as windows, doors, color, texture, landscaping or wall treatment to provide visual interest and prevent the development of a long continuous blank wall.*
- 4. Building design shall provide for architecturally integrated signage. Signs shall be in proportion to the development and oriented to the main direction access and to pedestrian movement. Signs shall meet the requirements of Chapter 22B.10 BCC, Bellevue Sign Code.*

The new and remodeled buildings are proposed to have architectural features that are sensitive to the surrounding area and distinguish the property from other commercial centers in the City. Features include a masonry product in an ashlar pattern to give a warmth and complexity to the buildings, earth-toned colors, rough-sawn lumber at main entry points to major tenants, and new light fixtures to provide more attractive, enhanced lighting that serves the property as well as providing cut-off so that light doesn't extend beyond the property.

The sides of all buildings facing the public streets will have windows to provide visual access to the activity within the buildings. The new Buildings C and D have doors, windows and interesting architectural elements on all facades since they are viewed and accessed from all four sides. The existing Buildings A and B will maintain the existing facades on the sides and back with the exception of Building A along Main Street and facing the parking lot, and Building B along 148th Avenue SE and facing the parking lot. Area dedicated for architecturally-integrated building signs is depicted on the building elevation drawings. See the building elevations on page 3 and in the attached plan set.

Internal Walkways

- 1. Walkways, of six feet in width minimum, shall be provided from the public sidewalk or right-of-way to the building. At a minimum, walkways shall be located to connect focus points of pedestrian activity such as transit stops and street crossings to the major building entry points.*

2. *Walkways shall be provided to connect with walkways or potential walkway locations on adjoining properties in the district to create an integrated internal walkway system along the desired lines of pedestrian travel. The width of the walkway should be commensurate with the anticipated level of pedestrian activity along the connecting walkway. On the sides of the building which provide public access into the building, the walkway should be wide enough to allow for sidewalk seating area or window gazing as well as pedestrian travel.*
3. *Internal walkway surfaces shall be designed to be visually attractive and distinguishable from driving surfaces through the use of durable, low maintenance surface materials such as pavers, bricks, or scored concrete to enhance pedestrian safety and comfort.*

Concrete walkways are proposed throughout the site and even to the Larson Lake greenbelt with a formal connection at the property's southeast corner. While a few of the minor walkways are only as wide as the 6 foot minimum required, 8 foot wide collector walkways, 10 foot wide walkways along buildings, and plaza spaces with special paving of up to 30 feet wide between buildings and the public sidewalk will provide for safe and pleasant access. Further, as stated for items B.1-3 above, parking areas have been designed to minimize the conflicts between pedestrians and vehicles, with additional landscape areas to provide separation and enhance the pedestrian experience.

Other Development Standards

1. *Rooftop equipment shall be visually screened pursuant to LUC 20.20.525. The design and color of equipment should be integrated with the building architectural style.*

Refer to discussion in Section III.A.2 regarding rooftop mechanical equipment and screening, along with permit requirements and an associated condition of approval.

2. *The outdoor display of building materials and similar bulky products shall be screened from views from the public street, sidewalk, and properties outside the district with a durable, solid wall or fence, or an evergreen hedge or a combination of the above. The screening requirement does not apply to the display of seasonal products of a decorative nature such as bedding plants, Halloween pumpkins, and holiday greens.*

No outdoor display is proposed, but a condition of approval is included in section X of this report to address this design standard should outdoor display be proposed in the future.

3. *Bicycle racks shall be provided on-site. Facilities for a minimum of 10 bicycles shall be provided for developments having 100 or fewer parking stalls. For each 100 additional stalls, facilities for five additional bicycles shall be provided.*

Bicycle racks are proposed at five locations for a total of 33 bicycles: near the Main Street sidewalk outside the addition to Building A, in front of Building A (west side), near the 148th Avenue SE sidewalk outside Building B, and in front of Buildings C and D. However, with 816 parking stalls, at least 46 bicycles must be accommodated by racks. Therefore, a condition of approval is included in Section X of this report requiring enough bike racks for at least 46 bicycles; more to meet the requirement of this Land Use Code section if there is an increase in parking.

IV. PUBLIC NOTICE AND COMMENT

Application Date: September 14, 2010

Application Completeness Date: October 14, 2010

Notice of Application published: October 21, 2010

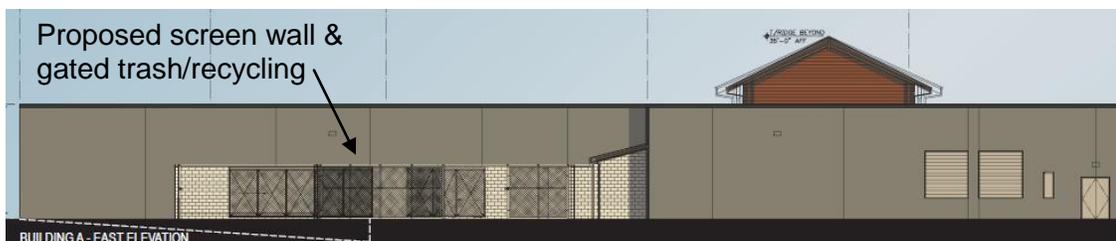
Public Notice Sign installed: October 21, 2010

East Bellevue Community Council Public Meeting held: November 3, 2010

Minimum Public Comment Period ended: November 4, 2010

Although the minimum required public comment period ended on November 4, 2010, written comments were accepted up to the date of this decision. Four comments were received and are summarized below.

- Jack Mefford, resident, wrote on 11/4/10 with general questions about parking, traffic, and the stream control gates at the entrance to the Kelsey Creek culvert. Mr. Mefford was referred to the City's technical review staff including Carl Wilson, Transportation and Rob Hutchinson, Utilities who discussed Mr. Mefford's questions further. No follow-up was received from Mr. Mefford.
- Karen Walter, Muckleshoot Indian Tribe/Fisheries Division, wrote on 11/4/10 with questions about the planned Kelsey Creek culvert replacement, status of restoration of the area south of (behind) the existing multi-tenant commercial building, on-site storm water management, impacts to salmon, and impacts of proposed lighting. Responses were provided by the applicant (see letter from Group Mackenzie dated 11/23/10 in the project file). No follow-up was received from Ms. Walter.
- Russ Ayers, resident, wrote on 11/4/10 to express his concern about the potential increase in traffic resulting from the proposed project. A traffic study was completed to address potential traffic impacts and mitigation measures, and the site plan reflects findings and recommendations contained therein (see Traffic Impact Analysis dated August 2010 and February 5, 2011 addendum from Transportation Solutions, Inc in the project file).
- Gillian Esson, resident, wrote on 11/29/10 to offer suggestions on buffering noise which may emanate from the future loading, deliveries, and garbage planned for the back/east side of Building A. In response to this comment, the plans include a reconfigured loading area screened behind a new 10 foot tall concrete block wall.



A copy of these comments and City responses is in the project file at the Development Services Department Records Room in City Hall.

V. TECHNICAL REVIEW

Clearing & Grading, Building and Fire

These reviewers have no comments or requirements for this Design Review; all comments are reserved for construction permit review.

Utilities

The City has adequate capacity for providing water, sewer and storm drainage service for this proposal. See Section X of this report for Utilities related Conditions of Approval.

Transportation

Site Access

Access to the proposed project will be provided via three driveways on 148th Avenue SE and two driveways on Main Street. One driveway on each street will be primarily for truck access. Left turns in and out of the site will continue to be prohibited on 148th Avenue SE. At the site's northeastern corner, a portion of the existing median in Main Street will be removed to allow emergency vehicle access to and from any direction, but other traffic will be continue to be restricted to right turns in and right turns out only, with a further restriction against exiting by large trucks toward the east on Main Street. On-street loading is not permitted.

Mitigation of the Development's Traffic Impacts

The Transportation Department has determined that the following street improvements are necessary to mitigate the traffic impacts of the proposed redevelopment. Each of the three mitigation measures listed below are tied to two separate conditions of approval in section X of this report. The first condition is titled *Civil Engineering Plans – Transportation*, to be complied with on plans submitted with the Clearing & Grading permit application. The second condition is titled *Transportation Mitigation Measures and Street Frontage Improvements*, to be complied with prior to issuance of the Temporary Certificate of Occupancy.

1. On 148th Avenue SE north of Main Street, the developer shall extend the existing southbound left turn lane by approximately 200 feet, plus a standard taper, approximately as shown on a sketch provided by the Transportation Department. This will require revising existing landscaping, slightly moving an existing streetlight within the raised median, installing new asphalt for the extended lane, and installing new curb and gutter consistent with the appropriate Transportation Department standard drawings. The final design must be shown on the engineering and landscape plans prior to approval of the clearing and grading permit for this development.
2. On 148th Avenue SE adjacent to and south of the site, the developer shall lengthen the existing northbound right turn lane to cover the site's entire frontage on 148th Avenue SE, plus extending south to a point approximately 50 feet south of Building B, plus a

standard taper, approximately as shown on a sketch provided by the Transportation Department. This will require installing new asphalt for the extended lane and installing new curb and gutter consistent with the appropriate Transportation Department standard drawings. In order to avoid impacts to the existing wetland south of the site, the sidewalk and planter width may be adjusted adjacent to the taper area as described below in paragraph 3.d under *Street Frontage Improvements* and approximately as shown on the referenced Transportation Department sketch.

3. In order to improve capacity for westbound left turns from Main Street into the site, the developer shall remove the existing raised island near the central driveway on Main Street. This also requires restoring the street surface as necessary and installing channelization for the center lane, as determined by the Transportation Department prior to approval of the clearing and grading permit for this development.

Street Frontage Improvements

To provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, street frontage improvements are required as a condition of approval. Each of the thirteen requirements listed below are tied to two separate conditions of approval in section X of this report. The first condition is titled *Civil Engineering Plans – Transportation*, to be complied with on plans submitted with the Clearing & Grading permit application. The second condition is titled *Transportation Mitigation Measures and Street Frontage Improvements*, to be complied with prior to issuance of the Temporary Certificate of Occupancy.

1. A street tree and street light plan is required for approval prior to completion of engineering and landscape plans. The goal is to provide the optimum number of street trees while not compromising the light and safety provided by streetlights. Street trees and street lights must be shown on the same plan with the proper separation (generally 25 feet apart) and the proper spacing from driveways (ten feet from Point A in standard drawing DEV-6). Streetlights and related hardware must be brought up to standard on both 148th Avenue SE and Main Street. In 2010, the city produced an initial lighting analysis and plan to determine the number, location, and wattage of new streetlights. That plan must be refined by the developer to the City's satisfaction prior to approval of final engineering plans, and all lighting and related hardware shown on the final plan shall be installed by the developer at the developer's expense prior to approval of any Certificate of Occupancy described in the *Phasing* section below. In addition to lighting on the site's frontage, installation by the developer will include two lights adjacent to the turn lane taper area south of the site, relocation of a light in the median of the 148th Avenue SE north of Main Street, and installation of at least one light on the north side of Main Street.
2. The Americans with Disabilities Act (ADA) requires that sidewalk cross slopes not exceed two percent. The sidewalk cross slope may be less than two percent only if the sidewalk has a longitudinal slope sufficient to provide adequate drainage. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm compliance. New curb and sidewalk shall be constructed in compliance with these requirements.

- ADA also requires provision of a consistent travel path for visually handicapped pedestrians. Potential tripping hazards are not allowed in the main pathway. Installation of colored or textured bands to guide pedestrians in the direction of travel may be advisable, subject to the requirements for non-standard sidewalk features. ADA-compliant curb ramps shall be installed where needed, consistent with standard drawings TE-12 or TE-13.
3. The sidewalks on both streets adjacent to the site and adjacent to the turn lane taper area south of the site shall be completely removed and reconstructed with a sidewalk width of at least eight feet and a planter strip of at least four feet, except as follows:
 - a. Adjacent to the retaining wall parallel to Main Street at the northeast corner of the site, the sidewalk may remain at its present width with no planter strip between the curb and sidewalk, in order to avoid reconstruction of the retaining wall. The retaining wall must be separated from the sidewalk by a fence of a design that is aesthetically acceptable to the Land Use Division.
 - b. The existing planter strip along part of the site's frontage on Main Street is approximately eight feet wide. As much as feasible, existing street trees along Main Street must be preserved by preserving the existing planter strip. This will require the developer to dedicate a narrow strip of sidewalk / utility easement to encompass the eight-foot sidewalk behind that planter strip.
 - c. From the south side of Building C to the north side of the driveway north of Building B, the sidewalk adjacent to 148th Avenue SE may have a width of six feet provided that prior to approval of final construction plans the site dimensions will be reevaluated to determine if additional sidewalk width can be achieved.
 - d. South of Building B, the sidewalk width may be reduced to six feet and the planter strip may be eliminated or reduced to less than four feet in order to connect to the existing six-foot sidewalk south of the site while avoiding impacts to the existing wetland.
 4. The design and appearance of the public sidewalk on both adjacent streets shall comply with the standards and drawings in the Transportation Department Design Manual, including standard drawing TE-11. Alternative paving surfaces, colors, and patterns in the public sidewalk may be proposed by the developer as described below under Alternative Sidewalk Materials.
 5. Street frontage improvements may require replacement of existing curb and gutter, if the Transportation Department inspector determines that the existing curb and gutter are significantly deteriorated or have been damaged during construction.
 6. Landscaping within planter strips between the curb and sidewalk on both adjacent streets shall be irrigated with a metered water source. Electrical connections for lighting in planter strips may be allowed, if installed in compliance with the electrical code and subjected to an electrical inspection. Irrigation devices and electrical components shall not create a tripping hazard in the sidewalk.
 7. Two-lane commercial driveways are generally required to have an approach width of 30 feet, as defined in standard drawing DEV-6, unless the Transportation Department approves greater width for truck access. Within the site, driveways may

- be narrower. The driveway apron design shall be consistent with standard drawing DEV-6, except that the sidewalk ramps on each side of the driveway shall be longer than six feet, if feasible, in order to achieve less severe sidewalk grades.
8. No new utility vaults that serve only one development will be allowed within a public sidewalk. Vaults serving a broader public purpose may be located within a public sidewalk, with a non-skid surface.
 9. No fixed objects, including fire hydrants, trees, and streetlight poles, are allowed within ten feet of a driveway edge, defined as Point A in standard drawing Dev-6. Fixed objects are defined as anything with breakaway characteristics stronger than a 4-inch by 4-inch wooden post.
 10. No new overhead utility lines will be allowed within or across any right of way or sidewalk easement, and existing overhead lines within the public right of way must be relocated underground.
 11. A portion of the landscaped median in Main Street near the northeast corner of the site shall be removed by the developer to allow full access by emergency vehicles at the northeastern driveway, while discouraging access by other vehicles. The removed portion shall be sufficient to allow fire truck turning movements both in and out of the site. The removed portion of the landscaped median shall be replaced with a hard surfaced median that is slightly raised with a mountable curb that allows passage by emergency vehicles. The hard surfaced area shall be designed with materials, color, and texture approved by the Transportation Department. Signs approved by the Transportation Department shall be installed by the developer to prohibit left turns by non-emergency traffic and to prohibit large trucks from using that driveway to exit the site.
 12. Adjacent to arterial streets, landings on sloping driveway approaches should not exceed a 7% slope for a distance of 30 feet approaching the back edge of sidewalks. Spot elevations must be shown on engineering plans at the back edge of sidewalk and 30 feet back. Grade changes must be rounded off so vehicles do not bottom out. The slope of the existing driveway at the northeastern corner of the site significantly exceeds 7%; however, a complete re-grading of that driveway might not be feasible, since the driveway sub-grade rests against a building wall that will be retained. The developer must submit a proposal that improves the driveway grade immediately behind the sidewalk, and improves sight lines for vehicles exiting the driveway, but does not create excessive conflicts with the building.
 13. All street improvements required for this development shall be shown on the engineering plans to be approved as part of the clearing and grading permit. All Transportation Department standard drawings necessary to clarify the design of such improvements shall be copied exactly from the Transportation Department Design Manual.

