



DEVELOPMENT SERVICES DEPARTMENT
 ENVIRONMENTAL COORDINATOR
 450 110TH Avenue NE, P.O. BOX 90012
 Bellevue, WA 98009-9012

DETERMINATION OF NON-SIGNIFICANCE

PROPONENT:

John Ellingsen, Barghausen Consulting Engineers

LOCATION OF PROPOSAL:

106 148th Avenue SE, Bellevue, WA

DESCRIPTION OF PROPOSAL:

Application for Design Review approval and a SEPA Determination to construct a 2,850 square foot, 22 foot tall Keybank building with associated parking, circulation and landscape improvements.

FILE NUMBER: 10-121139-LD

The Environmental Coordinator of the City of Bellevue has determined that this proposal does not have a probable significant adverse impact upon the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(C). This decision was made after the Bellevue Environmental Coordinator reviewed the completed environmental checklist and information filed with the Development Services Section. This information is available to the public on request.

- There is no comment period for this DNS
- This DNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on the DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's Office by 5 p.m. on _____.
- This DNS is issued under WAC 197-11-340(2) and is subject to a 14-day comment from the date below. Comments must be submitted by 5 p.m. on _____.

This DNS may be withdrawn at any time if the proposal is modified so that it is likely to have significant adverse environmental impacts; if there is significant new information indicating, or on, a proposals probable significant adverse environmental impacts (unless a non-exempt license has been issued if the proposal is a private project); or if the DNS was procured by misrepresentation or lack of material disclosure.

Carol V. Hayward
 Environmental Coordinator

4/26/11
 Date

OTHERS TO RECEIVE THIS DOCUMENT:

- State Department of Fish and Wildlife
- State Department of Ecology, Shoreline Planner N.W. Region
- Army Corps of Engineers
- Attorney General
- Muckleshoot Indian Tribe



**City of Bellevue
Development Services Department
Land Use Staff Report**

Proposal Name: **Keybank @ Kelsey Creek Center**

Proposal Address: 3240 156th Avenue SE

Proposal Description: Application for Design Review approval and a SEPA Determination to construct a 2,850 square foot, 22 foot tall Keybank building with associated parking, circulation and landscape improvements.

File Number: **10-121139-LD**

Applicant: John Ellingsen, Barghausen Consulting Engineers

Decisions Included: Process II, Combined Design Review and SEPA Determination

Planner: Mike Upston, AICP 425/452-2970

State Environmental Policy Act Threshold Determination: **Mitigated Determination of Non-Significance (MDNS)**

Carol V. Helland

Carol V. Helland, Environmental Coordinator
Development Services Department

Director's Decision: **Approval with Conditions**
Michael A. Brennan, Director
Development Services Department

By: *Carol V. Helland*

Carol V. Helland, Land Use Director
Development Services Department

Notice of Application: 10/21/10
Decision Publication Date: 4/28/11
Appeal Deadline: 5/12/11
Expiration of Vesting for Decision: 4/28/13

For information on how to appeal a proposal, visit Development Services at City Hall or call (425) 452-6800. Comments on State Environmental Policy Act (SEPA) Determinations can be made with or without appealing the proposal within the noted comment period for a SEPA Determination. Appeal of the Decision must be received in the City's Clerk's Office by 5 PM on the date noted for appeal of the decision.

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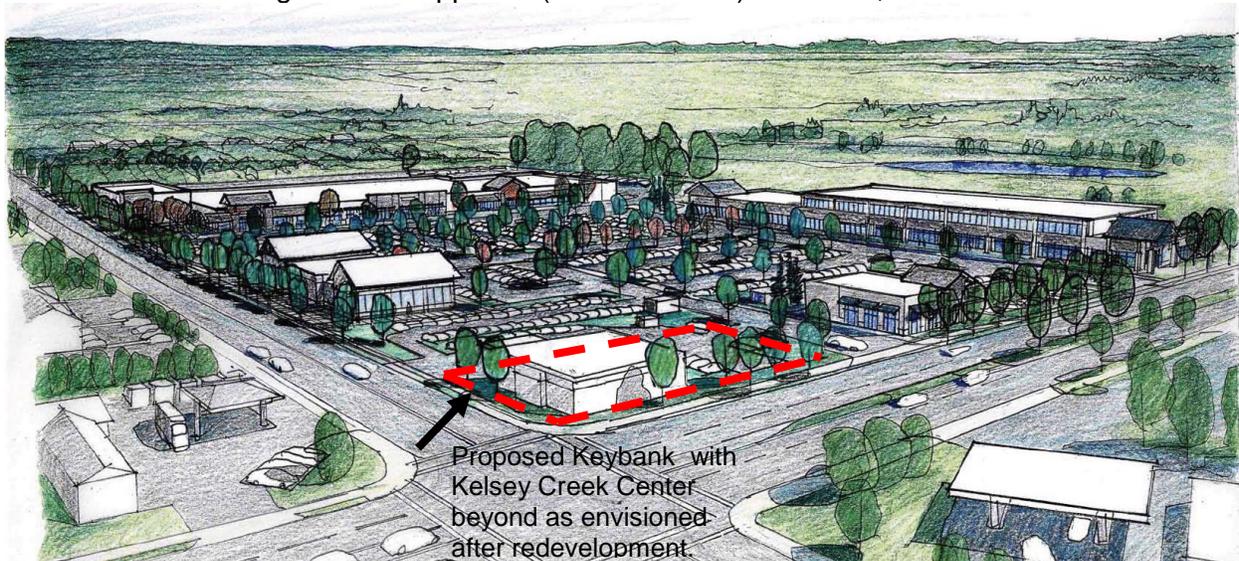
Attached: Project Plans & Drawings

I. REQUEST/PROPOSAL DESCRIPTION

The applicant requests Design Review approval and a SEPA determination to construct a 2,850 square foot, 22 foot tall Keybank building with associated parking, circulation and landscape improvements within the Neighborhood Business land use district.

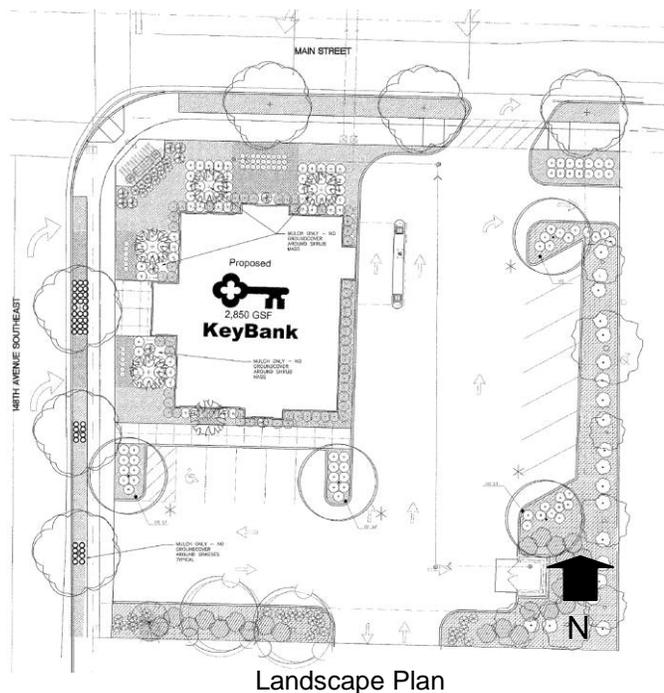
A. Site Design

Site design objectives focus on providing an easily accessible parking area and drive-thru for vehicles while providing for basic pedestrian needs, a gateway corner feature, and landscape improvements compatible with the planned redevelopment of Kelsey Creek Center which received its own Design Review approval (10-121806-LD) March 31, 2011.



The existing site has very little landscape area and the proposal increases it significantly. 85% of the lot is currently paved and the proposal reduces this to 74%, resulting in much more landscape area. This includes a mix of new deciduous and evergreen trees, shrubs, and groundcovers in a palette that will be compatible with the landscape plant selections planned for the adjacent Kelsey Creek Center redevelopment.

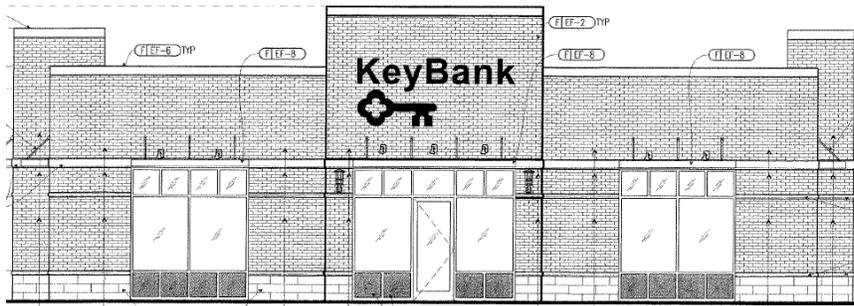
See color sketch above, landscape plan to the right, and attached Project Plans and Drawings for additional information about the site design.



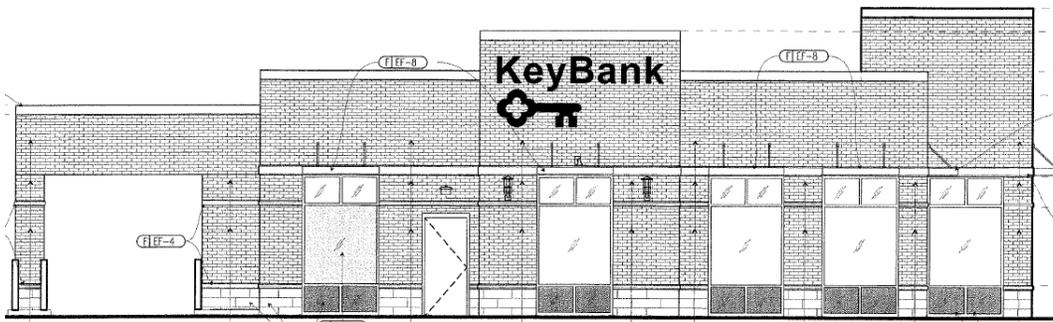
Landscape Plan

B. Building Design

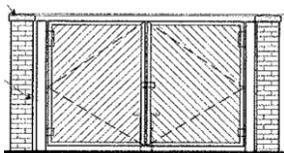
Established nearby uses include gas stations, a church, and a hospitality use to the north; a gas station and small scale office uses to the west; and Kelsey Creek Center to the adjacent south and east. The overall size, height, and architectural treatments of the proposed building are compatible with this established pattern. The building's exterior will be clad in a red brick, with stone accent bands and base, black aluminum storefront windows with vision glass, and aluminum coping along the roofline. The drive-thru canopy will have matching brick, stone, and coping. Last, the detached trash & recycling area will have matching brick, stone cap, and chain link gate with screen color to match the brick. All of the signs needed for the bank are graphically depicted within a sign plan set which is part of this Design Review approval. After this Design Review has been approved, a subsequent sign permit consistent with plans submitted with this application will have to be approved before any signs could be installed. The sign drawings are not included in the attached Project Plans & Drawings, but they are in the Design Review project file at the City Hall Records Office.



West Elevation (facing 148th Avenue SE)



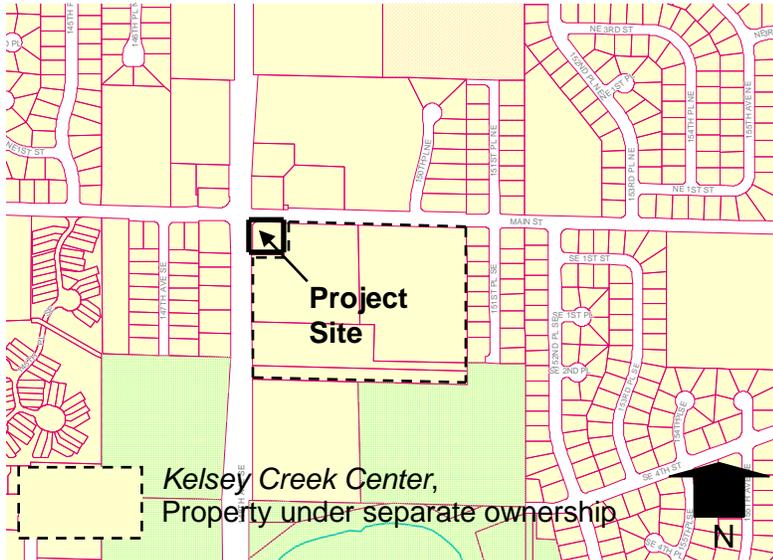
North Elevation (facing Main Street)



Trash &
Recycling
Enclosure

See the building elevations above, as well as the attached Project Plans and Drawings for full information about the building design.

II. SITE DESCRIPTION, ZONING, & LAND USE CONTEXT



The subject property is located on the southeast corner of Main Street and 148th Avenue SE (see left). This ½-acre project site was formerly used as a gas station.

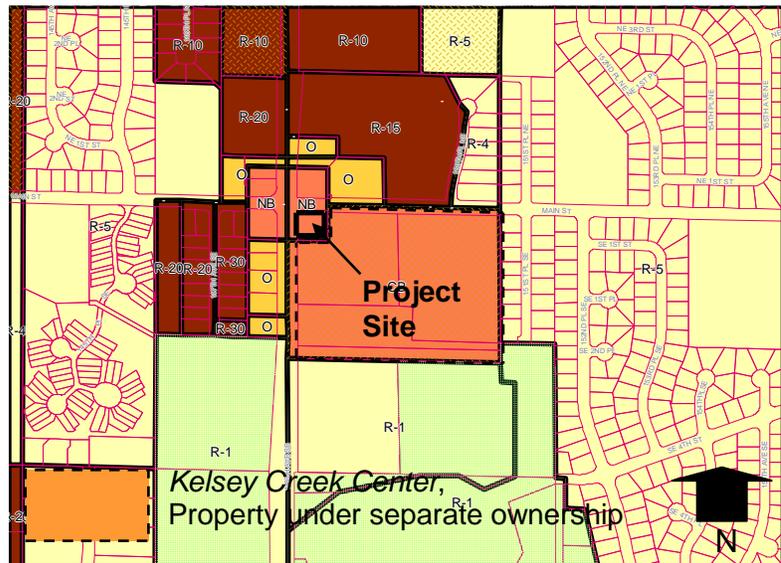
The property is located within the Neighborhood Business (NB) land use district (see below). The NB zoning designation is overlaid by the Community Retail Design District, within which project proposals must receive a Design Review approval prior to the issuance of construction permits (LUC 20.25I).

This commercial site is surrounded by other commercial property on all sides.

North: Gas stations, a church, and hospitality (Neighborhood Business & Office zoning).

West: Gas station and small scale offices (Neighborhood Business & Office zoning).

South & East: Kelsey Creek Center. (Community Business base zoning, with a concomitant zoning agreement overlay).



III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS

A. General Provisions of the Land Use Code

1. Use

Uses are regulated by LUC 20.10.400 (Use Charts), LUC 20.20 (General Development Requirements) and LUC 20.25I Community Retail Design District. The use proposed for this project is permitted in the NB land use district subject to Design Review approval.

2. Dimensional Requirements

All applicable dimensional requirements of the Land Use Code will be met. Refer to the following chart for specific information.

