



DEVELOPMENT SERVICES DEPARTMENT
ENVIRONMENTAL COORDINATOR
450 110th Avenue NE, P.O. BOX 90012
BELLEVUE, WA 98009-9012

DETERMINATION OF NON-SIGNIFICANCE

PROPONENT: Wright Runstad & Company
Tara Howard, (206) 805-5850

LOCATION OF PROPOSAL: 1255 123rd Avenue NE

DESCRIPTION OF PROPOSAL: Construction of a 3-story office building consisting of approximately 90,390 square feet of university/office space and below grade parking, and 3,473 square feet of ground floor retail space. There will be 2 levels of underground parking with approximately 210 parking stalls. Site improvements include street improvements, site utilities, landscaping, and pedestrian circulation. The proposed structure will be used for the Global Innovation Exchange (GIX), a joint venture between the University of Washington and Tsinghua University, to create a technology master's program. The project limit area is 1.4 acres.

FILE NUMBERS: 15-126163-LD **PLANNER:** Laurie Tyler, Associate Land Use Planner

The Environmental Coordinator of the City of Bellevue has determined that this proposal does not have a probable significant adverse impact upon the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(C). This decision was made after the Bellevue Environmental Coordinator reviewed the completed environmental checklist and information filed with the Land Use Division of the Development Services Department. This information is available to the public on request.

- There is no comment period for this DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's office by 5:00 p.m. on _____.
- This DNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on the DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's Office by 5 p.m. on **5/26/2016**
- This DNS is issued under WAC 197-11-340(2) and is subject to a 14-day comment period from the date below. Comments must be submitted by 5 p.m. on _____. This DNS is also subject to appeal. A written appeal must be filed in the City Clerk's Office by 5:00 p.m. on _____.

This DNS may be withdrawn at any time if the proposal is modified so as to have significant adverse environmental impacts; if there is significant new information indicating a proposals probable significant adverse environmental impacts (unless a non-exempt license has been issued if the proposal is a private project); or if the DNS was procured by misrepresentation or lack of material disclosure.

Carle Heller
Environmental Coordinator

5/12/16
Date

OTHERS TO RECEIVE THIS DOCUMENT:

- State Department of Fish and Wildlife / Stewart.Reinbold@dfw.gov; Christa.Heller@dfw.wa.gov;
- State Department of Ecology, Shoreline Planner N.W. Region / Jobu461@ecy.wa.gov; sepaunit@ecy.wa.gov
- Army Corps of Engineers Susan.M.Powell@nws02.usace.army.mil
- Attorney General ecyolyef@atg.wa.gov
- Muckleshoot Indian Tribe Karen.Walter@muckleshoot.nsn.us; Fisheries.fileroom@muckleshoot.nsn.us



**City of Bellevue
Development Services Department
Land Use Staff Report**

Proposal Name: The Spring District Block 14 – Global Innovation Exchange (GIX)

Proposal Address: 1255 123rd Avenue NE

Proposal Description: Construction of a 3-story office building consisting of approximately 90,390 square feet of university/office space and below grade parking, and 3,473 square feet of ground floor retail space. There will be 2 levels of underground parking with approximately 210 parking stalls. Site improvements include street improvements, site utilities, landscaping, and pedestrian circulation. The proposed structure will be used for the Global Innovation Exchange (GIX), a joint venture between the University of Washington and Tsinghua University, to create a technology master's program. The project limit area is 1.4 acres.

File Numbers: 15-126163-LD

Applicant: Wright Runstad & Company

Decisions Included: Combined Design Review and SEPA Determination

Process: Process II, (LUC 20.35.200)

Planner: Laurie Tyler, Associate Land Use Planner, 425-452-2728

Threshold Determination: State Environmental Policy Act (SEPA): Determination of Non-Significance with incorporation by reference of the Bel-Red Corridor Project Draft and Final Environmental Impact Statements issued July 19, 2007

**Carol V. Helland, Environmental Coordinator
Development Services Department**

Director's Decision: Approval with Conditions
Michael A. Brennan, Director
Development Services Department

By Carol V. Helland, Land Use Director

Application Date: 11-02-2015
Completeness Date: 11-19-2015
Notice of Decision Date: 05-12-2016
Appeal Deadline: 05-26-2016 at 5pm

For information on how to appeal a proposal, visit the Development Services Center at City Hall or call (425) 452-6800. Comments on State Environmental Policy Act (SEPA) Determinations can be made with or without appealing the DR decision within the noted comment period for a SEPA Determination. Appeals must be received in the City Clerk's Office by 5 pm on the date noted for appeal of the decision.

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- Attachments:
- A - Project Plans
 - B - Environmental Checklist
 - C - Development Agreement signed on July 10, 2009 and recorded September 11, 2009 (Copy in project file)
 - D - Technical Memo by JMJ Team dated May 10, 2013 regarding Greenhouse Gas Emissions – Phase 1A Update
 - E - Geotechnical Report by Hart Crowser dated October 27, 2015 (Copy in project file)
 - F - FAR Amenity Incentive System Spreadsheet (Land Use Code 20.25D.090)
 - G – Parking Analysis prepared by Parametrix, dated April 5, 2016
 - H – Impervious Surface/Lot Coverage Cumulative Memo, dated April 5, 2016
 - I - Certificate of Concurrence

I. REQUEST and REVIEW PROCESS

The applicant requests Design Review approval and a Threshold Determination under the State Environmental Policy Act (SEPA) to develop the Global Innovation Exchange building on Block 14 of The Spring District Master Development Plan (“MDP”).¹ The total square footage of the proposed structure is 93,863 gross square feet, excluding the parking areas. There will be 2 levels of underground parking for 210 parking stalls. The total site area for the building and associated improvements is 1.4 acres. Other site improvements will include site utilities, landscaping, streets, sidewalks, and pedestrian/bicycle amenities.

The Global Innovation Exchange (GIX) is a joint venture between the University of Washington and Tsinghua University in China, creating a Master’s program for technology students. GIX further enhances The Spring District by providing a unique university/makers lab/classroom use to the emerging neighborhood. In addition, ground floor retail will activate the pedestrian realm along NE District Way.

**The Spring District
Project Site within Master Development Plan Boundary**

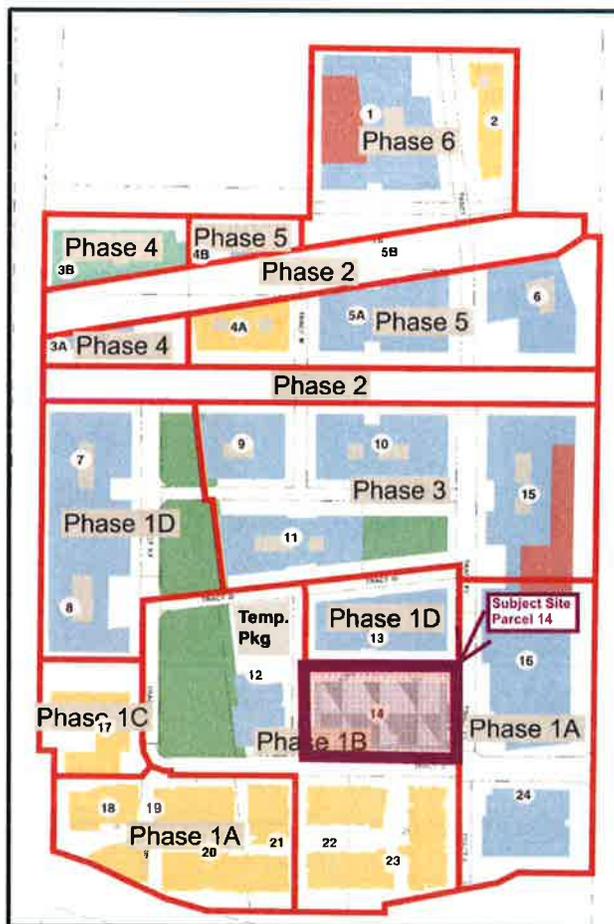


¹ The MDP was affirmed on appeal by the Hearing Examiner on October 8, 2012. Development Services File reference 11-125943-LP; Hearing Examiner File reference: AAD 12-41.

A. Site Design

The project site is part of Phase 1B of The Spring District Master Development Plan. The subject design review application is for only part of the improvements within Phase 1B, and includes one building on Parcel 14, and continued construction of 123rd Avenue NE (public) and 122nd Avenue NE (private). Phase 1B of The Spring District Master Development Plan includes the development of the Spring Phase 2 Residential project on parcels 22 and 23, development of the Brewpub on parcel 12, and the existing developed community park located at the corner of NE District Way and 121st Avenue NE.

The Spring District MDP Phasing Plan (Amended April, 2016)



The project site is generally flat and rectangular, measuring approximately 301.4' feet from east to west and approximately 197.5 feet from north to south. The applicant recorded a Binding Site Plan (May 6, 2016, King County Recording Number 20160506000760) which allows for flexibility in site development, including building location, shared open space, and shared parking.

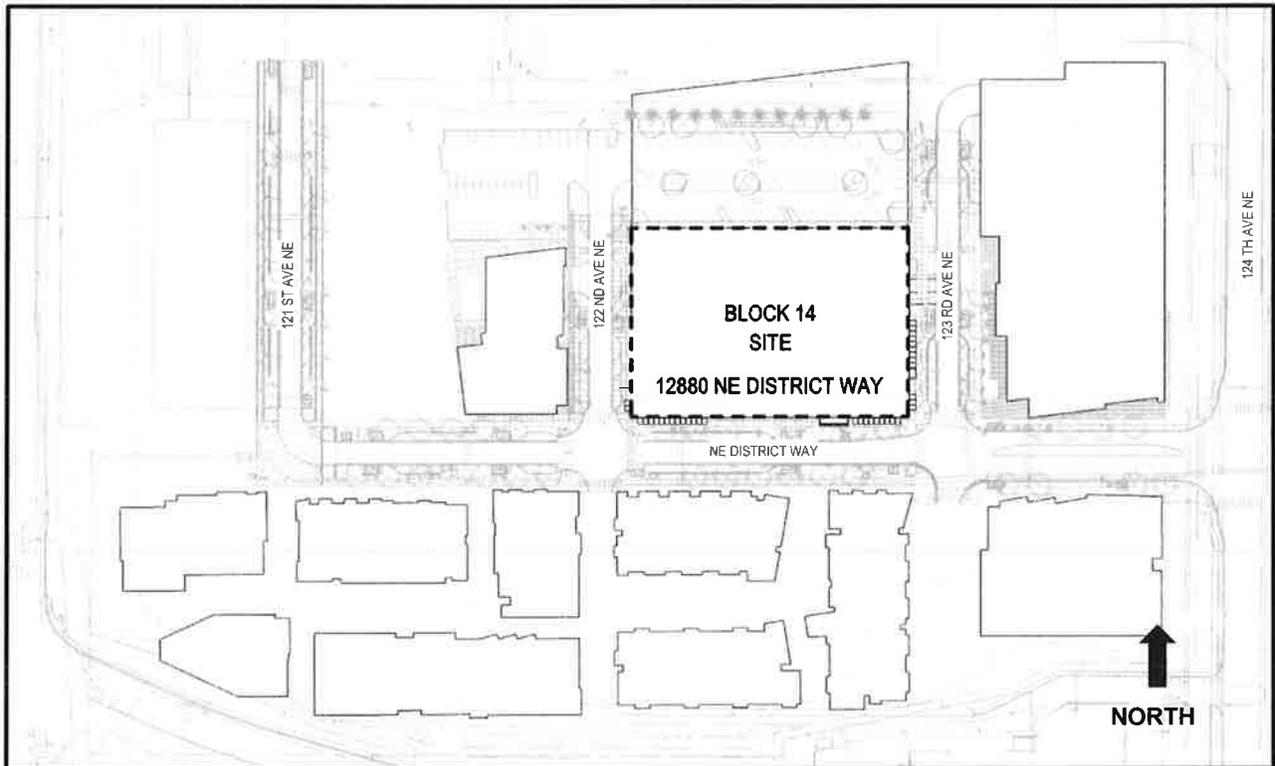
Vehicular access to the site will be from NE District Way (public), 123rd Avenue NE (public) and 122nd Avenue NE (private). A singular garage access point will be located at the northeast corner of the building, via 123rd Avenue NE. New segments of both 123rd Avenue NE and 122nd

Avenue NE adjacent to the project site will be developed, and will later extend north as project sites north of the site develop in the future.

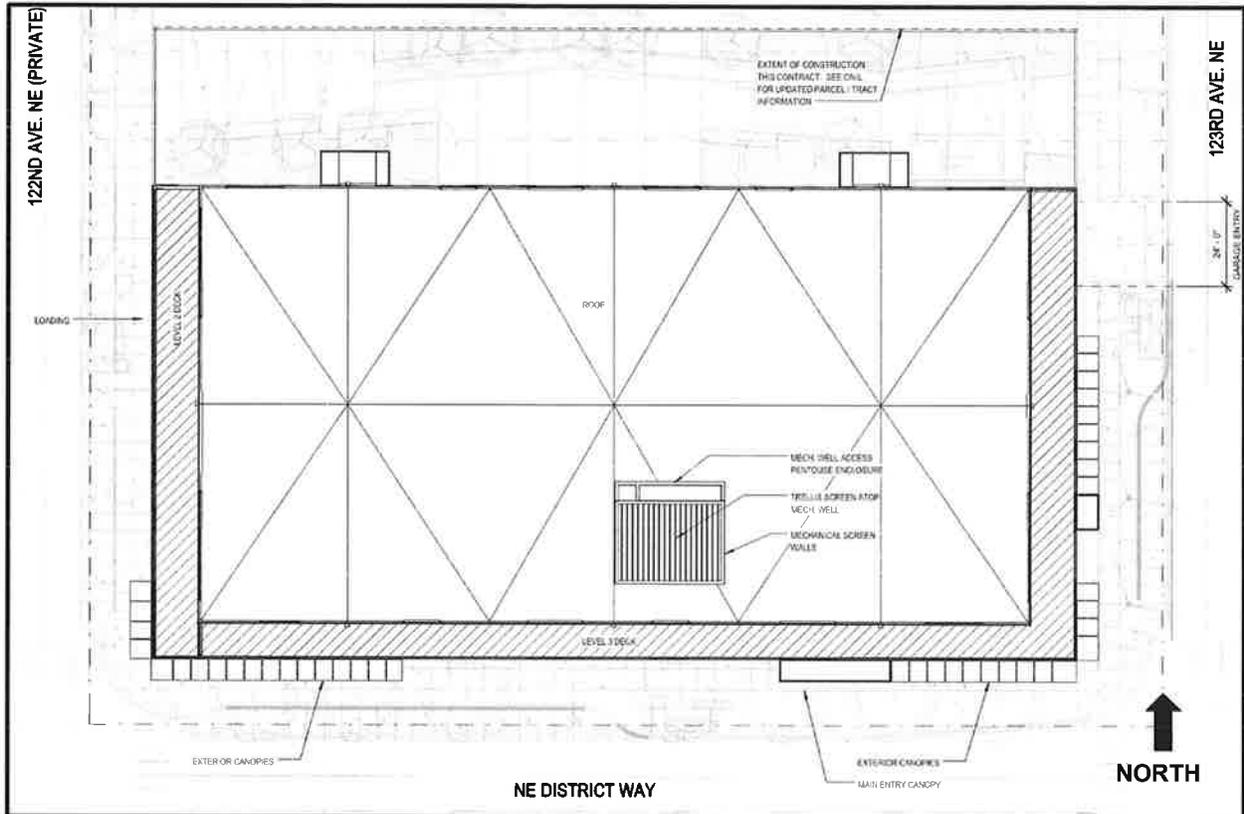
The primary pedestrian entrance to the building will be from NE District Way. Two small retail spaces on both the southeast and southwest corners of the building will also have individual entries on NE District Way. A third retail space along 123rd Avenue NE will have its own entry via 123rd Avenue NE.

The proposed project incorporates 6-foot wide sidewalks plus 5-foot wide planting areas or tree wells along NE District Way and portions of 122st Avenue NE and 123rd Avenue NE. An outdoor plaza area on the north side of the building will provide an area for students to congregate, as well as provide a pedestrian/bicycle connection between 123rd Avenue NE and 122nd Avenue NE. On-street parking and other streetscape features will be implemented consistent with the Bel-Red Design Guidelines.

Project Area



Site Plan



B. Building Design – Global Innovation Exchange

The Global Innovation Exchange building will be located on property identified in The Spring District Master Plan as Parcel 14. The building will contain three levels of university/office space, and include two small ground floor retail spaces. The building facades will consist of large bays of vertical glass combined with a solid surface such as metal panels and brick. The building will have large floor plates to maximize daylight penetration into the interior spaces. An angular roof profile, with a total height of approximately 55 feet at the highest point in the roof line, provides for a unique building design. The second floor of the structure will have an approximately fifteen foot deep outdoor deck space on the west side of the building. The third floor will also have an approximately fifteen foot deep outdoor deck on the east side of the building which wraps around to the south side of the building. Additional outdoor space includes a pedestrian plaza to the north of the building to activate the interaction between the building and the neighborhood. Loading and refuse/recycling pick-up for the building will be provided at the ground level on the west side of the structure.

Table 1 – Global Innovation Exchange

Global Innovation Exchange Building/Use	Gross Floor Area (GFA) See definition of "Floor Area, Gross." LUC 20.50.020
University/Office	86,841
Ground Floor Retail	3,473

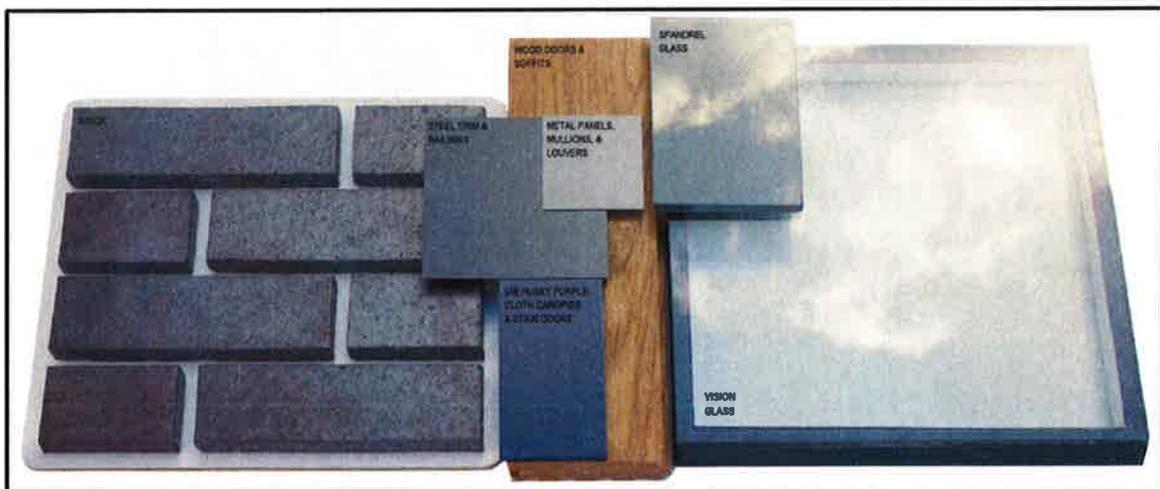
Building Materials:

Materials have been chosen specifically to address durability (weathering characteristics), the human-scale (masonry modules sized to fit in a hand), and forms that are reflective of the building process (rolled steel and metal shapes, molded brick, ground face aggregates, etc.). This is consistent with the vision for the future Bel-Red Corridor as expressed in *The Transportation Design Manual Appendix B – Bel-Red Corridor Plan: Streetscape Character, Guidelines, and Standards* (updated March 2015) and the Bel Red Subarea Plan (2010), and Land Use Code 20.25D.150 Design Guidelines for the Bel Red Corridor.

Massing, fenestration, and exterior materials are intended to convey a strong rhythm of solid/void along the elevations. Brick, steel and wood are used to evoke the industrial heritage of the site, with updated detailing for the future. Detailed articulation of the materials and details will convey a feeling of quality and permanence. Large bays of vertical glass help to maximize daylight penetration, and when combined with a solid surface such as the ironspot brick, will help to convey an expression of "creative warehouse" with straightforward materials and careful attention to details of the structure.

The building will have several low impact development features and will seek Leadership in Energy and Environmental Design (LEED) certification. LEED features may include LED lighting, sustainable or renewable materials, and the purchase of local building materials to limit truck transit.

Building Materials



C. Design Intent

The proposed project was designed to meet the requirements of the approved Master Development Plan and advance the goals of the adopted Bel-Red Subarea Plan.

The site lies near the southern edge of the Spring District MDP property, directly north of the Phase 2 Residential project located across NE District Way to the south, and east of the Brew Pub project across 122nd Avenue NE, to the west. A previously permitted office building (Block 16) is anticipated to be constructed across 123rd Avenue NE, east of the site. The proposed light rail station will be located north of the subject site.

**Corner of NE District Way & 123rd
Facing Northwest**



**Aerial View – NE District Way & 122nd
Looking Northeast**



Northeast Corner Viewed from 122nd Ave



Aerial View – Northeast Corner Viewed from 123rd Ave



As stated in the MDP staff report (page 5 of 74), The Spring District goals are as follows:

- Developing a new urban community composed of high quality regional business, commercial, retail, residential, and open spaces that are achieved with compact and sustainable urban growth.
- Establishing neighborhood identity.

- Creating a world class project that is a catalyst for new development throughout The Spring District and the Bel-Red Corridor in order to help develop a vibrant new district with a diversified economic base.
- Creating a project that is complementary to Downtown Bellevue.
- Creating a new business environment that is flexible for future market demands and achieves an economically sustainable future.
- Providing vibrant new parks, plazas, and open spaces that are environmentally responsive and encourage social interaction.

The proposed Global Innovation Exchange (GIX) building will establish the first educational use within The Spring District, which further diversifies the mix of uses within the neighborhood. As GIX is a joint venture between the University of Washington and Tsinghua University in China, this use will provide a unique university and maker's lab to this emerging neighborhood. In addition, the project will also provide small ground floor retail uses, which will complement the educational use, in addition to further activation of the pedestrian realm along NE District Way.

The proposed streetscape along NE District Way, 123rd Avenue NE and 122nd Avenue NE will be installed to match the existing streetscape within The Spring District MDP, including plantings, street trees, street lighting and street furniture. This streetscape design provides for shade, seating, rain/wind protection and a pleasant pedestrian experience.

The proposed outdoor plaza along the north side of the building has been designed to complement the university use. Large bay doors will open out toward the plaza at the first floor, allowing for the outside to come into the building at this level. A slight meandering pathway extending the length of the plaza will be bordered on both sides with lush landscaping, including areas of tree canopy. The plaza also incorporates areas for respite and activities, allowing for a peaceful space for students to congregate outdoors during the summer months.

A pedestrian connection will be established along the north side of the building, between 123rd Avenue NE and 122nd Avenue NE, within the proposed outdoor plaza area. **Refer to Section XI.C.18 for Condition of Approval regarding Sidewalk/Utility/Access Easements.** This pedestrian connection will provide a means to walk/cycle westward, from 123rd Avenue NE over to the existing park area, and then north to the future light rail station.

D. Review Processes

Design Review applications are Process II applications (LUC 20.35.200) with an administrative decision by the Director of Development Services (LUC 20.30V). The SEPA Determination is also a Process II decision with an administrative decision by the Environmental Coordinator. Appeals are heard and decided by the Hearing Examiner for Process II applications.

Any modification to this approval shall be documented as an Amendment to this Design Review, or as an addition/revision to this issued land use approval, processed as a Land Use Exemption. The applicant shall demonstrate compliance with the Land Use Code consistent with the Governing Regulations section of the Development Agreement. Any modification of the project design must be reviewed for consistency with the design intent as stated in this report.

Conditions of Approval run for the life of the project. **Refer to Section XI.A.8 for Condition of Approval regarding Modification to the Design Review Plans.**

II. SITE DESCRIPTION AND CONTEXT

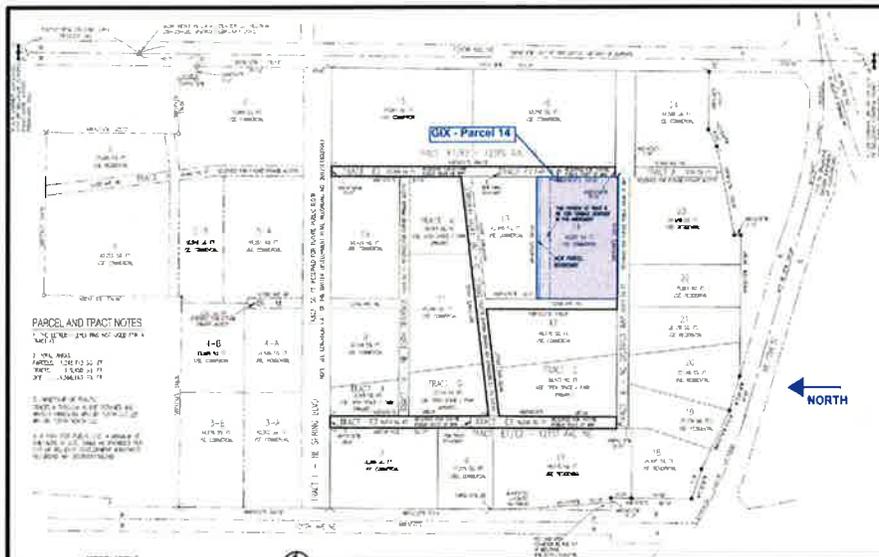
A. Site Description

The proposal site consists of approximately 1.4 acres of the entire 36-acre Spring District Master Development Plan. The MDP is located within the southwestern portion of the City's Bel-Red Corridor, east of downtown, approximately 1/2 mile east of Interstate 405 and 1/3 mile south of State Route 520. The Spring District is bounded by 120th Avenue NE, NE 12th Street, and 124th Avenue NE. The proposal site for this application is located in The Spring District Phase 1B, which is directly north of the southern central portion of The Spring District boundary, corresponding to parcel 14.

Vicinity Map



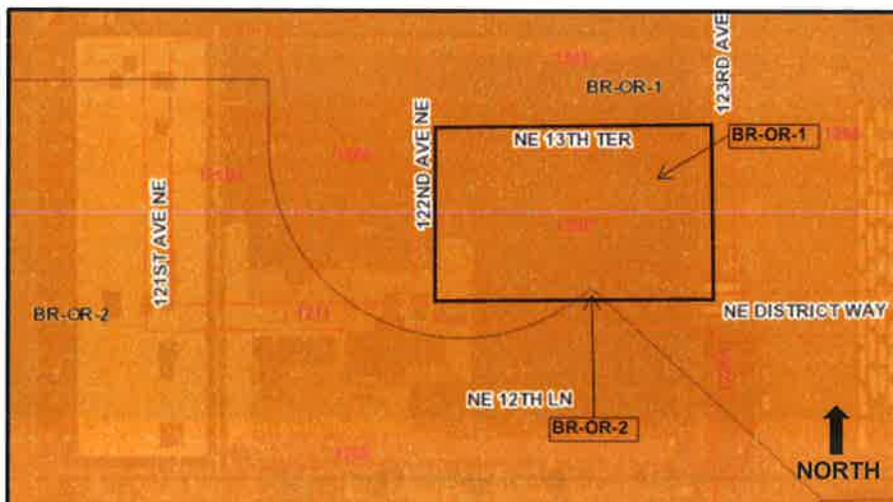
Binding Site Plan
Parcel 14 of Spring District MDP



The subject site is generally flat and is currently unimproved, as several of the existing buildings were demolished as part of The Spring District MDP site infrastructure permit. North of The Spring District is the existing Safeway Ice Cream and Milk production buildings. Coca-Cola is located to the east, across 124th Avenue NE. The Safeway/Albertsons main distribution office is located southeast of the subject site. Office and light industrial uses are located to the northeast and west of the MDP. Retail uses are located south of NE 12th Street. The Lake Bellevue neighborhood (restaurants, retail, office, multi-family uses) is located to the southwest of the MDP. Residential neighborhoods (Wilburton Subarea) are located to the south and southeast of the site.

The subject site is primarily located within the Bel-Red-Office/Residential-1 (BR-OR-1) land use district; however, as shown in the graphic below, a small corner of the southern boundary of the property is located within the Bel-Red-Office/Residential-2 (BR-OR-2) land use district. The site has a Comprehensive Plan Designation of both Bel-Red-Office/Residential-1 and Bel-Red-Office/Residential-2, and is located within the Bel-Red Subarea.

Zoning Map



B. Site Context

The Spring District Master Development Plan was approved as a catalyst project under The Spring District MDP per LUC 20.25D.035. This project is a portion of the overall Master Development Plan proposal. Through a long planning process, the city created a framework to transform the Bel-Red Corridor to meet the city's future residential and economic needs. This proposal is the first educational development under the City's adopted Bel-Red Subarea Plan and related development regulations.

Wright Runstad & Company has received land use approval to develop office Buildings 16 and 24, adjacent to 124th Avenue NE, east of the GIX proposal site. Security Properties is currently developing an approximately 300 unit residential project southwest of the subject site, known as Phase 1, and a 279 unit residential project south of the site, known as Phase 2. A brewpub is currently in review for Parcel 12, west of the subject site.

C. Site History

Beginning in the 1910s, The Spring District property and adjacent areas contained several single-family homes with the balance of the land being used for agricultural purposes. Beginning in the 1950s, Safeway Inc. substantially graded the site to flatten it and facilitate development of distribution and processing plants. Additions and improvements to the warehouses continued into the 1970s. In 2005, Safeway relocated much of their operation to a new plant in Auburn, WA. Also in 2005, the City began a process to rezone the 900-acre Bel-Red corridor from industrial to mixed-use. In 2007, Wright Runstad & Company in partnership with Shorestein Properties purchased 36 acres of surplus Safeway land located around the future 122nd development node. In 2009, the City completed its rezone of the entire Bel-Red Subarea and executed a Development Agreement with The Spring District to be implemented through a new Master Development Plan for the property. The current Design Review application is the fifth application associated with the 2012-approved Spring District Master Development Plan.

III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS

A. General Provisions of the Land Use Code

The site is located in the Bel-Red Land Use district. As such, the proposal is subject to the following Bel-Red requirements of Land Use Code 20.25D:

- Design Review LUC 20.25D.140
- Catalyst Project LUC 20.25D.035
- Bel Red Dimensional Requirements LUC 20.25D.080
- Bel Red FAR Amenity Incentive System LUC 20.25D.090
- Bel Red Landscape Development Standards LUC 20.25D.110
- Bel Red Parking, Circulation and Internal Walkway Requirements LUC 20.25D.120
- Bicycle Parking LUC 20.25D.120.G
- Bel Red Development Standards LUC 20.25D.130
- Bel-Red Street Development Standards LUC 20.25D.140
- Bel Red Design Guidelines LUC 20.25D.150
- Bel-Red Corridor Plan (Appendix B of the Transportation Design Manual)
- The Spring District Master Development Plan File #11-125943-LP, issued October 8, 2012 by Hearing Examiner
- Development Agreement signed July 10, 2009 and recorded September 11, 2009

The building proposed under this application is primarily located within the BR-OR-1 land use district. The Spring District was rezoned in 2009 as part of the City's major initiative to transform the Bel-Red corridor to "a major mixed use employment and residential area characterized by a transit-oriented, nodal development pattern, over time replacing the area's original low intensity light industrial and commercial past." The Spring District Development Agreement (2009) and Master Development Plan (2012) both apply to the site.

The Land Use Code states the following purpose for the BR-OR-1 zoning which **applies to the majority of the site:**

Bel-Red-Office/Residential Node 1 (Bel-Red-OR-1). The purpose of the Bel-Red-OR-1 Land Use District is to provide an area for a mix of office, housing and retail uses within the core of a nodal area, with offices as the predominant use. The district is limited in extent in order to provide the level of intensity appropriate for

areas in close proximity to the highest levels of transit service within the Bel-Red area.

The Land Use Code states the following purpose for the BR-OR-2 zoning, which **applies to the small corner of the southern boundary of the site:**

Bel-Red-Office/Residential Node 2 (Bel-Red-OR-2). The purpose of the Bel-Red-OR-2 Land Use District is to provide an area for a mix of office, housing and retail uses, with office as the predominant use. The district is located within a node but outside the node’s core, and building heights provide for a transition between the node’s core and areas outside the node.

Adjacent development and zoning are as follows:

North: Remainder of The Spring District (BR-OR-1) and Safeway Ice Cream/Milk Plant (BR-OR-1 and BR-OR-2).

South/Southwest: Multi-Family Residential (BR-OR-1 and BR-OR-2).

West: Future Brewpub currently in review (BR-OR-2).

East: Future Office, Building 16 (BR-OR-1).