Phasing

If construction is phased, then the following will apply. Each of the three requirements below are tied to two separate conditions of approval in section X of this report. The first condition is titled *Civil Engineering Plans – Transportation*, to be complied with on plans submitted with the Clearing & Grading permit application. The second condition is titled *Transportation Mitigation Measures and Street Frontage Improvements*, to be complied with prior to issuance of the Temporary Certificate of Occupancy.

1. Buildings A and D are directly related to access along Main Street. Therefore, all Main Street improvements must be completed by the developer and accepted by the City prior to any major Certificate of Occupancy for either of those buildings.
2. Extension of the southbound turn lane on 148th Avenue SE north of Main Street is related to access into the site from Main Street. Therefore, that turn lane extension must be completed by the developer and accepted by the Transportation Department inspector prior to any major Certificate of Occupancy for Buildings A or D.
3. Buildings B and C are directly related to access along 148th Avenue SE. Therefore, all improvements along 148th Avenue SE south of Main Street must be completed by the developer and accepted by the Transportation Department inspector prior to any major Certificate of Occupancy for Building C or for any major remodel or expansion of Building B that would generate a significant increase in traffic.

Easements

The applicant shall provide sidewalk and utility easements to the City as needed to encompass the full required width of any sidewalks located outside the city right of way fronting this site. The developer must determine whether any utility easements within the site will be affected by this development. Any negative impact this development has on those utility easements must be mitigated or easements relinquished. Transformers and utility vaults serving the site shall be placed within the site or below grade. Any such proposed facilities that cannot be placed within the site shall not be placed in the travel lane of a street or in the main walking path of a sidewalk, to the extent feasible.

Holiday Construction & Traffic Restrictions

Work involving lane closures and other impacts to traffic will be restricted in certain areas of Bellevue from November 15th each year to January 5th of each year. If work activities are scheduled to fall within this window of time, the applicant will be required to obtain specific approval in advance for lane closures or other impacts to traffic. There will be no guarantees that daytime lane closures on 148th Avenue SE will be allowed during this time period. The applicant shall contact the Right-of-Way Section to confirm the specifics of this restriction prior to applying for a Right-of-Way Use Permit.

Use of the Right of Way During Construction

Applicants often request use of the right of way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right of Way Use Permit for such activities must be acquired prior to

issuance of any construction permit including demolition permit. Sidewalks may not be closed except as specifically allowed by a Right of Way Use Permit.

Pavement Restoration

Under the City's Trench Restoration Program, every street in the City has been placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are, "No Street Cuts Permitted," "Overlay Required," and "Standard Trench Restoration." Each category has different trench restoration requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that comes with small strip patching.

Main Street and 148th Avenue SE adjacent to the Kelsey Creek Center are presently classified as "Overlay Required," based on the existing condition of the pavement. Pavement restoration for any trenching or damage to the street surface will require a grind and overlay at least 50 feet long for the full width of any affected lane. The exact limits of the grind and overlay will be established through a right of way use permit.

Alternative Sidewalk Materials

The Transportation Department, in conjunction with other departments as appropriate, will review proposals for the installation of alternative materials by private developers. Any alternative paving surfaces, colors, and patterns in the public sidewalk must be ADA compliant and shall not create a tripping or slipping hazard. If the alternative material is approved, the property owner must sign an indemnification agreement which states that all future maintenance and replacement is the responsibility of the property owner. Work within the alternative material area by City, franchise utility, or other workers as a result of either emergency, normal maintenance, or new installation will result in replacement of the surface by standard materials. Advance notification of such work will not be provided to the property owner. In such a circumstance, should the property owner wish to replace or repair the surface with the alternative material, a Right of Way Use Permit may be required. Approval of the alternative is not guaranteed. Paving samples must be submitted to the Transportation Department prior to building permit approval.

Transportation Management Program

In order to reduce single occupant vehicle trips and provide enhanced options to employees and infrastructure users, the City has adopted code provisions for a transportation management program. The owner of this development shall, prior to initial occupancy of the first new building or first major remodel, sign and record an agreement approved by the City of Bellevue to establish a transportation management program to the extent required by BCC14.60.070. Mixed-use retail centers over 150,000 square feet are required to post information, provide a transportation coordinator, and provide preferential parking as described in BCC 14.60.070 E.

See Section X of this report for Transportation Department related Conditions of Approval.

VI. STATE ENVIRONMENTAL POLICY ACT (SEPA)

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the proposal. Therefore, issuance of a Mitigated Determination of Non-Significance (MDNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements, with incorporation by reference of the *2009-2020 Transportation Facilities Plan Final Environmental Impact Statement* (TFP EIS) updated November 2009. This document is available in the project file at the Development Services Department Records Room in City Hall. Transportation-related impacts associated with this project are consistent with the potential projected impacts analyzed in the 2009-2020 TFP EIS.

This section of the staff report is an addendum to the adopted EIS referenced above. Adverse impacts which are less than significant are usually subject to City Code or Standards which are intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code Section 22.02.140 provides substantive authority to mitigate impacts disclosed through the environmental review process. A discussion of the impacts is noted below. These impacts will be mitigated through exercise of Code authority as well as through project-specific conditions of approval, contained in Section X of this report.

EARTH

The project site will require minimal earthwork and a Temporary Erosion Control (TESC) Plan will be implemented in accordance with City standards. The TESC will provide Best Management Practices including temporary erosion control measures, catch basin inserts placed in all adjacent and on-site catch basins during construction, and temporary siltation and erosion controls maintained during construction.

AIR

Air emissions from construction are anticipated to be minimal. During construction, there will be equipment exhaust that will be emitted from vehicles using gasoline or diesel fuels. These emissions will be temporary in nature and not have a lasting or harmful affect on the project or adjacent properties. There will also be airborne dust particles affecting air quality, principally during the grading phase of the project.

WATER

Replacement of the culvert which conveys Kelsey Creek under the project site between Main Street to the north and the Larson Lake greenbelt to the south was reviewed by the City under separate project application 10-105559-LO, and construction permits are pending. To lessen stormwater runoff, on-site stormwater will be directed into the proposed landscape areas which will double as rain gardens. From there, the runoff will be conveyed to an on-site detention vault and then pumped at runoff rates meeting the requirements of the *2010 City of Bellevue Stormwater Management Manual* into the new culvert prior to release into the existing public storm sewer system located within the adjacent street.

PLANTS & AESTHETICS

Surrounding properties are developed with a mix of commercial and residential uses. Perimeter landscape buffer and parking lot landscaping will be comprised of groundcover, shrubs, and trees that will complement the neighborhood character, and be adaptable to the climatic and hydrological characteristics of the site. At least 50% of the required area landscape will be native planting. Landscape provided along the perimeter buffers will help shield parking lot vehicles from street level views.

The proposed renovation for buildings A and B, as well as the proposed buildings C and D, will be clad with materials which are compatible with the surrounding area, such as wood siding, metal panel siding, and an ashlar concrete masonry block. The proposed facades incorporate setbacks and offsets that will break down the scale of the buildings and create visual relief in the elevations. Additionally, various height elements such as pedestrian scale canopies, stepped roof forms, and pitched roofs are utilized to diminish the scale of the buildings as well. Pitched roofs are proposed for all of the buildings to enhance the relationship with nearby residential areas.

Additional site features such as short walls, refuse enclosures, and light fixtures have been selected to compliment the architectural design of the surrounding primary structures. See attached plan drawings for more information.

NOISE

Noise will be generated from the construction phase of the development and from daily project operations. The City of Bellevue Noise Ordinance, BCC 9.18.040 regulates hours of construction-related noise and the conditions under which they may be expanded. In addition, as addressed previously in Section IV above, noise is expected to emanate from the future loading dock, garbage compactors and cardboard compactors planned for the back/east side of the existing K-Mart building. Therefore, the plans include a reconfigured loading area screened behind a new 10 foot tall concrete block wall. Conditions of approval are included in Section X of this report regarding construction hours, abatement of construction noise, and buffering of operational noise.

TRANSPORTATION

Long-Term Impacts and Mitigation

The long-term impacts of all development projected to occur in the City by 2020 have been addressed in the City's Transportation Facilities Plan EIS. The impacts of growth projected to occur by 2020 are evaluated on the roadway network assuming that all the transportation improvement projects proposed in the City's current Transportation Facilities Plan (TFP) are in place. The Transportation Facilities Plan EIS divides the City into Mobility Management Areas for analysis purposes. The Kelsey Creek Center lies within MMA # 9, which has a 2020 total growth projection of 36,962 square feet of new retail space and 13,431 square feet of new office space, for a combined growth total of 50,393 new square feet of commercial development.

The project is proposed to include construction of two new retail buildings and a small expansion of the former K-Mart building, for a total of up to 21,304 square feet of new retail space. This is less than the 36,962 square feet of new retail growth assumed for MMA # 9 by 2020. No new space is proposed for any other land use type. Thus, the proposed development is within the assumptions of the Transportation Facilities Plan EIS.

In addition to the new retail space in Buildings A, C and D, 27,335 square feet of existing office space and 155,302 square feet of existing retail space in Buildings A and B will be remodeled. The site's total commercial space will ultimately be up to 203,941 square feet (21,304 square feet from the Building A addition and new Buildings C & D per the paragraph above + 27,335 square feet existing office space in Building B + 155,302 square feet existing for existing Buildings A and B), of which 176,606 square feet will be retail (21,304 + 155,302 square feet described previously). For this long-term analysis, the former Kmart building is assumed to be existing retail space, even though it has been vacant for many years. Mid-term and short-term impacts of that building are viewed differently, as described below.

Traffic impact fees are used by the City to fund street improvement projects to alleviate traffic congestion caused by the cumulative impacts of development throughout the City. Payment of the transportation impact fee, as required by BCC 22.16, contributes to the financing of transportation improvement projects in the current adopted Transportation Facilities Plan, and is considered to be adequate mitigation of long-term traffic impacts. Fee payment is required at the time of building permit issuance.

Mid-Range Impacts and Mitigation

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more net new p.m. peak hour trips undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained. Concurrency analysis is intended to compare existing conditions to conditions after development; therefore, the trip generation number used in the concurrency analysis assumes that trips generated by any remodeling or reuse of the existing Kmart building are all new trips, since that building was vacant during the base year of the concurrency analysis period.

The developer has proposed three different options, with slightly different mixes of square footage for different commercial tenants. Trip generation for the three options is estimated to range from 577 to 593 net new PM peak hour trips; however, the concurrency test was run with 600 net new PM peak hour trips in order to cover minor variations in the square footage or mix of tenants that may occur in the future. City staff distributed and then assigned project-generated trips to the street network using the City's EMME-2 travel forecasting model with the current Capital Investment Program network. By adding the project-generated trips to the traffic volumes in the model, the area average levels of service were determined. To create a baseline condition for comparison, the levels of service were also determined using traffic volumes without the project-generated trips. In this analysis, 37 system intersections received 20 or more new p.m. peak hour trips generated by the development. Neither the maximum area-

average levels of service nor the congestion allowances would be exceeded as a result of traffic generated from this proposal, so the proposed development passes the concurrency test. The results and analysis spreadsheet are included in the Transportation Department's project file.

The rules of concurrency reservation are outlined in the Traffic Standards Code Director's Rules, updated May 23, 2001. A concurrency determination is issued on the date of issuance of the land use decision. This project complies with the Traffic Standards Code and is receiving a Certificate of Concurrency. The concurrency determination is reserved to this project at the land use decision date. The concurrency reservation expires one year from the land use decision date unless a complete building permit application for a major building or a major remodel is filed (BCC 14.10.010.D). At the time of a complete building permit application, the Certificate of Concurrency will remain in effect for the life of the building permit application, pursuant to BCC 23.05.090.H. At issuance of a major building permit, the Certificate of Concurrency will be extended and remain in effect for one additional year (with the possibility of up to two one-year extensions) as provided for in BCC 23.05.100.

See the Land Use Division's Design Review project file for the Certificate of Concurrency.

Short-Term Operational Impacts and Mitigation

City staff analyzed the short-term operational impacts of this proposal for the PM weekday peak hours in order to recommend traffic mitigation. Issues that were analyzed included traffic operations and safety at the site's access points, the direction and volume of traffic flow to and from the site, and the expected traffic volume versus capacity for relevant turn lanes affecting the access points and the intersection of 148th Avenue SE and Main Street. City-recommended traffic mitigation includes turn lane lengthening, improvements to access points, and standard frontage improvements, as described in section V Technical Review above; see the sub-section for Transportation, *Mitigation of the Development's Traffic Impacts and Street Frontage Improvements*.

The *Kelsey Creek Center Traffic Impact Analysis (TIA)*, dated August 2010, by Transportation Solutions, Inc (TSI) and February 5, 2011 addendum included options for the land use assumptions and updated the trip generation and parking analyses. Note that, based on the judgment of the city's traffic operations engineers, the city staff's conclusions differ to some extent from the recommendations published in the *TIA* and its addendum, both of which are included in the Transportation Department project file.

If the site's redevelopment is phased, then the implementation of transportation mitigation measures must be consistent with the ultimate development of the entire site, as described in section V Technical Review above; see the sub-section for Transportation, *Phasing*.

VII. CHANGES TO PROPOSAL DUE TO STAFF REVIEW

Building Exterior

Originally, the buildings were proposed to be more generic, with the property owner's intent that future tenants could design according to their own individual preferences. However, the City wanted to ensure a consistent style, compatibility with the surroundings, and appearance consistent with the overlay district's objectives applicable to this site. Therefore, the owner was asked to design the buildings with specific detail at this Design Review stage of development, rather than allowing design decisions to occur under construction permits. Following are key exterior features of the buildings:

1. The proposed renovation for buildings A and B, as well as the proposed buildings C and D, are clad with materials which are compatible with the surrounding area, such as wood and metal panel siding, rough-sawn lumber, masonry, and earth tone paint. These materials will create a soft and natural appearance that will mesh well with the nearby residential and commercial areas.
2. The proposed building facades incorporate setbacks and offsets that serve to break down the scale of the buildings and create visual relief in the elevations. Additionally, pedestrian scale canopies, stepped roof forms, and pitched roofs are utilized to diminish the scale of the buildings.
3. Pitched roofs are proposed for all of the buildings to enhance the relationship with the residential neighborhood.
4. The sides of all buildings facing the public streets include entries and windows to allow physical and visual access to the activity within the buildings. Buildings C and D have windows and interesting architectural elements on all facades since the buildings are viewed and accessed from all four sides. Buildings A and B will maintain the existing facades on the sides and back of the buildings, with the exception of building A along Main Street and facing the parking lot, and building B along 148th Avenue SE and facing the parking lot.
5. A 10 foot tall screen wall has been designed into the existing loading and trash/recycling area on the back/east side of Building A to lessen the amount of noise carried to the neighboring residents.