	Permitted/Required	Proposed
Height	20 feet maximum (LUC 20.20.070)	22 feet
Lot Coverage	35% (LUC 20.20.010)	13%
Setbacks	None, except to meet minimum landscape buffer and gateway boulevard & intersection requirements as described in Subsections III.B & C below. (LUC 20.20.010, 20.25I.040 & 050)	75 feet – east/rear side 14 feet – west/front side 65 feet – south side 23 feet – north side
Parking	4 stalls/1,000 SF (LUC 20.20.590) 2,613 SF x 4/1,000 = 10 stalls minimum	12 parking stalls
Landscape	1) Street frontage and internal property lines: 10 feet, Type III 2) Parking Area: Type V 17.5 square feet per stall x 12 stalls = 210 SF (LUC 20.20.520)	1) 10 feet, Type III for internal property lines. Variable along the street frontage greater than 10 feet. 2) 508 SF, Type V. <u>In addition, conditions of approval are included in Section X of this report requiring replacement of damaged street trees, landscape installation and maintenance assurance devices.</u>
Queue at Drive-Up	7 car lengths x 12 feet/car = 84 feet (LUC 20.20.590.K.5)	84 feet
Minimum Lot Area	No minimum (LUC 20.20.010)	22,070 square feet

<p>Recycling & Solid Waste Collection Area</p>	<p>At least one area of a size measuring at least 14.25 SF, as calculated here (LUC 20.20.725): 5 SF/1,000 SF @ 2,850 SF = 14.25 SF min</p>	<p>A 48 SF screened dumpster and recycling enclosure is proposed. In addition, <u>a condition of approval is included in Section X of this report requiring endorsement by Allied Waste prior to building permit issuance.</u></p>
<p>Mechanical Equipment</p>	<p>Locate on the roof or below grade and visually screen, unless this requirement is modified by the City for projects requiring discretionary approval per LUC 20.20.525.C.5 & 6.</p>	<p>All mechanical equipment is proposed on the roof behind parapet walls. Since the plans do not provide a specific location and dimension, <u>a condition of approval is included in Section X of this report requiring these details on the building permit plans.</u></p>
<p>Minimum Street Frontage Width</p>	<p>0 feet (LUC 20.20.010)</p>	<p>148th Ave SE - 124 feet Main Street – 123 feet</p>
<p>Minimum Lot Width</p>	<p>0 feet (LUC 20.20.010)</p>	<p>148 feet</p>
<p>Maximum Impervious Surface</p>	<p>80% (LUC 20.25C.040)</p>	<p>74%</p>
<p>Loading</p>	<p>Off-street, at least 10'x55' plus maneuvering length and accessible from a public street. (LUC 20.20.590.K.4)</p>	<p>Loading will be short term and occur at the south side of the building, away from the street and driveway. In addition to the loading provisions proposed, <u>a condition of approval is included in Section X of this report which expressly prohibits loading from the public right of way.</u></p>

B. Community Retail Design District Design Guidelines (LUC 20.25.I.040)

Building Design Guidelines

1. *Buildings in a multi-building complex should achieve unity of design through use of similar architectural elements, such as roof form, exterior building materials, colors, and window pattern.*
2. *Individual buildings should incorporate similar design elements, such as surface materials, color, roof treatment, windows and doors, on all sides of the building to achieve a unity of design.*

The somewhat standardized KeyBank design incorporates brick, stone, and metal awnings that will be compatible with the architecture proposed for redevelopment of Kelsey Creek Center. Refer to the drawings on pages 2 & 3 of this report and the attached Project Plans & Drawings for more information.

Site Design Guidelines

1. *Parking areas should be designed to minimize conflicts between pedestrian and vehicular movements. Parking area landscaping should be used to define and separate parking, access, and pedestrian areas within parking lots.*
2. *The landscape design for the site should include plantings which emphasize the major points of pedestrian and vehicular access to the site.*

Parking areas have been designed to minimize the conflicts between pedestrians and vehicles, with landscaping to enhance the pedestrian areas particularly on the street side of the building.

3. *Site features such as fences, walls, refuse and recycle enclosures, and light fixtures should be designed to be consistent with the scale and architectural design of the primary structure(s). Such site features should be designed and located to contribute to the pedestrian environment of the site development.*

The recycling and solid waste enclosure proposed at the rear of the site will have minimal visibility to the streets and sidewalk, and will be made of materials consistent with the building. Light fixtures, the bench within the corner landscape area, and all other site furnishings are proposed to be of a style that will be compatible with the building.

4. *Loading areas should not be located between the building and the street unless there is no alternative location possible. Loading areas, if located between the building and the street, should be oriented away from the street and screened to minimize views of the loading area from the street and sidewalk. Loading areas should not be located on the side of a building which faces toward a residential use.*

Loading and refuse collection is proposed to occur on-site on the side of the building that faces away from the street. These activities will not be readily apparent to pedestrians or drivers on 148th Avenue SE or Main Street.

5. *In multiple-building complexes, buildings should be located to facilitate safe and comfortable pedestrian movement between buildings. On sites which are adjacent to other properties within the Community Retail Design District, building location should be chosen to facilitate pedestrian and vehicular connections to buildings on those adjacent properties.*
6. *In locations and districts specifically described in the Comprehensive Plan Urban Design Element Figure UD.1, consideration should be given to locating buildings closer to the public street with entrances to the buildings from the public sidewalk, with no intervening parking or driving area. Corner locations are particularly appropriate for this treatment.*
7. *Opportunities should be found for safe, convenient, and pleasant pedestrian connections to existing transit facilities. Where needed, shelters and lay-bys for transit vehicles should be incorporated into the site development.*

Building placement is influenced by the intent to provide convenient and efficient vehicular circulation to the building and drive-thru, safe pedestrian access on this parcel and to the adjacent Kelsey Creek Center, buildings near and facing the public sidewalk, and a gateway feature at the corner. As stated for items 1 – 3 above, parking areas have been designed to minimize the conflicts between pedestrians and vehicles, with additional plaza, walkway, and landscape areas to enhance the pedestrian experience throughout the site. Further, pedestrian access is provided to and from the public sidewalks where existing transit services are provided.

C. Community Retail Design District Design Standards (LUC 20.25.I.050)

Special Corner Feature

If the property is located at the intersection of two arterial streets, the site development shall incorporate a special corner feature at the corner of the site. A special corner feature can be a landscape feature, seasonal color planting area, sculpture or water feature. The feature should provide a visual landmark and some amount of seating area. If the property is not located at an intersection, a similar feature should be considered in conjunction with a transit stop or at the primary access point to the site.

Because the project is oriented to the intersection of 148th Avenue SE and Main Street, a gateway feature is included consisting of a plaza, expanded landscape area, and seating. The area will serve as a place of refuge for pedestrians waiting to cross the street, as a resting place, and as an identifying element for this major street intersection.

Building Design Standards

1. *The design of buildings shall incorporate elements such as special architectural details, distinctive color schemes, special art and other features, which are sensitive to and enhance the surrounding area and serve to distinguish the complex from other retail complexes in the City.*
2. *The sides of a building which face toward a public street shall include public entrances to the building and windows to provide visual access to the activity within the building.*
3. *The sides of a building which face toward an adjoining property, but not toward a public street, shall include elements such as windows, doors, color, texture, landscaping or wall treatment to provide visual interest and prevent the development of a long continuous blank wall.*

4. Building design shall provide for architecturally integrated signage. Signs shall be in proportion to the development and oriented to the main direction access and to pedestrian movement. Signs shall meet the requirements of Chapter 22B.10 BCC, Bellevue Sign Code.

The building will have architectural features that are sensitive to the surrounding area and compatible with the architecture proposed for Kelsey Creek Center such as brick, stone, metal roof, and lighting with cut-off panels so light doesn't extend beyond the property. The street sides of the building will have windows with visual access to the activity within the building, and interesting architectural elements on all facades since they are viewed and accessed from all four sides. See the drawings on page 3 and the attached Project Plans & Drawings.

Internal Walkways

- 1. Walkways, of six feet in width minimum, shall be provided from the public sidewalk or right-of-way to the building. At a minimum, walkways shall be located to connect focus points of pedestrian activity such as transit stops and street crossings to the major building entry points.*
- 2. Walkways shall be provided to connect with walkways or potential walkway locations on adjoining properties in the district to create an integrated internal walkway system along the desired lines of pedestrian travel. The width of the walkway should be commensurate with the anticipated level of pedestrian activity along the connecting walkway. On the sides of the building which provide public access into the building, the walkway should be wide enough to allow for sidewalk seating area or window gazing as well as pedestrian travel.*
- 3. Internal walkway surfaces shall be designed to be visually attractive and distinguishable from driving surfaces through the use of durable, low maintenance surface materials such as pavers, bricks, or scored concrete to enhance pedestrian safety and comfort.*

Concrete walkways are proposed throughout the site and plaza spaces with special paving between the building and the public sidewalk providing for safe and pleasant access. Parking areas have been designed to minimize the conflicts between pedestrians and vehicles, with additional landscape areas to provide separation and enhance the pedestrian experience.

Other Development Standards

- 1. Rooftop equipment shall be visually screened pursuant to LUC 20.20.525. The design and color of equipment should be integrated with the building architectural style.*

Refer to Section III.A.2 regarding rooftop mechanical equipment and screening.

- 2. The outdoor display of building materials and similar bulky products shall be screened from views from the public street, sidewalk, and properties outside the district with a durable, solid wall or fence, or an evergreen hedge or a combination of the above. The screening requirement does not apply to the display of seasonal products of a decorative nature such as bedding plants, Halloween pumpkins, and holiday greens.*

No outdoor display is proposed, but a condition of approval is included in Section X of this report should outdoor display be proposed in the future if a different tenant occupies this building.

3. Bicycle racks shall be provided on-site. Facilities for a minimum of 10 bicycles shall be provided for developments having 100 or fewer parking stalls.

Bicycle racks are not shown on the site plan. Therefore, a condition of approval is included in Section X of this report requiring enough bike racks for at least 10 bicycles.

IV. PUBLIC NOTICE AND COMMENT

Application Date: August 25, 2010
Application Completeness Date: October 1, 2010
Notice of Application published: October 7, 2010
Public Notice Sign installed: October 7, 2010
Minimum Comment Period ended: October 21, 2010

Although the minimum required public comment period ended on October 21, 2010, comments were accepted up to the date of this decision. No written public comments were received prior to finalization of this staff report.

V. TECHNICAL REVIEW

Clearing & Grading, Building and Fire

These reviewers have no comments or requirements for this Design Review; all comments are reserved for construction permit review.

Utilities

The City has adequate capacity for providing water, sewer and storm drainage service for this proposal. See Section X of this report for Utilities related Conditions of Approval.

Transportation

Site Access: Access to the proposed project will be provided via a driveway off 148th Avenue SE. That driveway will be located on the adjacent Kelsey Creek Center property approximately 40 feet south of Keybank's southern property line. Use of, and responsibility for, said driveway will be shared with the adjacent property under a private easement agreement between the property owners. This agreement must be executed and recorded against the affected properties at the King County Recorder's Office prior to issuance of a building permit for this project.

Egress from the site will be available via three options listed below.

1. Use of the same shared driveway described above.
2. Use of a one-way outbound drive aisle into a portion of the Kelsey Creek Center's parking lot near the northeastern corner of the Keybank site.
3. Use of a one-way, outbound, right-turn-only driveway onto Main Street, located approximately 110 feet east of 148th Avenue SE. During the subsequent review of engineering plans for this development, the Transportation Department may decide to require the developer to install a c-curb within Main Street in order to further restrict undesirable movements at that

driveway and improve traffic operations and safety. After occupancy of the site, the city may, at city expense, decide to install such a c-curb in Main Street at any time. Loading/unloading shall be provided within the Keybank site or, by agreement between the property owners, in the Kelsey Creek Center site. Note that on-street loading/unloading will not be allowed.

Street Frontage Improvements: In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, the construction of street frontage improvements is required as a condition of development approval. The design of the improvements must conform to the requirements of the Americans with Disabilities Act (ADA) and the Transportation Development Code (BCC 14.60), and the provisions of the Transportation Department Design Manual.

1. A combined street frontage plan is required as part of the final clearing and grading plan set. This plan must show street trees, landscaping strips, sidewalks, driveway aprons, handicapped ramps, crosswalks, the existing traffic signal pole and related equipment, and new pedestrian push buttons and related equipment. Street trees must be shown with the proper separation, which is generally 25 feet apart, 25 feet from the traffic signal pole, and at least ten feet from any driveway edge as defined by Point A in standard drawing DEV-6. Street tree spacing must consider trees and streetlights planned for adjacent property. No new streetlights are required.

2. Whenever public sidewalks, crosswalks, handicapped ramps, et cetera are revised, compliance with the ADA must be achieved. Title II of the federal ADA regulations, part 35.151, says that new construction or alterations of public facilities shall, "to the maximum extent feasible, be altered in such manner that the altered portion of the facility is readily accessible to and usable by individuals with disabilities." This requires bringing the sidewalks, crosswalks, ramps, and pedestrian signals up to the latest ADA standards, which include the following:

- Sidewalk cross slopes shall not exceed two percent. The sidewalk cross slope may be less than two percent only if the sidewalk has a longitudinal slope sufficient to provide adequate drainage. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm compliance. New curb and sidewalk shall be constructed in compliance with these requirements.
- ADA-compliant curb ramps shall be installed where needed, consistent with standard drawings TE-12 or TE-13. Two new handicapped ramps at the northwest corner of the site would be preferred over one ramp, if adequate space is available. If the position of the existing ramp is revised, then crosswalk revisions may be required.
- Pedestrian push buttons immediately adjacent to the Keybank property at the intersection of 148th Avenue SE and Main Street must meet the latest ADA standards, which will require an upgrade of the pedestrian push button system. This system upgrade may require revising some of the pedestrian pushbutton system hardware on other corners of the intersection. Complete plans for upgrading the pedestrian pushbutton system must be prepared by a licensed engineer familiar with such systems and submitted for review and approval as part of the development's final engineering plans.

3. The sidewalks on both adjacent street frontages for the Keybank site shall be completely removed and reconstructed with a sidewalk at least eight feet wide, behind a planter strip at

least 4 four feet wide, not including the curb. The design and appearance of the sidewalks shall comply with the standards and drawings in the Transportation Department Design Manual, including standard drawing TE-11. Sidewalks on the Keybank site must align with sidewalks planned for construction on the adjacent property.

4. Old driveways that will not be reused must be closed. Concrete and subgrade must be removed from planter areas. Old curb cuts must be closed with new sidewalk per Transportation Department Design Manual standard drawing TE-11 and new curb and gutter per standard drawing TE-10.
5. The driveway on Main Street shall have an approach width, as defined in standard drawing DEV-6, of 20 feet. Signs and arrows shall be installed by the developer as needed to ensure that the driveway is restricted to one-way, outbound, right-turn-only usage. Specific details and locations for such signs and arrows will be determined in the review and approval of the final engineering plans for the development. The driveway approach to Main Street shall be consistent with standard drawing DEV-6, which may be modified with Transportation Department approval to reduce the severity of sidewalk grade changes and to encourage right turns.
6. During review of the engineering plans for the development, the Transportation Department may decide to require the developer to install a c-curb within Main Street in order to create a more effective prohibition against left turn movements at the driveway. If required, such c-curb will be installed per Transportation Department standard drawings TE-9A and 9B.
7. Landscaping strips on the street frontages shall be irrigated with a metered water source. Electrical connections for lighting in planter strips may be allowed, if installed in compliance with the electrical code and subjected to an electrical inspection. Irrigation devices and electrical components shall not create a tripping hazard in the sidewalk.
8. No new utility vaults that serve only one development will be allowed within a public sidewalk. Vaults serving a broader public purpose may be located within a public sidewalk.
9. No fixed objects, including fire hydrants, trees, signs, and streetlights, are allowed within ten feet of a driveway edge, defined as Point A in standard drawing Dev-6. Fixed objects are defined as anything with breakaway characteristics stronger than a 4 x 4-inch wooden post.
10. No new overhead utility lines will be allowed within or across any right of way or sidewalk easement, and existing overhead lines must be relocated underground.

Easements: The applicant shall provide sidewalk and utility easements to the City as needed to encompass the full required width of any public sidewalks located outside the city right of way fronting this site.

The developer must determine whether any utility easements within the site will be affected by this development. Any negative impact that this development has on those utility easements must be mitigated or easements relinquished.

Transformers and utility vaults to serve the site shall be placed within the site or below grade, to the extent feasible. Any such facilities that cannot be placed within the site shall not be placed

in the travel lane of a street or in the main walking path of a sidewalk, to the extent feasible. Holiday Construction & Traffic Restrictions: Work involving lane closures and other impacts to traffic will be restricted in certain areas of Bellevue from November 15th each year to January 5th of each year. If work activities are scheduled to fall within this window of time, the applicant will be required to obtain specific approval in advance for lane closures or other impacts to traffic. There will be no guarantees that daytime lane closures on 148th Avenue SE will be allowed during this time period. The applicant shall contact the Transportation Department Right of Way Section to confirm the specifics of this restriction prior to applying for a Right-of-Way Use Permit.

Use of the Right of Way during Construction: Applicants often request use of the right of way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right of Way Use Permit for such activities must be acquired prior to issuance of any construction permit including demolition permit. Sidewalks may not be closed except as specifically allowed by a Right of Way Use Permit.

Pavement Restoration: The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration Program, every street in the City of Bellevue has been examined and placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are, "No Street Cuts Permitted," "Overlay Required," and "Standard Trench Restoration." Each category has different trench restoration requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that often comes with small strip patching.

Near this project both Main Street and 148th Avenue SE have been classified as "Overlay Required," based on the existing condition of the pavement. Pavement restoration for any trenching or damage to the street surface will require a grind and overlay at least 50 feet long for the full width of any affected lane. The exact limits of the grind and overlay will be established through a right of way use permit.

See Section X of this report for Transportation Department related Conditions of Approval.

VI. STATE ENVIRONMENTAL POLICY ACT (SEPA)

Projects expected to involve more than 500 cubic yards of grading volume are required to receive environmental review under SEPA (Bellevue Development Code 22.02.032.3). This project proposes 1,155 cubic yards of earthwork. Therefore, environmental review was conducted.