**Table 2
 Dimensional Requirements**

Item	Required	Proposed	Comments
Land Use Designation	Bel-Red OR-1 Bel-Red OR-2	Bel-Red OR-1 Bel-Red OR-2	No Change Note: No portion of the building within BR-OR-2
Project Limit	No minimum indicated	1.4 acres (59,527 sq. ft.)	Not applicable
Permitted Land Uses Land Use Code 20.25D.070	Universities and Colleges, Office, Retail	University and office with ground level retail	Uses within proposed structures are compliant with Land Use Code
Building Height Land Use Code 20.25D.080.A	BR-OR-1 Base= 45'-0" Maximum= 150'-0" BR-OR-2 Base=45'-0" Maximum=125'-0"	54'-11"	Heights of proposed structure is compliant with Land Use Code.
Floor Area Ratio (FAR) Land Use Code 20.25D.080.A, Development Agreement, Master Development Plan	Per the Development Agreement, development across the MDP property shall have a minimum 2.5 FAR. Land Use Code maximum is 4.0 FAR.	2.97 FAR	Meets the Land Use Code, Development Agreement (DA) and Master Development Plan (MDP). (File # 11-1259943-LP) See condition XI.A.4

Item	Required	Proposed	Comments
Total Gross Square Feet (GSF)	<u>Phase 1B:</u> Minimum 106,024 SF designated Office and minimum 14,778 SF designated as retail development within MDP (per MDP File #11-125943-LP and Development Agreement & 15-129285-LJ)	93,863 GSF University/Office (includes parking levels) 3,473 Retail	Meets MDP & DA for office and retail square footage within the Phase 1B boundary.
Gross Square Feet/ Floor (GSF/F) Above 40 Feet Land Use Code 20.25D.080.A	28,000 GSF	Level 3 = 26,983 GSF	Proposed structure is compliant with Land Use Code
Gross Square Feet/ Floor (GSF/F) Above 80 Feet Land Use Code 20.25D.080.A	28,000 GSF	Not applicable	No occupiable floor level above 80 feet within the project limit
Front, Rear & Side Setbacks Land Use Code 20.25D.080.A (5)	Front 0'-0" (2)	17'-1" from property line 12'-3" from back of curb	No setback required. Footnote (2) does not apply, however, per Footnote (5), a 15' deep stepback is incorporated as exterior deck
	Rear 0'-0" (14)	45'-2"	No setback required. Footnote 14 is not applicable.
	Side 0'-0" (14)	123 rd Ave NE: 22'-4" from PL 15'-5" from curb 122 nd Ave NE: 17'-4" from PL 10'-6" from curb	No setback required. Footnote 14 is not applicable. 123 rd Avenue subject to Footnote (5) which requires 15' stepback

Item	Required	Proposed	Comments
Façade Setbacks Upper level Land Use Code 20.25D.080.A	(2) Where building height exceeds 45 feet, and the building is located within 15 feet of the front property line, the building shall incorporate a 15-foot-deep setback in that facade at a height no more than 40 feet above the average finished grade along that façade	The building incorporates a 15 foot deep setback at the second level, which has been incorporated into the design of the structure as a deck.	Stepback for proposed structure provided as required by Land Use Code
Façade Setbacks Upper Level Land Use Code 20.25D.080.A	(5) A building façade on any street identified as a Required Sidewalk-Oriented Development pursuant to LUC 20.25D.130.C shall incorporate a 15-foot deep setback in that façade at a height no more than 40 feet above the average finished grade along that façade.	The building is subject to this provision on NE District Way and 123 rd Avenue NE. Both sides of the building incorporate 15 foot deep setbacks at the second level, which have been incorporated into the design as deck spaces.	Meets Land Use Code
Maximum Impervious Surface Area/Lot Coverage Land Use Code 20.25D.080.A	Maximum 75% *	87%* (51,788 sq. ft.) Cumulative = 70.8%	Maximum impervious surface/lot coverage shall not exceed 75%, MDP wide. See attachment H. See condition XI.C.21
Street frontage and Sidewalk Width	Street frontage required per Land Use Code 20.25D.110 for local streets (street trees, 5' planter strip). Minimum 6' sidewalk.	Site landscaping provided per Land Use Code 20.25D.110 for local streets (street trees, 5' planter strip). This work, including street trees, has been constructed independently as part of the MDP site infrastructure by Wright-Runstad, under permit # 13-104267-GD.	Meets Land Use Code
Street Tree Caliper and Species	Village Green Zelkova, minimum 2.5" caliper or Greenspire Linden - Tilia cordata "Greenspire"	122 nd & 123 rd Avenue NE: Greenspire Linden – Tilia cordata "Greenspire" – 3" caliper NE District Way: Village Green Zelkova – 3" caliper (installed)	Landscape assurance devices will be required. See conditions XI.D.35 & 36 Meets Land Use Code

* Under the MDP, a site may exceed the 75% impervious surface/lot coverage maximum, so long as the overall impervious surface/lot coverage MDP area wide does not exceed 75%. Cumulative for Phase 1&2 Residential, GIX, Block 16 & 24, and park on Tract C equates to 70.8% impervious surface/lot coverage.

**Table 3
 Parking and Loading**

Item	Minimum Required	Maximum Required	Proposed	Comments
Parking LUC 20.25D.120.B.2				Parking and bicycle racks are shared for all uses on Level A & B
Office Parking*	80,172/1,000 NSF x 2.0 = 160 Stalls	80,172/1,000 NSF x 3.0 = 240 Stalls	203 Stalls	If university ceases to exist, office parking is in compliance with LUC.
Retail Parking	3,473 NSF/1000 NSF x 2.0 min = 7 stalls	3,473 NSF/1000 NSF x 3.5 max = 12	7	Parking is shared for all uses on Level A & B. See Condition XI.C.32
University Parking*	Unspecified Use	Unspecified Use	Per parking memo, ranges from 49-222 stalls	Per LUC 20.25D.120.H, Universities are considered unspecified uses – see attachment G for parking analysis prepared by Parametrix
Total Parking Note: Considers University, Office and Retail scenarios	Minimum required = 167 stalls	Maximum required = 262 stalls	Total Provided = 210 stalls	Within Level A & B. Not visible from street. Meets LUC.
Bicycle Parking LUC 20.25D.120.G	Minimum required for office/retail = 8	Not applicable	Total Provided = 48 racks	Located in garage, level A. Meets Land Use Code. See Condition XI.C.30
Loading Area	Required on-site, which can access a public street. 10' wide by 55' long.			Loading area provided on-site, adjacent to 122 nd Avenue NE, a private street. See section A below, for discussion.

* In the event that the University use ceases to exist, adequate parking for general office use will be provided on site.

**Table 4
 Recycling and Solid Waste Collection Areas
 Based upon Gross Square Feet (GSF)**

Item	Required	Proposed	Comments
Recycling and Solid Waste Collection Areas LUC 20.20.725	Office = 2 SF per 1,000 GSF Retail = 5 SF per 1,000 GSF		Republic Services has approved proposed storage and pick up location, see file for letter. See Condition XI.C.28
GIX Building	88,239 Office = 176 SF 3,473 Retail = 17 SF Total = 193 SF	507 SF Provided on Level 1. Refuse room located on west side of building for pick up by Republic Services via 122 nd Avenue NE.	Meets Land Use Code

Other Land Use Code Topics:

B. Loading

Per LUC 20.20.590.K.4, an off-street loading space is required which can access a public street. The standard requirement for a loading space is 10 feet wide by 55 feet long. However, the Director may reduce the required stall length and maneuvering length if the property owner demonstrates that known delivery vehicles can park and maneuver within the proposed loading and maneuvering spaces so that no part of a vehicle using the loading space projects into a public right of way, access easement or private road.

The applicant has indicated that the proposed development will not have significant loading needs. Therefore, they have allocated one of the parking bays on 122nd Avenue NE, a private road, for minor loading needs and deliveries. The parking bay is technically located on-site, as the property line extends into the roadway about 8 feet. The proposed loading bay will be approximately 8 feet wide and 36 feet long. Signage will be required to be installed to indicate that this parking bay is for loading purposes only. **Refer to Section XI.D.38 for Condition of Approval regarding Loading Signage.**

C. Street-Level Build-to Lines

According to LUC 20.25D.130.B, build-to lines occur along select block faces to help establish a continuous “street wall” providing a sense of enclosure and visual interest for pedestrians. Build-to lines also contribute to the economic viability of retail and commercial uses by providing direct pedestrian access and visual exposure to potential drive-by customers. Buildings are required to be constructed to the back of sidewalk. The proposed GIX building is subject to this provision along 123rd Avenue NE, and is in compliance with the build-to lines provisions.

D. Required Sidewalk-Oriented Development

Per LUC 20.25D.130.C, sidewalk-oriented development is characterized as ground floor building frontages with direct entries from the sidewalk and/or building frontages with a high degree of transparency and activating land use. This increased visual and physical

interaction between the people inside and outside the buildings results in “eyes on the street” while creating a greater degree of safety and a vibrant public realm. The proposed GIX building is subject to this provision along NE District Way and 123rd Avenue NE. The building incorporates a primary building entrance along NE District Way with retail entrances along NE District Way and 123rd Avenue NE. In addition, the building has a proposed ground floor façade transparency of 42 % on NE District Way and 41% on 123rd Avenue NE. The proposed building is therefore in compliance with the development standards for required sidewalk-oriented development.

E. Required Local Streets

Per LUC 20.25D.140.A, the intent of local streets is to introduce a public right-of-way system that improves mobility by increasing access for local vehicular and pedestrian traffic throughout Bel-Red. The proposal is subject to the applicable standards for local streets and is shown to be in compliance.

F. Required On-Street Parking

Per LUC 20.25D.140.E, on-street parking can contribute to the pedestrian environment, be a great benefit to retail uses and enhance elements of neighborhood character. On-street parking is required for this development along NE District Way and 123rd Avenue NE. On-street parking is shown along both frontages to comply with this provision.

G. Restricted Driveway Access

Per LUC 20.25D.140F, driveway access along certain streets is restricted to minimize conflicts with transit, autos, bicycles and pedestrians, and to reinforce active edge continuity along commercial and retail frontages. The proposal is subject to restricted driveway access along NE District Way. Therefore, the garage entrance is located via 123rd Avenue NE to comply with this provision.

H. Required Street Furniture

Per LUC 20.25D.140, street furniture elements support a consistent and uniform street character, reinforce the identity of a district and minimize conflicts with pedestrian mobility. Street furniture is required along 123rd Avenue NE. **Refer to Section XI.B.14 for Condition of Approval regarding Street Furniture.**

I. Garage Vents

Garage exhaust vents shall be located and designed to avoid pedestrian impacts. **Refer to Section XI.C.22 for Condition of Approval regarding Garage Vents.** The applicant shall provide certification by a noise consultant that the noise from the exhaust fans will not exceed 60 dBA, and a determination that the velocity and direction of airflow from the exhaust system within pedestrian areas will not exceed 500 CFM. This documentation shall be submitted and reviewed with the application for a building permit. **Refer to Section XI.C.23 for Condition of Approval regarding Certification by a Noise Consultant.**

J. Mechanical Screening

Roof plans submitted for the building permit application shall indicate locations of mechanical equipment, including any communication equipment such as satellite dishes. The applicant shall demonstrate that screening for rooftop mechanical equipment is architecturally integrated with the building. Screening shall be provided so that rooftop mechanical equipment is not visible from adjacent streets, public sidewalks, or above. All

HVAC rooftop equipment shall be consolidated and screened, including painting of equipment/screening as necessary.

Power, telephone, traffic control, or other equipment shall not be located in above ground cabinets in sidewalk areas. Such equipment shall be located in underground vaults, in a building, or substantially screened per the approval of the Development Services Department.

Any at grade grills/grates or panels shall be the minimum size necessary, be ADA compliant and flush with the sidewalk or driveway in which it is installed. Review and approval of these locations and grates will be through the building permit review process. **Refer to Section XI.C.27 for Condition of Approval regarding Mechanical Equipment Screening.**

K. Garage Entry and Soffits

The proposed garage entry and soffit shall be finished to provide pedestrian and vehicles a new that is consist with the level of architectural detailing found on the rest of the buildings. **Refer to Section XI.C.26 for Condition of Approval regarding Garage Soffit Views.**

IV. CONSISTENCY WITH THE SPRING DISTRICT DEVELOPMENT AGREEMENT (DA) & MASTER DEVELOPMENT PLAN (MDP)

In addition to consistency with the Land Use Code and Zoning Requirements, the project must also be consistent with The Spring District Development Agreement (DA) and Master Development Plan (MDP). **Refer to Section XI.A.4 for Condition of Approval regarding Development Agreement and Master Development Plan.** Below each item from the DA and MDP is an explanation of how the subject Design Review application for GIX complies.

Spring District Development Agreement (DA) Requirements

On July 10, 2009, a Development Agreement (DA) was signed between the City of Bellevue and WR-SRI 120th LLC. The Development Agreement was recorded on September 11, 2009. This agreement required specific items for the development of the property known as "The Spring District."

Note: Items are listed in the order of the Development Agreement:

A. 1 and 2 Catalyst Project Criteria and Minimum Contents of MDP

Findings: The Spring District MDP met the catalyst project criteria in LUC 20.25D.035 and contains the minimum contents required under the DA to be granted the benefits of the Development Agreement, including extended vesting and specific governing regulations. See Spring District Master Development Plan at pages 25-27.

B. Development Standards and Vesting Period

Findings: Pursuant to the terms of the Development Agreement, the MDP is vested for fifteen years from its effective date, October 8, 2012 (date of the City's final administrative decision). See LUC 20.35.045. The proposed GIX building meets the conditions to be reviewed under the Development Agreement, including the DA's Governing Regulations.

C. Governing Regulations

Findings: Pursuant to the terms of the DA, the GIX application is reviewed under the Governing Regulations, Title 20 (Land Use Code) and Title 21 (Comprehensive Plan) as they existed on the effective date of the DA, July 10, 2009, with certain limited exceptions. The City shall not exercise its substantive SEPA authority to impose conditions on Land Use Code approvals issued during the Vesting Period in a manner that is inconsistent with the Governing Regulations. Neither Sound Transit's East Link Project, nor any other high-capacity transit operating within a dedicated transit-only right-of-way has begun service to a transit station within The Spring District. Therefore, the Revised Governing Regulations provision of the DA is inapplicable.

D-E. Consistency with DA, Proportional Compliance

Findings: The proposal will be consistent with DA requirements. Proportional compliance is not applicable since the subject Design Review application is for one new university/office building.

F. FAR Amenity Bonus System

Findings: The Spring District is a Catalyst Project consistent with the terms of the DA and Chapter 20.25D LUC. Accordingly, the FAR Amenity Bonuses are calculated pursuant to Paragraphs F.1, 2, and 3 of the DA that are included below for reference:

1. Adjustment of Tier 1 Fee-in-lieu Rate. For a Catalyst Project on the Property, the Owner may choose to comply with the LUC 20.25D.090 requirements for Tier 1 amenities by paying a fee-in-lieu at a rate of \$3.75 for each square foot of floor area for the first 750,000 square feet of development under the MDP, and by paying a fee-in-lieu rate of \$4.00 for each square foot of floor area above 750,000 square feet. In the alternative, this Development Agreement provision shall not bar the Owner, at its choice, from utilizing the LUC 20.25D.090 standards as of the Effective Date to provide required amenities.
2. Amenity Rate for Certain Required Open Space. Owner may receive credit towards required amenities for the mini-park and activated park or recreation space required to be included in Phase 1 of the MDP under Section A above as provided in this Section F.
 - a) Mini-Park: Owner shall receive credit towards required amenities for dedication of the M-3 mini-park at the bonus rate set forth in 20.25D.090.C.7 Tier 1b.2 (Park Dedication) and/or Tier 1b.3 (Park Improvements).
 - b) Other Activated Park or Recreation Space: Owner may receive credit towards required amenities for the activated green space as follows:
 - at the Tier 2 bonus rate so long as it is developed according to the design criteria set forth in 20.25D.090.C.7 Tier 2.12 (Active Recreation Area); or
 - at the Tier 1b bonus rate set forth in 20.25D.090.C.7 Tier 1b.2 (Park Dedication) if the entire area is dedicated to the City, and it meets the design criteria 2 through 4 in that section; and/or

- at the Tier 1b bonus rate set forth in 20.25D.090.C.7 Tier 1b.3 (Park Improvements) if improvements are made according to the design criteria 1 through 5 set forth in such section.

Unless the activated park or recreation space meets one of the provisions above, it shall not receive credit towards required amenities.

3. Eligibility for Other City Credits, Bonuses or Offsets. Notwithstanding any provision of city code to the contrary, any public infrastructure required as a condition of approving the MDP that meets the criteria of LUC 20.25D.035.A.3.a through c shall not be eligible for and shall not earn any of the credits, bonuses, or offsets described in LUC 20.25D.035.A.3.d through f.

G. Concurrency.

Findings: Pursuant to the Development Agreement, traffic concurrency analysis was conducted for all of the proposed Spring District Phase 1B development, and a Certificate of Concurrency for all of Phase 1B is issued on May 5, 2016. Per the Development Agreement, that Certificate of Concurrency covers this development for five years until May 5, 2021, and may be extended consistent with the provisions of BCC 14.10.040.F.1.

H. Catalyst Project Residential Requirements and Delay Penalty.

Findings: The MDP designated 6.9 acres of residentially-restricted property, which exceeded the DA requirement of 5.8 acres. The GIX application does not contain any residential development that count toward the minimum DA requirement of 784,000 square feet of residential use in The Spring District. Under the DA, residential uses must be developed proportionally to commercial uses no later than the date that Sound Transit's East Link Project or other high-capacity transit operating within a dedicated transit-only right-of-way begins regularly scheduled passenger-carrying service to a transit station within The Spring District. No such service has begun so this requirement is not yet applicable; however, residential development should proceed within a reasonable timeframe to ensure this condition is met in the future.

Spring District Master Development Plan (MDP) Conditions

Note: Items are listed in the order of the Master Development Plan:

Compliance with prior MDP Conditions: X.A. GENERAL CONDITIONS:

1. Vested Status of the Master Development Plan.

Findings: The MDP is vested for 15 years from October 8, 2012 and this application was received within the vesting period.

2. Development Agreement (DA) between the City and Applicant:

(a) Residential Property.

Findings: There are no residential uses being constructed under this application. Current and future development applications will address construction on the residentially-restricted property. As discussed under the DA requirements above, Sound

Transit or other high-capacity transit is not operational on the property, so there is not yet a requirement for residential uses to be built.

(b) Parks:

- **Within Phase 1, the project shall include development of a public mini-park a minimum of one acre in size (designated as project M-3 in the Bel-Red Parks and Open Space Project List in the Comprehensive Plan) and an activated park or recreation space of at least 30,000 contiguous square feet.**

Findings: The future public mini park is not part of this application, but remains as part of the Phase 1 development of the MDP. The activated park of at least 30,000 square feet is also not part of this application, but has been installed west of the subject site, and remain as part of the Phase 1 development of the MDP.

- **All park tracts developed as a condition of the Development Agreement shall, if owned and maintained by the applicant, record an easement securing public access over the park tracts.**

Findings: The future public Mini Park and existing activated park are not part of this application.

- **Future design and development of the one acre public park (M-3), per the Development Agreement, shall be approved by the Parks & Community Services Department and be consistent with BCC 3.43.**

Findings: The future public mini park is not part of this application.

3. **Review Process for Parks/Plaza/Gateways: Review and approval of all park/plaza/gateway areas shall be conducted under Design Review approval, with input by the City of Bellevue into the design of these areas.**

Findings: The applicant designed an outdoor plaza along the north side of the building which connects between 123rd Avenue NE and 122nd Avenue NE. The proposed design of the plaza is included with this Design Review application and meets the intent of the MDP.

4. **Phasing Plan: The Phasing Plan shall be followed per the Conceptual Phasing Plan (Attachment F). Modifications to the phasing plan may occur, per LUC 20.30V.160.**

Findings: This application is for Parcel 14, which is within Phase 1B of the MDP. The City is continuing to receive design review applications for the overall Phase 1 development of the MDP.

5. **City Council Approval of NE 15th Street/NE 16th Street and portrayal on the Binding Site Plan:**

If shifting of the NE 15th Street/NE 16th Street alignment (horizontal/vertical) at a later date is in conflict with the recorded BSP, the applicant shall amend the BSP to match the City Council approved alignment (horizontal/vertical) of NE 15th Street/NE 16th Street. Such amendment of the BSP shall occur prior to submittal of the next Design Review application for the MDP site after the final alignment is approved. The vested status of final street locations does not occur until an adjacent DR is issued.

Findings: This condition does not apply to this application because NE 15th/NE 16th Street is not located adjacent to the subject parcels.

- 6. Public Plaza at the Light Rail Station: As a part of the master planning process for the Sound Transit 120th East Link Station, the applicant shall coordinate with the City of Bellevue and Sound Transit regarding the design of a public plaza adjacent to Sound Transit’s 120th East Link subterranean light rail station. The applicant shall designate the plaza as a “public plaza” on the recorded Master Development Plan.**

Findings: This condition does not apply to this application because the station is not within Phase 1B of the MDP.

7. Pedestrian Amenities:

- **Multi-purpose trail: A multi-purpose trail (12-14 feet in width) shall be provided along the entire length of The Spring District’s property along 124th Avenue NE to connect to the multi-purpose trail on NE 16th Street as shown on the MDP plans. See Attachment A. For Phase 1a, the applicant shall design the entire trail (for both Phase 1a and Phase 1b) and construct the Phase 1a trail adjacent to any Phase 1a development. For Phase 1b, the applicant shall complete construction of the entire trail.**
- **Landscaping along 124th Avenue NE: The area within the Seattle City Light easement (adjacent to 124th Avenue NE) shall be landscaped along with a multi-purpose trail. Vehicular parking areas, vehicular lanes or load/unload areas will be prohibited within this easement area. This landscaping shall be installed as a part of Phase 1.**

Findings: This condition does not apply to this application because this area of The Spring District is not part of the subject site, nor is it adjacent to the subject site.

- **Mid-block Public Pedestrian Connection: A mid-block public pedestrian connection (12-14’ wide) between Buildings 15 & 16 (as shown on the MDP plans, Attachment A) shall be provided from 123rd Avenue NE to 124th Avenue NE and shall meet ADA accessibility requirements and the intent of the Green Streets Development Standards. The location and design shall meet the intent of LUC 20.25D.140.D. This mid-block public pedestrian connection with its associated easement and signage shall be installed as a part of Phase 1.**

Findings: This condition does not apply to this application because this area of The Spring District is not part of the subject site, nor is it adjacent to the subject site.

- **Green Streets: The modification approval to relocate the Green Street from NE 13th Street to the loop roads is applicable to future reviews of Design Review applications. The applicant shall provide a mid-block public pedestrian connection between Buildings 15 & 16. See the condition above: Mid-block Public Pedestrian Connection.**

Findings: This condition does not apply to this application because the Green Streets, as modified by the Spring District Master Development Plan approval, are not adjacent to the subject site.

- **Sidewalks: The applicant shall provide sidewalks along all city/private streets within the project limits.**

Findings: The applicant is providing appropriate sidewalks, as required, fronting NE District Way, 123rd Avenue NE and 122nd Avenue NE, which will be constructed as part of this development. It should be noted that portions of these sidewalks are already constructed under the master infrastructure permit for The Spring District.

- **“Pedestrian” Gateway Connection: A “Pedestrian” Gateway Connection shall be located at the southwest corner of the property between Buildings 18 & 19 as shown on the MDP plans. See Attachment A. The gateway shall provide pedestrian access from the site (approximately NE 13th Street and 121st Avenue NE) to the adjacent city street at NE 12th Street (near 120th Avenue NE). The pedestrian connection shall be designed and permitted during the Design Review process for the first adjacent building to the gateway.**

Findings: This condition does not apply to this application because it does not involve a development adjacent to the gateway location at the southwest corner of The Spring District.

8. **Other Gateway Opportunities:**

The applicant will be required to provide a gateway entrance at the street level for pedestrians, motorists, and those passing by the site. The location and timing of the construction of this gateway entrance at street level shall be determined during Phase 1. Likely locations would be the entrance to the site at NE 13th Street (from 124th Avenue NE) or NE 15th Street (from 120th Avenue NE or 124th Avenue NE). Such location shall be reviewed and approved under Design Review and approved by the Transportation Department.

Findings: This condition does not apply to this application because these gateway areas of The Spring District are not part of the subject site, nor are they adjacent to the subject site.

9. **Sign Master Plan: The applicant shall submit a sign master plan for the entire MDP project. The sign master plan may be submitted with the first Design Review**

application or separately as a Land Use Exemption to the approved Master Development Plan. Proposed signs on-site for individual buildings will not be approved until found consistent with the approved sign master plan.

Findings: The Sign Master Plan will be submitted as a separate application as allowed under this condition. **Refer to Section XI.E.40 for Condition of Approval regarding Sign Master Plan.**

10. **Recycling and Solid Waste Collection: With each Design Review application, the applicant shall document how recycling and solid waste will be collected. Recycling and solid waste receptacles may not be pulled out to the adjacent street/sidewalk. Refuse and recycling areas may be not located within adjacent public/private streets or adjacent rights-of-way. These activities must be located on each individual building site and screened from public view. The applicant shall coordinate the location of receptacles with Republic Services or any successor in interest to the Bellevue Waste Hauling franchise.**

Findings: The building includes an interior refuse and recycling area on the west side of the building, adjacent to a PSE transformer room, which has a roll up door for access. Trucks will temporarily pull into the on-street loading space adjacent to the roll up door for pick up. The applicant is working with Republic Services regarding pick up service to avoid having trash/recycling receptacles left on the street. The design meets the MDP requirements.

11. **Street Development: Future street development must be approved by the Transportation Department and be consistent with LUC 20.25D.140.**

Findings: The streets proposed in this application are consistent with the street development standards in LUC 20.25D.140 and have been approved by the Transportation Department.

12. **Access to Underground Parking Garages: In order to provide alternate vehicular routes to city streets (due to the modification of the NE 12th Street local street), the applicant shall provide two vehicular access points to the underground parking garages, one along NE 12th Street and the other along 120th Avenue NE (signalized). The exact location of these parking garage driveways shall be determined at the Design Review stage of the subject buildings/phases.**

Findings: This condition does not apply to this application as the access point to the proposed underground garage is via 123rd Avenue NE (private). This condition is applicable to the Phase 1 residential project, currently under construction.

13. **Modification to MDP: The applicant shall submit a modification to the Master Development Plan for minor changes to the approved MDP. Modifications can be processed as a Land Use Exemption application (LUC 20.30V.160.B). Minor changes include updates to road configurations, FAR calculations, the Amenity Chart, impervious surface/lot coverage and the Sign Master Plan. It will still be necessary to record the updated MDP.**

Findings: Any modification to the MDP or this Design Review approval shall be processed as a Land Use Exemption. **Refer to Section XI.A.4 and XI.A.8 for Condition of Approval regarding Development Agreement and Master Development Plan and Modification to the Design Review Plans.**

14. **Binding Site Plan:** The Binding Site Plan shall be recorded upon the final decision of the Master Development Plan before the sale or lease of any lot, tract or parcel. The BSP shall include all required dedications and easements per the MDP conditions of approval at the time of each DR application for each project.

Findings: The Binding Site Plan was recorded on November 1, 2012 under King County Recording No. 20121101000274. An Amended Binding Site Plan related to this application was recorded on May 6, 2016 under King County Recording No. 20160506000760.

15. **Preliminary Design, Utility Codes and Engineering Standards:** Utility review has been completed on the preliminary information submitted at the time of this application. The review of this application has no implied approvals for water, sewer and storm drainage components of the project. Final plan approval will occur under a Utility Extension Agreement which will be required for review and approval of the utility design. Submittal of the utility extension will coincide with future clearing and grading permit review. Final civil engineering may require some changes to the site layout to accommodate the utilities.

Findings: A utility extension agreement has been submitted by the applicant. Approval of the utility extension agreement is required prior to the issuance of clearing and grading permits. **Refer to Section XI.A.3 for Condition of Approval regarding Preliminary Design, Utility Codes and Engineering Standards.**

16. **Art Concept:** An art concept is required to be consistent with LUC 25D.150.B.5.a. Such art shall be installed within one of the public parks/plaza/gateways and reviewed under the subject Design Review application. Review/approval by the City of Bellevue Arts Commission is not required. The art concept shall be installed prior to completion/final certificate of occupancy of The Spring District.

Findings: Not applicable, as this site does not contain a required public park, plaza or gateway per the MDP. The installation of an art concept was implemented under the Phase 1 residential development, located west of the subject site, which was the bicycle wall art located on Building E.

17. **Addressing of Buildings/Tracts:**

Findings: The applicant will be required to contact Parcel and Addressing Coordinator, Jami Carter, Information Technology Department, phone 425-452-4210 regarding the addressing of buildings/tracts.

18. **Restricted Driveway Access:** With each new Design Review application, the applicant shall document how the driveway access of that development meets the restricted driveway access intent of LUC 20.25D.140.F.

Findings: A singular driveway access to the building is located via 123rd Avenue NE, a private street. 123rd Avenue NE is a side street to the development, which will minimize conflicts with transit, autos, bicycles, and pedestrians and to reinforce active edge continuity along commercial and retail frontages. This driveway is also located away from the main pedestrian entries and retail stores. Accordingly, the Design Review application and submitted plans demonstrate that the driveway access meets the intent of LUC 20.25D.140.F.

19. **Arterial Street Design Standards: The applicant shall meet the intent of the “Arterial Street Design Standards within the Bel-Red Subarea.”**

Findings: Not applicable, as there are no arterial streets adjacent to this development.

20. **Right of Way and Easements: The applicant shall dedicate to the City all necessary right of way such that street improvements including on-street parking to the back of curb are located within the fee public right of way. The applicant shall also grant any necessary construction, pedestrian, landscaping and utility easements. The applicant shall provide easements to the City for location of signal and street light facilities such as above-grade boxes and below-grade vaults between the building and sidewalk within the landscape area.**

Findings: The Applicant will dedicate all necessary rights of way and grant any necessary easements consistent with applicable city requirements.

21. **Vehicular Access Restrictions: All non-signalized driveways and private roads adjacent to 120th Avenue NE, NE 15th Street, 124th Avenue NE and NE 12th Street will be restricted to right in/right out. No left turns will be allowed at the signalized intersection of NE 15th Street/ 123rd Avenue NE. The applicant will purchase and post “No Left Turn” signs at the subject street and/or driveway locations, when and if necessary as directed by the City. All access is subject to additional restrictions based on traffic operations conditions as determined by the Transportation Department.**

Findings: Not applicable, as the access to this development is located on 123rd Avenue NE, which is a private street that connects to NE District Way.

22. **Local Improvement District (LID): The applicant shall not protest the city's formation of a LID or Transportation Improvement District.**

Findings: This condition was recorded as part of the Master Development Plan dated October 16, 2012 under King County recording number 20121016001049.

Compliance with prior MDP Conditions: X.B. PRIOR TO ANY DESIGN REVIEW APPROVAL FOR A BUILDING, PHASE, AND/OR PARK/PLAZA/GATEWAY: The following conditions must be complied with prior to any Design Review approval for a building, phase, and/or park/plaza/gateway:

1. **Design Review: Each new site development (i.e. building, parks, plaza, and gateway) shall require Design Review approval. If appropriate, Design Review applications may be combined.**

Findings: This application is for administrative Design Review approval for a building and other related site improvements, consistent with this MDP condition.

- 2. Outward Focus of Perimeter Buildings: During the Design Review of individual buildings on the perimeter (124th Avenue NE, NE 12th Street, 120th Avenue NE), the applicant shall provide building designs that convey an outward focus toward the city streets as well as toward the interior of the development. The applicant shall incorporate the design guidelines of LUC 20.25D.150. The use of blank walls or flat nondescript walls that are not articulated by any visual interest is not consistent with applicable design criteria.**

Findings: Not applicable, as this development is not located on the perimeter of The Spring District MDP.

- 3. Coordinated Landscaping Plan along Site Perimeter: With the first perimeter Design Review submittal along 124th Avenue NE and the first perimeter Design Review submittal along 120th Avenue NE, a coordinated perimeter landscaping plan shall be submitted for the length of The Spring District property along each right-of-way. Note: This coordinated landscaping plan is for on-site landscaping and not off-site landscaping associated with any city right-of-way project.**

Findings: Not applicable, as the subject site is located interior to The Spring District, and does not contain frontage on 120th Avenue NE or 124th Avenue NE.

- 4. Impervious Surface/Lot Coverage: The maximum impervious surface/lot coverage is 75% MDP area-wide. This calculation shall be provided upon each Design Review application to keep track towards the final phase of the MDP.**

Findings: The Applicant provided the City with an updated calculation for this review. The total impervious surface/lot coverage of the area redeveloped under this application is 87% (51,788 sq. ft.). The cumulative impervious surface/lot coverage for The Spring District MDP is 70.8 %, which includes Block 16, Block 24, Block 14, Spring Residential Phase 1 & 2 and the recently constructed public park on Tract C. See attachment H. Under the MDP, a site may exceed the 75% impervious surface/lot coverage maximum, so long as the overall impervious surface/lot coverage MDP area wide does not exceed 75%. This means that another parcel of development will likely need to be developed with less than 75% in order to not exceed the MDP total.

- 5. Parking Stall Calculations: Each building shall submit parking stall calculations to document how it meets the Land Use Code requirements for onsite parking.**

Findings: The application includes parking stall calculations showing compliance with on-site parking requirements. Refer to Table 3 in Section III.A above which summarizes parking for the development. It should be noted that the applicant was required to provide a parking analysis for the University component of the building, as schools are considered an unspecified use in the LUC for parking ratio requirements. As such, the applicant intends to provide enough parking for both a University use as well as a general office use in the event that the school use closes, allowing the building to be re-tenanted as an office.