Site Design

Originally, the site design lacked clarity regarding how it would address development standards and dimensional requirements contained in the City's Land Use Code, as well as how the site would respond to applicable Comprehensive Plan policies and design guidelines for projects proposed within the Community Retail Design District such as this one. More detail was requested regarding frontage improvements, perimeter landscaping, pedestrian pathways, plaza design, lighting, tree quantities and locations, trash area screening, vehicular access and circulation, and loading issues. Following are key features implemented during design review in response to City direction:

1. Frontage improvements along Main Street and 148th Avenue SE have been designed to be particularly accommodating for pedestrians, with plenty of sidewalk width and landscaping intended to provide a buffer from the roadway. A landscape strip at least 4 feet wide planted with groundcover and street trees is proposed along the entire length of street frontage between the curb and a sidewalk which is primarily 8 feet wide and as much as 30 feet wide where it combines with building-associated plaza space on the street sides of buildings C and D.
2. Landscaping along Main Street and 148th Avenue SE has been designed to screen the parking lot from the street and adjacent private property. A landscape buffer at least 10 feet wide planted with groundcover, shrubs and trees is proposed along the entire perimeter of the parking area. This buffer is shown as being up to 16 feet wide along the 148th Avenue SE perimeter and approximately 18 feet wide along most of the Main Street perimeter, with the segment along Building A even wider.
3. 6 – 10 foot wide pedestrian pathways flank buildings and cross the parking lot in each direction, providing walkers safe and pleasant access between the public sidewalk, their cars, and the various buildings.
4. Buildings C and D are located close to 148th Avenue SE and Main Street in response to the Comprehensive Plan's Urban Design Element/ *Urban Design Treatment for Boulevards and Designated Intersections* (Figure UD.1), as well as design guidelines contained in the Community Retail Design District (LUC 20.25I) which call for a strong relationship between buildings and the public sidewalk.
5. Building D was re-oriented to present its longest side to Main Street with improved pedestrian-oriented building frontage, plaza space and removal of an originally-proposed drive-through which would have created circulation conflicts.
6. Trees were added to previously bare parking lot islands throughout the site.
7. Trash dumpsters are now shown hidden inside enclosures, and the enclosures have been designed to be architecturally compatible with the proposed buildings.
8. Plaza spaces with special paving, seating, bike racks, lighting, community boards, water features and trash receptacles are now proposed alongside each building.
9. A formal pedestrian connection to the Larson Lake greenbelt trail is proposed south of Building A and includes a plaza with seating, informational kiosk, and lighting.
10. An easement agreement allows the adjacent property at the northwest corner of 148th Avenue SE & Main Street access through the project site. Though not itself a design feature, this easement had a significant effect on the location of Building C and the layout of driveways, parking lot, and landscape areas particularly within the northwest area of the project site.

VIII. DECISION CRITERIA

The Director may approve, or approve with modifications, an application for Design Review if:

1. The proposal is consistent with the Comprehensive Plan.

The Community Business Comprehensive Plan designation and the Southeast Bellevue Subarea Plan generally support neighborhood commercial centers as a land use. Refer to Attachment A for a listing and discussion of Comprehensive Plan policies applicable to this proposal.

2. The proposal complies with the applicable requirements of this Code.

As conditioned, the proposal complies with all applicable requirements of the Land Use Code. Refer to Section III.A.2 of this report for specific information on Land Use Code consistency.

3. The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent.

The proposal complies with all applicable design guidelines and standards contained in LUC 20.25.B and LUC 20.25.I as discussed in Sections III.B – D of this report.

4. The proposal is compatible with, and responds to, the existing or intended character, appearance, and quality of development and physical characteristics of the subject property and immediate vicinity.

The building renovations and two proposed new buildings are designed to capture what is commonly identified as northwest architectural style. Materials consist of split-face concrete masonry blocks, vertical wood siding, rough cut timber, corrugated metal sheet panels, and standing seam metal roof panels. These materials are found throughout the immediate vicinity and blend well with the physical characteristics of the community.

5. The proposal will be served by adequate public facilities including streets, fire protection, and utilities.

All required public services and facilities are available to the site.

IX. DECISION

After conducting the various administrative reviews associated with the proposal, including applicable Land Use consistency, SEPA and City Code & Standard compliance reviews, the Development Services Department Director does hereby APPROVE WITH CONDITIONS the subject proposal.

X. CONDITIONS OF APPROVAL:

The following conditions are imposed under the authority referenced:

A. GENERAL CONDITIONS: The following conditions apply to all phases of development.

1. COMPLIANCE WITH BELLEVUE CITY CODES AND ORDINANCES

The applicant shall comply with all applicable Bellevue City Codes and Ordinances including but not limited to:

Clearing and Grading Code - BCC 23.76	Janney Gwo, 425-452-6190
Transportation Development Code - BCC 14.60	Carl Wilson, 425-452-4228
Traffic Standards Code – BCC 14.10	Carl Wilson, 425-452-4228
Trans. Impact Fee Program - BCC.22.16	Carl Wilson, 425-452-4228
Transportation Development Standards	Carl Wilson, 425-452-4228
Right-of-Way Use Permit - BCC 14.30	Ron Kessack, 425-452-4631
Land Use Code - BCC Title 20	Mike Upston, 425-452-2970
Sign Code - BCC Title 22B	Mike Upston, 425-452-2970
Noise Control - BCC 9.18	Mike Upston, 425-452-2970
Utilities Code - BCC Title 24	Rob Hutchinson, 425-452-7930
Construction Codes - BCC Title 23	Lee Kranz, 425-452-2732
Uniform Fire Code - BCC 23.11	Adrian Jones, 425-452-6032

2. CONSTRUCTION HOURS

Noise related to construction is allowed from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020C.1 when necessary to accommodate construction which cannot be undertaken during exempt hours. Allowances for short term work outside of normal construction hours shall be limited and will be reviewed on a case by case basis to verify necessity and ensure appropriate noise mitigation is utilized to protect surrounding uses and properties. Written requests for exemption from the Noise Control Code must be submitted two weeks prior to the scheduled onset of extended hour construction activity. Such request shall include a noise analysis prepared by a noise consultant, including recommendations for achieving the noise limitations of the Noise Ordinance for new residential construction.

Authority: BCC 9.18.040
Reviewer: Mike Upston, Land Use

3. DAMAGE TO STREET TREES

Street trees damaged during construction shall be replaced by the applicant with the same size and type, and planted as specified by Parks Dept tree planting standards.

Authority: LUC 20.25A.040
Reviewer: Mike Upston, Land Use

4. OUTDOOR DISPLAY

The outdoor display of bulky materials shall be screened from the public sidewalk with a durable, solid wall or fence, evergreen hedge, or a combination of these. The screening

requirement does not apply to the display of seasonal products of a decorative nature such as bedding plants, Halloween pumpkins, and holiday greens.

Authority: LUC 20.251.050.D.2

Reviewer: Mike Upston, Land Use

5. HOLIDAY CONSTRUCTION & TRAFFIC RESTRICTIONS

Work involving lane closures and other traffic impacts will be restricted from November 15th to January 5th of each year. If work activities are scheduled to fall within this window of time, the applicant will be required to obtain specific approval in advance for lane closures or other impacts to traffic. There will be no guarantees that daytime lane closures on 148th Avenue will be allowed during this time period. The applicant shall contact the Transportation Department Right-of-Way Section to confirm the specifics of this restriction prior to applying for a Right-of-Way Use Permit. The Transportation Department will be monitoring traffic and may modify this moratorium accordingly.

Authority: BCC 14.30.060

Reviewer: Ron Kessack, 425-452-4631

6. VEHICULAR ACCESS RESTRICTIONS

Access to this site from 148th Avenue will continue to be restricted to right-turn-in and right-turn-out only. On Main Street near the site's northeastern corner, a portion of the median will be removed for emergency access, but left turns by other traffic will continue to be prohibited at that location.

Authority: BCC 14.60.150

Reviewer: Carl Wilson, 425-452-4228

7. ON-STREET LOADING & UNLOADING

On-street loading and unloading will not be permitted.

Authority: LUC 20.20.590.K.4

Reviewer: Mike Upston, 425-452-2970

8. PRELIMINARY DESIGN APPROVAL

Utility Department approval is based on the conceptual design only. Changes to the site layout may be required to accommodate the utilities after utility engineering is approved.

Authority: BCC Title 24.02, 24.04, 24.06

Reviewer: Rob Hutchinson, 425-452-7930

9. UTILITY CODES & ENGINEERING STANDARDS

The water, sewer, and storm drainage systems shall be designed per current City of Bellevue Utility Codes and Utility Engineering Standards.

Authority: BCC Title 24.02, 24.04, 24.06

Reviewer: Rob Hutchinson, 425-452-7930

10. PLAN APPROVAL & FIELD INSPECTION

Plan approval and field inspection shall be performed under individual permits and Utility Developer Extension Agreements.

Authority: BCC Title 24.02, 24.04, 24.06

Reviewer: Rob Hutchinson, 425-452-7930

B. PRIOR TO CLEARING & GRADING PERMIT: The following conditions are imposed to ensure compliance with the relevant decision criteria and Code requirements and to mitigate adverse environmental impacts not addressed through applicable Code provisions. These conditions must be complied with on plans submitted with the Clearing & Grading permit application:

1. TREE PROTECTION

Include on the project plans tree preservation detail Drawing #35, Tree Protection, of the City of Bellevue Development Standards.

Authority: BCC 23.76 and Bellevue Development Standards

Reviewer: Mike Upston, Land Use

2. RIGHT-OF-WAY USE PERMIT

Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

- Designated truck hauling routes.
- Truck loading/unloading activities.
- Location of construction fences.
- Hours of construction and hauling.
- Requirements for leasing of right of way or pedestrian easements.
- Provisions for street sweeping, excavation and construction.
- Location of construction signing and pedestrian detour routes.
- All other construction activities as they affect the public street system.

In addition, the applicant shall submit for approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevents access. General materials storage and contractor convenience are not reasons for preventing access. The applicant shall secure sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.

Authority: BCC 11.70 & 14.30

Reviewer: Ron Kessack, (425) 452-4631

3. CIVIL ENGINEERING PLANS – TRANSPORTATION

Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all off-site mitigation, street frontage improvements, and driveway access must be in conformance with the requirements of the Americans with Disabilities Act, Transportation Development Code, Transportation Department Design Manual, and requirements stated elsewhere in this document. Relevant standard drawings from the Transportation Department Design Manual shall be copied exactly into the engineering plans. Requirements for the engineering plans include, but are not limited to:

- Traffic signs and markings, including channelization for turn lane extensions and median revisions as, and signage for turning restrictions at the northeast driveway and any other signage required to meet Transportation Department standards.

- Curb, gutter, sidewalk, and driveway approach design. (The engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as needed.) Requirements include sidewalk width, driveway width, and driveway grade as required above under Street Frontage Improvements.
- Installation/relocation of streetlights & related equipment, including off-site streetlights.
- Sight distance per BCC 14.60.240 and 241 for all driveways. (Show the required sight triangles and include any sight obstructions, including those off-site.)
- Location of fixed objects in the sidewalk or near the driveway approach.
- Trench restoration within any street right of way.
- Two turn lane extensions on 148th Avenue, as described above under Mitigation of the Development's Traffic Impacts.
- Median removals and revisions on both 148th Avenue and Main Street as described above under Mitigation of the Development's Traffic Impacts and Street Frontage Improvements.

Authority: BCC 14.60; Transportation Department Design Manual

Reviewer: Carl Wilson, 425-452-4228

C. PRIOR TO BUILDING PERMIT SUBMITTAL: The following conditions are required by City Code. Unless specified otherwise below, these conditions must be complied with upon submittal of the Building permit application:

1. RECYCLING & SOLID WASTE COLLECTION

A letter from Allied Waste shall be submitted to the Land Use reviewer confirming their endorsement of the proposed size and function of the recycling & solid waste collection areas, and that garbage containers will be kept out of view.

Authority: BCC 22B.025

Reviewer: Mike Upston, Land Use

2. ROOF-TOP MECHANICAL EQUIPMENT SCREENING

Since mechanical equipment screening is not shown on the Design Review plans for Building A, a Land Use Exemption from Design Review will be required prior to building permit approval for Building A. The amendment drawings shall show the location of each piece of mechanical equipment (including communication equipment such as satellite dishes) and demonstrate that the equipment is not visible from adjacent streets, public sidewalks, from the tops of surrounding buildings, or adjacent residential areas.

Authority: LUC 20.20.525, 20.20.650, 20.20.730, 20.25B, 20.25I

Reviewer: Mike Upston, Land Use

3. GROUND-MOUNTED MECHANICAL EQUIPMENT SCREENING

No mechanical equipment (including power, telephone, traffic control, etc) shall be located in above ground cabinets in sidewalk areas. Such equipment shall be located in underground vaults, in a building, or substantially screened per the approval of DSD.

Authority: LUC 20.20.525, 20.20.650, 20.20.730, 20.25B, 20.25I

Reviewer: Mike Upston, Land Use

4. BUILDING B RECYCLING AND SOLID WASTE AREA SCREENING

Screening shall be provided for all existing and proposed recycling and solid waste collection areas associated with Building B so that users of the public right of way and the adjacent public park to the south will not see inside the collection area.

Authority: LUC 20.20.25B.040.D

Reviewer: Mike Upston, Land Use

5. SOUTH FENCE

A new fence along the south side of the property shall be shown on the plans submitted for a building permit for Building B or for plans submitted for the project site improvements, whichever is submitted first.

Authority: LUC 20.20.25B.040.D

Reviewer: Mike Upston, Land Use

6. BIKE RACKS

Bike racks will be provided for at least 46 bicycles; more to meet the requirement of this Land Use Code section if parking increases above the 816 stalls approved herein.

Authority: LUC 20.20.25I.050.D

Reviewer: Mike Upston, Land Use

D. PRIOR TO BUILDING PERMIT ISSUANCE: The following conditions are required by City Code. Unless specified otherwise below, these conditions must be complied with prior to Building permit issuance:

1. TRANSPORTATION IMPACT FEE

Payment of transportation impact fees is required at issuance of each building permit, per BCC 22.16. Fee rates are subject to change and the most current fee schedule in effect at the time of permit issuance will apply, unless a Development Agreement modifies this requirement. All permits that will generate one or more net new trips will be classified as “developments” for impact fee purposes under BCC 22.16.020.C, because SEPA review is required for the entire site and because trip generation for each tenant is part of the site’s cumulative total, which exceeds the 11-trip threshold stated in BCC 22.16.020C. However, the existing building along the southwestern edge of the site is a type that normally experiences turnover of small tenants; therefore, impact fees will not be required for tenant improvements in that building unless the Transportation Department determines that a tenant improvement would be an abnormally large trip generator. Removal of previous land uses or tenants will be eligible to provide credit against transportation impact fees only for establishments that were in normal operation within two years prior to submittal of a complete building permit application.

Authority: BCC 22.16

Reviewer: Carl Wilson, 425-452-4228

2. EXISTING EASEMENTS

The developer must determine whether any utility easements within the site will be affected by this development. Any negative impact that this development has on those utility easements must be mitigated or easements relinquished.

Authority: BCC 14.60.100

Reviewer: Ron Kessack, (425) 452-4631

3. DEDICATION OF NEW PEDESTRIAN / UTILITY EASEMENTS

The applicant shall provide sidewalk and utility easements to the City such that sidewalks outside of the City right of way along the property frontage are located within a pedestrian easement area.

Authority: BCC 14.60.100
Reviewer: Carl Wilson, 425-452-4228

4. ALTERNATIVE PAVING MATERIALS AGREEMENT

The applicant shall sign and record an indemnification agreement that acknowledges that the applicant is fully responsible for all future maintenance and replacement of any alternative paving materials that may be approved for use in the public sidewalks along the site's frontage.

Authority: BCC 14.60.110
Reviewer: Carl Wilson, 425-452-4228

5. TRANSPORTATION MANAGEMENT PROGRAM

The owner of the property being developed shall sign and record at the King County Office of Records and Elections an agreement to establish a Transportation Management Program to the extent required by Transportation Development Code Section 14.60.070.

Authority: BCC 14.60.070
Reviewer: Carl Wilson, 425-452-4228

E. PRIOR TO TCO: The following conditions are required by City Code and supported by City Policy. These conditions shall be complied with prior to issuance of the Temporary Certificate of Occupancy (TCO):

1. LANDSCAPE INSTALLATION ASSURANCE DEVICE

All site landscaping shall be 100% complete per the plan approved by the City. Alternatively, the applicant shall submit the following: 1) a red-marked plan identifying which landscape areas are incomplete; 2) an estimate for the total cost to complete these areas; and 3) a notarized Assignment of Savings dedicated to the City for 150% of the estimated cost to complete these areas per the approved Landscape Plan.