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the proposal. Therefore, issuance of a Determination of Non-Significance (DNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements, with incorporation by reference of the *2009-2020 Transportation Facilities Plan Final Environmental Impact Statement* (TFP EIS) updated

November 2009. This document is available in the Records Room, Bellevue City Hall, 450 110th Avenue NE. Transportation-related impacts associated with this project are consistent with the potential projected impacts analyzed in the 2009-2020 TFP EIS. Adverse impacts which are less than significant are usually subject to City Codes or Standards intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code Sec. 22.02.140 provides substantive authority to mitigate impacts disclosed through the environmental review process.

EARTH

The project site will require minimal earthwork to shape the sub-grade for new improvements. In order to control erosion, a Temporary Erosion Control (TESC) Plan will be implemented in accordance with City standards. The TESC will provide Best Management Practices including temporary erosion control measures, inserts in all adjacent and on-site catch basins during construction, and temporary siltation and erosion controls maintained during construction.

AIR

Air emissions from construction are anticipated to be minimal. During construction, there will be equipment exhaust that will be emitted from vehicles using gasoline or diesel fuels. These emissions will be temporary and not have a lasting or harmful affect on the project or adjacent properties. There will also be airborne dust particles affecting air quality, principally during the grading and filling phase of the project. Following construction, it is anticipated that there will be a reduction in air quality impacts with the cessation of the existing fueling facility operations.

WATER

The project does not include work within or near any surface water body, nor will groundwater be withdrawn or discharged. As such, water impacts are not anticipated to result from this project. Water runoff will occur from the impervious paved surfaces, the building roof top and, to a lesser extent, from the landscape areas. On-site storm water will be collected in a series of catch basins that will be routed through a treatment chain consisting of settlement pipe and storm filter vault prior to release into the existing public storm sewer system located within the adjacent right of way.

PLANTS

The project will increase the amount of landscape area and provide a plant palette compatible with that approved for the adjacent Kelsey Creek Center under Design Review 10-121806-LD. See page 2 and attached Project Plans & Drawings for a landscape plan.

AESTHETICS

Surrounding properties are developed with a mix of commercial, office, church and hospitality uses. These uses consist primarily of single-story structures. The proposal is a single-story structure of compatible style and scale as the other uses within the surrounding area. See page 3 and attached Project Plans & Drawings for exterior elevations of the proposed bank building.

NOISE

Noise will be generated from the construction phase of the development. The City of Bellevue Noise Ordinance, BCC 9.18.040 regulates hours of construction-related noise and the conditions under which they may be expanded. Conditions of approval are included in Section X of this report regarding construction hours and noise abatement.

TRANSPORTATION

Long-Term Impacts and Mitigation

The long-term impacts of development projected to occur in the City by 2020 have been addressed in the City's Transportation Facilities Plan (TFP) EIS. The impacts of growth which are projected to occur by 2020 are evaluated on the roadway network assuming that all the transportation improvement projects proposed in the current TFP are in place. The TFP EIS divides the City into several Mobility Management Areas (MMAs) for analysis purposes. The Keybank project lies within MMA # 9, which has a 2020 total growth projection of 36,962 square feet of new retail space. The adjacent Kelsey Creek Center development proposes growth of 21,063 new square feet of retail space, leaving 15,899 square feet of unallocated retail growth available in MMA 9. The Keybank development proposes to construct 2,850 square feet of retail space. However, a gas station/convenience store with 1,933 square feet of retail space was decommissioned, resulting in a net increase of only 917 square feet of retail space. This net increase is significantly less than the available retail growth in the MMA. Therefore, the volume of proposed development is within the assumptions of the TFP EIS.

Transportation impact fees are used by the City to fund street improvement projects to alleviate traffic congestion caused by the cumulative impacts of development throughout the City. Payment of the transportation impact fee, as required by BCC 22.16, contributes to the financing of transportation improvement projects in the current adopted TFP, and is considered to be adequate mitigation of long-term traffic impacts.

Payment of the transportation impact fee is required at the time of building permit issuance. The gross impact fee for the bank with drive-up windows is estimated to be \$58,681.50, assuming a building of 2850 square feet. Per Transportation Department administrative policy, the developer may be eligible for impact fee credit for removing the gas station/convenience store, if that facility was in typical use within two years prior to application for a new building permit. The impact fee credit for removing a gas station / convenience store with eight fueling positions would be \$40,944. Thus, the net impact fee would be \$17,737.50. These fee rates are subject to change and the fee schedule in effect at the time of building permit issuance will apply. A significant fee increase has been adopted to take effect on January 1, 2013, with another increase on January 1, 2016.

Mid-Range Impacts and Mitigation

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more PM peak hour trips undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained.

The proposed bank development with drive-up windows will generate approximately 40 new (non pass-by) PM peak hour trips, based on the city's standard trip generation rate of 14.2 trips per 1000 square feet. However, the gas station/convenience market which formerly occupied the site generated approximately 48 new (non pass-by) PM peak hour trips, based on the rate of 6.02 per fueling position. Thus, the proposed development is not expected to create an increase in traffic, and no concurrency analysis is required at this time. If the Keybank development is significantly delayed, and the gas station/convenience market remains dormant for an extended period, then the use of credit for the preexisting gas station/convenience trips will be reevaluated.

Short-Term Operational Impacts and Mitigation

Bellevue Transportation Department staff analyzed the short-term operational impacts of preliminary versions of the proposed site plan in order to recommend transportation mitigation and frontage improvements. This analysis included traffic operations and safety on the adjacent streets and queuing at the proposed drive-up windows during the PM peak hour. Transportation Department staff concluded the following, all of which are now reflected on the attached Project Plans & Drawings:

1. In order to reduce operational impacts and improve safety at the intersection of 148th Avenue SE and Main Street, as well as on 148th Avenue SE south of that intersection, and to reduce conflicts between adjacent driveways on 148th Avenue SE, the site's access from 148th Avenue SE should be moved toward the south onto the adjacent property.
2. In order to reduce operational impacts and improve safety on Main Street and at the intersection of 148th Avenue SE and Main Street, the site's driveway on Main Street should be as far from the intersection as is feasible on this site. This driveway must be one-way, restricted to right turns out only. During engineering plan review for the development, the Transportation Department may decide to require the developer to install a c-curb within Main Street in order to create a more effective prohibition against other turn movements at the driveway.
3. A connection that allows traffic to go from the Keybank site into the Kelsey Creek Center parking lot without using the adjacent streets is recommended.

According to research on drive-through stacking presented at a 2009 meeting of the Institute of Transportation Engineers, the maximum queue length at the bank's drive-up windows is not expected to exceed four vehicles in each row, which could be accommodated on the bank site. In the unlikely event of longer queues, additional vehicles can be accommodated within a parking area in the Kelsey Creek Center, and would not spill back onto 148th Avenue SE.

See Section X of this report for Transportation Department Conditions of Approval related to items 1 – 3 above.

VII. CHANGES RESULTING FROM DESIGN REVIEW

Building Exterior

1. All sides of the building have entries or windows to allow physical and visual access to the activity within the building.

Site Design

1. Landscape frontage improvements along Main Street and 148th Avenue SE have been designed to be particularly accommodating for pedestrians, with an 8 foot wide sidewalk and a 4 foot wide planted strip intended to provide a buffer from the roadway.

2. Landscaping along Main Street and 148th Avenue SE has been designed to screen the parking lot from the street. A landscape buffer at least 10 feet wide planted with groundcover, shrubs and trees is proposed along the street side perimeter.

3. A gateway feature with special landscaping, paving, and seating is shown at the corner of Main Street and 148th Avenue SE.

4. Mechanical equipment that was originally proposed on the ground in the space between the building and street is now shown on the building rooftop and screened as required.

5. A two foot minimum width is now specified between the building and adjacent sidewalk where the plans originally were not clear whether enough space would be provided to accommodate plantings.

6. The trash and recycling containers are now shown hidden inside an enclosure designed to be architecturally compatible with the proposed building.

7. An originally-proposed driveway to 148th Avenue SE has been removed. Instead, an easement agreement with the adjacent Kelsey Creek Center allows access through the project site. Though not itself a design feature, this easement had a significant effect on the orientation of the building and site design.

VIII. DECISION CRITERIA

A. *The Director may approve, or approve with modifications, an application for Design Review if:*

1. The proposal is consistent with the Comprehensive Plan.

The Neighborhood Business Comprehensive Plan designation generally supports banks as a land use. The following Comprehensive Plan policies are applicable to this proposal:

Land Use Element

Policy LU-8: Ensure that commercial uses are contained within carefully delineated areas.

The proposal is to redevelop an existing commercial use within an established commercial neighborhood.

Policy LU-9: Maintain compatible use and design with the surrounding built environment when considering new development or redevelopment within an already developed area.

As an allowed use, banks are presumed to be compatible with other uses within the same land use district. The site improvements are in conformance with the applicable design standards and are consistent with the surrounding development.

Policy LU-13: Reduce the regional consumption of undeveloped land when appropriate.

The proposal is to re-use existing developed property; it will not use undeveloped land.

Urban Design Element

Policy UD-17: Consider the maintenance of existing and added vegetation in site design and development.

The project adds a considerable amount of additional landscaping as shown on the attached Project Plans & Drawings and summarized in the chart in sub-section III.A.2 of this report.

Policy UD-39: Include clear and ample walkways from street sidewalks and parking areas to building entrances and within and between developments as part of site design.

A new pedestrian connection is proposed from 148th Avenue SE to the bank building, along with access planned through the parking area at the adjacent Kelsey Creek Center.

Environmental Element

Policy EN-7: Promote growth management strategies that protect air, water, land, and energy resources consistent with Bellevue's role in the regional plan to contain an Urban Center.

This redevelopment project promotes growth management strategies by concentrating commercial development within an established commercial area. Neither undeveloped land nor consumption of more resources is impacted by this project.

Policy EN-15: Integrate site-specific development standards with urban watershed scale approaches to managing and protecting the functions of critical areas.

The project incorporates water quality measures in accordance with the Vasa Creek Basin within the Lake Sammamish watershed.

Policy EN-38: Restore and protect the biological health and diversity of the Lake Washington and Lake Sammamish watersheds within the City of Bellevue's jurisdiction.

Adherence to the prescriptive requirements of the Lake Sammamish water quality design standards promotes the restoration and protection of this watershed.

Policy EN-49: Preserve existing vegetation or provide or enhance vegetation that is compatible with the natural character of Bellevue.

Preservation of existing on-site landscaping and use of plantings from the City's plant palette promote this policy.

2. The proposal complies with the applicable requirements of this Code.

The proposal complies with all applicable requirements of the Land Use Code. Refer to Section III of this report for specific information on Land Use Code consistency.

3. The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent.

As conditioned, the proposal complies with all applicable Design Standards and Guidelines contained in LUC 20.25I (Community Retail Design District) as described in Section III.B of this staff report.

4. The proposal is compatible with, and responds to, the existing or intended character, appearance, and quality of development and physical characteristics of the subject property and immediate vicinity.

The project is located within an established commercial area as envisioned by the Neighborhood Business Comprehensive Plan designation. Established uses include gas stations, churches, hospitality, and offices with various design themes. The site design and architectural treatment of the proposed building are consistent with the appearance and quality of the established surrounding area. The project will increase the amount of landscape area and provide a plant palette compatible with that approved for the adjacent Kelsey Creek Center under Design Review 10-121806-LD. See attached Project Plans & Drawings for a landscape plan.

5. The proposal will be served by adequate public facilities including streets, fire protection, and utilities.

All required public services and facilities are available to the site.

IX. DECISION

After conducting the various administrative reviews associated with the proposal, including applicable Land Use consistency, SEPA and City Code & Standard compliance reviews, the Development Services Department Director does hereby APPROVE WITH CONDITIONS the subject proposal.

X. CONDITIONS OF APPROVAL:

The following conditions are imposed under the authority referenced:

A. GENERAL CONDITIONS: The following conditions apply to all phases of development.

1. COMPLIANCE WITH BELLEVUE CITY CODES AND ORDINANCES

The applicant shall comply with all applicable Bellevue City Codes and Ordinances including but not limited to:

Clearing and Grading Code - BCC 23.76
Bellevue Development Standards

Savina Uzunow
“

425/452-7860

Transportation Code - BCC 14.60	Carl Wilson	425/452-4394
Trans. Improvement Program - BCC.22.16	"	
Right-of-Way Use Permit - BCC 14.30	Ron Kessack	425/425-4599
Bellevue Utilities Code - BCC Title 24	Don Rust	425/452-4856
Construction Codes - BCC Title 23	Lee Kranz	425/452-5369
Land Use Code - BCC Title 20	Mike Upston	425/452-2970
Sign Code - BCC Title 22B	"	
Noise Control - BCC 9.18	"	
International Fire Code	Adrian Jones	425/452-6034

2. CONSTRUCTION HOURS

Noise related to construction is allowed from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. Exceptions to the construction noise hours limitation contained in the Noise Control Code may be granted pursuant to 9.18.020C.1 when necessary to accommodate construction which cannot be undertaken during exempt hours. Allowances for short term work outside of normal construction hours shall be limited and will be reviewed on a case by case basis to verify necessity and ensure appropriate noise mitigation is utilized to protect surrounding uses and properties. Written requests for exemption from the Noise Control Code must be submitted two weeks prior to the scheduled onset of extended hour construction activity.

Authority: BCC 9.18.040

Reviewer: Mike Upston

3. USE OF BEST AVAILABLE NOISE ABATEMENT TECHNOLOGY

The use of best available noise abatement technology consistent with feasibility is required during construction to mitigate construction noise impacts to surrounding uses.

Authority: BCC 9.18.020.F

Reviewer: Mike Upston

4. OUTDOOR DISPLAY

Outdoor display and storage is prohibited on this property.

Authority: LUC 20.25A.040

Reviewer: Mike Upston, Land Use

5. ON-STREET LOADING & UNLOADING

On-street loading and unloading will not be permitted.

Authority: LUC 20.20.590.K.4

Reviewer: Mike Upston, 425-452-2970

6. PRELIMINARY DESIGN APPROVAL

The Utilities Department approval of the Design Review application is based on the preliminary utility design only. Final civil engineering of the utility design may require changes to the site layout to accommodate the utilities.

Authority: BCC Title 24.02, 24.04, 24.06

Reviewer: Don Rust

7. UTILITY CODES & ENGINEERING STANDARDS

The water, sewer, and storm drainage systems shall be designed per the current City of Bellevue Utility Codes and Utility Engineering Standards. Utilities Department design review,

plan approval, and field inspection is performed under the Developer Extension Agreement and Utilities Permit Processes.

Authority: BCC Title 24.02, 24.04, 24.06

Reviewer: Don Rust

8. HOLIDAY CONSTRUCTION & TRAFFIC RESTRICTIONS

Work involving lane closures and other impacts to traffic will be restricted in certain areas of Bellevue from November 15th each year to January 5th of each year. If work activities are scheduled to fall within this window of time, the applicant will be required to obtain specific approval in advance for lane closures or other impacts to traffic. There will be no guarantees that daytime lane closures on 148th Avenue will be allowed during this time period. The applicant shall contact the Transportation Department Right-of-Way Section to confirm the specifics of this restriction prior to applying for a Right-of-Way Use Permit. The Transportation Department will be monitoring traffic and may modify this moratorium accordingly.

Authority: BCC 14.30.060

Reviewer: Ron Kessack, 425-452-4631

9. VEHICULAR ACCESS RESTRICTIONS

Access to this site from 148th Avenue and egress to 148th Avenue will be restricted to a shared driveway located on the adjacent property approximately 40 feet south of the Key Bank's southern property line. Egress to Main Street will be via a one-way, outbound, right-turn-only driveway. No direct access from Main Street will be allowed.

Authority: BCC 14.60.150

Reviewer: Carl Wilson, Transportation Department, 425-452-4228

B. PRIOR TO CLEARING & GRADING PERMIT: The following conditions are imposed to ensure compliance with the relevant decision criteria and Code requirements and to mitigate adverse environmental impacts not addressed through applicable Code provisions. These conditions must be complied with on plans submitted with the Clearing & Grading or Demolition permit application:

1. RIGHT-OF-WAY USE PERMIT

Prior to issuance of any construction permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

- a) Designated truck hauling routes.
- b) Truck loading/unloading activities.
- c) Location of construction fences.
- d) Hours of construction and hauling.
- e) Requirements for leasing of right of way or pedestrian easements.
- f) Provisions for street sweeping, excavation and construction.
- g) Location of construction signing and pedestrian detour routes.
- h) All other construction activities as they affect the public street system.

In addition, the applicant shall submit for review and approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevent access. General materials storage and contractor convenience are not reasons for preventing access. The applicant shall secure

sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.

