



DEVELOPMENT SERVICES DEPARTMENT  
 ENVIRONMENTAL COORDINATOR  
 450 110<sup>th</sup> AVENUE NE  
 BELLEVUE, WA 98009-9012

## DETERMINATION OF NON-SIGNIFICANCE

**PROPONENT:** 1001 Properties, LLC

**LOCATION OF PROPOSAL:** 1001 106<sup>th</sup> Avenue NE

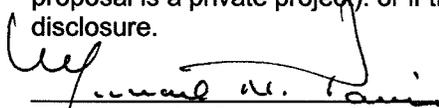
**DESCRIPTION OF PROPOSAL:** Demolition of a former auto dealership to construct two, 20 story high rise apartment buildings with commercial at base in two phases. Phase I (westerly tower): 165 apartments, 1.5 levels of underground parking with 3.5 partial levels of above grade parking. Phase II (easterly tower): 161 apartments, two levels of underground parking with 2.5 partial levels of above grade parking. An urban plaza to be provided at the SE corner of site with Phase II. Binding Site Plan included with this decision.

**FILE NUMBER:** 08-112057 LD

The Environmental Coordinator of the City of Bellevue has determined that this proposal does not have a probable significant adverse impact upon the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(C). This decision was made after the Bellevue Environmental Coordinator reviewed the completed environmental checklist and information filed with the Land Use Division of the Development Services Department. This information is available to the public on request.

- There is no comment period for this DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's office by 5:00 p.m. on \_\_\_\_\_.
- This DNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on the DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's Office by 5 p.m. on December 1, 2008.
- This DNS is issued under WAC 197-11-340(2) and is subject to a 14-day comment period from the date below. Comments must be submitted by 5 p.m. on \_\_\_\_\_. This DNS is also subject to appeal. A written appeal must be filed in the City Clerk's Office by 5 p.m. on \_\_\_\_\_.

This DNS may be withdrawn at any time if the proposal is modified so that it is likely to have significant adverse environmental impacts; if there is significant new information indicating, or on, a proposals probable significant adverse environmental impacts (unless a non-exempt license has been issued if the proposal is a private project); or if the DNS was procured by misrepresentation or lack of material disclosure.

  
 Environmental Coordinator

11/06/2008  
 Date

**OTHERS TO RECEIVE THIS DOCUMENT:**

State Department of Fish and Wildlife  
 State Department of Ecology,  
 Army Corps of Engineers  
 Attorney General  
 Muckleshoot Indian Tribe

Joni Pratt  
3/13/08

**WAC 197-11-960 Environmental checklist.**

ENVIRONMENTAL CHECKLIST

*Purpose of checklist:*

The State Environmental Policy Act (SEPA), chapter 43.21C RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An environmental impact statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

*Instructions for applicants:*

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

*Use of checklist for nonproject proposals:*

Complete this checklist for nonproject proposals, even though questions may be answered "does not apply." IN ADDITION, complete the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (part D).

For nonproject actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.

**A. BACKGROUND**

1. Name of proposed project, if applicable: Hanover Bellevue / Cadillac Site
2. Name of applicant: 1001 Properties, LLC; c/o The Hanover Company
3. Address and phone number of applicant and contact person:  
6720 North Scottsdale Road, Suite 110, Scottsdale, AZ 85253  
Mr. Eric E. Kenney, Managing Partner | PH: (480) 505-3377 | E-mail: ekenney@hanoverco.com
4. Date checklist prepared: February 26, 2008
5. Agency requesting checklist: City of Bellevue, Washington | Department of Planning & Community Development
6. Proposed timing or schedule (including phasing, if applicable):  
*All permits noted are City of Bellevue, WA, jurisdiction unless noted otherwise:*  
Design Review Application & Approval: February 2008 – November 2008 (Phases I & II)  
Right of Way Permit: July 2008 (Phases I & II)  
Demolition Permit: August 2008 (Phases I & II)  
Clear & Grade Permit: November 2008 (Phases I & II)  
Shoring & Below-Grade Structure Permit: November 2008 (Phase I only)

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Superstructure Permit: December 2008 (Phase I only)  
Above-grade Architectural Permit: January 2009 (Phase I only)  
Deferred Permits (misc): February 2009 (Phase I only)  
Demolition & Site Remediation: August 2008 – November 2008 (Phases I & II, entire site)  
Construction: November 2008 – February 2011 (Phase I only)

With exception to the Design Review permit, the schedule for Phase II clearing, grading, shoring & other building construction permits has not been established by the Applicant.

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

Yes, only in that the project will be constructed in two (2) phases. Timing between Phases has not been determined by the Applicant.

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

A summary of the previous environmental investigations and remediation completed at the site are provided in the Phase I Environmental Assessment Report (Phase I ESA) dated October 5, 2007 and the Phase II ESA Report dated October 8, 2007 prepared for the property. Cleanup of a localized area of petroleum contaminated soil will be performed during construction. The cleanup will be performed as an independent remedial action under the Washington State Department of Ecology Voluntary Cleanup Program (VCP) and in accordance with the Washington State Model Toxics Control Act Cleanup Regulation (MTCA) as established in Chapter 173-340 of the Washington Administrative Code (WAC 173-340). An Environmental Media Management Plan is currently being prepared to provide protocols for the identification, segregation, storage, sampling, analysis, disposal, and documentation of contaminated media that may be encountered during redevelopment activities at the property.

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.

None known at time of Application.

10. List any government approvals or permits that will be needed for your proposal, if known.

Refer to responses at Item A.6 above.

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.)

The existing site area equals 114,078 square feet (2.6189 acres). The maximum Floor Area Ratio (FAR) allowed by the City of Bellevue & available to the Applicant (including various bonuses) is 5.0, equivalent to a maximum above-grade buildout of 570,390 gross square feet.

Project entails demolition of an existing automobile dealership showroom, associated service bay structures, existing asphaltic concrete paving & miscellaneous site walls.

Proposed new Phase I construction consists of: one (1) level (49,000 gsf) of subterranean parking & three point five (3.5) partial levels (63,450 gsf total) of above-grade, structured parking. The above-grade parking levels are open to the exterior on the north & west sides, and surrounded by a twenty (20)-story high-rise, for-rent apartment residential tower to the south. The east side of the above-grade parking is opaque (no openings), in anticipation of an interface with Phase II development. The tower is comprised of ground floor residential project amenity space, as well as for-rent, mixed-use retail space.

Proposed Phase I floor plates range in size from approximately 15,700 gsf at the Ground Floor to approximately 10,000 gsf (max.) at Floor 20. The total proposed Phase I FAR-applicable Gross Square Footage is 272,690 gsf.

Proposed new Phase II construction consists of: two (2) levels (80,995 gsf total) of subterranean parking & two point five (2.5) partial levels (31,290 gsf total) of above-grade, structured parking. The above-grade parking levels are open to the exterior on the north sides, and surrounded by a twenty (20)-story high-rise, for-rent apartment residential tower to the east & south. The west side of the above-grade parking is opaque (no openings), in anticipation of an interface with previously-constructed Phase I development. The tower is comprised of ground floor residential project amenity space, as well as for-rent, mixed-use retail space.

Proposed Phase II floor plates range in size from approximately 15,700 gsf at the Ground Floor to approximately 10,000 gsf (max.) at Floor 20. The total proposed Phase II FAR-applicable Gross Square Footage is 280,550 gsf.

Thus, the total proposed Phase I & II FAR-applicable Gross Square Footage is 553,240 gsf.

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist.

Project site is located at the northwest corner of the intersection of NE 10<sup>th</sup> Street & 106<sup>th</sup> Avenue NE, Bellevue, WA 98004; immediately north of NE 10<sup>th</sup> Street & immediately west of 106<sup>th</sup> Avenue NE.

Property Tax Account No. 292505-9085-02 (legal property description located at upper-right hand corner of attached ALTA & Topographic Survey dated 9/2007)

Existing auto dealership business address at time of Application: 1001 106<sup>th</sup> Avenue NE

Future Phase I address (assigned by City of Bellevue): 10500 NE 10<sup>th</sup> Street

Future Phase II address (assigned by City of Bellevue): 10600 NE 10<sup>th</sup> Street

## B. ENVIRONMENTAL ELEMENTS

### 1. Earth

a. General description of the site (circle one): Flat, rolling, hilly, steep slopes, mountainous, other.

b. What is the steepest slope on the site (approximate percent slope)?

Approx. 5% max. (8' drop over 160' run) at existing & future planned conditions.

c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any prime farmland.

According to geologic mapping by Galster and Laprade (1991), the site is located on the Interlake Drift Upland ridge, which is underlain by glacial till of the Vashon Till. The general stratigraphy encountered at the Site in borings advanced during recent subsurface investigations consists of medium-dense to

dense silt and silty sand to 20 feet below ground surface (bgs), underlain by dense to very dense poorly-graded sand and gravel to the total depth explored of approximately 65.5 feet bgs.

- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe.

None known.

- e. Describe the purpose, type, and approximate quantities of any filling or grading proposed. Indicate source of fill.

The project will be a net export site and have approximately 50,000 cubic yards of cut. Any fill will be minor and will be from an approved source.

- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

The risk of erosion is minimal. Approved erosion control methods will be used during construction and the site will be stabilized upon completion.

- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

The site will have approximately 98% impervious paving at Phase II buildout.

- h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any:

A construction entrance, filter fabric fence around the perimeter of the site, and catch basin inserts will be installed prior to any clearing.

## 2. Air

- a. What types of emissions to the air would result from the proposal (i.e., dust, automobile, odors, industrial wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known.

During construction, some soil dust from grading operations & exhaust emissions from construction equipment.

At project completion, emissions may consist of: automobile exhaust, restaurant operations exhaust & gas-fired hot water boiler exhaust on a regular basis. Diesel-powered emergency generators (one at each phase) shall emit combustion exhaust approximately once per month during systems testing, as required by governmental agencies.

Quantities of emissions during construction & at project completion have not been determined at this initial Application.

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.

The proposed project is bounded to the immediate south & east by (at times) very busy city streets, therefore automobile exhaust emissions will be present. A large Hyatt hotel complex exists & is currently being expanded to the south, across NE 10<sup>th</sup> Street, therefore additional automobile exhaust & restaurant/food services operations exhaust will be present. At the time of this Application, there exist multiple other construction sites in the immediate surrounding Downtown Bellevue vicinity, thus various possible sources of construction-related emissions.

c. Proposed measures to reduce or control emissions or other impacts to air, if any:

Dust control will be effected via use of sprayed water to minimize airborne dust.

At project completion, restaurant exhaust systems will be filtered to reduce smoke & odor-causing particulates.

### 3. Water

a. Surface:

- 1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

No. According to the Environmental Database Report prepared by Environmental Data Resources, Inc. (EDR), there are no designated federal wetlands located on the Site. Lake Sturtevant is located approximately 0.75 mile east of the Site, and Lake Washington is located approximately 0.75 mile southwest of the Site. The nearest designated federal wetlands are located approximately 0.75 mile east and southwest of the Site.

- 2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.

No.

- 3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

Does not apply.

- 4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

Does not apply.

- 5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.

The Site is not located within a 100-year flood zone. A review of the Flood Insurance Rate Map (FIRM) for King County, Washington dated May 16, 1995 indicates that the Site is located outside the 100- and 500-year flood zone.

- 6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

No.

b. Ground:

- 1) Will ground water be withdrawn, or will water be discharged to ground water? Give general description, purpose, and approximate quantities if known.

No.

- 2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals. . . ; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

No waste material will be discharged into the ground.

c. Water runoff (including stormwater):

- 1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

Most storm water runoff will be roof runoff which will not require water quality. There is a proposed fire lane (approximately 15,000 square feet) along the northern and western property lines that will require water quality treatment. Storm water from the vehicular areas will be collected and routed to a mechanical water quality system prior to exiting the site. The project site is within the Meydenbauer Basin and per the City of Bellevue's requirements, detention is not required for the site.

- 2) Could waste materials enter ground or surface waters? If so, generally describe.

No

d. Proposed measures to reduce or control surface, ground, and runoff water impacts, if any:

A collection system will be installed to collect surface runoff and direct to the city maintained storm drains within NE 10<sup>th</sup> Street or 106<sup>th</sup> Avenue N.E. A sub-surface drainage system will be installed and pumped to the city maintained storm drain within NE 10<sup>th</sup> Street or 106<sup>th</sup> Avenue N.E. as well. The developed conditions runoff rate should be similar to the runoff rate for the existing conditions.

#### 4. Plants

a. Check or circle types of vegetation found on the site:

- deciduous tree: alder, maple, aspen, other  
 evergreen tree: fir, cedar, pine, other  
 shrubs  
 grass  
 pasture  
 crop or grain  
 wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other  
 water plants: water lily, eelgrass, milfoil, other  
 other types of vegetation

b. What kind and amount of vegetation will be removed or altered?

(10) Street trees at N.E. 10<sup>th</sup> and miscellaneous shrubs and ornamental evergreen trees.

c. List threatened or endangered species known to be on or near the site.

None known or observed.

TR.

- d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

All planting improvements to be congruent with guidelines and recommendations set by the City of Bellevue "Great Streets Project".

## 5. Animals

- a. Circle any birds and animals which have been observed on or near the site or are known to be on or near the site:

birds: hawk, heron, eagle, songbirds, other:       None observed  
mammals: deer, bear, elk, beaver, other:       None observed  
fish: bass, salmon, trout, herring, shellfish, other:   None observed

- b. List any threatened or endangered species known to be on or near the site.

None known or observed.

- c. Is the site part of a migration route? If so, explain.

None known or observed.

- d. Proposed measures to preserve or enhance wildlife, if any:

None known or observed.

## 6. Energy and natural resources

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.

Electricity & natural gas will be employed on-site at project completion. Electricity will be used to power lighting, mechanical ventilation & plumbing pumping systems, as well as all residential appliances. Natural gas shall be implemented at central hot water boilers for use in conditioning air at the mechanical ventilation systems. Restaurant operations uses will vary between electricity & natural gas, depending on various types of equipment. Natural gas may be used at exterior amenity area fireplaces. The emergency power generator systems shall be powered by diesel fuel.

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe.

Yes. The proposed project includes two (2) 20-story above-grade residential towers that would block, at various times of the day & at various times of the year, direct sunlight from reaching properties located primarily to the north & west; and secondarily to properties located east across 106<sup>th</sup> Avenue NE.

- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:

Features and/or measures not identified at time of Application.

**7. Environmental health**

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe.

Cleanup of a localized area of petroleum contaminated soil will be performed during construction. The cleanup will be performed as an independent remedial action under the Washington State Department of Ecology Voluntary Cleanup Program (VCP) and in accordance with the Washington State Model Toxics Control Act Cleanup Regulation (MTCA) as established in Chapter 173-340 of the Washington Administrative Code (WAC 173-340).

- 1) Describe special emergency services that might be required.

Does not apply

- 2) Proposed measures to reduce or control environmental health hazards, if any:

The cleanup action will meet the threshold requirements of WAC 173-340-360 to protect human health and the environment, comply with cleanup standards, and comply with applicable state and federal laws. A health and safety plan will be prepared in accordance with MTCA and Part 1910.120 of Title 29 of the Code of Federal Regulations prior to initiating the cleanup action.

**b. Noise**

- 1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

The proposed project is bounded to the immediate south & east by (at times) very busy city streets, therefore automobile traffic will be present. A large Hyatt hotel complex exists & is currently being expanded to the south, across NE 10<sup>th</sup> Street, therefore additional automobile noise will be present. At the time of this Application, there exist multiple other construction sites in the immediate surrounding Downtown Bellevue vicinity, thus various possible sources of construction-related noise sources.

- 2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

Short-term (during construction): Construction traffic (vehicular); construction equipment; construction activities, etc. Levels are not quantified at the time of this Application. Generally, noise generating activities will occur between the hours of 7:00AM & 6:00PM, in compliance with the City of Bellevue's noise requirements.

*M-F, 9-6 Sat*

Long-term (project completion): Residential operations: no noise generation other than automobile traffic entering & leaving the site, parking garage exhaust equipment; and rooftop heating ventilation & air conditioning equipment mounted at the roof above the 20<sup>th</sup> Floor. Restaurant operations: some limited automobile traffic, limited noise from exhaust equipment. Levels of noise are not quantified at the time of this Application.

- 3) Proposed measures to reduce or control noise impacts, if any:

Short Term: None proposed.

Long-term (project completion): Exhaust equipment will be outfitted with quiet-run motors & drive assemblies & generally located inside the building(s) with discharge points situated above-grade.

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8. Land and shoreline use

a. What is the current use of the site and adjacent properties?

The site is currently used as an automobile dealership showroom, associated service department operations & exterior, paved automobile storage lot. The property located immediately to the north consists of neighborhood retail ('strip') center & associated amenities, including a bank. The properties located immediately to the west encompass a swimming pool & spa equipment sales & service business; and a piano/musical instrument sales & service business.

South & across NE 10<sup>th</sup> Street is the Hyatt Hotel, currently undergoing expansion & renovation construction; as well as a planned performing arts center to replace existing neighborhood retail operations.

East & across 106<sup>th</sup> Avenue NE is a mid-rise residential project, currently under construction.

b. Has the site been used for agriculture? If so, describe.

No

c. Describe any structures on the site.

Existing site structures consist of: one (1), single-story, approximately 28,655 gsf, concrete 'tilt-up' structure with a decorative wood & glass structural element located at the southeast corner. This structure currently houses the automobile dealership showroom & associated service department operations, and;

One (1), single-story, approximately 2,543 gsf, concrete structure. This structure currently houses additional service department & automobile detailing operations.

d. Will any structures be demolished? If so, what?

Yes, both structures described at Item 8.c above

e. What is the current zoning classification of the site?

City of Bellevue Downtown-Multiple Use (DNTN-MU), Subdistrict C

f. What is the current comprehensive plan designation of the site?

City of Bellevue, City Center North

g. If applicable, what is the current shoreline master program designation of the site?

Not applicable.

h. Has any part of the site been classified as an "environmentally sensitive" area? If so, specify.

No.

i. Approximately how many people would reside or work in the completed project?

312 residential units total are planned. While difficult to project precisely how many persons will reside in each unit, 2 persons per unit (624 persons total, plus or minus), may reside at the project at Phase II

for final

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completion. It is estimated that 30 non-residents may work at the project, either for the apartment management company or for various retail uses planned for incorporation into the mixed-use elements.

j. Approximately how many people would the completed project displace?

Total number of existing employees at the automobile dealership is not certain, but estimated at thirty (30) persons.

k. Proposed measures to avoid or reduce displacement impacts, if any:

None

l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

The project is currently being planned by the Applicant & its Design Team to all applicable City of Bellevue land use & building code requirements. The Applicant is not intending to request variances from any of the currently-established requirements.

## 9. Housing

a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing.

Approximately 150 Phase I & approximately 168 Phase II housing units; all high-income rent basis.

b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.

None; no existing housing is being displaced.

c. Proposed measures to reduce or control housing impacts, if any:

Not applicable.

## 10. Aesthetics

a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

Height: 245 feet maximum above site average grade

Principal exterior building materials: Precast concrete, cast-in-place concrete, natural stone, aluminum & glass curtainwall & storefront systems, composite metal panels, steel & glass balcony guardrails.

b. What views in the immediate vicinity would be altered or obstructed?

Project will partially alter & obstruct northward views from the Hyatt Hotel project, as it is a high-rise complex. Project will partially alter & obstruct northwesterly views from the high-rise residential project located at the southeast corner of the intersection of NE 10<sup>th</sup> Street & 106<sup>th</sup> Avenue NE. All other structures & uses within the immediate project vicinity are low- to mid-rise facilities and face west, south or east from their respective locations toward the proposed project; and what are currently views toward nearby building-densified slopes (to the east & west); and to the downtown core/business district (to the south).

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Submittal

c. Proposed measures to reduce or control aesthetic impacts, if any:

The project is being carefully planned & evaluated within the requirements of the City of Bellevue's Transition Zones A, B & C (project is located in Transition Zone C), creating a layering/diminishing building height limitation moving northward away from the proposed project. Additionally, the Applicant has studied multiple view corridor scenarios, resulting in the proposed project massing & phasing, allowing views between & around the Phase I & Phase II tower elements from multiple surrounding locations.

11. Light and glare

a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

The proposed project will not create bright or glaring artificial light. Although the building exterior will be constructed of a variety of materials, the building skin will invariably incorporate glazed components that will create reflected sunlight glare at various times of the day, at various times of the year. Reflected sunlight glare will be generated predominantly off of the south-facing facades during the mid-day hours; off of the east-facing facades during the morning hours; and off of the west-facing facades during the afternoon hours.

b. Could light or glare from the finished project be a safety hazard or interfere with views?

No.

c. What existing off-site sources of light or glare may affect your proposal?

Applicant is not aware of any off-site sources of artificial light glare that would affect the proposal. For similar reasons stated in Item 11.a above, adjacent properties may produce some limited reflected sunlight glare issues. However, the proposed project is located north of most other high-rise buildings in the immediate vicinity & therefore not perceived as a significant issue.

d. Proposed measures to reduce or control light and glare impacts, if any:

Because the proposed project will incorporate a variety of exterior building materials, many non-reflective (except glazing), the buildings will not incorporate continuous, large expanses of glass.

12. Recreation

a. What designated and informal recreational opportunities are in the immediate vicinity?

A series of existing & planned Urban Parks & Open Space occurs within 2 to 6 blocks of the proposed project. The proposed project is also squarely within a network of Pedestrian Crossings & inter-related site accesses (i.e., mid-block connections) that will allow pedestrians to pass through various sites as alternatives to the street right-of-way sidewalks. The project-proper will incorporate outdoor, rooftop amenities adjacent to indoor club rooms & recreational activity spaces.

b. Would the proposed project displace any existing recreational uses? If so, describe.

No.

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- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

The project will not hinder or create impacts on existing recreation activities.  
See also response to Item 12.a above

### 13. Historic and cultural preservation

- a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? If so, generally describe.

No.

- b. Generally describe any landmarks or evidence of historic, archaeological, scientific, or cultural importance known to be on or next to the site.

None known.

- c. Proposed measures to reduce or control impacts, if any:

Not applicable.

### 14. Transportation

A trip generation and concurrency request memo was submitted to the City of Bellevue Transportation Department on February 11, 2008. A full *Transportation Impact Analysis* (TIA) will be prepared by Heffron Transportation, Inc. for the Cadillac Site project.

- a. Identify public streets and highways serving the site, and describe proposed access to the existing street system. Show on site plans, if any.

The project site is located at the northwest corner of the intersection of NE 10<sup>th</sup> Street & 106<sup>th</sup> Avenue NE, Bellevue, WA 98004; immediately north of NE 10<sup>th</sup> Street & immediately west of 106<sup>th</sup> Avenue NE. The project lies approximately 4 blocks west of Interstate 405. The main roadways serving the site would be 106<sup>th</sup> Avenue NE and NE 10<sup>th</sup> Street.

Primary vehicular access to Phase I shall be from a right turn-only into a driveway/fire lane off of westbound NE 10<sup>th</sup> Street, at the extreme southwest corner of the site. Secondary vehicular access to Phase I may be made from a driveway/fire lane on 106<sup>th</sup> Avenue NE, at the extreme northeast corner of the site. During Phase II construction, the secondary access will be temporarily inaccessible & all access must occur off of NE 10<sup>th</sup> Street.

At full build-out of the project, both driveways would serve the entire site with a right-in/right-out only driveway on NE 10<sup>th</sup> Street and a full access driveway on 106<sup>th</sup> Avenue NE.

- b. Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop?

Yes; one-quarter block to the west on Bellevue Way.

- c. How many parking spaces would the completed project have? How many would the project eliminate?

The total site includes 574 parking spaces (283 in Phase I and 291 in Phase II).

The project would eliminate 86 spaces, thus a net increase of 488 parking spaces would be provided.

- d. Will the proposal require any new roads or streets, or improvements to existing roads or streets, not including driveways? If so, generally describe (indicate whether public or private).

No, this proposal does not. It shall be noted that the City of Bellevue's pending plans to widen & otherwise modify 106<sup>th</sup> Avenue NE is not associated directly with nor caused by the Applicant's proposal herein.

- e. Will the project use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

No.

- f. How many vehicular trips per day would be generated by the completed project? If known, indicate when peak volumes would occur.

PM peak hour trip generation for the proposed project was estimated using the City of Bellevue trip rates for "multi-family" and "miscellaneous retail" land uses in the Downtown area. The existing trip generation estimates were based on the City of Bellevue trip rates for "car sales – new/used". The City of Bellevue supplied pass-by trip percentages for both the retail and car sales land uses. Ingress and egress percentages were derived from the Institute of Transportation Engineers' (ITE) *Trip Generation Manual*, 7<sup>th</sup> Edition, 2003 for the appropriate land uses.

Based on this information the project would generate 180 total PM peak hour trips (99 in and 81 out) at the site driveways. However, since the project is currently occupied, the proposed project would only add 83 net new PM peak hour trips (60 in and 23 out) to the roadway system.

The City of Bellevue does not supply daily trip rates; however, using ITE rates for "High-Rise Apartments" (Land Use Code 222) and "Specialty Retail" (Land Use Code 814), the project is estimated to generate 1,870 total daily trips. Using the ITE rates for "New Car Sales" (Land Use Code 841) with this estimate, the Cadillac project would have a net new daily trip increase of 760 trips to the roadway system.

- g. Proposed measures to reduce or control transportation impacts, if any:

Details regarding any transportation mitigation measures will be provided in the *Transportation Impact Analysis*, by Heffron Transportation, including transportation impact fees, and site access driveway restrictions.

#### 15. Public services

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe.

None known or yet identified to the Applicant by the City of Bellevue.

- b. Proposed measures to reduce or control direct impacts on public services, if any.

None planned.

16. Utilities

- a. Circle utilities currently available at the site: **electricity, natural gas, water, refuse service, telephone, sanitary sewer,** septic system, other. Storm drain & cable television.
- b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

Sanitary sewer connections; domestic, fire protection and irrigation connections; and storm drain connections will be made through the City of Bellevue. Puget Sound Energy will provide the electricity and gas connections. Comcast will provide cable.

C. SIGNATURE

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature: 

By: Steven T. Schrader, Architect | Associate | Carrier Johnson Architects  
Authorized Agent for Applicant, 1001 Properties, LLC

Date Submitted: February 26, 2008.....

T.P.



### Orthophoto Map



City of Bellevue  
 Information Technology  
 Geographic Information Services  
 March 20, 2008  
 Orthophotos flown March 2005

This map is derived from the Bellevue Geographic Information System and designed for City staff use. It is not guaranteed accurate.

If you have specific questions concerning information contained on this map please contact the department shown.

-  Parcel
-  School
-  Park
-  CityLimit



VICINITY MAP



**City of Bellevue  
Development Services Department  
Land Use Division Staff Report**

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Proposal Name: Hanover Cadillac

Proposal Address: 1001 106<sup>th</sup> Avenue NE

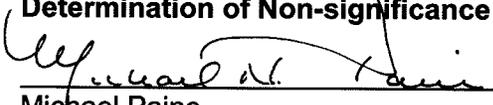
Proposal Description: Demolition of a former auto dealership to construct two, 20 story high rise buildings with commercial at base in two phases. Phase I (westerly tower): 165 units with 1.5 levels of underground parking with 3.5 partial levels of above grade parking. Phase II (easterly tower): 161 units with two levels of underground parking with 2.5 partial levels of above grade parking. An urban plaza will be provided at the SE corner of the site with Phase II. Binding Site Plan included with this decision.

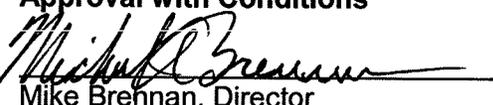
File Number: **08-112057 LD**

Applicant: 1001 Properties, LLC  
Hanover Company

Decisions Included: Design Review with SEPA, Process II  
Binding Site Plan, Process II

Planner: Antoinette Pratt, Senior Planner

State Environmental Policy Act  
Threshold Determination: **Determination of Non-significance**  
  
Michael Paine  
Environmental Coordinator

Director's Decision: **Approval with Conditions**  
  
Mike Brennan, Director  
Development Services Department

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Application Date:	<u>March 3, 2008</u>
Notice of Application Date:	<u>March 27, 2008</u>
Minimum Comment Period:	<u>April 10, 2008</u>
Bulletin Publication Date:	<u>November 13, 2008</u>
Appeal Deadline:	<u>December 1, 2008</u>

For information on how to appeal a proposal, visit Development Services at City Hall or call (425) 452-4570. Appeal of the Decision must be made by 5 p.m. on the date noted for appeal of the decision.

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Development Services Department ■ 425-452-4570 ■ Hearing Impaired: dial 711  
450 110<sup>th</sup> Avenue NE, Bellevue, WA 98004

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## I. REQUEST/PROPOSAL DESCRIPTION

### A. Request

1001 Properties, LLC requests Design Review and Binding Site Plan approvals to demolish a former Cadillac dealership to construct two, 20 story high rise apartment buildings with commercial at its base in two phases:

Phase I (westerly tower): 1.5 levels of underground parking with 3.5 partial levels of above grade parking embedded within a 20 story tower. The tower will contain 165 units. An urban plaza will be provided at the southeast corner of this structure. A garden deck is proposed at level four for tenants with connection to active recreational facilities. Phase I will contain 283,705 gross square feet (includes 291 parking stalls). The applicant has devoted 1,020 square feet of ground-floor retail space, 6,810 square feet of restaurant space, 3,330 square feet of resident amenity space (lobby, mailroom, etc) on level 01, 3,050 square feet of amenity space on level 02 and 1,050 square feet of amenity space on level 04.

Phase II (easterly tower): Two levels of underground parking with 2.5 partial levels of above grade parking embedded within a 20 story tower. The tower will contain 161 units. An urban plaza will be provided at the entire southern exposure of this structure to connect with Phase I plaza improvements. A garden deck is proposed at level three for tenants with connection to active recreational facilities. Phase II will contain 282,520 gross square feet (includes 316 parking stalls). The applicant devoted 4,280 square feet of ground-floor retail space, 2,790 square feet of restaurant space, 9,990 square feet of resident amenity space ( lobby, mailroom, etc).