Authority: LUC 20.40.490
Reviewer: Mike Upston, Land Use

2. LANDSCAPE MAINTENANCE ASSURANCE DEVICE

The applicant shall file with the Development Services Department a landscape maintenance assurance device for a 3-year period; provide an assignment of savings or letter of credit for 20% of the cost of labor and materials for all required landscaping.

Authority: LUC 20.40.490
Reviewer: Mike Upston, Land Use

3. TRANSPORTATION MITIGATION MEASURES AND STREET FRONTAGE IMPROVEMENTS

All mitigation, street frontage improvements and other transportation requirements described in Section V of this staff report (see the Transportation subsections titled "Mitigation of the Development's Traffic Impacts", "Street Frontage Improvements", and

“Phasing”) must be constructed by the applicant and accepted by the City Inspector. All required improvements must be constructed as per the approved plans or as per direction of the Transportation Department inspector. An assurance device will not be accepted in lieu of construction, unless the city requires a delay. The design of the improvements must conform with the Americans with Disabilities Act, the Transportation Development Code (BCC 14.60), and the Transportation Department Design Manual.
Authority: BCC 14.60 and Transportation Department Design Manual
Reviewer: Carl Wilson, 425-452-4228

4. PAVEMENT RESTORATION

Both Main Street and 148th Avenue adjacent to the Kelsey Creek Center are presently classified as “Overlay Required,” based on the existing condition of the pavement. Pavement restoration for any trenching or damage to the street surface will require a grind and overlay at least 50 feet long for the full width of any affected lane. The exact limits of the grind and overlay will be established through a right of way use permit.
Authority: BCC 14.60. 250; Design Manual Design Standard #21
Reviewer: Ron Kessack, (425) 452-4631

5. IMPLEMENT THE TRANSPORTATION MANAGEMENT PROGRAM

The Transportation Management Program per a condition of approval above must be functional prior to issuance of the initial certificate of occupancy.
Authority: BCC 14.60.070
Reviewer: Carl Wilson, 425-452-4228

Attachment A: Comprehensive Plan Consistency

Land Use Element

Policy LU-8: Ensure that commercial land uses are contained within carefully delineated areas.

Policy LU-9: Maintain compatible use and design with the surrounding built environment when considering new development or redevelopment within an already developed area.

Policy LU-13: Reduce the regional consumption of undeveloped land by facilitating redevelopment of existing developed land when appropriate.

Policy LU-22: Protect residential areas from the impacts of non-residential uses of a scale not appropriate to the neighborhood.

The proposed would make new use of land that has already been designated for neighborhood-oriented commercial uses rather than trying to rezone land to a higher intensity. The 2010 CZA increases development viability in exchange for improved environmental and economic conditions. However, development flexibility is not provided in the recommended 2010 CZA without limitation. Mitigation objectives that were contained in the original 1987 CZA that allowed the site to be rezoned from NB to CB are maintained in the 2010 CZA to ensure that the character and level of redevelopment at Kelsey Creek Center is compatible with the adjacent residential community.

Transportation Element

Policy TR-7: Locate new community facilities near major transit routes and in areas convenient to pedestrians and bicyclists.

Policy TR-8: Incorporate transit-supportive and pedestrian-friendly design features in new development through the development review process.

Policy TR-25: Provide for adequate roadway, pedestrian, and bicycling connections in newly developing and redeveloping areas of the city, promoting both internal access and linkages with the rest of the city.

Policy TR-55: Work with private developers and transit providers to integrate transit facilities and pedestrian and bicycle connections into residential, retail, manufacturing, office, and other types of development.

Policy TR-84: Secure sidewalk and trail improvements and easements, and on-site bicycle parking and storage consistent with the Pedestrian and Bicycle Transportation Plan through the development review process.

Policy TR-117: Evaluate neighborhood impacts as part of corridor and subarea transportation studies.

Policy TR-119: Minimize spillover parking from commercial areas, parks, and other facilities encroaching on residential neighborhoods, through residential parking zones and other measures.

Enhancements for pedestrians, bicyclists and vehicular uses have all been incorporated into the proposed site plan. Pedestrian access within the retail center will be improved by creating wider, more distinguished pedestrian pathways and flanking many of them with landscape borders. Bicycle and vehicular traffic through the site will also be better. Instead of one-way angled parking, the new proposal calls for two-way drive isles with 90 degree parking stalls. Parking isles have also been redesigned to be perpendicular to the store fronts of building A and B, which will provide safer pedestrian movement. Parking stalls provided for the redeveloped center will meet the quantity required by the land use code, and spill over into residential zones is not anticipated.

Economic Development Element

Policy ED-1: Maintain a business climate that supports the retention and expansion of the city's economic base.

Policy ED-3: Develop and maintain regulations that allow for continued economic growth while respecting the environment and quality of life of city neighborhoods.

Policy ED-18: Encourage high quality design and urban amenities for public and private development, maintaining development standards to recognize that a quality built environment helps attract the talented workers who will sustain economic growth.

Policy ED-24: Maintain land use development standards within the city's commercial areas that promote high quality, aesthetically attractive development, in order to add economic value to these areas and to ensure they are good neighbors.

Policy ED-27: Where a commercial revitalization effort involves significant changes to plans and regulations that may impact a residential neighborhood, develop strategies to avoid or minimize these impacts.

Policy ED-28: Facilitate the redevelopment and re-invigoration of older neighborhood shopping centers that are experiencing decline. Work with stakeholders to transform such centers into high quality and dynamic retail/mixed use commercial areas that also provide a gathering place and sense of community for the neighborhood.

Both of the existing buildings on the site, the two new proposed buildings, and site improvements have been designed to attract a diversity of retail uses and revitalize the center as a gathering place to enhance the quality of life for the community. The redeveloped center will provide new jobs and fill thousands of square feet of vacant retail space, which in turn will expand the City's economic base.

Environmental Element

Policy EN-27: Implement the citywide use of low impact development techniques and green building practices that provide benefits to critical areas functions.

Policy EN-37: Reduce runoff from streets, parking lots and other impervious surfaces and improve surface water quality by utilizing low impact development techniques in new development and redevelopment.

Policy EN-49: Preserve existing vegetation or provide or enhance vegetation that is compatible with the natural character of Bellevue.

Policy EN-68: Encourage residents and professional landscaping firms to utilize native plants in residential and commercial landscapes.

Policy EN-88: Ensure that excessive noise does not impair the permitted land use activities in residential, commercial, and industrial land use districts.

The storm water detention system will be designed to meet the City's current standards, which includes utilizing low impact development techniques. For example, rain gardens are incorporated into landscape islands within the parking lot and pervious concrete is proposed for the parking area around building D (the presence of a high ground water table throughout the rest of the site precludes utilizing more of the pervious concrete). All of the existing landscaping within the perimeter buffers will remain, and in areas where the perimeter buffers do not meet code requirements enhancement to those buffers will be made utilizing a mixture of native and non-native plants. Noise levels will be typical to those found in other retail centers and allowed by code.

Urban Design Element

Policy UD-1: Encourage high quality, attractive, architecturally appealing designs for major buildings in order to create distinctive visual reference points in the community.

Policy UD-2: Support designs for the built environment that are visually stimulating and thoughtful and which convey excellence in architecture and workmanship, and durability in building materials

Policy UD-3: Encourage a variety of site and building designs which are compatible and consistent with surrounding development and that implement the policies of this Plan.

Policy UD-4: Ensure that development relates, connects, and continues design quality and site functions from site to site

Policy UD-5: Include accessible and attractive places for the general public, employees and visitors to wait, to be outdoors, or to socialize in more intensive commercial development. Less intensive commercial development should include such places for employees and visitors.

Policy UD-6: Design buildings located on the edge of public places using materials, forms, details and other architectural elements that will enrich the appearance of the places and encourage people to use them.

Policy UD-8: Design rooftop mechanical screening so that it is integral with building architecture. Consider the visual effects of technical advances such as satellite dishes, on building design.

Policy UD-9: Use site design, landscaping, and appropriate lighting to reduce the visual impact of parking lots to public areas.

Policy UD-12: Place and design outdoor lighting around buildings, in parking lots, and along streets to prevent excessive glare into residential areas.

Policy UD-14: Encourage seasonal color plantings in public and semi-public areas.

Policy UD-15: Use landscape design that is urban in character in more urban settings and use natural landscape design in more suburban settings.

Policy UD-18: Preserve significant trees and mature vegetation, with special consideration given to the protection of groups of trees and associated undergrowth, specimen trees, and evergreen trees.

Policy UD-25: Assure that sign design and placement is compatible with building architecture.

Policy UD-26: Ensure compatible signs in commercial development with multiple businesses.

Policy UD-27: Ensure that signs are compatible with their surroundings. Signs should provide information and make a positive visual contribution to the character of the community.

Policy UD-39: Include clear and ample walkways from street sidewalks and parking areas to building entrances and within and between developments as a part of site design.

Policy UD-40: Ensure that sidewalks, walkways, and trails are furnished, where needed and appropriate, with lighting, seating, landscaping, street trees, trash receptacles, public art, bike racks, railings, handicap access, newspaper boxes, etc. without interfering with pedestrian circulation.

Policy UD-49: Design and coordinate the proximity of bike racks, wheelchair access, pedestrian amenities, and other modes of transportation with transit facilities.

Policy UD-54: Give identity and continuity to street corridors by using a comprehensive street tree plan and other landscaping to enhance circulation routes, soften the appearance of pavement and separate pedestrians from traffic.

Policy UD-55: Incorporate architectural character, landscaping, and signs into commercial and public centers that make them appear and function cohesively.

Policy UD-56: Ensure that perimeter areas of major commercial and public centers use appropriate planting, lighting, and signs to blend with surrounding development and to be compatible with surrounding residential neighborhoods.

Policy UD-57: Allow buildings to be sited at or near the public sidewalk as long as the full sidewalk potential is not diminished.

Policy UD-59: Assure that more intense development is compatible with adjacent, less intense development by incorporating elements in site and building design that soften the impact from commercial to residential areas or from multifamily to single family areas.

The proposal is consistent with these policies. The discussion provided in subsections III.B - D of the staff report provides specific information which illustrates how these policies are addressed.

Southeast Bellevue Subarea Plan

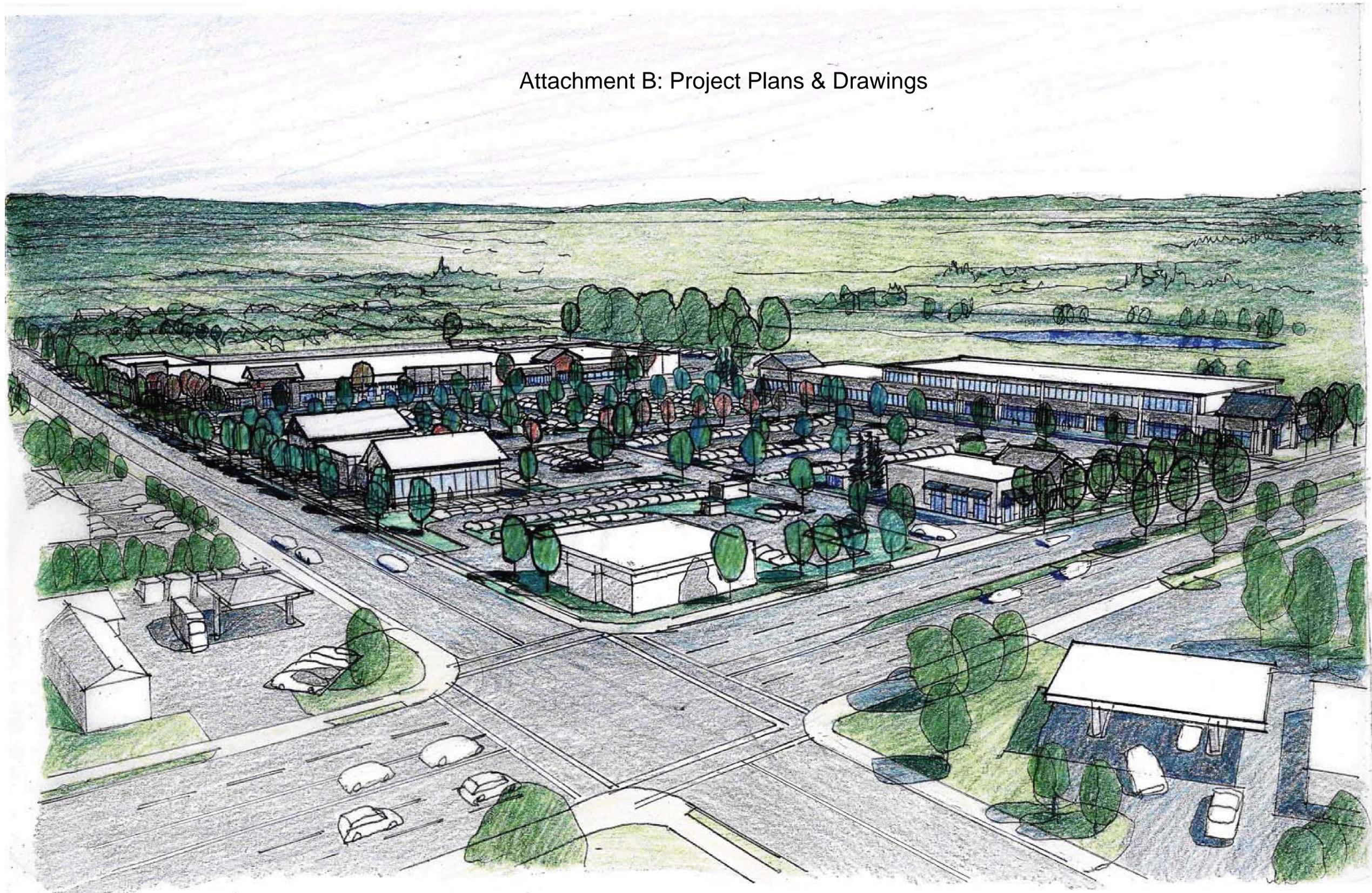
Policy S-SE-7: Expand uses in neighborhood commercial districts to better serve the needs of surrounding neighborhoods by allowing for a variety of retail, office, and residential uses; provided that the character and level of development is of a low intensity which is compatible with the adjacent residential districts.

Policy S-SE-8: Require upgrading of buildings and landscaping of all existing retail and office development areas to meet current City standards.

Policy S-SE-9: Require designs to complement and enhance the residential character of the area, including maintenance of buildings, grounds, signs, and site development.