Authority: BCC 11.70 & 14.30

Reviewer: Ron Kessack (425) 452-4631

2. CIVIL ENGINEERING PLANS – TRANSPORTATION

Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all street frontage improvements and driveway accesses must comply with the Americans with Disabilities Act, the Transportation Development Code, the Transportation Department Design Manual, and specific requirements stated elsewhere in this document. All relevant standard drawings from the Transportation Department Design Manual shall be copied exactly into the final engineering plans. Requirements for the engineering plans include, but are not limited to:

- a) Traffic signs and markings for turning restrictions at the northeast driveway and any other signage required to meet Transportation Department standards.
- b) Curb, gutter, sidewalk, and driveway approach design. Requirements include sidewalk width, driveway width, and driveway grade per the Transportation Department Design Manual. The engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as needed.
- c) Design of one-way driveway with a width of 20 feet connecting to Main Street.
- d) Upgrade of handicapped ramp(s) at the adjacent street corner.
- e) Installation or relocation of pedestrian push-buttons and related equipment to meet ADA.
- f) Relocation of streetlight or traffic signal equipment if necessary.
- g) Sight distance per BCC 14.60.240 and 241 for all driveways. Show the required sight triangles and include any sight obstructions, including those off-site. Consider all fixed objects and mature landscape vegetation.
- h) Location of fixed objects in the sidewalk or near the driveway approach.
- i) Trench restoration within any street right of way.
- j) Transformers and utility vaults to serve the building shall be placed within the site or below grade, to the extent feasible.

Authority: BCC 14.60, Transportation Department Design Manual, and Transportation Department Standard Drawings

Reviewer: Carl Wilson, Transportation Department, 425-452-4228

C. PRIOR TO BUILDING PERMIT: The following conditions are required by City Code. Unless specified otherwise below, these conditions must be complied with on plans submitted with the Building permit application:

1. BICYCLE RACKS

Bicycle racks shall be provided for at least 10 bicycles in a location(s) to be approved by Land Use under a Land Use Exemption combined with the building permit application.

Authority: LUC 20.20.25I.050.D.2

Reviewer: Mike Upston, Land Use

2. MECHANICAL EQUIPMENT

All mechanical equipment shall be clustered on the roof and fully screened behind the parapet, with the specific location and dimensions clearly depicted on the final building plans. Land Use

review will occur under a Land Use Exemption combined with the building permit application.

Authority: LUC 20.20.525.C.5 & 6

Reviewer: Mike Upston, Land Use

3. ALLIED WASTE ENDORSEMENT

The applicant shall provide a written endorsement by Allied Waste of the recycling and solid waste collection area's accessibility and function.

Authority: LUC 20.20.725

Reviewer: Mike Upston

4. TRANSPORTATION IMPACT FEE

Payment of the transportation impact fee is required at the time of building permit issuance. The gross impact fee for the bank with drive-up windows is estimated to be \$58,681.50, assuming a building of 2850 square feet. The developer may be eligible for impact fee credit for removing the previous use, as described above under Long-Term Impacts and Mitigation. If credit is granted, the impact fee would be reduced by up to \$40,944. These fee rates are subject to change and the fee schedule in effect at the time of building permit issuance will apply.

Authority: BCC 22.16

Reviewer: Carl Wilson, Transportation Department, 425-452-4228

5. BUILDING AND SITE PLANS – TRANSPORTATION

The building grade and elevations shall be consistent with the curb and sidewalk grade shown in the approved civil engineering plans. During construction, city inspectors may require additional survey work at any time in order to confirm proper elevations. Building, landscaping, and architectural site plans must accommodate on-site traffic markings, signs and driveway design as specified in the engineering plans. These plans must also comply with vehicle and pedestrian sight distance requirements, as shown on the engineering plans.

Authority: BCC 14.60.060, 110, 120, 150, 180, 181, 190, 240, 241

Reviewer: Carl Wilson, Transportation Department, 425-452-4228

6. EXISTING EASEMENTS

The developer must determine whether any utility easements within the site will be affected by this development. Any negative impact that this development has on those utility easements must be mitigated or easements relinquished.

Authority: BCC 14.60.100

Reviewer: Ron Kessack (425) 452-4631

7. DEDICATION OF NEW PEDESTRIAN / UTILITY EASEMENTS

The applicant shall provide sidewalk and utility easements to the City such that public sidewalks outside of the City right of way are located within a pedestrian/utility easement area.

Authority: BCC 14.60.100

Reviewer: Carl Wilson, Transportation Department, 425-452-4228

8. PRIVATE ACCESS EASEMENT AGREEMENT

Access to the proposed project will be provided via a driveway off 148th Avenue to be located on the adjacent property (Kelsey Creek Center) approximately 40 feet south of the Key Bank's southern property line. Use of and responsibility for said driveway will be shared with the adjacent property under a private Easement Agreement between the property owners. This

agreement must be executed and recorded against the affected properties at the King County Recorder's Office prior to approval of a building permit for the Key Bank development.

Authority: BCC 14.60.110

Reviewer: Carl Wilson, Transportation Department, 425-452-4228

D. PRIOR TO TCO: The following conditions are required by City Code and supported by City Policy. These conditions shall be complied with prior to issuance of the Temporary Certificate of Occupancy (TCO):

1. LANDSCAPE INSTALLATION ASSURANCE DEVICE

All site landscaping shall be 100% complete per the plan approved by the City. Alternatively, the applicant shall submit the following: 1) a red-marked plan identifying which landscape areas are incomplete; 2) an estimate for the total cost to complete these areas; and 3) a notarized Assignment of Savings dedicated to the City for 150% of the estimated cost to complete these areas per the approved Landscape Plan.

Authority: LUC 20.40.490

Reviewer: Mike Upston

2. LANDSCAPE MAINTENANCE ASSURANCE DEVICE

The applicant shall file with the Development Services Department a landscape maintenance assurance device for a one-year period; provide an assignment of savings or letter of credit for 20% of the cost of labor and materials for all required landscaping.

Authority: LUC 20.40.490

Reviewer: Mike Upston

3. STREET FRONTAGE IMPROVEMENTS

All street frontage improvements and other required transportation elements, including pedestrian signal revisions, must be constructed by the applicant and accepted by the Transportation Department Inspector. Existing street light and traffic signal apparatus affected by this development, including traffic controllers, pedestrian signal poles, traffic signal poles, and power sources, must be relocated if necessary. All required improvements must be constructed per the approved plans or as directed by the Transportation Department inspector. Bonding or other types of assurance devices will not be accepted in lieu of construction, unless the city requires a delay.

Authority: BCC 14.60, Transportation Department Design Manual, and Transportation Department Design Manual Standard Drawings.

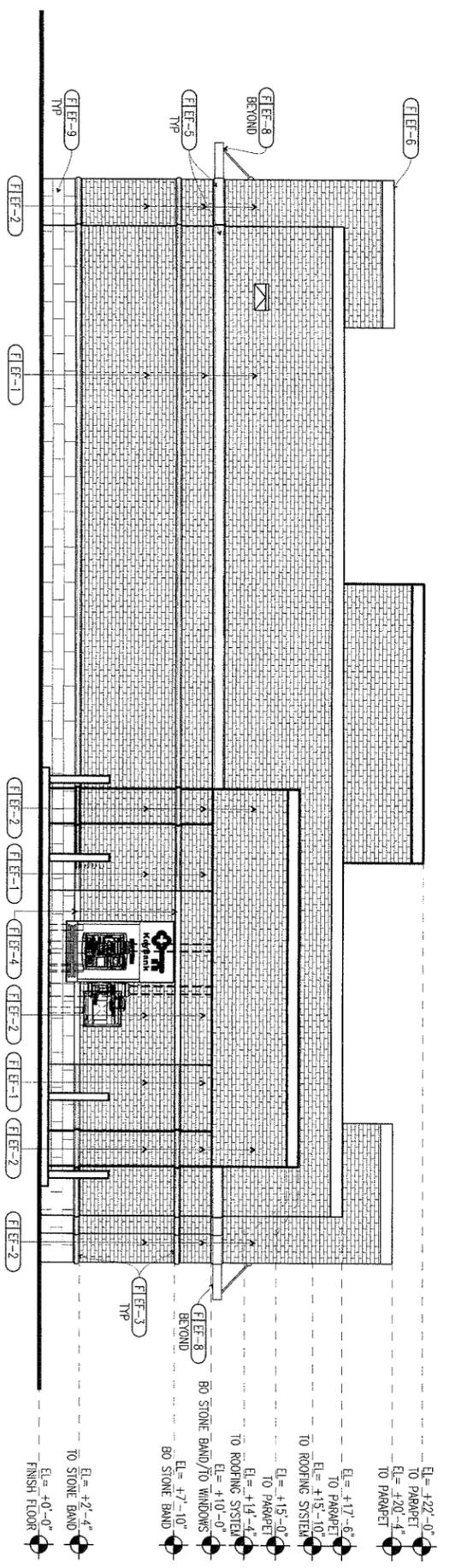
Reviewer: Carl Wilson, Transportation Department, 425-452-4228

4. PAVEMENT RESTORATION

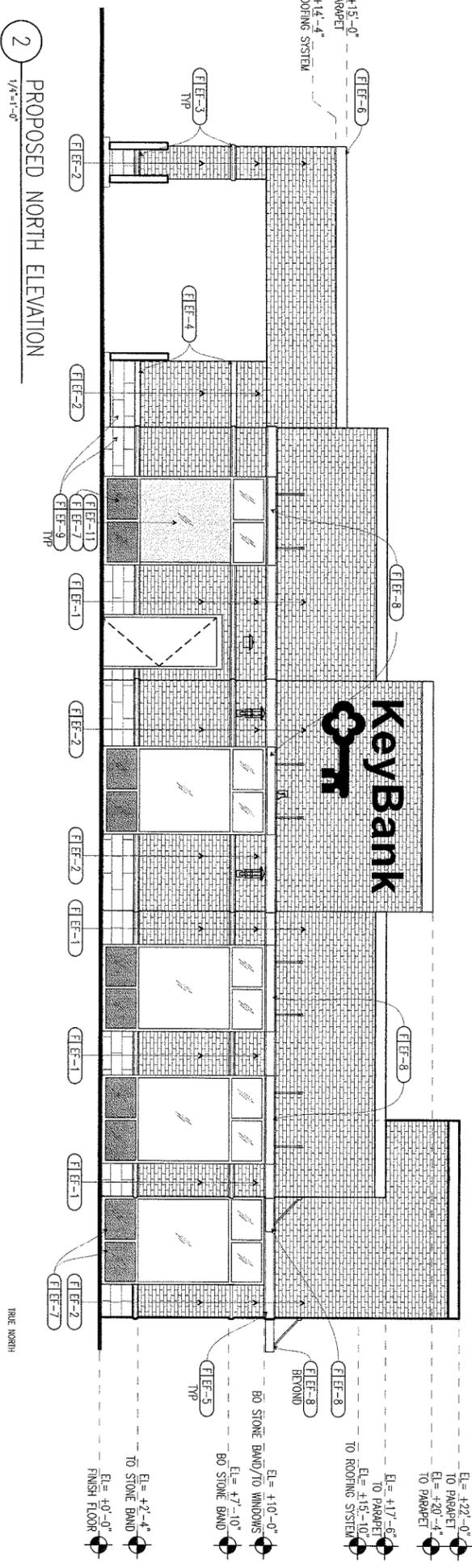
Both Main Street and 148th Avenue adjacent to the Key Bank site are presently classified as "Overlay Required," based on the existing condition of the pavement. Pavement restoration for any trenching or damage to the street surface will require a grind and overlay at least 50 feet long for the full width of any affected lane. The exact limits of the grind and overlay will be established through a right of way use permit.

Authority: BCC 14.60. 250; Design Manual Design Standard #21

Reviewer: Ron Kessack (425) 452-4631



1
1/4"=1'-0"
PROPOSED EAST ELEVATION



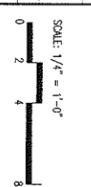
2
1/4"=1'-0"
PROPOSED NORTH ELEVATION

EXTERIOR FINISH SCHEDULE

CODE	MATERIAL	MANUFACTURER	STYLE	COLOR	MODULE	REMARKS
EF-1	FACE BRICK	BEDELI BRICK COMPANY	MODULAR	SI. SINGLE	3 5/8" X 2 1/4" X 7 5/8"	
EF-2	FACE BRICK	BEDELI BRICK COMPANY	MODULAR	QUAKER VENDOR	3 5/8" X 3 1/4" X 7 5/8"	
EF-3	ACCENT STONE BAND	ROCKCAST	WIRYS WATERFALL	CAST STONE COLOR	4 5/8" X 3 5/8" X 47 5/8"	
EF-4	ACCENT STONE BAND	ROCKCAST	STONE	CAST STONE BAND COLOR "BURFSTONE" (GP-A)	3 5/8" X 3 5/8" X 23"	AT RAIL & VESTIBULE ONLY
EF-5	ACCENT STONE BAND	ROCKCAST	WIRID WATERFALL	CAST STONE COLOR "BURFSTONE" (GP-A)	4 5/8" X 7 5/8" X 47 5/8"	
EF-6	METAL CORNICE			TO MATCH KEYBANK RED		
EF-7	COMPOSITE METAL PANEL	UMAHATIONS, INC.	THERMOCLITE	TO MATCH STONEWORK SYSTEM		
EF-8	METAL AWNING			KEYBANK RED		FINISHED AND INSTALLED BY OWNER'S VENDOR UNDER SEPARATE PERMIT
EF-9	NATURAL TWIN ROCK	ROBINSON BRICK	ASPBANK	CAST STONE CAP PIECE "BURFSTONE"	4" X 14" X 47 5/8"	TRASH ENCLOSURE AND MECHANICAL SCREEN WALLS ONLY, END RETURN AT WALL ENDS
EF-10	STONE DWP	ROCKCAST	OD100			AT ETD WINDOW ONLY
EF-11	SPHARICAL GLASS					

SHEET SYMBOLS

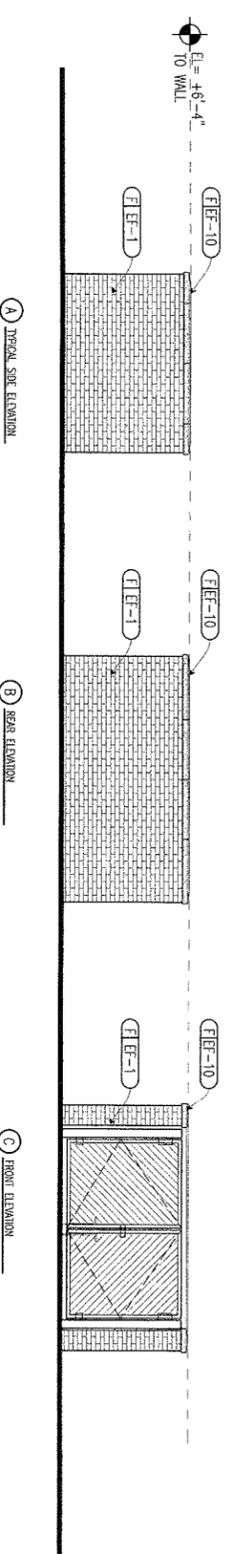
SEE EXTERIOR FINISH SCHEDULE
FINISH INDICATOR THIS SHEET



KeyBank
#6367-Kelsey Creek
106 148th St. SE
Bellevue, WA 98007

ISSUED / REVISED	DATE
DESIGN REVIEW	08/28/10
DESIGN REVIEW	12/03/10
RESUBMITTAL	

Proposed Elevations
A-202



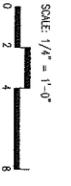
1
1/4"=1'-0"
PROPOSED TRASH ENCLOSURE ELEVATIONS

EXTERIOR FINISH SCHEDULE

CODE	MATERIAL	MPR	STYLE	COLOR	MODULE	REMARKS
EF-1	FACE BRICK	BEDFORD BRICK COMPANY	MODULAR	ST. SIMON	3 5/8" X 2 1/4" X 7 5/8"	
EF-2	FACE BRICK	BEDFORD BRICK COMPANY	MODULAR	QUAKER VECOLOR	3 5/8" X 2 1/4" X 7 5/8"	
EF-3	4" ACCENT STONE BAND	ROCKCAST	WIBBS WATERFALL	CAST STONE COLOR "BUFFSTONE" (GP-A)	4 5/8" X 3 5/8" X 47 5/8"	
EF-4	4" ACCENT STONE BAND	ROCKCAST	STONE	CAST STONE BAND COLOR "BUFFSTONE" (GP-A)	3 5/8" X 3 5/8" X 23"	AT RAIL & VESTIBULE ONLY
EF-5	8" ACCENT STONE BAND	ROCKCAST	WIBBS WATERFALL	CAST STONE COLOR "BUFFSTONE" (GP-A)	4 5/8" X 7 5/8" X 47 5/8"	
EF-6	METAL CORNICE			TO MATCH KETRAK RED		
EF-7	COMPOSITE METAL PANEL	LANAMATORS, INC.	THERMOCLITE	TO MATCH STONEROMANT SYSTEM		
EF-8	METAL AWNING			KETRAK RED		FURNISHED AND INSTALLED BY OWNER'S VENDOR. UNDER SEPARATE PERMIT
EF-9	NATURAL THIN ROCK	ROCKCAST	ASPBARK			
EF-10	STONE CAP	ROCKCAST	CO100	CAST STONE CAP PIECE "BUFFSTONE"	4" X 14" X 47 5/8"	TRASH ENCLOSURE AND MECHANICAL SCREEN WALLS ONLY. END RETURN AT WALL ENDS
EF-11	SPANDREL GLASS					AT TID WINDOW ONLY