The project includes street frontage improvements along NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE, along with vehicular and pedestrian mid-block connections along the north and west property boundaries.



See attached Project Plans & Drawings for more information.

**B. Review Process**

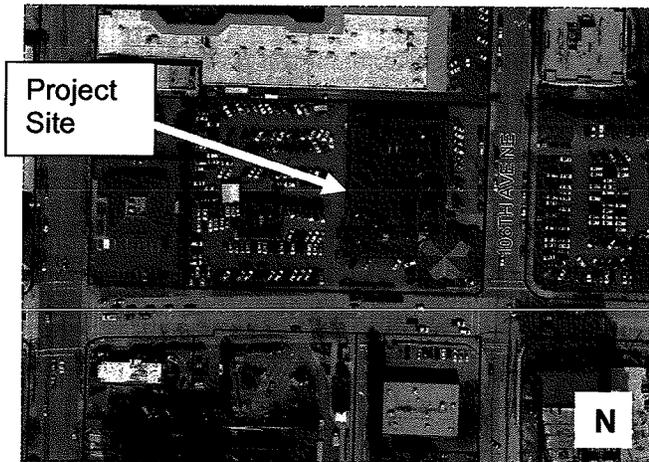
Design Review is required by Land Use Code (LUC) 20.30F. The Design Review, Binding Site Plan and SEPA Determination are all Process II decisions. Process II is an administrative process. The Environmental Coordinator issues the SEPA Threshold Determination and the Director of the Development Services Department issues the Design Review and Binding Site Plan decisions. An appeal of any Process II decision is heard and decided upon by the City of Bellevue Hearing Examiner.

LUC 20.30.165.A permits merger of Binding Site Plans with Design Review applications. In this instance, the State standards of RCW 58.17.035, Alternative Method of Land Division – Binding Site Plan applies. Upon completion of this application, the applicant will be required to file the approved Binding Site Plan with King County Records and Elections prior to issuance of building permits for phase I. See Section XI.C for related condition.

Extended vesting was requested on June 19, 2008, per LUC 20.25A.125 which allows extended vesting of this project to the Land Use Code for ten years for phased projects. Although this project vests to the LUC for ten years, it does not vest the project to ancillary codes such as Building, Fire, Utilities, etc. The project must comply with the codes in effect at the time of permit issuance. See Section X.C for related condition.

**II. SITE, ZONING & LAND USE CONTEXT**

**A. Site**



Total site size for this project is 114,078 square feet or 2.61 acres. Phase 1 is composed of 56,748 square feet or 1.3 acres while Phase II is composed of 57,330 square feet or 1.31 acres.

The project site is located at the northwest corner of NE 10th Street and 106<sup>th</sup> Avenue NE.

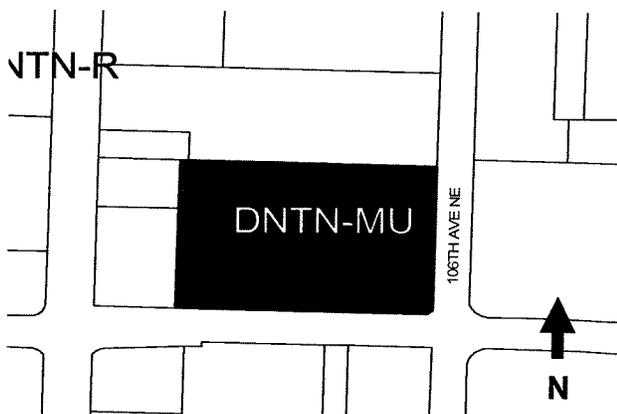
The site occupies a prominent corner of this intersection that is designated by the City's Comprehensive Plan as the "Entertainment Avenue" of the downtown (106<sup>th</sup> Avenue NE).

The designation was applied to this street given the broad choices of entertainment options located on this roadway. Alternatively, NE 10th Street is a collector arterial connecting low-density residential neighborhoods to the west with Downtown.

The site lies near the north limits of downtown zoning and is surrounded by existing low-rise development to the north and west. The best available views orient to the west overlooking low-rise neighborhoods to Lake Washington and the Seattle shoreline beyond. Views of the Cascades lie to the east and southeast. However, future development to the west will, most likely, block westerly views for Phase I development. Fortunately, NE 10th Street provides a protected view corridor to the east and south.

As part of the City Center North District, the site connects to other downtown neighborhoods. The City Center North neighborhood is characterized by medium density development and acts as a transition zone between the Downtown Core and its peripheral areas. Urban activities will be available through the urban plaza that has been designed with pedestrian friendly elements at the south portion of the site. This plaza will provide connections between the residential lobby and outdoor dining and seating areas, it will also provide connections to the proposed Performing Arts Center (PACE) and the Hyatt Hotel Expansion across NE 10<sup>th</sup> Street. Retail and restaurant uses are proposed at the base of this project to further support activities of the plaza.

### B. Zoning



The property is located within the Downtown-Mixed Use (DNTN-MU) land use district, Subdistrict C. No concomitant agreement applies to the property.

### C. Building Design

The applicant has proposed two, 20-story towers in two phases on an east-west axis. The Phase I tower structure is positioned generally in the center, easterly area of the parcel, while the Phase II tower structure is positioned generally in the south-center portion of the parcel in a SW direction to allow placement of the urban plaza below. Tenants and retail users will benefit from the southern exposure and views to the west from both towers. One of the towers, Phase II has been designed to stand on the corner and act as an anchor for the block and focal point of the intersection.

The positioning of the towers will support the view corridor down NE 10<sup>th</sup> Street and reinforce the urban plaza below. The plaza, in turn, will connect to the urban trail system of parks, public spaces, and mid-block connections required on this site and connections east across 106<sup>th</sup> Avenue NE

### Massing

Subdistrict C is characterized by medium density development and medium pedestrian activity, it is the transition zone between the Downtown core area and the peripheral residential areas. The density of development is allowed to “loosen” as it opens up toward the north setting-up the opportunity to create additional urban plazas in Downtown Bellevue.

The staggered orientation of the towers maximizes view opportunities as well as minimizes the impact to the views of the neighboring high-rise residential towers to the southeast (Washington Square). Phase I tower is shifted north allowing optimal sun exposure to residents and public amenities such as the plaza space along NE 10<sup>th</sup> Street. Phase II tower engages the intersection and has a strong presence at the corner of NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE. At the convergence of these significant streets and view corridors it becomes a landmark identifying and anchoring the north end of the City of Bellevue's Signature Entertainment Street.

The two towers are symbolically linked together with a "grand gesture". The subtly curved southern exposure serves multiple purposes. The upper towers "open" toward the west and on a regional level this simple gesture begins to identify a "place in the city". The southern walls of the mid-towers are oriented more toward the city views and on a local level help to define the public realm. The lower four levels become interactive, inhabited with resident amenity spaces, roof decks, and balconies. At street level an arcade at the base of the building provides a sheltered edge of the plaza.

Executed in two phases, the project proposes two 20 story residential towers. Each tower will maximize the allowable building height of 200 feet with an additional 30 feet (a 15% increase) for a total building height of 230 feet excluding the mechanical penthouse. This proposal intends to qualify for this additional building height bonus with the incorporation of significant floor plate modulation and interesting roof forms. Parking will be accommodated in approximately three levels above grade and two levels below grade. The above grade parking will be "wrapped" with residential amenity, and retail space on both street frontages. The respective bases of the two buildings will abut one another at an internal property line delineated on the Binding Site Plan. Vehicular access onto the site will be provided in one location per frontage as close as possible to mid-block on both NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE. The result of the massing will be a dynamic contrast between the orthogonal orientation, with respect to the City grid, of the north, east, and west elevations and the curvilinear form of the south wall.

### **Building Character / Materiality**

The character of the north exterior will be that of a "Super Trellis" with layered precast concrete creating a large scale texture in which resident balconies are captured. Limestone accents at the street level storefronts will provide a finished urban edge / street wall. The character of the south wall will be more "organic". At the lower levels, where this wall defines the plaza, "natural" materials indicative of the regional environment will create a more tactile experience.

The project will be of enduring design and quality. At this time, the applicant anticipates that the entire project will be of a concrete structure with an exterior skin that includes, but is not limited to, precast concrete panels with various beige/brown colors and materials, anchored and adhered stone veneer, curtain wall and storefront glazing systems.

## **III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS**

### **A. General Provisions of the Land Use Code**

Uses are regulated by Section 20.10.400 (Use Charts) and 20.25A (Downtown-Mixed Use zoning district). In addition, the uses proposed for this project are permitted with Design Review approval

subject to meeting the Design Review criteria of Land Use Code (LUC) 20.25A.110 and the Design Guidelines – Building/Sidewalk Relationships of LUC section 20.25A.115, summarized as follows:

**PHASE I**

Item	Permitted/Required	Proposed	Comments/Conditions
<b>Project Limit</b>	No minimum	56,748 SF	
<b>Building Height</b>	230 FT	244'-7" FT	LUC 20.25A.020.4.a (Includes 14 foot mechanical screen enclosure)
<b>Floor Area Ratio (FAR)</b>	5.0	5.0	LUC 20.25A.020
<b>Total Gross Square Feet (GSF)</b>	No minimum	291,535 GSF (Parking Excluded per 20.50.022G)	Excludes vertical penetrations, vent shafts, stairwells, and balconies per LUC 20.50.022
<b>Exempt Retail/Ped-Oriented Frontage Floor Area</b>	Maximum 1.0 FAR	7,830 SF (0.14 FAR)	LUC 20.25A.030.C Meets criteria of 20.25A.115 and Building/Sidewalk Guidelines.
<b>GSF for FAR</b>	283,740 GSF	283,705 GSF	Excludes parking and mechanical areas from GSF per LUC 20.50.022 . Exempt Retail Excluded per LUC 20.25A.020.3.a
<b>Floor Area per Floor Above 40 Feet</b>	20,000 SF	19,644 FAR area 20,685 GSF per Note [24] 20.25A.020	Floor plates above 40 ft are averaged per LUC 20.25A.020.A.2, note 5. (Floor plates average is 3.5%).
<b>Floor Area per Floor Above 80 Feet</b>	12,000 SF	12,237 FAR area 12,989 SF GSF per Note [24] (Floor levels 09 and above)	Maximum Floor Area per floor above 100' per 20.25A.020 Note [21]. Modification requested under 20.25A.020 B.1.a. (Floor plates average is 8.2%).
<b>Setbacks</b>			
<b>Front</b>	0 FT	8'-8"/34'-2" FT	LUC 20.25A.020, subnote 1
<b>Side(s)</b>	0/20 FT	1'-0" Below 40' 25'/30'-2" Above 40'	LUC 20.25A.020 In compliance with note [25].
<b>Rear</b>	0/20 FT	0' Below 40' 35'-2" Above 40'	LUC 20.25A.020
<b>Sidewalk Width</b>	12 feet	16 feet	LUC 20.25A.060.A

			Easement necessary. Secured through BSP.
<b>Landscape</b>	Perimeter sidewalk along both streets will be 16 feet side to include a minimum 4 foot wide landscape strip with street trees as follows: 106 <sup>th</sup> Avenue NE – European Hornbeam NE 10 <sup>th</sup> Street – Marshall Seedless Ash (LUC 20.25A.060)	Existing street trees to be removed along NE 10 <sup>th</sup> Street and 106 <sup>th</sup> Avenue NE. New 3” caliper trees will be installed within new 4 foot wide planting strips. <u>See Section X.C of this report for conditions of approval as well as landscape installation and maintenance.</u>	LUC 20.25A.060.B (See sheet L1-00)
<b>Mid-Block Walkways</b>	North and West property boundaries	Provided	LUC 20.25A.060.C (See Sheet L1-00)

**Parking and Loading**

Item	Required		Proposed	Comments/ Conditions
Parking	Minimum and Maximum Permitted Ratios	Required and Permitted Parking Stalls	Proposed Parking Stalls	LUC 20.25A.050
<b>Residential Units</b> 165	Min. 1.0 Max. 2.0	Reqd. 165 Permitted 330	217	
<b>Office</b> (net square feet)	N/A	N/A		
<b>Retail</b> 815 nsf (assumed 80% of gsf)	Min. 2.0 Max. 4.0 per 1000 sf	Reqd. 2 Permitted 4	2	LUC 20.25A.050, subnote 2 1,020 gsf for retail
<b>Restaurant</b> 5,450 nsf (assumed 80% of gsf)	Min. 10.0 Max. 20.0 per 1000 sf	Reqd. 59 Permitted 118	60	6,810 gsf for restaurant
<b>Other (Visitor)</b> 7% of total stalls			12	
<b>Total</b>			291	

	Required/Permitted	Proposed	
<b>Compact Stalls</b>	Max. 65% of total stalls = 188 stalls	65	LUC 20.25A.050.F.2
<b>Loading Area</b>	One off-street 10'x55' space, or other as approved by the Director (LUC 20.20.590K.4.a-c)	Provided within the garage. Accessed off NE 10 <sup>th</sup> Street Phase I.	
<b>Recycling &amp; Solid Waste</b>	Phase I 1.5 SF/unit x 165 units = 247 SF 5 SF/1000 SF retail x 1,020 SF = 5 SF 5 SF/1000 SF rest. x 6,810 SF = 34SF (LUC 20.20.725) 286SF	300 SF Phase I	

**PHASE II**

Item	Permitted/Required	Proposed	Comments/Conditions
<b>Project Limit</b>	No minimum	57,330 SF	
<b>Building Height</b>	230 FT	243'-6" FT	LUC 20.25A.020.4.a (Includes 14 foot mechanical screen/enclosure)
<b>Floor Area Ratio (FAR)</b>	5.0	4.93	LUC 20.25A.020
<b>Total Gross Square Feet (GSF)</b>	No minimum	289,590 GSF	Excludes vertical penetrations, vent shafts, stairwells, and balconies per LUC 20.50.022. Parking Excluded per 20.50.022.G.
<b>Exempt Retail/Ped-Oriented Frontage Floor Area</b>	Maximum 1.0 FAR	7,070 SF (0.12 FAR)	LUC 20.25A.030.C Meets criteria of 20.25A.115 and Building/Sidewalk Guidelines
<b>GSF for FAR</b>	286,650 GSF	282,520 GSF	Excludes parking and mechanical areas from GSF per LUC 20.50.022. Exempt Retail Excluded per LUC 20.25A.020.3.a.
<b>Floor Area per Floor Above 40 Feet</b>	20,000 SF	20,350 FAR area 21,430 GSF per	Modification requested under 20.25A.020 B.1a Floor plates above 40 ft are averaged per LUC 20.25A.020.A.2, note 5. (Floor plate's average is 8.2%).

<b>Floor Area per Floor Above 80 Feet</b>	12,000 SF	11,470 FAR area 12,300 SF GSF per Note [24] (Floor levels 09 and above)	Maximum Floor Area per floor above 100' per 20.25A.020 Note [21] Modification requested under 20.25A.020 B.1.a. (Floor plate's average is 2.5%).
<b>Setbacks</b>			
<b>Front</b>	0 FT	8'-9"	LUC 20.25A.020, subnote 1
<b>Side(s)</b>	0/20 FT	0'-0"/24'-5" Below 40' 39'-7"/24'-5" Above 40'	LUC 20.25A.020 In compliance with note [25].
<b>Rear</b>	0/20 FT	0' Below 40' 36'-2" Above 40'	LUC 20.25A.020
<b>Sidewalk Width</b>	12 feet	16 feet	LUC 20.25A.060.A Easement necessary. Secured through BSP.
<b>Landscape</b>	Perimeter sidewalk along both streets will be 16 feet side to include a minimum 4 foot wide landscape strip with street trees as follows: 106 <sup>th</sup> Avenue NE – European Hornbeam NE 10 <sup>th</sup> Street – Marshall Seedless Ash (LUC 20.25A.060)	Existing street trees to be removed along NE 10 <sup>th</sup> Street and 106 <sup>th</sup> Avenue NE. New 3" caliper trees will be installed within new 4 foot wide planting strips. <u>See Section X.C of this report for conditions of approval as well as landscape installation and maintenance.</u>	LUC 20.25A.060.B (See sheet L1-00)
<b>Mid-Block Walkways</b>	North and West property boundaries	Provided	LUC 20.25A.060.C (See Sheet L1-00)
<b>Special streetscape at intersections</b>	City Center North District	Provided	POLICY S-DT-67 seeks development of a soft or hard open space amenity in the vicinity of 106 <sup>th</sup> Avenue NE and NE 10 <sup>th</sup> Street within the <i>City Center North</i> District.

**Parking and Loading**

Item	Required		Proposed	Comments/ Conditions
<b>Parking</b>	<b>Minimum and Maximum Permitted Ratios</b>	<b>Required and Permitted Parking Stalls</b>	<b>Proposed Parking Stalls</b>	<b>LUC 20.25A.050</b>
<b>Residential Units</b> 161	Min. 1.0 Max. 2.0	Reqd. 161 Permitted 322	271	
<b>Retail</b> 3,425 nsf (assumed 80% of gsf)	Min. 2.0 Max. 4.0 per 1000 sf	Reqd. 8 Permitted 16	8	LUC 20.25A.050, subnote 2 4,280 gsf
<b>Restaurant</b> 2,232 nsf (assumed 80% of gsf)	Min. 10.0 Max. 20.0 per 1000 sf	Reqd. 25 Permitted 46	25	2,790 gsf
<b>Other (Visitor)</b> 7% of total stalls		12	12	
<b>Total</b>			316	
	<b>Required/Permitted</b>	<b>Proposed</b>		<b>Comments/ Conditions</b>
<b>Compact Stalls</b>	Max. 65% of total stalls = 188 stalls	36		LUC 20.25A.050.F.2
<b>Loading Area</b>	One off-street 10'x55' space, or other as approved by the Director (LUC 20.20.590K.4.a-c)	Provided within the garage. Accessed off 106 <sup>th</sup> Avenue NE for Phase II and NE 10 <sup>th</sup> Street for Phase I.		
<b>Recycling &amp; Solid Waste</b>	<b>Phase II</b> 1.5 SF/unit x 161units = 241 SF 5 SF/1000 SF retail x 4,280 SF = 21 SF 5 SF/1000 SF rest. x 2,790 SF = 13 SF (LUC 20.20.725) 275 SF	830 SF Phase II		

**B. Analysis of Height Modification**

1001 Properties has requested a height increase beyond the underlying zoning district. LUC 20.25A.090, subnote (2) states the following regarding height modifications:

The maximum height may be increased by no more than 15 percent or 15 feet, whichever is greater, and only if the applicant can demonstrate that the additional height accommodates architecturally integrated mechanical equipment, interesting roof forms, significant floor plate modulation, significant facade modulation, or other such unique architectural features, and that the resulting design exceeds the quality and design requirements of LUC 20.25A.110.

The basic height for the DNTN-MU, Subdistrict C is 200 feet. Height may be increased to 230 feet per LUC 20.25A.090, subnote (2) as noted above. An additional 15 feet increase may occur for mechanical equipment and screening. Total maximum height is 245 feet. Building height for Phase I is proposed at 244'-7" and 243'-6" for Phase II which includes the allowable 15 foot increase for mechanical equipment and screening.

The applicant should note that if the design of the mechanical equipment increases in height, a building height reduction will be necessary to conform to this standard. Changes to the shown roof details below are subject to approval by staff and consistency with the code during building permit review. Modification to the roof design/mechanical equipment without approval may place the height increase gained at risk for reduction to the normal maximum building height of 200 feet. See Section X.C for related condition.

Height modifications are granted for Phase I and II because the applicant has proposed the following:

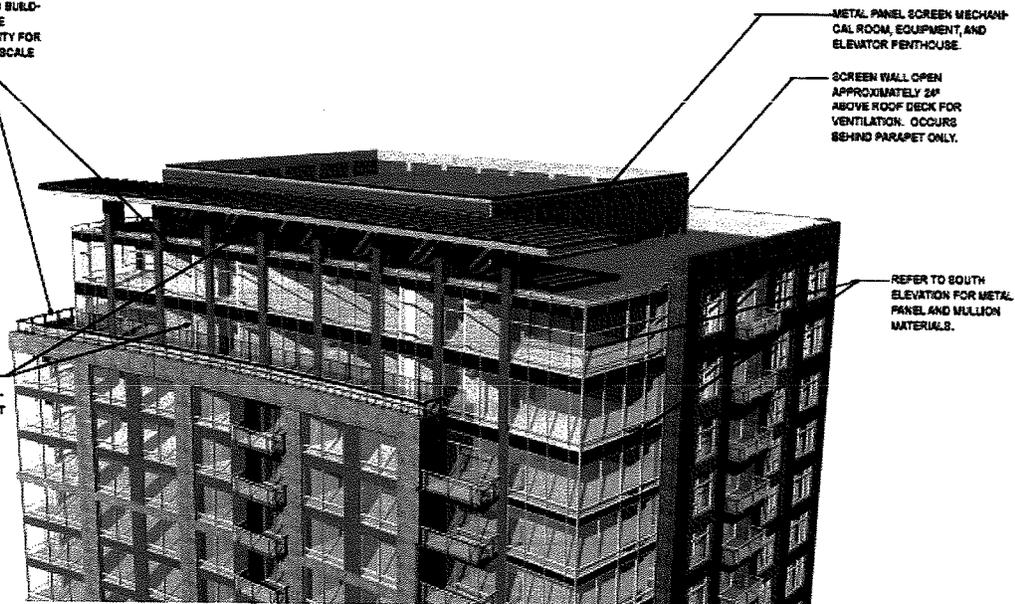
- Placement of the elevator overrun in the center of the building.
- A 14 foot metal screen wall to screen the elevator overrun and to provide additional area for placement of associated equipment.
- Roof decks at the west elevation for Phase I are provided to create façade modulation and an interesting roof form.
- A steel roof trellis will provide building identity on the south side of the structure and a unique architectural feature.
- The colors and materials used for the mechanical equipment will match the building body creating an architecturally integrated system.

See images below:

### Phase I Roof:

SIGNIFICANT MODIFICATION TO BUILDING FOOTPRINT AT PENTHOUSE LEVELS PROVIDES OPPORTUNITY FOR ROOF DECKS, ADDING HUMAN SCALE AND VISUAL INTEREST.

VERTICAL EXTENSION OF CURVED GLASS FORM WITH EXPRESSED PILASTERS AND UPLIFT CANTILEVERED BRON.



**Phase II Roof:**



As conditioned, the proposal is eligible for a modification of height.

**C. Special District Requirements**

**1. FAR Amenity System Requirements**

Subject to LUC 20.25A.020C.2, the amount of bonus floor area in square feet generated by the amenities provided to meet the Basic Floor Area Ratio (FAR) requirement must at a minimum be equal to 20% of the Project Limit area times (X) the Basic FAR permitted for a non-residential building in the land use district.

The Basic FAR requirement for **Phase I** in square feet =  $0.20 \times \text{the Project Limit area} \times \text{the Basic Non-residential FAR}$  ( $0.20 \times 56,748 \times 0.5 = 5,675\text{SF}$ ). The Basic FAR requirement for **Phase II** in square feet =  $0.20 \times \text{the Project Limit area} \times \text{the Basic Non-residential FAR}$  ( $0.20 \times 57,330 \times 0.5 = 5,733 \text{ SF}$ ).

The amount of "Basic" FAR Amenity Earned—18,300 SF for Phase I and 29,232 SF for Phase II—calculated below, exceeds the minimum required 5,675SF and 5,733 SF for Phase I and II respectively.

**Table 1: Bonus Amenity Area Earned  
 Phase I**

Project Gross Floor Area (GFA)	283,705 SF
Basic Permitted Floor Area (Basic FAR X Project Limit)	113,496 SF (2.0 X 56,748 SF)
Additional Floor Area Requested	170,209 SF (283,705 – 113,496)
“Basic” FAR Amenity Earned	18,300 SF (Refer to Table 2, Phase I below)
Remaining (“Bonus”) FAR Amenity to Earn (Project GFA - Basic Permitted Floor Area - FAR Earned)	151,909 SF (282,705 – 113,496 – 18,300)
Total FAR Amenity Earned	358,525 SF (Refer to Table 2 below)
Excess FAR Amenity Earned (Total FAR Amenity Earned – Basic FAR Amenity Earned – Bonus FAR Amenity to Earn)	188,316 SF (358,525 – 18,300 – 151,909)

**Table 2: Bonus Amenity Area Earned  
 Phase II**

Project Gross Floor Area (GFA)	282,520 SF
Basic Permitted Floor Area (Basic FAR X Project Limit)	114,660 SF (2.0 X 57,330 SF)
Additional Floor Area Requested	167,860 SF (282,520 – 114,660)
“Basic” FAR Amenity Earned	29,232 SF (Refer to Table 2 below)
Remaining (“Bonus”) FAR Amenity to Earn (Project GFA - Basic Permitted Floor Area - FAR Earned)	138,628 SF (282,520 – 114,660 – 29,232)
Total FAR Amenity Earned	352,726 SF (Refer to Table 2 below)
Excess FAR Amenity Earned (Total FAR Amenity Earned – Basic FAR Amenity Earned – Bonus FAR Amenity to Earn)	184,866 SF (352,726 – 29,232 – 138,628)

**Table 3: Bonus Amenities  
 Phase I**

Subject to LUC 20.25A.030.D, the total amount of bonus floor area earned through the Amenity Incentive System for a project, and the total amount of bonus floor area to be utilized on-site for that project, must be recorded with the King County Division of Records and Elections and with the Bellevue City Clerk. Therefore, a condition is included in Section X of this report requiring that the applicant record a copy of the approved bonus point calculations, project drawings and conditions of this Design Review approval. See Section X.D for related condition.

Amenity		Units of Measure	Bonus Ratio	Bonus Floor Area Earned	How it Meet the Description and Design Criteria? Public Benefit?
<b>Basic</b>	Pedestrian Oriented Frontage	Square Feet (SF)	100:1	14,600 SF (146 SF x 100)	Stimulates sidewalk pedestrian activity.
	Landscape Feature	Square Feet (SF)	4:1	1,000 SF	Vegetative focal point and visual landmark. Increased pervious surface.
	Marquee	Square Feet (SF)	2:1	2,700 SF (1,350 LF x 2)	Weather protection along building and sidewalk.
	<b>Sub-Total Basic</b>			<b>18,300 SF</b>	
<b>Non-Basic</b>	Underground Parking	Square Feet (SF)	0.5:1	24,710 SF (49,420 SF x 0.5)	Hides parking area underground. Optimizes use of site for pedestrian and commercial activities.
	Above-grade parking located under structure	Square Feet (SF)	4:1	255,760 SF (63,940 SF x 4)	Hides parking area behind podium façade.
	Residential Uses	Square Feet (SF)	2:1	45,350 SF (22,675 X 2)	Area devoted to service cores and community facilities. Residential uses not counted in this figure.
	Landscape Area	Square Feet (SF)	1:1	9,775 SF (3426 SF at base & 6,349 SF)	Visual landscape area for tenants and increases pervious surface.
	Active Recreation Area	Square Feet (SF)	1:1	4,630 SF (3,640 + 990)	Active recreational space for tenants.
	<b>Sub-Total Non-Basic</b>			<b>340,225 SF</b>	
	<b>Combined Total</b>			<b>358,525 SF</b>	

**Table 4: Bonus Amenities  
 Phase II**

Subject to LUC 20.25A.030.D, the total amount of bonus floor area earned through the Amenity Incentive System for a project, and the total amount of bonus floor area to be utilized on-site for that project, must be recorded with the King County Division of Records and Elections and with the Bellevue City Clerk. Therefore, a condition is included in Section X of this report requiring that the applicant record a copy of the approved bonus point calculations, project drawings and conditions of this Design Review approval. See Section X.D for related condition.

Amenity		Units of Measure	Bonus Ratio	Bonus Floor Area Earned	How it Meet the Description and Design Criteria? Public Benefit?
<b>Basic</b>	Pedestrian Oriented Frontage	Square Feet (SF)	100:1	20,800 SF (280 SF x 100)	Stimulates sidewalk pedestrian activity.
	Landscape Feature	Square Feet (SF)	4:1	1,500 SF	Vegetative focal point and visual landmark. Increased pervious surface.
	Arcade	Square Feet (SF)	4:1	6,932 (1,733 X 4)	Continuous covered area that provide weather protection to publicly accessible space.
	<b>Sub-Total Basic</b>			<b>29,232 SF</b>	
<b>Non-Basic</b>	Underground Parking	Square Feet (SF)	0.5:1	41,685 SF (83,370 SF x 0.5)	Hides parking area underground. Optimizes use of site for pedestrian and commercial activities.
	Above-grade parking located under structure	Square Feet (SF)	4:1	192,400 SF (48,100 SF x 4)	Hides parking area behind podium façade.
	Residential Uses	Square Feet (SF)	2:1	68,732 SF (34,366 X 2)	Area devoted to service cores and community facilities. Residential uses not counted in this figure.
	Landscape Area	Square Feet (SF)	1:1	19,277 SF (6,073 SF at base & 13,240 SF)	Visual landscape area for tenants and increases pervious surface.
	Active Recreation Area	Square Feet (SF)	1:1	1,400 SF	Active recreational space for tenants.
	<b>Sub-Total Non-Basic</b>			<b>323,494 SF</b>	
	<b>Combined Total</b>			<b>352,726 SF</b>	

**2. Design Review Criteria and Design Guidelines**

The proposal meets the Design Review criteria (LUC 20.25A.110) as follows (see LUC for specific criteria language):

### **Vehicular Circulation and Parking (LUC 20.25A.110.A.1)**

Two vehicular connections will occur on-site: one at the northwest corner and the other at the southwest corner of the property. This connection will serve as emergency fire access to allow fire department access around the building. Mid-block pedestrian connections of eight feet will can be accessed in these same locations; however, the pedestrian connections will be separated by vegetation and/or bollards. Special pavements materials will be used as well. Continuous pedestrian access will continue across 106<sup>th</sup> Avenue NE leading east through mid-block connections that end at the Ashwood playfield.