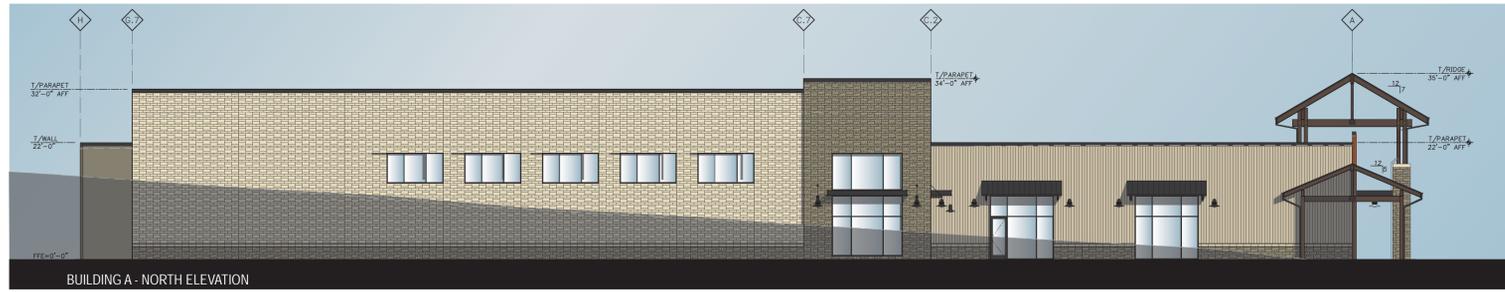
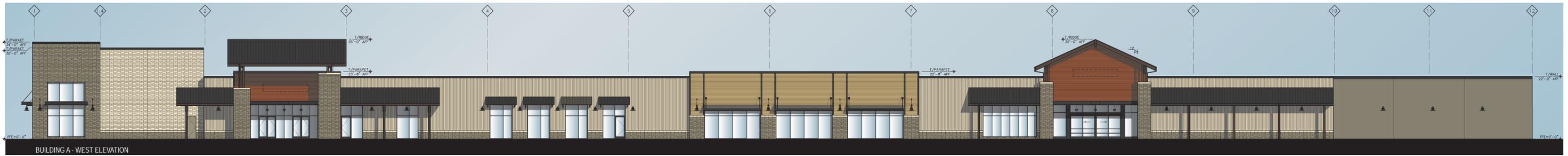
The 2010 CZA allows flexibility for redevelopment of Kelsey Creek Center, but maintains objectives contained in the original 1987 CZA which ensure that the character and level of redevelopment is compatible with the adjacent residential community. The proposed project meets current code requirements, design standards and guidelines, and will result in a substantial upgrade from the current condition. Land Use Code regulations for the Community Retail Design District and Transition Area Design District together with the use and size limitations required by the 2010 CZA and City authority to apply design review discretion ensure that redevelopment of the site will be compatible with the surrounding neighborhood.

Attachment B: Project Plans & Drawings



KELSEY CREEK CENTER RENOVATION - DESIGN REVIEW

PERSPECTIVE - REVISED



MATERIAL LEGEND

- STANDING SEAM MTL ROOF
DARK BRONZE, U.N.O.
- ASHLAR CMU BLOCK
SPLIT-FACE
- ASHLAR CMU BLOCK
GROUND-FACE
- WOOD SIDING
- METAL SIDING, P-4
- STOREFRONT WINDOW
SYSTEM

PAINT SCHEDULE

- P-1
SW, DOWNING STRAW
- P-2
SW, RENWICK OLIVE
- P-3
SW, PEWTER GREEN
- P-4
SW, DOWNING SAND
- P-5
SW, ROOKWOOD TERRA COTTA
- P-6
SW, ADAPTIVE SHADE

KELSEY CREEK CENTER RENOVATION - DESIGN REVIEW

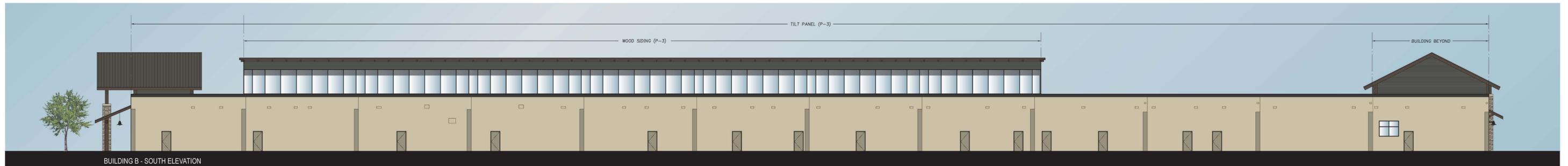
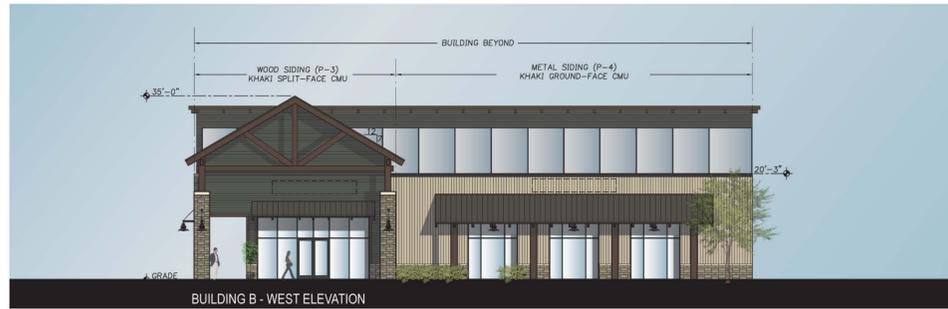
BUILDING A - REVISED



BELLEVUE, WASHINGTON

DECEMBER 9, 2010

REVISED: FEBRUARY 17, 2011

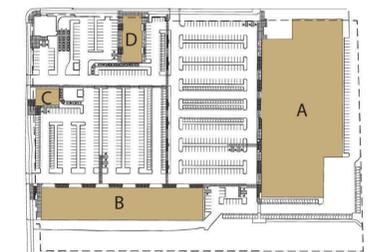


MATERIAL LEGEND

- STANDING SEAM MTL ROOF DARK BRONZE, U.N.O.
- ASHLAR CMU BLOCK SPLIT-FACE
- ASHLAR CMU BLOCK GROUND-FACE
- WOOD SIDING
- METAL SIDING
- STOREFRONT WINDOW SYSTEM

PAINT SCHEDULE

- P-1 SW, DOWNING STRAW
- P-2 SW, RENWICK OLIVE
- P-3 SW, PEWTER GREEN
- P-4 SW, DOWNING SAND
- P-5 SW, ROOKWOOD TERRA COTTA
- P-6 SW, ADAPTIVE SHADE



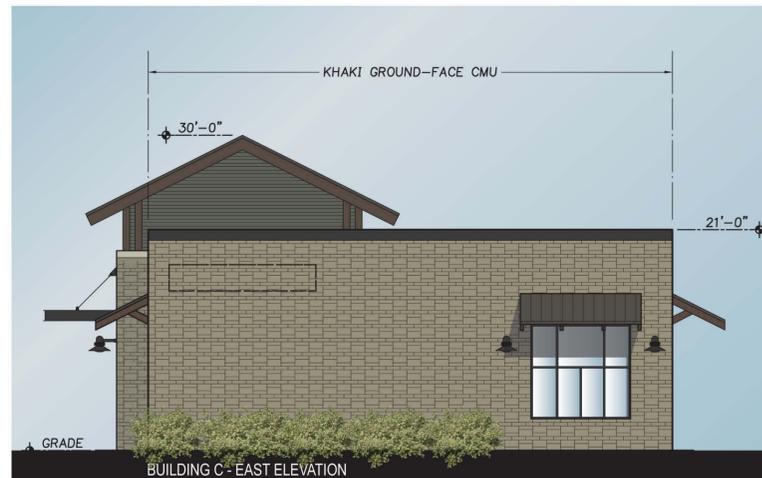
KEY PLAN
N.T.S.

KELSEY CREEK CENTER RENOVATION - DESIGN REVIEW

BUILDING B



BELLEVUE, WASHINGTON
SEPTEMBER 13, 2010

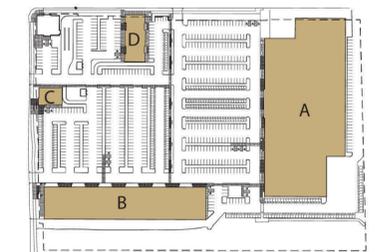


MATERIAL LEGEND

- STANDING SEAM MTL ROOF DARK BRONZE, U.N.O.
- ASHLAR CMU BLOCK SPLIT-FACE
- ASHLAR CMU BLOCK GROUND-FACE
- WOOD SIDING
- METAL SIDING
- STOREFRONT WINDOW SYSTEM- DARK BRONZE

PAINT SCHEDULE

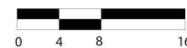
- P-1 SW, DOWNING STRAW
- P-2 SW, RENWICK OLIVE
- P-3 SW, PEWTER GREEN
- P-4 SW, DOWNING SAND
- P-5 SW, ROOKWOOD TERRA COTTA
- P-6 SW, ADAPTIVE GRAY



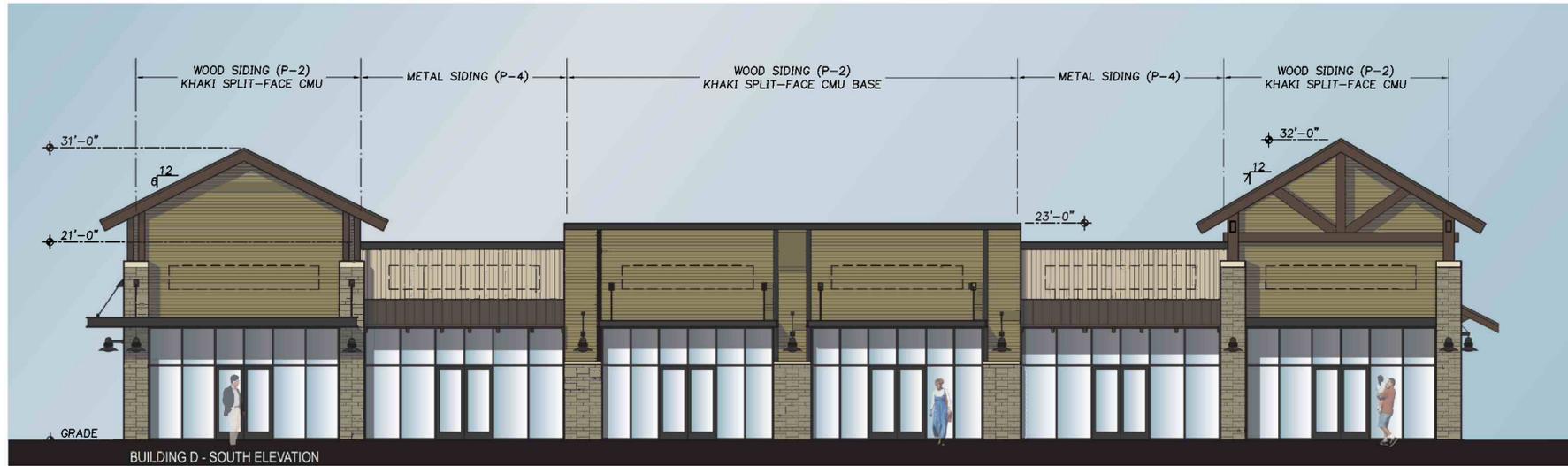
KEY PLAN
N.T.S.

KELSEY CREEK CENTER RENOVATION - DESIGN REVIEW

BUILDING C



BELLEVUE, WASHINGTON
SEPTEMBER 13, 2010

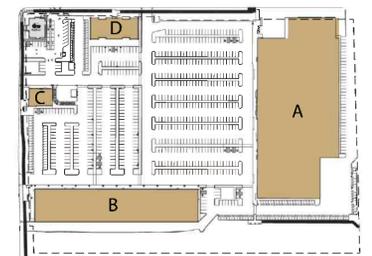


MATERIAL LEGEND

- STANDING SEAM MTL ROOF DARK BRONZE, U.N.O.
- WOOD SIDING
- ASHLAR CMU BLOCK SPLIT-FACE
- METAL SIDING
- ASHLAR CMU BLOCK GROUND-FACE
- STOREFRONT WINDOW SYSTEM

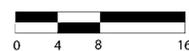
PAINT SCHEDULE

- P-1 SW, DOWNING STRAW
- P-2 SW, RENWICK OLIVE
- P-3 SW, PEWTER GREEN
- P-4 SW, DOWNING SAND
- P-5 SW, ROOKWOOD TERRA COTTA
- P-6 SW, ADAPTIVE GRAY

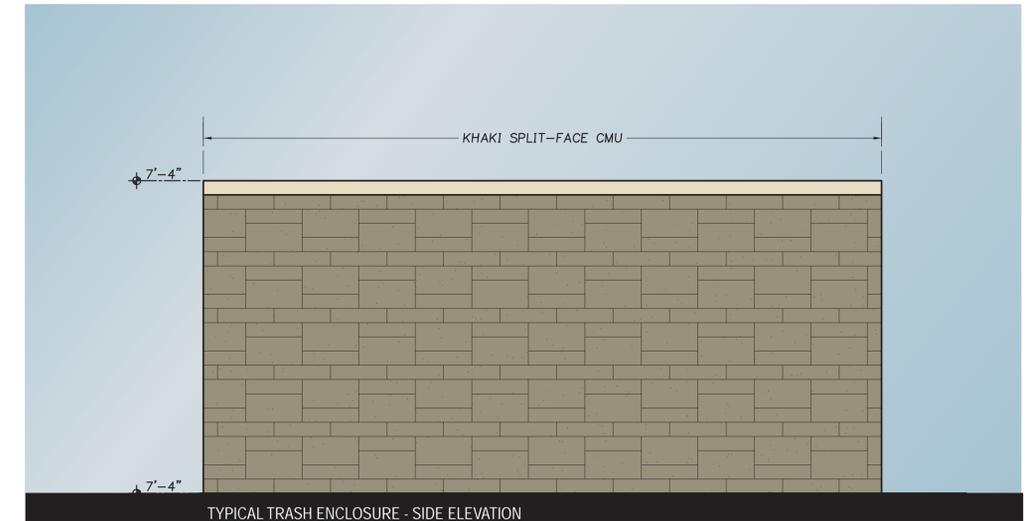
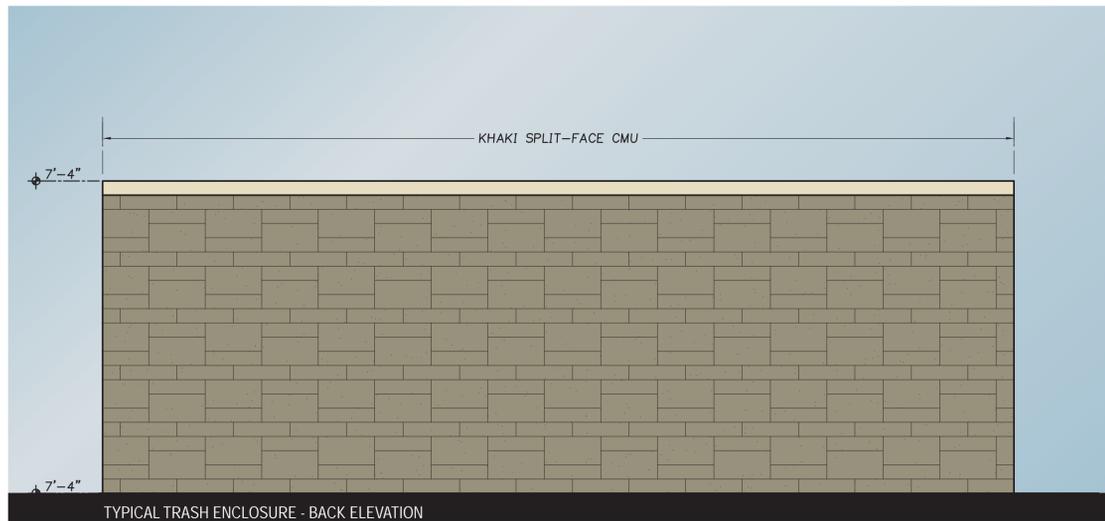
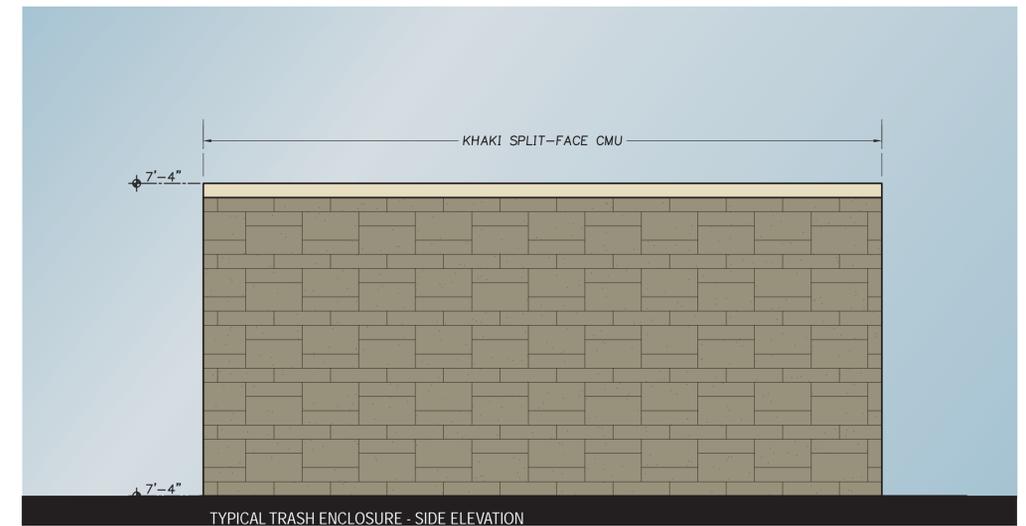
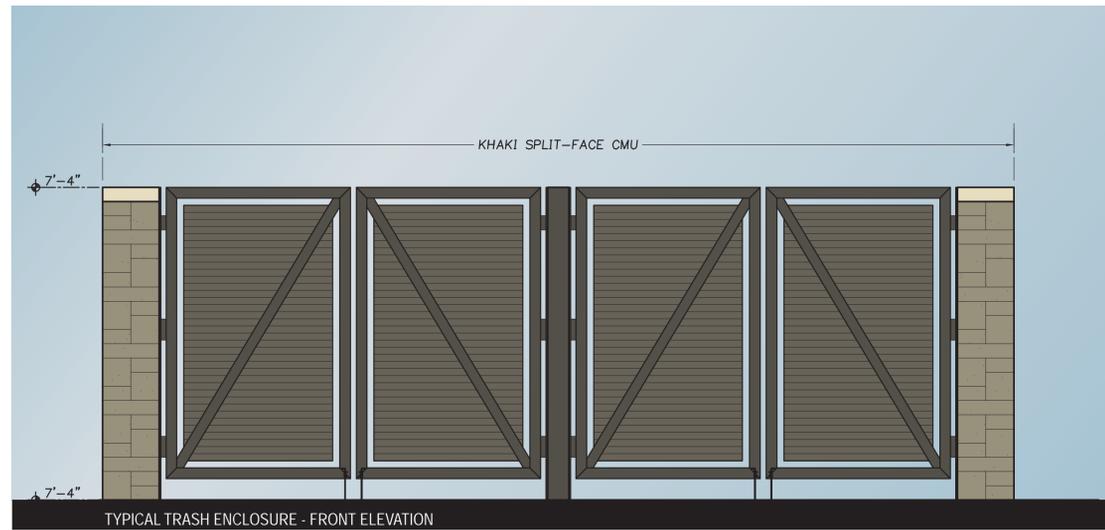