SHEET SYMBOLS

SEE EXTERIOR FINISH SCHEDULE
FINISH INDICATOR THIS SHEET



KeyBank
#6367-Kelsey Creek
106 148th St. SE
Bellevue, WA 98007

ISSUED / REVISED	DATE
DESIGN REVIEW	08/28/10
PERMITS REVIEW	12/09/10
REVISIONS	

207251.82

Proposed Elevations
A-203

SECTION 35, TOWNSHIP 25N, RANGE 5E, W1M
LANDSCAPE PLANTING PLAN

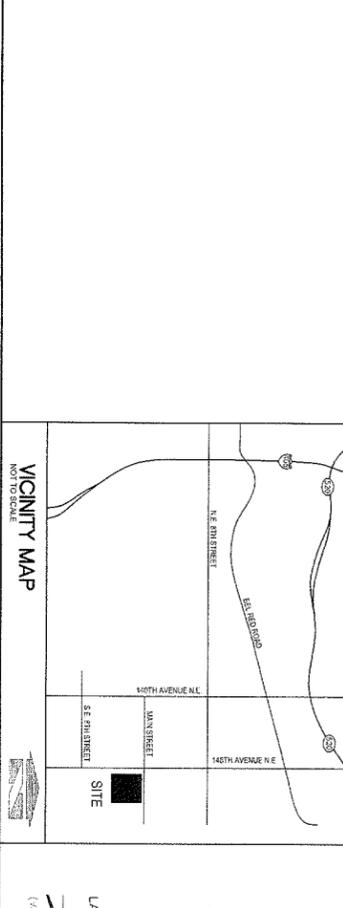
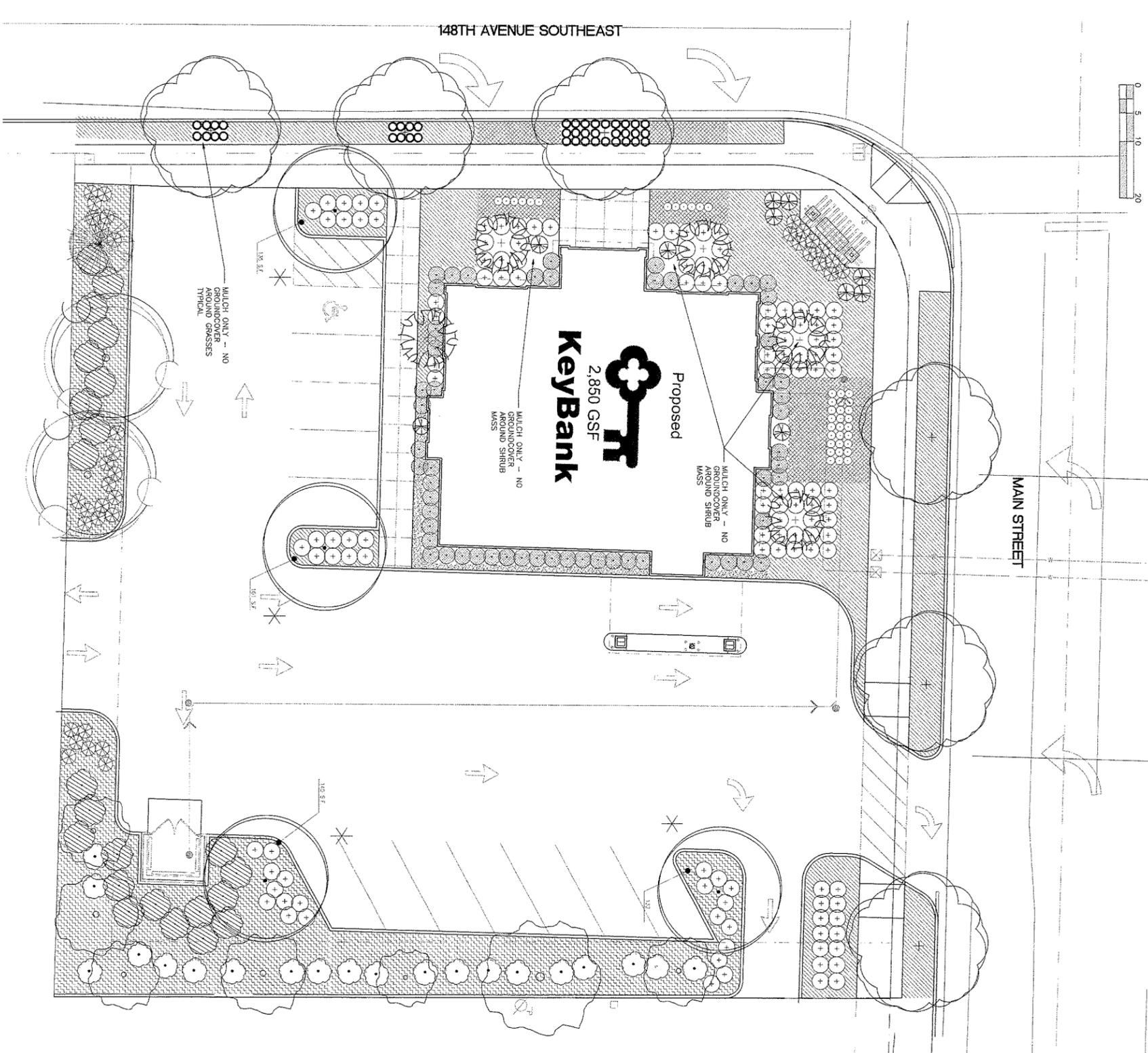
LANDSCAPE PLANT MATERIAL LEGEND

SYMBOL	BOTANICAL / COMMON NAMES	SIZE	CONDITION	SPACING	QUANTITY	REMARKS
	PRUNUS MONTICOLA / WESTERN WHITE PINE	6' - 7' HI.	AS SHOWN		1	STAKE & QTY ONE GROWING SEASON; NURSERY GROWN, UN-CUT LEADER
	THUJA OCCIDENTALIS 'CORNINGHAM' / NO COMMON NAME	1 1/2" CAL.	AS SHOWN		2	STAKE & QTY ONE GROWING SEASON; NURSERY GROWN FOR STREET TREE USE, BRANCHED AT 6'
	STEWARTIA PSEUDOCAMELIA / JAPANESE STEWARTIA	1 1/2" CAL.	AS SHOWN		5	STAKE & QTY ONE GROWING SEASON; NURSERY GROWN, LOW BRANCHED
	ULMUS FRONTALIS / FRONTIER ELM (PARKING LOT TREE)	1 1/2" CAL.	AS SHOWN		4	STAKE & QTY ONE GROWING SEASON; NURSERY GROWN FOR STREET TREE USE, BRANCHED AT 6'
	ZELKOVA 'NUSUSHINO' / ZELKOVA (STREET TREE)	3" CAL.	AS SHOWN		6	STAKE & QTY ONE GROWING SEASON; NURSERY GROWN FOR STREET TREE USE, BRANCHED AT 7'
	INDICATES REQUIRED PARKING LOT TREE					
	EXISTING DECIDUOUS TREE					
	SHRUBS:					
	CALLIANDRA ACUMINATA 'KARI FORESTER' / KARI FORESTER RED CACTI	1 GALLON	4'		37	
	NANDINA DOMESTICA / HONOLULU SANDO	18" - 21"	3' O.C.		57	MATCHED HEIGHT AND FORM, 3 CANES
	PHORADENDRON FLAVESCENS 'BAY' / BAY RED ROBIN FRAGERS PHOTINIA	5 GALLON	AS SHOWN		10	
	PHOTINIA FRASERI 'RED ROBIN' / RED ROBIN FRAGERS PHOTINIA	21" - 24"	5' O.C.		23	
	RUDBECKIA FULGIDA / BLACK-EYE-SUSAN	1 GALLON	1.5' O.C.		39	
	TAXUS PACIFICA 'SEMPERENS' / SPREADING ENGLISH YEW	18" - 21"	3' O.C.		129	MATCHED HEIGHT AND FORM
	EXISTING SHRUB					TO REMAIN, SAVE AND PROTECT
	GROUNDCOVERS:					
	ARCTOSTAPHYLOS UVA-URSI / KANAWANUCK	4" POT	12" O.C.		AS RECD	HOLD 16" FROM BORDERS, SHRUBS, AND TREES
	EPHYPHYLLUM GRANDIFOLIUM / BISHOP'S HAT	4" POT	12" O.C.		AS RECD	HOLD 16" FROM BORDERS, SHRUBS, AND TREES
	FRAGARIA CHLORISSE / BEACH STRAWBERRY	4" POT	12" O.C.		AS RECD	HOLD 16" FROM BORDERS, SHRUBS, AND TREES
	HELICTOTRICHON SEMPERVIRENS / BLUE OAT GRASS	1 GALLON	2' O.C.		42	
	LOROPECHIUM / BIG BLUE LILY TURT	4" POT	12" O.C.		AS RECD	HOLD 16" FROM BORDERS, SHRUBS, AND TREES
	THYMUS PSEUDOLANGUNOSUS / WOOLLY THYME	4" POT	12" O.C.		AS RECD	HOLD 16" FROM BORDERS, SHRUBS, AND TREES

PARKING LOT TREE-V CALCULATIONS:
 17.5 SQUARE FEET OF LANDSCAPE AREA PER PARKING STALL
 NO. STALLS SQUARE FEET REQUIRED SQUARE FEET PROVIDED

12 210 S.F. 508 S.F.

INDICATES PARKING LOT LANDSCAPE AREA



No.	Date	By	Clk.	Appr.	Revision
1	12/03/10	AMS	DS	JE	SITE PLAN CHANGES

Job Number: **14458**

Sheet: **L1 of 4**

Scale: Horizontal 1"=10', Vertical N/A

For: **KEYBANK
106 148th AVE SE
BELLEVUE, WA**

Title: **LANDSCAPE PLANTING PLAN**

Designed: AMS, Drawn: AMS, Checked: DAS, Approved: JE, Date: 08/16/10

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CML ENGINEERING, LAND PLANNING,
 SURVEYING, ENVIRONMENTAL SERVICES

BARGHAUSEN
 CONSULTING ENGINEERS, INC.

STATE OF WASHINGTON REGISTERED LANDSCAPE ARCHITECT
 JEFFREY A. STREIB
 LICENSE NO. 786
 EXPIRES 10/30/12

LANDSCAPE PLANTING NOTES AND MATERIALS

SECTION 35, TOWNSHIP 25N, RANGE 5E, WM
LANDSCAPE PLANTING NOTES AND DETAILS

SCOPE OF WORK:
FURNISH ALL MATERIALS, LABOR, EQUIPMENT AND RELATED ITEMS NECESSARY TO ACCOMPLISH TOPSOIL, TREATMENT AND PREPARATION OF SOIL, FINISH GRADING, PLACEMENT OF SPECIFIED PLANT MATERIALS, FERTILIZER, STAKING, MULCH, CLEAN-UP, DEBRIS REMOVAL, AND 90-DAY MAINTENANCE.

QUALIFICATIONS:
LANDSCAPE CONTRACTOR TO BE SKILLED AND KNOWLEDGEABLE IN THE FIELD OF WORK AND HAVE A MINIMUM FOR FIVE (5) YEARS EXPERIENCE INSTALLING SIMILAR WORK. CONTRACTOR TO BE LICENSED TO PERFORM THE WORK SPECIFIED WITHIN THE PRECISING JURISDICTION.

JOB CONDITIONS:
IT IS THE CONTRACTOR'S RESPONSIBILITY TO REVIEW THE SITE AND REPORT ANY DISCREPANCIES TO THE OWNER OR THE OWNER'S REPRESENTATIVES. ALL PLANT MATERIAL AND FINISH GRADES ARE SUBJECT TO APPROVAL BY THE OWNER.

PROTECTION:
SAVE AND PROTECT ALL EXISTING PLANTINGS SHOWN TO REMAIN. DO NOT PLANT UNTIL OTHER PLANTINGS ARE PROTECTED. ALL EXISTING PLANTINGS TO BE PROTECTED BY A PROTECTIVE SYSTEM AS TO BE INSTALLED ON THE PLANT UNITS. ALL PLANTINGS TO BE PROTECTED BY A PROTECTIVE SYSTEM AS TO BE INSTALLED ON THE PLANT UNITS. ALL PLANTINGS TO BE PROTECTED BY A PROTECTIVE SYSTEM AS TO BE INSTALLED ON THE PLANT UNITS.

REPAIR OF EXISTING PLANTINGS:
DURING THE COURSE OF WORK, REPAIR ALL EXISTING PLANTING AREAS BY PRUNING DEAD GROWTH, RE-ESTABLISHING FINISH GRADE AND RE-INCLINING TO SPECIFIED DEPTH.

REPAIR OF IRRIGATION SYSTEM:
DURING THE COURSE OF WORK, REPAIR ANY DAMAGE TO THE IRRIGATION SYSTEM TO MATCH CONDITIONS PRIOR TO THE DAMAGE.

GUARANTEE:
GUARANTEE ALL PLANT MATERIAL FOR A PERIOD OF ONE YEAR FROM DATE OF FINAL ACCEPTANCE OF THE JOB BY OWNER.

90-DAY MAINTENANCE:
CONTRACTOR TO PROVIDE OWNER WITH A SCOPE OF WORK AT THE END OF INITIAL PROJECT BID TO PROVIDE LAOSCAPE AND IRRIGATION MAINTENANCE FOR 90 DAYS FOLLOWING COMPLETION OF PROJECT (ACCEPTANCE) OF FACILITY BY OWNER. WORK TO INCLUDE MAINTENANCE AS DESCRIBED BELOW, IN PLANTING AND IRRIGATION MAINTENANCE.

PLANT MATERIALS:
PLANT MATERIALS TO BE GRADE NO. 1, SIZED IN ACCORDANCE WITH (CAN) AMERICAN STANDARDS FOR NURSERY STOCK (ANSI Z60.1-1998). PRUNE PLANTS RECEIVED FROM THE NURSERY ONLY UPON AUTHORIZATION BY THE LANDSCAPE ARCHITECT. 'B' & 'C' INDICATES BALLED AND BURLAPPED, 'CONT.' INDICATES CONTAINER, 'PRT' INDICATES BARE ROOT, 'CAL.' INDICATES CALIPER AT 6" ABOVE SOIL LINE, 'CAL.' INDICATES BARE ROOT, 'CAL.' INDICATES CALIPER AT 6" ABOVE SOIL LINE, 'CAL.' INDICATES BARE ROOT, 'CAL.' INDICATES CALIPER AT 6" ABOVE SOIL LINE.

A) SPECIFIED PLANT CANOPY SIZE OR CALIPER IS THE MINIMUM ACCEPTABLE PLANT QUALITY.
SPECIFIED PLANT CANOPY SIZE OR CALIPER IS THE MINIMUM ACCEPTABLE PLANT QUALITY. SPECIFIED PLANT CANOPY SIZE OR CALIPER IS THE MINIMUM ACCEPTABLE PLANT QUALITY.

B) PLANT MATERIAL TO COMPLY WITH STATE AND FEDERAL LAWS FOR DISEASE RESISTANCE.
PLANT MATERIAL TO COMPLY WITH STATE AND FEDERAL LAWS FOR DISEASE RESISTANCE. PLANT MATERIAL TO COMPLY WITH STATE AND FEDERAL LAWS FOR DISEASE RESISTANCE.

C) SUBSTITUTION OF PLANT MATERIAL, SPECIES OR VARIETY WILL BE PERMITTED UNLESS WRITTEN EVIDENCE IS SUBMITTED TO THE OWNER FROM TWO QUALIFIED PLANT SPECIALISTS.
SUBSTITUTION OF PLANT MATERIAL, SPECIES OR VARIETY WILL BE PERMITTED UNLESS WRITTEN EVIDENCE IS SUBMITTED TO THE OWNER FROM TWO QUALIFIED PLANT SPECIALISTS. SUBSTITUTION OF PLANT MATERIAL, SPECIES OR VARIETY WILL BE PERMITTED UNLESS WRITTEN EVIDENCE IS SUBMITTED TO THE OWNER FROM TWO QUALIFIED PLANT SPECIALISTS.