6. Amenities and FAR for each Design Review:

- **The phasing plan for the MDP shall provide for proportionate installation of amenities that must be included when each phase of development is constructed.**
- **Required amenities for each individual building shall be provided at the time of that building construction. In-lieu fees shall be assessed and collected at building permit issuance. Required amenities cannot be “borrowed” from future buildings or phases.**
- **The applicant shall meet the amenities outlined in the Development Agreement as well as the Land Use Code. The applicant shall use the attached Amenity calculation sheet (Attachment B) to show compliance with the DA and Land Use Code.**
- **The calculations shall show that for the overall MDP at full build-out, there is at least a minimum 2.5 FAR overall (per the Development Agreement) with a maximum 4.0 FAR overall (per the Land Use Code).**
- **With each Design Review application submitted under the MDP, the applicant shall include the physical FAR of the proposed project, as well as the calculated FAR of the project as part of the overall MDP rolling average. At no time shall the averaged MDP FAR fall below 2.5 (DA) or exceed 4.0 (LUC).**
- **Use of an alternate MDP FAR calculation or a change to the target MDP FAR must be reviewed and approved by the City of Bellevue as a modification, processed as a Land Use Exemption application to Design Review or as a modification request with the subject Design Review application.**

Findings: As shown below, the FAR for this application is 2.97, which is within the required range of 2.5 to 4.0 under the Development Agreement and the Land Use Code. The application does not include an alternate FAR calculation or change to the target FAR.

The proposed FAR Amenity calculations, below, are consistent with the requirements of the Development Agreement, MDP, and Chapter 20.25D of the LUC. Fees will be collected at the time of building permit issuance. **Refer to Section XI.C.31 for Condition of Approval regarding Payment of Fee-In-Lieu of Floor Area Amenities.**

The Development Agreement stipulates fees-in-lieu of amounts for amenity payments for this specific Master Development Plan. Each phase of development is required to provide the installation of amenities through payments in-lieu of installation (to be paid at building permit issuance). For this specific Master Development Plan, the first 750,000 square feet is paid at \$3.75 per square foot. After 750,000 square feet, the fee is \$4.00 per square foot.

The applicant intends to meet the FAR Amenity Incentive System (20.25D.090) code requirements through payment of fee-in-lieu to satisfy the Tier 1 Parks and Streams bonus category triggered. Based on the current building design and based on the terms of the Spring District Development Agreement, the estimated amount to be paid is \$469,264.00, which will satisfy the Tier 1 Parks and Streams requirements. This estimated fee calculation is included as Attachment F. Per 20.25D.090.C.6, in-lieu fees shall be assessed and collected at building permit issuance. The fees will be recalculated at that time, prior to collection. Should a different Spring District project (Spring Residential Phase 2) pull building permits prior to the issuance of permits for this project, the amenity fees may have to be recalculated using both the \$3.75 and the \$4.00 rate.

The Spring District Development Agreement provides a two-tiered discount rate for fee-in-lieu payments made under the amenity incentive zoning system. The first 750,000 Gross Square Feet (GSF) of development can make fee-in-lieu payments at \$3.75 per square foot of Gross Floor Area (GFA). The first development under the MDP (office building 16 & 24) used 344,363 of the available GSF, and therefore pays 100% of their GFA fee-in-lieu obligation at the \$3.75 rate. The Phase 1 Residential project used 320,924 of the available GSF and therefore paid 100% of their GFA fee-in-lieu obligation at the \$3.75 rate. The Spring Phase 2 Residential project will use the remaining 84,713 GSF eligible for the \$3.75 rate, which is 32% of the projects total GSF. Therefore, 32% of the projects GFA to be paid at the \$3.75 rate. The remaining 68% of the projects GFA will be paid at the fee-in-lieu rate of \$4.00.

The total floor area subject to the fee-in-lieu obligation for the GIX project is 117,316, which will be paid at the \$4.00 rate. See tables below.

Non-Residential Development	GIX Building
Aggregate Parcel Size (sf)	59,527 square feet
Total Allowed SF at Max FAR (4.0)	238,108 square feet
GFA of Proposed Development *	176,843 square feet
FAR	2.97 (176,843/59,527)
Base FAR Allowed (1.0)	59,527
GFA Subject to Incentive System	117,316
GFA in Tier 1 (up to 2.5 FAR)	117,316
GFA in Tier 2 (up to 0.5 FAR)	0

Amenity Fee-in-Lieu Payment Calculation	GIX Building
Gross Square feet (GSF) of Development *	93,863 square feet
% Floor Area Eligible for DA \$3.75 rate	0%
% Floor Area Eligible for DA \$4.00 rate	100%
Total Floor Area Subject to Amenity System	117,316 square feet
Square Feet Paid at \$3.75	0
Square Feet Paid at \$4.00	117,316
Tier 1a: Affordable Housing	\$0
Tier 1 or Tier 1b: Parks and Streams	\$469,264.00

* Gross Floor Area (GFA) – the area included within the inside finished wall surface of the surrounding exterior walls of a building excluding interior openings in floor plates (e.g. vent shafts, stair wells, and interior atriums), outdoor courts and exterior balconies.

* Gross Square Feet (GSF) – total number of square feet within the inside finished wall surface of the outer building walls of a structure, excluding vent shafts, outdoor courts and parking.

See Attachment F for detailed amenity calculations.

Compliance with prior MDP Conditions: X.C, D, and E. PRIOR TO CLEARING & GRADING PERMIT, BUILDING PERMIT, AND ISSUANCE OF ANY CERTIFICATE OF OCCUPANCY:

Response: Conditions contained in this section of the MDP are required by City Code and the MDP and therefore must be met before clearing and grading permits, building permits, or certificates of occupancy, respectively, are issued under this Design Review approval. Refer to Section XI for Conditions of Approval XI.B.10 through XI.D.39.

V. PUBLIC NOTICE AND COMMENTS

The applicant and the city held a public meeting on December 15, 2015 at City Hall. The purpose of the meeting was to discuss and clarify the proposal and listen to neighbor concerns. Only 1 person attended this meeting who was only interested in the construction timeline. Comments were accepted during and after this public meeting, as well as up to the date of this decision. During the overall review of the project, the City did not receive any public comments on the application.

VI. TECHNICAL REVIEW

A. Clearing & Grading

The Clear and Grade reviewer has reviewed the plans and materials submitted for this project and has determined that the clearing and grading portion of this land use application can be approved. The future Clearing and Grading Permit application for this development must comply with the City of Bellevue Clearing and Grading Code (BCC 23.76).

B. Utilities

Storm Drainage

The project will trigger (MR) minimum requirements 1-9 from the Department of Ecology Stormwater Management Manual for Western Washington. The project qualifies as redevelopment under the Department of Ecology Stormwater Management Manual for Western Washington. MR 5 will be required because the new and replaced impervious surface is greater than 2,000 S.F. The project proposes to use MR 5, Tier 1-3 BMP's to reduce peak flows and treat stormwater prior to leaving the site. This Commercial project site will require enhanced MR6 Storm Water Runoff Treatment for all new and replaced pollution generating surfaces. The driveway that provides entry to the underground parking garage off 123rd Ave NE will enter and be treated by a bio retention cell on 123rd Ave NE before entering the stormwater conveyance system. This project drains to Lake Washington through the Sturtevant Creek 40-20 Basin, a tributary to the Mercer Slough and will not need to provide runoff control. MR 7 Flow Control will not be required because the impervious area on-site is decreasing with the construction of GIX versus current conditions.

Water

The water supply for this project is available from existing City of Bellevue owned water mains located on site in the 400 pressure zone. Water improvements consist of domestic, irrigation and fire lines. Backflow prevention is required for fire sprinklers. It appears from the size and

scale of the project that both domestic and fire requirements can be met by connecting to the 8-inch water main. Water meters are not allowed inside building structures and must be located in a location approved by the Utility Department.

Sewer

Sewer service for the GIX Building will be available from an existing 8-inch side sewer stub on NE District Way.

Refer to Section XI.A.3 for Condition of Approval regarding Preliminary Design, Utility Codes and Engineering Standards.

C. Transportation

Site Access

This project is situated on Block 14 in the Spring District Master Development Plan, and is in Phase 1B according to the current phasing plan for the development. The MDP proposes a grid comprising public and private streets with extensive pedestrian connections to meet the intent of the Bel-Red Street Development Standards. Access to the proposed project was proposed in the MDP to be provided by public streets NE District Way and 123rd Avenue NE on the south and east sides of the block, and private streets on the west and north sides. Revision 3 to the MDP allowed the size of Block 14 to increase by expanding to the north into the private street tract, eliminating vehicular access and circulation but providing a pedestrian corridor instead. This corridor is on the northern portion of Block 14, and will be developed with the GIX project.

On the west and south sides of the GIX building, most of the required improvements for 123rd Avenue NE and NE District Way are under construction under the Phase 1A infrastructure grading permit, and are planned to be dedicated as public streets during the construction of the building. On the west side of the building, most of the required improvements for 122nd Avenue NE are under construction with a separate grading permit. This street will remain private, however public sidewalk easements will be required on both sides per the MPD conditions. A public pedestrian easement will also be required for the pedestrian corridor providing access to the north side of the proposed building.

Vehicular access to the GIX building will be provided via a driveway on 123rd Avenue NE leading to the underground garage. This driveway will be a minimum of 26 feet wide at the back of the curb, and will be constructed according to standard drawing DEV-7E. To provide the minimum width, the driveway is offset slightly from the garage opening to avoid conflict with the existing drainage features south of the entrance.

The building design at the garage access point limits pedestrian sight distance to the planned sidewalk alignment along 123rd Avenue NE. To provide adequate pedestrian sight distance from the driveway, the sidewalk was located away further from the building and closer to the curb. Since this is in the area of a curb extension, sufficient planter strip area can be maintained between the sidewalk and curbline to meet the requirements of the Bel Red land use code for a six-foot sidewalk and five-foot planter strip in areas where there is no on-street parking.

This building will not provide a loading area that is accessible to vehicles on-site. Loading is intended to take place through an overhead door adjacent to 122nd Avenue NE, also used to access the refuse collection area. An on-street parking bay will be reserved for loading, and a

ramp will be provided from the sidewalk to the street to facilitate the transfer of goods from vehicles in this loading zone to the building. **Refer to Section XI.A.1 for Condition of Approval regarding Provisions for Loading.**

Street Frontage Improvements

In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, the construction of street frontage improvements is required as a condition of development approval. The design of the improvements must conform to the requirements of the Americans with Disabilities Act, the Transportation Development Code (BCC 14.60), and the provisions of the Transportation Department Design Manual.

Frontage improvements are required for this project on both public and private streets as laid out in the Spring District MDP. Much of the frontage improvements along 123rd Avenue NE and NE District Way for this site are being permitted and constructed through an infrastructure permit approved for Phase 1A. These are expected to be complete and accepted into the City system in late 2016 or early 2017, but must be complete and accepted prior to occupancy of this project. Sidewalk construction was not included in the previous permit and will be required with this approval, as well as curb ramps and crosswalks that are not completed through the infrastructure permit.

Some of the improvements in the previous permit will be modified to accommodate this project, and those modifications will be included in this approval. This will include the removal of curb returns for the previously proposed vehicle connection between Block 13 and Block 14, and sidewalk modifications at the garage entrance and along NE District Way, including the relocation of a streetlight. Frontage improvements for 122nd Avenue NE have also been approved under a separate permit, but have not yet been constructed. Sidewalk construction and some modifications for the proposed loading ramp on 122nd Avenue NE will be included in the approval for this project. **Refer to Section XI.B.11, XI.C.16 and XI.D.33 for Conditions of Approval regarding Civil Engineering Plans-Transportation, Building and Site Plans – Transportation, and Street Frontage Improvements.**

1. A street light plan for NE District Way and 123rd Avenue NE is required for review and approval prior to completion of engineering and landscape plans showing any relocated fixture(s) along with street tree location and driveway separation (ten feet from Point A in standard drawing DEV-7E or equivalent).
2. The Americans with Disabilities Act (ADA) requires that sidewalk cross slopes not exceed two percent. The sidewalk cross slope may be less than two percent only if the sidewalk has a longitudinal slope sufficient to provide adequate drainage. Bellevue's standard for curb height is six inches, except where curb ramps are needed. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm compliance. New curb and sidewalk shall be constructed in compliance with these requirements. Building elevations shall be consistent with the required curb and sidewalk elevations. Spot elevations must be included in the building plans in a manner that proves that building elevations are designed to correspond to the sidewalk elevations shown in the engineering plans, especially at entrances and other key points. Curb and sidewalk elevations will not be revised to fit the building, and city inspectors may require spot surveys during construction in order to confirm the required elevations.

ADA also requires provision of a safe travel path for visually handicapped pedestrians. Potential tripping hazards are not allowed in the main pathway. Any planter boxes installed in the sidewalk to improve pedestrian sight distance at driveways must be designed to reduce the tripping potential and must not extend more than two feet into the public sidewalk. Traffic signal controller boxes and streetlight contactor cabinets must be located so as not to interfere with the main pedestrian path. Buildings shall be designed so that doors do not swing out into the pedestrian path. Installation of colored or textured bands to guide pedestrians in the direction of travel is advisable, subject to the requirements for non-standard sidewalk features. ADA-compliant curb ramps shall be installed where needed, consistent with Design Manual standard drawings.

ADA-compliant curb ramps shall be installed on NE District Way at the intersections with 123rd Avenue NE and 122nd Avenue NE and on 123rd Avenue NE at the pedestrian crossing location if not provided by others, consistent with City and WSDOT standard drawings. If such standards cannot be met, then deviation from standards must be justified on a Design Justification Form to be filed with the Transportation Department.

3. Sidewalk along 123rd Avenue NE, NE District Way, and 122nd Avenue NE shall be constructed to a width of at least 6 feet with a planter strip of at least 5 feet, not including the curb width. On 122nd Avenue NE, the planter strip may be replaced with 5-foot by 5-foot tree wells as was approved in the grading permit for the infrastructure construction.
4. The existing curb, gutter, and sidewalk on 123rd Avenue NE at the driveway location and existing curb returns shall be completely removed and reconstructed with a sidewalk width of at least six feet, not including the curb, and a planter strip that is at least five feet wide.
5. The driveway on 123rd Avenue NE shall have a minimum width of 26 feet at the back of the curb, and shall be constructed per standard drawing DEV-7E.
6. No new building structure or garage shall be constructed under a street right of way or existing public sidewalk/utility easement. No soil nailing is allowed under a street right of way or sidewalk/utility easement without an indemnification agreement that protects the city.
7. To the extent feasible, no new utility vaults that serve only one development will be allowed within a public sidewalk. Vaults serving a broader public purpose may be located within a public sidewalk. To the extent feasible, no utility vaults may be located within the primary walking path in any sidewalk.
8. Any awning, marquee, balcony, etc. over a sidewalk or utility easement must be at least 16 feet above the sidewalk, or be removable (with an agreement regarding removal and replacement); and must have at least three feet horizontal clearance from any streetlight or traffic signal pole.
9. No fixed objects, including fire hydrants, trees, and streetlight poles, are allowed

within ten feet of a driveway edge, defined as Point A in standard drawings. Fixed objects are defined as anything with breakaway characteristics greater than a four-inch by four-inch wooden post.

10. No new overhead utility lines will be allowed within or across any right of way or sidewalk easement.

Easements

The applicant shall provide sidewalk and utility easements to the City as needed to encompass the full required width of any sidewalks located outside the city right of way fronting this site on NE District Way and 123rd Avenue NE, and for the sidewalk adjacent to the private street, 122nd Avenue NE, according to the MDP conditions. A pedestrian easement for the corridor north of the GIX building shall also be provided. A vehicle access easement shall be provided for 122nd Avenue NE. **Refer to Section XI.C.17 and XI.C.18 for Conditions of Approval regarding Existing Easements and Sidewalk/Utility/Access Easement.**

Right of Way Dedication

To incorporate street improvements which are reasonably necessary to mitigate the direct results of the development, and to accommodate the street widening described elsewhere in this document, the developer is required to dedicate property such that street surface to back of curb is accommodated within the public right of way.

Future rights-of-way for Spring District roadway alignments are shown in tracts on the Binding Site Plan for the development. These will be dedicated by the master developer for required infrastructure for each phase of the development. These tracts are narrow and require additional right-of-way to be dedicated on the street frontages of adjacent blocks, including Block 14, in order to provide sufficient right-of-way width for street elements such as on-street parking bays and bio retention cells. For this project, right-of-way shall be dedicated to a line that extends from the back of the curblines adjacent to the parking bays on NE District Way and 123rd Avenue NE. MDP condition X.D.2.E. requires that the right-of-way be dedicated prior to the issuance of the project building permit. **Refer to Section XI.C.19 for Condition of Approval regarding Dedication of Right of Way.**

Use of the Right of Way During Construction

Applicants often request use of the right of way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right of Way Use Permit for such activities must be acquired prior to issuance of any construction permit including demolition permit. Sidewalks may not be closed except as specifically allowed by a Right of Way Use Permit. **Refer to Section XI.B.10 for Conditions of Approval regarding Right-of-Way Use Permit.**

Pavement Restoration

The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration Program, every street in the City of Bellevue has been examined and placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. NE District Way and 123rd Avenue NE will be classified as "No Cut" streets when accepted into the City system. **Refer to Section XI.D.34 for Condition of Approval regarding Pavement Restoration.**

Transportation Management Program

In order to reduce single occupant vehicle trips and provide enhanced options to employees and infrastructure users, the City has adopted code provisions for a transportation management program. A TMP is required for an office use, but not for a university use. Since the initial use of the building will be as a university, a TMP would only be required if the use changes. The owner of this development shall, prior to any change in the occupancy of the building structure, sign and record an agreement approved by the City of Bellevue to establish a transportation management program to the extent required by BCC14.60.070. **Refer to Section XI.A.2 for Conditions of Approval regarding Transportation Management Program.**

D. Building Division

The plans for this decision have not been sufficiently developed for a thorough review under the International Building Code requirements. This review will occur during review of the Building Permits. The plans generally conform to the requirements applicable to this stage of the design process.

E. Fire

The Fire Department has reviewed this proposal and recommends approval subject to conditions. **Refer to Section XI.C.20 for Conditions of Approval regarding Fire Department Conditions.**

F. Addressing

The property is currently addressed as 1255 123RD AVE NE (AFN 793330-0040). The following are the proposed addresses for the project:

Parcel and Building Shell - 12280 NE DISTRICT WAY

Storefronts on NE DISTRICT WAY

- 12216 NE DISTRICT WAY
- 12296 NE DISTRICT WAY

Storefront on 123RD AVE NE

- 1275 123RD AVE NE

The final addresses for this project will depend upon the final building configuration. A change in site or building configuration may require readdressing. **Refer to Section XI.D.37 for Condition of Approval regarding Addressing of Lots, Buildings and Storefronts.**

VII. STATE ENVIRONMENTAL POLICY ACT (SEPA)

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the proposed Design Review approval that are beyond those identified in the Bel-Red Corridor EIS and the Transportation Facilities Plan EIS. The Environmental Checklist together with information submitted (in the official file) adequately discloses expected environmental impacts associated with the proposed Design Review approval. The City codes and requirements, including SEPA, Land Use Code, Noise Ordinance, Building Code and other construction codes adequately mitigate expected environmental impacts. Therefore, issuance of a Determination of Non-Significance (DNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements, with incorporation by reference of:

- The Bel-Red Corridor Project Draft and Final Environmental Impact Statement, issued July 19, 2007 and addendum.
- Final Environmental Impact Statement, 2016-2027, Transportation Facilities Plan, December 2015

These documents are available in the Development Services Department, Records Room, Lobby Floor, Bellevue City Hall, 450 110th Avenue NE under file #05-127994-LE.

Adverse impacts which are less than significant are typically subject to City Codes or Standards which are intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code, Section 22.02.140, provides substantive authority to mitigate impacts disclosed through the environmental review process.

The following amendments to the Environmental Checklist are provided by the City of Bellevue.

Transportation

Long Term Impacts and Mitigation

The long-term impacts of development projected to occur in the City by 2027 have been addressed in the City's 2016 – 2027 Transportation Facilities Plan FEIS Addendum. The impacts of growth which are projected to occur within the City by 2027 are evaluated on the roadway network assuming that all the transportation improvement projects proposed in the City's 2016 - 2027 Transportation Facilities Plan are in place. The Transportation Facilities Plan EIS divides the City into several Mobility Management Areas (MMAs) for analysis purposes. Block 14 GIX lies within MMA #12, which has a 2027 total growth projection of 3,050,584 square feet of office development (as this development is categorized) and 2,854,930 square feet of retail development. This development proposes 85,524 square feet of office development and 3,473 square feet of retail. Therefore, the volume of proposed development is within the assumptions of the Transportation Facilities Plan FEIS Addendum.

Traffic impact fees are used by the City to fund street improvement projects to alleviate traffic congestion caused by the cumulative impacts of development throughout the City. Payment of the transportation impact fee, as required by BCC 22.16, contributes to the financing of transportation improvement projects in the current adopted Transportation Facilities Plan, and is considered to be adequate mitigation of long-term traffic impacts. Fee payment is required at the time of building permit issuance. **Refer to Section XI.C.15 for Condition of Approval regarding Transportation Impact Fee.**

Mid-Range Impacts and Mitigation

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more new p.m. peak hour trips undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained.

The Spring District Development Agreement and Master Development Plan allow concurrency to be analyzed by phase. This project is part of Phase 1B of the MDP, along with the development of Block 12 and Blocks 22 and 23. A concurrency analysis and traffic impact analysis were prepared for this phase by Parametrix in March 2016.

Phase 1B will generate approximately 357 new p.m. peak hour trips, with 123 of them associated with this project. These trips were distributed and then assigned to the street network using the City's EMME-2 travel forecasting model with the current Capital Investment Program network. By adding the expected Phase 1B trips to the traffic volumes in the model, the area average levels of service were determined. To create a baseline condition for comparison, the levels of service were also determined using traffic volumes without the project-generated trips. In this project analysis, 35 system intersections received 20 or more p.m. peak hour trips.

Neither the maximum area-average levels of service nor the congestion allowances would be exceeded as a result of traffic generated from this proposal. Therefore, Phase 1B passes the concurrency test and complies with the Traffic Standards Code. The concurrency test results are included in the Transportation Department file for this development. A concurrency determination for Phase 1B is issued on the date of issuance of the first land use decision for this phase. The Phase 1B Certificate of Concurrency is issued May 5, 2016 in conjunction with this project (see Attachment I).

The development agreement for The Spring District allows the concurrency approval each phase to be vested for a period of five years. At the time of a complete building permit for each project in the phase, the concurrency reservation for each building will remain in effect for the life of the building permit application, pursuant to BCC 23.05.090.H. Upon issuance of the building permit, concurrency is reserved for the life of the building permit as provided for in BCC 23.05.100.E.

Short Term Operational Impacts and Mitigation

A traffic impact analysis was prepared for Phase 1B to determine if impacts would be created that would require mitigation. The TIA analyzed traffic operations during the p.m. peak hours for the 35 intersections in the system that are expected to receive 20 or more trips from this phase. With Phase 1B, 16 of the 20 study intersections will operate at LOS D or better. Two intersections, 148th Avenue NE/NE Bel-Red Road and 116th Avenue NE/NE 4th Street will operate at LOS E with and without the project. Two intersections, 134th Avenue NE/NE 8th Street and 132nd Avenue NE/NE 8th Street will operate at LOS F with and without the project. Phase 1B trips will cause minor increases in delays, but are not expected to adversely impact traffic operations at any of the study intersections.

This project is intended to house a university program; however, since it could be converted to office use in the future which would generate more trips than a university use, the higher office trip rate was used in the traffic analysis to be conservative. These trips were accounted for in the Phase 1B TIA, and no traffic mitigation is required for this project.

Earth

The affected geographic area is relatively level and paved with asphalt and concrete. According to the geotechnical report of record, by Hart Crowser dated October 27, 2015, very dense, gravelly, silty to very silty sand and silty gravel were encountered beneath the surface through two borings in the northwest and southwest quadrants. The soils encountered are generally consistent with glacial till deposits. During drilling at depths of 30 to 45 bgs, water was encountered; however, based on observations it is not anticipated that significant groundwater above 30 feet bgs would be encountered. Perched groundwater and seepage throughout the

subsurface, particularly where less silty zones are encountered, would be minimally expected. Groundwater encountered above the planned bottom of excavation can likely be managed using conventional sumps and pumps.

Proposed earthwork includes the excavation of approximately 41,000 bank cubic yards and 53,000 loose cubic yards of soil to be removed from the site for construction of the underground parking garage. Approximately 100 loose cubic yards of soil will be imported and used for fill within the site, if suitable. On site soils may be utilized for fill, however, this will be determined at the time of construction and will also be weather dependent. If necessary, fill trucked in from off-site will come from approved suppliers.

There is potential for erosion and sedimentation impacts as a result of the earth movement activities. Applicants and permittees are responsible for preventing erosion and discharge of sediment and other pollutants into the storm and surface water system and receiving waters. A construction stormwater pollution prevention plan (CSWPPP) prepared according to the Washington State Department of Ecology Stormwater Management minimum technical requirement # 2, will be required at time of Clearing and Grading permit application for this development. (Bellevue City Code 23.76)

By incorporating the requirements of existing codes and standards, the recommendations included in the geotechnical report (including site management techniques), use of Best Management Practices for temporary erosion and sedimentation control, and rainy season restrictions, it is expected that anticipated impacts to earth resources will be adequately mitigated.

Water

Domestic and fire service will connect to existing stubs provided to the site during the Phase 1A infrastructure for Spring District.

Sewer

Sewer service will connect to sewer stubs provided to the site during the Phase 1A infrastructure for Spring District.

Storm Water Controls

The existing project limit area is within an area currently used for light industry. This area is already paved for automobile use and storage of dry/cold consumer goods. The proposed development will decrease overall adverse impacts to the quality of surface water leaving the site. Stormwater from rooftops and roadways will be treated and conveyed through the Spring District Phase 1A storm system and water quality BMPs. Additional water quality BMPs using bio retention LID Best Management Practices are proposed to meet the requirement for water quality treatment for 122nd Avenue NE, a private road. The site proposes to decrease the amount of existing impervious surface from 100% impervious lot coverage to 87%. No detention facility will be required since the 100 year peak storm water threshold will not be exceeded which is a .10 cubic foot per second increase in the 100 year developed peak runoff. The City's Utility Codes and Engineering Standards provide adequate direction to mitigate for water quality treatment for pollutants for any impacts related to the proposal. See utility code 24.06 and 2012 Utility Engineering Standards.

Air Quality

During construction, emissions to the air will be released by construction vehicles and heavy equipment. Following construction, emissions from student vehicles will be released.

Construction would temporarily increase dust and vehicle emissions near the construction area. Mitigation will include using BMPs to control dust, covering exposed soils, and requiring idling vehicles to be shut off. Construction vehicles will be fitted with required, factory-installed emission control devices. To reduce the potential of dust, construction accesses will be covered with rock or aggregate. Dust emissions will also be reduced during construction through the use of spray water as necessary during dry weather conditions and planting disturbed areas with erosion control seed mix as soon as is practical. Material stockpiles will also be covered or watered as necessary to control dust. These are standard practices imposed on the Clearing & Grading permit. (Bellevue City Code 23.76)

Construction vehicles and heavy construction equipment shall emit the least amount of air pollution as possible. While on city streets, all construction vehicles shall meet the requirements of the Revised Code of Washington 46.61.655 for covered loads. **Refer to Section XI.B.12 for Condition of Approval regarding Air Pollution from Construction Vehicles and Equipment.**

The Bel-Red Corridor DEIS states that to the extent that pollutant-generating land uses (i.e. repair shops, drycleaners) redevelop over time within the Bel-Red Corridor, overall emissions would be lower in the future than they are now. And, that despite the predicted increase in traffic volumes and emissions, the Bel-Red Corridor redevelopment is not likely to result in any exceedance of the air quality standards. Maintaining traffic flow will reduce vehicles idling and, therefore, reduce pollutant emissions from vehicles. With the future light rail trips, pollutant emissions from vehicles are assumed to be further reduced.

The applicant provided a technical memorandum by JMJ Team, civil engineering consultants, dated October 29, 2015 (see Attachment D) which describes the anticipated greenhouse gas emissions associated with the construction and operation of the proposed GIX development.

Need for Qualitative or Quantitative Disclosure

Washington Department of Ecology (Ecology) has jurisdiction over air quality in Washington State. The City of Bellevue does not have code authority to regulate air quality. Air quality is under the jurisdiction of the Washington State Department of Ecology. Ecology provides thresholds based on building type and square footage to estimate the annual CO₂e from the buildings and associated traffic. These thresholds are used to determine the need for either a qualitative or quantitative analysis of CO₂e for a project.

Ecology's Guidance for including Greenhouse Gas Emissions in SEPA states that for projects that are expected to annually produce an average estimate of at least 10,000 but less than 25,000 metric tons CO₂e, proponents should at least qualitatively disclose the GHG emissions caused by the project. A qualitative disclosure should include a general description of the project's expected source(s) of the emissions, as well as any proposed GHG mitigation measures incorporated or designed into the project.

Qualitative Disclosure

The expected sources of emissions for GIX include embodied emissions created through the manufacturing, transportation, construction, and disposal of building materials, as well as emissions created through landscape disturbance. Emissions associated with the operation of the project include building energy usage and maintenance, as well as vehicle trips accessing the site.

The project has been estimated using the King County Department of Development and Environmental Services SEPA GHG Emissions Worksheet, Version 1.7. Using this worksheet, the project has been screened for ecology's thresholds for qualitative and quantitative reporting requirements. No mitigation reductions in MTCO₂e emissions were applied to these calculations.

As shown in the table below, the GIX project does not meet the 10,000 MTCO₂e threshold to prompt a qualitative analysis of greenhouse gas emissions, nor does it meet the 25,000 MTCO₂e threshold of new emissions to trigger a quantitative analysis.

Ecology GHG Screening Table – Parcel 14

Parcel 14					Ecology Threshold Determination	
Building Types	Units or SF	Lifespan Emissions (MTCO ₂ e)	Estimated Lifespan (Years)	Annual Emissions (MTCO ₂ e)	Qualitative Analysis (10,000 MTCO ₂ e Annually)	Quantitative Analysis (25,000 MTCO ₂ e Annually)
University/Office Space	85,524 SF	115,000	100	1,150	> 399,000 SF	> 998,000 SF
Retail	3,438 SF	2,600	100	26		
Total	88,962			1,176		

Though the project does not meet the ecology threshold of 25,000 MTCO₂e of new emissions to trigger a quantitative analysis, the low impact development methods apply to the entire Spring District MDP, which includes the subject site. Mitigation measures for potential impacts to air quality include:

1. Transit-Oriented Development: These mitigation measures include transit-oriented development (light rail station, walkable community, multi-use path with regional connection, and mixed-use development) and low impact development techniques (i.e. rain gardens, bio retention, porous concrete, and LEED® certified buildings). These techniques can reduce the emissions by at least 11% compared to the no-mitigation option.
2. Building Lifespan: These mitigation measures include applicant commitment to ecological construction, green building management and a practice of energy conservation. Other mitigation measures include adaptive building reuse, sustainably

grown and regionally produced products (i.e. extracted, harvested, or recovered within 500 miles of the project) and high-performance systems (i.e. ventilation, HVAC systems, water conservation strategies). Additional mitigation measures include applicant commitment to waste reduction, as well as, reuse and recycling in all buildings. By selecting durable and less energy consuming building components (i.e. durable shell, permanent interior systems), the applicant has a proven history of sustainable 100-year buildings.

3. Other Mitigation Measures. While it is difficult to quantify the MTCO₂e reductions of many low impact development techniques, the following features will provide additional emissions reductions:
 - **Bike and Pedestrian Facilities:** Bike and pedestrian facilities will encourage non-motorized transportation that will further reduce the number of vehicle trips.
 - **LEED Building Design:** Buildings and site design meeting LEED standards will result in energy efficiencies and possible water reuse. During construction, materials will be recycled where possible. This mitigation reduces stationary combustion emission sources.
 - **Low Impact Development (LID):** LID techniques include bio retention cells (rain gardens) to treat pollution-generating surfaces and green roof treatments.

Plants and Animals

The existing 1.4-acre project limit was a light industrial site with 100% existing impervious surface. Upon completion of the Design Review proposal, the overall impervious surface will be reduced from 100% to 87%. With the installation of site landscaping i.e. plaza and streetscape, there will be the opportunity for new plant material (native/ornamental). It is anticipated that small urban animals and native birds will occupy the site area.