KEY PLAN
N.T.S.

KELSEY CREEK CENTER RENOVATION - DESIGN REVIEW
BUILDING D



BELLEVUE, WASHINGTON
NOVEMBER 19, 2010
REVISED: DECEMBER 21, 2010

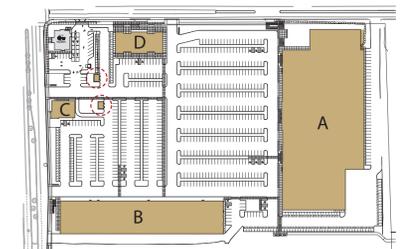


MATERIAL LEGEND

	STANDING SEAM MTL ROOF DARK BRONZE, U.N.O.		WOOD SIDING
	ASHLAR CMU BLOCK SPLIT-FACE		METAL SIDING
	ASHLAR CMU BLOCK GROUND-FACE		STOREFRONT WINDOW SYSTEM

PAINT SCHEDULE

	P-1 SW, DOWNING STRAW		P-4 SW, DOWNING SAND
	P-2 SW, RENWICK OLIVE		P-5 SW, ROOKWOOD TERRA COTTA
	P-3 SW, PEWTER GREEN		P-6 SW, ADAPTIVE GRAY



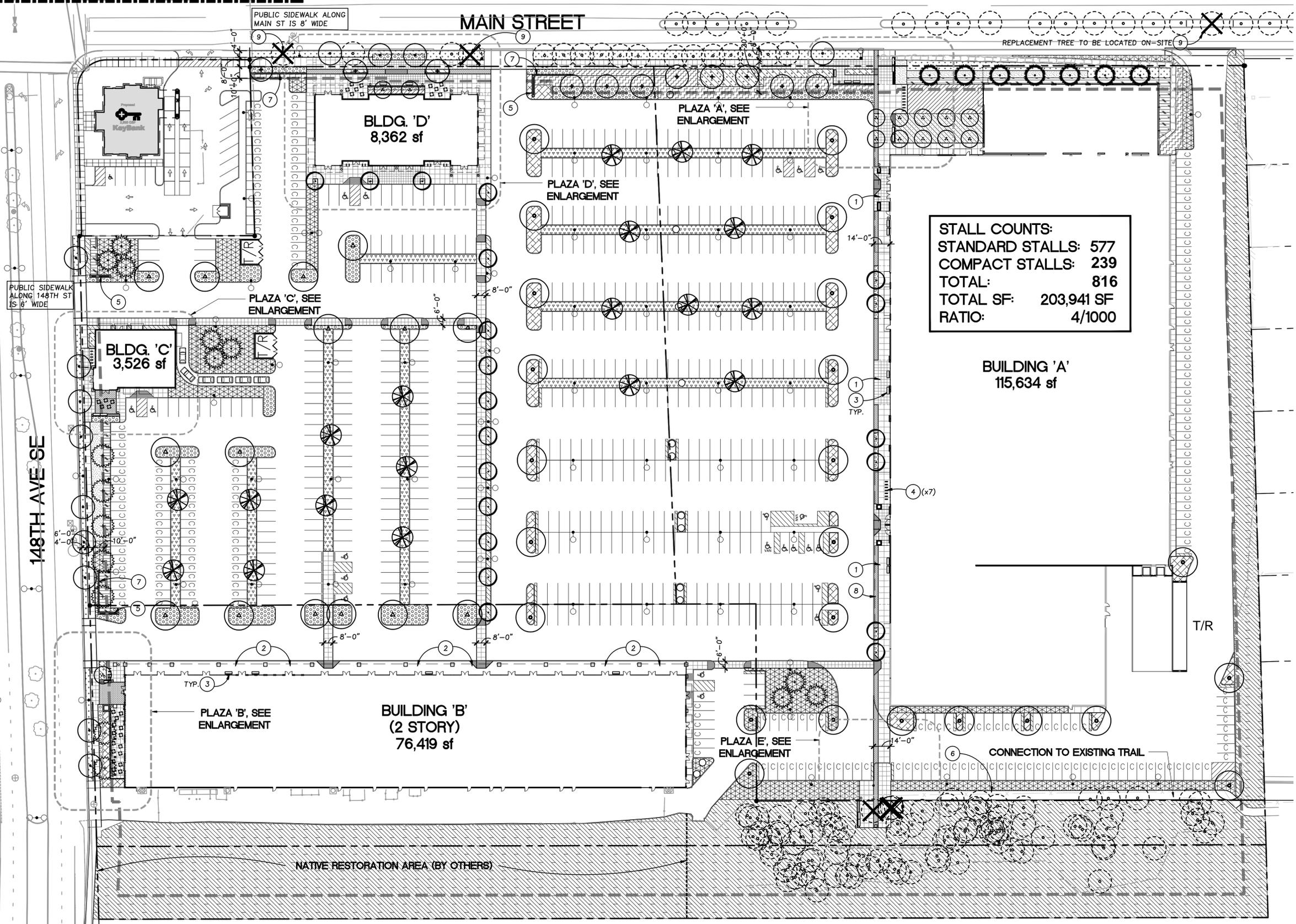
KEY PLAN
N.T.S.

KELSEY CREEK CENTER RENOVATION - DESIGN REVIEW

TYPICAL TRASH ENCLOSURE



BELLEVUE, WASHINGTON
NOVEMBER 19, 2010



STALL COUNTS:
STANDARD STALLS: 577
COMPACT STALLS: 239
TOTAL: 816
TOTAL SF: 203,941 SF
RATIO: 4/1000

PLANT MATERIAL SCHEDULE

SYMBOL	#	BOTANICAL NAME - COMMON NAME	SIZE/SPACING
TREES			
(Symbol)		EXISTING TREES TO REMAIN	AS SHOWN
(Symbol)		EXISTING TREE TO BE REMOVED	AS SHOWN
(Symbol)		PROPOSED STREET TREE TO MATCH EXISTING	2" CAL. B&B AS SHOWN
(Symbol)		ACER RUBRUM 'BOWHALL' BOWHALL MAPLE	2" CAL. B&B AS SHOWN
(Symbol)		CORNUS KOUSA KOUZA DOGWOOD	2" CAL. B&B AS SHOWN
(Symbol)		FRAXINUS PENNSYLVANICA 'PATMORE' PATMORE ASH	2" CAL. B&B AS SHOWN
(Symbol)		GINKGO BILOBA 'COLONNADE' GOLDEN COLONNADE GINKGO	2" CAL. B&B AS SHOWN
(Symbol)		PRUNUS SARGENTII 'COLUMNARIS' COLUMNAR SARGENT CHERRY	2" CAL. B&B AS SHOWN
(Symbol)		QUERCUS COCCINEA SCARLET OAK	2" CAL. B&B AS SHOWN
(Symbol)		CHAMAECYPARIS OBTUSA 'GRACILIS' SLENDER HINOKI CYPRESS	6-8" HT. B&B AS SHOWN
(Symbol)		PINUS STROBUS 'FASTIGIATA' COLUMNAR WHITE PINE	6-8" HT. B&B AS SHOWN
(Symbol)		PSEUDOTSUGA MENZIESII DOUGLAS FIR	6-8" HT. B&B AS SHOWN
SHRUBS			
(Symbol)		EXISTING VEGETATION TO REMAIN	
(Symbol)		BERBERIS THUNBERGII 'ROSE GLOW' ROSE GLOW BARBERRY	2 GAL. CONT. 36" O.C. OR AS SHOWN
(Symbol)		HELICTOTRICHON SEMPERVIRENS BLUE OAT GRASS	1 GAL. CONT. 24" O.C. OR AS SHOWN
(Symbol)		ILEX GLABRA 'SHAMROCK' SHAMROCK INKBERRY	2 GAL. CONT. 30" O.C. OR AS SHOWN
(Symbol)		MAHONIA AQUIFOLIUM 'COMPACTA' COMPACT OREGON GRAPE	2 GAL. CONT. 30" O.C.
(Symbol)		NANDINA DOMESTICA 'HARBOUR DWARF' HARBOUR DWARF NANDINA	2 GAL. CONT. 30" O.C.
(Symbol)		PENNISETUM ALOPECUROIDES 'HAMELN' DWARF FOUNTAIN GRASS	1 GAL. CONT. 30" O.C. OR AS SHOWN
(Symbol)		SALIX PURPUREA 'NANA' DWARF ARCTIC WILLOW	2 GAL. CONT. 36" O.C.
(Symbol)		SPIRAEA JAPONICA 'GOLDMOUND' GOLDMOUND SPIRAEA	2 GAL. CONT. 30" O.C.
(Symbol)		VIBURNUM DAVIDII DAVID'S VIBURNUM	2 GAL. CONT. 36" O.C.
GROUND COVER			
(Symbol)		ARCTOSTAPHYLOS UVA-URSI KINNIKINICK	4" POTS 24" O.C.
(Symbol)		FRAGARIA CHILOENSIS WILD STRAWBERRY	4" POTS 24" O.C.
STORMWATER PLANTINGS			
(Symbol)		SIDE SLOPES CORNUS SERICEA 'KELSEY' (KELSEY DOGWOOD)	1 GAL. @ 24" O.C.
(Symbol)		HELICTOTRICHON SEMPERVIRENS (BLUE OAT GRASS)	1 GAL. @ 24" O.C.
(Symbol)		FESTUCA MAIREI (ATLAS FESCUE)	1 GAL. @ 24" O.C.
(Symbol)		BOTTOM JUNCUS TENUIUS (SLENDER RUSH)	1 GAL. @ 12" O.C.

KEY NOTES

1. CONCRETE PAVING
2. EXISTING PAVING TO REMAIN
3. BENCH
4. BIKE RACK
5. ENTRY SIGN, BY OTHERS
6. TRAIL TO MATCH EXISTING
7. SCREEN WALL
8. PAVERS
9. EXISTING TREE TO BE REMOVED

GENERAL NOTES

1. FULLY AUTOMATIC IRRIGATION SYSTEM TO BE INSTALLED TO MAINTAIN ALL LANDSCAPE MATERIAL.

SITE DATA

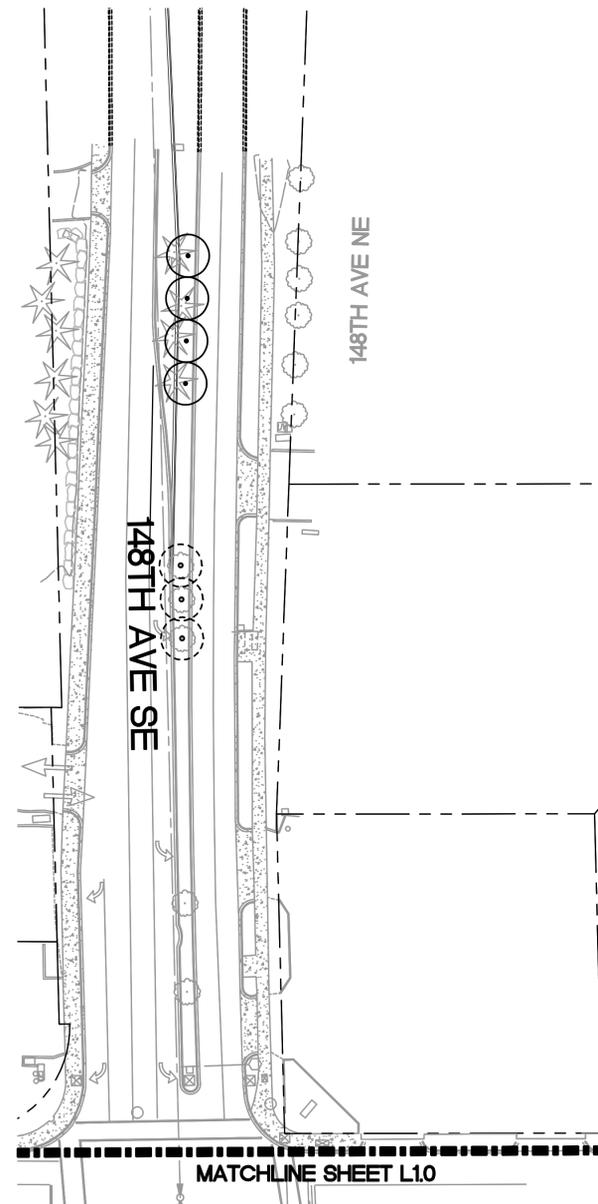
EXISTING SITE AREA	= 704,696 SF
REQUIRED PARKING LOT LANDSCAPE	= 28,455 SF
PROPOSED PARKING LOT LANDSCAPE	= 29,325 SF
REQUIRED BUFFER LANDSCAPE (EXCLUDES DRIVEWAYS)	= 52,320 SF
PROPOSED BUFFER LANDSCAPE	
PLANTING AREA	= 49,770 SF
PLAZA AREA	= 2,930 SF
TOTAL PROPOSED BUFFER LANDSCAPE	= 52,700 SF
TOTAL PROPOSED REQUIRED LANDSCAPE	= 82,025 SF
TOTAL PROPOSED REQUIRED NATIVE LANDSCAPE	= 49,778 SF (60%)