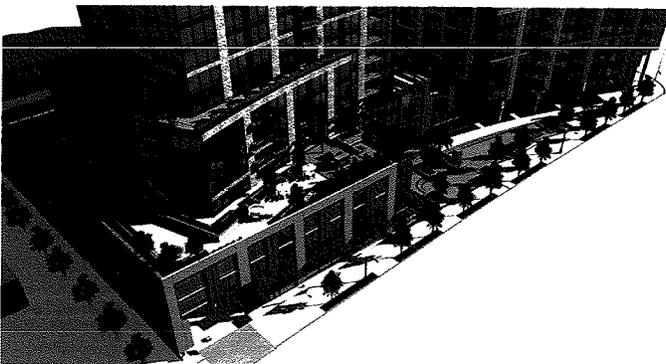
All parking is located internal to the building. Two garage openings are proposed for Phase I while one is proposed for Phase II each leading to 291 and 316 parking stalls respectively. Each phase will also have a separate loading dock for deliveries. Phase I will contain 1.5 levels of underground parking with 3.5 levels of above grade parking. Phase II will contain two levels of underground parking with 2.5 partial levels of above grade parking.

### **Pedestrian Circulation and Amenities (LUC 20.25A.110.A.2)**

Pedestrians can circulate around the entire building by way of sidewalks and the north/south and east/west mid-block pedestrian connections at the west and north property boundaries. Street trees, weather protection, landscaping, and special paving are used to provide a more pleasant pedestrian experience.

### **Wind and Sun (LUC 20.25A.110.A.3)**

The building is designed to step down to a pedestrian level particularly with the Phase I building design. The four story podium at the base of Phase I will frame the plaza area at the southeast corner of this phase. The south facing orientation will optimize sun exposure to this area. Sheet L1, landscaping also makes provision for trees along the south edge of this plaza to block prevailing winds from the south. Additionally, the applicant will provide a weather protection amenity for pedestrians with Phase II. This canopied sitting area will provide an area of respite from inclement weather.



### **Open Space (LUC 20.25A.110.A.4)**

The project has two large open space areas for residents: Phase I courtyard at Level 4 and Phase II courtyard at level 3. The placement of the urban plaza at the southern property boundary will be beneficial to proposed restaurants as a portion of the plaza could be used for outdoor seating. Such an area is proposed as a "Winter Garden" at the SW corner of the plaza.

### **Light and Glare (LUC 20.25A.110.A.5)**

The exterior skin of this structure is a combination of glass, stone with matte finish metal panels with a low reflection and glare factor. The applicant submitted a colors and materials board to substantiate the low reflectivity of their chosen materials.

**Downtown Patterns and Context (LUC 20.25A.110.B)**

**Natural Setting and Topography (LUC 20.25A.110.B.1)**

Topography on this site slopes gently down from the northwest to the southeast with a grade differential of approximately 8 feet or 10 percent. The urban plaza reflects this grade change with transitional steps at various intervals. Both buildings have been designed to accommodate the change in grade at the first level. The massing of the towers provides for the maximization of views of the mountains, Lake Washington and the Seattle skyline from the units as well as the amenity spaces placed at levels three and four of these structures.

**Landscape Design (LUC 20.25A.110.B.2)**

There is significant landscaping in the urban plaza and on levels three and four of these structures. The site currently does not contain significant existing vegetation, with the exception of street trees on NE 10<sup>th</sup> that were cut by the previous owner without City permission and are now in decline. These trees will be removed with this application and replaced with four foot planter areas that will contain a combination of street trees, shrubs, and ground cover. The current planned utility vaults for each Phase are located below grade, and all new utility equipment will be contained inside the building envelope or below grade.

**Views (LUC 20.25A.110.B.3)**

As discussed previously, the tower massing was set to reduce the impact of views from buildings to the north, south, and west of the site. The project will enhance the Bellevue skyline and therefore contribute to more interesting views from public spaces. Both towers, by their orientation will have westerly views of Seattle.

**Building Height and Bulk (LUC 20.25A.110.B.4)**



The building is oriented northwest to southeast. This orientation allows Phase II to have a strong presence at the corner of NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE. The southeast corner will be a focal point on 106<sup>th</sup> Avenue NE which is identified as Bellevue’s “Entertainment Street”. The design has provided slender towers that are 80 feet apart. Attention has been paid to the residential neighborhoods to the north, south, and west of the site. The

roofline of both towers creates interest by their unique forms.

**Transitions (LUC 20.25A.110.B.5)**

The ground floor of both buildings contain restaurant, retail and leasing functions. An arcade will be featured north of the urban plaza to promote activity at the street level. As mentioned earlier, a “winter garden” is planned for Phase I for an outdoor eating and gathering area. The areas

contain mostly storefront glass with marquees of spandrel frosted glass that will allow natural light into these spaces.

**Patterns of Activity (LUC 20.25A.110.B.6)**

The building provides various opportunities for pedestrian-level activity at the ground plane, including ground level retail, restaurant, and residential lobby and leasing spaces. There are areas within the urban plaza that will contain seatwalls adjacent to planter areas, tables and chairs, and scored pavement materials.

**Signage (LUC 20.25A.110.B.7)**

Signage information has not been provided at this time. General signage information for the building will be required prior to building permit issuance with a formal sign package following such submittal. See Sections X.C and E.

**Design Guidelines – Building/Sidewalk Relationships (LUC 20.25A.115)**

NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE are designated as “D” Right-of-Ways. These rights-of-way have been given this designation as it is anticipated that they shall have low to moderate orientation to pedestrians. Although these streets have this classification, the north end of Bellevue at NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE may become a focal point for this area. This proposal will bring retail to this portion of 106<sup>th</sup> Avenue NE. Additionally, in the near future, the proposed PACE fine arts building is slated to be constructed at the SW corner of 106<sup>th</sup> Avenue NE and NE 10<sup>th</sup> Street. Once complete, the restaurants and retail could provide evening activities after concerts. This proposal incorporates restaurant, service and commercial activities along both street frontages. On 106<sup>th</sup> Avenue NE, 100 percent of the ground level façade is dedicated to retail activities while NE 10<sup>th</sup> Street contains a combination of restaurant, retail and residential functions.

**Perimeter Design District (LUC 20.25A.090.E)**

This subdistrict requires midblock connections to encourage pedestrian movement in this location. The applicant has fulfilled the standards for pedestrian connection by providing a north/south and east/west connections. Additional pedestrian connection will occur with the sidewalk easements provided at the south and east portions of the site. An urban plaza will further define the southern portion of the site by providing hardscape and softscape materials. The building will provide wind shelter from the north and westerly winds.

This district also requires complimentary building colors and materials. The applicant has chosen to clad the building in brown/beige building materials as shown on the colored elevation submitted by the applicant (see file for elevation). Accent colors in green and brown will be used at various intervals throughout the building body. A light brown metal panel material will be used for the mechanical screen. All building materials have been proposed to minimize reflected light. Garden terraces at level 4 of phase I and level 3 of phase II. Private balconies at various levels throughout the building could also provide additional vegetative relief.

**IV. PUBLIC NOTICE AND COMMENT**

As of the date of this staff report, no written comments were received for this project.

## **V. TECHNICAL REVIEW**

### **A. Utility Department**

Utility review has been on a conceptual basis only, consequently there are no implied approvals of the engineering specifications for the water, sewer and storm drainage components of the project. Engineering review of the water, sewer and storm drainage infrastructure will be performed under the Utility Developer Extension Agreements, and will coincide with the clearing and grading permit review. Final civil engineering may require changes to the site layout to accommodate the utilities. See Section X for related condition.

### **B. Fire Department**

The site development plans for this application generally conform to Fire Code requirements. However, there are a number of conditions that must be met prior to issuance of building permits. See Section X.C for related conditions.

### **C. Transportation Department**

#### **Project Phasing and Binding Site Plan**

The applicant requested a Binding Site Plan to allow splitting the development into two phases. The Binding Site Plan will apply to the entire site, regardless of the number of phases. Any street right of way or sidewalk/utility easement dedications needed for either phase must be granted as part of the Binding Site Plan no later than the time of Phase I building permit approval.

All public transportation infrastructure for both phases, including any temporary transition features, must be designed prior to approval of final engineering plans for Phase I. If approved standards for transportation infrastructure change between approval of Phase I and the beginning of construction for Phase II, then engineering plan revisions may be necessary for Phase II in order to comply with standards at that time.

Transportation infrastructure for Phase I must be constructed prior to occupancy of Phase I, along with appropriate transitions to Phase II. Acceptable pedestrian facilities must be provided on both adjacent streets during all phases, although some pedestrian facilities for Phase II may be temporary during Phase I, with city approval. The design of a new driveway connection from Phase I across Phase II to 106<sup>th</sup> Avenue NE must be acceptable to the city prior to approval of final engineering plans for Phase I. Prior to occupancy of Phase I, an acceptable financial assurance device must be provided to guarantee the construction of Phase II transportation infrastructure on an agreed schedule not to exceed 24 months beyond initial occupancy of Phase I.

Current city code requires a minimum sidewalk width of twelve feet on both adjacent street frontages. The applicant voluntarily agreed to provide public sidewalks sixteen feet wide on both frontages. Therefore, the Binding Site Plan must include dedication of a standard sidewalk/utility easement covering all area that will not be within a street right of way within the first sixteen feet behind the back of the future curb on both streets. At the street corner, the back of the sidewalk/utility easement shall follow a curve parallel to and sixteen feet behind the proposed

back-of-curb radius. Also at the street corner, the Binding Site Plan shall include dedication of additional right of way as needed to encompass any area between the existing right of way and the back of the proposed curb that is not already within existing right of way.

See Section X: A, B, C, and D for related Conditions of Approval. See Street Frontage Improvements below for more information regarding sidewalk construction.

### **Site Access**

Access to the proposed project will be provided via a driveway connecting to NE 10<sup>th</sup> Street near the southwestern corner of the site and a driveway connecting to 106<sup>th</sup> Avenue NE near the northeastern corner of the site. The two driveways will be connected to each other by an interior access road along the site's western and northern edges. Accesses to the parking garages and truck loading areas will be off the interior access road. The driveway connection to 106<sup>th</sup> Avenue crosses Phase II and will be used by Phase I in what may be a temporary configuration that will be reconstructed for Phase II. See Section X: A, B, C, and D for related Conditions of Approval.

### **Street Frontage Improvements**

In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, the construction of street frontage improvements, including at least the items listed below, is required as a condition of development approval. The design of the improvements must conform to the requirements of the Americans with Disabilities Act, the Transportation Development Code (BCC 14.60), the Transportation Department Design Manual, and requirements stated elsewhere in this document. See Section X: A, B, C, and D for related Conditions of Approval.

1. Streetlight, traffic signal, and street tree plan: Prior to issuance of the clearing and grading permit, a combined streetlight, signal, and street tree plan, showing street trees, streetlights, and traffic signal poles on one plan sheet, must be submitted and must be acceptable to the Transportation Department regarding streetlight type and location, including proper separation between streetlights and trees or other fixtures. The goal is to provide the optimum number of street trees while not compromising the light and safety provided by streetlights.

Streetlight locations must be determined by light level analysis acceptable to the Transportation Department. Generally, streetlights must be at least 25 feet from the nearest tree and at least ten feet from Point A in standard drawing DEV-6 or DEV-7D. In order to produce a plan that is compatible with the surroundings, the final combined street tree and streetlight plan sheet must refer to streetlights on adjacent frontage or across the street and to nearby trees. Subsequent landscaping and architectural plans for the development must comply with the streetlight and traffic signal locations shown on said combined plan sheet. The streetlight plan submitted by DCI Engineers on June 19, 2008 is satisfactory as to streetlight and street tree position for the design review phase. Further technical review will be required prior to approval of final engineering and landscape plans.

Relocation of the traffic signal pole and related equipment at the street intersection is likely, due to the required street widening. Prior to approval of the final engineering plans for Phase I, a final streetlight and traffic signal plan with technical specifications must be prepared by a

licensed engineer experienced with streetlight analysis and familiar with Bellevue's standards for the design of streetlight and traffic signal systems. Full details and specifications for the lights, poles, foundations, junction boxes, conduit, wiring, and related equipment must be submitted for review and approval along with civil engineering and landscape plans. The developer's engineers must confirm that no known or planned underground utilities will conflict with the proposed location of streetlight or traffic signal foundations. Streetlight contactor cabinets and other streetlight or signal system hardware must be located so as not to interfere with the main pedestrian path.

2. Sidewalk construction: The curb, gutter, and sidewalk on both streets fronting the Cadillac project site shall be completely removed and reconstructed with a public sidewalk at least sixteen feet wide, not including the curb. Planters and tree wells are included in the sixteen-foot width. Any temporary sidewalk or related features needed to transition between Phase I and Phase II shall be included in the Phase I plans and constructed as part of Phase I. Existing curbs located in the proper future alignment may remain in place only if the Transportation Department inspector accepts the condition of such curb.
3. Sidewalk cross slopes, elevations, and ADA compliance: The Americans with Disabilities Act (ADA) requires that cross slopes on accessible sidewalks shall not exceed two percent. The sidewalk cross slope may be less than two percent (but not less than -2.0%) if the sidewalk has a slope sufficient to provide adequate drainage. Bellevue's standard for curb height is six inches, except where curb ramps are needed. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm compliance. New curbs and sidewalks shall be constructed in compliance with these requirements. Building elevations shall be consistent with the required curb and sidewalk elevations. Curb and sidewalk elevations will not be revised to fit the building. The following process is required at various stages during design and construction to ascertain that construction will achieve the proper final sidewalk elevations and cross slope.
  - a. As part of the building permit submittal, the developer will prepare a detailed exhibit illustrating spot elevations and sidewalk cross slopes at key points where the back of the sidewalk is at or near the building wall, especially at door thresholds or other points where the sidewalk and building elevations must match. The sidewalk elevation is tied to the top of the curb, which has a fixed height (6 inches) above the street's gutter line, and the gutter elevation is fixed by the street's intended cross slope and centerline elevation.
  - b. Working from the points identified in a) above, the developer will document the elevation differences between said points at the surface and other key points to be established during construction, such as:
    - i. Elevation at the bottom of the excavation or the bottom deck.
    - ii. One or more intermediate stages, such as two decks below sidewalk level. It is not necessary to document the elevation of every subsurface deck; the intent is to confirm that construction of intermediate stages is on track to reach the correct elevation of the main deck.
    - iii. Main deck elevation, established to mesh properly with the required sidewalk elevations.

- c. Confirming key elevations and differences identified in a) and b) will be part of building permit review and approval.
  - d. During construction, the developer will provide documentation, signed and stamped by a licensed surveyor, confirming the elevations at each level described in b) and other levels that may be determined during review or construction. Surveyed elevations must be accurate to within 1 in 5000. The intent is to confirm that construction of the bottom deck and intermediate stages is on track to reach the correct elevation of the main deck.
  - e. If the survey data in d) shows that elevations at the bottom or intermediate levels are off, then corrections must be made during building construction in order to achieve the required elevation of the main deck. Such corrections must be submitted for city review as revisions of the building permit.
  - f. The city recognizes that some deviation is unavoidable during construction. Therefore, in order to not exceed a sidewalk cross slope of 2%, the project design should aim for a sidewalk cross slope of approximately 1.5% to 2%, with a clear understanding that 2% must not be exceeded, and that slope sufficient for adequate drainage must be provided.
4. Sidewalk design, materials, features, and ADA compliance: The design, materials, and appearance of the sidewalk and landscaping on both adjacent street frontages shall comply with the standards and drawings in the Transportation Department Design Manual, including standard drawings TE-11 and DEV-3, and the requirements of the Americans with Disabilities Act. ADA also requires provision of a consistent travel path for visually handicapped pedestrians.

The sidewalk shall be constructed of standard concrete with a broom finish and a two-foot by two-foot score pattern, with four-foot by six-foot tree wells or four-foot wide planter strips, unless both the Transportation Department and the Development Services Department agree to accept any non-standard pattern, color, or other features.

Install a full-depth expansion joint at the back of the public sidewalk easement, unless there is an accepted engineering reason not to do so. At any location where the public sidewalk extends over a basement or parking garage, a construction method designed to prevent differential settling must be used. Such method must be acceptable to the Transportation Department and may overrule the need for a full depth expansion joint in some situations.

Any non-standard materials, features, or vegetation in the public sidewalk shall not create a sight obstruction within any required sight triangle; shall not create a tripping, slipping, or bumping hazard in the main path of the sidewalk, including no outward door swings, door pads, or railings in the sidewalk area; and shall not create a raised fixed object in the street's clear zone. The materials and installation methods must meet typical construction requirements and must be ADA compliant.

Prior to issuance of the final engineering plans for Phase I, the developer must provide a plan sheet or sheets showing the sidewalk scoring pattern, the placement of full depth expansion

joints, placement of any non-standard materials and features in the public sidewalk, and the grade of any parts of the sidewalk, ramps, or driveway approach that differ from the general sidewalk grade.

5. Curb ramps, crosswalks and pedestrian pushbuttons: ADA-compliant curb ramps shall be installed where needed, including both crosswalks at the intersection of NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE, consistent with standard drawings TE-12, TE-13, or variations approved by the Transportation Department.

Installation of Polara-type pedestrian pushbuttons, "countdown" pedestrian signal heads, and related equipment shall be installed if needed to update the developer's end of both crosswalks at the street intersection. Plan details and specifications for any such equipment must be included in the final engineering plans, along with plans for relocation of the traffic signal pole, if necessary.

6. Irrigation and other features in planter area: Tree wells, planters, or other landscaping within any public sidewalk shall be irrigated with a metered water source. Electrical connections for lighting in tree wells or planter strips may be allowed, if installed in compliance with the electrical code and subjected to an electrical inspection. Irrigation devices and electrical components shall not create a tripping hazard in the sidewalk.
7. Driveway approach design: The driveway connections to both adjacent streets shall have concrete driveway approaches constructed as an approved variation of standard drawing DEV-6, DEV-7D, or a similar approved design. Driveway approaches shall be 30 feet wide; or, if a variation of standard drawing DEV-7D is used, the driveway approach shall be at least 26 feet wide, flaring out to at least 31 feet at the curb. Driveway approaches shall not exceed a slope of 7% for a distance of 30 feet behind the sidewalk. Any grade changes in the driveways must be designed to prevent vehicles from bottoming out.
8. Traffic signs and markings for access points: Prior to initial occupancy, the Transportation Department will determine whether a standard stop bar, stop sign, or both, or any other traffic signs or markings, shall be installed by the developer on either or both driveway approaches. Exact locations of any such features will be determined on site by Transportation Department inspectors. In some cases, the recommended position for the stop sign is on a bracket arm on the side of a column or other building structure.
9. Structure over sidewalk: Any awning, marquee, balcony, or the like over a public sidewalk easement must be at least 16 feet above the sidewalk, or be removable (with an agreement regarding removal and replacement), and must have at least three feet of horizontal clearance from any streetlight or traffic signal pole. No awning, marquee, balcony, or the like will be allowed above any city street right of way without a long-term lease of air space approved by the city council. No supports shall be installed within the public sidewalk.
10. Structure under sidewalk: No new building structure or garage shall be constructed under a street right of way or existing public sidewalk/utility easement. In some conditions (to be finalized during engineering and building plan review), new structure may be allowed under a new sidewalk/utility easement, if not in conflict with utility needs. No soil nailing is allowed under a street right of way or sidewalk/utility easement without an indemnification

agreement acceptable to the city. Temporary shoring walls may be allowed under a sidewalk / utility easement (but not under right of way) if the wall and pilings will not interfere with existing or planned utilities and if pilings under the proposed sidewalk are cut off at least eight feet below grade.

11. Vaults: No new utility vaults that serve only one development will be allowed within a public sidewalk. Vaults serving a broader public purpose may be located within a public sidewalk, but intrusion into the main walking path must be minimized. Any vaults lids in the public sidewalk must have a non-skid surface.
12. Fixed objects on street frontage: No fixed objects, including fire hydrants and other fire apparatus, trees, and streetlight poles, are allowed within ten feet of a driveway edge, defined as Point A in standard drawing DEV-6 or DEV-7DA. Fixed objects are defined as anything with breakaway characteristics stronger than a 4-inch by 4-inch wooden post. Fixed objects shall be located no closer than three feet from the face of curb on either street, except that with Transportation Department approval, some fixed objects other than streetlight poles may be located as close as 1.5 feet from the face of curb.
13. Stormwater: Treatment of stormwater flowing along a street frontage or flowing from the site onto any city street or public sidewalk shall generally meet the requirements of the Utilities Department; however, further means to divert stormwater from the sidewalk or street surface may be required if necessary to protect pedestrians and vehicles from the effects of excessive flow or ice buildup.
14. Metal covers in streets: As much as feasible, manhole lids and other metal covers shall be located outside the tire paths of through lanes on any city street and outside bicycle facilities.
15. Overhead wires: No new overhead utility lines will be allowed within or across any right of way or sidewalk easement, and existing overhead lines must be relocated underground.
16. Vehicle and pedestrian sight lines: Vehicle and pedestrian sight triangles shall be achieved per BCC 14.60.240 and 14.60.241, including consideration of all fixed objects and mature landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance. Achieving pedestrian sight distance to the west of the western driveway may require modifying an existing rockery on the frontage of the adjacent parcel. Both vehicle and pedestrian sight triangles must be shown on a sheet of the approved plans.
17. Street widening: The travel surface on 106<sup>th</sup> Avenue must be widened to achieve a width of 56 feet from face of curb to face of curb. The structural design of the widened area must be consistent with standard drawings DEV-9 and DEV-10. Any area of street widening shall have pavement depth and subgrade at least matching the existing, and in no case less than shown on standard drawing DEV-9. The cross slope of the widened area shall be determined during review of engineering plans. In designing the street cross slope, consideration will be given to AASHTO guidelines and to the existing street cross slope. If feasible, the desired cross slope will be approximately two percent from center line to flow line.

18. Street channelization: Prior to approval of the clearing and grading permit, the developer shall submit a street channelization plan for review and approval by the Transportation Department. At the appropriate time during construction, the developer shall install channelization or revise existing channelization per the approved plans.

### **Easements and Right of Way**

As part of the Binding Site Plan, the applicant shall provide sidewalk and utility easements to the City as needed to encompass the full width of any public sidewalks located outside the city right of way fronting this site on either adjacent street. The Binding Site Plan shall also include dedication of additional right of way as needed to encompass any area between the existing right of way and the back of the proposed curb that is not already within existing right of way.

Any existing utility easements within the site that will be negatively affected by this development must be relinquished or impacts must be mitigated. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.

See Section X: A, B, C, and D for related Conditions of Approval.

### **Holiday Construction & Traffic Restrictions**

From November 15<sup>th</sup> to January 5<sup>th</sup>, construction activities such as hauling and lane closures will be allowed only between the hours of 10:00 p.m. and 6:00 a.m. due to holiday traffic. The dates and times of these restrictions are subject to change. The applicant shall contact the Transportation Department Right-of-Way Section to confirm the specifics of this restriction prior to applying for a Right-of-Way Use Permit, which is issued directly by the Transportation Department. See Section X: A, B, C, and D for related Conditions of Approval.

### **Use of the Right of Way During Construction**

Applicants often request use of the right of way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right of Way Use Permit for such activities must be acquired prior to issuance of any construction permit including demolition permit. Sidewalks or vehicle lanes may not be closed except as specifically allowed by a Right of Way Use Permit. See Section X: A, B, C, and D for related Conditions of Approval.

### **Pavement Restoration**

The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration Program, every street in the City of Bellevue has been examined and placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are, "No Street Cuts Permitted", "Overlay Required", and "Standard Trench Restoration". Each category has different trench restoration requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that often comes with small strip patching.

Adjacent to the site, both NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE are presently classified as "Overlay Required". Pavement restoration for any street cuts or damage during construction will require a grind and overlay at least 50 feet long for the full width of any affected lane. Pavement restoration on a concrete street surface (NE 10<sup>th</sup>) requires replacement of entire concrete panels. The exact extent of pavement restoration will be specified in the project's right of way use permit. See Section X: A, B, C, and D for related Conditions of Approval.

### **Alternative Paving Materials**

The Transportation Department, in conjunction with other departments as appropriate, will review proposals for the installation of alternative materials by private developers. The materials and installation methods must meet typical construction requirements. If the alternative material is approved, the property owner must sign and record an indemnification agreement stating that all future maintenance and replacement is the responsibility of the property owner. Work within the alternative material area by City, franchise or other workers as a result of either emergency, normal maintenance or new installation will result in replacement of the surface by standard materials. Advance notification of such work will not be provided to the property owner. In such a circumstance, should the property owner wish to replace or repair the surface with the alternative material, a Right of Way Use Permit may be required. A subsequent approval of the alternative material is not guaranteed. Paving samples must be submitted for review by the Transportation Department and Development Services Department prior to building permit approval. Sand-set pavers are not allowed in a public sidewalk. Cobblestones or other paving stones may be allowed in a public sidewalk only if fully compliant with ADA requirements for stability, smoothness, and slip resistance. See Section X: A, B, C, and D for related Conditions of Approval.

### **Transportation Management Program**

In order to reduce single occupant vehicle trips and provide enhanced options to employees and infrastructure users, the City has adopted code provisions for a transportation management program (TMP). The owner of this development shall, prior to issuance of the primary building permit, sign and record an agreement approved by the City of Bellevue to establish a transportation management program to the extent required by BCC14.60.070 and 14.60.080. The program must be established and ready for implementation prior to initial occupancy.

Developments with more than 100 dwelling units are required to meet the minimum TMP requirements which include posting ridesharing and transit information from Metro or other approved sources in a visible central location in the building. See Section X: A, B, C, and D for related Conditions of Approval.

## **VI. STATE ENVIRONMENTAL POLICY ACT**

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the proposal. Therefore, issuance of a Determination of Non-Significance (DNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements, with incorporation by reference of the *2006-2017 Transportation Facilities Plan Final Environmental Impact Statement* (TFP EIS), dated November 2006. This document is available in the Records Office at City Hall, 450 110<sup>th</sup>

Avenue NE. Transportation-related impacts associated with this project are consistent with the potential projected impacts analyzed within this document.

Adverse impacts which are less than significant are subject to City Codes or Standards which are intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code Sec. 22.02.140 provides substantive authority to mitigate impacts disclosed through the environmental review process.

## **TRANSPORTATION**

### **Long-Term impacts and Mitigation**

The long-term impacts of development projected to occur in the City by 2017 have been addressed in the City's Transportation Facilities Plan EIS. The impacts of growth which are projected to occur within the City by 2017 are evaluated on the roadway network assuming that all the transportation improvement projects proposed in the City's current Transportation Facilities Plan are in place. The Transportation Facilities Plan EIS divides the City into several Mobility Management Areas (MMAs) for analysis purposes. The Cadillac Project site lies within MMA # 3 (Downtown). That MMA has a 2017 total growth projection of 6050 new multifamily dwelling units, 1,451,863 square feet of new retail space, and 5,207,311 square feet of new office. Approved and pending new developments have already claimed much of the projected growth. For retail, 366,320 square feet are not yet allocated to approved or pending developments. For office, approximately one million square feet are not yet allocated to new developments. However, the projection for downtown multifamily dwelling units has already been exceeded by approximately 500 units allocated to approved or pending developments.

According to the developer's traffic consultant, the Cadillac Project proposed 327 multifamily dwelling units and 14,970 square feet of retail space (total for both phases). The site previously held an automobile dealership with 31,200 square feet. For purposes of the TFP EIS, the automobile dealership counts as retail space. Thus, the development will cause a net reduction of 16,230 square feet of retail space in MMA # 3. Even though the proposed development will further exceed the TFP projection for multifamily dwelling units, the reduction in retail space for the site and the unallocated retail and office square footage for the MMA # 3 growth projections can more than account for the long-term traffic impacts associated with the Hanover Cadillac development. Therefore, the proposed development is within the traffic growth assumptions of the Transportation Facilities Plan EIS. (Note: After the traffic analysis was completed, the proposed development was reduced from 327 to 326 multifamily dwelling units and from 14,970 to 14,900 square feet of retail space. These reductions will reduce the PM peak hour trip generation by less than one trip, which will not change the conclusions of the traffic analysis). The TFP is updated every two years, at which time land use projections can be updated to meet current growth trends. With this considered, the long-term transportation impacts are fully mitigated by payment of traffic impact fees, as described below. It should also be recognized that the City of Bellevue's Downtown Implementation Plan forecasts beyond the TFP to 2020. Included with this additional forecast are additional transportation improvements, which are shown to further improve the area-wide level of service in the Downtown.

Traffic impact fees are used by the City to fund street improvement projects to alleviate traffic congestion caused by the cumulative impacts of development throughout the City. Payment of

the transportation impact fee, as required by BCC 22.16, contributes to the financing of transportation improvement projects in the current adopted Transportation Facilities Plan, and is considered to be adequate mitigation of long-term traffic impacts. Fee payment is required at the time of building permit issuance. See Section X.C.1 for a Condition of Approval regarding transportation impact fees.

### **Mid-Range Impacts and Mitigation**

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more p.m. peak hour trips undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained.

Assuming 327 multifamily dwelling units, 14,970 square feet of miscellaneous retail space, and removal of the automobile dealership, the proposed development will generate approximately 94 net new PM peak hour trips (total for both phases). That number was used to check for concurrency. City staff distributed and then assigned project-generated trips to the street network using the City's EMME-2 travel forecasting model with the current Capital Investment Program network. By adding the expected project-generated trips to the traffic volumes in the model, the area average levels of service were determined. To create a baseline condition for comparison, the levels of service were also determined using traffic volumes without the project-generated trips.