Noise

As conditioned, short term impacts related to noise generation as a result of the construction will be minimized. Noise related to construction is permitted from 7:00 a.m. to 6:00 p.m., Monday through Friday, and 9:00 a.m. to 6:00 p.m. on Saturday, except for Federal holidays and as further defined by the Bellevue City Code. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020.C.1 when necessary to accommodate construction which cannot be undertaken during exempt hours. Written requests for exemption from the Noise Control Code must be submitted two weeks prior to the scheduled onset of extended hour construction activity. The use of best available noise abatement technology consistent with feasibility is required during construction to mitigate construction noise impacts to surrounding uses. **Refer to Section XI.A.6 and XI.A.7 for Conditions of Approval regarding Construction Noise Hours and Use of Best Available Noise Abatement Technology.**

Light and Glare

There is the potential for light and glare with the exterior building lights during the night from an office building and the commercial tenant spaces. Potential light and glare impacts will extend to

adjacent residential units and any businesses that operate after dark, in addition to vehicular/pedestrian traffic along adjacent streets. In order to mitigate potential impacts to adjacent vehicular drivers/businesses/residents, any light source emitting from the project area shall incorporate cutoff shields and be designed so as not to result in light and glare spillover offsite. **Refer to Section XI.C.29 for Condition of Approval regarding Shielded Lights.**

VIII. CHANGES TO PROPOSAL DUE TO CITY REVIEW

A. Transportation Department

As a result of review from the Transportation Department, the applicant provided the following changes:

- The design of the building near the garage entrance limits the ability of exiting drivers to see pedestrians using the sidewalk on 123rd Avenue NE. To provide pedestrian sight distance from the garage entrance, the sidewalk was transitioned to be farther from the building through the use of planter beds adjacent to the south and north sides of the driveway entrance.

B. Land Use Division

The Land Use Division reviewed the proposal and requested the following:

- In order to meet the setback requirements of LUC 20.25D.080.A (5), the building was slightly shifted in order to result in the required 15 foot deep setback on 123rd Avenue NE and NE District Way. These setbacks have been incorporated into the design of the building as exterior decks.
- A 9' diameter and 10 foot tall exterior garage exhaust was proposed adjacent to the northwest corner of the building, within the outdoor plaza area. Working with the applicant, the garage exhaust has been reduced to the maximum extent permitted under the mechanical code, and has been architecturally treated with a board form concrete design to match other planter walls and seat walls within the outdoor plaza. In addition, bike racks will be installed on the north side of the exhaust to provide a clever way to encourage alternative forms of transportation.
- Due to the removal of Tract D north of the site, which was to provide vehicular and pedestrian access between 123rd Avenue NE and 122nd Avenue NE, a pedestrian easement will be required in its place over the outdoor plaza to ensure pedestrian and bicycle access through this area of The Spring District MDP.

IX. DECISION CRITERIA

Per Land Use Code 20.30F.145, the Director may approve or approve with modifications an application for Design Review if:

A. The proposal is consistent with the Comprehensive Plan.

Finding: The proposal, as conditioned, is consistent with the Comprehensive Plan and Bel-Red Subarea Plan. Staff has reviewed the policies which apply to the project and has determined that those applicable policies will be implemented through the application of City Codes and the adopted Design Guidelines identified in the Bel-Red Chapter of the Land Use Code. **Refer to Section XI.A.5, XI.A.9, XI.B.13, XI.C.24, XI.C.25, XI.E.40 and XI.F.41 for Conditions of Approval regarding Project Plans, Conditions of Approval, Final Landscape Plan, Vision Glass, Building**

Materials/Details and Color Samples, Sign Master Plan and Future Tenant/Storefront Improvements.

The proposal is consistent with the Comprehensive Plan, including the following goals and policies from the Bel-Red Subarea Plan:

1) ***Vision:***

- *The Bel-Red corridor in 2030 will be an area that is unique within the city of Bellevue and the entire Puget Sound region. It will be an area where thriving businesses will be adjacent to, and sometimes mixed with, livable neighborhoods, all served by a multi-modal transportation system that connects the area to the greater city and region. The area will also be distinguished by environmental and community amenities that will serve residents and employees in the area, as well as residents from surrounding neighborhoods and the entire city. The area will transition gracefully over time, with existing businesses being accommodated while new types of development will occur as conditions warrant.*
- *Bel-Red will be home to major employers, types of businesses and employment sectors unique to this part of Bellevue, and services that are important to the local community.*
- *A comprehensive, connected parks and open space system. System components will include trails along stream corridors, urban and natural open space areas, community facilities, community and neighborhood parks, and cultural/arts features.*
- *Vibrant, diverse and walkable neighborhoods: Neighborhoods will have a pedestrian friendly and walkable character, with convenient access to shopping, jobs, and community amenities, and will also be well connected to the larger city and region.*

Finding:

The proposed Global Innovation Exchange (GIX) project is consistent with the goals and policies of the Bel-Red Subarea Plan. The proposal implements the vision of the Bel-Red Subarea Plan by providing an office building for a new educational venture between the University of Washington and Tsinghua University. Retail businesses will be located at the ground level of the building, which helps to provide activation of the sidewalk around the building, and to provide uses for the adjacent residential units to the south and south west. In addition, further enhancement of pedestrian and bicycle activity will be provided within the outdoor plaza area north of the building, between 123rd Avenue NE and 122nd Avenue NE.

- 2) ***Land Use Goal:*** *To develop a land use pattern that is environmentally sustainable and economically vibrant, and that creates distinctive new commercial and residential neighborhoods for the Eastside.*

POLICY S-BR-6. *Concentrate the majority of future Bel-Red growth into a series of mixed use, pedestrian-friendly and transit-oriented development nodes, with higher density and height therein, as enabled through a land use incentive system. Within each node, provide for tiered building heights, with*

maximums at the center.

POLICY S-BR-8. *Encourage mixed use development, promoting opportunities to live, work, shop, and recreate within close proximity.*

Finding:

The proposed Global Innovation Exchange (GIX) proposal meets the Land Use goals and policies of the Bel-Red Subarea plan noted above. The university/office building will be one of the first office buildings to be constructed in the 36-acre Spring District Master Development Plan area. It should be noted that building 16, which is also an office type use, has received design review approval, but construction has not yet commenced. The GIX building will provide the first educational use within the Spring District MDP, further diversifying the type of uses throughout the MDP. Small retail spaces at the ground level will provide for a mixed-use type of development which supports both the university/office use, as well as the adjacent residential units to the south and southwest of the subject site. In addition, the outdoor plaza north of the building provides a space not only for university students, but also residents and other employees a space to recreate and to connect between larger block developments within the overall development.

- 3) ***Urban Design Goal:*** *To achieve a design character that results in aesthetically beautiful, distinctive, and long-lasting places that evoke a strong sense of Bellevue and the Northwest, and a dynamic public realm that encourages social interaction.*

POLICY S-BR-14. *Use design guidelines to promote pedestrian-friendly and transit-oriented design, ensure quality and a sense of permanence, promote environmental sustainability, and create a distinct sense of place. Conduct design review for all mixed use, office and residentially designated areas of the Subarea. Apply additional depth and attention to the details of design review within transit-oriented development nodes.*

Discussion: Design review should pay special attention to creating a pedestrian-friendly environment, by helping to create vibrant, interesting, safe, walkable and interconnected sites.

POLICY S-BR-16. *Encourage place-making and a dynamic public realm by integrating publicly accessible plazas, open spaces and other gathering places with development, in public and private projects.*

POLICY S-BR-18. *Encourage diversity in the built environment through a variety of building heights and forms, building articulation and modulation. Encourage building rooflines and floorplates that break down the scale of buildings, help to differentiate Bel-Red from Downtown, and enhance the architectural variety of the area.*

Policy S-BR-19. *Encourage the use of building materials that are of high quality and durability, are appropriate for the area climate, and that have a*

sense of permanence.

Policy S-BR-20. Reinforce the area's sense of place and Northwest provenance by encouraging innovative building and site design, construction techniques and materials that reflect the industrial roots of the area while emphasizing the emerging urban character of Bel-Red.

POLICY S-BR-25. Design and develop an outstanding street environment that promotes streets as key urban places, sensitive to their context and providing an interesting and aesthetically rich experience. Apply a street hierarchy with design guidelines and street standards that provides an appropriate combination of the following elements:

- a. *Strong consideration of character and aesthetics in the design and implementation of all street projects;*
- b. *Integration of open space and landscaping, including street trees;*
- c. *Environmentally sensitive practices, including natural drainage systems where appropriate;*
- d. *Sidewalk development standards that promote pedestrian functionality and interest, and avoid obstructions;*
- e. *Ground floor differentiation, including preferred uses, visual and physical access;*
- f. *Mid-block pedestrian crossings; and*
- g. *On-street parking, where it contributes to pedestrian convenience and safety.*

Finding:

The Urban Design goals and policies of the Bel-Red Subarea Plan are incorporated into the Global Innovation Exchange (GIX) proposal. The proposed university/office building will create a distinct sense of place at the first eastern intersection within the Spring District MDP (corner of NE District Way and 123rd Avenue NE). The project is subject to the plans submitted and attached. **Refer to Section XI.A.5 for Condition of Approval regarding Project Plans.** The GIX building will be a 3-story university/office building that incorporates industrial type materials with the use of brick and steel trim. The angular metal roof structure of the building will provide visual interest and result in an iconic building when viewed in relation of the remaining buildings within the Spring District MDP.

The proposed outdoor plaza on the north side of the building is designed to complement the building structure, while paying homage to the Asian culture. The landscape includes a reflexology path, special landscape plantings, including Vine Maple and Katsura trees, and a Dawn Redwood tree, which was a gift from the President of Tsinghua University. As the plaza provides a pedestrian and bicycle connection between 123rd Avenue NE and 122nd Avenue NE, public access signage will be required to be installed to clearly indicate this public connection through the project site. **Refer to Section XI.D.39 for Condition of Approval regarding Public Access signage for Pedestrian Connections.**

This project also intends to create pedestrian-friendly sidewalks along NE District Way, 123rd Avenue NE and 122nd Avenue NE. NE District Way will connect to adjacent amenities, such as the existing park and proposed Brew Pub to the west, future light rail to the north, and retail/restaurant uses with the building and on the corner of NE District Way and 123rd Avenue NE. Overall, the sidewalk layout, including streetscape planter design will continue to match those currently existing along NE District Way and those approved under the master clear and grade infrastructure permit for The Spring District.

The applicant will provide a master sign package for consistency in signage within the project. **Refer to Section XI.E.40 for Condition of Approval regarding Sign Master Plan.**

- 4) ***Environment:*** *To redevelop the Bel-Red area as a model of environmental sustainability, realizing opportunities provided by new development to achieve significant improvements over current conditions.*

Policy S-BR-28. *Encourage natural drainage practices where feasible in public and private projects, as an alternative to traditional stormwater treatment and control. Allow natural drainage practices to offset traditional treatment and control standards to the extent practicable, and provide other incentives to promote their use if needed.*

Finding:

Along the streets within The Spring District, low impact development techniques in the form of stormwater bio retention facilities are located within streetscape planter areas.

- 5) ***Parks and Open Space Goal:*** *To create a robust, aesthetically beautiful, and functional parks and open space system that serves the needs of Bel-Red and the broader community, and that connects with and complements the larger Bellevue parks and open space system.*

POLICY S-BR-39. *Promote the development of “green streets” throughout the corridor, with an abundance of street trees and areas of landscaping to improve and reduce the amount of stormwater runoff, be aesthetically pleasing, and provide an attractive pedestrian experience.*

Finding:

Along the streets within The Spring District, the original developer, Wright-Runstad, has provided the required local street landscaping, which includes street trees, planting strips, tree wells and rain gardens. In addition, a public park has been created west of the project site, which will provide recreational opportunities for residents of the development.

- 6) ***Transportation Goal:*** *To create a more complete, connected, and well balanced transportation system, while protecting neighborhoods from spillover traffic impacts and while ensuring that transportation investments contribute to the area’s sense of place and sustainability.*

POLICY S-BR-57. Encourage garage and service vehicle access via local and secondary streets and alleys. Limit access points along arterial streets.

POLICY S-BR-63. Improve pedestrian connectivity and the quality of the pedestrian environment with a comprehensive sidewalk and trail system, including through-block pedestrian connections, and mid-block crossings. Include pedestrian amenities such as pedestrian-scaled lighting, seating, transit shelters and weather protection.

Finding:

The developer, Wright-Runstad, has provided the required local streets for the subject site, which include on-street parking areas. The proposed project will include a singular garage access point off 123rd Avenue NE, in compliance with the restricted driveway access provisions within the LUC. General loading and deliveries will be located within a parking bay adjacent to 122nd Avenue NE, which is a private road. A pedestrian connection will be established north of the proposed building within the outdoor plaza area, which will connect office buildings along 123rd Avenue NE, westward toward the public park areas and then northward toward the light rail station.

B. The proposal complies with the applicable requirements of this Code.

As conditioned, the proposal complies with applicable requirements of the Land Use Code:

- Dimensional requirements.
- Parking/Loading requirements.
- Local Street requirements.
- Building and Site Design requirements.
- FAR Amenity Incentive System.
- Use charts.
- Catalyst Project.
- Bel-Red Corridor Plan.
- The Spring District Master Development Plan.
- The Spring District Development Agreement.

See Section III, Consistency with Land Use Code/Zoning Requirements.
See Section V, Technical Review.

C. The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent.

As conditioned, the proposal complies with applicable requirements of the Land Use Code. Refer to Section I.B, Building Design and I.C, Design Intent, for discussions on overall design of the proposal. In addition, refer to Section III, Consistency with Land Use Code/Zoning Requirements and Section IV, Consistency with the Spring District Development Agreement (DA) and Master Development Plan (MDP for additional design

requirements.

D. The proposal is compatible with and responds to the existing or intended character, appearance, quality of development and physical characteristics of the subject property and immediate vicinity.

As conditioned, the project meets this criterion. See responses to Criteria VIII.A-C above.

E. The proposal will be served by adequate public facilities including streets, fire protection, and utilities.

All urban level public utilities/infrastructure are available to the site or will be constructed/installed within the development. All city departments have reviewed the proposal and required associated conditions, as necessary. See Section V. for Technical Review.

X. DECISION

After reviewing the proposal for consistency with applicable City of Bellevue requirements, policies, development standards, and SEPA, the Director hereby **APPROVES WITH CONDITIONS THE DESIGN REVIEW APPLICATION FOR THE SPRING DISTRICT BLOCK 14 – GLOBAL INNOVATION EXCHANGE (GIX).**

XI. CONDITIONS OF APPROVAL

COMPLIANCE WITH BELLEVUE CITY CODES AND ORDINANCES:

NOTE – Vested Status of the Design Review approval: The vested status of the Design Review approval is per Land Use Code 20.25D.035.B and the Development Agreement, signed July 10, 2009 and recorded September 11, 2009, recording number #200909110002269. These provisions allow for extended vesting up to a maximum of 15 years from the date of the City's final decision, unless a completed building permit application is filed before the end of the 15 year term. Upon issuance of a building permit, the vested status of a land use permit or approval shall be automatically extended for the life of the project.

COMPLIANCE WITH BELLEVUE CITY CODES AND ORDINANCES: The applicant shall comply with all applicable Bellevue City Codes (BCC) and Ordinances including but not limited to:

Clearing and Grading Code - BCC 23.76	Savina Uzunow	425-452-7860
Bellevue Development Standards	Savina Uzunow	425-452-7860
Transportation Code - BCC 14.60	Molly A. Johnson	425-452-6175
Trans. Development Review - BCC.22.16	Molly A. Johnson	425-452-6175
Right-of-Way Use Permit - BCC 14.30	Tim Stever	425-425-4294
Bellevue Utilities Code - BCC Title 24	Chris Brookes	425-452-6825
Construction Codes - BCC Title 23	Mark Chang	425-452-6997
Structural Codes – BCC Title 23	Mark Chang	425-452-6997
Land Use Code - BCC Title 20	Laurie Tyler	425-452-2728
Sign Code - BCC Title 22B	Laurie Tyler	425-452-2728
Noise Control - BCC 9.18	Laurie Tyler	425-452-2728
Uniform Fire Code - BCC 23.11	Travis Ripley	425-452-6042
Transportation Department Design Manual	Molly A. Johnson	425-452-6175

The Bel-Red Corridor Plan Streetscape Character,
Guidelines, and Standards
Addressing

Molly A. Johnson
Laurie Tyler
Jami Carter

425-452-6175
425-452-2728
425-452-4310

A. GENERAL CONDITIONS: The following conditions apply to all phases of development.

TRANSPORTATION DEPARTMENT CONDITIONS (GENERAL)

1. PROVISIONS FOR LOADING

The property owner shall provide an off-street loading space which can access a public street. This must include an off-street location for garbage pick-up, which must be acceptable to the garbage hauler. On-street loading and unloading in the public right-of-way will not be permitted.

AUTHORITY: LUC 20.20.590.K.4; BCC 14.60.180
REVIEWER: Molly Johnson, Transportation Department
Laurie Tyler, Development Services Department

2. TRANSPORTATION MANAGEMENT PROGRAM

If the use of this property changes from university to another occupancy, the owner of the property shall sign and record at the King County Office of Records and Elections an agreement to establish a Transportation Management Program to the extent required by Sections 14.60.070.

AUTHORITY: Bellevue City Code 14.60.070
REVIEWER: Molly Johnson, Transportation Department

UTILITIES DEPARTMENT CONDITIONS (GENERAL)

3. PRELIMINARY DESIGN, UTILITY CODES AND ENGINEERING STANDARDS

Utility review has been completed on the preliminary information submitted at the time of this application. The review has no implied approvals for water, sewer and storm drainage components of the project. A Utility Extension Agreement will be required for review and approval of the utility design for water, sewer, and storm and has already been submitted under permit number 16-123956 UE. The applicant will be required to apply for a separate over-the-counter water service application for any additional water connections or services. The side sewer connection will be reviewed and permitted under a separate commercial side sewer permit. The applicant may apply for the side sewer connection permit when preconstruction items are submitted. Submittal of the Utility Extension will coincide with future clearing and grading permit review. Final civil engineering may require changes to the site layout to accommodate the utilities.

AUTHORITY: Bellevue City Code Title 24.02, 24.04, 24.06
REVIEWER: Chris Brookes, Utilities

LAND USE DIVISION CONDITIONS (GENERAL)

4. DEVELOPMENT AGREEMENT AND MASTER DEVELOPMENT PLAN

The applicant is subject to all conditions and requirements of the Development Agreement between the City and Applicant, signed July 10, 2009 and recorded

September 11, 2009 recording number #200909110002269. The applicant is also subject to all approval conditions of the Master Development Plan (File number 11-125943 LP) approved May 3, 2012 by Development Services and affirmed by the Hearing Examiner on October 8, 2012 (File number AAD 12-41).

AUTHORITY: Development Agreement, signed July 10, 2009 and recorded September 11, 2009, recording number #200909110002269 Master Development Plan (File number 11-125943 LP) approved May 3, 2012 by Development Services and affirmed by the Hearing Examiner on October 8, 2012 (File number AAD 12-41).

REVIEWER: Laurie Tyler, Development Services Department

5. PROJECT PLANS

The project is subject to the plans submitted April 14, 2016 and attached.

AUTHORITY: Bellevue City Code 20.35.230

REVIEWER: Laurie Tyler, Development Services Department

6. CONSTRUCTION NOISE HOURS

Noise related to construction is allowed from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday, except for Federal holidays and as further defined by the Bellevue City Code. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020C.1 when necessary to accommodate construction which cannot be undertaken during exempt hours. Written requests for exemption from the Noise Control Code must be submitted two weeks prior to the scheduled onset of extended hour construction activity.

AUTHORITY: Bellevue City Code 9.18.020, .040

REVIEWER: Laurie Tyler, Development Services Department

7. USE OF BEST AVAILABLE NOISE ABATEMENT TECHNOLOGY

The use of best available noise abatement technology consistent with feasibility is required during construction to mitigate construction noise impacts to surrounding uses.

AUTHORITY: Bellevue City Code 9.18.020.F

REVIEWER: Laurie Tyler, Development Services Department

8. MODIFICATION TO THE DESIGN REVIEW PLANS

Any modification to this approval shall be documented as an Amendment to this Design Review OR as an addition or revision to this issued land use approval, processed as a Land Use Exemption. The applicant shall demonstrate compliance with the Land Use Code consistent with the Governing Regulations section of the Development Agreement. Any modification of the project design must be reviewed for consistency with the design intent as stated in Section I.C of this report. Conditions of Approval run for the life of the project.

AUTHORITY: Land Use Code 20.30F.175, Development Agreement Section C

REVIEWER: Laurie Tyler, Development Services Department

9. CONDITIONS OF APPROVAL

A copy of these conditions of approval shall be copied onto a plan sheet at the beginning of the plan set(s) submitted for building permits.

AUTHORITY: Land Use Code 20.35.230
REVIEWER: Laurie Tyler, Development Services Department

- B. PRIOR TO CLEARING & GRADING (CG) PERMIT ISSUANCE:** The following conditions are imposed to ensure compliance with the relevant decision criteria and Code requirements. These conditions must be complied with on plans submitted with the Clearing & Grading and Demolition permit applications:

TRANSPORTATION DEPARTMENT CONDITIONS (prior to CG issuance)

10. RIGHT-OF-WAY USE PERMIT

Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

- a) Designated truck hauling routes.
- b) Truck loading/unloading activities.
- c) Location of construction fences.
- d) Hours of construction and hauling.
- e) Requirements for leasing of right of way or pedestrian easements.
- f) Provisions for street sweeping, excavation and construction.
- g) Location of construction signing and pedestrian detour routes.
- h) All other construction activities as they affect the public street system.

In addition, the applicant shall submit for review and approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevent access. General materials storage and contractor convenience are not reasons for preventing access.

The applicant shall secure sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.

AUTHORITY: Bellevue City Code 11.70 & 14.30
REVIEWER: Tim Stever, Right of Way

11. CIVIL ENGINEERING PLANS – TRANSPORTATION

Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all street frontage improvements and driveway accesses must be in conformance with the requirements of the Americans with Disabilities Act, the Transportation Development Code, the provisions of the Transportation Department Design Manual, and specific requirements stated elsewhere in this document. All relevant standard drawings from the Transportation Department Design Manual shall be

copied exactly into the final engineering plans. Requirements for the engineering plans include, but are not limited to:

- a) Traffic signs and markings.
- b) Curb, gutter, sidewalk, and driveway approach design for the frontage improvement modifications. The engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as needed.
- c) Curb ramps and crosswalk revisions.
- d) Installation or relocation of streetlights and related equipment.
- e) Undergrounding of existing overhead utility lines, which should be coordinated with adjacent sites. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.
- f) Sight distance. Show the required sight triangles and include any sight obstructions, including those off-site. Sight distance triangles must be shown at all driveway locations and must consider all fixed objects and mature landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance.
- g) Landings on sloping approaches are not to exceed a 10% slope for a distance of 20 feet approaching the back edge of sidewalk. Driveway grade must be designed to prevent vehicles from bottoming out due to abrupt changes in grade.
- h) Driveway aprons must be constructed in accordance with Design Manual Standard Drawing DEV-7E, with a minimum width of 26 feet.
- i) Location of fixed objects in the sidewalk or near the driveway approach.
- j) Trench restoration within any right of way or access easement.

Construction of all street and street frontage improvements must be completed prior to closing the clear and grade permit and right of way use permit for this project. A Design Justification Form must be provided to the Transportation Department for any aspect of any pedestrian route adjacent to or across any street that cannot feasibly be made to comply with ADA standards. Design Justification Forms must be provided prior to approval of the clear and grade plans for any deviations from standards that are known in advance. Forms provided in advance may need to be updated prior to project completion. For any deviations from standards that are not known in advance, Forms must be provided prior to project completion.

AUTHORITY: BCC 14.60; Transportation Department Design Manual;
Americans with Disabilities Act
REVIEWER: Molly Johnson, Transportation Department

LAND USE DIVISION CONDITIONS (prior to CG issuance)

12. AIR POLLUTION FROM CONSTRUCTION VEHICLES AND EQUIPMENT

Construction vehicles and heavy construction equipment shall emit the least amount of air pollution as possible. While on city streets, all construction vehicles shall meet the requirements of the Revised Code of Washington 46.61.655 for covered loads.

AUTHORITY: State Environmental Policy Act, Bellevue City Code, 23.76,
Revised Code of Washington 46.61.655
REVIEWER: Laurie Tyler, Development Services Department

13. FINAL LANDSCAPE PLAN

A final landscape plan and irrigation plan shall be submitted with the Clearing and Grading application for review by Land Use. Provide details for any electrical outlets that are proposed to be located within the planting strips.

AUTHORITY: Land Use Code 20.20.520.I
REVIEWER: Laurie Tyler, Development Services Department

14. STREET FURNITURE

Street furniture is required along 123rd Avenue NE. The type of furniture, locations and specifications shall be approved by Land Use and the Transportation Department.

AUTHORITY: Land Use Code 20.25D.140.G
REVIEWER: Laurie Tyler, Development Services Department
Molly Johnson, Transportation Department

- C. PRIOR TO BUILDING PERMIT (BP) ISSUANCE:** The following conditions are required by City Code. Unless specified otherwise below, these conditions must be complied with on plans submitted with the Building Permit application:

TRANSPORTATION DEPARTMENT CONDITIONS (prior to BP issuance)

15. TRANSPORTATION IMPACT FEE

Payment of the traffic impact fee will be required at the time of building permit issuance. If multiple building permits will be issued, the impact fee will be tied to the primary above-ground permit. Impact fees are subject to change and the fee schedule in effect at the time of building permit issuance will apply.

AUTHORITY: Bellevue City Code 22.16
REVIEWER: Molly Johnson, Transportation Department

16. BUILDING AND SITE PLANS – TRANSPORTATION

The building grade and elevations shall be consistent with the curb and sidewalk grade shown in the approved civil engineering plans. During construction, city inspectors may require additional survey work at any time in order to confirm proper elevations. Building plans, landscaping plans, and architectural site plans must accommodate on-site traffic markings and signs and driveway design as specified in the engineering plans. Building plans, landscaping plans, and architectural site plans must comply with vehicle and pedestrian sight distance requirements, as shown on the engineering plans.

AUTHORITY: BCC 14.60.060; 110; 120; 150; 180; 181; 190; 240; 241
REVIEWER: Molly Johnson, Transportation Department

17. EXISTING EASEMENTS

Any utility easements contained on this site which are affected by this development must be identified. Any negative impact that this development has on those easements must be mitigated or easements relinquished.

AUTHORITY: Bellevue City Code 14.60.100

REVIEWER: Tim Stever, Right of Way

18. SIDEWALK/UTILITY/ACCESS EASEMENTS

The applicant shall provide sidewalk, utility, and access easements to the City such that sidewalks and pedestrian paths outside of the City right of way are located within a pedestrian easement area. This includes the sidewalks on NE District Way, 123rd Avenue NE, 122nd Avenue NE, and the pedestrian corridor on the north side of Block 14. A vehicle access easement shall be provided for the 122nd Avenue NE.

AUTHORITY: Bellevue City Code 14.60.100
REVIEWER: Molly Johnson, Transportation Department

19. DEDICATION OF RIGHT OF WAY

The applicant shall dedicate right of way to the City along the property frontage such that street improvements to the back of curb are located within the public right of way, prior to issuance of the primary above grade building permit.

AUTHORITY: Bellevue City Code 14.60.090
REVIEWER: Molly Johnson, Transportation Department

FIRE DEPARTMENT CONDITIONS (prior to BP issuance)

20. FIRE DEPARTMENT CONDITIONS

(a) ACCESS ROAD

The access road shall be signed and posted "No Parking-Fire Lane" per Bellevue Standards. If the road is 20 feet wide, the posting shall be on both sides.
(AUTHORITY: Bellevue Amended (BA) International Fire Code (IFC) 503.3)

(b) TURNAROUND

Provide an approved turnaround within 150 feet of a dead end street.
(AUTHORITY: Bellevue Amended International Fire Code (IFC) 903.2.5)

(c) LOADING

Vehicle load/unloading shall not obstruct the fire lane access.
(AUTHORITY: Bellevue Amended International Fire Code (IFC) 503.2.1)

(d) FIRE FLOW

Provide the fire flow required for the building.
(AUTHORITY: International Fire Code (IFC) Appendix B)

(e) FIRE SPRINKLERS

Provide automatic fire sprinklers designed per NFPA 13 throughout the building.
(AUTHORITY: International Fire Code 903)

(f) FIRE DEPARTMENT CONNECTION

Provide a fire department connection at an approved location at the address of the building.
(AUTHORITY: International Fire Code 903 & 912)

(g) FIRE ALARM

Provide a fire alarm notification system throughout the building.
(AUTHORITY: International Fire Code 907)

(h) EMERGENCY RESPONDER RADIO

Provide an emergency responder radio coverage system throughout the building.
(AUTHORITY: Bellevue Amended IFC 510)

(i) DEMOLITION AND CONSTRUCTION

Demolition and construction shall conform to the requirement of International Fire Code Chapter 33.

(j) SCHOOL EQUIPMENT

Provide information on all equipment and installation of the dust collector for the wood shop.
(AUTHORITY: International Fire Code Chapter 22)

(k) ADDRESSING

Provide addressing for the building before submitting a building permit.
(AUTHORITY: International Fire Code IFC 505)

(l) ELEVATOR

Provide full information about the elevator type and equipment to be used.
(AUTHORITY: International Fire Code IFC 607)

(m) EXTERIOR DOORS

Provide door handles on each exterior door for fire department access.
(AUTHORITY: International Fire Code IFC 504)

(n) KNOX BOX

Provide Knox Boxes with keys to all doors at approved access points around the building.
(AUTHORITY: International Fire Code IFC 506)

AUTHORITY: As noted above.
REVIEWER: Travis Ripley, Fire Department

LAND USE DIVISION CONDITIONS (prior to BP issuance)

21. MAXIMUM IMPERVIOUS SURFACE/LOT COVERAGE

The maximum impervious surface/lot coverage shall be 75% per Land Use Code 20.25D.080.

AUTHORITY: Land Use Code 20.25D.080
REVIEWER: Laurie Tyler, Development Services Department

22. GARAGE VENTS

Garage vents shall be directed away from pedestrians. Objectionable odors shall not

emit from vents. All vents shall be architecturally integrated to the building design.

AUTHORITY: Land Use Code 20.25D.150.D
REVIEWER: Laurie Tyler, Development Services Department

23. CERTIFICATION BY A NOISE CONSULTANT

The applicant shall provide certification by a noise consultant that the noise from the exhaust fans will not exceed 60 dBA and a determination from the noise consultant that the velocity and direction of airflow from the exhaust system within pedestrian areas not exceed 500 CFM.

AUTHORITY: Bellevue City Code 9.18.030 and Land Use Code 20.30F.145
REVIEWER: Laurie Tyler, Development Services Department

24. VISION GLASS

The applicant shall portray vision glass along the storefronts and office lobbies to provide a high level of pedestrian interest. Storefronts and office lobbies shall not be substantially closed or blocked from view.

AUTHORITY: Land Use Code 20.25D.150.D
REVIEWER: Laurie Tyler, Development Services Department

25. BUILDING MATERIALS/DETAILS AND COLOR SAMPLES

The development is subject to the building materials/details and color samples as indicated on the attached plans.

AUTHORITY: Land Use Code 20.25D.150.D
REVIEWER: Laurie Tyler, Development Services Department

26. GARAGE SOFFIT VIEWS

Sections submitted for building permit applications shall provide finish for any soffits at the second floor under the slab insulation where it may be visible to public. Any sections of the soffit insulation that will be visible when the garage door is open must be finished at the entry in a manner consistent with the design of the building.

AUTHORITY: Land Use Code 20.25D.150.C
REVIEWER: Laure Tyler, Development Services Department

27. MECHANICAL EQUIPMENT SCREENING

Roof plans submitted for the building permit application shall indicate locations of mechanical equipment, including any communication equipment such as satellite dishes. The applicant shall demonstrate that screening for rooftop mechanical equipment is architecturally integrated with the building. Screening shall be provided so that rooftop mechanical equipment is not visible from adjacent streets and public sidewalks. All HVAC rooftop equipment shall be consolidated and screened, including painting of equipment/screening as necessary.

Power, telephone, traffic control, or other equipment shall not be located in above ground cabinets in sidewalk areas. Such equipment shall be located in underground vaults or in a building or substantially screened per the approval of the Development

Services Department.

Any at grade grills/grates or panels shall be the minimum size necessary, be ADA compliant and flush with the sidewalk or driveway in which it is installed. Review and approval of these locations and grates will be through the building permit review process.

AUTHORITY: Land Use Code 20.25D.150.D, 20.20.525.C.5
REVIEWER: Laurie Tyler, Development Services Department

28. SOLID WASTE/RECYCLING COLLECTION FACILITIES REQUIRED

Prior to the issuance of any construction permit, the applicant shall provide a written document demonstrating that Republic Services (City's waste hauler service) has been contacted and has approved the proposed sizing of and access to the recycling and solid waste collection area(s) for this building, using current standards. In addition, all rights of way and public easement shall not be occupied by trash receptacles, dumpsters, recycling bins or other such items. Collection and storage facilities for recyclable materials shall be shown on building and site plan drawings. This area shall not be visible from the street.

AUTHORITY: Land Use Code 20.20.720
REVIEWER: Laurie Tyler, Development Services Department

29. SHIELDED LIGHTS

In order to mitigate potential impacts to adjacent vehicular drivers/businesses/residents, any light source emitting from the project area shall be designed so as not to provide light and glare and spillover offsite. Cutoff shields shall be used.