KELSEY CREEK CENTER RENOVATION - DESIGN REVIEW

PRELIMINARY LANDSCAPE PLAN - SHEET L1.0



BELLEVUE, WASHINGTON
 FEBRUARY 1, 2011



PLANT MATERIAL SCHEDULE

SYMBOL	#	BOTANICAL NAME – COMMON NAME	SIZE / SPACING
TREES			
		EXISTING TREES TO REMAIN	AS SHOWN
		EXISTING TREE TO BE REMOVED	AS SHOWN
		PROPOSED STREET TREE TO MATCH EXISTING	2" CAL. B&B AS SHOWN
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		CORNUS KOUSA KUSA DOGWOOD	2" CAL. B&B AS SHOWN
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		GINKGO BILOBA 'COLONNADE' GOLDEN COLONNADE GINKGO	2" CAL. B&B AS SHOWN
		PRUNUS SARGENTII 'COLUMNARIS' COLUMNAR SARGENT CHERRY	2" CAL. B&B AS SHOWN
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		CHAMAECYPARIS OBTUSA 'GRACILIS' SLENDER HINOKI CYPRESS	6-8' HT. B&B AS SHOWN
		PINUS STROBUS 'FASTIGIATA' COLUMNAR WHITE PINE	6-8' HT. B&B AS SHOWN
		PSEUDOTSUGA MENZIESII DOUGLASFIR	6-8' HT. B&B AS SHOWN
SHRUBS			
		EXISTING VEGETATION TO REMAIN	
		BERBERIS THUNBERGII 'ROSE GLOW' ROSE GLOW BARBERRY	2 GAL. CONT 36" O.C. OR AS SHOWN
		HELICTOTRICHON SEMPERVIRENS BLUE OAT GRASS	1 GAL. CONT 24" O.C. OR AS SHOWN
		ILEX GLABRA 'SHAMROCK' SHAMROCK INKBERRY	2 GAL. CONT 30" O.C. OR AS SHOWN
		MAHONIA AQUIFOLIUM 'COMPACTA' COMPACT OREGON GRAPE	2 GAL. CONT 30" O.C.
		NANDINA DOMESTICA 'HARBOUR DWARF' HARBOUR DWARF NANDINA	2 GAL. CONT 30" O.C.
		PENNISETUM ALOPECUROIDES 'HAMELN' DWARF FOUNTAIN GRASS	1 GAL. CONT 30" O.C. OR AS SHOWN
		SALIX PURPUREA 'NANA' DWARF ARCTIC WILLOW	2 GAL. CONT 36" O.C.
		SPIRAEA JAPONICA 'GOLDMOUND' GOLDMOUND SPIRAEA	2 GAL. CONT 30" O.C.
		VIBURNUM DAVIDII DAVID'S VIBURNUM	2 GAL. CONT 36" O.C.
GROUNDCOVER			
		ARCTOSTAPHYLOS UVA-URSI KINNICKINICK	4" POTS 24" O.C.
		FRAGARIA CHILOENSIS WILD STRAWBERRY	4" POTS 24" O.C.
STORMWATER PLANTINGS			
	SEE DETAIL 6/LB.1 FOR PLANTING LAYOUT	SIDE SLOPES CORNUS SERICEA 'KELSEYI' (KELSEY DOGWOOD) HELICTOTRICHON SEMPERVIRENS (BLUE OAT GRASS) FESTUCA MAIREI (ATLAS FESCUE)	1 GAL. @ 24" O.C. 1 GAL. @ 24" O.C. 1 GAL. @ 24" O.C.
		BOTTOM JUNCUS TENIUS (SLENDER RUSH)	1 GAL. @ 12" O.C.

KEY NOTES

1. CONCRETE PAVING
2. EXISTING PAVING TO REMAIN
3. BENCH
4. BIKE RACK
5. ENTRY SIGN, BY OTHERS
6. TRAIL TO MATCH EXISTING
7. SCREEN WALL
8. PAVERS
9. EXISTING TREE TO BE REMOVED

GENERAL NOTES

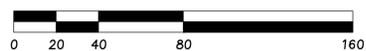
1. FULLY AUTOMATIC IRRIGATION SYSTEM TO BE INSTALLED TO MAINTAIN ALL LANDSCAPE MATERIAL.

SITE DATA

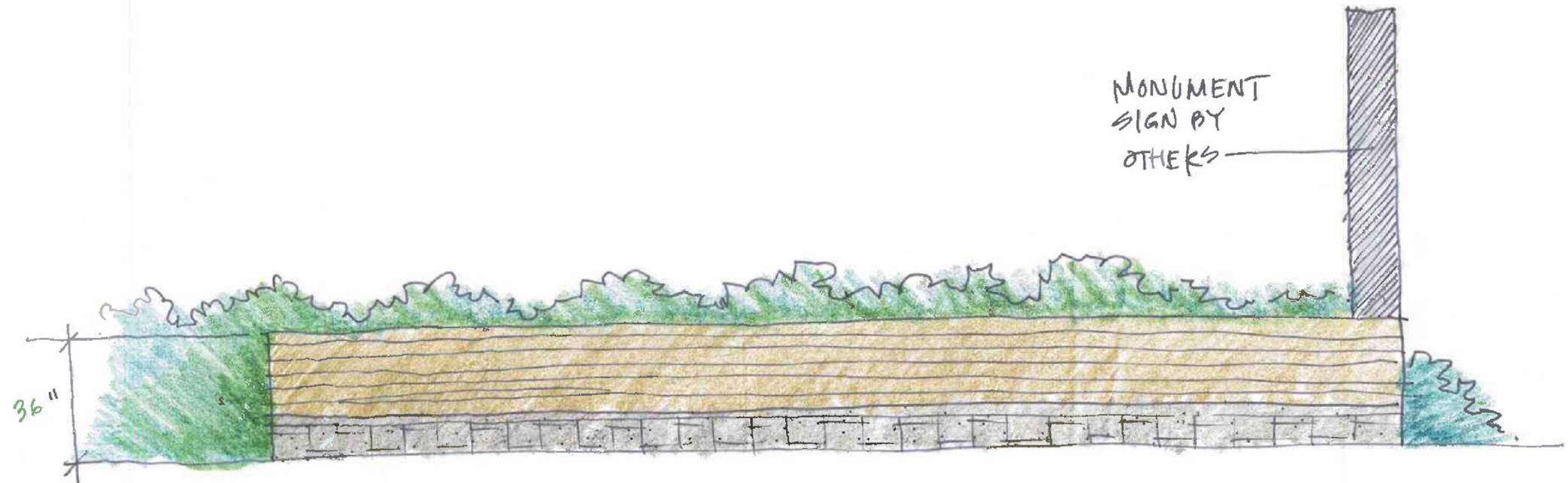
EXISTING SITE AREA	= 704,696 SF
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PROPOSED PARKING LOT LANDSCAPE	= 29,325 SF
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PLANTING AREA	= 49,770 SF
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TOTAL PROPOSED REQUIRED NATIVE LANDSCAPE	= 49,778 SF (60%)

KELSEY CREEK CENTER RENOVATION - DESIGN REVIEW

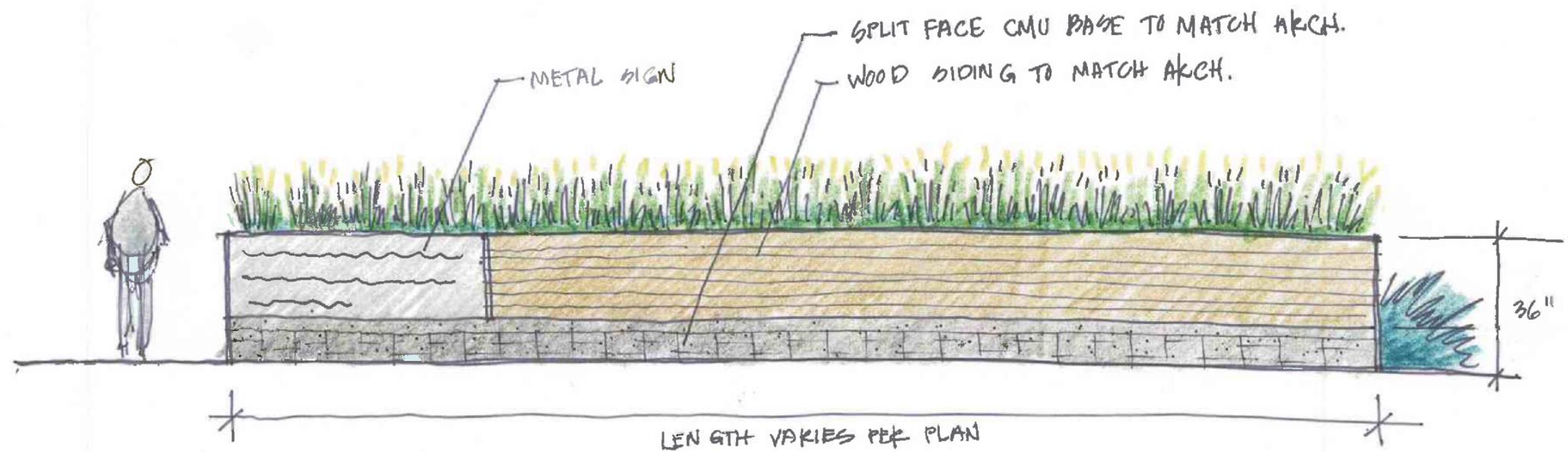
PRELIMINARY LANDSCAPE PLAN - SHEET L1.1



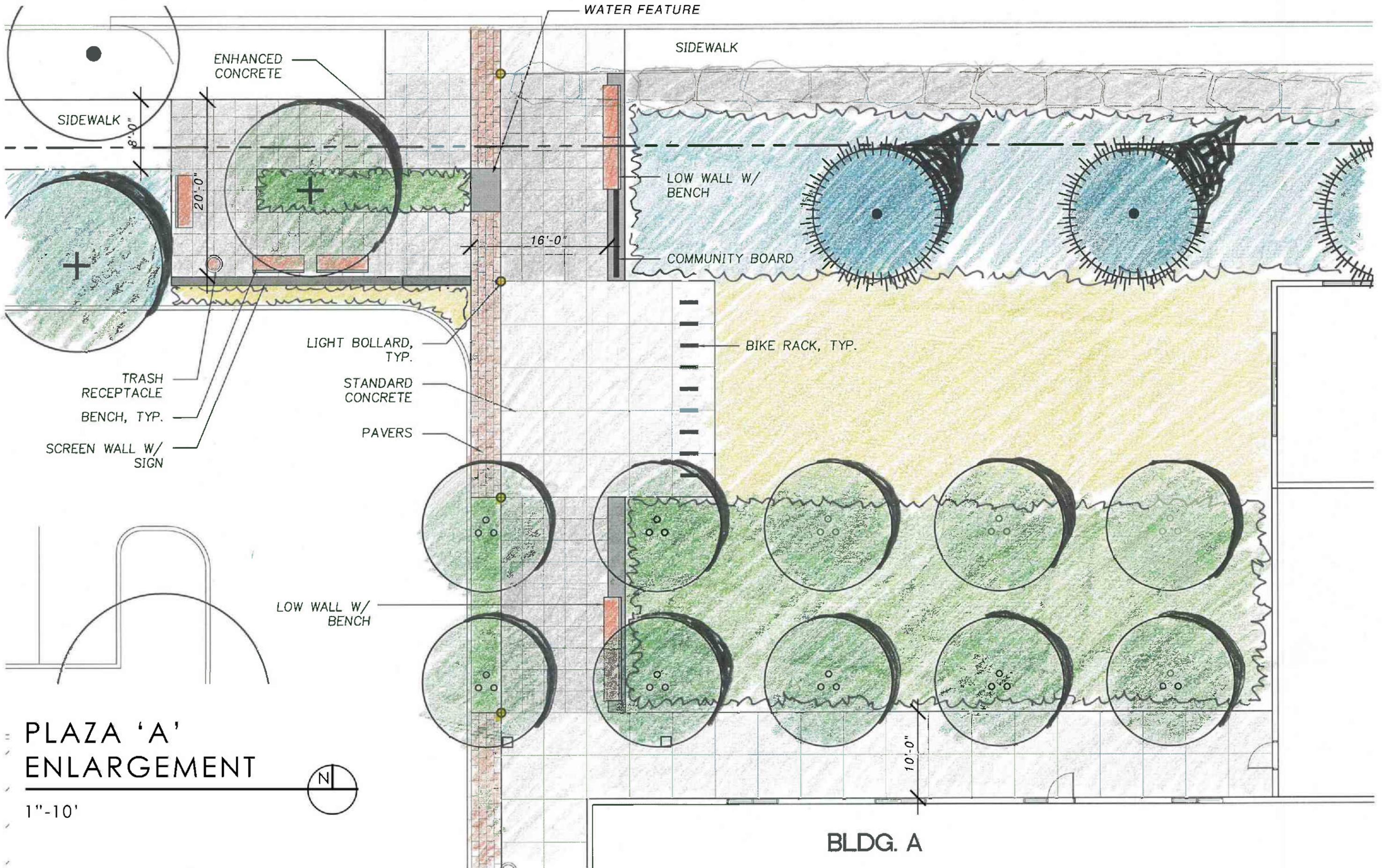
BELLEVUE, WASHINGTON
FEBRUARY 1, 2011



SCREEN WALL @ MONUMENT SIGN
 1/4" = 1'-0"

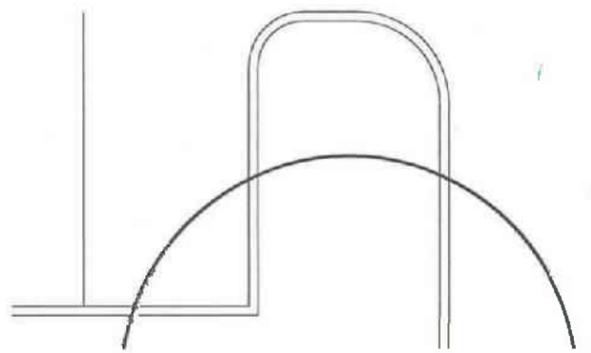
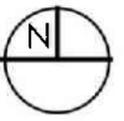