SOIL PREPARATION:
TOPSOIL, AMENDMENT, AND BACKFILL, ARE GENERAL REQUIREMENTS FOR ALL LANDSCAPE AREAS. UNLESS NOTED OTHERWISE ON THE PLANS, SOIL AMENDMENTS AND FERTILIZER NOTED BELOW ARE TO BE USED FOR 80 PERCENT BASIS ONLY. SPECIFIC AMENDMENTS AND FERTILIZERS TO BE USED FOR THE REMAINING 20 PERCENT SHALL BE DETERMINED BY THE CONTRACTOR. PROVIDE CHANGE ORDER FOR ADDITIONAL OR REDUCTION OF MATERIALS REQUIRED OR NOT REQUIRED BY THE SOILS REPORT.

A) TOPSOIL:
CONTRACTOR IS RESPONSIBLE FOR SUPPLYING ALL TOPSOIL AND FOR DETERMINING THE VOLUME OF TOPSOIL REQUIRED. REFER THE INFORMATION ON PLANS AND NOTED RESULTING FROM CONTAMINATED OFF SITE SOURCES.

B) TOPSOIL TO CONSIST OF WINTER MIX AS PRODUCED AND REMAINED BY PACIFIC TOPSOILS, INC.
WINTER MIX TO CONSIST OF 1/3 BY VOLUME SANDY LOAM, 1/3 BY VOLUME COMPOSTED GARDEN MULCH, AND 1/3 BY VOLUME COARSE WASHED SAND OR AMENDMENTS AS LISTED IN 'C'.

C) NATIVE SURFACE SOIL, INCLUDING STOCKPILED SURFACE SOIL, ON-SITE, MAY MEET TOPSOIL PRODUCT STANDARDS AS NOTED ABOVE, PROVIDED THAT NECESSARY AMENDMENTS NOTED BELOW, ARE ADDED AND EVENLY BLENDED TO SPECIFIED DEPTH.
NATIVE SURFACE SOIL/STOCKPILED SURFACE SOIL (AND TOPSOIL BROUGHT ON SITE), THE FOLLOWING AMOUNT PER 1000 SQUARE FEET:

1. 6-CUBIC YARDS ORGANIC COMPOST, COMPOST TO BE FREE OF NON-FARM PRODUCTS.
2. 30-POUNDS NITROGEN (38-0-0)
3. 5-POUNDS AMMONIUM SULFATE
4. 40-POUNDS CALCIUM CARBONATE LIMESTONE
5. 5-QUINCE BORON (AS BORON)
6. 3-POUNDS MOLYBDENUM SULFATE
7. 3-POUNDS ZINC SULFATE
8. 2-POUNDS MANGANESE SULFATE
9. 2-POUNDS COPPER SULFATE
10. 2-POUNDS POTASSIUM SULFATE

E) PLANTING BACKFILL FOR ALL TREES, SHRUBS, AND GROUNDCOVERS:
ALL AMENDMENTS TO BE THOROUGHLY MIXED PRIOR TO INCORPORATION INTO TOPSOIL.

F) TOPSOIL PREPARATION AND INSTALLATION:
1. VERIFY SUBGRADES TO -7 INCHES BELOW FINISH ELEVATION IN ALL LANDSCAPE AREAS.
2. EMULCATE ANY SURFACE VEGETATION ROOTED IN THE SUB-GRADE PRIOR TO SUB-GRADE.
3. PREPARATION, SCARIFY AND PREP ALL LANDSCAPE SUB-GRADES WHICH HAVE BECOME COMPACTED TO A DEPTH OF 12 INCHES WITH MULTIPLE PASSES, 90 DEGREES TO EACH OTHER. SCARIFY AREAS INACCESSIBLE TO MECHANIZED EQUIPMENT OR AROUND EXISTING PLANTINGS NOTED TO REMAIN WITH HAND TOOLS.
4. REMOVE SOIL LUMPS, ROCK, VEGETATION AND/OR DEBRIS LARGER THAN 2 INCHES.
5. REMOVE ALL ASPHALT EXTENDING BEYOND 6 INCHES FROM CURBS INTO ADJACENT LANDSCAPE AREAS.
6. PARKING LOT PLANTER ISLANDS TO BE OVER EXCAVATED BY BACKHOE. REMOVE PAVING WASTE, GRAVEL BASE MATERIAL AND INTERLOCKING SUBSOL TO 18 INCHES BELOW FINISH GRADE. REMOVE ALL EXISTING CURB AND LEAVE IN PLACE PLAN 711 SECTION 12 INCHES TO MINIMIZE STRUCTURAL COMPACTION.

G) TOPSOIL PLACEMENT:
TOPSOIL AND AMENDMENTS OVER THE PREPARED SUB-GRADE AND THOROUGHLY ROTILL WITH MULTIPLE PASSES AND THE PREPARED SUB-GRADE FOR A TOTAL DEPTH OF 8 INCHES. PLACE AN ADDITIONAL 2 INCHES LEFT OR TOPSOIL OVER THE AMENDED SOIL AS A SURFACE COURSE. PLACE ADDITIONAL TOPSOIL AS NOTED TO MEET FINISH ELEVATIONS IN SEED AREAS OR IN AREAS INDICATED TO BE MOISTENED.

BARK MULCH (COMPRESSION):
ONE-HALF-INCH (1/2") SIZE, TO ONE-QUARTER (1/4") I.E., "FINE." HEAVYLOCK/FR BARK. 2-INCH DIAMETER BY 8-FOOT MINIMUM LOGPOLE PINE STAKES.

ANTI-DESICCANT:
"WILT-PROOF" 48 HOURS PRIOR TO SHIPMENT TO SITE FROM JUNE 1 THROUGH SEPTEMBER THROUGHOUT ROOT WATER PLANTS PRIOR TO DELIVERY. PLANT MATERIAL DELIVERED TO SITE TO BE KEPT CONTINUALLY MOIST THROUGH INSTALLATION.

FINISH GRADES:
FINISH GRADE AND REMOVE ROCKS AND FOREIGN OBJECTS OVER 2 INCHES DIAMETER FROM TOP BELOW CURBS, WALKS AND/OR OTHER ADJACENT AREAS. FINISH GRADE FOR 2 INCHES BELOW FINISH GRADE REFER TO GRADES PRIOR TO INSTALLATION OF MULCH. ALL FINISH GRADES TO BE SMOOTH EVEN GRADES, LIGHTLY COMPACTED, AS SHOWN ON THE PLAN AND DETAIL. PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDINGS AND STRUCTURES. SEE CIVIL DRAWINGS IDENTIFY FINAL ELEVATIONS.

TREES:
ARRANGE TREES ON SITE IN PROPOSED LOCATIONS PER DRAWINGS. EXCAVATE PIT, PLANT AND BACKFILL WITH TOPSOIL AND AMENDMENTS AS NOTED. SETTLE BACKFILL USING WATER ONLY. NO MECHANICAL COMPACTION.

SHRUBS:
INSTALL SHRUBS AS SPECIFIED FOR TREES.

GROUNDCOVERS:
EXCAVATE PITS TO A MINIMUM OF 3 INCHES BELOW AND TWICE THE ROOT BALL DIAMETER. WATER PROPER GRADE, AS DETAIL.

MULCH:
MULCH ALL LANDSCAPE AREAS APPLY SUFFICIENT QUANTITY TO PROVIDE A 2-INCH DEPTH. UTILITY CLEARANCES:
FIELD ADJUST PLANT LOCATIONS FOR 8-FOOT SEPARATION OF TREES/SHRUBS AND 2-FOOT SEPARATION FOR GROUNDCOVER FROM FIRE HYDRANTS AND UTILITY VALVES.

PLANTING MAINTENANCE:
CONTRACTOR TO MAINTAIN PLANTINGS THROUGH COMPLETED INSTALLATION, AND UNTIL ACCEPTANCE OF LANDSCAPE INSTALLATION. PLANTING MAINTENANCE TO INCLUDE WEEDING, WEDDING, CLEANING, FERTILIZING AND REPAIRING OF TREE GOTS, RESISTING PLANTS TO PROPER GRADES ONE YEAR FOLLOWING LANDSCAPE INSTALLATION. INCLUDED IS REPLACEMENT OF DEAD PLANTS AND PLANTS SHOWING LOSS OF 40 PERCENT OR MORE OF CANOPY.

IRRIGATION MAINTENANCE:
THE IRRIGATION SYSTEM TO BE MAINTAINED INCLUDING ADJUSTMENTS FOR BALANCED WATER DISTRIBUTION AND PRECIPITATION. FAULTED OR MALFUNCTIONING IRRIGATION EQUIPMENT SHALL BE REPLACED AND/OR CORRECTED. PLANTING AND IRRIGATION MAINTENANCE TO INCLUDE THOSE REPAIRS NECESSARY TO THE PROPER GROWTH AND SURVIVAL OF ALL PLANT MATERIALS. CONTRACTOR TO PROVIDE THIS WORK IN ADDITION TO SPECIFIC WORKITEMS/QUANTITIES.

PRUNE DAMAGED TWIGS AFTER PLANTING:
PLACE IN VERT. POSITION. DOUBLE LEADERS WILL BE REFLECTED.
NOTE:
KEEP ROOTBALL MOIST AND PROTECTED AT ALL TIMES.
HOLD CROWN OF ROOTBALL AT OR JUST ABOVE FINISH PROTECT TRUNK AND LIMBS FROM RAUPLY.
BACKFILL TO BE SETTLED USING WATER ONLY - NO MECHANICAL COMPACTION.

REMOVE ALL WRAP, TIES & CONTAINERS, REBARLESS (2) LOGPOLE STAKES, PLUMB WITH ELASTIC CHAIN, LOOK TYPE OR RUBBER GUSSETTED IN FOURTH EIGHT. REMOVE AFTER ONE GROWING SEASON.

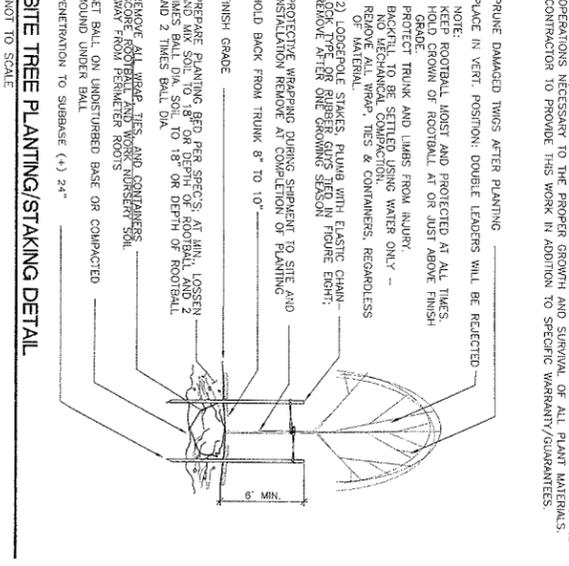
PROTECTIVE WEEDING BARRIERS SUBJECT TO SITE AND INSTALLATION REMOVE AT COMPLETION OF PLANTING.
HOLD BACK FROM TRUNK 8" TO 10"

FINISH GRADE:
PREPARE PLANTING BED PER SPECS. AT MIN. LOSSEN AND MIX SOIL TO 18" OR DEPTH OF ROOTBALL AND 2 TIMES BALL DIA. SOIL TO 18" OR DEPTH OF ROOTBALL AND 2 TIMES BALL DIA.

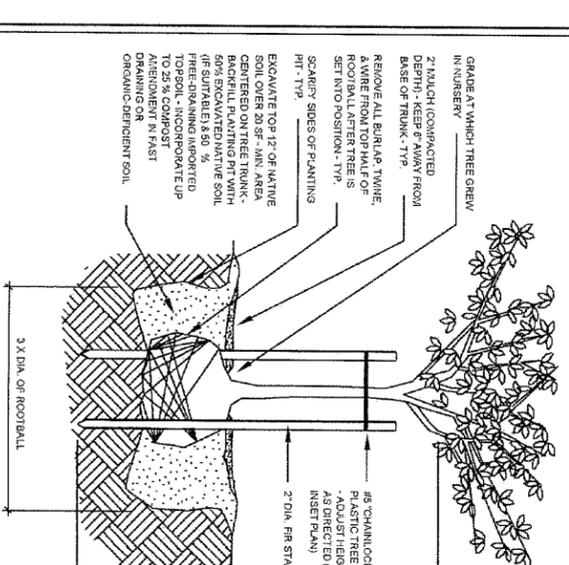
REMOVE ALL WRAP, TIES, AND CONTAINERS. SCOPE ROOTBALL AND WORK UNDER SOIL. SET BALL ON UNDISTURBED BASE OR COMPACTED MOUND UNDER BALL.

SET BALL ON UNDISTURBED BASE OR COMPACTED MOUND UNDER BALL.

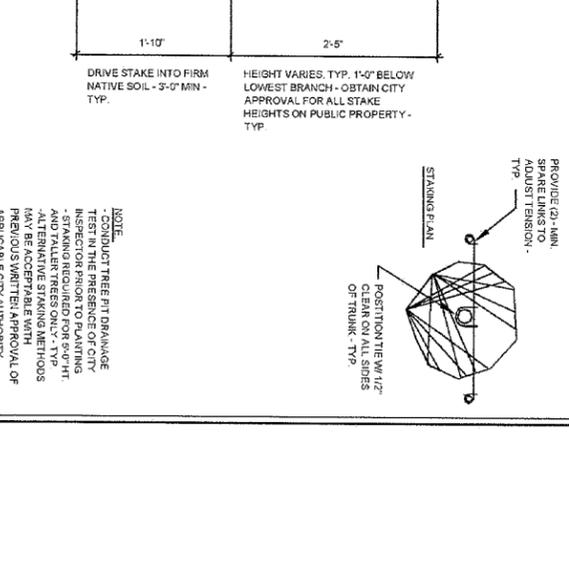
SITE TREE PLANTING/STAKING DETAIL



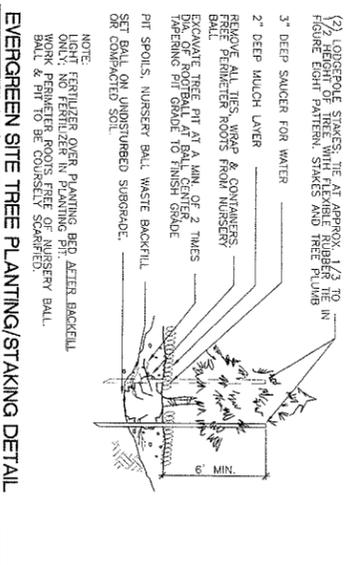
MULCH AT CURB DETAIL



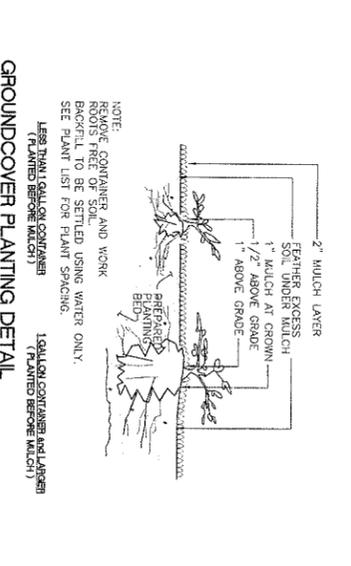
STREET TREE PLANTING/STAKING DETAIL



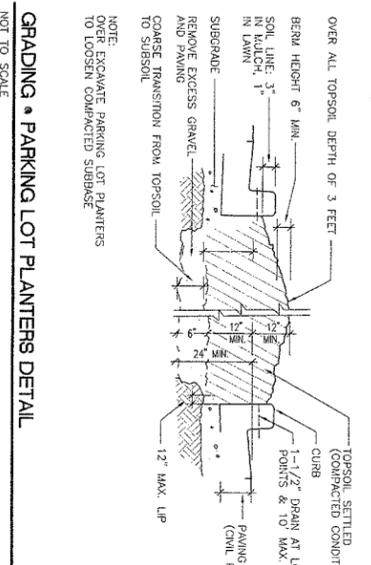
EVERGREEN SITE TREE PLANTING/STAKING DETAIL



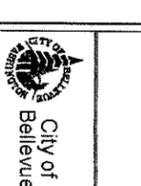
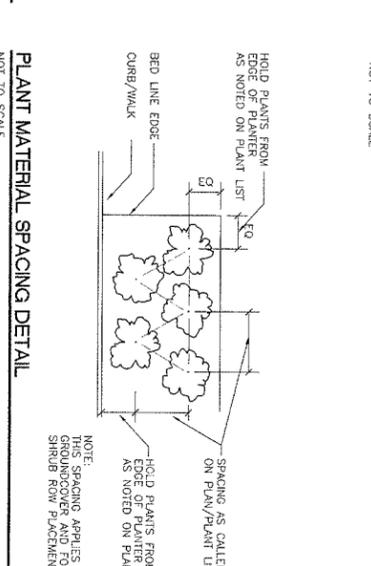
GROUNDCOVER PLANTING DETAIL



GRADING • PARKING LOT PLANTERS DETAIL



PLANT MATERIAL SPACING DETAIL



CITY OF BELLEVUE
TITLE: PLANTING, TREE

DRAWING NUMBER	32
SCALE	1/2" = 1'-0"
REVISION DATE	DATE
DEPARTMENT	PARKS



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CIVIL ENGINEERING, LAND PLANNING,
SURVEYING, ENVIRONMENTAL SERVICES

For: **KEYBANK**
106 148th AVE SE
BELLEVUE, WA

No.	Date	By	Ckd.	Appr.	SIT. PLAN CHANGES	Revision
1	12/03/10	AMS	DS	JE		
Title: LANDSCAPE PLANTING NOTES AND DETAILS						

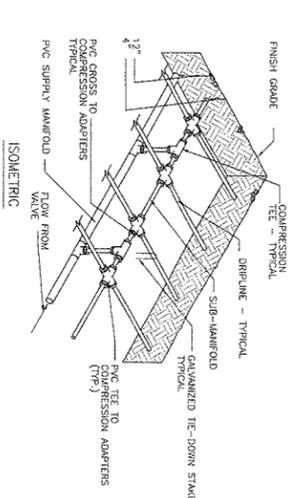
LANDSCAPE IRRIGATION NOTES AND DETAILS

SECTION 35, TOWNSHIP 25N, RANGE 5E, W.M.