AUTHORITY: Land Use Code 20.20.522
REVIEWER: Laurie Tyler, Development Services Department

30. BICYCLE PARKING

The applicant shall provide bicycle parking in accordance with the Bel-Red Land Use Code 20.25D.120.G.

AUTHORITY: Land Use Code 20.25D.120.G
REVIEWER: Laurie Tyler, Development Services Department

31. PAYMENT OF FEE-IN-LIEU OF FLOOR AREA AMENITIES

The applicant intends to meet the FAR Amenity System (LUC 20.25D.090) code requirements through payment of fee-in-lieu to satisfy all tiered bonus categories triggered. The applicant shall pay the city the fee-in-lieu of the floor area amenities at building permit issuance. Final fee amount to be re-assessed at time of building permit issuance.

AUTHORITY: Land Use Code 20.25D.090.C.6; Development Agreement,
Paragraph F.1, Recording Number 20090911000269
REVIEWER: Laurie Tyler, Development Services Department

32. NON-RESIDENTIAL PARKING STALL IDENTIFICATION

Plans submitted for building permits shall indicate the locations of retail parking stalls for this development. Prior to Temporary Certificate of Occupancy (TCO), the applicant shall install signs which clearly designate the commercial parking stalls within the parking garage.

AUTHORITY: Land Use Code 20.20.590 & 20.25D.120
REVIEWER: Laurie Tyler, Development Services Department

D. PRIOR TO ISSUANCE OF ANY CERTIFICATE OF OCCUPANCY (CO)

TRANSPORTATION DEPARTMENT CONDITIONS (prior to issuance of any CO)

33. STREET FRONTAGE IMPROVEMENTS

All street frontage improvements and other required transportation elements, including street light and traffic signal revisions, must be constructed by the applicant or others and accepted by the Transportation Department inspector. This includes all frontage improvements for NE District Way, 123rd Avenue NE, and 122nd Avenue NE that are being constructed through existing permits by others.

All existing street light and traffic signal apparatus affected by this development, including traffic controllers, pedestrian signal poles, traffic signal poles, and power sources, must be relocated as necessary. Existing overhead lines must be relocated underground. All required improvements must be constructed as per the approved plans or as per direction of the Transportation Department inspector. Bonding or other types of assurance devices will not be accepted in lieu of construction, unless the City requires a delay.

AUTHORITY: BCC 14.60; Comprehensive Plan Policy UT-39; Transportation Department Design Manual; and Transportation Department Design Manual Standard Drawings.
REVIEWER: Molly Johnson, Transportation Department

34. PAVEMENT RESTORATION

Pavement restoration associated with street frontage improvements or to repair damaged street surfaces shall be provided as follows:

NE District Way and 123rd Avenue NE: These streets were recently completed and a five year no-street cut moratorium will be in effect beginning after they are accepted into the system. Should street cuts prove unavoidable or if the street surface is damaged in the construction process, a half-street or full-street (depending on the extent of street cuts or damage) grind and overlay will be required.

AUTHORITY: BCC 14.60. 250; Design Manual Design Standard #23
REVIEWER: Tim Stever, Right of Way

LAND USE DIVISION AND ADDRESSING CONDITIONS (prior to issuance of any CO)

35. LANDSCAPE INSTALLATION ASSURANCE DEVICE

If a Temporary Certificate of Occupancy is requested prior to completion of the landscaping installation, the applicant shall file with the Development Services Department a landscape installation assurance device equal to 150% of the cost of labor and materials for any landscaping that has not yet been installed.

AUTHORITY: Land Use Code 20.40.490
REVIEWER: Laurie Tyler, Development Services Department

36. LANDSCAPE MAINTENANCE ASSURANCE DEVICE

The applicant shall file with the Development Services Department a landscape maintenance assurance device in the form of an assignment of savings or letter of credit for 20% of the cost of labor and materials for all landscaping on the site.

AUTHORITY: Land Use Code 20.40.490
REVIEWER: Laurie Tyler, Development Services Department

37. ADDRESSING OF LOTS, BUILDINGS, AND STOREFRONTS

The applicant shall contact Jami Carter, Information Technology Department, phone 425-452-4310 regarding the addressing of lots, buildings, and storefronts. Addressing shall be approved by the Fire and Transportation Department.

AUTHORITY: Uniform Fire Code 505
REVIEWER: Jami Carter, Information Technology Department
Travis Ripley, Fire Department
Molly Johnson, Transportation Department

38. LOADING SIGNAGE

A sign indicating "No Parking, Loading Only", shall be installed adjacent to the loading bay along 122nd Avenue NE. The design and installation details shall be approved by Land Use and Transportation, prior to installation.

AUTHORITY: Land Use Code 20.20.590.K.4.b
REVIEWER: Laurie Tyler, Development Services Department
Molly Johnson, Transportation Department

39. PUBLIC ACCESS SIGNAGE FOR PEDESTRIAN CONNECTIONS

"Public Access" signage as depicted in the Bellevue Wayfinding Manual shall be prominently located to clearly convey the message that the public is welcome in all plazas and pedestrian connections. Prior to Temporary Certificate of Occupancy, the specific locations and sign designs shall be determined and installed.

AUTHORITY: Land Use Code 20.25A.060, Land Use Code 20.30F.145
REVIEWER: Laurie Tyler, Development Services Department

E. PRIOR TO ISSUANCE OF ANY SIGN PERMITS (SIGN)

LAND USE DIVISION CONDITION (prior to issuance of a SIGN permit)

40. SIGN MASTER PLAN

A comprehensive sign package shall be submitted to the Development Services Department for review and approval through a Pre-Development Services (DC) application. Contents shall include but is not limited to:

- a. Location
- b. Lighting
- c. Color Palate
- d. Materials
- e. Design concept

Signage shall be the minimum necessary to convey information and shall be architecturally compatible and integrated with the building. Maximum letter and sign size shall be determined based on proposed location and other design criteria. Sign Code limitations are not a guaranteed maximum and cannot be exceeded. Final design review of individual signs and compliance with the approved sign package will occur through review of each sign permit application.

No freestanding signs, cabinet signs or signs above the rooftop are permitted except as permitted in the Sign Code Bellevue City Code 22B.10.

AUTHORITY: Bellevue City Code 22B.10 and Land Use Code 20.25D.150.F
REVIEWER: Laurie Tyler, Development Services Department

F. PRIOR TO ISSUANCE OF TENANT IMPROVEMENT (TI) PERMITS

LAND USE DIVISION CONDITION (prior to issuance of TI permits)

41. FUTURE TENANT/STOREFRONT IMPROVEMENTS

Future retail/commercial tenant improvements shall reflect the design intent shown on the attached preliminary design. Vision glass is required to provide pedestrian access to interior spaces. Fire Control Room doors shall look like retail/office storefronts. Each tenant improvement or revision to this proposal shall be approved by the City.

AUTHORITY: Land Use Code 20.30.F
REVIEWER: Laurie Tyler, Development Services Department

ADMINISTRATIVE DESIGN REVIEW



Wright Runstad **BLOCK 14 - GIX**

THE SPRING DISTRICT

100908.11

OCTOBER 29, 2015

nbbj
223 YALE AVENUE NORTH
SEATTLE, WASHINGTON 98109
PHONE 206 223 5555
FAX 206 223 5000

ZONING CODE INFORMATION

APPLICABLE CODE: CITY OF BELLEVUE TITLE 20 LAND USE CODE
 ZONING / LAND USE CLASSIFICATION: BR-OR-1
 SPECIAL REVIEW DISTRICT / OVERLAY DISTRICT: BEL-RED DISTRICT

AREA SUMMARY

FLOOR AREA, GROSS: THE AREA INCLUDED WITHIN THE INSIDE FINISHED WALL SURFACE OF THE SURROUNDING EXTERIOR WALLS OF A BUILDING EXCLUDING INTERIOR OPENINGS IN FLOOR PLATES (E.G. VENT SHAFTS, STAIR WELLS, AND INTERIOR ATRIUMS), OUTDOOR COURTS AND EXTERIOR BALCONIES. (ORD 5050, 1-20-88, §13; ORD 2845 2-2-81, §20)

GROSS FLOOR AREA

LEVEL	NAME	AREA
LEVEL B	PARKING GARAGE	42,099
LEVEL B	STORAGE	180
LEVEL B	LOBBY	167
LEVEL B	MECH / ELEC / PLUMB ROOMS	140
LEVEL B SUBTOTAL		42,586
LEVEL A	PARKING GARAGE	40,276
LEVEL A	STORAGE	186
LEVEL A	LOBBY	164
LEVEL A	MECH / ELEC / PLUMB ROOMS	860
LEVEL A	BIKE PARKING	457
LEVEL A SUBTOTAL		41,943
LEVEL 1	UNIVERSITY / B-OCCUPANCY SPACE	24,977
LEVEL 1	TRASH, RECYCLE, LOADING	507
LEVEL 1	RESTROOMS	711
LEVEL 1	LOBBY	1067
LEVEL 1	COMMON HALLWAYS	185
LEVEL 1	MECH, ELEC, PLUMB ROOMS	740
LEVEL 1	RETAIL	3473
LEVEL 1	PARKING GARAGE	2002
LEVEL 1 SUBTOTAL		33,662
LEVEL 2	UNIVERSITY / B-OCCUPANCY SPACE	30,286
LEVEL 2	RESTROOMS	710
LEVEL 2	COMMON HALLWAYS	718
LEVEL 2	MECH, ELEC, PLUMB ROOMS	465
LEVEL 2 SUBTOTAL		32,179
LEVEL 3	UNIVERSITY / B-OCCUPANCY SPACE	24,050
LEVEL 3	RESTROOMS	711
LEVEL 3	COMMON HALLWAYS	834
LEVEL 3	MECH, ELEC, PLUMB ROOMS	526
LEVEL 3	STORAGE	352
LEVEL 3 SUBTOTAL		26,473
TOTAL GROSS FLOOR AREA		176,843

GROSS SQUARE FEET: TOTAL NUMBER OF SQUARE FEET WITHIN THE INSIDE FINISHED WALL SURFACE OF THE OUTER BUILDING WALLS OF A STRUCTURE, EXCLUDING VENT SHAFTS, OUTDOOR COURTS AND PARKING. (ORD 3747, 1-20-87, § 19)

GROSS SQUARE FEET

LEVEL	NAME	AREA
LEVEL B	STORAGE	180
LEVEL B	LOBBY	167
LEVEL B	MECH / ELEC / PLUMB ROOMS	140
LEVEL B SUBTOTAL		487
LEVEL A	STORAGE	186
LEVEL A	LOBBY	164
LEVEL A	MECH / ELEC / PLUMB ROOMS	860
LEVEL A	STAIRWELLS & ELEV. SHAFTS	454
LEVEL A SUBTOTAL		1,664
LEVEL 1	UNIVERSITY / B-OCCUPANCY SPACE	24,977
LEVEL 1	TRASH, RECYCLE, LOADING	507
LEVEL 1	RESTROOMS	711
LEVEL 1	LOBBY	1067
LEVEL 1	COMMON HALLWAYS	185
LEVEL 1	MECH, ELEC, PLUMB ROOMS	740
LEVEL 1	RETAIL	3473
LEVEL 1	STAIRWELLS & ELEV. SHAFTS	487
LEVEL 1 SUBTOTAL		32,147
LEVEL 2	UNIVERSITY / B-OCCUPANCY SPACE	30,286
LEVEL 2	RESTROOMS	710
LEVEL 2	COMMON HALLWAYS	718
LEVEL 2	MECH, ELEC, PLUMB ROOMS	465
LEVEL 2	STAIRWELLS & ELEV. SHAFTS	403
LEVEL 2 SUBTOTAL		32,582
LEVEL 3	UNIVERSITY / B-OCCUPANCY SPACE	24,050
LEVEL 3	RESTROOMS	711
LEVEL 3	COMMON HALLWAYS	834
LEVEL 3	MECH, ELEC, PLUMB ROOMS	526
LEVEL 3	STORAGE	352
LEVEL 3	STAIRWELLS & ELEV. SHAFTS	510
LEVEL 3 SUBTOTAL		26,983
TOTAL GROSS SQUARE FEET		93,863

NET SQUARE FEET: THE TOTAL NUMBER OF SQUARE FEET WITHIN THE INSIDE FINISHED WALL SURFACE OF THE OUTER BUILDING WALLS OF A STRUCTURE EXCLUDING MAJOR VERTICAL PENETRATIONS OF THE FLOOR (ELEVATOR AND OTHER MECHANICAL SHAFTS, STAIRS WELLS), MECHANICAL EQUIPMENT, PARKING AREA, COMMON RESTROOMS, COMMON LOBBIES, AND COMMON HALLWAYS. STORAGE AREA IS INCLUDED IN THE NET SQUARE FEET CALCULATION UNLESS THE PROPERTY OWNER DEMONSTRATES THAT IS CANNOT BE CONVERTED TO HABITABLE SPACE. (ORD 3747, 1-20-87, §20)

NET SQUARE FEET

LEVEL	NAME	AREA
LEVEL B	STORAGE	180
LEVEL B SUBTOTAL		180
LEVEL A	STORAGE	186
LEVEL A SUBTOTAL		186
LEVEL 1	UNIVERSITY / B-OCCUPANCY SPACE	24,977
LEVEL 1	TRASH, RECYCLE, LOADING	507
LEVEL 1	RETAIL	3473
LEVEL 1 SUBTOTAL		28,957
LEVEL 2	UNIVERSITY / B-OCCUPANCY SPACE	30,286
LEVEL 2 SUBTOTAL		30,286
LEVEL 3	UNIVERSITY / B-OCCUPANCY SPACE	24,050
LEVEL 3	STORAGE	352
LEVEL 3 SUBTOTAL		24,402
TOTAL NET SQUARE FEET		84,011

CHARGEABLE FLOOR AREA RATIO (F.A.R.): EQUIVALENT TO GROSS FLOOR AREA EXCLUDING PARKING, RETAIL, AND BELOW GRADE CONSTRUCTION

CHARGEABLE FAR

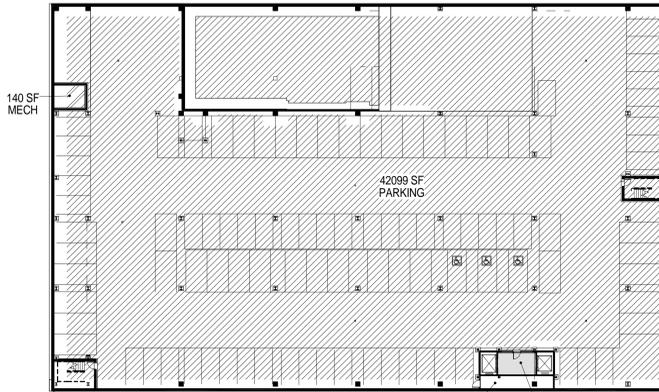
LEVEL	NAME	AREA
LEVEL 1	UNIVERSITY / B-OCCUPANCY SPACE	24,977
LEVEL 1	TRASH, RECYCLE, LOADING	507
LEVEL 1	RESTROOMS	711
LEVEL 1	LOBBY	1067
LEVEL 1	COMMON HALLWAYS	185
LEVEL 1	MECH, ELEC, PLUMB ROOMS	740
LEVEL 1 SUBTOTAL		28,187
LEVEL 2	UNIVERSITY / B-OCCUPANCY SPACE	30,286
LEVEL 2	RESTROOMS	710
LEVEL 2	COMMON HALLWAYS	718
LEVEL 2	MECH, ELEC, PLUMB ROOMS	465
LEVEL 2 SUBTOTAL		32,179
LEVEL 3	UNIVERSITY / B-OCCUPANCY SPACE	24,050
LEVEL 3	RESTROOMS	711
LEVEL 3	COMMON HALLWAYS	834
LEVEL 3	MECH, ELEC, PLUMB ROOMS	526
LEVEL 3	STORAGE	352
LEVEL 3 SUBTOTAL		26,473
TOTAL CHARGEABLE FAR		86,839

PROPOSED FAR

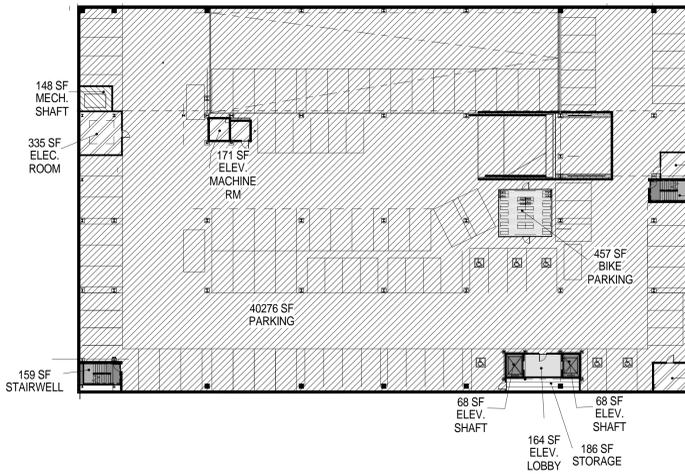
TOTAL AGGREGATE PARCEL SF - 59,527
 GFA OF PROPOSED DEVELOPMENT - 176,843
 PROPOSED FAR - 2.97

BICYCLE PARKING COUNT

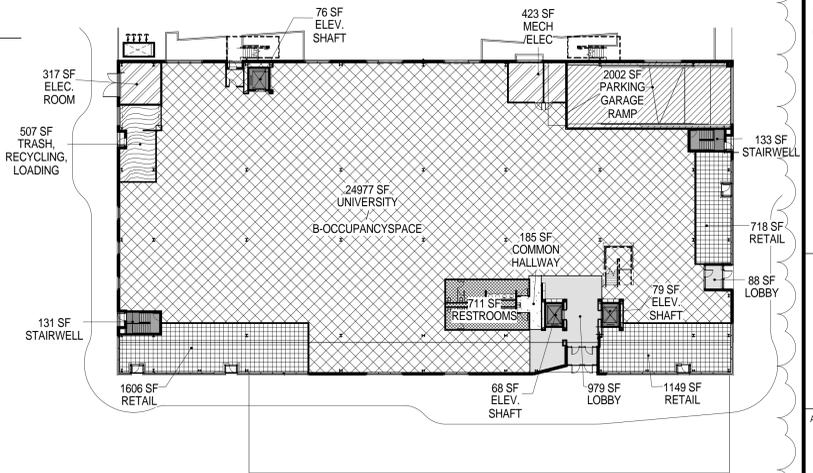
Level	Name	Area	Required Bike Parking
RETAIL			
LEVEL 1	RETAIL	3473 SF	3
UNIVERSITY / B-OCCUPANCYSPACE			
LEVEL 1	UNIVERSITY / B-OCCUPANCYSPACE	24977 SF	3
LEVEL 2	UNIVERSITY / B-OCCUPANCYSPACE	30285 SF	4
LEVEL 3	UNIVERSITY / B-OCCUPANCYSPACE	24050 SF	3
ABOVE GRADE			
		82784 SF	13



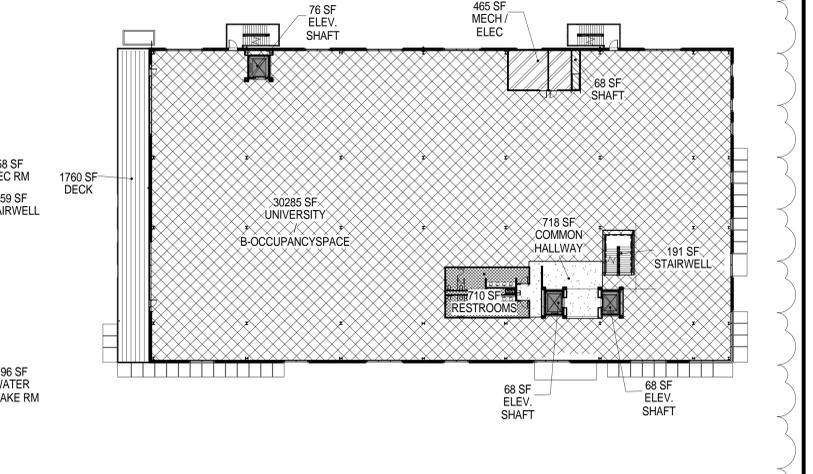
1 GARAGE LEVEL B - AREA PLAN - ADR
1" = 30'-0"



2 GARAGE LEVEL A - AREA PLAN - ADR
1" = 30'-0"



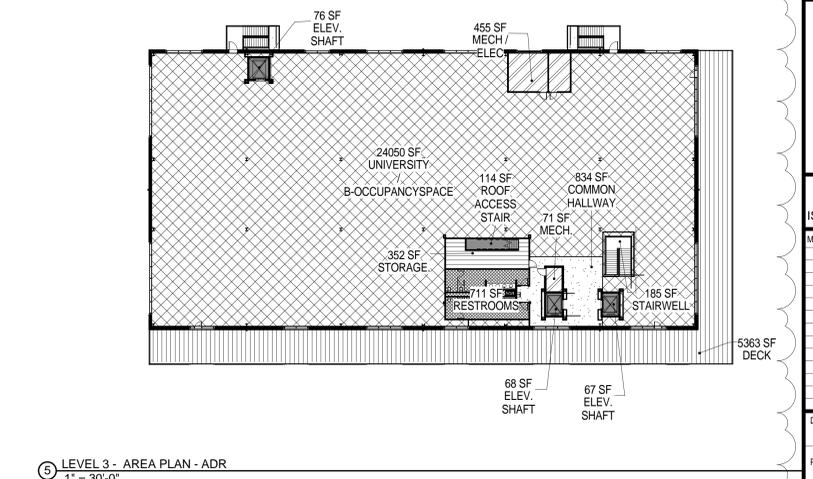
3 LEVEL 1 - AREA PLAN - ADR
1" = 30'-0"



4 LEVEL 2 - AREA PLAN - ADR
1" = 30'-0"

HATCH PATTERN KEY FOR AREA PLANS

- [Hatch Pattern] UNIVERSITY - B OCCUPANCY SPACE
- [Hatch Pattern] STORAGE
- [Hatch Pattern] TRASH, RECYCLING, LOADING
- [Hatch Pattern] RESTROOMS
- [Hatch Pattern] LOBBY
- [Hatch Pattern] COMMON HALLWAYS
- [Hatch Pattern] MECH, ELEC, PLUMB ROOMS
- [Hatch Pattern] RETAIL
- [Hatch Pattern] PARKING GARAGE
- [Hatch Pattern] BIKE PARKING
- [Hatch Pattern] OUTDOOR DECK
- [Hatch Pattern] VENT SHAFTS
- [Hatch Pattern] STAIRWELLS & ELEVATOR SHAFTS



5 LEVEL 3 - AREA PLAN - ADR
1" = 30'-0"

AGENCY APPROVAL:

BLOCK 14 - G1X

ADMINISTRATIVE DESIGN REVIEW

ISSUE

MARK	DATE	DESCRIPTION
2	03-11-2016	ADR REV 2
1	02/01/2016	ADR REV 1

DRAWN BY: **NBBJ** PROJ. ARCH: **NBBJ**

PROJ. NO.: **100908.11**

DATE: **03/11/2016**

LAND USE DIAGRAMS (AREA PLANS)

AGENCY APPROVAL:

BLOCK 14 - G1X

ADMINISTRATIVE
DESIGN
REVIEW

ISSUE

MARK	DATE	DESCRIPTION

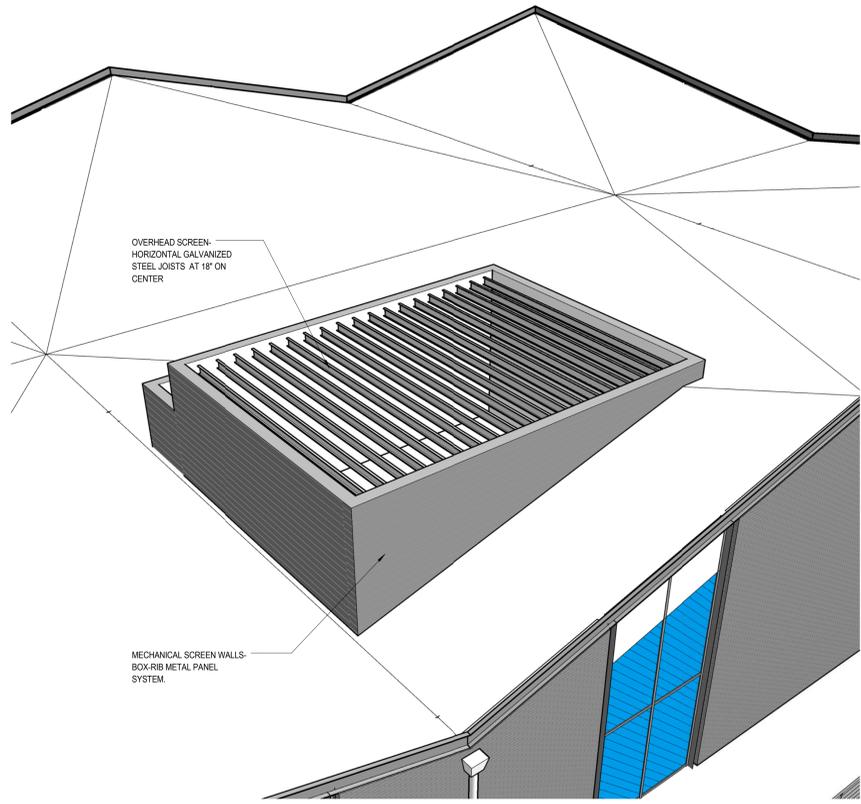
DRAWN BY: **NBBJ** PROJ. ARCH: **NBBJ**

PROJ. NO.: **100908.11**

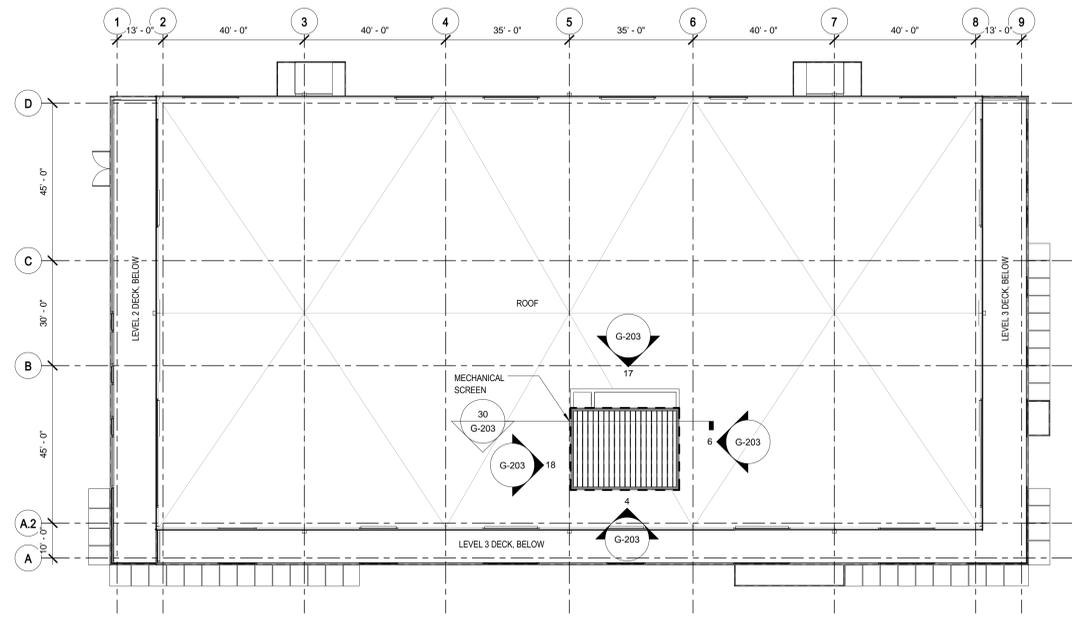
DATE: **10/29/2015**

DWG.
**LAND USE
DIAGRAMS (MECH
SCREENS)**

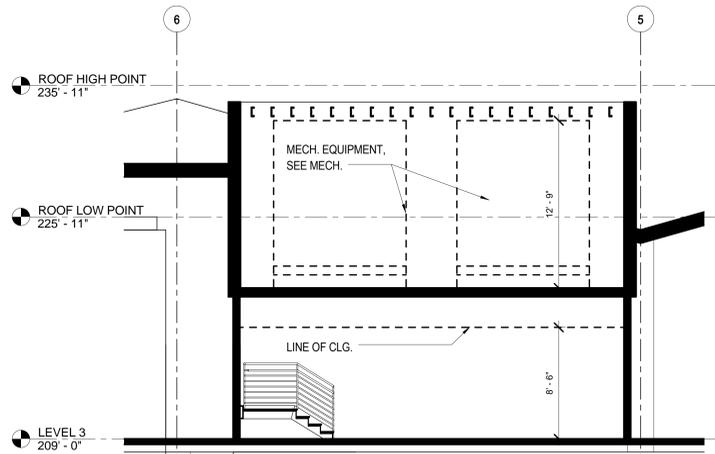
DWG.
G-203



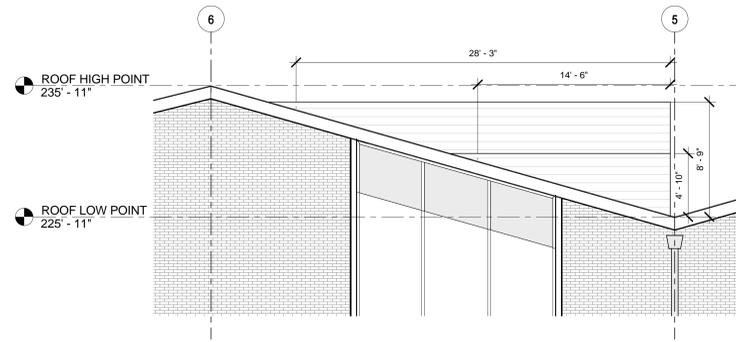
27 ROOFTOP MECH. SCREEN PERSPECTIVE VIEW



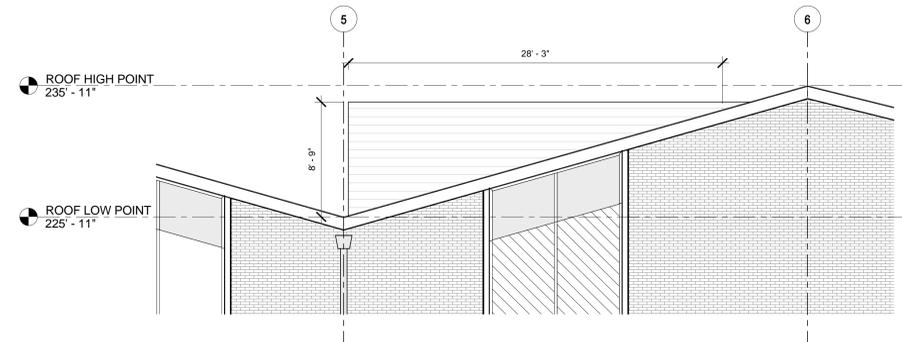
19 MECHANICAL SCREEN - PLAN DIAGRAM
1" = 20'-0"



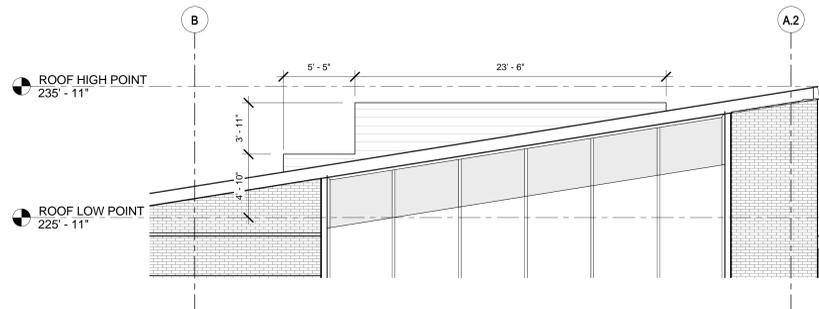
30 SECTION @ MECH. WELL
3/16" = 1'-0"



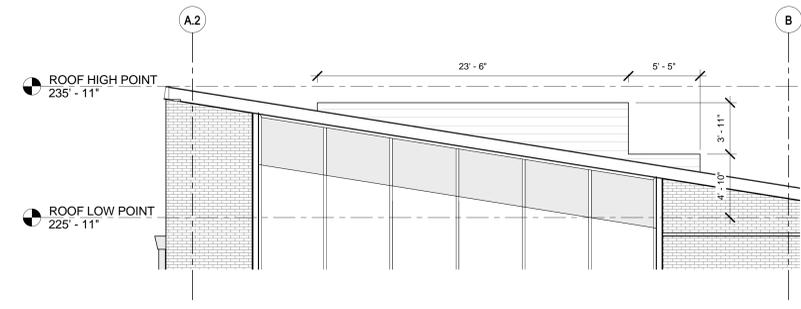
17 BUILDING ELEVATION - NORTH - MECH SCREEN DIAGRAM
3/16" = 1'-0"



4 BUILDING ELEVATION - SOUTH - MECH SCREEN DIAGRAM
3/16" = 1'-0"



18 BUILDING ELEVATION - WEST - MECH SCREEN DIAGRAM
3/16" = 1'-0"



6 BUILDING ELEVATION - EAST - MECH SCREEN DIAGRAM
3/16" = 1'-0"



AGENCY APPROVAL:

BLOCK 14 - G1X

ADMINISTRATIVE
DESIGN REVIEW

ISSUE

MARK	DATE	DESCRIPTION

1 02/01/2016 ADR REV 1

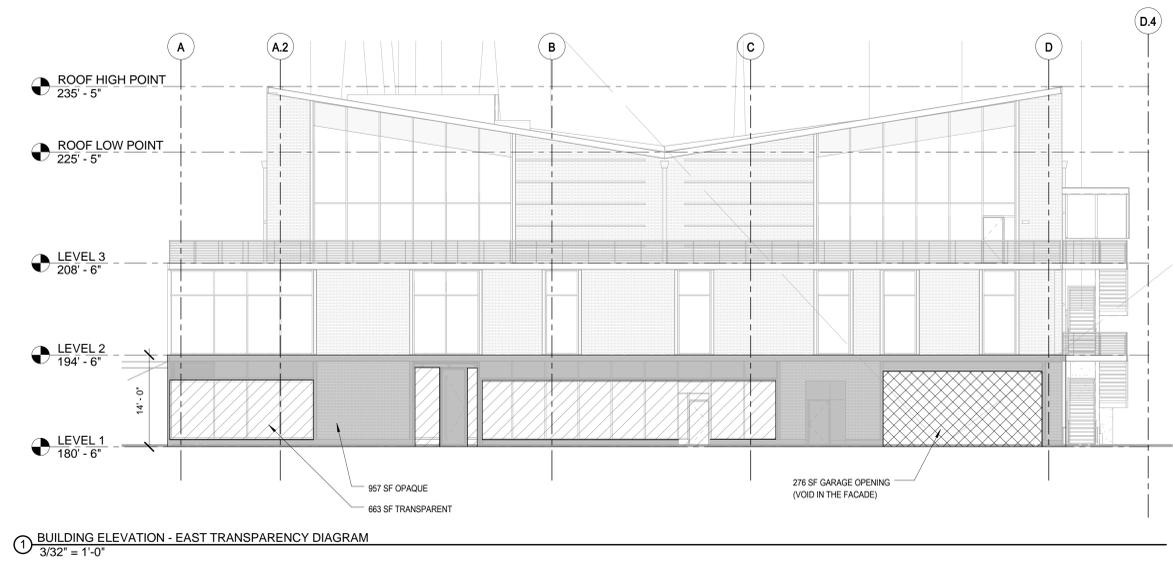
DRAWN BY: NBBJ PROJ. ARCH: NBBJ

PROJ. NO.: 100908.11

DATE: 02/01/2016

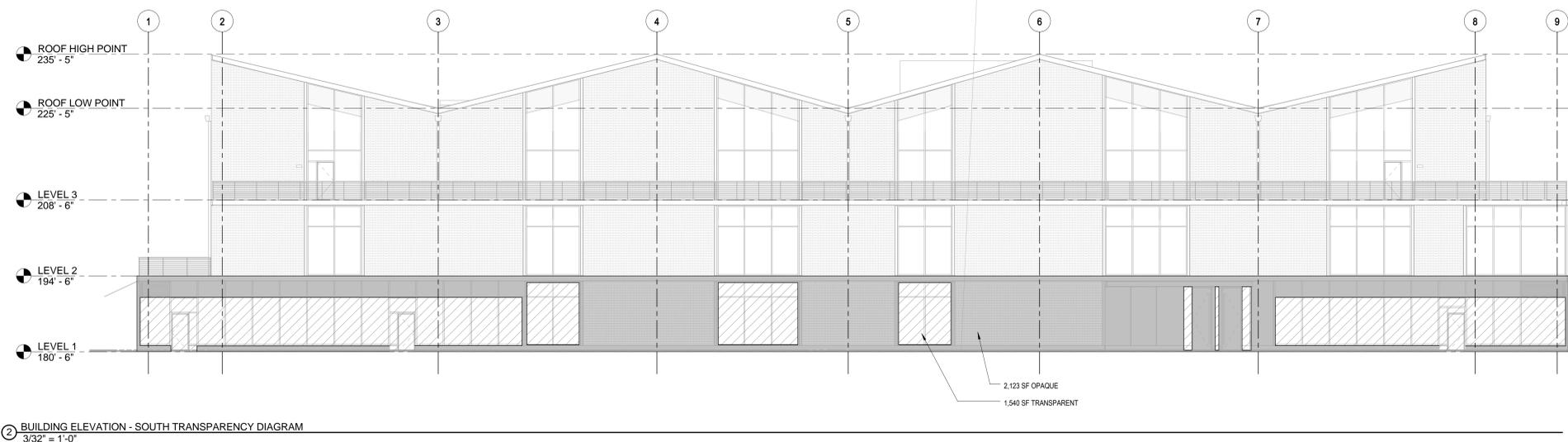
DWG.
LAND USE
DIAGRAMS (FACADE
TRANSPARENCY)

DWG.
G-204.