System intersections are major signalized intersections identified in the Traffic Standards Code. In this project analysis, no system intersections received 20 or more PM peak hour trips. Neither the maximum area-average levels of service nor the congestion allowances were exceeded as a result of traffic generated from this proposal. (The concurrency analysis spread sheet is available in the project file.) Therefore, the proposed development passes the concurrency test. The concurrency test results are included in the Transportation Department file for this development.

The rules of concurrency reservation are outlined in the Traffic Standards Code Director's Rules, updated May 23, 2001. A concurrency determination is issued on the date of issuance of the land use decision. This project complies with the Traffic Standards Code and is receiving a Certificate of Concurrency. See Attachment C for this certificate.

The concurrency determination is reserved to this project at the land use decision date. The concurrency reservation expires one year from the land use decision date unless a complete building permit application is filed (BCC 14.10.010.D). At the time of a complete building permit application, the Certificate of Concurrency will remain in effect for the life of the building permit application, pursuant to BCC 23.05.090H. At issuance of building permit, the Certificate of Concurrency will be extended and remain in effect for one additional year (with the possibility of up to two one-year extensions) as provided for in BCC 23.05.100.

Per BCC 14.10.020 B, the requirements of the Traffic Standard Code (including the concurrency test and Certificate of Concurrency) may be adjusted for the second phase. The Traffic Standards Code Director's Rules include the following statement: "The result of the concurrency analysis for the entire phased development will remain in effect for all buildings within the phased development so long as building applications and permits remain active. If any

Certificate of Occupancy (temporary or final) is issued for a building within a phased development when there is no building application or permit in process for a subsequent building within the phased development, the concurrency reservation for the remaining buildings will lapse.”

### **Short-Term Operational Impacts and Mitigation**

City staff analyzed the short-term operational impacts of this proposal during the PM peak hour in order to recommend mitigation if necessary. Issues that were analyzed included site access and level of service, queue length, and storage capacity at nearby intersections, based on the total impact of both phases. The analysis led to the following conclusions:

1. At project completion, nearby signalized intersections will not exceed the traffic levels of service adopted in the Traffic Standards Code. The worst nearby intersection will be at NE 8<sup>th</sup> Street / 106<sup>th</sup> Ave, which is projected to have level of service D with average delay of 50 seconds per vehicle in the PM peak hour. Traffic generated by the development will increase the overall average delay per vehicle by less than one second at that location.
2. Proposed access points were evaluated to determine whether queues extending back from nearby signals would interfere with site access and to determine whether the access points would function with an acceptable level of service and safety. No significant problems were identified.
3. Based on paragraphs 1 and 2 above, the development is not required under SEPA authority to mitigate any specific short-term problems regarding traffic operations or safety. City code requirements for street and sidewalk improvements will adequately mitigate transportation impacts on-site and on the adjacent street frontages.

The results of the short-term traffic analysis and related issues are published in the “Transportation Impact Analysis for the Cadillac Tower Project,” July 31, 2008, by Heffron Transportation, Inc. The study is included in the Transportation Department file for this development.

### **Amendments to Environmental Checklist**

The Environmental Checklist submitted with the application was revised per the revised design concept and it adequately discloses expected environmental impacts associated with the proposed project. The City codes and requirements, including the Clear and Grade Code, Utility Code, Land Use Code, Noise Ordinance, Building Code and other construction codes adequately mitigate expected environmental impacts except as noted below. The following amendments to the Environmental Checklist are needed. These impacts will be mitigated through exercise of code authority as well as through project-specific conditions of approval, contained in Section X.A.

### **NOISE**

Exterior Noise: As conditioned, short term impacts related to noise generation as a result of the construction will be minimized. Normal hours for allowed generation of noise related to

construction are from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. Exceptions to the construction noise hour limitation contained in the Noise Control Code **MAY NOT** be granted pursuant to 9.18.020C.1 & 2. However, prolonged exposure to noise created by extended hour construction activity is likely to have a significant impact on inhabitants of surrounding commercial and residential properties during the proposed timeline for construction. If expanded hours are necessary to accommodate a specific component of construction, a noise permit shall be required and must be submitted in advance of the scheduled onset of extended hour construction activity. In accordance with the Bellevue City Code – BCC 9.10 – Noise Control, the City will only be providing construction noise exemptions for the following work:

- Work in the ROW and essential public facilities (i.e., Water connections that require a main shut off and are required to be done at night by the Utility Dept.)
- Work to accommodate transportation mitigation
- Required evening haul routes

Work that has been previously determined by sound level monitoring is not to exceed the maximum permissible noise levels. Utility/site work on private projects/property is not essential public facilities. See Section X.A for related condition.

Interior Noise Levels: The Bellevue City Code, BCC 9.18, limits interior noise levels within residential structures to 40 dBA in sleeping areas and 45 dBA in non-sleeping areas. Special construction is generally necessary to meet these thresholds. Prior to the issuance of any occupancy permits, the applicant shall verify that this threshold has been met.

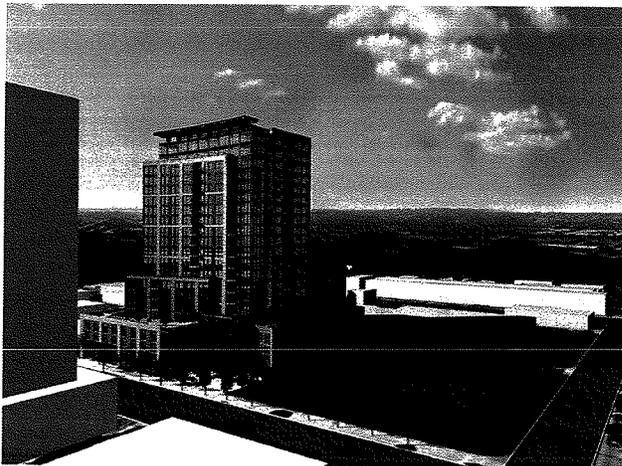
## **VII. CHANGES TO PROPOSAL DUE TO CITY REVIEW**

### **Building Design**

- The roof for Phases I and II were architecturally redesigned to fulfill the requirements of LUC 20.25A.110. Specifically, the south elevation was redesigned to increase the height of the iconic brow to add architectural interest to these elevations. Two revisions were reviewed by staff with the final revision being accepted. The proposal now complies with the intent of this section and qualifies for extra height.

### **Site Design**

- A pedestrian shelter was required along NE 10<sup>th</sup> Street to add back of sidewalk weather protection. This shelter will be installed with Phase II adjacent to a landscape planter. Further design details for this shelter shall be documented prior to building permit issuance of Phase II. See Section X.C for related conditions.



An interim landscape plan was submitted for Phase II to provide vegetative cover between phases. This interim plan will be required to be installed prior to Certificate of Occupancy for Phase I. See Section X.C for related condition. Trees planted under this interim plan will be removed for Phase II development and relocated to a temporary area for storage. Upon completion of Phase II, these trees will be brought back to the site for re-installation per landscape plan for Phase II.

- The applicant added a Binding Site Plan to their Design Review to divide the site into two lots. The applicant was later requested to reconcile the lot dimensions between the Design Review and Binding Site Plan along with several modifications to the Binding Site Plan by the Survey, Transportation and Land Use Departments. Of primary concern is the establishment of easements with this site.
- Landscaping strips on NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE were modified in certain locations to account for street lights, fire hydrants, etc. along these frontages.

#### VIII. APPLICABLE DESIGN REVIEW DECISION CRITERIA

The Director may approve, or approve with modifications, an application for Design Review if:

1. **The proposal is consistent with the Comprehensive Plan.**

**Following are land use policies from the Urban Design Element of the City's Comprehensive Plan that are pertinent to, and supportive of, the project proposal:**

***Policy UD-1:*** Encourage high-quality, attractive, architecturally appealing designs for major buildings in order to create distinctive visual reference points in the community. This project will create an architecturally attractive building in its massing, materials, and detail. The siting of the Phase II tower towards the corner of NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE enables it to become a distinctive visual reference point for the City Center North District.

***Policy UD-2:*** Support Designs for the built environment that are visually stimulating and thoughtful and which convey excellence in architecture and workmanship, and durability in building materials. This project proposes high quality durable materials, including light and medium-sandblasted pre-cast concrete panels, aluminum window wall and curtain wall, metal panels, as well as pavement materials.. The building design incorporates careful attention to detail, variety in building heights and elements, integration of landscape, hardscape and building forms to create a visually stimulating, cohesive project.

**Policy UD-3:** Encourage a variety of site and building designs which are compatible and consistent with surrounding development and that implement the policies of this plan.

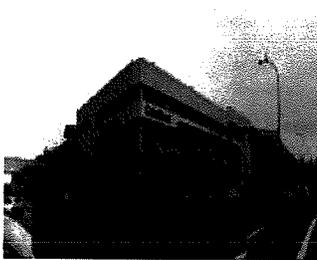
**Policy UD-4:** Ensure that the development relates, connects, and continues design quality and site functions from site to site. This project will be compatible with the high quality high rise and mid rise residential development already under construction along NE 10<sup>th</sup> Street to the east, as well as the lower rise retail development along Bellevue Way to the west and south of this project. This proposal includes three different scales of development from the roof top garden on Level 4 of Phase I and Level 3 of Phase II to the balcony at Floor 8 and upper level decks on Floors 19 of Phase I and II; and Floor 20 of Phase I.



HYATT HOTEL



LINCOLN SQUARE



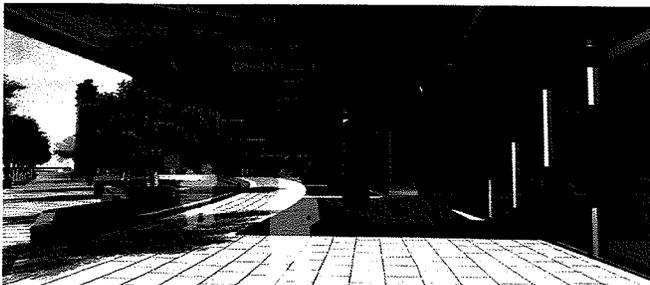
BELLEVUE SQUARE



ART MUSEUM

**Policy UD-6:** Design buildings located on the edge of public spaces using materials, forms, details and other architectural elements that will enrich the appearance of the places and encourage people to use them. The south side of the buildings which face NE 10<sup>th</sup> Street incorporates façade articulation, overhead weather protection for retail activities, and pedestrian-scaled lighting to create a low-scale and inviting environment. The sidewalks on NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE will contain 16 foot sidewalks with 4 foot planter strips to separate vehicular traffic from pedestrians for comfortable mobility. The required mid-block connections are located along the service-oriented sides of the project at its north and west property boundaries.

**Policy UD-8:** Design rooftop mechanical screening so that it is integral with building architecture. Consider the visual effects of technical advances such as satellite dishes, on building design. All roof-top mechanical equipment will be enclosed within a penthouse that has been designed in integration with the building design. See discussion above in Section III.B



**Policy UD-11:** Encourage architectural elements that provide for both rain cover and access to sunlight in pedestrian areas.

Transparent/ translucent canopies will be provided on NE 10<sup>th</sup> Street at back of sidewalk at the southwest corner of the building with Phase I. For Phase II, canopies will be provided along 106<sup>th</sup>

Avenue NE at back of sidewalk; however, north of the urban plaza canopies will not be at back of sidewalk due to the recess of the building (see above image). To compensate for the lack of weather protection at the back of sidewalk, the applicant will provide a respite shelter above the

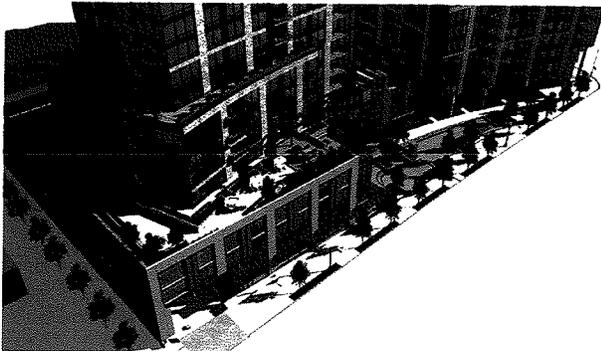
seatwall at back of sidewalk for Phase II. A draft proposal for this shelter has been submitted for review. This design will be required to be complete prior to issuance of a building permit. See Section X.C for related condition.

**Policy UD-14:** *Encourage seasonal color plantings in public and semi-public areas.*

**Policy UD-15:** *Use landscape design that is urban in character in more urban settings*

**Policy UD-19:** *Preserve trees as a component of the skyline to retain the image of a "City in the Park."*

The landscape design incorporates both hardscape and landscape to respond to the urban setting, and includes street trees along all public and private streets. The existing street trees will be removed with this application as they are in very poor condition due to past tree topping by the previous owner.



**Policy UD-20:** *Preserve and encourage open space as a dominant element of the community's character.*

A terraced, urban plaza is proposed for the southern portion of the site. It will offer seatwalls framed by lush vegetated planters with interesting pavement materials. The plaza's placement in this vicinity will create a focal point for the intersection of NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE. It could also be

an area of congregation after hours if the PACE facility is developed as scheduled at the southwest corner of this site.

**This proposal is also consistent with the following policies from the Downtown Sub-area of the Comprehensive Plan:**

**POLICY S-DT-1** *promotes Downtown livability, with provisions made for the needs, activities, and interests of Downtown residents, employees, shoppers, and visitors.* The retail will provide additional retail activities for area residents.

**POLICY S-DT-2** *encourages a variety of land uses to occur in mixed-use buildings or complexes where appropriate.* This is a mixed use building with retail at the base with residential above.

**POLICY S-DT-3** *promotes development of Downtown as an aesthetically attractive area.* This proposal will add to the aesthetic environment of the Downtown.

**POLICY S-DT-4** *states that the highest intensity development shall be located in the core of Downtown, with diminishing intensities towards the edges of Downtown.* By its location, this site lies outside the Downtown core within Subdistrict C which has special dimensional standards for the perimeter district.

**POLICY S-DT-5** *sets out to organize Downtown to provide complementary functional relationships between various land uses.* This proposal will complement the mixed use environment of Downtown-Mixed Use district by adding to the retail/residential base.

**POLICY S-DT-7** *encourages the Downtown to continue to serve surrounding residential areas as a neighborhood retail district.* Residents of this project will be served by the higher intensity retail/service/entertainment uses within the Downtown.

**POLICY S-DT-9** promotes use of bonus incentives (related to permitted intensity, height, etc.) for private developments to accomplish the public objectives. See Section III.B above for discussion on how this project qualifies for the bonus incentive for height.

**POLICY S-DT-24** promotes density incentives to encourage urban residential development throughout Downtown. See Section III.A above for review of project density.

**POLICY S-DT-25** supports for a range of Downtown urban residential types and densities. The proposal will increase residential housing in downtown by 326 units.

**POLICY S-DT-26** encourages residential uses to occur in mixed-use structures or complexes. The proposal will contain both residential and retail uses. Residential amenities will also be available in the form of athletic facilities, outdoor decks, etc.

**POLICY S-DT-36** seeks to utilize development standards for building bulk, heights, setbacks, landscaping requirements, stepbacks, floor area ratios, open space requirements, and development incentives. See Section III.A to review dimensional standards.

**POLICY S-DT-37** supports linking building intensity to design guidelines relating to building appearance, amenities, pedestrian orientation and connections, impact on adjacent properties, and maintenance of view corridors. These guidelines will seek to enhance the appearance, image, and design character of the Downtown. See Section III.C above for project attributes.

**POLICY S-DT-40** seeks to enhance the appearance of all types of streets and adjoining sidewalks with street trees, landscaping, water features, pedestrian-scaled lighting, street furniture, paving treatments, medians, or other softening treatments as appropriate. The project complies with this standard. See Sheet L1-00 for details.

**POLICY S-DT-57** promotes the creation of pedestrian linkages within and between the Downtown Districts as well as to surrounding residential areas outside Downtown. The redevelopment of the surrounding sidewalks will provide connections to PACE and adjacent residential uses through required pedestrian corridors.

**POLICY S-DT-65** seeks to encourage the development of high-rise housing along NE 10th Street within this City Center North district. This proposal is located in the City Center North district so by its location, will increase the supply of high rise housing along NE 10<sup>th</sup> Street.

**POLICY S-DT-67** seeks development of a soft or hard open space amenity in the vicinity of 106<sup>th</sup> Avenue NE and NE 10<sup>th</sup> Street within the City Center North District. An urban plaza is proposed at the south side of this building to create a gathering space for pedestrians. Additionally, this could become a focal point if PACE is constructed at the southwest corner of this intersection.

## **2. The proposal complies with the applicable requirements of this Code.**

As conditioned, the proposal complies with applicable requirements of the Land Use Code and Bellevue City Code with exception of the items detailed below. Additionally, see Section III.A above for further Code discussion.

- More information was requested regarding building and site lighting. The lighting strategy of the supplemental information provides a very good overview of site lighting. The applicant will need to provide more information regarding actual fixtures for the building and site use. See Section X.C for related condition.
- More information was requested regarding exhaust noise. The applicant has hired an acoustical engineer who is drafting a report. Review and approval of such report shall occur prior to building permit issuance to ensure proper dba of the proposed equipment. See Section X.C for related condition.

- More information is necessary regarding proposed ventilation and air vents as they have not been finalized for this project. See Section X.C for related condition.
- The applicant was requested to provide general information on where project signage will be portrayed per building elevation. See Sections X.C and E for related condition.
- An east/west midblock connection is required along the north portion of the site. The applicant is providing this connection. However, future development will occur to the west and continuous connection is required to Bellevue Way. This applicant is required to provide site access for the future developer to the west to address grade issues for such items as stairs, etc. to continue this connection. This applicant, however, shall not be required to construct and/or pay for grade change devices such as stairs or ramps. Such devices shall be constructed and paid for by the future developer, and shall be constructed entirely on the future developer's site. See Section X.C for related condition.

**3. The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent.**

See Section III.C.2 of this report for discussion relating to how the applicable design guidelines and perimeter criteria are met.

**4. The proposal is compatible with, and responds to, the existing or intended character, appearance, and quality of development and physical characteristics of the subject property and immediate vicinity.**

The project is designed to respond to existing site conditions and existing surrounding development, as well as development that are in planning and initial stages of construction along NE 10<sup>th</sup> Street to the east.

Unlike many of the building designs within the downtown, Phase I contains a podium while Phase II structure will sit flush to the ground above the parking garage with the towers offset by 80 feet. Both tower elements contain stepbacks and recesses to create interesting super structures that culminate in well defined iconic elements at the roof. The design intent is to be compatible with the nearby towers to the east including BRE, Vue Hanover, Ten20 Tower, and Washington Square, but doesn't mimic their design features. Additionally, the building's prominent location at the intersection with 106<sup>th</sup> Avenue NE is reflected in the placement of the urban plaza to correspond to entertainment activities on this street.

The proposal provides residents with many amenities: Level 3 with an amenity deck for Phase II and Level 4 amenity deck for Phase I. Both decks will contain fire places, outdoor seating, vegetation, pet relief areas, arbors, concrete pavers, lush vegetation in planters, etc. Units located above these areas will have views that will contain vegetative relief areas to contrast to the building massing.

Finally, the street trees proposed will be consistent with the type, pattern and size required by the Land Use Code. However, a condition of approval is necessary to ensure that required landscaping is installed correctly along with a maintenance assurance device. See Section X.D for related condition.

**5. The proposal will be served by adequate public facilities including streets, fire protection, and utilities.**

All required public services and facilities are available to the site. The applicant has received an email from Allied dated February 11, 2008, stating that they see no service issues for this project. The owner will provide for the return of trash and recycling receptacles removed from the property back into the building the day of pick-up.

**IX. DECISION**

After conducting the various administrative reviews associated with the proposal, including applicable Land Use consistency, SEPA and City Code & Standard compliance reviews, the Director of Development Services Department does hereby **APPROVE WITH CONDITIONS** the subject proposal.

**X. CONDITIONS OF APPROVAL**

The following conditions are imposed under the authority referenced:

**A. GENERAL CONDITIONS**

**1. COMPLIANCE WITH BELLEVUE CITY CODES AND ORDINANCES:** The applicant shall comply with all applicable Bellevue City Codes and Ordinances including but not limited to:

Clearing and Grading Code - BCC 23.76	Janney Gwo	425/452-6190
Bellevue Development Standards	"	
Transportation Code - BCC 14.60	Carl Wilson	425/452-4228
Trans. Improvement Program - BCC.22.16	"	
Right-of-Way Use Permit - BCC 14.30	Jon Regalia	425/425-4599
Bellevue Utilities Code - BCC Title 24	Brad Ayers	425/452-6054
Construction Codes - BCC Title 23	Behrooz Khorrami	425/452-6143
Land Use Code - BCC Title 20	Antoinette Pratt	425/452-5374
Sign Code - BCC Title 22B	"	
Noise Control - BCC 9.18	"	
Uniform Fire Code - BCC 23.11	Adrian Jones	425/452-6032

**2. Conceptual Approval:** Utility Department approval of the design review application (08-112057 LD) is based on the conceptual design only. Changes to the site layout may be required to accommodate the utilities after utility engineering is approved.

Authority: BCC Title 24.02, 24.04, 24.06  
Reviewer: Brad Ayers

**3. City Code and Standards:** The water, sewer, and storm drainage systems shall be designed per the current City of Bellevue Utility Codes and Utility Engineering Standards. All design review, plan approval, and field inspection shall be performed under the Utility Developer Extension Agreements.

Authority: BCC Title 24.02, 24.04, 24.06  
Reviewer: Brad Ayers

4. **On-Site Storm:** The existing on-site storm conveyance line bisecting the property must be relocated and operational prior to site construction.

Authority: BCC Title 24.06.120  
Reviewer: Brad Ayers

5. **Construction Hours:** Noise related to construction is allowed from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. Construction noise is prohibited on all Sundays and WA state holidays. Posting of construction hours is required. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020C.1&2 when necessary to accommodate construction which cannot be undertaken during exempt hours. Prolonged exposure to noise created by extended hour construction activity is likely to have a significant impact on inhabitants of surrounding residential properties during the proposed timeline for construction. In order to minimize detriment on residential uses in the immediate vicinity of the project, the Contractor shall not rely on City issuance of a blanket exemption from the Noise Control Code during the construction period. To vary construction noise hours, the applicant shall apply for a noise permit that must be submitted two weeks prior to the scheduled onset of extended hour construction activity. Such requests may be required to include a noise analysis prepared by a noise consultant, including recommendations for achieving the noise limitations of the Noise Ordinance for new residential construction.

Authority: BCC 9.18.040  
Reviewer: Antoinette Pratt

6. **Holiday Construction & Traffic Restrictions:** Construction activities such as hauling and lane closures between November 15<sup>th</sup> and January 5<sup>th</sup> will be allowed only between the hours of 10:00 pm and 6:00 am due to holiday traffic. The Transportation Department will be monitoring traffic and may modify this moratorium accordingly.

Authority: BCC 14.30.060  
Reviewer: Jon Regalia

7. **Vehicular Access Restrictions:** Access to this site from NE 10<sup>th</sup> Street will continue to be restricted to right-turn-in and right-turn-out only. Left turns to or from 106<sup>th</sup> Avenue NE may be restricted by the city in the future, if necessary for operational or safety reasons, or as part of future transportation projects.

Authority: BCC 14.60.150  
Reviewer: Carl Wilson

8. **Provisions for Loading:** The property owner shall provide an off-street truck loading space accessed from the private road within the site. After construction is complete, no loading or unloading will be permitted on any public street, unless in compliance with a loading zone that may be established, with Transportation Department approval, on 106<sup>th</sup> Avenue NE.

Authority: LUC 20.20.590.K.4, BCC 14.60.150  
Reviewer: Carl Wilson

9. **Project Phasing, Binding Site Plan, and Dedications:** Any street right of way or sidewalk/utility easement dedication(s) needed for either phase must be dedicated as part of the Binding Site Plan no later than the time of Phase I building permit approval. All public transportation infrastructure for both phases, including any temporary transition features, must be designed prior to Phase I approval. Public transportation infrastructure for Phase I must be constructed prior to occupancy of Phase I, along with appropriate transitions to infrastructure that will temporarily remain on Phase II. Acceptable pedestrian facilities must be provided on both adjacent streets during all phases, although some pedestrian facilities for Phase II may be temporary during Phase I, with city approval. The design of a new driveway connection from Phase I across Phase II to 106<sup>th</sup> Avenue NE must be acceptable to the city prior to approval of Phase I. Prior to occupancy of Phase I, an acceptable financial assurance device must be provided to guarantee the construction of Phase II transportation infrastructure on an agreed schedule not to exceed 24 months beyond initial occupancy of Phase I.

The Binding Site Plan must include dedication of a standard sidewalk/utility easement covering all public sidewalk area that is not already within a sidewalk/utility easement or street right of way within the first sixteen feet behind the back of the future curb, as described in the section entitled Summary of Technical Reviews, Project Phasing and Binding Site Plan.

The Binding Site Plan must include dedication of additional right of way as needed to encompass any area between the existing right of way and the back of the proposed curb.

Authority: LUC 20.30F.165; BCC 14.60.100, 110, and 181  
Reviewer: Carl Wilson

- B. **The following conditions are imposed to ensure compliance with the relevant decision criteria and Code requirements and to mitigate adverse environmental impacts not addressed through applicable Code provisions. These conditions must be complied with on plans submitted with the Clearing & Grading or Demolition permit application:**
1. **Right-of-Way-Use Permit:** Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:
- a) Designated truck hauling routes.
  - b) Truck loading/unloading activities.
  - c) Location of construction fences.
  - d) Hours of construction and hauling.
  - e) Requirements for leasing of right of way or pedestrian easements.
  - f) Provisions for street sweeping, excavation and construction.
  - g) Location of construction signing and pedestrian detour routes.

- h) All other construction activities as they affect the public street system.
- i) Sufficient off-street parking for construction workers.

In addition, the applicant shall submit for review and approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevent access. General materials storage and contractor convenience are not reasons for preventing access.

**The applicant shall secure sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.**

Authority: BCC 11.70 & 14.30  
Reviewer: Jon Regalia

2. **Civil Engineering Plans - Transportation:** Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all street frontage improvements and driveway accesses must be in conformance with the requirements of the Americans with Disabilities Act, the Transportation Development Code, the provisions of the Transportation Department Design Manual, and specific requirements stated elsewhere in this document. All relevant standard drawings from the Transportation Department Design Manual shall be copied exactly into the final engineering plans. Specific requirements for the engineering plans include, but are not limited to:
- a) Traffic signs and markings in access points.
  - b) Curb, gutter, sidewalk, and driveway approach design. At least one plan sheet must show sidewalk scoring, joints, relevant grades, and non-standard materials or features within the sidewalk. The sidewalk design must include the full sixteen-foot width specified above in the section entitled Summary of Technical Reviews, Project Phasing and Binding Site Plan. (The engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as needed.)
  - c) Handicapped ramps, pedestrian pushbuttons, and crosswalk revisions, as needed.
  - d) Street widening and channelization.
  - e) Installation or relocation of streetlights, traffic signals, and related equipment. Street trees shall also be shown, with acceptable separation from streetlights and signals.
  - f) Sight distance. (Show the required vehicle and pedestrian sight triangles and include any relevant sight obstructions, including those off-site.)
  - g) Location of fixed objects in the sidewalk or near the driveway approach.
  - h) Trench restoration within any right of way or access easement.
  - i) Stormwater treatment affecting streets and public sidewalks.
  - j) The engineering plans must show how to accommodate project phasing, including any temporary improvements needed due to phasing.
  - k) If approved standards for transportation infrastructure change between approval of Phase I and the beginning of construction for Phase II, then engineering plan

revisions may be necessary for Phase II in order to comply with standards at that time.

Authority: BCC 14.60; Transportation Department Design Manual  
Reviewer: Carl Wilson

**C. The following conditions are required by City Code. These conditions must be complied with prior to Building Permit Issuance:**

- 1. Transportation Impact Fee:** Payment of the transportation impact fee will be required at the time of building permit issuance for the above-ground structure for each phase. Based on present rates, the impact fee would be \$151.69 for each multifamily dwelling unit and \$0.44 per square foot for miscellaneous retail space. Other types of retail, including restaurants, have different fees. The type of use will be based on the latest information at the time of building permit issuance. These fees are subject to change, and the fee schedule in effect at building permit issuance for each phase will apply.

Authority: BCC 22.16  
Reviewer: Carl Wilson

- 2. Building and Site Plans - Transportation:** The building grade and elevations shall be consistent with the curb and sidewalk grade shown in the approved civil engineering plans, and the final building plans shall document same, as described under Street Frontage Improvements, Section 3, above. During construction, the procedures described under Street Frontage Improvements, Section 3, shall be followed to ensure consistency between required sidewalk elevations and the elevations of key parts of the building. City inspectors may require additional survey work at any time in order to confirm proper elevations. Building plans, landscaping plans, and architectural site plans must accommodate on-site traffic markings and signs and driveway design as specified in the engineering plans. Building plans, landscaping plans, and architectural site plans must comply with vehicle and pedestrian sight distance requirements, as shown on the engineering plans. The building plans must show how to accommodate project phasing, including any temporary improvements needed due to phasing. The building plans must accommodate the full sixteen-foot sidewalk width described above in the section entitled Summary of Technical Reviews, Project Phasing and Binding Site Plan.

Authority: BCC 14.60.060, 110, 120, 150, 180, 181, 190, 240, 241  
Reviewer: Carl Wilson

- 3. Existing Utility Easements:** The applicant shall identify existing utility easements on the site. Any negative impact that this development has on those easements must be mitigated or easements relinquished.

Authority: BCC 14.60.100  
Reviewer: Jon Regalia

- 4. Transportation Management Program:** The owner of the property being developed shall sign and record at the King County Office of Records and Elections an agreement

to establish a Transportation Management Program to the extent required by Sections 14.60.070 and 14.60.080.

Authority: BCC 14.60.070, 14.60.080  
Reviewer: Carl Wilson

- 5. Binding Site Plan (BSP):** The applicant shall be required to file the submitted Binding Site Plan with King County Records and Elections prior to issuance of a building permit for Phase I of this project.