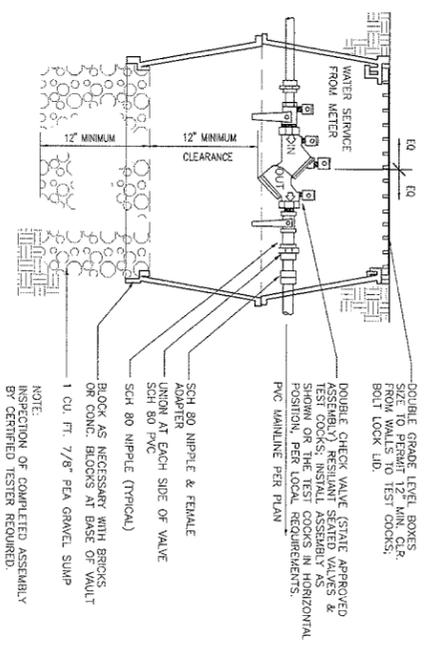
LANDSCAPE IRRIGATION NOTES

- GENERAL CONTRACTOR AND LANDSCAPE CONTRACTOR TO COORDINATE:
 - INSTALLATION OF 110V ELECTRICAL SERVICE FROM ELECTRICAL SOURCE TO AUTOMATIC CONTROLLER INCLUDING WIRE-HOOK-UP INTO MOUNTED CONTROLLER. IRRIGATION CONTRACTOR WILL MOUNT CONTROLLER PER DESIGN AND COORDINATE WITH GENERAL CONTRACTOR.
 - INSTALLATION OF IRRIGATION/SENSE METERS AND STUB TO PROVIDE STANDARD THREADED STUB-OUT WITH THREADED CAP ON DISCHARGE SIDE OF METER. STUB-OUT TO BE INSTALLED APPROXIMATELY 18 INCHES BELOW FINISHED GRADE.
 - VERIFICATION OF STATIC WATER PRESSURE AT POINT-OF-CONNECTION (P.O.C.) CONTRACTOR SHALL NOTIFY OWNER AND BARGHAUSEN CONSULTING ENGINEERS, INC. OF ANY VARIATION IN STATIC PRESSURE OVER 5 PSI GREATER/LESS THAN DESIGN PRESSURE.
 - INSTALLATION OF SLEEVING.
- SYSTEM HAS BEEN DESIGNED FOR 12 GPM AT 15 PSI. STATIC PRESSURE BY WATER DEPARTMENT TEST/CALCULATION 40 PSI.
- ALL WORK PER LOCAL CODE. INSTALLATION PER MANUFACTURER'S WRITTEN SPECIFICATIONS.
- ALL VALVES TO BE PLACED IN "CORROW" GRADE LEVEL. BOXES WITH BOLT-LOCK LIDS (OR APPROVED EQUIVALENT). SET BOXES 2 INCHES HIGHER THAN FINISH GRADE IN MUDCH ACRES AND FLUSH WITH FINISH GRADE IN LAWN ACRES.
- MAINLINE PIPE TO BE BURIED 18 INCHES AND LATERALS 12 INCHES BELOW FINISH GRADE. NO ROCK OR DEBRIS TO BE BACKFILLED OVER PIPE.
- HEAD AND LINE POSITIONING IS DIAGRAMMATIC ON PLAN. ADJUST IN FIELD AS NECESSARY FOR ADEQUATE COVERAGE. VALVES TO BE POSITIONED ADJACENT TO PAVEMENT/CURBS, IN SHRUB BEDS WHERE POSSIBLE.
- PROVIDE OWNER WITH TWO (2) SETS "AS-BUILT" DRAWINGS AND OPERATOR'S MANUAL UPON COMPLETION. INSTRUCT OWNER AS TO PROPER WINTERIZATION OF IRRIGATION SYSTEM (BLOW OUT).
- FAULT/LEAKAGE OWNERS FACILITY OPERATOR WITH IRRIGATION SYSTEM FUNCTION, CONTROLLER PROGRAMMING, SYSTEM OPERATION AND MAINTENANCE REQUIREMENTS.
- ALL CONTROL WIRE SPICES TO BE MADE AT VALVE BOXES WITH WATER TIGHT ELECTRICAL SPLICES, 3M, SCOTT'S LOCK SEAL, TACK 3376-78, OR EQUIVALENT.
- EACH VALVE BOX TO CONTAIN A MINIMUM OF ONE (1) SPARE ORANGE CONTROL WIRE. ROUTE ONE SPARE WIRE FROM THE CONTROLLER TO THE LAST VALVE OF EACH MAINLINE BRANCH. COMMON WIRE TO BE WHITE. SINGLE STRAND WIRE TO BE A MINIMUM OF 14 GAUGE.
- ALL ELECTRICAL EQUIPMENT TO BE U.L. TESTED AND APPROVED, AND BEAR THE U.L. LABEL.
- GROSS CONNECTION PROTECTION INSPECTION REQUIRED. THE BACKFLOW DEVICE TO BE TESTED UPON THE ORIGINAL INSTALLATION. THE TESTING TO BE PERFORMED BY A PERSON HOLDING A CURRENT CERTIFICATE AS A BACKFLOW TESTER. THE TEST REPORT TO BE SUBMITTED TO THE LOCAL WATER DISTRICT, OR PURVEYOR, AND OWNER WITH A COPY TO BARGHAUSEN CONSULTING ENGINEERS, INC. CONTRACTOR TO INCLUDE TESTING IN THE SCOPE OF WORK. OWNER IS RESPONSIBLE FOR ANNUAL INSPECTIONS AFTER THE INITIAL INSPECTION.
- CONTRACTOR TO PROVIDE SYSTEM WINTERIZATION/SPRING SERVICE WHEN INSTALLATION HAS BEEN COMPLETED WITHIN 90 DAYS OF COMPLETION. WINTERIZATION SERVICE HAS BEEN COMPLETED WITHIN 90 DAYS OF COMPLETION. WINTERIZATION SERVICE IS NOT TO BE PROVIDED FOR SPRING SERVICE. OWNER TO BE RESPONSIBLE FOR SCHEDULING SERVICE TO THE ABOVE DATES, OR AS FREEZE/PRECIPITATION CONDITIONS DETERMINE SERVICE NEEDED.
- IRRIGATION SCHEDULING:

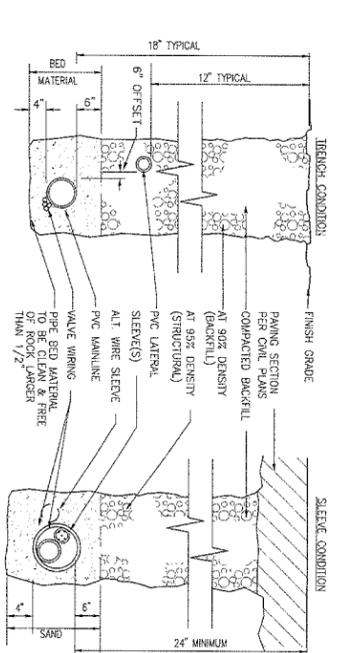
IRRIGATION CONTROLLER CONTAINS A WATER BUDGET FEATURE. PERIODIC (WEEKLY) ADJUSTMENT OF THE WATER SCHEDULE IS INTENDED TO BE MADE VIA BUDGET ADJUSTMENT. RE-ADJUST WATERING DAYS AT 100 PERCENT BUDGET WHEN ADJUSTMENT EXCEEDS 30%. SET CONTROLLER FOR HIGHEST ETS WATER SCHEDULE BASED ON PUBLISHED LOCAL EVAPOTRANSPIRATION DATA. SYSTEM HAS BEEN DESIGNED FOR 50 TO 80 PERCENT DISTRIBUTION UNIFORMITY. ALL WATERING IN EXCESS OF THE LOCAL ETS FIELD RECOMMENDATION TO BE COMPLETED DURING THE CONSTRUCTION PHASE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SCHEDULING THE IRRIGATION SYSTEM. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SCHEDULING TO REPAIR ANY RESULTANT DAMAGES AT CONTRACTOR'S OWN EXPENSE.
- SUBSTITUTION OF IRRIGATION MATERIAL/EQUIPMENT TO BE MADE ONLY UPON WRITTEN APPROVAL OF OWNER'S REPRESENTATIVE.



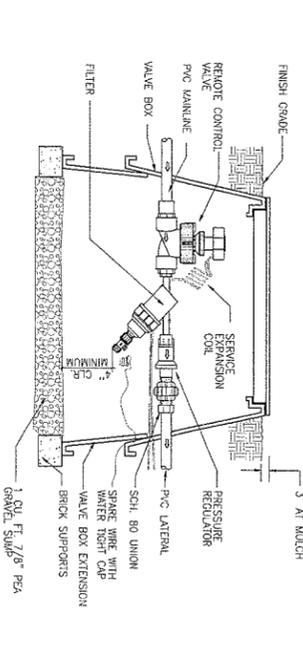
NOTE:
SEE PLANS & LEGEND FOR ALL DIMENSIONS AND LATERAL SPACING.
RATIO OF LATERALS TO STUB LAY VARY PER HYDRAULIC DESIGN.
CENTER-FEED SUPPLY SUB MANIFOLD (DRIP ZONE)
NOT TO SCALE



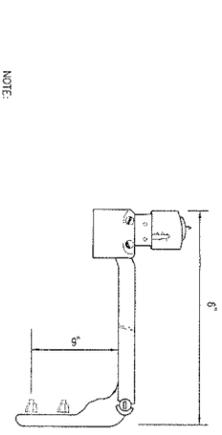
NOTE:
INSPECTION OF COMPLETED ASSEMBLY BY CERTIFIED TESTER REQUIRED.
CHECK VALVE ASSEMBLY
NOT TO SCALE



NOTE:
DIMENSIONS ARE MIN. CLEARANCES.
ALL IRRIGATION SLEEVING TRENCH BACKFILL MATERIAL TO BE CLASS "B" OR BETTER (MAX. OF 10% PASSING NO.40 SCREEN) AND BE COMPACTED TO MIN. 95% OPTIMUM DENSITY PER ASTM D-1557-70 (MODIFIED PROCTION).
SLEEVE/TRENCHING DETAIL
NOT TO SCALE

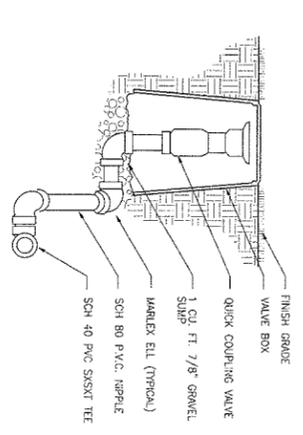


NOTE:
SEE PLANS & LEGEND FOR ALL DIMENSIONS AND LATERAL SPACING.
RATIO OF LATERALS TO STUB LAY VARY PER HYDRAULIC DESIGN.
REMOTE CONTROL VALVE, PRESSURE REGULATOR AND FILTER DETAIL
NOT TO SCALE

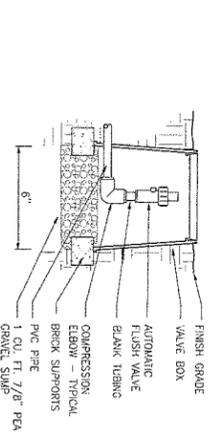


NOTE:
MOUNT ON WEATHER EXPOSED WALL 8\"/>

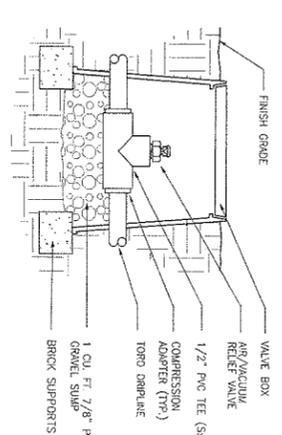
RAIN SENSOR DETAIL (WALL MOUNT) DETAIL
NOT TO SCALE



NOTE:
INSPECTION OF COMPLETED ASSEMBLY BY CERTIFIED TESTER REQUIRED.
QUICK COUPLING VALVE ASSEMBLY DETAIL
NOT TO SCALE



NOTE:
INSPECTION OF COMPLETED ASSEMBLY BY CERTIFIED TESTER REQUIRED.
AUTOMATIC FLUSH VALVE DETAIL
NOT TO SCALE



NOTE:
AIR/VACUUM RELIEF VALVE CANNOT BE CONNECTED LOWER THAN DRIPLINE LATERALS FOR USE ON ZONES OF 7 GPM OR LESS ONLY (PLUMBED TO TUBING).
**1/2\"/>

NOT TO SCALE**

No.	Date	By	AMS	DS	JE	SITE PLAN CHANGES	Revision
1	12/03/10	AMS	DS	JE			

For: **KEYBANK
106 148th AVE SE
BELLEVUE, WA**

Scale:
Horizontal N/A
Vertical N/A

Designed AMS
Drawn AMS
Checked DAS
Approved AMS
Date 08/16/10

Job Number **14458**

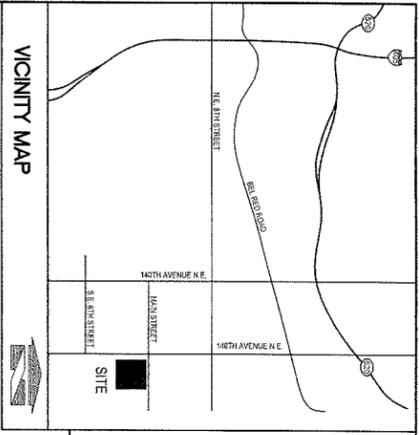
Sheet **L4 of 4**

18215 72ND AVENUE SOUTH
KENT, WA 98032
(425)251-6222
(425)251-8782 FAX

CIVIL ENGINEERING, LAND PLANNING,
SURVEYING, ENVIRONMENTAL SERVICES

BARGHAUSEN
CONSULTING ENGINEERS, INC.

STATE OF WASHINGTON
REGISTERED LANDSCAPE ARCHITECT
ARCHER M. SEMEL
CERTIFICATE NO. 766
(V.L.S. EXPIRES 12/31/13)



LEGEND:

EXISTING	PROPOSED
Water Line	Water Meter
Sanitary Sewer	Catch Basin
Storm Drain	Storm Drain
Concrete Barrier CURB	Concrete Barrier CURB
Concrete CURB & GUTTER	Concrete CURB & GUTTER
Concrete Pavement	Concrete Pavement
Asphalt Pavement	Asphalt Pavement
Limits of Clearing	Limits of Clearing

LEGAL DESCRIPTION
 PER COMMITMENT NO. NCS-373110-CLE, EFFECTIVE OCTOBER 17, 2008
 THE NORTH 180 FEET OF THE WEST 200 FEET OF THE NORTHWEST QUARTER OF SECTION 35, TOWNSHIP 25 NORTH, RANGE 5 EAST W.M. IN KING COUNTY, WASHINGTON;
 EXCEPT THE NORTH 30 FEET CONVEYED TO KING COUNTY FOR MAIN STREET BY DEED RECORDED UNDER RECORDING NO. 5382720;
 AND EXCEPT THE WEST 50 FEET OF 148TH AVENUE SOUTHEAST AS ESTABLISHED BY DEEDS TO KING COUNTY, RECORDED UNDER RECORDING NOS. 5036352 AND 5382721;
 AND EXCEPT THOSE PORTIONS OF MAIN STREET AND 148TH AVENUE SOUTHEAST CONVEYED TO THE CITY OF BELLEVUE BY DEED RECORDED OCTOBER 24, 1978 UNDER RECORDING NO. 781024101Z.