EAST FACADE TRANSPARENCY

FACADE TRANSPARENCY % = SUM OF TRANSPARENT AREAS / SUM OF ALL AREAS
SUM OF TRANSPARENT AREAS = 663 SF
SUM OF ALL AREAS = 1,620 SF
FACADE TRANSPARENCY % = 663 / 1,620 = 41%

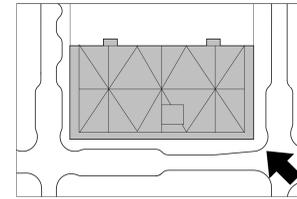


SOUTH FACADE TRANSPARENCY

FACADE TRANSPARENCY % = SUM OF TRANSPARENT AREAS / SUM OF ALL AREAS
SUM OF TRANSPARENT AREAS = 1,540 SF
SUM OF ALL AREAS = 3,663 SF
FACADE TRANSPARENCY % = 1,540 / 3,663 = 42%



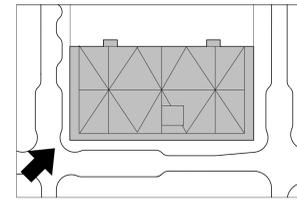
CORNER OF NE DISTRICT WAY & 123RD - FACING NORTHWEST



KEYPLAN



AERIAL VIEW - NE DISTRICT WAY & 122ND - LOOKING NORTHEAST



KEYPLAN

WRIGHT
RUNSTAD
& COMPANY

nbbj

223 YALE AVENUE NORTH
SEATTLE, WASHINGTON 98109
PHONE 206 223 5555
FAX 206 223 5000

AGENCY APPROVAL:

BLOCK 14 - G1X

ADMINISTRATIVE
DESIGN
REVIEW

ISSUE

MARK	DATE	DESCRIPTION

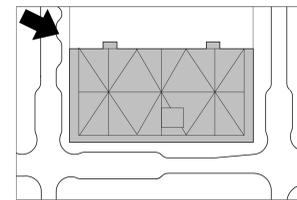
DRAWN BY: **NBBJ** PROJ. ARCH: **NBBJ**
 PROJ. NO.: **100908.11**
 DATE: **10/29/2015**

DWG. **RENDERINGS**

DWG. **G-500**



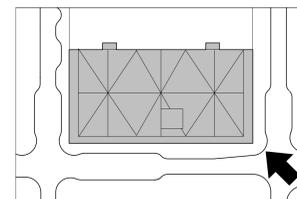
NORTHEAST CORNER VIEWED FROM 122ND AVE



KEYPLAN



AERIAL VIEW - CORNER OF NE DISTRICT WAY & 123RD - LOOKING NW



KEYPLAN

AGENCY APPROVAL:

BLOCK 14 - GIX

ADMINISTRATIVE
DESIGN
REVIEW

ISSUE

MARK	DATE	DESCRIPTION

DRAWN BY: **NBBJ** PROJ. ARCH: **NBBJ**

PROJ. NO.: **100908.11**

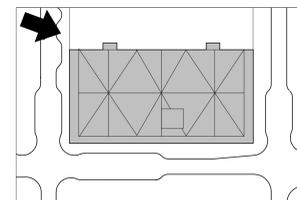
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DWG. **RENDERINGS**

DWG. **G-501**



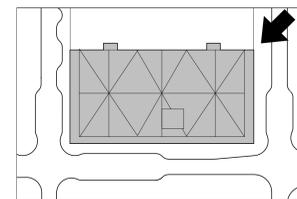
AERIAL VIEW - NORTHWEST CORNER VIEWED FROM 122ND AVE



KEYPLAN



AERIAL VIEW - NORTHEAST CORNER VIEWED FROM 123RD AVE



KEYPLAN

BLOCK 14 - GIX

ADMINISTRATIVE
DESIGN
REVIEW

ISSUE

MARK	DATE	DESCRIPTION

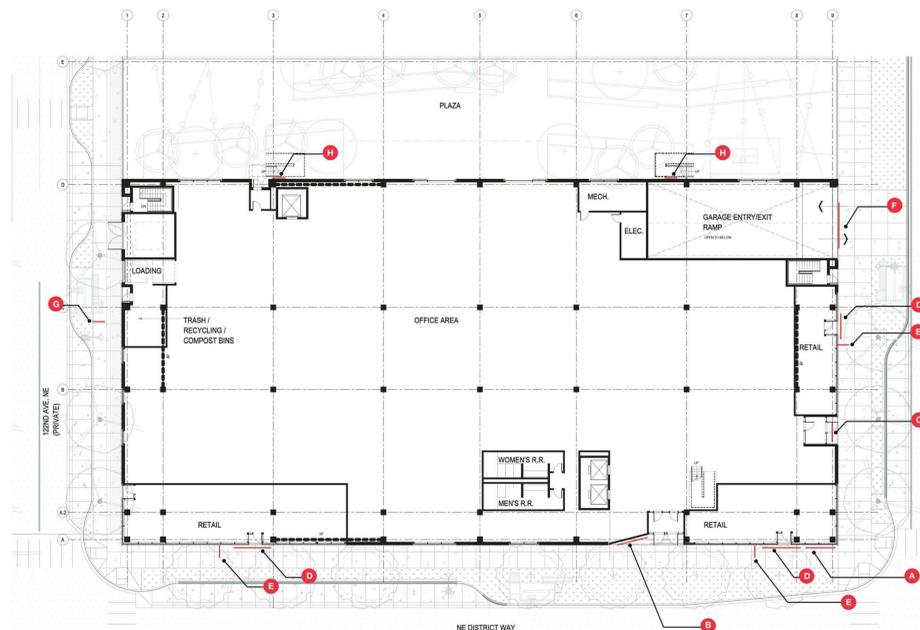
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PROJ. NO.: 100908.11

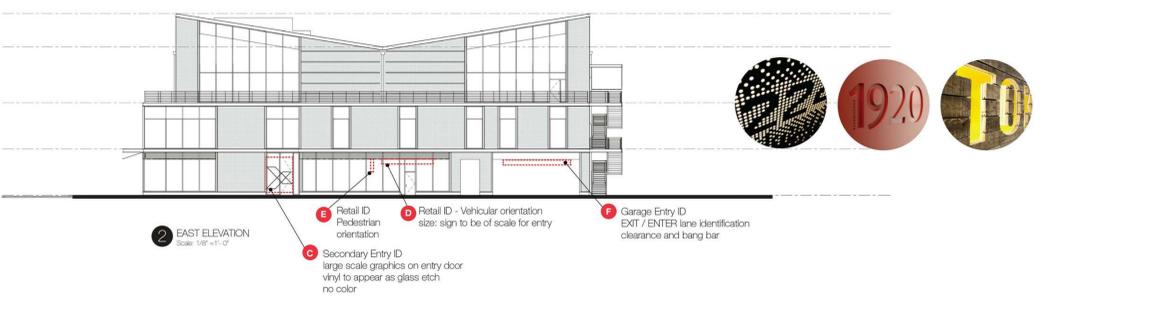
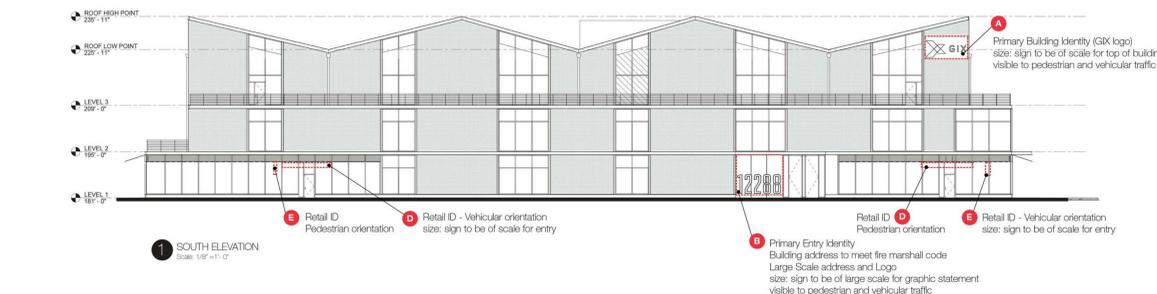
DATE: 10/29/2015

DWG. RENDERINGS

MARK	DATE	DESCRIPTION

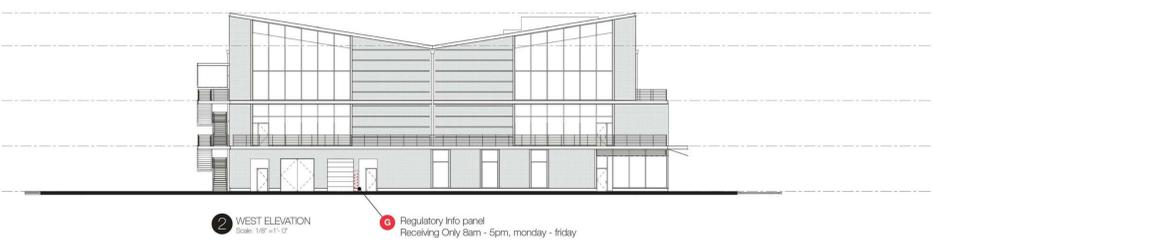
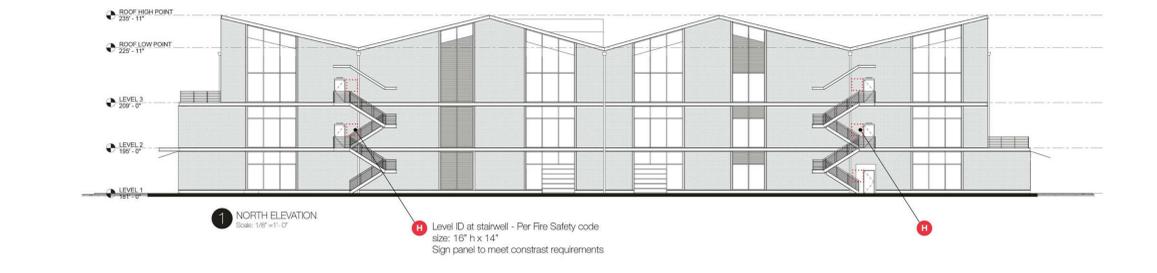


- EXTERIOR SIGN TYPES**
- A** Primary Building Identity (GIX logo) size: sign to be of scale for top of building visible to pedestrian and vehicular traffic
 - B** Primary Entry ID / Street address for Fire Marshall and GIX logo size: sign to be of large scale for entry visible to vehicle and pedestrian traffic
 - C** Secondary Entry vinyl logo oversize on exterior door
 - D** Retail ID - Vehicular orientation size: sign to be of scale for entry visible to vehicular traffic
 - E** Retail ID - Pedestrian orientation size: sign to be of scale for entry visible to pedestrian approach
 - F** Garage Entry ID EXIT / ENTER lane identification clearance and bang bar
 - G** Regulatory Information Panel No parking, loading zone, drop off only ... text as required
 - H** Fire Safety Required Sign Panel Level ID in high contrast



OLIVE DESIGN STUDIO | GIX office building | SPRING DISTRICT | 02
prepared for WRIGHT RUNSTAD & CO | ADR | 10 29 2015

OLIVE DESIGN STUDIO | GIX office building | SPRING DISTRICT | 01
prepared for WRIGHT RUNSTAD & CO | ADR | 10 29 2015



OLIVE DESIGN STUDIO | GIX office building | SPRING DISTRICT | 03
prepared for WRIGHT RUNSTAD & CO | ADR | 10 29 2015

SIGN TYPE A
Primary Building Identity
QTY - 1

MATERIALS: Fabricated Metal or painted finish logo and letterforms
SIZE: Logo and letterforms scaled to SE corner area as indicated with dashed box for vehicular visibility
FONT: To be determined by tenant branding standards
LIGHTING: Internal illumination or halo lighting in color TBD, UL approved

SIGN TYPE B
Primary Entry Identity
QTY - 1

MATERIALS: Fabricated Metal or painted finish logo and letterforms, Option to be reversed out of architectural metal panels by perforation or other method TBD
SIZE: Building Address to be provided to meet Fire Marshall code near entry door
FONT: Letter forms or numerals based on scale of entry and location, Logo to be large scale
LIGHTING: Internal illumination or halo lighting in color TBD, UL approved

SIGN TYPE C
Secondary Entry Identity
QTY - 1

MATERIALS: Large scale graphics at glass doors, vinyl
SIZE: Logo and letter forms based on scale of entry and location
LIGHTING: Internal illumination or halo lighting in color TBD, UL approved

SIGN TYPE D
Retail Identity - Vehicular Orientation, Hanging panel above entry
QTY - 3

MATERIALS: Logo and text
SIZE: Scaled for wall fbd
FONT: To be determined by code requirements

SIGN TYPE E
Retail Identity - Pedestrian Orientation - Panel is perpendicular to retail entry, near entry
QTY - 3

MATERIALS: Logo and text or graphic painted directly on wall surface
SIZE: Scaled for wall fbd
FONT: To be determined by code requirements
LIGHTING: possible wall wash of light, soft glow

SIGN TYPE F
Garage Entry
QTY - 1

MATERIALS: Fabricated Metal with painted finish
SIZE: To be determined by code requirements
LIGHTING: Possible illumination

SIGN TYPE G
Regulatory Information Panel
QTY - 1

MATERIALS: Fabricated Metal with painted finish
SIZE: To be determined by code requirements
LIGHTING: N/A

SIGN TYPE H
Fire Safety information at Stairwell Landing (exterior)
QTY - 5

MATERIALS: Fabricated Metal with painted finish
SIZE: To be determined by code requirements
LIGHTING: N/A

OLIVE DESIGN STUDIO | GIX office building | SPRING DISTRICT | 04
prepared for WRIGHT RUNSTAD & CO | ADR | 10 29 2015

BLOCK 14 - GIX

**ADMINISTRATIVE
DESIGN
REVIEW**



ISSUE

MARK	DATE	DESCRIPTION
1	01-11-16	Design Review Revisions
2	04-07-16	Design Review Revisions

DRAWN BY: **D. Torrey** PROJ. ENG.: **J. Jones**

PROJ. NO.: **1500-001-010**

DATE: **2016/04/07**

DWG.

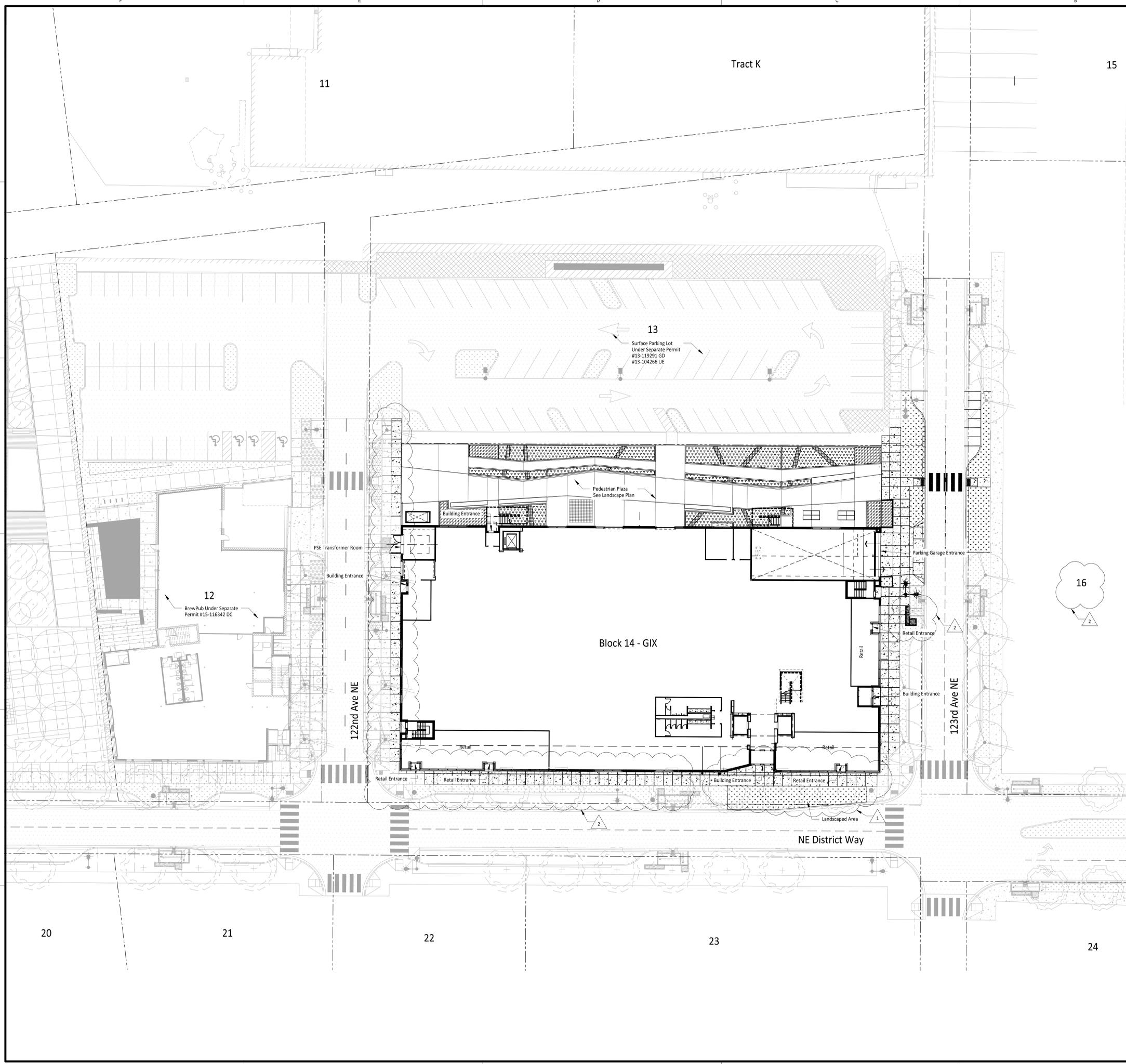
GIX Site Plan

DWG.

C1-002

General Notes

1. Parcel 12 BrewPub shown as Existing Condition, permitted under separate permits, to be constructed, and opened prior to completion of Proposed GIX Building.
2. Parcel 13 Surface Parking Lot and 122nd Ave NE shown as Existing Condition, permitted under separate permits, to be constructed after Mass Excavation of Proposed GIX Building, and opened prior to completion of BrewPub/GIX Buildings.
3. Site Conditions of BrewPub, Surface Parking Lot, and 122nd prior to construction is Concrete Slab from demolished Warehouse Building.





ISSUE

MARK	DATE	DESCRIPTION
1	01-11-16	Design Review Revisions
2	04-07-16	Design Review Revisions

DRAWN BY: **D. Torrey** PROJ. ENG.: **J. Jones**

PROJ. NO.: **1500-001-010**

DATE: **2016/04/07**

DWG. **2**

Binding Site Plan

DWG.

C1-101

286 / 035

OWNERS CERTIFICATE AND DEDICATION

KNOW ALL PEOPLE BY THESE PRESENTS THAT WE, THE UNDERSIGNED OWNERS OF INTEREST IN PARCELS 8 AND 17 HEREIN DESCRIBED, DECLARE THIS DOCUMENT AS PROVIDED BY CHAPTER 58.17 RCW, TO BE THE GRAPHIC REPRESENTATION OF THE AMENDED BINDING SITE PLAN MADE HEREBY AND DEDICATE TO THE PUBLIC FOREVER ALL STREETS AND AVENUES NOT SHOWN AS PRIVATE. WE DEDICATE THE USE FOR ALL PUBLIC PURPOSES NOT INCONSISTENT WITH PUBLIC HIGHWAY PURPOSES AND ALSO THE RIGHT TO MAKE ALL NECESSARY SLOPES FOR CUTS AND FILLS UPON THE PARCELS SHOWN IN THE REASONABLE GRADING OF SAID STREETS AND AVENUES. WE FURTHER DEDICATE TO THE USE OF THE PUBLIC ALL THE EASEMENTS AND TRACTS SHOWN ON THIS PLAN FOR ALL PUBLIC PURPOSES AS INDICATED, INCLUDING BUT NOT LIMITED TO PARKS, OPEN SPACE, UTILITIES, AND DRAINAGE UNLESS SUCH EASEMENTS OR TRACTS ARE SPECIFICALLY IDENTIFIED ON THIS PLAN AS BEING DEDICATED OR CONVEYED TO A PERSON OR ENTITY OTHER THAN THE PUBLIC, IN WHICH CASE WE DEDICATE SUCH STREETS, EASEMENT, OR TRACTS TO THE PERSON OR ENTITY IDENTIFIED AND FOR THE PURPOSE STATED.

FURTHER, WE WAIVE FOR OURSELVES, OUR HEIRS AND ASSIGNS, AND ANY PERSON OR ENTITY DERIVING TITLE FROM THE UNDERSIGNED ANY AND ALL CLAIMS FOR DAMAGES AGAINST THE CITY OF BELLEVUE AND ITS SUCCESSORS AND ASSIGNS WHICH MAY BE OCCASIONED TO THE ADJACENT LANDS OF THIS BINDING SITE PLAN BY THE ESTABLISHMENT, CONSTRUCTION, OR MAINTENANCE OF ROADS AND/OR DRAINAGE SYSTEMS WITHIN THIS BINDING SITE PLAN.

THIS BINDING SITE PLAN, DEDICATION, AND WAIVER OF CLAIMS ARE MADE WITH THE FREE CONSENT AND IN ACCORDANCE WITH OUR DESIRES.

IN WITNESS WHEREOF WE SET OUR HANDS AND SEALS:
WR-SRI 120TH LLC, A DELAWARE LIMITED LIABILITY COMPANY

BY: [Signature]
ITS: VICE PRESIDENT

ACKNOWLEDGEMENT
STATE OF California
COUNTY OF San Francisco

I CERTIFY THAT I KNOW OR HAVE SATISFACTORY EVIDENCE THAT James A. Pierce IS THE PERSON WHO APPEARED BEFORE ME, AND SAID PERSON ACKNOWLEDGED THAT SAID PERSON SIGNED THIS INSTRUMENT, ON OATH STATED THAT SAID PERSON WAS AUTHORIZED TO EXECUTE THE INSTRUMENT AND ACKNOWLEDGED IT AS THE Vice President OF WR-SRI 120TH LLC, A DELAWARE LIMITED LIABILITY COMPANY, TO BE THEIR FREE AND VOLUNTARY ACT OF SUCH PARTY FOR THE USES AND PURPOSES MENTIONED IN THE INSTRUMENT.

DATED THIS 17th OF February, 2016

[Signature]
(SIGNATURE OF NOTARY)
Shanie Cook
(LEGIBLY PRINT OR STAMP NAME OF NOTARY)

NOTARY PUBLIC IN AND FOR THE STATE OF California, RESIDING AT 235 Montgomery St., San Francisco, CA
MY APPOINTMENT EXPIRES January 3, 2017

SURVEY NOTES:
1. HORIZONTAL DATUM: WASHINGTON STATE COORDINATE SYSTEM NAD 83/91, NORTH ZONE.
2. BASIS OF BEARINGS: THE NORTH-SOUTH CENTER LINE OF SECTION 28, T. 25 N., R. 5 E., W.M. (BETWEEN CITY OF BELLEVUE CONTROL MONUMENTS 0058 AND 0080) BEARS S 00°42'18" W.
3. DATA FOR EXTERIOR BOUNDARY, NE 12TH ST. AND NE BEL RED RD. SHOWN HEREON IS ACCORDING TO CITY OF BELLEVUE SHORT PLAT NUMBER LF-06-135856, RECORDING NUMBER 20070319900012.
4. METHOD: TOPCON GRS-3 FOR GPS TIES TO MONUMENTS AND SOME CORNERS, LEICA 1203 TOTAL STATION FOR CONVENTIONAL SURVEY.
5. THIS SURVEY MEETS OR EXCEEDS THE CURRENT MINIMUM TRAVERSE STANDARDS CONTAINED IN W.A.C. 332-130-090.
6. INTERIOR CORNERS FOR PARCELS 8 (LOTS) AND TRACTS HAVE NOT BEEN SET FOR THIS BINDING SITE PLAN BUT MAY BE REQUIRED AS PART OF THE DEVELOPMENT PROCESS FOR EACH PARCEL AND TRACT.

COVENANTS, CONDITIONS AND RESTRICTIONS NOTES:
1. ALL PARCELS AND TRACTS IN THIS BINDING SITE PLAN ARE SUBJECT TO THE COVENANTS, CONDITIONS AND RESTRICTIONS RECORDED WITH KING COUNTY.
2. ALL DEVELOPMENT IS SUBJECT TO CONDITIONS PER THE MASTER DEVELOPMENT PLAN, RECORDING NO. 20121016001049

SHEET INDEX:
SHEET 1: SIGNATURE AND NOTE PAGE
SHEET 2: PARCEL LAYOUT AND AMENDED AREA
SHEET 3: AMENDED PARCEL DETAILS - AMENDED AREA

DESCRIPTION

(PER FIRST AMERICAN TITLE INSURANCE COMPANY TITLE COMMITMENT FOR SPRING DISTRICT, FILE NO. NCS-752053-WA1 DATED: FEBRUARY 02, 2016 AT 7:30 A.M.)

UNITS 8 AND 17 OF THE SPRING DISTRICT MASTER CONDOMINIUM, PER CONDOMINIUM DECLARATION THEREOF RECORDED UNDER KING COUNTY, RECORDING NO. 20131025000704, AND AMENDMENTS THERETO, SAID UNITS ARE LOCATED ON SURVEY MAP AND PLANS RECORDED IN VOLUME 278 OF CONDOMINIUMS AT PAGES 94 AND 95, IN KING COUNTY, WASHINGTON. EXCEPT THOSE PORTIONS CONVEYED TO THE CITY OF BELLEVUE BY DEED RECORDED UNDER RECORDING NO. 20151210000755.

REFERENCES:

1. CITY OF BELLEVUE SHORT PLAT NUMBER LF-06-135856, RECORDING NUMBER 20070319900012.
2. BELLEVUE DISTRIBUTION CENTER, A CONDOMINIUM, RECORDING NO. 20120516000184.
3. AMENDMENT NO. 1, BELLEVUE DISTRIBUTION CENTER, A CONDOMINIUM, RECORDING NO. 20131025000702.
4. SPRING DISTRICT MASTER CONDOMINIUM, RECORDING NO. 20131025000705.
5. SPRING DISTRICT MASTER CONDOMINIUM, AMENDMENT NO. 1, RECORDING NO. 20140625000484.
6. BINDING SITE PLAN NO. 12-1110450 LF RECORDING NO. 20121101000274.
7. FIRST AMENDED BINDING SITE PLAN NO. 14-128831 LJ, RECORDING NO. 20140423000863.

DEVELOPMENT NOTES:

ALL DEVELOPMENT AND USE OF THE LAND DESCRIBED HEREIN SHALL BE IN ACCORDANCE WITH THIS BINDING SITE PLAN, AS IT MAY BE AMENDED WITH THE APPROVAL OF THE CITY, TOWN, OR COUNTY HAVING JURISDICTION OVER THE DEVELOPMENT OF SUCH LAND, AND IN ACCORDANCE WITH SUCH OTHER GOVERNMENTAL PERMITS, APPROVALS, REGULATIONS, REQUIREMENTS, AND RESTRICTIONS THAT MAY BE IMPOSED UPON SUCH LAND AND THE DEVELOPMENT AND USE THEREOF. UPON COMPLETION, THE IMPROVEMENTS ON THE LAND SHALL BE INCLUDED IN ONE OR MORE CONDOMINIUMS OR OWNED BY AN ASSOCIATION OR OTHER LEGAL ENTITY IN WHICH THE OWNERS OF UNITS THEREIN OR THEIR OWNERS ASSOCIATIONS HAVE A MEMBERSHIP OR OTHER LEGAL OR BENEFICIAL INTEREST. THIS BINDING SITE PLAN SHALL BE BINDING UPON ALL NOW OR HEREAFTER HAVING ANY INTEREST IN THE LAND DESCRIBED HEREIN.