Authority: RCW 58.17.035, Alternative Land Division  
Reviewer: Antoinette Pratt

- 6. Solid Waste/Recycling:** The applicant/owner shall provide for the return of receptacles and trash not removed from the property back into the building the day of pick-up; all rights of way and public easements shall not be occupied by trash receptacles, dumpsters, recycling bins or other such items.

Authority: LUC 20.20.720  
Reviewer: Antoinette Pratt

- 7. Garage Exhaust Noise:** The garage exhaust fans can create noise levels at the public sidewalk which exceed that allowed by the Bellevue City Code. This decision is conditioned to require certification by the project noise consultant that the garage exhaust fan noise will not exceed 60 dBA at the public sidewalk prior to the issuance of any mechanical permits.

Authority: BCC 9.18.045  
Reviewer: Antoinette Pratt

- 8. Exterior and Retail Lighting:** The applicant shall submit a detailed design for all building exterior lighting. Lighting shall be contained to this site and not be permitted to spillover to adjacent sites. In addition to this, information on retail lighting along the pedestrian oriented frontage (NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE) shall be supplied as well to determine appropriate use for façade.

Authority: LUC 20.20.522, LUC 20.25A.110.5.a  
Reviewer: Antoinette Pratt

- 9. Extended Vesting to the LUC:** Extended vesting has been granted to this project for 10 years from the date of this approval. Vesting to the LUC does not vest the project to ancillary codes such as Building, Fire, Utilities, etc. The project shall comply with the codes in effect at the time of permit issuance for Phase I and Phase II.

Authority: LUC 20.25A.125  
Reviewer: Antoinette Pratt

- 10. Right of Access Agreement:** 1001 Properties, LLC or subsequent owner of this site shall provide site access for the future developer to the west to address grade issues for

such items as stairs, etc. to continue this connection. The applicant shall not be required to construct and/or pay for grade change devices such as stairs or ramps. Such devices shall be constructed and paid for by the future developer, and shall be constructed entirely on the future developer's site.

Authority: LUC 20.25A.060.C.3  
Reviewer: Antoinette Pratt

**11. Fire and Life Safety:** The following fire and life safety conditions must be addressed on the plans submitted with the building permit application. Authority is provided for each:

- The Fire Command Center for each building shall have direct exterior access at an approved location. (International Fire Code 509)
- Fire Department Connections (FDC) shall be at two approved locations for each building. (IFC 912)
- Provide a fire hydrant within 50 feet of each FDC. (Bellevue Fire Department Development Standards (BFDDS))
- Fire pits are noted on the elevated courtyards of the building. Show compliance with IFC section 307 for "Open Burning and Recreational Fires" .
- Provide an on-site water supply with 65,000 gallons of "Net Usable volume" and fire pump for the fire sprinklers system for each building. (IFC 903)
- Provide information on the location of the generator, size of fuel tank, tank venting and refueling station location for each building. (IFC Chapter 604, 27 & 34)
- The generator room for the east tower appears to be to far below grade. The maximum vertical distance from the bottom of the fuel tank to the top of the vent pipes (minimum 12 feet above adjacent ground) shall be 20 feet. (IMC 1305.7)
- Relocate the Fire Pump Room for phase I within easy access to a stairway for access during an incident. (IFC 913 & 2007 NFPA 20 section 5.12.1)
- Transformer rooms shall have exhaust directly to the exterior of the building without dampers (IMC 402).
- Provide access to the roof in both buildings. (IFC 1009.11)
- All areas of each floor, including the courtyards & decks shall be within 200 feet of a 2 1/2 inch hose station. (IFC 905)

**12. Phase II—Weather Protection:** A pedestrian shelter shall be installed with Phase II adjacent to a landscape planter. Further information/illustrations shall be provided prior to issuance of building permits for phase II.

Authority: LUC 20.25A.110.4.C.iv  
Reviewer: Antoinette Pratt

13. **Mechanical Equipment/Ventilation:** The current mechanical screen height is at 244'-7" for Phase I and 243'-6" for Phase II. The maximum height of 245 feet may not be exceeded for any reason within this application and approval. No changes shall be permitted to the shown roof details for this project without consent of staff during building permit review. Additional height is based on the details provided herein. Modification of the building design may place the additional height gained at risk for reduction to normal maximum building height of 200 feet. Additionally, all venting shall be designed so that it does not detract from the architectural integrity of the building for staff review prior to building permit issuance.

Authority: LUC 20.20.525  
Reviewer: Antoinette Pratt

- D. **The following conditions are required by City Code and supported by City Policy. The conditions shall be complied with prior to issuance of the Temporary Certificate of Occupancy (TCO):**

1. **Bonus System Recording:** The applicant shall record a copy of the approved bonus point calculations, a statement that 100% of the project frontage shall remain pedestrian-oriented retail, and the project drawings & conditions of this Design Review with the King County Records office.

Authority: LUC 20.25A.020.D.3  
Reviewer: Antoinette Pratt

2. **Landscape Installation Assurance Device:** If a Temporary Certificate of Occupancy is requested prior to completion of the landscaping installation, the applicant shall file with PCD a landscape installation assurance device equal to 150% of the cost of labor and materials for any landscaping that has not yet been installed.

Authority: LUC 20.40.490  
Reviewer: Antoinette Pratt

3. **Landscape Maintenance Device:** The applicant shall file with the Development Services Department a landscape maintenance assurance device for a one-year period from the date of final inspection in the form of an assignment of savings or letter of credit for 20% of the cost of labor and materials for all required landscaping.

Authority: LUC 20.40.490  
Reviewer: Antoinette Pratt

4. **Interim Phasing Plan:** An interim landscape plan was submitted for Phase II to provide vegetative cover between phases. This interim plan shall be required to be installed prior to Certificate of Occupancy for phase I.

Authority: LUC 20.20.520.J  
Reviewer: Antoinette Pratt

5. **Public Access:** 24-hour public vehicular and pedestrian access shall be accommodated along the north and west mid-block connections. In addition, public access signs located prominently and visible to pedestrians from the public right-of-way shall be posted at every point where the public access easement intersects with the public right-of-way. A written agreement that meets the satisfaction of the Development Services Department Director shall be recorded with the King County Records office.

Authority: BCC 22B and LUC 20.25B.040.G  
Reviewer: Antoinette Pratt

6. **Interior Noise:** The Bellevue City Code, BCC 9.18, limits interior noise levels within residential structures to 40 dBA in sleeping areas and 45 dBA in non-sleeping areas. Special construction is generally necessary to meet these thresholds. Prior to the issuance of any occupancy permits, the applicant shall verify that this threshold has been met.

Authority: BCC 9.18  
Reviewer: Antoinette Pratt

7. **Street Frontage Improvements and Phasing:** All street frontage improvements and other required transportation elements, including streetlights and landscaping, must be constructed by the applicant and accepted by the Transportation Department Inspector for each phase. Any Certificate of Occupancy for Phase I must include completion of features needed at that time to accommodate connections to Phase II. Bonding or other types of assurance devices will not be accepted in lieu of construction for each phase: however, prior to occupancy of Phase I, an acceptable financial assurance device must be provided to guarantee the construction of Phase II transportation infrastructure on an agreed schedule not to exceed 24 months beyond initial occupancy of Phase I. Specific requirements include the following:

- a) Curb, gutter, sidewalk, ramps, crosswalks, driveway approaches, and street improvements shall be constructed per the approved engineering plans, as needed for each phase, including any features needed to connect Phase I to or across Phase II.
- b) Any non-standard materials or features in the public sidewalk must be acceptable to the City.
- c) Driveway approaches shall not exceed a slope of 7% for a distance of 30 feet behind the sidewalk on either street. Any grade changes in the driveways must be designed to prevent vehicles from bottoming out.
- d) Traffic signs and markings for the access points and for street channelization shall be installed per the approved engineering plans for each phase, with possible modifications per Transportation Department inspectors.
- e) Vehicle and pedestrian sight triangles shall be achieved per BCC 14.60.240 and 14.60.241, including consideration of all fixed objects and mature landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance.
- f) Streetlights, traffic signals, pedestrian pushbuttons, and related equipment shall be installed or relocated per the approved streetlight and signal plan and specifications, for the relevant project phase, with street trees spaced per Transportation Department requirements for separation from streetlights and signal poles.

- g) Location of fixed objects within any right of way or near the driveway approaches shall meet Transportation Department standards per the Transportation Department Design Manual and as described elsewhere in this Design Review Staff Report.
- h) Landscaping and irrigation within any city right of way or sidewalk easement shall be installed per the approved landscaping plans for each phase.
- i) Transformers and utility vaults to serve the building shall be placed inside the site to the extent feasible or below grade.
- j) Stormwater treatment affecting streets and public sidewalks shall be installed per approved engineering plans.

Authority: BCC 14.60.100, 110, 120, 150, 180, 181, 190, 210, 230, 240, 241;  
Transportation Department Design Manual and Standard Drawings; and LUC  
20.25A.060 and 20.40.490.

Reviewer: Carl Wilson

- 8. Pavement Restoration:** Pavement restoration associated with street frontage improvements, utility installation, or to repair damaged street surfaces shall be provided as follows:

Adjacent to the site, NE 10<sup>th</sup> Street and 106<sup>th</sup> Avenue NE are both presently classified as "Overlay required". Pavement restoration for any streets cuts or damage during construction will require a grind and overlay at least 50 feet long for the full width of any affected lane. Pavement restoration on a concrete street surface (NE 10<sup>th</sup> Street) requires replacement of entire concrete panels. The exact extent of pavement restoration will be specified in the project's right of way use permit, and may be modified in the field by the Transportation Department inspector.

Authority: BCC 14.60. 250; Design Manual Design Standard #21

Reviewer: Jon Regalia

- 9. Implement Frontage Improvements and Phasing:** The Transportation Management Program required by Bellevue City Code Sections 14.60.070 and 14.60.080 per a condition of approval above must be functional prior to issuance of the initial certificate of occupancy.

Authority: BCC 14.60.070, 14.60.080

Reviewer: Carl Wilson

## **E. PRIOR TO ISSUANCE OF SIGN PERMITS**

- 1. Signs:** All signage shall be pedestrian oriented. A comprehensive sign package shall be submitted to DSD for review and approval prior to issuance of any sign permits. The sign package plans, elevations, and other sketches shall include but are not limited to:

- A. Location
- B. Lighting
- C. Color Palette
- D. Material
- E. Design (No cabinet or can signs are permitted)

Signage shall be the minimum size necessary to convey information and shall be architecturally compatible and integrated with the building. See Sign Code limitations or other design criteria. Design review of individual signs and compliance with the approved sign package will occur through review of each sign permit application.

Authority: LUC 20.25A.110, LUC 20.30F, BCC 22B  
Reviewer: Antoinette Pratt

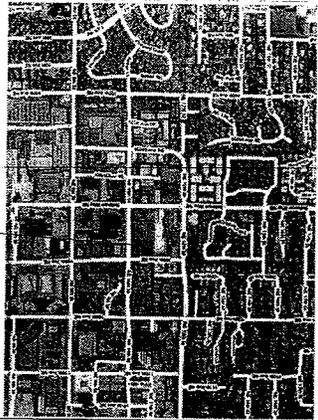
#### **ATTACHMENTS**

- A. Project Plans and Drawings
- B. Binding Site Plan
- C. Certificate of Concurrency

**ATTACHMENT A**  
**(Plans and Drawings)**

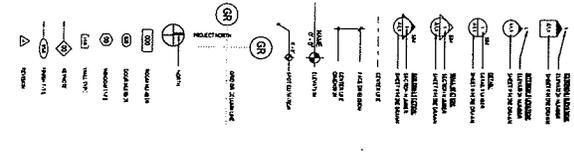
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VICINITY MAP



PROJECT LOCATION

ARCHITECTURAL LEGEND



**AREA ANALYSIS**  
**DEVELOPMENT SUMMARY PHASE I**  
**LANDSCAPE SERVICES**  
**PRE-EXISTING REQUIREMENTS**

**LAND USE/CODE ANALYSIS**  
**DEVELOPMENT SUMMARY PHASE I**  
**DEVELOPMENT SUMMARY PHASE II**

**PHASE I & II ANALYSIS**

USE	AREA (SQ FT)	PERCENTAGE	MAXIMUM HEIGHT (FT)	MAXIMUM FLOOR AREA (SQ FT)	MAXIMUM LOT COVERAGE (%)	MAXIMUM LOT AREA (SQ FT)	MAXIMUM LOT WIDTH (FT)	MAXIMUM LOT DEPTH (FT)
RESIDENTIAL	100,000	40%	35	100,000	40%	100,000	100	100
COMMERCIAL	150,000	60%	45	150,000	60%	150,000	150	150
PARKING	50,000	20%	15	50,000	20%	50,000	50	50
LANDSCAPE	10,000	4%	10	10,000	4%	10,000	10	10
TOTAL	250,000	100%		250,000	100%	250,000	250	250

PROJECT TITLE

**Geographic Grid Calculations Phase I**

Block	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50
Phase I	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50

DRAWING INDEX

**PHASE I 10600 NE 10TH STREET BELLEVUE, WA 98004**  
**(FUTURE) PHASE II 10600 NE 10TH STREET BELLEVUE, WA 98004**

**PROJECT TEAM**

**PROJECT DATA**

**APPLICABLE CODES & STANDARDS**

**NOT FOR CONSTRUCTION**

**PHASE I & II ANALYSIS**

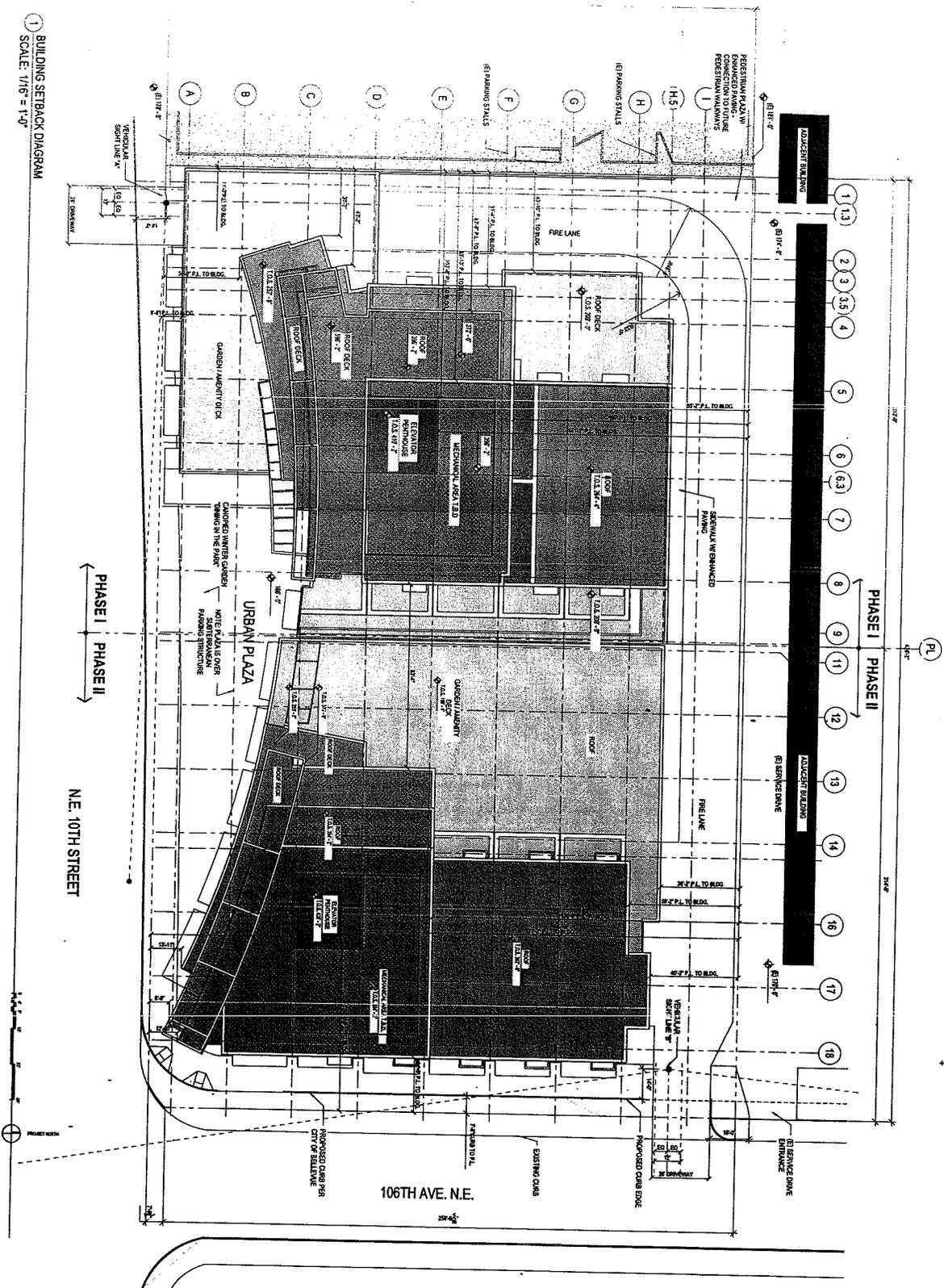
USE	AREA (SQ FT)	PERCENTAGE	MAXIMUM HEIGHT (FT)	MAXIMUM FLOOR AREA (SQ FT)	MAXIMUM LOT COVERAGE (%)	MAXIMUM LOT AREA (SQ FT)	MAXIMUM LOT WIDTH (FT)	MAXIMUM LOT DEPTH (FT)
RESIDENTIAL	100,000	40%	35	100,000	40%	100,000	100	100
COMMERCIAL	150,000	60%	45	150,000	60%	150,000	150	150
PARKING	50,000	20%	15	50,000	20%	50,000	50	50
LANDSCAPE	10,000	4%	10	10,000	4%	10,000	10	10
TOTAL	250,000	100%		250,000	100%	250,000	250	250

**PHASE I & II ANALYSIS**

USE	AREA (SQ FT)	PERCENTAGE	MAXIMUM HEIGHT (FT)	MAXIMUM FLOOR AREA (SQ FT)	MAXIMUM LOT COVERAGE (%)	MAXIMUM LOT AREA (SQ FT)	MAXIMUM LOT WIDTH (FT)	MAXIMUM LOT DEPTH (FT)
RESIDENTIAL	100,000	40%	35	100,000	40%	100,000	100	100
COMMERCIAL	150,000	60%	45	150,000	60%	150,000	150	150
PARKING	50,000	20%	15	50,000	20%	50,000	50	50
LANDSCAPE	10,000	4%	10	10,000	4%	10,000	10	10
TOTAL	250,000	100%		250,000	100%	250,000	250	250



1 BUILDING SETBACK DIAGRAM  
SCALE: 1/16" = 1'-0"



**NOT FOR CONSTRUCTION**

REVISIONS

NO.	DATE	DESCRIPTION

BUILDING SETBACKS DIAGRAM  
T0.3

1001 PROPERTIES, LLC

**BELLEVUE - CADILLAC SITE**

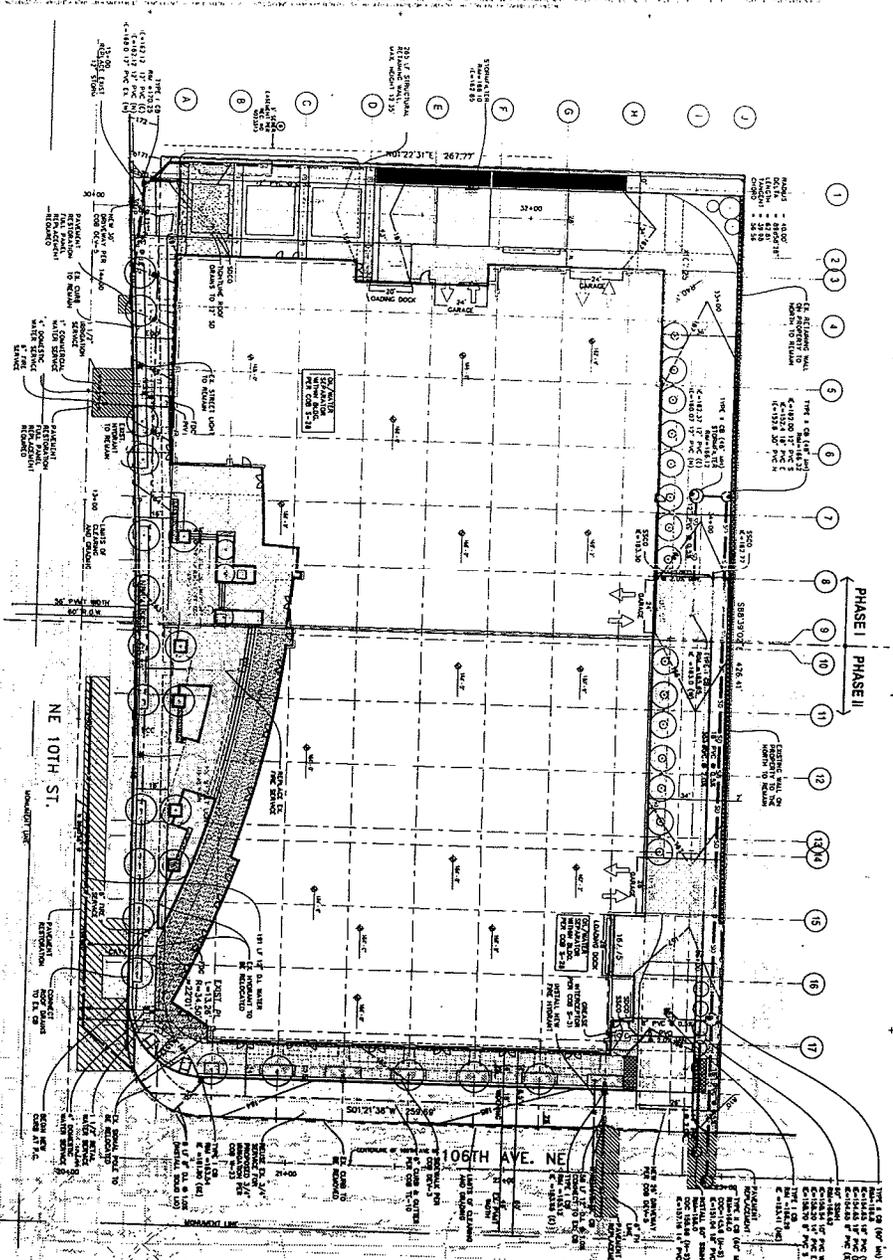
PHASE I  
10500 N.E. 10TH STREET  
BELLEVUE, WA 98004

PHASE II  
10600 N.E. 10TH STREET  
BELLEVUE, WA 98004

**carrierjohnson**

architecture for urban environments

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**OWNER/DEVELOPER**  
 1001 PROPERTIES, LLC  
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 SUITE 1000  
 SEATTLE, WA 98101  
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 SHAW GROUP, INC.  
 10000 N.E. 4TH STREET, SUITE 1200  
 BELLEVUE, WA 98004  
 CONTACT: STEVE SCHMIDT  
 (206) 333-1144 / 1-800-838-0088

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 BELLEVUE, WA 98004  
 CONTACT: STEVE SCHMIDT  
 (206) 333-1144 / 1-800-838-0088

**STATISTICAL INFORMATION - PHASE I**

NO.	DESCRIPTION	QUANTITY	UNIT	MARKED BY	DATE
1	CONCRETE	100	YD	100	10/10/10
2	STEEL	50	TON	50	10/10/10
3	WOOD	200	YD	200	10/10/10
4	PAVING	1000	SQ YD	1000	10/10/10
5	LANDSCAPING	500	SQ YD	500	10/10/10
6	UTILITIES	100	FT	100	10/10/10
7	WATER	100	FT	100	10/10/10
8	SEWER	100	FT	100	10/10/10
9	ELECTRICAL	100	FT	100	10/10/10
10	MECHANICAL	100	FT	100	10/10/10
11	HEATING	100	FT	100	10/10/10
12	COOLING	100	FT	100	10/10/10
13	INSULATION	100	FT	100	10/10/10
14	GLASS	100	FT	100	10/10/10
15	DOORS	100	FT	100	10/10/10
16	WINDOWS	100	FT	100	10/10/10
17	ROOFING	100	FT	100	10/10/10
18	FOUNDATION	100	FT	100	10/10/10
19	CONCRETE	100	YD	100	10/10/10
20	STEEL	50	TON	50	10/10/10
21	WOOD	200	YD	200	10/10/10
22	PAVING	1000	SQ YD	1000	10/10/10
23	LANDSCAPING	500	SQ YD	500	10/10/10
24	UTILITIES	100	FT	100	10/10/10
25	WATER	100	FT	100	10/10/10
26	SEWER	100	FT	100	10/10/10
27	ELECTRICAL	100	FT	100	10/10/10
28	MECHANICAL	100	FT	100	10/10/10
29	HEATING	100	FT	100	10/10/10
30	COOLING	100	FT	100	10/10/10
31	INSULATION	100	FT	100	10/10/10
32	GLASS	100	FT	100	10/10/10
33	DOORS	100	FT	100	10/10/10
34	WINDOWS	100	FT	100	10/10/10
35	ROOFING	100	FT	100	10/10/10
36	FOUNDATION	100	FT	100	10/10/10
37	CONCRETE	100	YD	100	10/10/10
38	STEEL	50	TON	50	10/10/10
39	WOOD	200	YD	200	10/10/10
40	PAVING	1000	SQ YD	1000	10/10/10
41	LANDSCAPING	500	SQ YD	500	10/10/10
42	UTILITIES	100	FT	100	10/10/10
43	WATER	100	FT	100	10/10/10
44	SEWER	100	FT	100	10/10/10
45	ELECTRICAL	100	FT	100	10/10/10
46	MECHANICAL	100	FT	100	10/10/10
47	HEATING	100	FT	100	10/10/10
48	COOLING	100	FT	100	10/10/10
49	INSULATION	100	FT	100	10/10/10
50	GLASS	100	FT	100	10/10/10
51	DOORS	100	FT	100	10/10/10
52	WINDOWS	100	FT	100	10/10/10
53	ROOFING	100	FT	100	10/10/10
54	FOUNDATION	100	FT	100	10/10/10
55	CONCRETE	100	YD	100	10/10/10
56	STEEL	50	TON	50	10/10/10
57	WOOD	200	YD	200	10/10/10
58	PAVING	1000	SQ YD	1000	10/10/10
59	LANDSCAPING	500	SQ YD	500	10/10/10
60	UTILITIES	100	FT	100	10/10/10
61	WATER	100	FT	100	10/10/10
62	SEWER	100	FT	100	10/10/10
63	ELECTRICAL	100	FT	100	10/10/10
64	MECHANICAL	100	FT	100	10/10/10
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66	COOLING	100	FT	100	10/10/10
67	INSULATION	100	FT	100	10/10/10
68	GLASS	100	FT	100	10/10/10
69	DOORS	100	FT	100	10/10/10
70	WINDOWS	100	FT	100	10/10/10
71	ROOFING	100	FT	100	10/10/10
72	FOUNDATION	100	FT	100	10/10/10
73	CONCRETE	100	YD	100	10/10/10
74	STEEL	50	TON	50	10/10/10
75	WOOD	200	YD	200	10/10/10
76	PAVING	1000	SQ YD	1000	10/10/10
77	LANDSCAPING	500	SQ YD	500	10/10/10
78	UTILITIES	100	FT	100	10/10/10
79	WATER	100	FT	100	10/10/10
80	SEWER	100	FT	100	10/10/10
81	ELECTRICAL	100	FT	100	10/10/10
82	MECHANICAL	100	FT	100	10/10/10
83	HEATING	100	FT	100	10/10/10
84	COOLING	100	FT	100	10/10/10
85	INSULATION	100	FT	100	10/10/10
86	GLASS	100	FT	100	10/10/10
87	DOORS	100	FT	100	10/10/10
88	WINDOWS	100	FT	100	10/10/10
89	ROOFING	100	FT	100	10/10/10
90	FOUNDATION	100	FT	100	10/10/10
91	CONCRETE	100	YD	100	10/10/10
92	STEEL	50	TON	50	10/10/10
93	WOOD	200	YD	200	10/10/10
94	PAVING	1000	SQ YD	1000	10/10/10
95	LANDSCAPING	500	SQ YD	500	10/10/10
96	UTILITIES	100	FT	100	10/10/10
97	WATER	100	FT	100	10/10/10
98	SEWER	100	FT	100	10/10/10
99	ELECTRICAL	100	FT	100	10/10/10
100	MECHANICAL	100	FT	100	10/10/10