SURVEYOR NOTES
 THIS SURVEY IS SUBJECT TO ANY AND ALL IRREVOCABLE RESTRICTIVE COVENANTS, ITEMS LISTED BELOW MAY REPRESENT AN EASEMENT THAT WAS NOT SHOWN IN SCHEDULE B DOCUMENTS
 [X] NONE FOUND

REFERENCE DOCUMENTS

1. TITLE REPORT PER COMMITMENT FOR TITLE INSURANCE BY FIRST AMERICAN TITLE INSURANCE COMPANY FILE NO. NCS-373110-CLE, EFFECTIVE OCTOBER 17, 2008
2. RECORD OF SURVEY A.F.N. 901069009
3. CITY OF BELLEVUE SIDE SEWER AS-BUILT PERMIT NO. 6721 DATED NOVEMBER 27, 1988.
4. CITY OF BELLEVUE SIDE SEWER AS-BUILT PERMIT NO. 9430 DATED JULY 23, 1992.

IMPROVEMENT NOTES
 THIS IS A LISTING OF OBSERVED IMPROVEMENTS THAT CROSS DEED LINES. STATEMENT OF OWNERSHIP OR POSSESSION IS NOT THE INTENT OF THIS LISTING.

- CONCRETE CURB CROSSES DEED LINE
- CONCRETE CURB RUNS 0.3' WEST OF DEED LINE
- CONCRETE CURB RUNS 0.2' NORTH OF DEED LINE
- 8' CHAIN LINK FENCE IS ON DEED LINE
- CENTER OF LIGHT STANDARD FALLS 0.2' WEST OF DEED LINE
- CENTER OF SIGNAL STANDARD FALLS 0.4' NORTHWEST OF DEED LINE

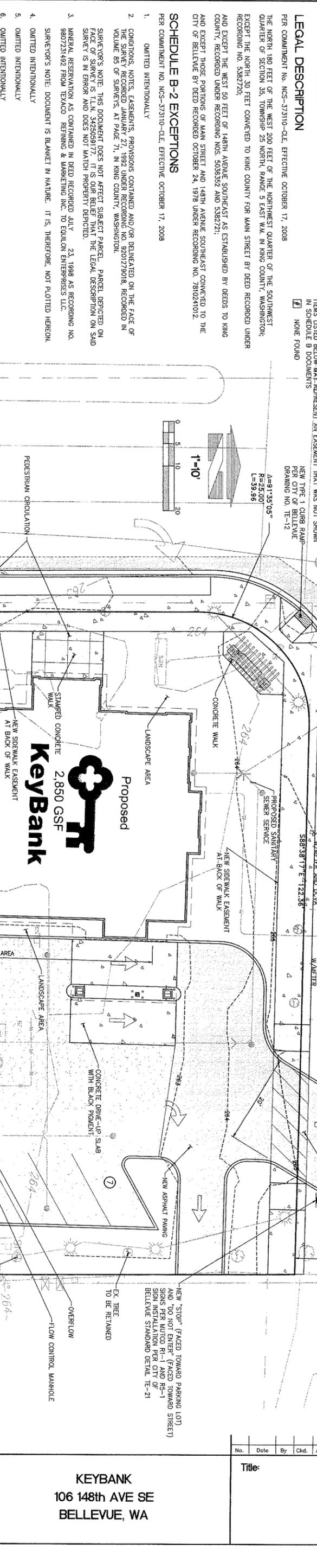
SCHEDULE B-2 EXCEPTIONS
 PER COMMITMENT NO. NCS-373110-CLE, EFFECTIVE OCTOBER 17, 2008

1. OMITTED INTENTIONALLY
2. CONDITIONS, NOTES, EASEMENTS, PROVISIONS CONTAINED AND/OR DELINEATED ON THE FACE OF THE SURVEY RECORDED JANUARY 27, 1992 UNDER RECORDING NO. 9201279018, RECORDED IN VOLUME 85 OF SURVEYS, AT PAGE 71, IN KING COUNTY, WASHINGTON.

SURVEYOR'S NOTE: THIS DOCUMENT DOES NOT AFFECT SUBJECT PARCEL. PARCEL DEPICTED ON FACE OF SURVEY IS T.L.N. 342609177. IT IS OUR BELIEF THAT THE LEGAL DESCRIPTION ON SAID SURVEY IS IN ERROR AND DOES NOT MATCH PROPERTY DEPICTED.

MINERAL RESERVATION AS CONTAINED IN DEED RECORDED JULY 23, 1998 AS RECORDING NO. 98023492 FROM TEXACO REFINING & MARKETING INC. TO EQUILON ENTERPRISES LLC.

SURVEYOR'S NOTE: DOCUMENT IS BLANKET IN NATURE. IT IS, THEREFORE, NOT PLOTTED HEREON.



ALTA/ACSM LAND TITLE SURVEY
 TO: KEY BANK NATIONAL ASSOCIATION;
 SURVEYOR'S CERTIFICATION
 THIS IS TO CERTIFY THAT THIS SURVEY ON WHICH IT IS BASED, WAS MADE IN ACCORDANCE WITH THE MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS IN 2005, AND INCLUDES ITEMS 2, 3, 4, 6, 7(a), 7(b), 7(c), 8, 9, 10, 11(a), 13 AND 18 OF TABLE A HEREOF. PURSUANT TO THE ACCURACY STANDARDS AS ADOPTED BY ALTA AND NSPS AND IN EFFECT ON THE DATE OF THIS SURVEY, THE SURVEYOR HAS CONDUCTED A REASONABLE INVESTIGATION OF THE STATE OF WASHINGTON, THE RELATIVE POSITIONAL ACCURACY OF THIS SURVEY DOES NOT EXCEED THAT WHICH IS SPECIFIED THEREIN.

FIELD WORK DATE: 10/24/08 **FIELD SURVEY:** AM AND ST. **P.L.S.:** STEFAN LAUFER, P.L.S.

REVISIONS

No.	Date	By	Ckd.	Appr.	Revision

Job Number 14458
Sheet 3 of 3

BARGHAUSEN CONSULTING ENGINEERS, INC.
 18215 72ND AVENUE SOUTH
 KENT, WA 98032
 (425)251-6222
 (425)251-8782 FAX
 CIVIL ENGINEERING, LAND PLANNING,
 SURVEYING, ENVIRONMENTAL SERVICES

Designed JAC,
Drawn JAC,
Checked DAS,
Approved DAS,
Date 08/28/10

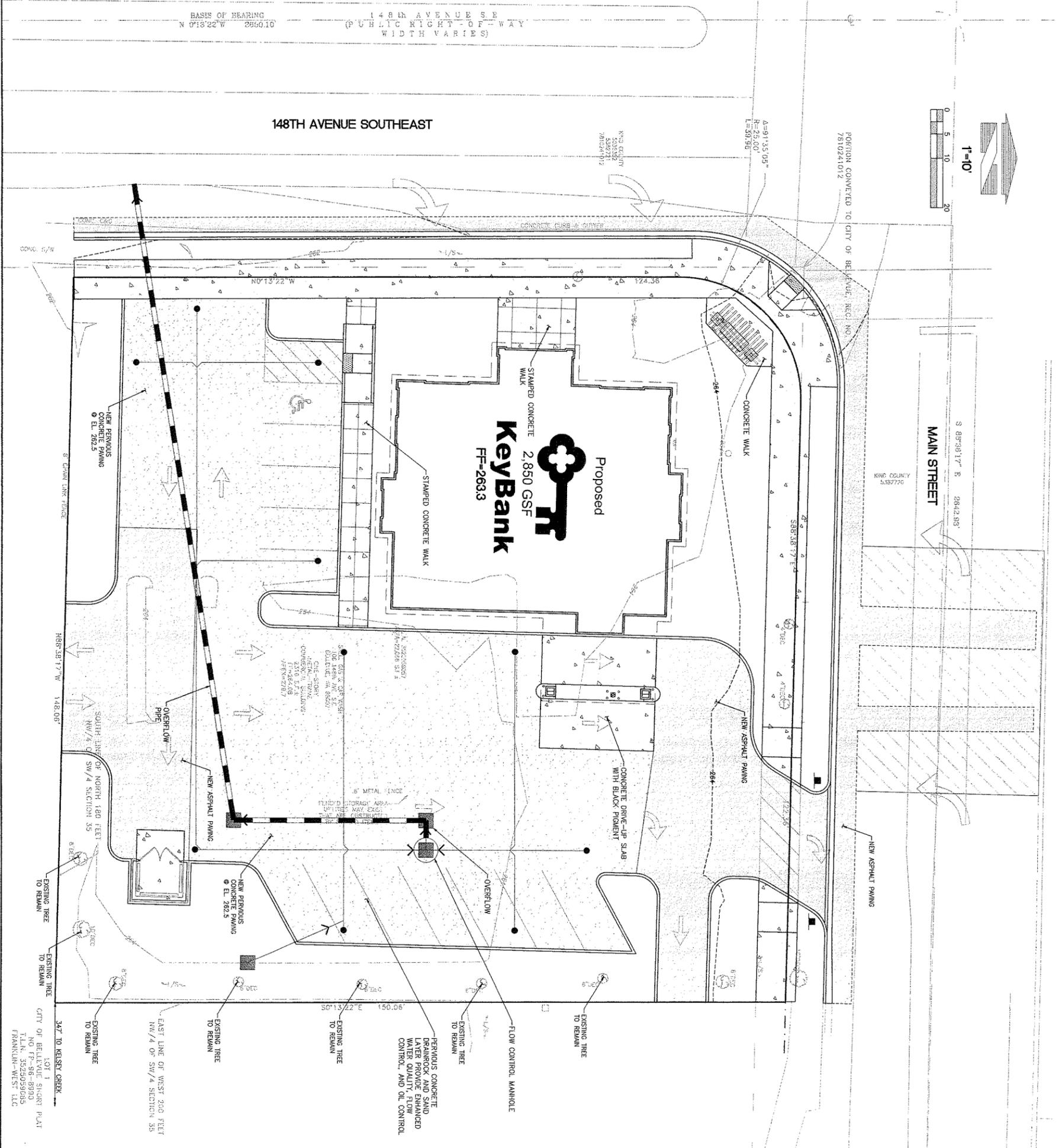
Scale:
 Horizontal 1"=10'
 Vertical N/A

For: KEYBANK
 106 148th AVE SE
 BELLEVUE, WA

Title: SITE PLAN B

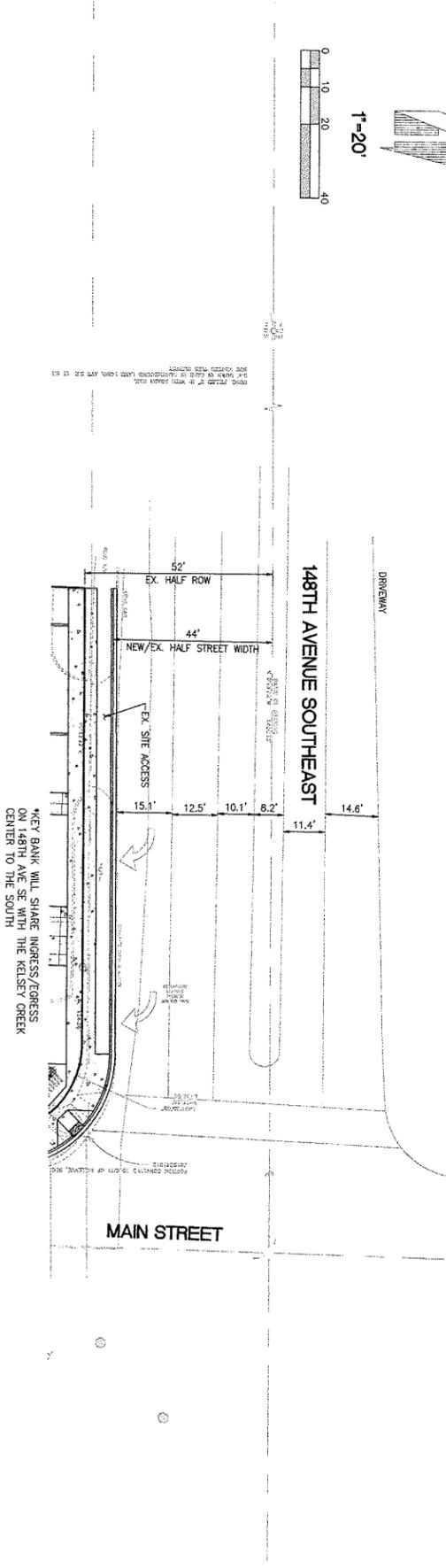
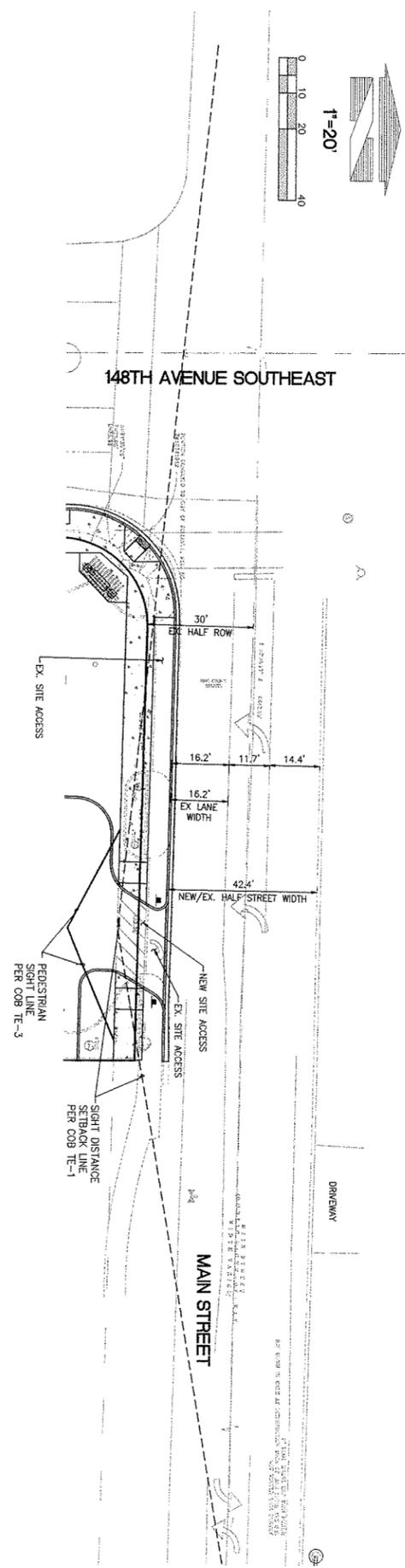
IMPROVED AREA=13,230 SF

SECTION 35, TOWNSHIP 25N, RANGE 5E, WM.
PRELIMINARY CLEARING AND GRADING PLAN



Job Number 14458	<p>18215 72ND AVENUE SOUTH KENT, WA 98032 (425)251-6222 (425)251-8782 FAX</p> <p>CIVIL ENGINEERING, LAND PLANNING, SURVEYING, ENVIRONMENTAL SERVICES</p>	Designed...JAC	Scale:		For:	<p>KEYBANK 106 148th AVE SE BELLEVUE, WA</p>	<p>No. Date By Ckd. Appr. Revision</p> <p>Title: PRELIMINARY CLEARING AND GRADING PLAN</p>
Sheet C2 of 3		Drawn...JAC	Horizontal 1"=10'		Checked...DAS		
		Date 08/25/10					

SECTION 35, TOWNSHIP 25N, RANGE 5E, W.M.
ROAD PLAN FOR DESIGN REVIEW



*KEY BANK WILL SHARE INGRESS/EGRESS ON 148TH AVE SE WITH THE KEISEY CREEK CENTER TO THE SOUTH

Job Number 14458	<p>18215 72ND AVENUE SOUTH KENT, WA 98032 (425)251-6222 (425)251-8782 FAX</p> <p>CIVIL ENGINEERING, LAND PLANNING, SURVEYING, ENVIRONMENTAL SERVICES</p>	Designed <u>JAC.</u>	Scale:		For:	<p>KEYBANK 106 148th AVE SE BELLEVUE, WA</p>	<p>Title:</p> <p>ROAD PLAN FOR DESIGN REVIEW</p>
Sheet C3 of 3		Drawn <u>JAC.</u>	Horizontal				
		Checked <u>DAS.</u>	1"=20'				
		Approved <u>DAS.</u>	Vertical				
		Date <u>08/25/10</u>	N/A				