TRACTS A THROUGH N ARE RETAINED AND WHOLLY OWNED BY WR-SRI 120TH LLC OR WR-SRI 120TH NORTH LLC. UPON RECORPATION OF THIS AMENDED BINDING SITE PLAN, AND SHALL BE RESERVED FOR FUTURE USE OF PUBLIC RIGHT-OF-WAY AND PRIVATE ACCESS FOR VEHICULAR, ON STREET PARKING, PEDESTRIAN, PARKS, OPEN SPACE AND UTILITY ACCESS. PUBLIC RIGHT OF WAY AND PRIVATE ACCESS WILL BE ESTABLISHED AT THE TIME OF FUTURE DEVELOPMENT IMPROVEMENTS ON A PHASE-BY-PHASE BASIS THROUGH BINDING SITE PLAN AMENDMENTS OR BY THE RECORDING OF OTHER INSTRUMENTS. AMENDMENTS OF TRACTS FOR PUBLIC RIGHT-OF-WAY WILL REFLECT ACTUAL ROADWAY DESIGN AND CONSTRUCTION.

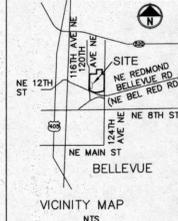
SECOND AMENDMENT TO BINDING SITE PLAN:

SURVEYORS DECLARATION:
IT IS HEREBY STIPULATED THAT THE BINDING SITE PLAN APPROVED UNDER CITY OF BELLEVUE NO. 12-1110450 LF AND RECORDED UNDER AUDITORS FILE NUMBER 20121101000274, RECORDS OF KING COUNTY, WASHINGTON SHALL BE AMENDED PURSUANT TO SECTION 20.458.240 FINAL SHORT PLAT REVISION OF THE CITY OF BELLEVUE LAND USE CODE AS FOLLOWS:

1. CHANGING THE PARCEL LINE LOCATION BETWEEN PARCELS 8 AND 17.
2. SHOW THE FEE TAKE AREA ALONG 120TH AVE NE, AFFECTING PARCELS 8 AND 17.

THIS SECOND AMENDMENT TO THE BINDING SITE PLAN REPRESENTS ONLY A GRAPHIC DEPICTION OF THE ORIGINAL BINDING SITE PLAN AND DOES NOT ALTER ANY ELEMENTS OTHER THAN THE LINES AND TEXT STIPULATED ABOVE. EXCEPT AS OTHERWISE PROVIDED HEREIN, THE TERMS AND CONDITIONS OF THE ORIGINAL PLAT SHALL REMAIN IN FULL FORCE AND EFFECT.

AMEND NO.	COB NO.	RECORDING NO.	PARCELS AFFECTED
1	14-128831 LJ	20140423000863	21 & 22



SE 1/4 OF NW 1/4; NE 1/4 & SE 1/4 OF SW 1/4
IN SEC. 28 T.25 N., R.5 E., W.M.

TRANSPORTATION DEPARTMENT
EXAMINED AND APPROVED WITH RESPECT TO STREETS, ALLEYS AND RIGHTS OF WAY FOR ROADS, PATHS AND SLOPES.
REAL PROPERTY & GIS UNIT: [Signature] DATE: 2/16/2016
DEVELOPMENT REVIEW: [Signature] DATE: 2/16/16

UTILITIES DEPARTMENT
EXAMINED AND APPROVED WITH RESPECT TO WATER, SEWER AND DRAINAGE SYSTEMS.
[Signature] DATE: 2/16/2016

DEVELOPMENT SERVICES DEPARTMENT
EXAMINED AND APPROVED:
[Signature] DATE: 2/16/16

KING COUNTY DEPT. OF ASSESSMENTS
EXAMINED AND APPROVED THIS 17th DAY OF February, 2016.
ASSESSOR: John Wilson DEPUTY ASSESSOR: Russell Schaideman
ACCOUNT NO. 99330-0010 & -0060



SURVEYOR'S CERTIFICATE
THIS MAP CORRECTLY REPRESENTS THE ALTERATION OF THE ORIGINAL SUBDIVISION PLAT STATED HEREON IN CONFORMANCE WITH THE REQUIREMENTS PURSUANT TO RCW 58.17.215.
[Signature] DATE: 2/15/16
PROFESSIONAL LAND SURVEYOR
KATHLEEN D. CASSOU PLS NO. 27133



City of Bellevue
AMENDED BINDING SITE PLAN NO. 15-126565 LJ
SECOND AMENDMENT

SHEET 1 OF 3

Parametrix
ENGINEERING PLANNING ENVIRONMENTAL SCIENCES
1019 30TH AVENUE SE, SUITE 100
PLYMOUTH, WASHINGTON 98374
T. 253.604.6000 F. 253.604.6799
www.parametrix.com
247-4575-002 02/03 FEB 2016

AGENCY APPROVAL:

BLOCK 14 - GIX

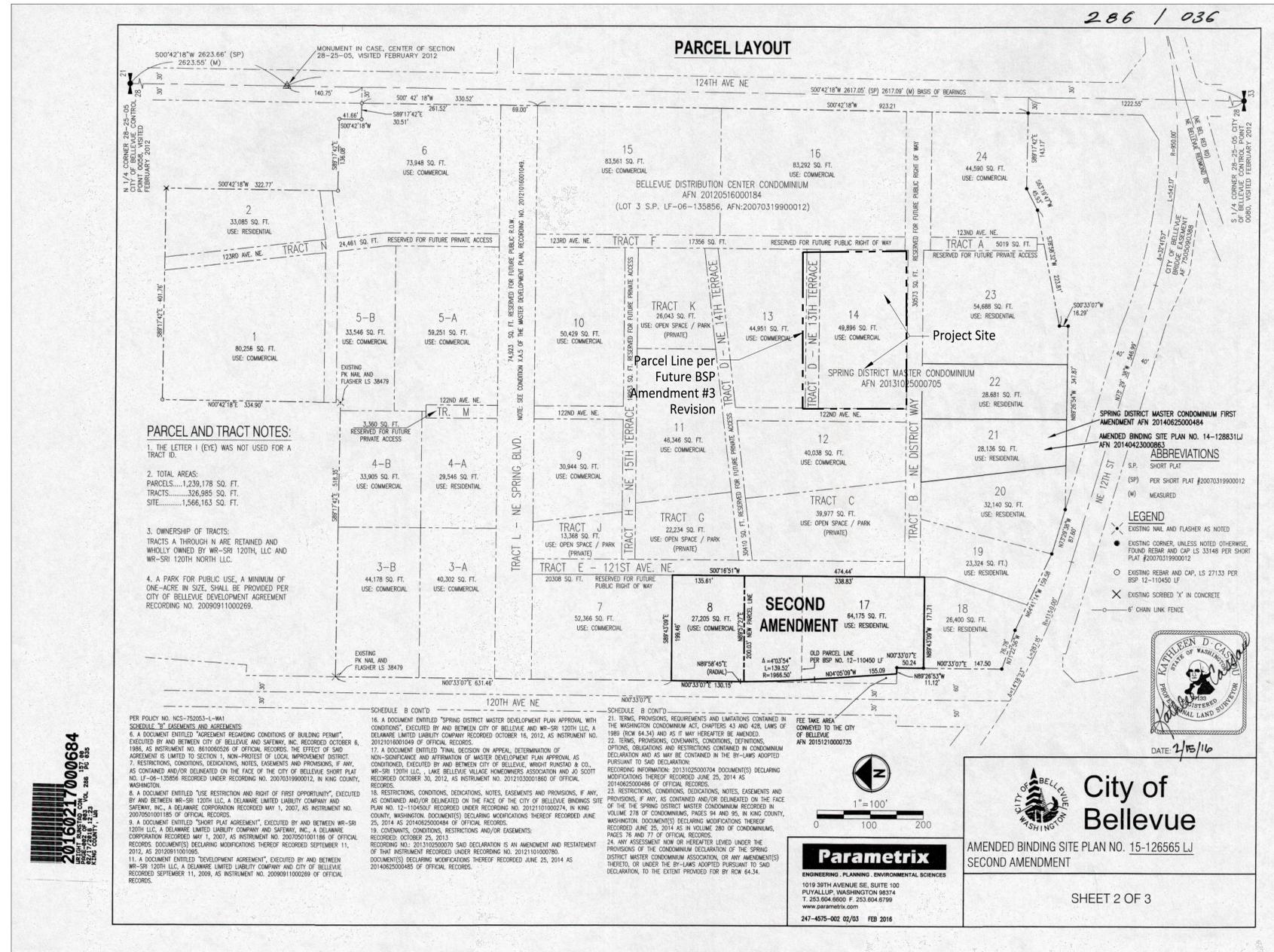
**ADMINISTRATIVE
DESIGN
REVIEW**

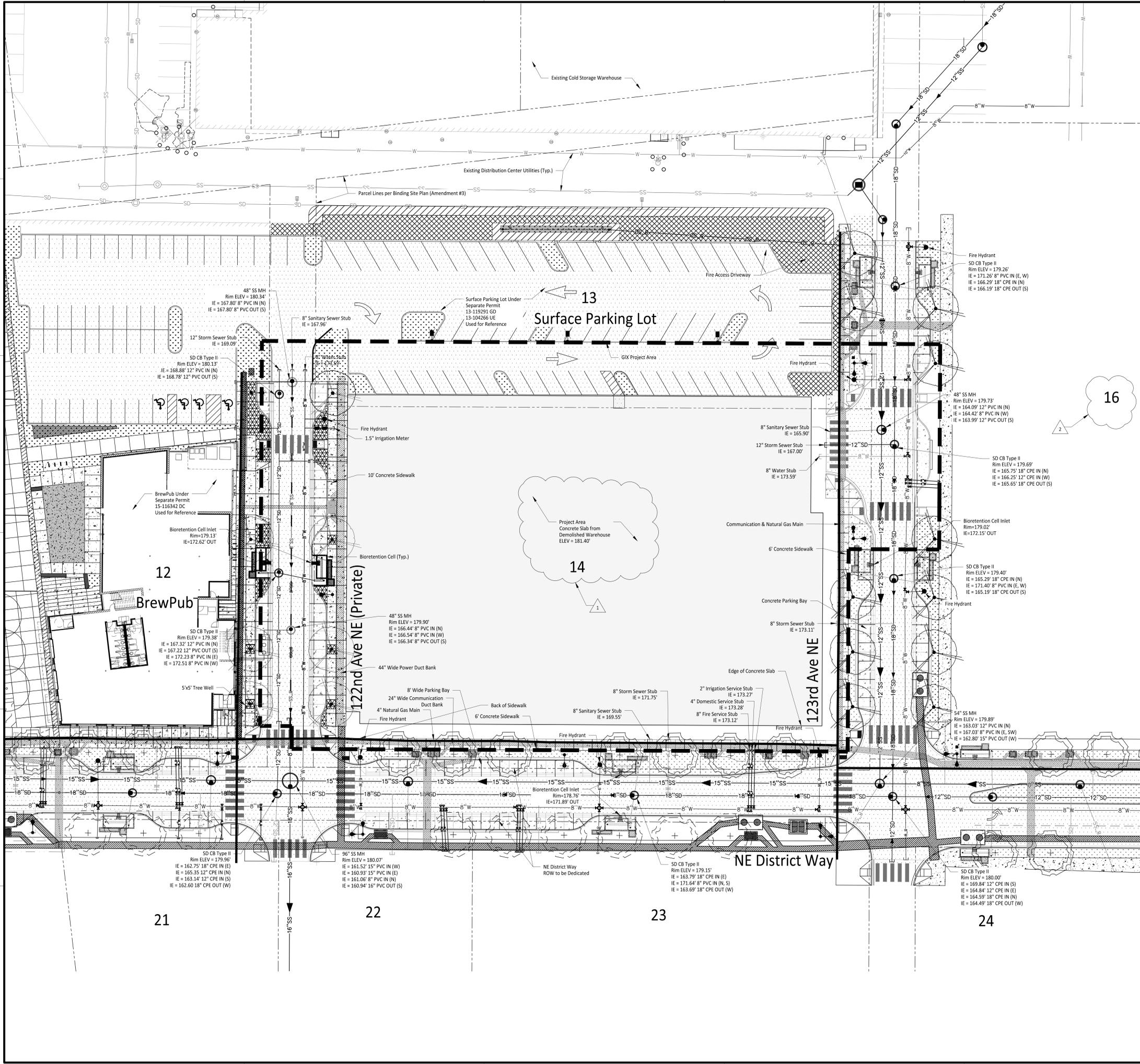


ISSUE		
MARK	DATE	DESCRIPTION
1	01-11-16	Design Review Revisions
2	04-07-16	Design Review Revisions

DRAWN BY: **D. Torrey** PROJ. ENG.: **J. Jones**
PROJ. NO.: **1500-001-010**
DATE: **2016/04/07**

DWG. 2
Binding Site Plan
DWG. **C1-102**





General Notes

- Parcel 12 BrewPub shown as Existing Condition, permitted under separate permits, to be constructed, and opened prior to completion of Proposed GIX Building.
- Parcel 13 Surface Parking Lot and 122nd Ave NE shown as Existing Condition, permitted under separate permits, to be constructed after Mass Excavation of Proposed GIX Building, and opened prior to completion of BrewPub/GIX Buildings.
- Site Conditions of BrewPub, Surface Parking Lot, and 122nd prior to construction is Concrete Slab from demolished Warehouse Building.

**WRIGHT
RUNSTAD
& COMPANY**

nbbj

223 YALE AVENUE NORTH
SEATTLE, WASHINGTON 98109
PHONE 206 223 5555
FAX 206 223 5000

JVTEAM
PO BOX 2066
SUMNER, WASHINGTON 98390
PHONE 206 596 2020

AGENCY APPROVAL:

BLOCK 14 - GIX

**ADMINISTRATIVE
DESIGN
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DRAWN BY: **D. Torrey** PROJ. ENG.: **J. Jones**

PROJ. NO.: **1500-001-010**

DATE: **2016/04/07**

DWG.

**Boundary &
Topographic Survey**

DWG.

C1-201

BLOCK 14 - GIX

ADMINISTRATIVE
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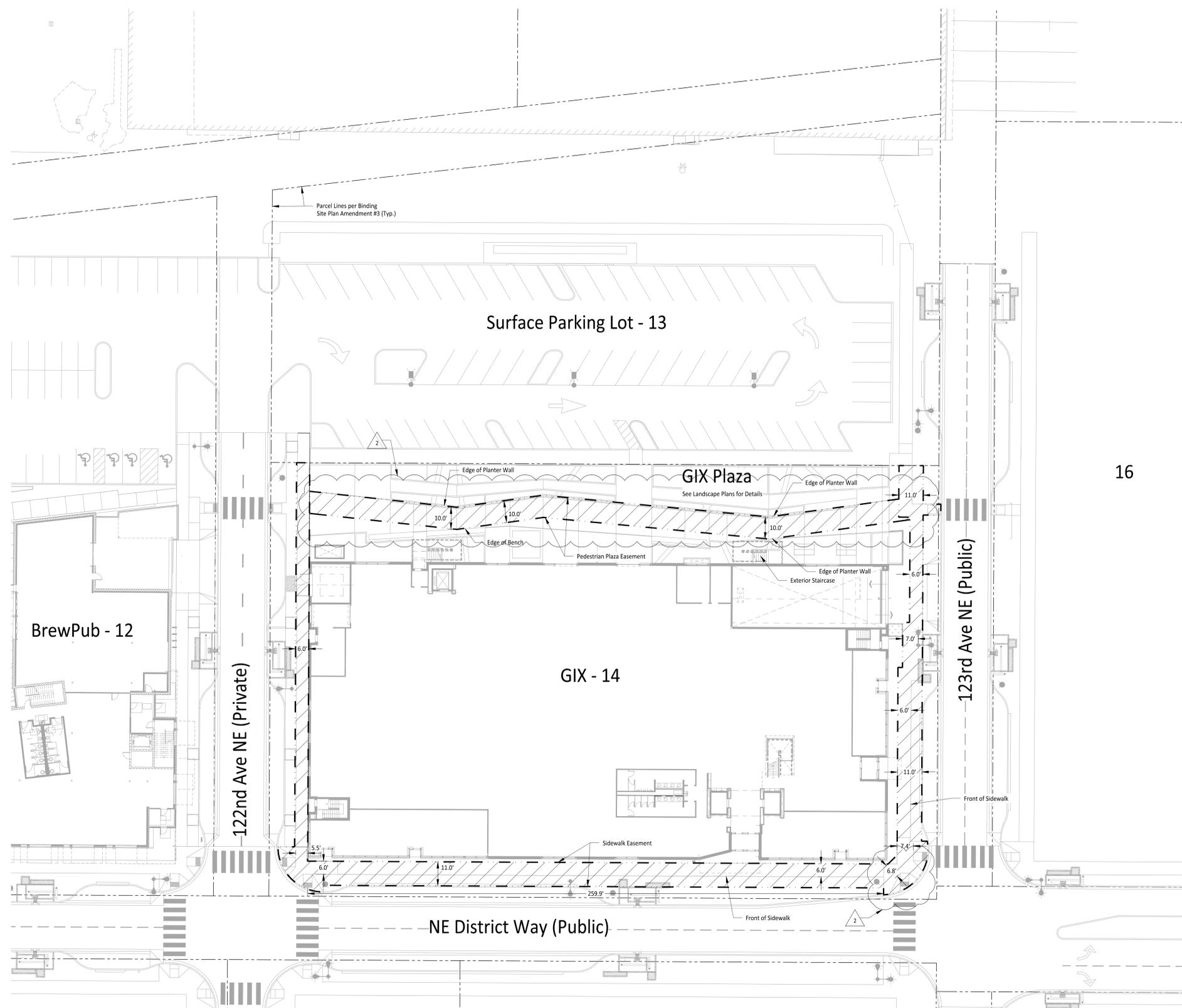
Sidewalk Easement
Plan

DWG.

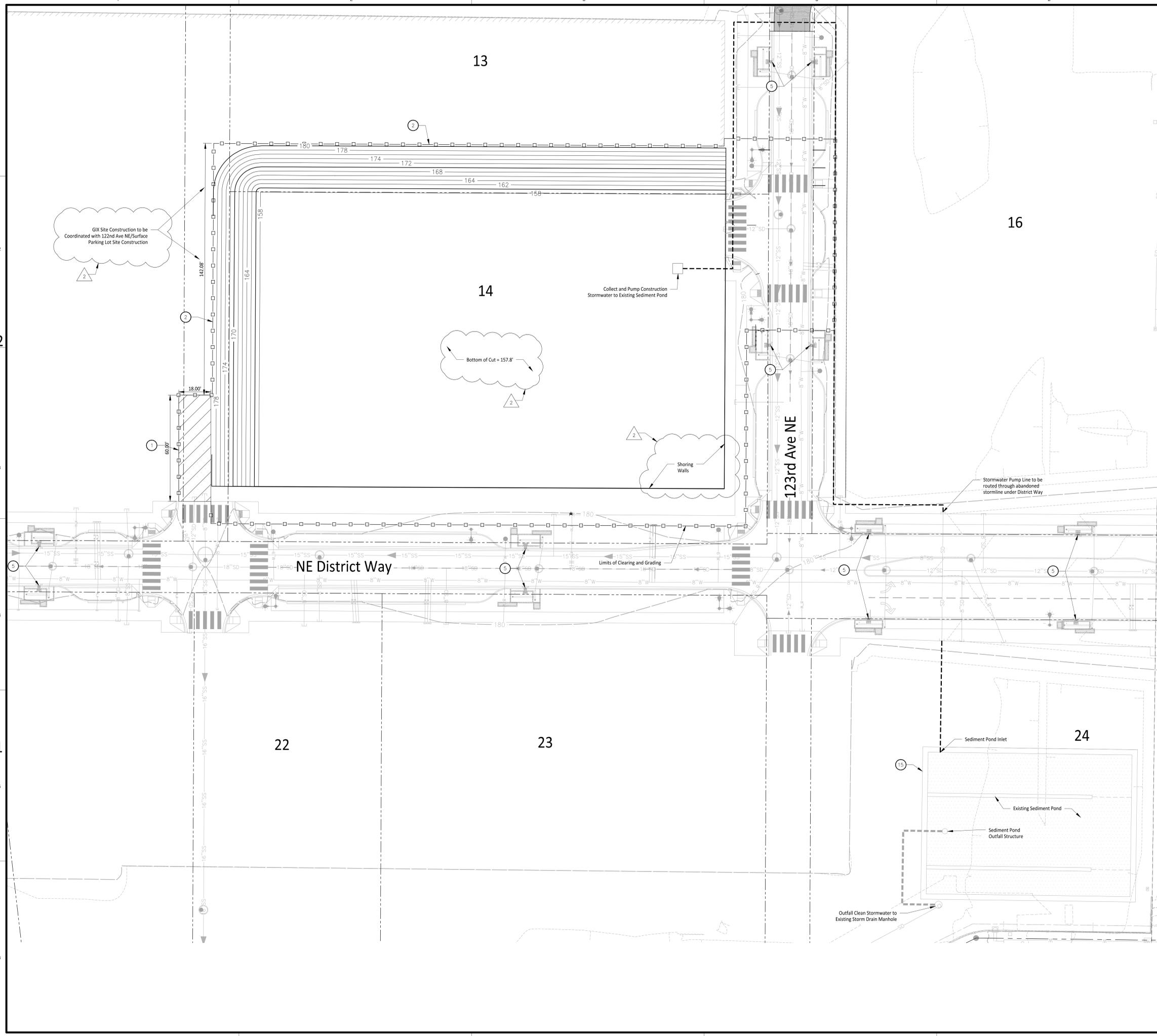
C1-301

LEGEND

— Sidewalk Easement



2



CONSTRUCTION NOTES

- 1 Install and maintain Construction Entrance per Washington Department of Ecology BMP C105 and Install Wheel Wash as needed per BMP C106.
- 2 Install Silt Fence per City of Bellevue and Ecology Standard Plans.
- 3 All Disturbed Pervious Areas shall be Stabilized, Soils Amended, and Hydroseeded or Strawed.
- 4 Install Storm Drain Inlet Protection in all Proposed and Existing Catch Basins within the Project Vicinity including 123rd Ave NE and NE District Way per WSDOT STD Plan I-7.
- 5 Protect Bioretention Cell inlets from Sediment and Construction Debris.
- 6 Install Straw Bale Barriers, wattles, and other necessary TESC Measures as necessary.
- 7 Exposed Soils shall be Watered as necessary to prevent dust from leaving the site.
- 8 Install High Visibility construction Fence where Silt Fence is not required as shown per WA DOE BMP C103.
- 9 Plug or Dam Catch Basins receiving runoff from Demolition Area, Divert to Tank for pH treatment prior to outfall to Storm Drain.
- 10 Maintain Natural Buffer Zone per WA DOE BMP C102.
- 11 Contractor to mark Clearing Limits with Lath and Flagging.
- 12 Turbidity Monitoring Point.
- 13 A CESCL shall be available on site or on call for the duration of Construction Operations.
- 14 Mass Excavation approximately 52,800 cubic yards of earth movement.
- 15 Contractor to maintain Approved, Existing Phase 1A Sediment Pond for use during GIX Construction.

**WRIGHT
RUNSTAD
& COMPANY**

nbbj

223 YALE AVENUE NORTH
SEATTLE, WASHINGTON 98109
PHONE 206 223 5555
FAX 206 223 5000

JUNTEAM
PO BOX 2066
SUMNER, WASHINGTON 98390
PHONE 206 596 2020

AGENCY APPROVAL:

BLOCK 14 - GIX

ADMINISTRATIVE
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DWG.

Temporary
Erosion Control
Plan

DWG.

C2-101



BMP C106: Wheel Wash

Purpose Wheel washes reduce the amount of sediment transported onto paved roads by motor vehicles.

Conditions of Use When a stabilized construction entrance (see BMP C105) is not preventing sediment from being tracked onto pavement.

- Wheel washing is generally an effective BMP when installed with careful attention to topography. For example, a wheel wash can be detrimental if installed at the top of a slope abutting a right-of-way where the water from the dripping truck can run unimpeded into the street.
- Pressure washing combined with an adequately sized and surfaced pad with direct drainage to a large 10-foot x 10-foot sump can be very effective.

Design and Installation Specifications

Suggested details are shown in Figure 4.3. The Local Permitting Authority may allow other designs. A minimum of 6 inches of asphalt treated base (ATB) over crushed base material or 8 inches over a good subgrade is recommended to pave the wheel wash.

Use a low clearance truck to test the wheel wash before paving. Either a belly dump or lowboy will work well to test clearance.

Keep the water level from 12 to 14 inches deep to avoid damage to truck hubs and filling the truck tongues with water.

Midpoint spray nozzles are only needed in extremely muddy conditions.

Wheel wash systems should be designed with a small grade change, 6 to 12 inches for a 10-foot-wide pond, to allow sediment to flow to the low side of pond to help prevent re-suspension of sediment. A drainpipe with a 2- to 3-foot riser should be installed on the low side of the pond to allow for easy cleaning and refilling. Polymers may be used to promote coagulation and flocculation in a closed-loop system. Polyacrylamide (PAM) added to the wheel wash water at a rate of 0.25 - 0.5 pounds per 1,000 gallons of water increases effectiveness and reduces cleanup time. If PAM is already being used for dust or erosion control and is being applied by a water truck, the same truck can be used to change the wash water.

Maintenance Standards

The wheel wash should start out the day with fresh water. The wash water should be changed a minimum of once per day. On large earthwork jobs where more than 10-20 trucks per hour are expected, the wash water will need to be changed more often.

Wheel wash or tire bath wastewater shall be discharged to a separate on-site treatment system, such as closed-loop recirculation or land application, or to the sanitary sewer with proper local sewer district approval.

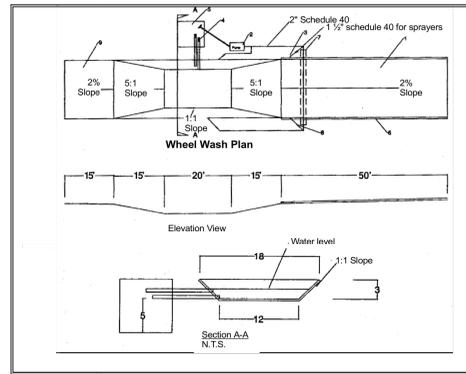


Figure 4.3 Wheel Wash

- Notes:
1. Asphalt construction entrance 6 in. asphalt treated base (ATB).
 2. 3-inch trash pump with floats on the suction hose.
 3. Midpoint spray nozzles, if needed.
 4. 6-inch sewer pipe with butterfly valves. Bottom one is a drain. Locate top pipe's invert 1 foot above bottom of wheel wash.
 5. 8 foot x 8 foot sump with 5 feet of catch. Build so can be cleaned with trackhoe.
 6. Asphalt curb on the low road side to direct water back to pond.
 7. 6-inch sleeve under road.
 8. Ball valves.
 9. 15 foot ATB apron to protect ground from splashing water.

BMP C105: Stabilized Construction Entrance

Purpose Construction entrances are stabilized to reduce the amount of sediment transported onto paved roads by vehicles or equipment by constructing a stabilized pad of quarry spalls at entrances to construction sites.

Conditions of Use Construction entrances shall be stabilized wherever traffic will be leaving a construction site and traveling on paved roads or other paved areas within 1,000 feet of the site.

On large commercial, highway, and road projects, the designer should include enough extra materials in the contract to allow for additional stabilized entrances not shown in the initial Construction SWPPP. It is difficult to determine exactly where access to these projects will take place; additional materials will enable the contractor to install them where needed.

Design and Installation Specifications

- See Figure 4.2 for details. Note: the 100' minimum length of the entrance shall be reduced to the maximum practicable size when the size or configuration of the site does not allow the full length (100').
- A separation geotextile shall be placed under the spalls to prevent fine sediment from pumping up into the rock pad. The geotextile shall meet the following standards:

Grab Tensile Strength (ASTM D4751)	200 psi min.
Grab Tensile Elongation (ASTM D4632)	30% max.
Mullen Burst Strength (ASTM D3786-80a)	400 psi min.
AOS (ASTM D4751)	20-45 (U.S. standard sieve size)

- Consider early installation of the first lift of asphalt in areas that will be paved; this can be used as a stabilized entrance. Also consider the installation of excess concrete as a stabilized entrance. During large concrete pours, excess concrete is often available for this purpose.
- Hog fuel (wood-based mulch) may be substituted for or combined with quarry spalls in areas that will not be used for permanent roads. Hog fuel is generally less effective at stabilizing construction entrances and should be used only at sites where the amount of traffic is very limited. Hog fuel is not recommended for entrance stabilization in urban areas. The effectiveness of hog fuel is highly variable and it generally requires more maintenance than quarry spalls. The inspector may at any time require the use of quarry spalls if the hog fuel is not preventing sediment from being tracked onto pavement or if the hog fuel is being carried onto pavement. Hog fuel is prohibited in permanent roadbeds because organisms in the subgrade soils cause degradation of the subgrade support over time.
- Fencing (see BMPs C103 and C104) shall be installed as necessary to restrict traffic to the construction entrance.

Maintenance Standards

- Whenever possible, the entrance shall be constructed on a firm, compacted subgrade. This can substantially increase the effectiveness of the pad and reduce the need for maintenance.
- Quarry spalls (or hog fuel) shall be added if the pad is no longer in accordance with the specifications.
- If the entrance is not preventing sediment from being tracked onto pavement, then alternative measures to keep the streets free of sediment shall be used. This may include street sweeping, an increase in the dimensions of the entrance, or the installation of a wheel wash.
- Any sediment that is tracked onto pavement shall be removed by shoveling or street sweeping. The sediment collected by sweeping shall be removed or stabilized on site. The pavement shall not be cleaned by washing down the street, except when sweeping is ineffective and there is a threat to public safety. If it is necessary to wash the streets, the construction of a small sump shall be considered. The sediment would then be washed into the sump where it can be controlled.
- Any quarry spalls that are loosened from the pad, which end up on the roadway shall be removed immediately.
- If vehicles are entering or exiting the site at points other than the construction entrance(s), fencing (see BMPs C103 and C104) shall be installed to control traffic.
- Upon project completion and site stabilization, all construction accesses intended as permanent access for maintenance shall be permanently stabilized.

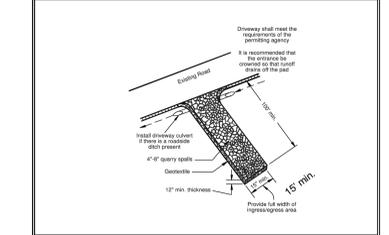


Figure 4.2 - Stabilized Construction Entrance

BMP C233: Silt Fence

Purpose Use of a silt fence reduces the transport of coarse sediment from a construction site by providing a temporary physical barrier to sediment and reducing the runoff velocities of overland flow. See Figure 4.19 for details on silt fence construction.

Conditions of Use Silt fence may be used downslope of all disturbed areas.

- Silt fence is not intended to treat concentrated flows, nor is it intended to treat substantial amounts of overland flow. Any concentrated flows must be conveyed through the drainage system to a sediment pond. The only circumstance in which overland flow can be treated solely by a silt fence, rather than by a sediment pond, is when the area draining to the fence is one acre or less and flow rates are less than 0.5 cfs.
- Silt fences should not be constructed in streams or used in V-shaped ditches. They are not an adequate method of silt control for anything deeper than sheet or overland flow.

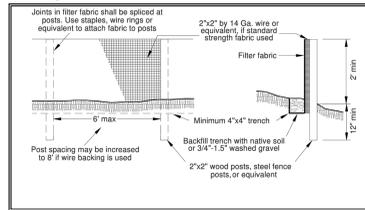


Figure 4.19 - Silt Fence

Design and Installation Specifications

- Drainage area of 1 acre or less or in combination with sediment basin in a larger site.
- Maximum slope steepness (normal (perpendicular) to fence line) 1:1.
- Maximum sheet or overland flow path length to the fence of 100 feet.
- No flows greater than 0.5 cfs.
- The geotextile used shall meet the following standards. All geotextile properties listed below are minimum average roll values (i.e., the test result for any sampled roll in a lot shall meet or exceed the values shown in Table 4.10):

CLEARING AND GRADING STANDARD NOTES

1. All clearing & grading construction must be in accordance with City of Bellevue (COB) *Clearing & Grading Code; Clearing & Grading Erosion Control Standard Details (EC-1 through EC-23); Development Standards; Land Use Code; Uniform Building Code; permit conditions; and all other applicable codes, ordinances, and standards.* The design elements within these plans have been reviewed according to these requirements. Any variance from adopted erosion control standards is not allowed unless specifically approved by the City of Bellevue Department of Planning & Community Development (PCD) prior to construction. It is the sole responsibility of the applicant and the professional civil engineer to correct any error, omission, or variation from the above requirements found in these plans. All corrections will be at no additional cost or liability to the COB. All details for structural walls, rockeries over four feet in height, geogrid reinforced rockeries, and geogrid reinforced modular block walls must be stamped by a professional engineer.
2. A copy of the approved plans must be on-site during construction. The applicant is responsible for obtaining any other required or related permits prior to beginning construction.
3. All locations of existing utilities have been established by field survey or obtained from available records and should, therefore, be considered only approximate and not necessarily complete. It is the sole responsibility of the contractor to independently verify the accuracy of all utility locations and to discover and avoid any other utilities not shown which may be affected by the implementation of this plan.
4. The area to be cleared and graded must be flagged by the contractor and approved by the clearing & grading inspector prior to beginning any work on the site.
5. A reinforced silt fence must be installed in accordance with COB EC-5 and located as shown on the approved plans or per the clearing & grading inspector, along slope contours and down slope from the building site.
6. A hard-surface construction access pad is required per Clearing & Grading Standard Detail EC-1 or EC-2. This pad must remain in place until paving is installed.
7. Clearing will be limited to the areas within the approved disturbance limits. Exposed soils must be covered at the end of each working day when working from October 1st through April 30th. From May 1st through September 30th, exposed soils must be covered at the end of each construction week and also at the threat of rain.
8. Any excavated material removed from the construction site and deposited on property within the City limits must be done in compliance with a valid clearing & grading permit. Locations for the mobilization area and stockpiled material must be approved by the clearing & grading inspector at least 24 hours in advance of any stockpiling.