**PHASE I PHASE II**

**NE 10TH ST.**

**106TH AVE. NE**

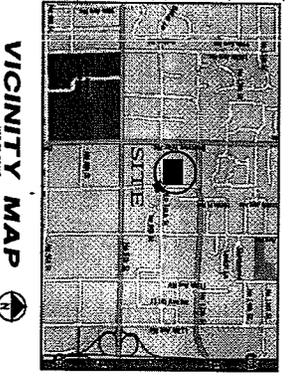
**STATISTICAL INFORMATION - PHASE I**

**STATISTICAL INFORMATION - PHASE 2**

**STATISTICAL INFORMATION - PHASE 2**

**DR**

**SITE PLAN**



**VICINITY MAP**

1001 PROPERTIES, LLC  
 10600 N.E. 10TH STREET  
 BELLEVUE, WA 98004

**1001 PROPERTIES, LLC**

**BELLEVUE - CADILLAC SITE**

PHASE I  
 10600 N.E. 10TH STREET  
 BELLEVUE, WA 98004

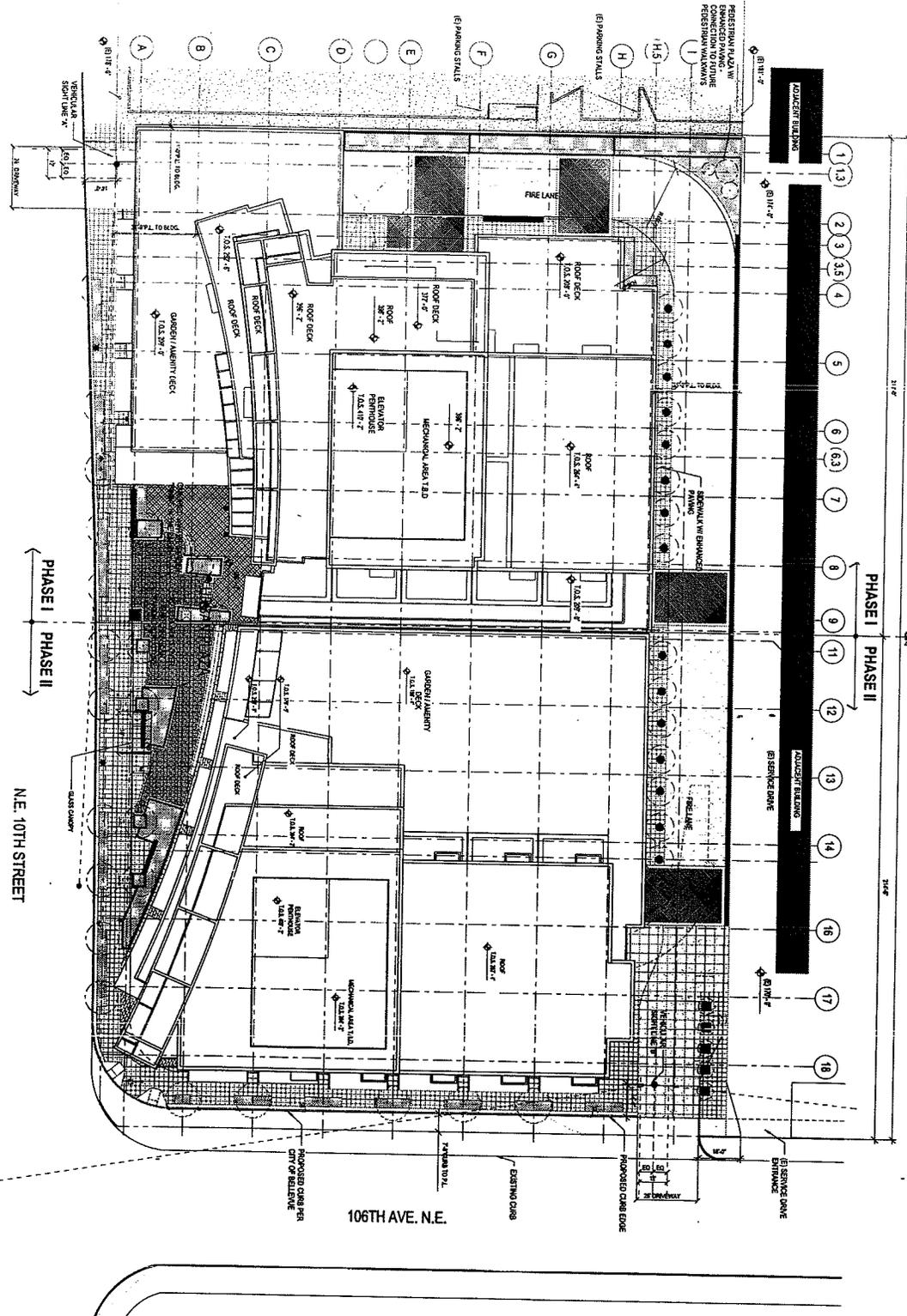
PHASE II  
 10600 N.E. 10TH STREET  
 BELLEVUE, WA 98004

**carrierjohanson**  
 architects for urban environments  
 1201 4th Avenue, Suite 2000, Seattle, WA 98101  
 phone 206.461.2500 | fax 206.461.2522

**DR**

**SITE PLAN**

1 ARCHITECTURAL SITE PLAN  
SCALE: 1/16" = 1'-0"



A1.0

NOT FOR CONSTRUCTION

1001 PROPERTIES, LLC

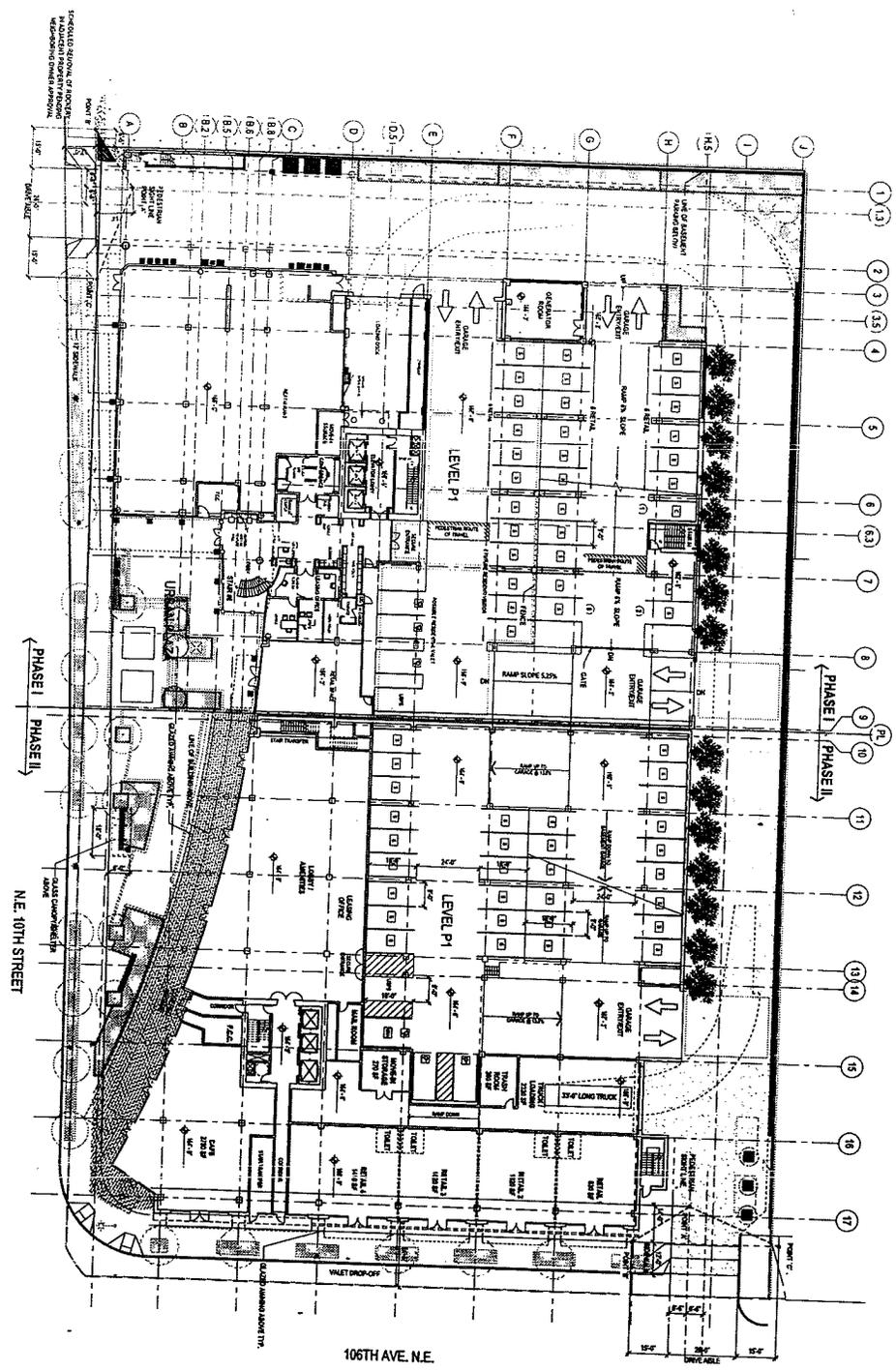
BELLEVUE - CADILLAC SITE  
 PHASE I 10500 N.E. 107TH STREET  
 BELLEVUE, WA 98004  
 PHASE II 10600 N.E. 107TH STREET  
 BELLEVUE, WA 98004

carrierjohnson  
 architecture for urban environments  
 1201 6th Avenue, Suite 200, Seattle, WA 98101  
 phone 206.228.2553 | fax 206.228.2527



PLANS AND ELEVATIONS SHALL BE CONSIDERED TO BE THE PROPERTY OF THE ARCHITECT AND SHALL NOT BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS IN THE PLANS OR ELEVATIONS. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY CONSTRUCTION DEFECTS OR DELAYS. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY CHANGES TO THE PLANS OR ELEVATIONS. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY COSTS INCURRED BY THE OWNER OR OTHER PARTIES. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY DAMAGES, INCLUDING CONSEQUENTIAL DAMAGES, ARISING FROM THE USE OF THE PLANS OR ELEVATIONS. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY INJURIES OR DEATHS. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY OTHER DAMAGES. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY OTHER DAMAGES. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY OTHER DAMAGES.

1 LEVEL 01 FLOOR PLAN  
SCALE: 1/16" = 1'-0"



A2.2

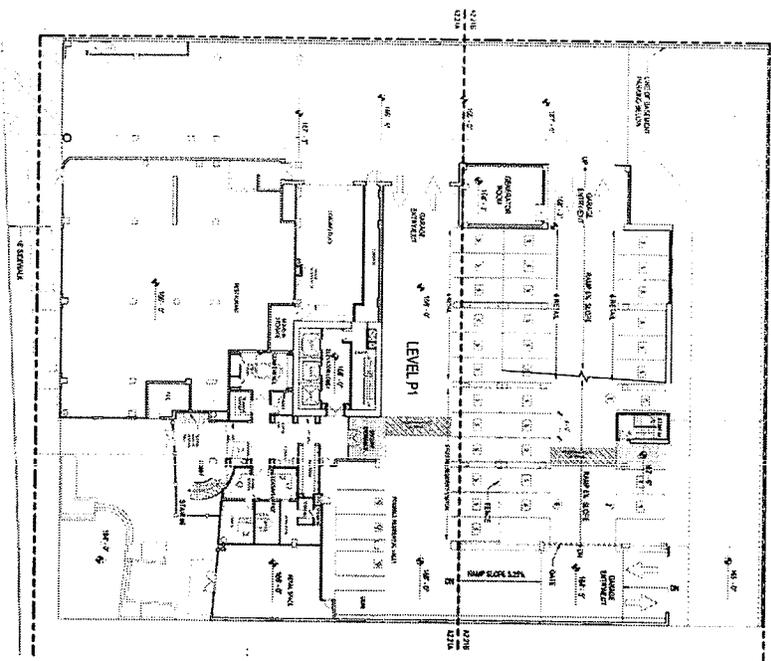
NOT FOR CONSTRUCTION  
LEVEL 01 FLOOR PLAN

1001 PROPERTIES, LLC

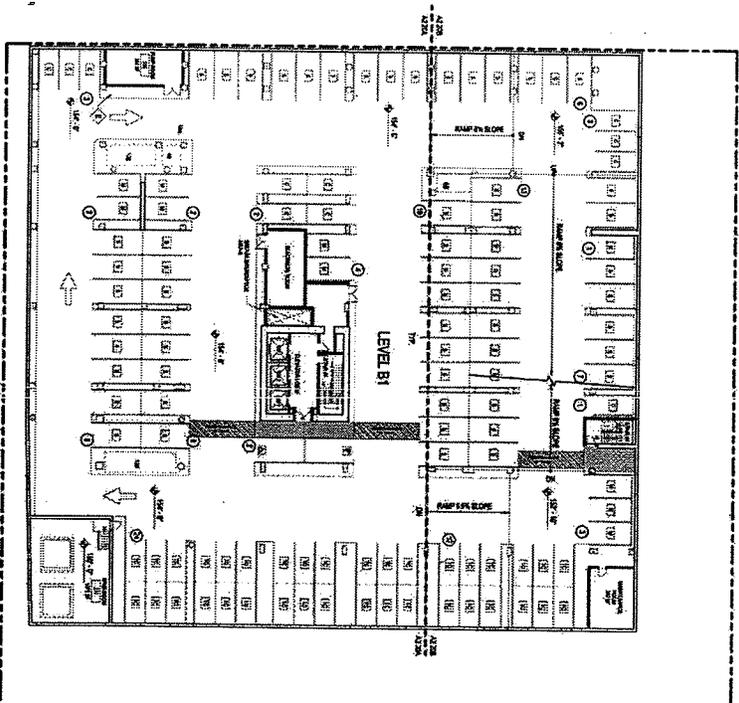
BELLEVUE - CADILLAC SITE  
PHASE I 10500 N.E. 10TH STREET BELLEVUE, WA 98004  
PHASE II 10600 N.E. 10TH STREET BELLEVUE, WA 98004

carrierjohnson  
architecture for urban environments  
1201 4th Avenue West, Suite 400 Seattle, WA 98101  
Phone: 206.465.2500 | Fax: 206.465.2527

2) Level 1  
SCALE: 1/16" = 1'-0"



1) Level B1  
SCALE: 1/16" = 1'-0"



A2.13

**NOT FOR CONSTRUCTION**

GARAGE  
FURNISHING  
ROUTE OF TRAVEL

1001 PROPERTIES, LLC

BELLEVUE - CADILLAC SITE

PHASE I 10500 N.E. 16TH STREET  
BELLEVUE, WA 98004

PHASE II 10600 N.E. 16TH STREET  
BELLEVUE, WA 98004

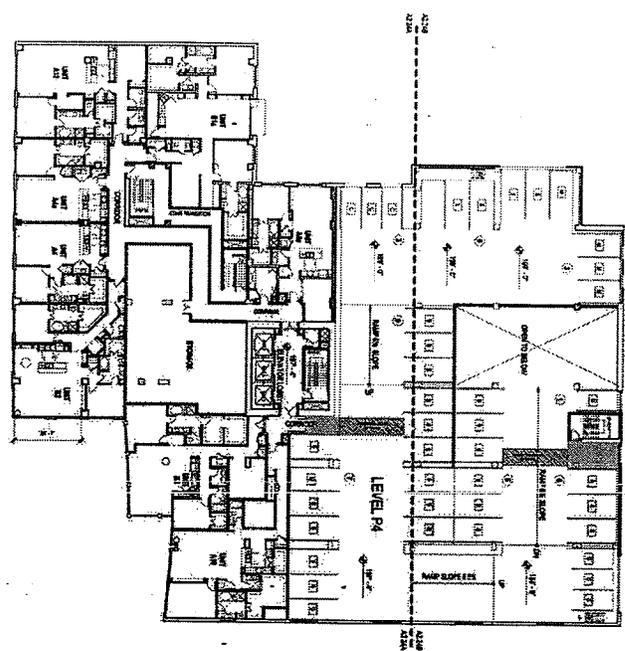
carrierjohnson

(Architects for utility subdivisions)

1021 194th Avenue, Suite 200, Bellevue, WA 98004  
Phone: (206) 226-2263 | Fax: (206) 226-8222



Level 3  
 SCALE: 1/8" = 1'-0"



A2.16

OWNER: 1001 PROPERTIES, LLC  
 ARCHITECT: carrierjohnson  
 DATE: 10/15/2013  
 PROJECT: BELLEVUE - CADILLAC SITE  
 SHEET: A2.16

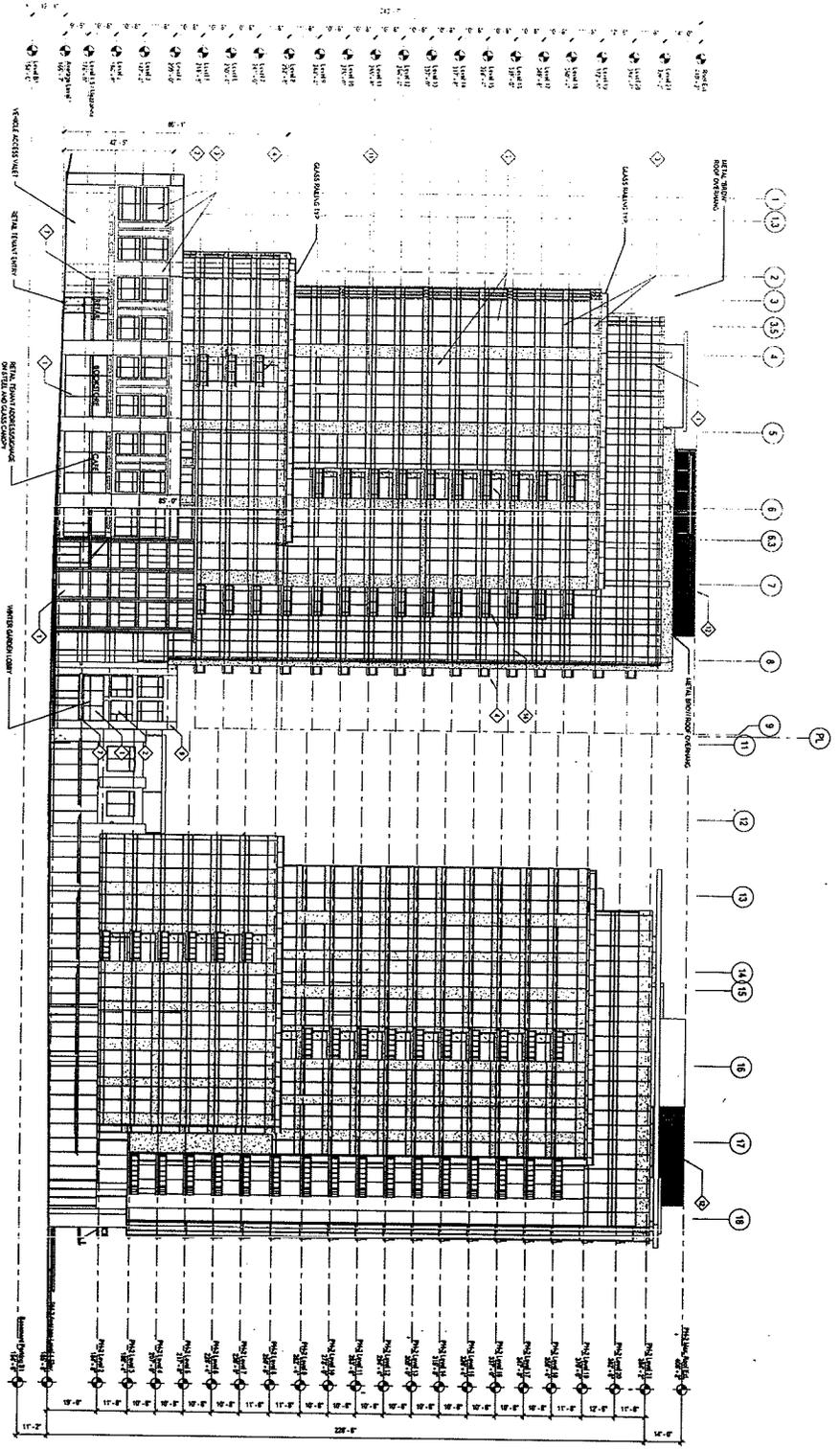
**NOT FOR CONSTRUCTION**

**1001 PROPERTIES, LLC**

**BELLEVUE - CADILLAC SITE**  
 PHASE I  
 10500 N.E. 10TH STREET  
 BELLEVUE, WA 98004  
 PHASE II  
 10500 N.E. 10TH STREET  
 BELLEVUE, WA 98004

**carrierjohnson**  
 4000 15th Avenue SW, Suite 200  
 Bellevue, WA 98004  
 Phone: 425.236.2200 | Fax: 425.236.2202





1 South Elevation  
SCALE: 1/16" = 1'-0"

EXT. ELEVATION GENERAL NOTES

1. GENERAL: ALL WORK SHALL BE IN ACCORDANCE WITH THE 2018 INTERNATIONAL BUILDING CODE (IBC) AND THE 2018 INTERNATIONAL RESIDENTIAL CODE (IRC) UNLESS OTHERWISE NOTED.

2. MATERIALS: ALL MATERIALS SHALL BE OF THE HIGHEST QUALITY AND SHALL BE SUBMITTED FOR APPROVAL TO THE ARCHITECT AND ENGINEER PRIOR TO INSTALLATION.

3. FINISHES: ALL FINISHES SHALL BE IN ACCORDANCE WITH THE ARCHITECT'S SCHEDULE OF FINISHES.

4. PROTECT EXISTING: ALL EXISTING STRUCTURES AND UTILITIES SHALL BE PROTECTED AND MAINTAINED THROUGHOUT THE CONSTRUCTION PROCESS.

5. SAFETY: ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE SAFETY REGULATIONS AND STANDARDS.

6. QUALITY CONTROL: THE CONTRACTOR SHALL MAINTAIN A QUALITY CONTROL PROGRAM THROUGHOUT THE CONSTRUCTION PROCESS.

7. SCHEDULING: THE CONTRACTOR SHALL SUBMIT A DETAILED CONSTRUCTION SCHEDULE TO THE ARCHITECT FOR REVIEW AND APPROVAL.

8. COMMUNICATION: THE CONTRACTOR SHALL MAINTAIN OPEN COMMUNICATION WITH THE ARCHITECT AND ENGINEER THROUGHOUT THE PROJECT.

9. SITE MANAGEMENT: THE CONTRACTOR SHALL MAINTAIN A NEAT AND SAFE WORK SITE AT ALL TIMES.

10. ENVIRONMENTAL: THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT THE ENVIRONMENT AND NEIGHBORHOODS.

11. UTILITIES: ALL UTILITIES SHALL BE IDENTIFIED AND PROTECTED PRIOR TO CONSTRUCTION.

12. EROSION CONTROL: THE CONTRACTOR SHALL IMPLEMENT EROSION CONTROL MEASURES TO PREVENT SOIL EROSION AND SEDIMENTATION.

13. TRAFFIC CONTROL: THE CONTRACTOR SHALL IMPLEMENT TRAFFIC CONTROL MEASURES TO MAINTAIN SAFE AND EFFICIENT TRAFFIC FLOW.

14. NEIGHBORHOOD RELATIONS: THE CONTRACTOR SHALL MAINTAIN POSITIVE RELATIONS WITH THE NEIGHBORHOOD THROUGHOUT THE PROJECT.

15. RECORD DRAWINGS: THE CONTRACTOR SHALL MAINTAIN ACCURATE RECORD DRAWINGS THROUGHOUT THE CONSTRUCTION PROCESS.

16. AS-BUILT: THE CONTRACTOR SHALL SUBMIT AS-BUILT DRAWINGS TO THE ARCHITECT UPON COMPLETION OF THE PROJECT.

17. WARRANTY: ALL WORK SHALL BE WARRANTED FOR THE FULL TERM OF THE WARRANTY PERIOD.

18. INSURANCE: THE CONTRACTOR SHALL MAINTAIN ALL NECESSARY INSURANCE COVERAGE THROUGHOUT THE PROJECT.

19. PAYROLL: THE CONTRACTOR SHALL MAINTAIN ACCURATE RECORDS OF ALL PAYROLL AND BENEFIT INFORMATION.

20. TAXES: THE CONTRACTOR SHALL MAINTAIN ACCURATE RECORDS OF ALL TAX INFORMATION.

EXTERIOR MATERIAL LEGEND

SYMBOL	DESCRIPTION	FINISH	SECTION
	ALUMINUM SHADED GLASS SIDING	SH1	SH1
	CAST-IN-PLACE CONCRETE	SH2	SH2
	PRECAST CONCRETE PANEL - FINISH A	SH3	SH3
	PRECAST CONCRETE PANEL - FINISH B	SH4	SH4
	EXTERIOR PLASTER	SH5	SH5

NOT FOR CONSTRUCTION

1001 PROPERTIES, LLC

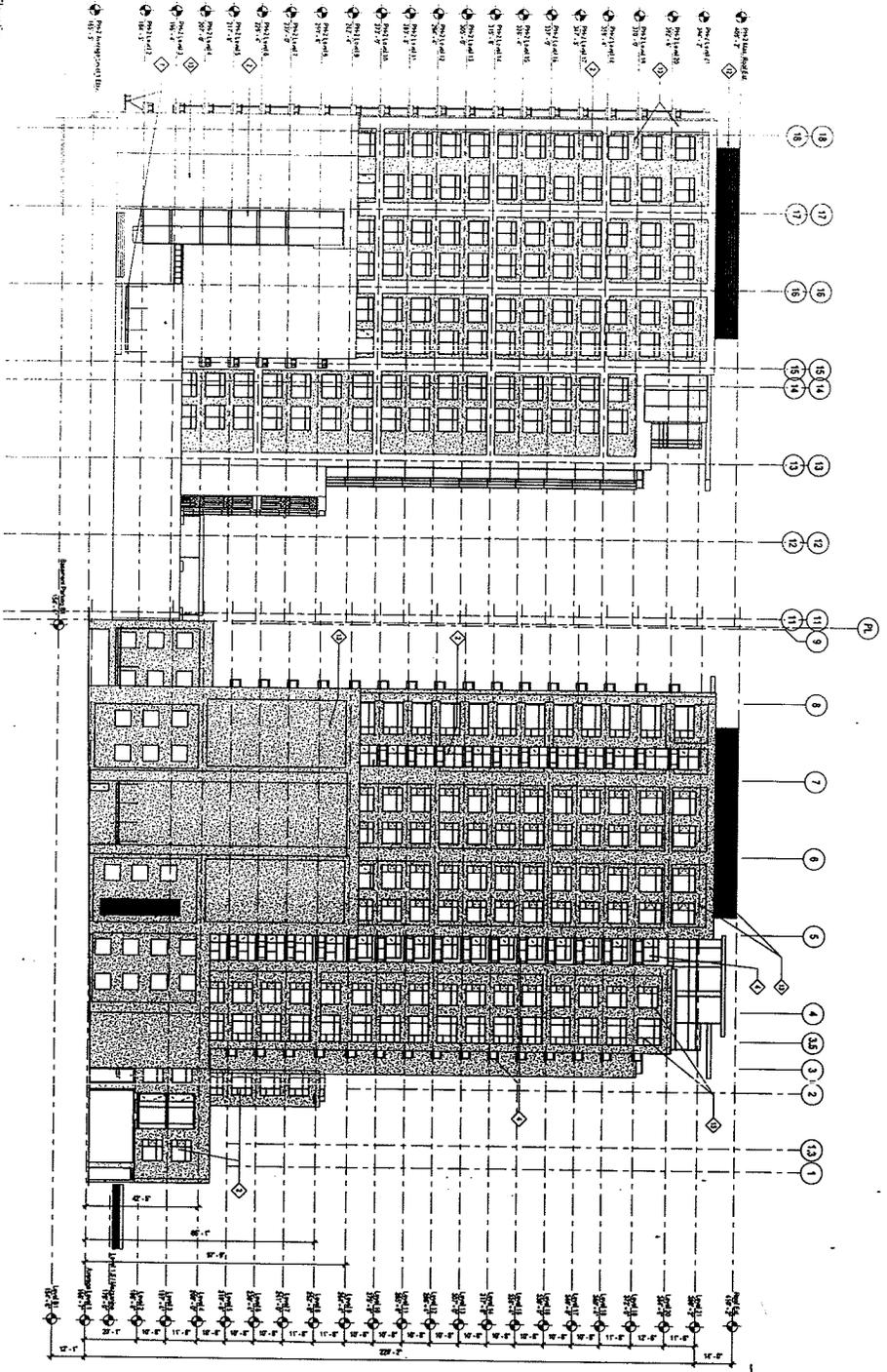
BELLEVUE - CADILLAC SITE  
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 PHASE II 10600 N.E. 10TH STREET BELLEVUE, WA 98004

carrierjohnson  
 architects for urban environments  
 1001 8th Avenue Suite 600 Seattle WA 98101  
 phone 206.224.2500 | fax 206.224.2522

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**EXT. ELEVATION GENERAL NOTES**

1 North Elevation  
SCALE: 1/8" = 1'-0"



- 1. GENERAL: THE ARCHITECT HAS CONDUCTED VISUAL GENERAL VERIFICATION OF THE EXISTING CONDITIONS AND MATERIALS OF THE BUILDING. THE ARCHITECT HAS NOT CONDUCTED A STRUCTURAL ANALYSIS OR INVESTIGATION OF THE BUILDING. THE ARCHITECT IS NOT RESPONSIBLE FOR THE STRUCTURAL INTEGRITY OF THE BUILDING OR THE ACCURACY OF THE EXISTING CONDITIONS SHOWN ON THIS DRAWING.
- 2. MATERIALS: ALL MATERIALS SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF MATERIALS. THE ARCHITECT HAS NOT CONDUCTED A MATERIALS TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE MATERIALS OR THE ACCURACY OF THE MATERIALS SHOWN ON THIS DRAWING.
- 3. FINISHES: ALL FINISHES SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF FINISHES. THE ARCHITECT HAS NOT CONDUCTED A FINISHES TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE FINISHES OR THE ACCURACY OF THE FINISHES SHOWN ON THIS DRAWING.
- 4. PAINTS: ALL PAINTS SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF PAINTS. THE ARCHITECT HAS NOT CONDUCTED A PAINTS TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE PAINTS OR THE ACCURACY OF THE PAINTS SHOWN ON THIS DRAWING.
- 5. GLAZING: ALL GLAZING SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF GLAZING. THE ARCHITECT HAS NOT CONDUCTED A GLAZING TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE GLAZING OR THE ACCURACY OF THE GLAZING SHOWN ON THIS DRAWING.
- 6. METALS: ALL METALS SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF METALS. THE ARCHITECT HAS NOT CONDUCTED A METALS TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE METALS OR THE ACCURACY OF THE METALS SHOWN ON THIS DRAWING.
- 7. MASONRY: ALL MASONRY SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF MASONRY. THE ARCHITECT HAS NOT CONDUCTED A MASONRY TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE MASONRY OR THE ACCURACY OF THE MASONRY SHOWN ON THIS DRAWING.
- 8. CONCRETE: ALL CONCRETE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF CONCRETE. THE ARCHITECT HAS NOT CONDUCTED A CONCRETE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE CONCRETE OR THE ACCURACY OF THE CONCRETE SHOWN ON THIS DRAWING.
- 9. STEEL: ALL STEEL SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF STEEL. THE ARCHITECT HAS NOT CONDUCTED A STEEL TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE STEEL OR THE ACCURACY OF THE STEEL SHOWN ON THIS DRAWING.
- 10. WOOD: ALL WOOD SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF WOOD. THE ARCHITECT HAS NOT CONDUCTED A WOOD TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE WOOD OR THE ACCURACY OF THE WOOD SHOWN ON THIS DRAWING.
- 11. GLASS: ALL GLASS SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF GLASS. THE ARCHITECT HAS NOT CONDUCTED A GLASS TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE GLASS OR THE ACCURACY OF THE GLASS SHOWN ON THIS DRAWING.
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- 13. PLASTER: ALL PLASTER SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF PLASTER. THE ARCHITECT HAS NOT CONDUCTED A PLASTER TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE PLASTER OR THE ACCURACY OF THE PLASTER SHOWN ON THIS DRAWING.
- 14. STUCCO: ALL STUCCO SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF STUCCO. THE ARCHITECT HAS NOT CONDUCTED A STUCCO TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE STUCCO OR THE ACCURACY OF THE STUCCO SHOWN ON THIS DRAWING.
- 15. TERRAZZO: ALL TERRAZZO SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF TERRAZZO. THE ARCHITECT HAS NOT CONDUCTED A TERRAZZO TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE TERRAZZO OR THE ACCURACY OF THE TERRAZZO SHOWN ON THIS DRAWING.
- 16. GRANITE: ALL GRANITE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF GRANITE. THE ARCHITECT HAS NOT CONDUCTED A GRANITE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE GRANITE OR THE ACCURACY OF THE GRANITE SHOWN ON THIS DRAWING.
- 17. MARBLE: ALL MARBLE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF MARBLE. THE ARCHITECT HAS NOT CONDUCTED A MARBLE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE MARBLE OR THE ACCURACY OF THE MARBLE SHOWN ON THIS DRAWING.
- 18. SLATE: ALL SLATE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF SLATE. THE ARCHITECT HAS NOT CONDUCTED A SLATE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE SLATE OR THE ACCURACY OF THE SLATE SHOWN ON THIS DRAWING.
- 19. QUARTZITE: ALL QUARTZITE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF QUARTZITE. THE ARCHITECT HAS NOT CONDUCTED A QUARTZITE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE QUARTZITE OR THE ACCURACY OF THE QUARTZITE SHOWN ON THIS DRAWING.
- 20. SANDSTONE: ALL SANDSTONE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF SANDSTONE. THE ARCHITECT HAS NOT CONDUCTED A SANDSTONE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE SANDSTONE OR THE ACCURACY OF THE SANDSTONE SHOWN ON THIS DRAWING.