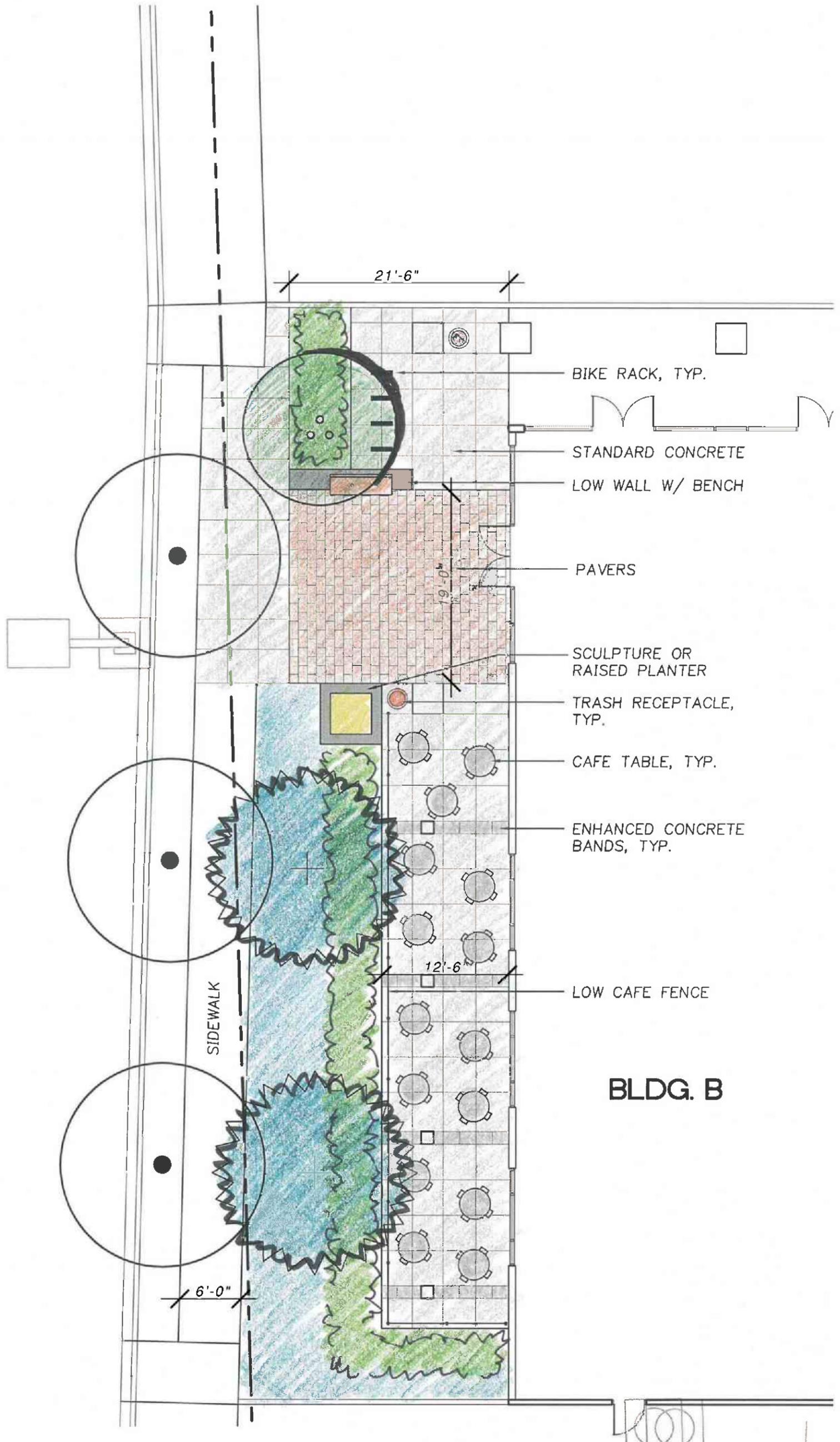
SCREEN WALL W/ SIGN
 1/4" = 1'-0"



PLAZA 'A'
ENLARGEMENT

1"=10'





BIKE RACK, TYP.

STANDARD CONCRETE

LOW WALL W/ BENCH

PAVERS

SCULPTURE OR RAISED PLANTER

TRASH RECEPTACLE, TYP.

CAFE TABLE, TYP.

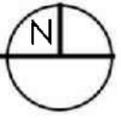
ENHANCED CONCRETE BANDS, TYP.

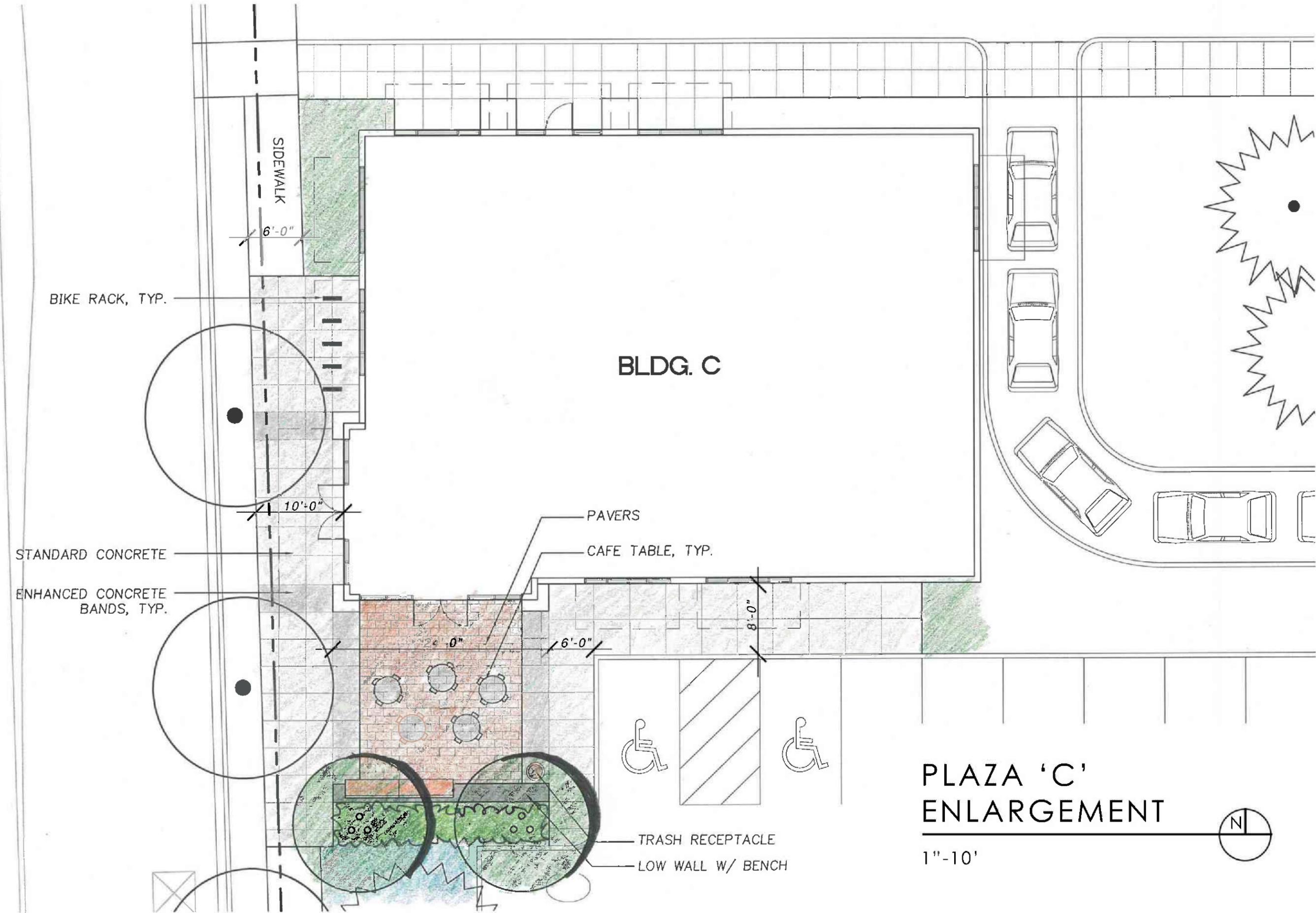
LOW CAFE FENCE

BLDG. B

**PLAZA 'B'
ENLARGEMENT**

1"=10'





BLDG. C

BIKE RACK, TYP.

SIDEWALK

6'-0"

10'-0"

STANDARD CONCRETE

ENHANCED CONCRETE BANDS, TYP.

PAVERS

CAFE TABLE, TYP.

24'-0"

6'-0"

8'-0"

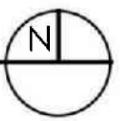


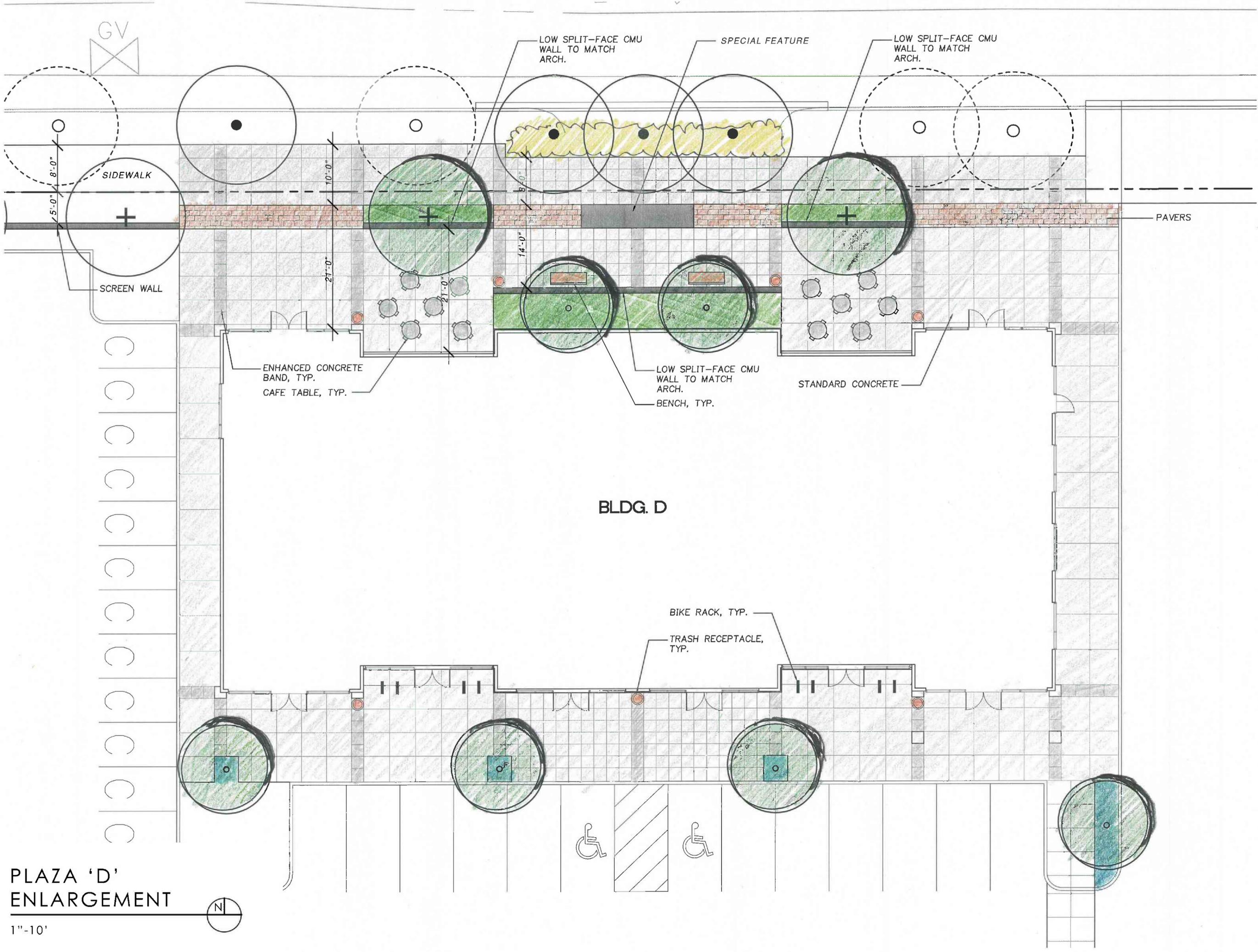
TRASH RECEPTACLE

LOW WALL W/ BENCH

PLAZA 'C'
ENLARGEMENT

1"-10'





LOW SPLIT-FACE CMU WALL TO MATCH ARCH.

SPECIAL FEATURE

LOW SPLIT-FACE CMU WALL TO MATCH ARCH.

SIDEWALK

PAVERS

SCREEN WALL

ENHANCED CONCRETE BAND, TYP.
CAFE TABLE, TYP.

LOW SPLIT-FACE CMU WALL TO MATCH ARCH.
BENCH, TYP.

STANDARD CONCRETE

BLDG. D

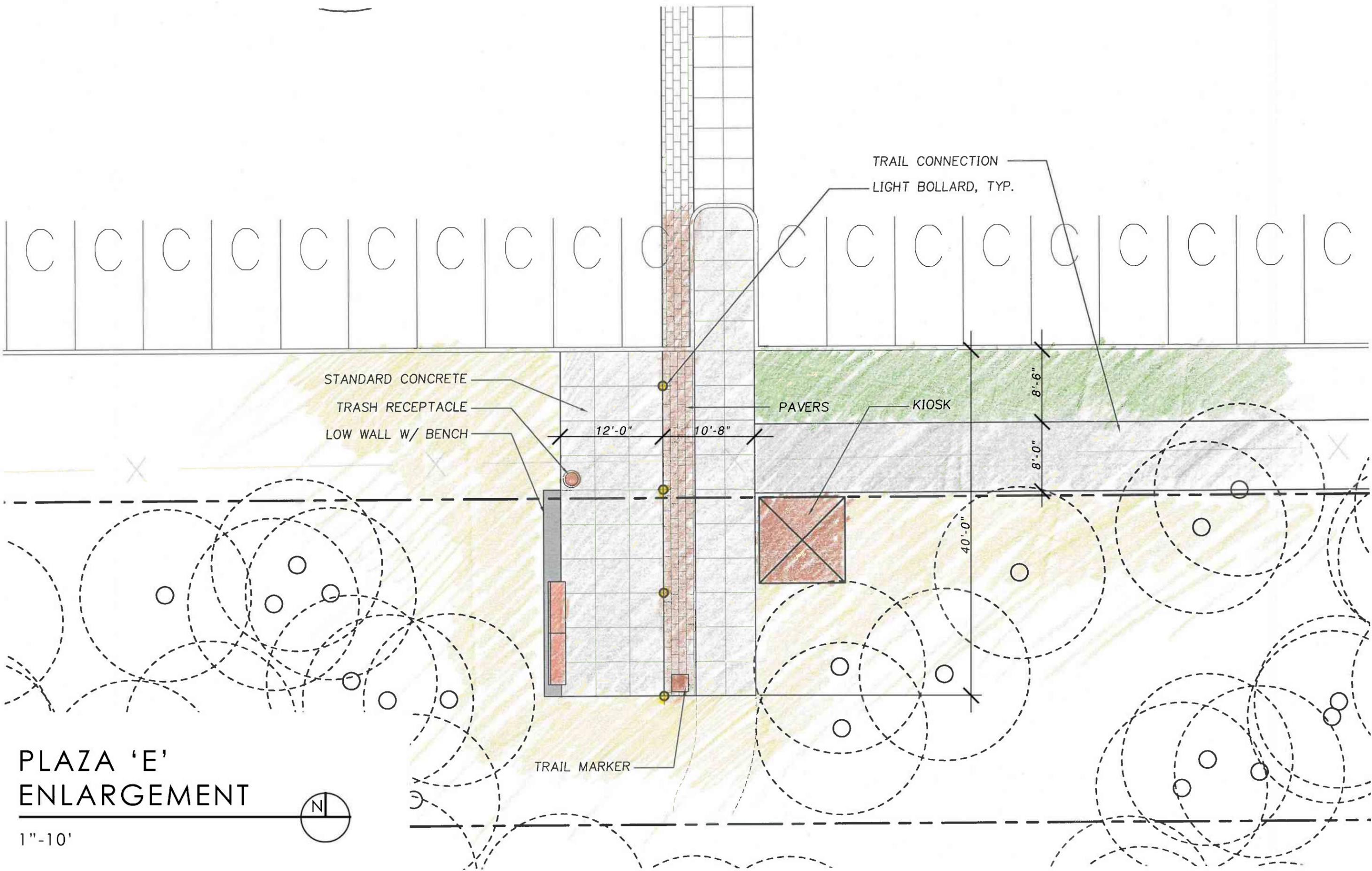
BIKE RACK, TYP.

TRASH RECEPTACLE, TYP.

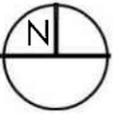
PLAZA 'D' ENLARGEMENT



1"=10'



PLAZA 'E'
ENLARGEMENT



1"-10'