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- 15. TERRAZZO: ALL TERRAZZO SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF TERRAZZO. THE ARCHITECT HAS NOT CONDUCTED A TERRAZZO TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE TERRAZZO OR THE ACCURACY OF THE TERRAZZO SHOWN ON THIS DRAWING.
- 16. GRANITE: ALL GRANITE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF GRANITE. THE ARCHITECT HAS NOT CONDUCTED A GRANITE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE GRANITE OR THE ACCURACY OF THE GRANITE SHOWN ON THIS DRAWING.
- 17. MARBLE: ALL MARBLE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF MARBLE. THE ARCHITECT HAS NOT CONDUCTED A MARBLE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE MARBLE OR THE ACCURACY OF THE MARBLE SHOWN ON THIS DRAWING.
- 18. SLATE: ALL SLATE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF SLATE. THE ARCHITECT HAS NOT CONDUCTED A SLATE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE SLATE OR THE ACCURACY OF THE SLATE SHOWN ON THIS DRAWING.
- 19. QUARTZITE: ALL QUARTZITE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF QUARTZITE. THE ARCHITECT HAS NOT CONDUCTED A QUARTZITE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE QUARTZITE OR THE ACCURACY OF THE QUARTZITE SHOWN ON THIS DRAWING.
- 20. SANDSTONE: ALL SANDSTONE SHALL BE OF THE QUALITY AND TYPE SPECIFIED IN THE SCHEDULE OF SANDSTONE. THE ARCHITECT HAS NOT CONDUCTED A SANDSTONE TEST PROGRAM. THE ARCHITECT IS NOT RESPONSIBLE FOR THE QUALITY OF THE SANDSTONE OR THE ACCURACY OF THE SANDSTONE SHOWN ON THIS DRAWING.

EXTERIOR MATERIAL LEGEND	
SYMBOL	DESCRIPTION
	ALUMINUM PAINT GLASS BLOCK
	CAST-IN-PLACE CONCRETE
	PRECAST CONCRETE PANEL - FINISH A
	PRECAST CONCRETE PANEL - FINISH B
	PRECAST CONCRETE PANEL - FINISH C
	EXTERIOR WATER

1001 PROPERTIES, LLC  
PHASE I  
10500 N.E. 10TH STREET  
BELLEVUE, WA 98004

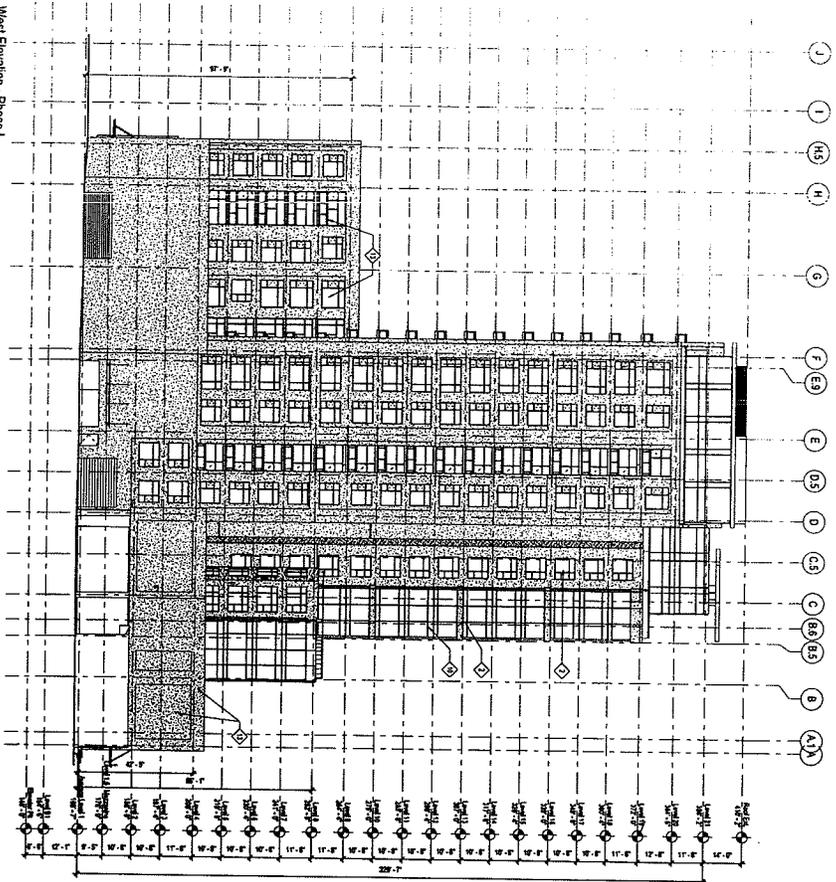
PHASE II  
10500 N.E. 10TH STREET  
BELLEVUE, WA 98004

carrierjohnson  
architecture for urban environments  
1371 8th Avenue, Suite 200 | Seattle, WA 98101  
phone 206.228.2353 | fax 206.228.8227

**EXT. ELEVATION GENERAL NOTES**

1. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.  
 2. FINISH GRADE SHALL BE AS SHOWN ON THE SITE PLAN.  
 3. ALL MATERIALS AND METHODS OF CONSTRUCTION SHALL BE AS SHOWN ON THE EXTERIOR MATERIAL LEGEND.  
 4. ALL MATERIALS SHALL BE APPROVED BY THE ARCHITECT PRIOR TO INSTALLATION.  
 5. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.  
 6. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE LOCAL BUILDING CODES.  
 7. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE LOCAL FIRE CODES.  
 8. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE LOCAL PLUMBING CODES.  
 9. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE LOCAL ELECTRICAL CODES.  
 10. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE LOCAL MECHANICAL CODES.  
 11. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE LOCAL SANITARY CODES.  
 12. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE LOCAL HEALTH CODES.  
 13. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE LOCAL ENVIRONMENTAL CODES.  
 14. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE LOCAL HISTORIC PRESERVATION CODES.  
 15. ALL MATERIALS SHALL BE INSTALLED IN ACCORDANCE WITH THE LOCAL ZONING CODES.

1 West Elevation - Phase I  
 SCALE: 1/8" = 1'-0"



**EXTERIOR MATERIAL LEGEND**

SYMBOL	DESCRIPTION	NOTES
	ALUMINUM PANEL CLADDING	
	CAST-IN-PLACE CONCRETE	
	PRECAST CONCRETE PANEL - FINISH A	
	PRECAST CONCRETE PANEL - FINISH B	
	PRECAST CONCRETE PANEL - FINISH C	
	EXTERIOR METAL	

**NOT FOR CONSTRUCTION**

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A6.3

2 East Elevation - Phase II  
 SCALE: 1/16" = 1'-0"

**EXT. ELEVATION GENERAL NOTES**

1. ALL MATERIALS AND FINISHES TO BE SHOWN ON THIS DRAWING ARE TO BE USED UNLESS OTHERWISE NOTED.

2. ALL MATERIALS AND FINISHES TO BE SHOWN ON THIS DRAWING ARE TO BE USED UNLESS OTHERWISE NOTED.

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10. ALL MATERIALS AND FINISHES TO BE SHOWN ON THIS DRAWING ARE TO BE USED UNLESS OTHERWISE NOTED.

**EXTERIOR MATERIAL LEGEND**

SYMBOL	DESCRIPTION	FINISH	SECTION
	ALUMINUM PAINT GLASS DOOR	ALUMINUM PAINT	SECTION 1
	CAST IN PLACE CONCRETE	CAST IN PLACE CONCRETE	SECTION 2
	PRECAST CONCRETE PANEL - FINISH A	PRECAST CONCRETE PANEL - FINISH A	SECTION 3
	PRECAST CONCRETE PANEL - FINISH B	PRECAST CONCRETE PANEL - FINISH B	SECTION 4
	PRECAST CONCRETE PANEL - FINISH C	PRECAST CONCRETE PANEL - FINISH C	SECTION 5
	EXTERIOR SIDING	EXTERIOR SIDING	SECTION 6

**NOT FOR CONSTRUCTION**

AG-4

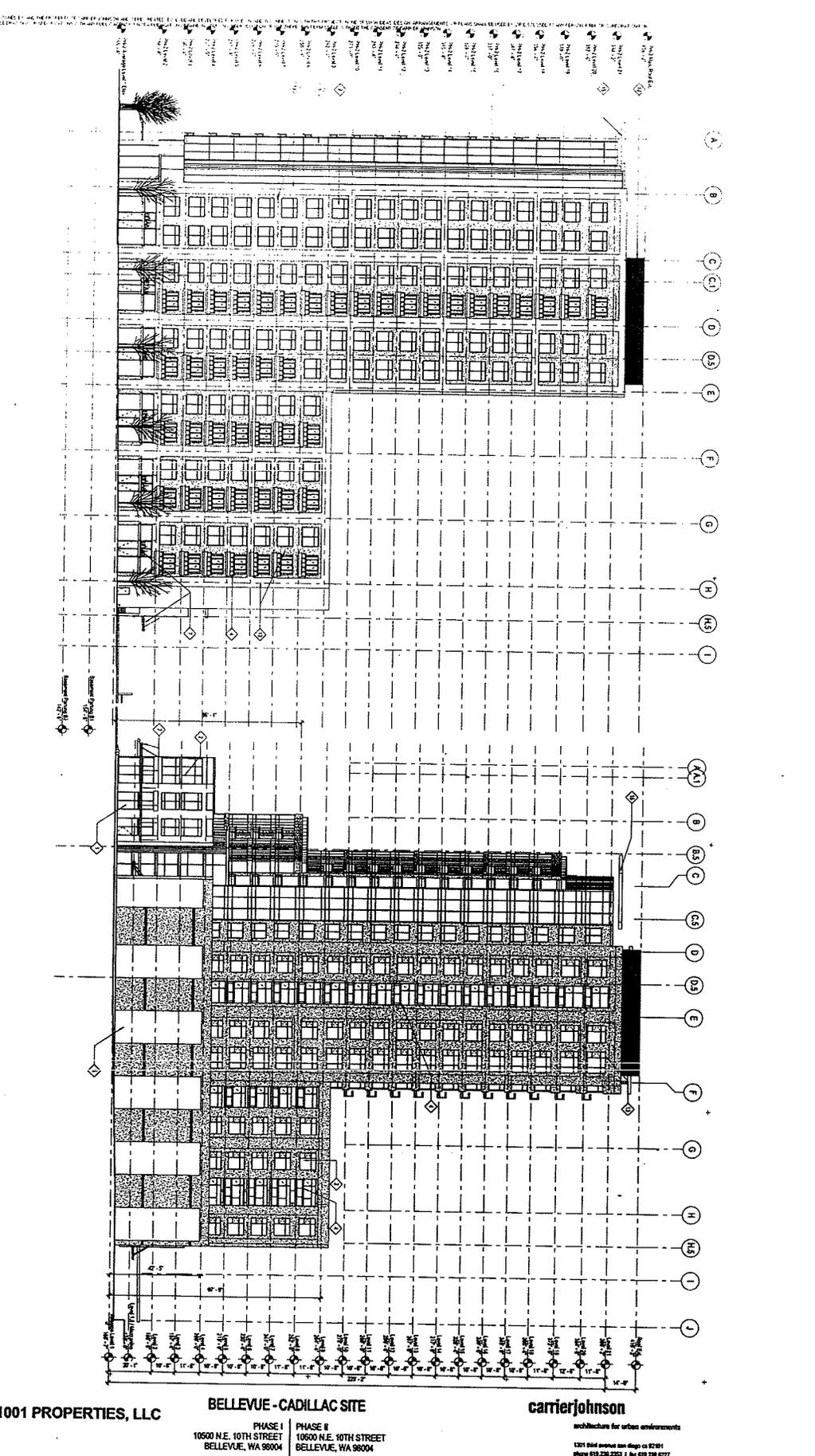
1001 PROPERTIES, LLC

BELLEVUE - CADILLAC SITE

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 BELLEVUE, WA 98004

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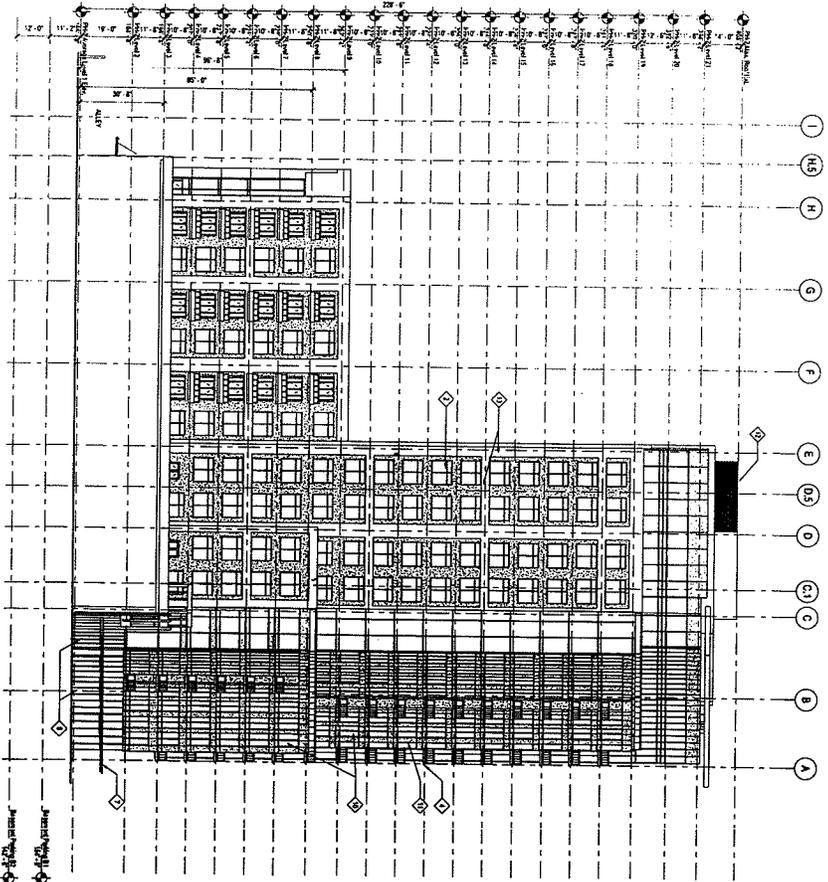


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**EXT. ELEVATION GENERAL NOTES**

- 1. REFER TO ARCHITECTURAL GENERAL NOTES FOR GENERAL NOTES.
- 2. REFER TO ARCHITECTURAL GENERAL NOTES FOR MATERIALS AND FINISHES.
- 3. REFER TO ARCHITECTURAL GENERAL NOTES FOR WINDOW AND DOOR SCHEDULES.
- 4. REFER TO ARCHITECTURAL GENERAL NOTES FOR ROOFING AND CLADDING.
- 5. REFER TO ARCHITECTURAL GENERAL NOTES FOR SIGNAGE AND MARKING.
- 6. REFER TO ARCHITECTURAL GENERAL NOTES FOR LIGHTING AND ELECTRICAL.
- 7. REFER TO ARCHITECTURAL GENERAL NOTES FOR MECHANICAL AND PLUMBING.
- 8. REFER TO ARCHITECTURAL GENERAL NOTES FOR STRUCTURAL AND FOUNDATION.
- 9. REFER TO ARCHITECTURAL GENERAL NOTES FOR LANDSCAPE AND SITEWORK.
- 10. REFER TO ARCHITECTURAL GENERAL NOTES FOR ACCESSIBILITY AND COMPLIANCE.

1 West Elevation - Phase II  
SCALE: 1/16" = 1'-0"



EXTENSION MATERIAL LEGEND		SYMBOL	DESCRIPTION	FINISH	SECTION	SPCS	SECTION
[Symbol]	ALUMINUM PANEL CLAD DOOR	[Symbol]	ALUMINUM PANEL CLAD DOOR	ALUMINUM	0410	0410	0410
[Symbol]	CASUALTY TYPING ALUMINUM PANEL WINDOW	[Symbol]	CASUALTY TYPING ALUMINUM PANEL WINDOW	ALUMINUM	0411	0411	0411
[Symbol]	ROOFING	[Symbol]	ROOFING	ASPH/FLT	0412	0412	0412
[Symbol]	CLADDING	[Symbol]	CLADDING	CONCRETE	0413	0413	0413
[Symbol]	CLADDING	[Symbol]	CLADDING	CONCRETE	0414	0414	0414
[Symbol]	CLADDING	[Symbol]	CLADDING	CONCRETE	0415	0415	0415
[Symbol]	CLADDING	[Symbol]	CLADDING	CONCRETE	0416	0416	0416
[Symbol]	CLADDING	[Symbol]	CLADDING	CONCRETE	0417	0417	0417
[Symbol]	CLADDING	[Symbol]	CLADDING	CONCRETE	0418	0418	0418
[Symbol]	CLADDING	[Symbol]	CLADDING	CONCRETE	0419	0419	0419
[Symbol]	CLADDING	[Symbol]	CLADDING	CONCRETE	0420	0420	0420

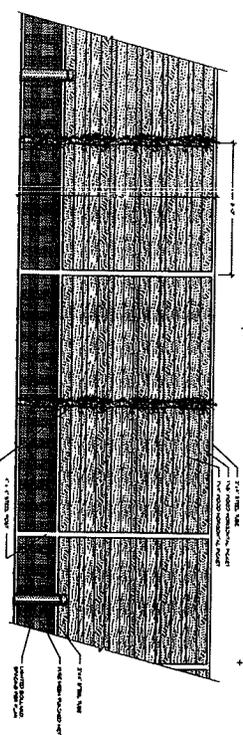
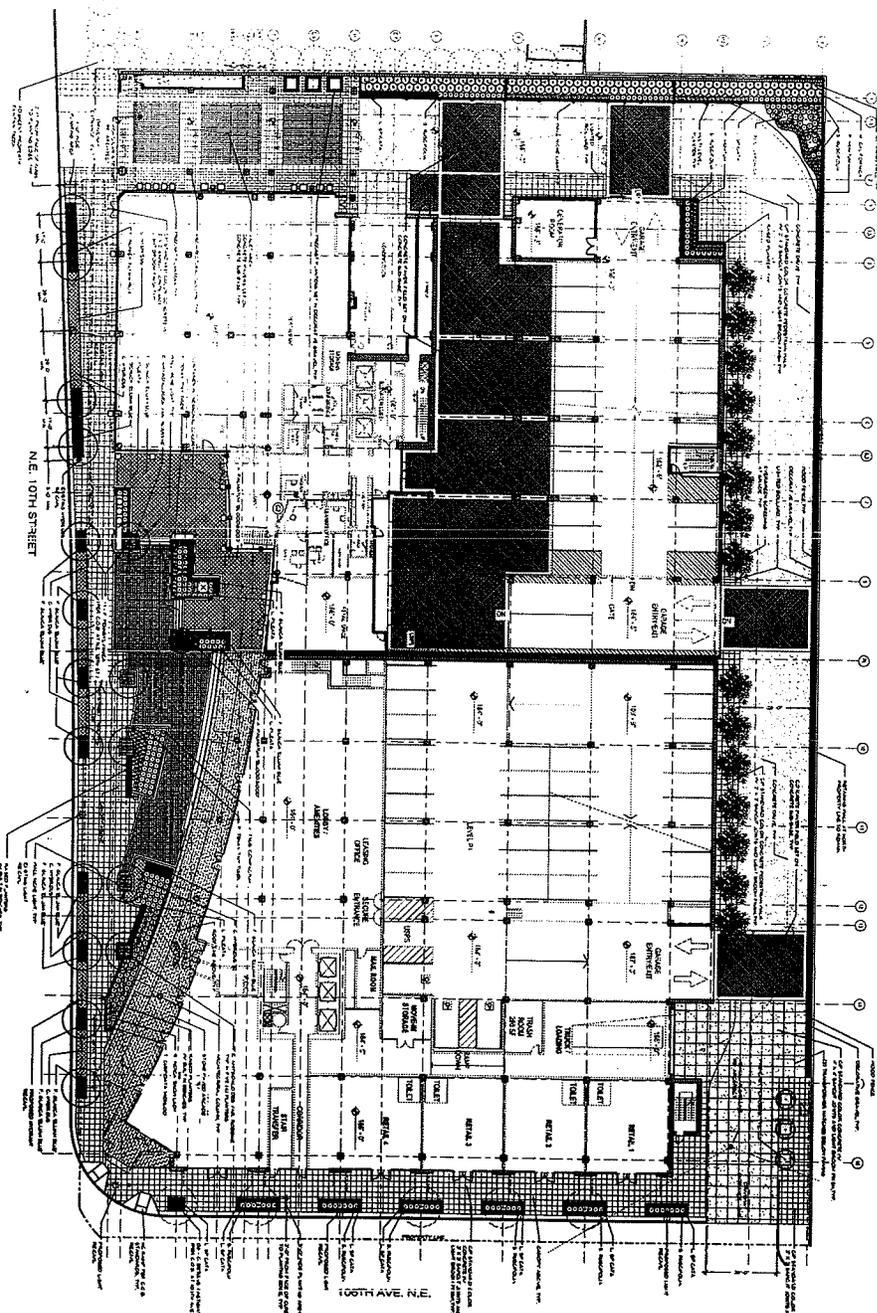
**NOT FOR CONSTRUCTION**

1001 PROPERTIES, LLC

**BELLEVUE - CADILLAC SITE**  
 PHASE I  
 10500 N.E. 10TH STREET  
 BELLEVUE, WA 98004  
 PHASE II  
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**carrierjohnson**  
 architecture for urban environments  
 1201 4th Avenue, Suite 2000, Seattle, WA 98101  
 phone 206.220.2253 | fax 206.220.2257

A6.5



REVISIONS	
NO.	DESCRIPTION
1	ISSUED FOR PERMIT
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SHEET LEGEND	
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L1-00  
 DESIGN REVIEW  
 LANDSCAPE PLAN  
 LEVEL 01  
**NOT FOR CONSTRUCTION**

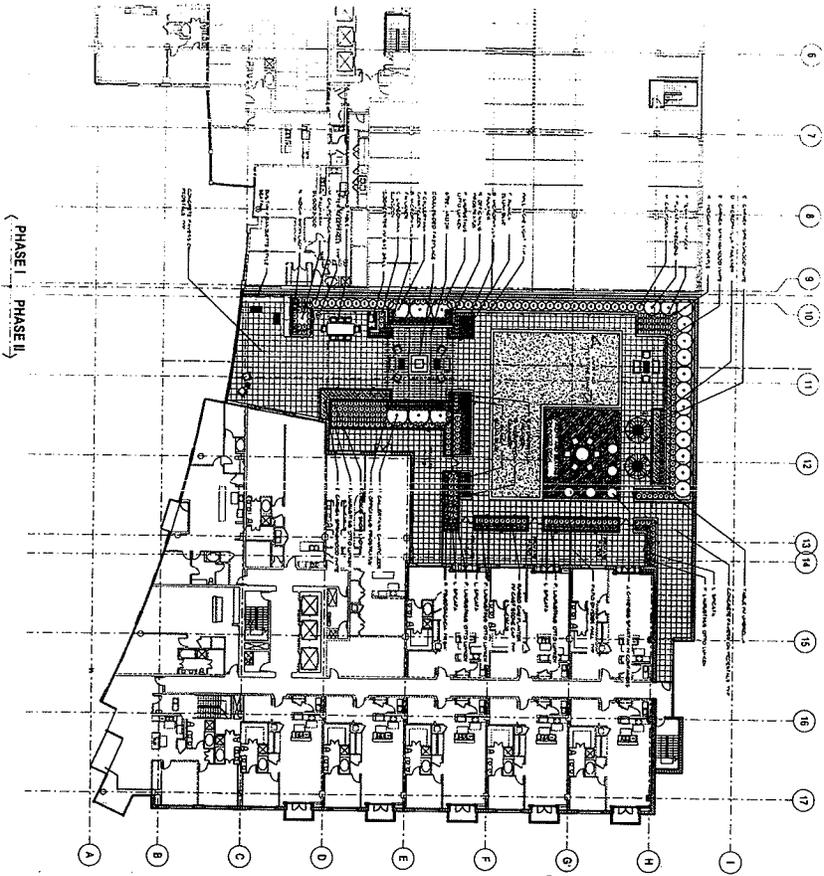


1001 PROPERTIES, LLC

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 BELLEVUE, WA 98004  
 PHASE II  
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 phone 206.461.2253 | fax 206.461.2227



NO.	DESCRIPTION	DATE	BY	CHECKED
1	PLANTING SHALL BE SPACED AND ZONED TO...			
2	PLANT SIZE AND SELECTION MAY BE...			
3	ALL IRRIGATION AND PAVERS SHALL BE ON...			
4	THE IRRIGATION SYSTEM IS TO BE DESIGNED...			
5	ALL PLANT MATERIAL WILL BE ZONED...			
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- NOTES:**
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L1-01

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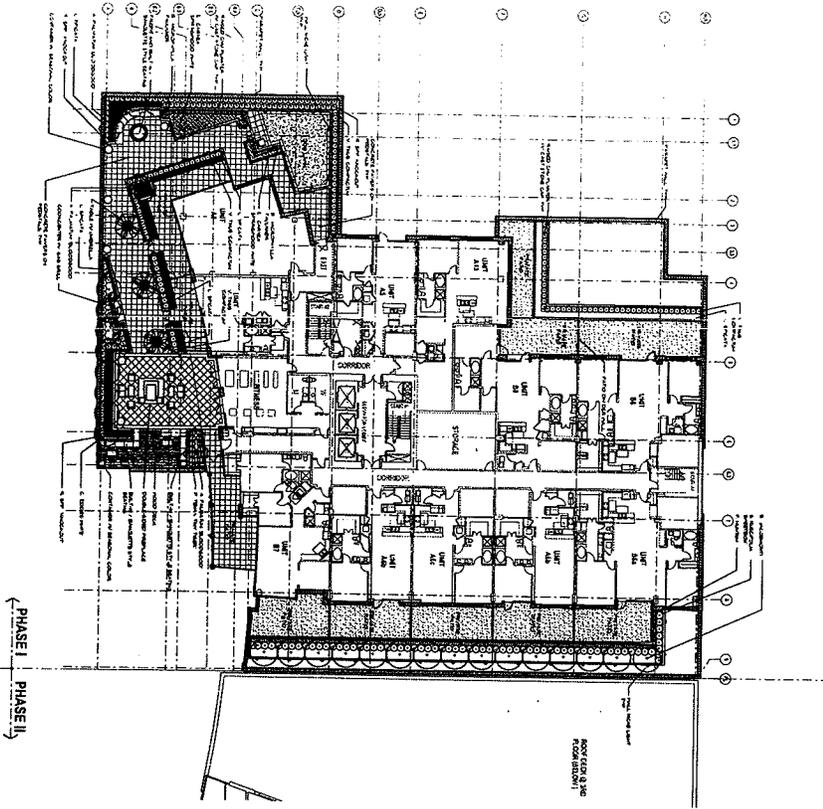


**1001 PROPERTIES, LLC**

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 BELLEVUE, WA 98004  
 PHASE II 10600 N.E. 10TH STREET  
 BELLEVUE, WA 98004



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 architects for urban environments  
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 phone (206) 226-2500 | fax (206) 226-8222



NO.	DESCRIPTION	DATE	BY	CHECKED
1	PLANTING SHALL BE BRANDED AND LINED TO MATCH THE SHAL PLANTING SCHEDULE WITHIN THE 15' STRIP.			
2	PLANTING SHALL BE BRANDED AND LINED TO MATCH THE SHAL PLANTING SCHEDULE WITHIN THE 15' STRIP.			
3	ALL PLANTS AND PLANTERS SHALL BE ON AN AUTOMATIC BRACKET SYSTEM WITH A CLOCK AND PHOTOGRAPHY.			
4	ALL PLANTING MATERIAL IS TO BE DESCRIBED BY THE MANUFACTURER'S NAME AND SPECIFICATIONS ACCORDING TO THE LISTING OF THAT MANUFACTURER ON A SEPARATE SHEET AND NOT CONSIDERED WITH ANY OTHER PLANT.			
5	A MINIMUM OF 70% OF ALL PLANT MATERIAL USED WILL BE PRODUCE TO ORDER.			
6	ALL PLANTERS SHALL BE BRANDED AND LINED TO MATCH THE SHAL PLANTING SCHEDULE WITHIN THE 15' STRIP.			
7	ALL PLANTERS SHALL BE BRANDED AND LINED TO MATCH THE SHAL PLANTING SCHEDULE WITHIN THE 15' STRIP.			

- NOTES:**
1. PLANTING SHALL BE BRANDED AND LINED TO MATCH THE SHAL PLANTING SCHEDULE WITHIN THE 15' STRIP.
  2. PLANTING SHALL BE BRANDED AND LINED TO MATCH THE SHAL PLANTING SCHEDULE WITHIN THE 15' STRIP.
  3. ALL PLANTS AND PLANTERS SHALL BE ON AN AUTOMATIC BRACKET SYSTEM WITH A CLOCK AND PHOTOGRAPHY.
  4. ALL PLANTING MATERIAL IS TO BE DESCRIBED BY THE MANUFACTURER'S NAME AND SPECIFICATIONS ACCORDING TO THE LISTING OF THAT MANUFACTURER ON A SEPARATE SHEET AND NOT CONSIDERED WITH ANY OTHER PLANT.
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  7. ALL PLANTERS SHALL BE BRANDED AND LINED TO MATCH THE SHAL PLANTING SCHEDULE WITHIN THE 15' STRIP.



**1001 PROPERTIES, LLC**

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 BELLEVUE, WA 98004  
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 BELLEVUE, WA 98004

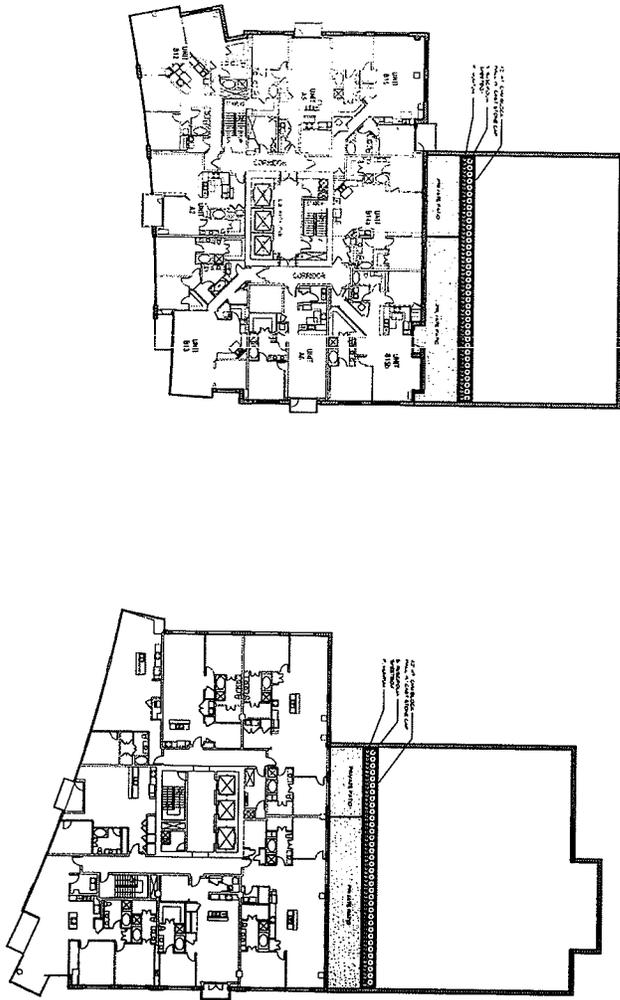


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 Phone: 206.461.2151 | Fax: 206.461.2222

**NOT FOR CONSTRUCTION**

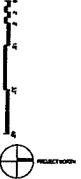
DATE: 04/15/11  
 TIME: 10:00 AM  
 DESIGN REVIEW  
 LANDSCAPE PLAN  
 LEVEL: 04

L1-02



NO.	DESCRIPTION	DATE	BY	CHECKED
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- NOTES:**
1. MATERIALS SHALL BE SPECIFIED AND KEYS TO AND TYPED IN ALL PLANNING SHEETS WHEN APPLICABLE. MATERIALS SHALL BE SPECIFIED AND TYPED IN ALL PLANNING SHEETS WHEN APPLICABLE.
  2. ALL TOILETS AND PLUMBING SHALL BE ON AN AUTOMATIC FLUSHING SYSTEM VIA A CLOCK AND PHOTO-EYE.
  3. ALL PLUMBING SHALL BE DESIGNED BY A LICENSED PLUMBER TO BE DESIGNED BY A LICENSED PLUMBER.
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L1-03

**NOT FOR CONSTRUCTION**

DESIGN REVIEW  
LANDSCAPE PLAN  
LEVEL 09



1001 PROPERTIES, LLC

**BELLEVUE - CADILLAC SITE**

PHASE I  
10500 N.E. 10TH STREET  
BELLEVUE, WA 98004

PHASE II  
10600 N.E. 10TH STREET  
BELLEVUE, WA 98004



**carrierjohnson**

architects for urban environments

1201 8th Avenue, Suite 2000 | Seattle, WA 98101  
phone 206.461.2200 | fax 206.461.2222

**ATTACHMENT B**  
**(Binding Site Plan)**

# OWNER'S CERTIFICATE & DEDICATION

KNOW ALL PEOPLE BY THESE PRESENTS THAT WE, THE UNDERSIGNED OWNERS OF INTEREST IN THE LAND HEREBY SUBDIVIDED, DECLARE THIS BINDING SITE PLAN TO BE THE GRAPHIC REPRESENTATION OF THE BINDING SITE PLAN MADE HEREBY AND DEDICATE TO THE PUBLIC FOREVER ALL THE STREETS AND AVENUES NOT SHOWN AS PRIVATE. WE DEDICATE THE USE OF THE STREETS AND AVENUES NOT SHOWN AS PRIVATE FOR ALL PUBLIC PURPOSES NOT INCONSISTENT WITH THE USE OF THE PUBLIC ALL THE RIGHT TO MAKE ALL NECESSARY SLOPES FOR THE PUBLIC ALL THE EASEMENTS SHOWN ON THIS BINDING SITE PLAN FOR ALL PUBLIC PURPOSES AS INDICATED, INCLUDING BUT NOT LIMITED TO PARKS, OPEN SPACE, UTILITIES, AND DRAINAGE--UNLESS SUCH EASEMENTS OR TRACTS ARE SPECIFICALLY IDENTIFIED ON THIS BINDING SITE PLAN AS BEING PUBLIC, IN WHICH CASE WE MAKE SUCH STREETS, EASEMENTS, OR TRACTS TO THE PERSON OR ENTITY IDENTIFIED AND FOR THE PURPOSE STATED.

FURTHER, WE WAIVE FOR OURSELVES, FOR OUR HEIRS AND ASSIGNS, AND FOR ANY PERSON OR ENTITY DERIVING TITLE FROM THE UNDERSIGNED ANY AND ALL CLAIMS FOR DAMAGES AGAINST THE CITY OF BELLEVUE AND ITS LAWS OF THIS DIVISIONS WHICH MAY BE OCCASIONED TO THE ADVANCEMENT OR MAINTENANCE OF ROADS AND/OR DRAINAGE SYSTEMS WITHIN THIS BINDING SITE PLAN.

THIS BINDING SITE PLAN, DEDICATION, AND WAIVER OF CLAIMS IS MADE WITH FREE CONSENT AND IN ACCORDANCE WITH OUR DESIRES.

1001 PROPERTIES, LLC, A DELAWARE LIMITED LIABILITY COMPANY

BY: KATHY K. BINFORD  
ITS: VICE PRESIDENT  
DATE: \_\_\_\_\_

## ACKNOWLEDGEMENTS

STATE OF TEXAS )  
COUNTY OF HARRIS ) SS

ON THIS DAY PERSONALLY APPEARED BEFORE ME KATHY K. BINFORD, TO ME KNOWN TO BE THE VICE PRESIDENT OF 1001 PROPERTIES, LLC, A LIMITED LIABILITY COMPANY DESCRIBED IN THE FOREGOING INSTRUMENT, HEREBY ALLEGED THE SAID INSTRUMENT TO BE THE FREE AND VOLUNTARY ACT AND DEED OF SAID PERSON, HEREBY AUTHORIZED HER TO SIGN AND TO EXECUTE THE SAID INSTRUMENT ON BEHALF OF SAID LIMITED LIABILITY COMPANY.

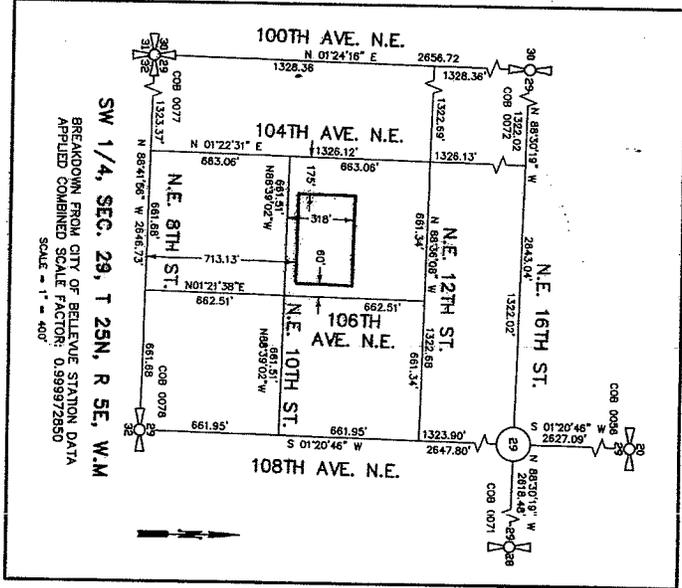
GIVEN UNDER MY HAND AND OFFICIAL SEAL THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 200\_\_.

NOTARY PUBLIC

## LEGAL DESCRIPTION

ORIGINAL PARCEL:  
THAT PORTION OF THE FOUR 316 FEET OF THE NORTHWEST QUARTER OF THE SOUTH-EAST QUARTER OF THE SOUTHWEST QUARTER SECTION 29, T29N, R5E, W1M, LINGUE EAST OF THE WEST 175 FEET OF SAID NORTHWEST QUARTER.  
EXCEPT THAT PORTION THEREOF LYING WITHIN THE SOUTH 713.13 FEET OF SAID SECTION 29.  
EXCEPT THE EAST 90 FEET THEREOF COMMERCE TO THE CITY OF BELLEVUE FOR TOWN VENTURE NORTH-EAST BY DEEDS RECORDED UNDER RECORDING NOS. 071218 AND 800744.  
AND EXCEPT THAT PORTION THEREOF COMMERCE FOR STREET PURPOSES IN KING COUNTY SUPERIOR COURT CASE NO. 80-2-03246-3.  
SITUATE IN THE CITY OF BELLEVUE, COUNTY OF KING, STATE OF WASHINGTON.

(TITLE REPORT REFERENCE:  
THIS SURVEY WAS CONDUCTED ACCORDING TO THE DESCRIPTION SHOWN, FINISHED BY THE SURVEYOR'S COMMENT NO. 1138322, DATED JULY 10, 2008. THE EASEMENTS SHOWN OR NOTED HEREIN RELATE TO THIS COMMENTARY.)



### LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- CENTER OF SECTION
- CITY OF BELLEVUE STATION DATA NUMBER

NOTE:  
1. THE FAR FOR BOTH PHASES I AND II IS AT ITS MAXIMUM. NO ADDITIONAL BUILDING AREA SHALL BE ADDED EXCEPT THAT WHICH IS NOTED WITHIN LUC 20.50.020 (DEFINITIONS), PAR. 7.

SE 1/4, SW 1/4, SEC. 29 TWP. 25 N., RGE. 5 E., W1M.

TRANSPORTATION DEPARTMENT  
EXAMINED AND APPROVED:

REAL PROPERTY & SURVEY  
DATE

DEVELOPMENT REVIEW  
DATE

UTILITIES DEPARTMENT  
EXAMINED AND APPROVED:

ADMINISTRATOR  
DATE

DEVELOPMENT SERVICES DEPARTMENT  
EXAMINED AND APPROVED:

ADMINISTRATOR  
DATE

KING CO. DEPT. OF ASSESSMENTS  
EXAMINED AND APPROVED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_

ASSESSOR  
DEPUTY ASSESSOR

ACCOUNT NO.

### RECORDER'S CERTIFICATE

FILED FOR RECORD THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_  
AT \_\_\_\_\_ (M, IN BOOK \_\_\_\_\_ OF SURVEYS/AT PAGES) \_\_\_\_\_  
AT THE REQUEST OF THE CITY OF BELLEVUE.

MANAGER  
SUPT. OF RECORDS  
RECORDING NO.

### SURVEYOR'S CERTIFICATE

THIS MAP CORRECTLY REPRESENTS A SURVEY MADE BY ME OR UNDER MY DIRECTION IN CONFORMANCE WITH THE REQUIREMENTS OF THE SURVEY RECORRING ACT AT THE REQUEST OF 1001 PROPERTIES, LLC, A DELAWARE LIMITED LIABILITY COMPANY IN JANUARY 2008.



**BUSH, ROED & HITCHINGS, INC.**  
CIVIL ENGINEERS  
& LAND SURVEYORS  
2009 LINGUE AVENUE, SUITE 6207  
SEATTLE, WASHINGTON 98102-5313  
(206) 323-4144



BINDING SITE PLAN NO.: 08-112067-LD

COB 0447  
 P.D. MON. IN CASE  
 0.22' N. OF SUBDIVISION LINE  
 FOR 106TH AVE. N.E.  
 VISITED 8-20-07  
 GND E 10350014930

**EXISTING EASEMENTS:**

THIS REPORT REFERENCE:  
 THIS SURVEY WAS CONDUCTED ACCORDING TO THE DESCRIPTION SHOWN,  
 THE SURVEYOR HAS REVIEWED THE RECORDS OF THE DISTRICT AND  
 1:500 SCALE PLANS TO DETERMINE THE EXISTING EASEMENTS SHOWN ON THE  
 HEREIN RELATE TO THIS COMMITMENT.  
 NOTE: EASEMENTS CREATED OR RESIGNED AFTER THIS DATE ARE NOT  
 SHOWN OR NOTED HEREON.  
 THE REPORT CONTAINS NO EXCEPTIONS:  
 ITEMS CHECKED ARE SHOWN ON PLAN.

7. EASEMENT AND THE TERMS AND CONDITIONS THEREOF:  
 GRANTEE: BELLEVUE SEWER DISTRICT  
 GRANTEE: BELLEVUE SEWER DISTRICT  
 PURPOSE: SEWER PIPE LINE OR LINES  
 RECORDED: JANUARY 14, 1980  
 RECORDING NO.: 5121453  
 THIS EASEMENT FALLS WITHIN THE  
 DISTRICT OF BELLEVUE SEWER DISTRICT  
 NE

8. EASEMENT AND THE TERMS AND CONDITIONS THEREOF:  
 PURPOSE: SEWER PIPELINE  
 AREA AFFECTED: A PORTION OF SAID PREMISES  
 RECORDED: FEBRUARY 14, 1980  
 RECORDING NO.: 5121453  
 THIS EASEMENT FALLS WITHIN THE  
 DISTRICT OF BELLEVUE SEWER DISTRICT  
 NE

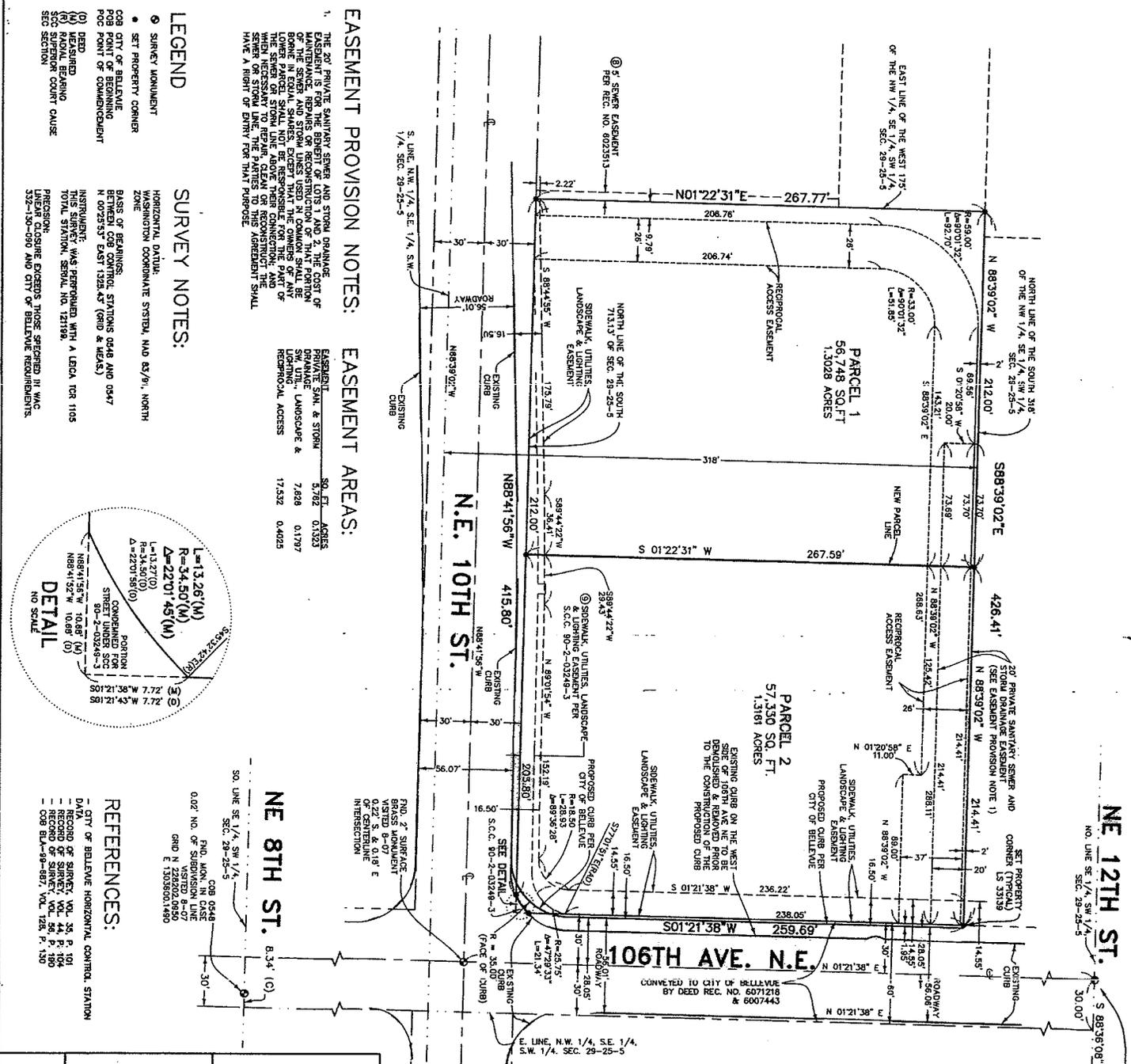
9. EASEMENT AND THE TERMS AND CONDITIONS THEREOF:  
 PURPOSE: SEWER PIPELINE  
 AREA AFFECTED: A PORTION OF SAID PREMISES  
 RECORDED: FEBRUARY 14, 1980  
 RECORDING NO.: 5121453  
 THIS EASEMENT FALLS WITHIN THE  
 DISTRICT OF BELLEVUE SEWER DISTRICT  
 NE

10. EASEMENT AND THE TERMS AND CONDITIONS THEREOF:  
 PURPOSE: SEWER PIPELINE  
 AREA AFFECTED: A PORTION OF SAID PREMISES  
 RECORDED: FEBRUARY 14, 1980  
 RECORDING NO.: 5121453  
 THIS EASEMENT FALLS WITHIN THE  
 DISTRICT OF BELLEVUE SEWER DISTRICT  
 NE

11. EASEMENT AND THE TERMS AND CONDITIONS THEREOF:  
 PURPOSE: SEWER PIPELINE  
 AREA AFFECTED: A PORTION OF SAID PREMISES  
 RECORDED: FEBRUARY 14, 1980  
 RECORDING NO.: 5121453  
 THIS EASEMENT FALLS WITHIN THE  
 DISTRICT OF BELLEVUE SEWER DISTRICT  
 NE

12. EASEMENT AND THE TERMS AND CONDITIONS THEREOF:  
 PURPOSE: SEWER PIPELINE  
 AREA AFFECTED: A PORTION OF SAID PREMISES  
 RECORDED: FEBRUARY 14, 1980  
 RECORDING NO.: 5121453  
 THIS EASEMENT FALLS WITHIN THE  
 DISTRICT OF BELLEVUE SEWER DISTRICT  
 NE

13. EASEMENT AND THE TERMS AND CONDITIONS THEREOF:  
 PURPOSE: SEWER PIPELINE  
 AREA AFFECTED: A PORTION OF SAID PREMISES  
 RECORDED: FEBRUARY 14, 1980  
 RECORDING NO.: 5121453  
 THIS EASEMENT FALLS WITHIN THE  
 DISTRICT OF BELLEVUE SEWER DISTRICT  
 NE



**EASEMENT PROVISION NOTES:**

1. THE 20' PRIVATE SANITARY SEWER AND STORM DRAINAGE EASEMENT IS FOR THE BENEFIT OF LOTS 1 AND 2. THE COST OF MAINTENANCE, REPAIRS OR RECONSTRUCTION OF THAT PORTION OF THE SEWER AND STORM DRAINAGE SYSTEM WHICH SHALL BE BORNE BY THE SEWER DISTRICT SHALL BE BORNE BY THE SEWER DISTRICT. THE SEWER OR STORM LINE ABOVE THEIR CONNECTION, AND THE SEWER OR STORM LINE, THE PARTIES TO THIS AGREEMENT SHALL HAVE A RIGHT OF ENTRY FOR THAT PURPOSE.

**EASEMENT AREAS:**

EASEMENT	90 FT. AREAS	57.62	0.1583
PRIVATE SANITARY & STORM DRAINAGE	7.828	0.1797	
SW UTIL. LANDSCAPE & RECREATIONAL ACCESS	17.532	0.4025	

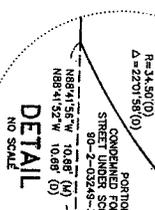
**LEGEND**

- ① SURVEY MONUMENT
- ② SET PROPERTY CORNER
- ③ COB POINT OF BEGINNING
- ④ POINT OF COMMENCEMENT
- ⑤ DEED
- ⑥ RECORDED
- ⑦ SEC SUPERIOR COURT CAUSE
- ⑧ SECTION

**SURVEY NOTES:**

HORIZONTAL DATUM: WASHINGTON COORDINATE SYSTEM, NAD 83/91, NORTH ZONE  
 BASIS OF BEARINGS: BEST AVAILABLE STATIONS 0448 AND 0547  
 COB POINT OF BEGINNING: N 09°23'51" EAST 125.43' (GND & MEAS.)  
 INSTRUMENT: WGA APPROVED WITH A LEICA TOR 1103 TOTAL STATION, SERIAL NO. 121819.  
 PRECISION: WITHIN THE TOLERANCES SPECIFIED IN IAC 132-1-30-090 AND CITY OF BELLEVUE REQUIREMENTS.

**DETAIL**



**REFERENCES:**

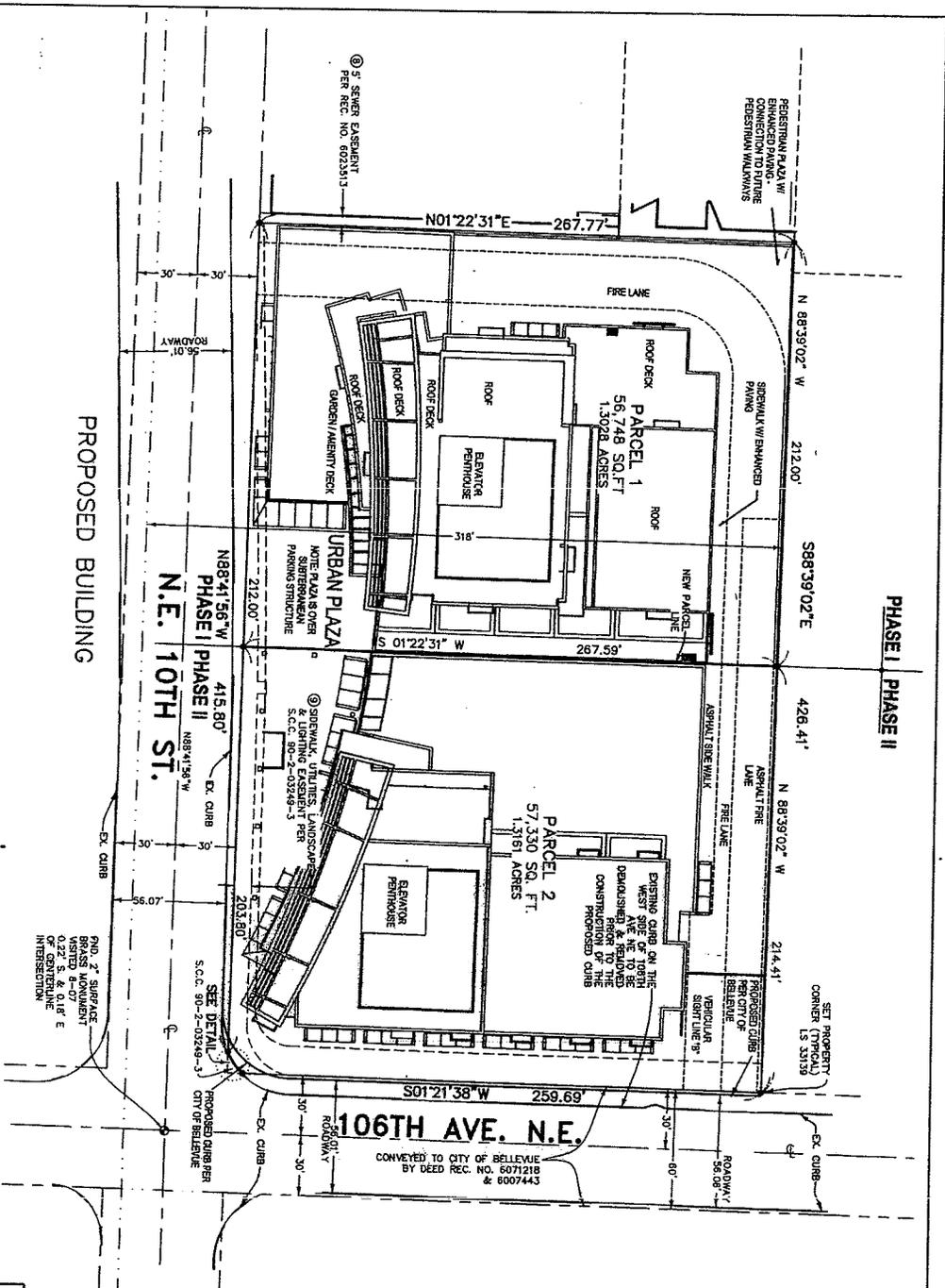
- CITY OF BELLEVUE HORIZONTAL CONTROL STATION DATA
- RECORD OF SURVEY VOL. 24, P. 101
- RECORD OF SURVEY VOL. 54, P. 199
- COB BLA-99-887, VOL. 125, P. 130



**BUSH, ROED & HITCHINGS, INC.**  
 CIVIL ENGINEERS  
 & LAND SURVEYORS  
 2009 W. MAJOR AVENUE EAST  
 SEATTLE, WASHINGTON 98102-3613  
 (206) 323-4144

**City of Bellevue**

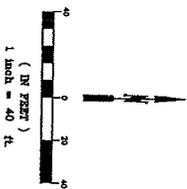
BINDING SITE PLAN NO.: 08-112057-LD



PROPOSED BUILDING

N.E. 10TH ST.

106TH AVE. N.E.



**City of  
Bellevue**

BINDING SITE PLAN NO.: 08-112057-LD

SHEET 3 OF 3

DATE: 07/20/08



**BLUSH, ROID & HITCHINGS, INC.**  
 CIVIL ENGINEERS  
 & LAND SURVEYORS  
 2009 LINCOLN AVENUE EAST  
 SEATTLE, WASHINGTON 98102-3513  
 (206) 323-4144



**ATTACHMENT C**  
**(Certificate of Concurrence)**

## CERTIFICATE OF CONCURRENCY

### HANOVER CADILLAC

This certificate documents the Transportation Department Director's decision that the development project at 1001 106<sup>th</sup> Avenue NE (Design Review File No. 08-112057 LD) complies with the requirements of the Traffic Standards Code (BCC 14.10). This decision reserves 94 p.m. peak hour trips to this two-phased project, subject to Process II appeal of either the concurrency determination or the Design Review decision. A building permit application (File No. 08-128729 BB) was filed for the project on August 5<sup>th</sup>, 2008 and deemed complete on September 2nd, 2008. This concurrency reservation will remain in effect for the life of the building permit application (BCC 23.05.090.H). Upon issuance of the building permit, concurrency is reserved for one year; the applicant may request up to two one-year extensions (BCC 23.05.100.E).

  
Director, Transportation Department

11/12/08  
Date

Certificate No. 51