9. To reduce the potential for erosion of exposed soils, or when rainy season construction is permitted, the following Best Management Practices (BMPs) are required:
 - Preserve natural vegetation for as long as possible or as required by the clearing & grading inspector.
 - Protect exposed soil using plastic (EC-14), erosion control blankets, straw or mulch (COB Guide to Mulch Materials, Rates, and Use Chart), or as directed by the clearing & grading inspector.
 - Install catch basin inserts as required by the clearing & grading inspector or permit conditions of approval.
 - Install a temporary sediment pond, a series of sedimentation tanks, temporary filter vaults, or other sediment control facilities. Installation of exposed aggregate surfaces requires a separate effluent collection pond on-site.
10. Final site grading must direct drainage away from all building structures at a minimum 2% slope, per the *Uniform Building Code*.
11. The contractor must maintain a sweeper on-site during earthwork and immediately remove soil that has been tracked onto paved areas as result of construction.
12. A public information sign listing 24-hour emergency phone numbers for the city and the contractor may be provided to the applicant at the time the clearing & grading permit is issued. The applicant must post the sign at the project site in full view of the public and the contractors, and it must remain posted until final sign-off by the clearing & grading inspector.
13. Turbidity monitoring may be required as a condition of clearing & grading permit approval. If required, turbidity monitoring must be performed in accordance with the approved turbidity monitoring plan and as directed by the clearing & grading inspector. Monitoring must continue during site (earthwork) construction until the final sign-off by the clearing & grading inspector.
14. Any project that is subject to Rainy Season Restrictions will not be allowed to perform clearing & grading activities without written approval from the PCD director. The rainy season extends from November 1st through April 30th, as defined in section 23.76.093A of the *Clearing & Grading Code*.

AGENCY APPROVAL:

ADMINISTRATIVE DESIGN REVIEW



ISSUE

MARK	DATE	DESCRIPTION
1	01-11-16	Design Review Revisions
2	04-01-16	Design Review Revisions

DRAWN BY: **D. Torrey** PROJ. ENG.: **J. Jones**

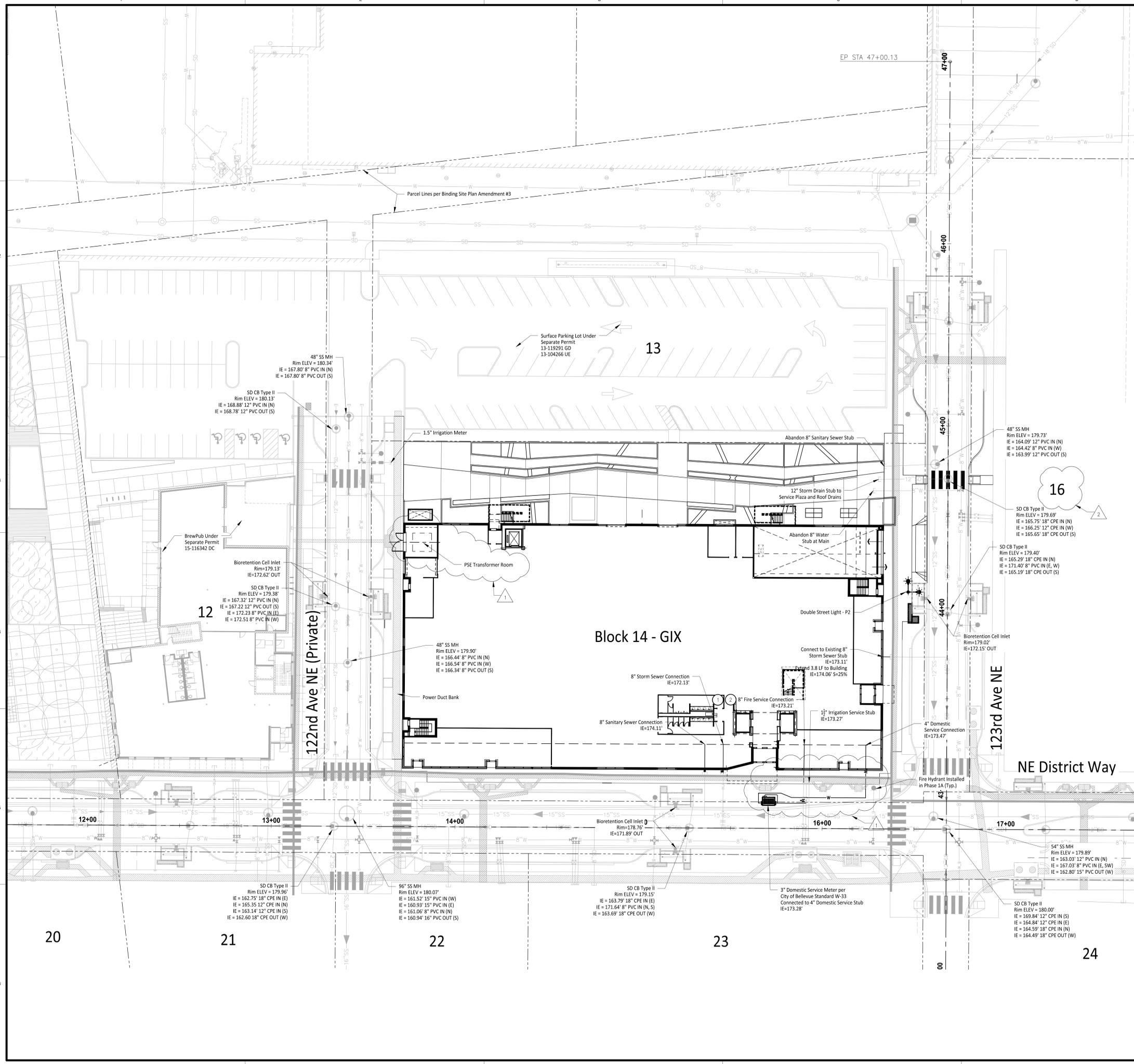
PROJ. NO.: **1500-001-010**

DATE: **2016/04/07**

DWG.

TESC Details

DWG.



CONSTRUCTION NOTES

- 1 Reduced Pressure Backflow Assembly per City of Bellevue STD. Plan W- to be located within Building.
- 2 Double Check Valve Assembly per City of Bellevue STD. Plan W-38 to be located within Building.

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AGENCY APPROVAL:

BLOCK 14 - GIX

ADMINISTRATIVE
DESIGN
REVIEW



ISSUE

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DATE: **2016/04/07**

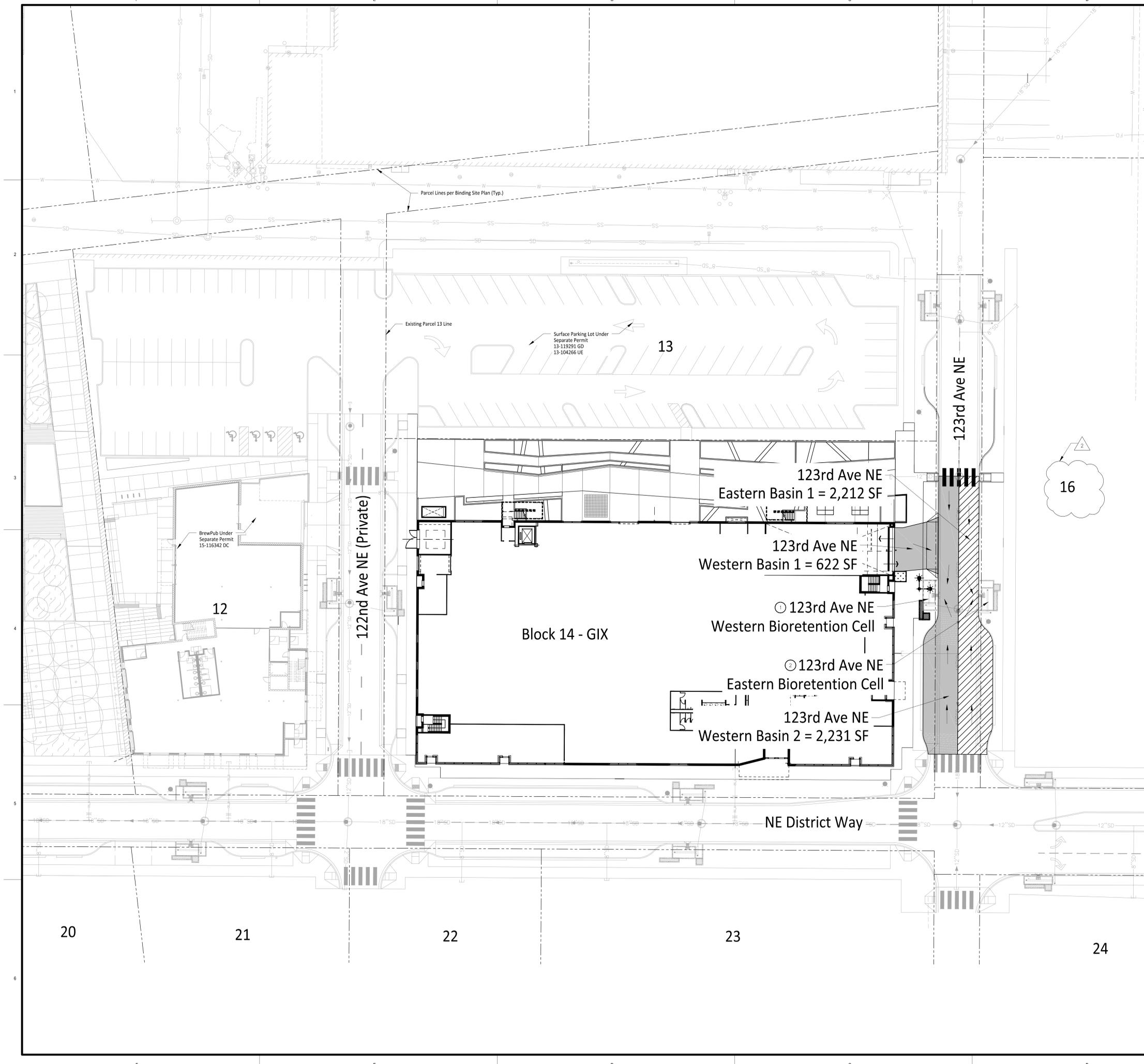
DWG.

Utility Plan

DWG.

C3-101





GENERAL NOTES

- 1 123rd Ave NE Western Basins 1-2 = 2,853 SF which will be conveyed to the 123rd Ave NE Western Bioretention Cell for treatment and discharge.
- 2 123rd Ave NE Eastern Basin 1 = 2,212 SF which will be conveyed to the 123rd Ave NE Eastern Bioretention Cell for treatment and discharge.

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PROJ. NO.: **1500-001-010**

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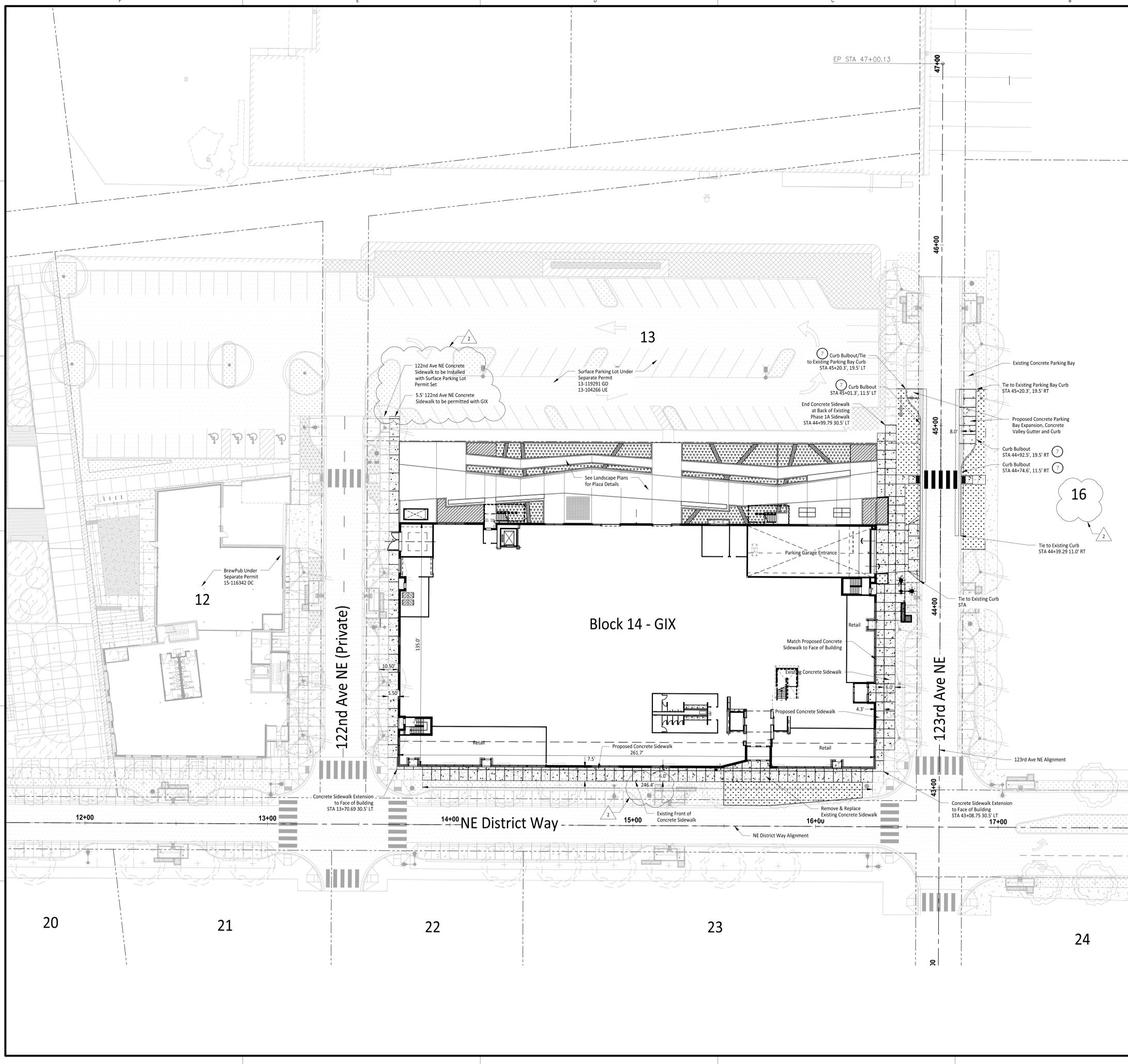
DWG.

Stormwater
Treatment Plan

DWG.

C3-102

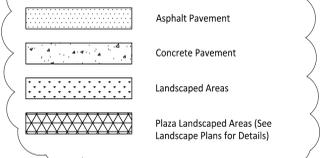




CONSTRUCTION NOTES

- 1 Channelization Lines per City of Bellevue STD. Drawing TE-4.
- 2 Road Pavement Arrow Markings per City of Bellevue STD. Drawing TE-5.
- 3 Crosswalk Markings per City of Bellevue STD. Drawing TE-7.
- 4 Cement Concrete Curb & Gutter, Cement Concrete Traffic Curb, and Extruded Curb per City of Bellevue STD. Drawing TE-10.
- 5 Sidewalks per City of Bellevue STD. Drawing TE-11.
- 6 Install Curb Ramp Type I per City of Bellevue STD. Drawing TE-12.
- 7 See Detail C on Sheet C3-401 for Parking Bulbout Dimensions & Locating Information.
- 8 See Details G & H on Sheet C3-401 for Tooled Control & Expansion Joint Details.

LEGEND



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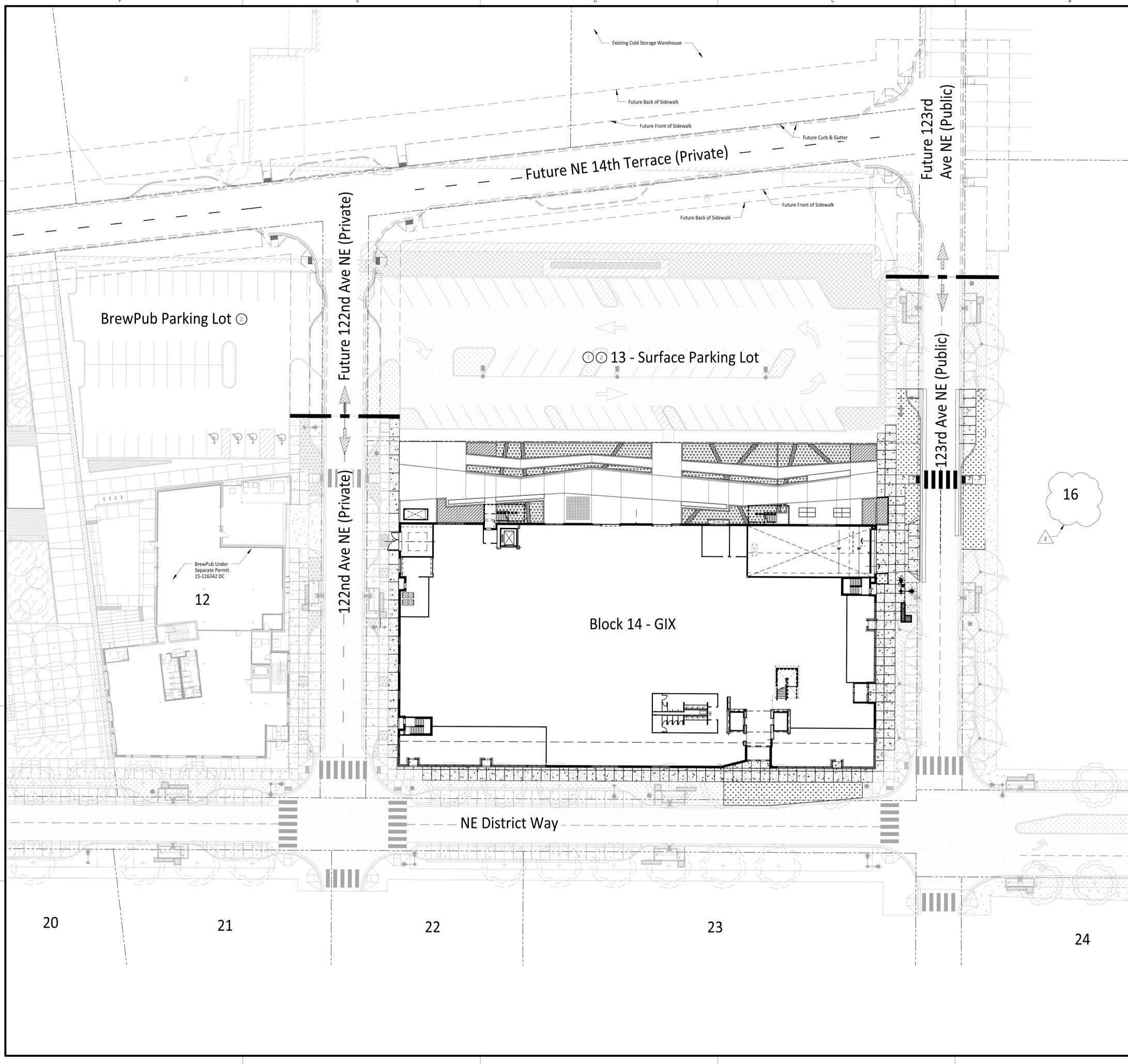
DWG.

Hardscape &
Landscape Plan

DWG.

C3-301





- GENERAL NOTES**
- Parcel 13 Surface Parking Lot and 122nd Ave NE shown as Existing Condition, permitted under separate permits, to be constructed after Mass Excavation of GIX Building, permitted under separate permits, and opened prior to completion of BrewPub/GIX Buildings.
 - Parking stalls to be mitigated to other locations within The Spring District if future transportation elements disrupt existing parking stalls.

- LEGEND**
- Asphalt Pavement
 - Concrete Pavement
 - Landscaped Areas
 - Plaza Landscaped Areas (See Landscape Plans for Details)

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Road Plan

DWG.
C3-302

BLOCK 14 - GIX

ADMINISTRATIVE
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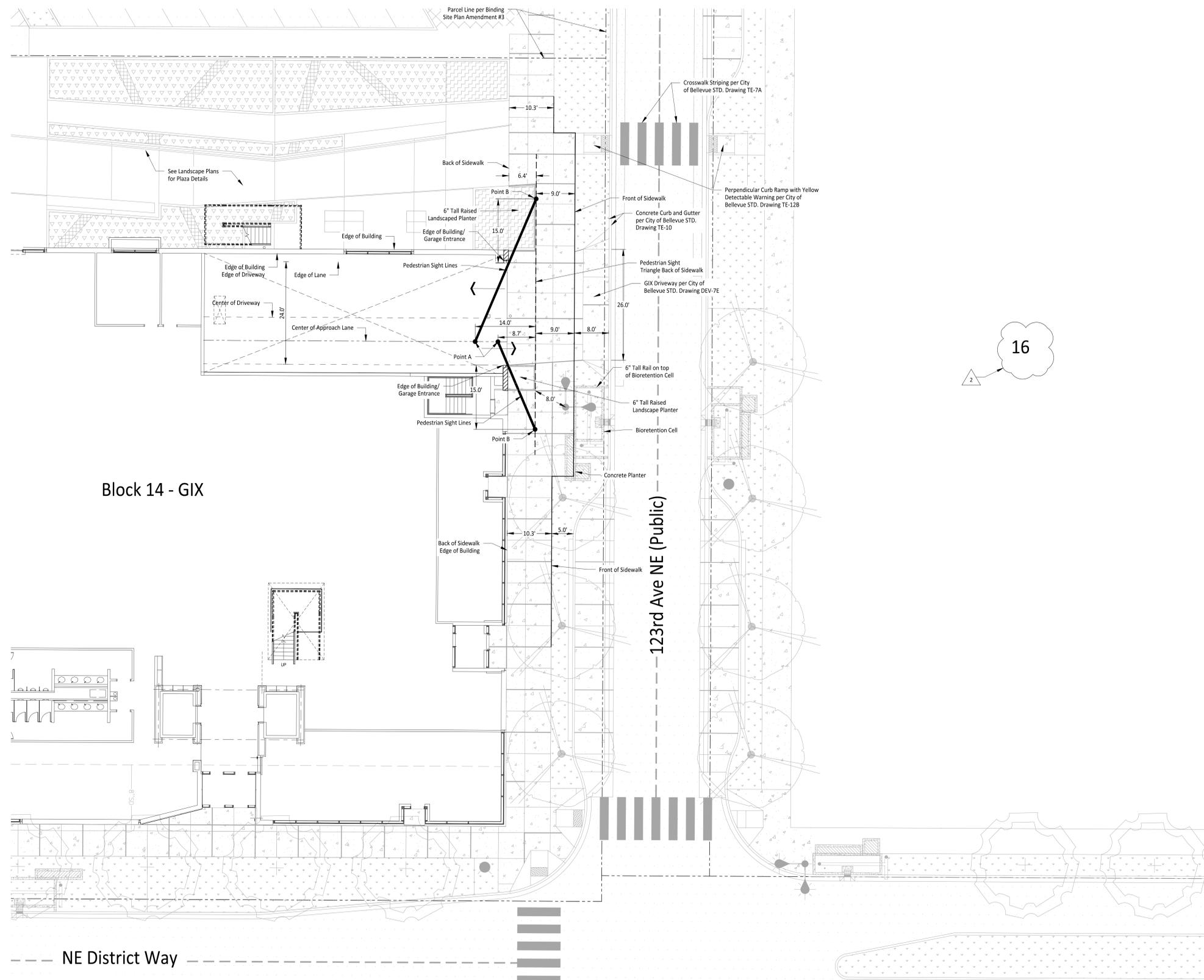
DATE: **2016/04/07**

DWG.

Pedestrian Sight
Triangle Plan

DWG.

C3-303



LEGEND

- Asphalt Pavement
- Concrete Pavement
- Landscaped Areas
- Plaza Landscaped Areas (See Landscape Plans for Details)

16



1

BLOCK 14 - GIX

**ADMINISTRATIVE
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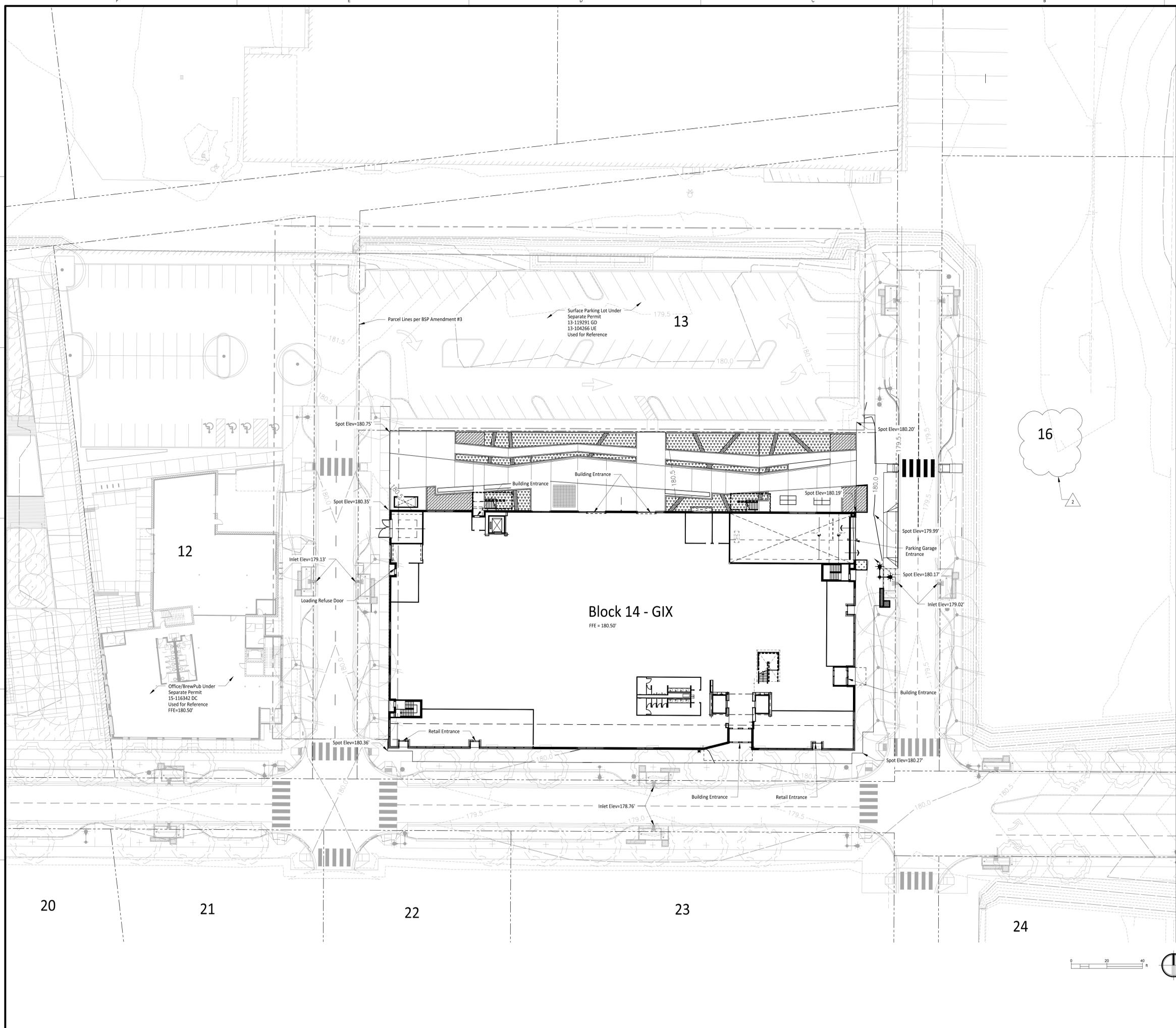
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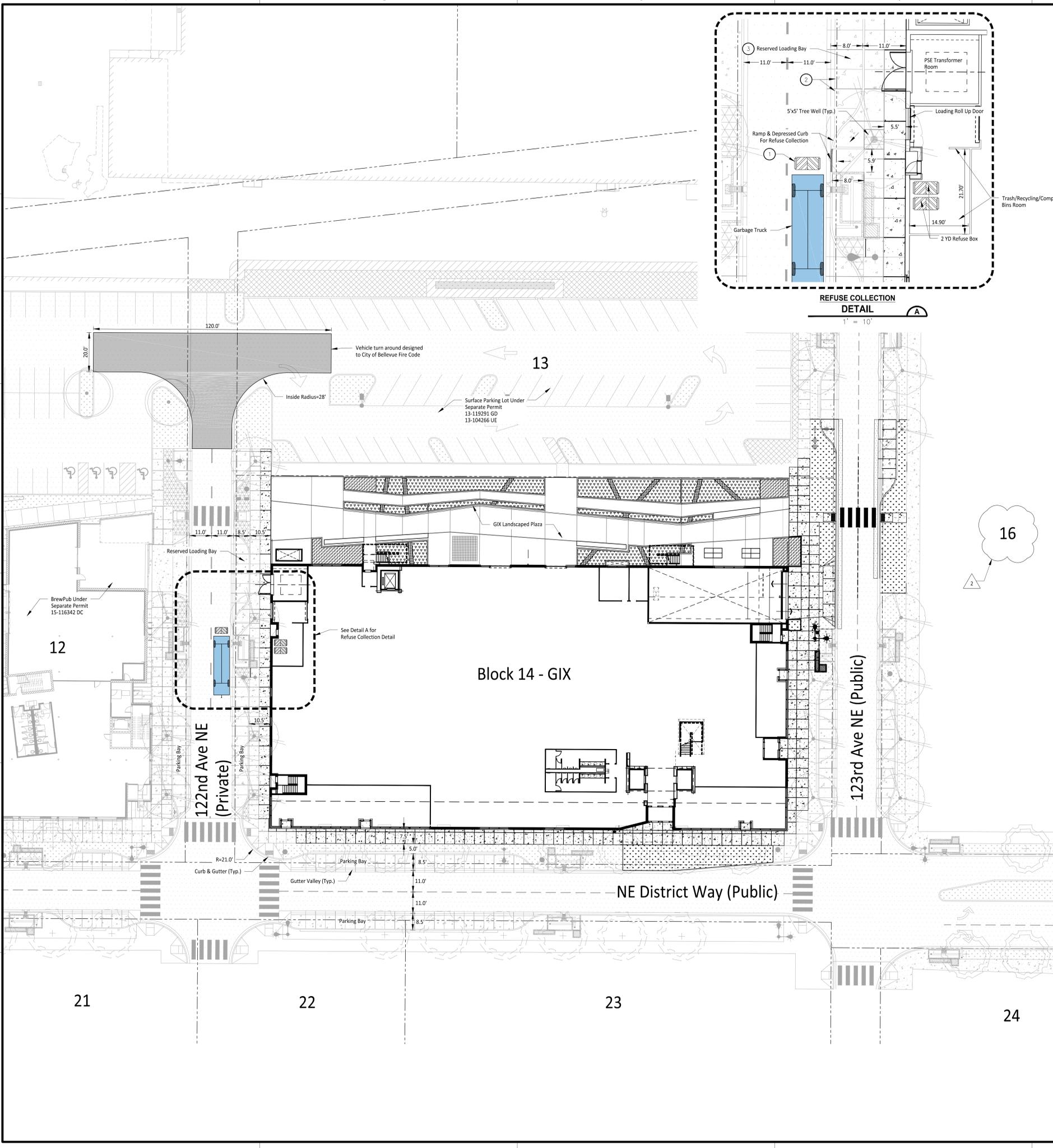
DWG.

Grading Plan

DWG.

C3-401





GENERAL NOTES

1. Republic Services to wheel two 2 YD (See Refuse Collection Document B) Refuse Boxes from Trash/Recycling/Compost Bins Room to 122nd Ave NE (Private) for pickup during the hours of 12am-6am and be wheeled back to Trash/Recycling/Compost Bins Room when Pick-Up is completed.
2. Building Operations to wheel two 2 YD (See Refuse Collection Document B) Refuse Boxes from Trash/Recycling/Compost Bins Room to 122nd Ave NE Reserved Loading Bay for Republic Services Pick-Up during the hours of 12am-6am and Building Operations shall return the 2 YD Refuse Boxes back to the Trash/Recycling/Compost Bins Room after Pick-Up.
3. Reserved 8' Loading Bay to be used for PSE Transformer Room Vehicle Access, general loading for GIX, and staging for refuse collection services by Republic Services.

LEGEND

- Asphalt Pavement
- Concrete Pavement
- Landscaped Areas
- Plaza Landscaped Areas (See Landscape Plans for Details)

WASTEQUIP
Naturally smart for personal service

Model Size	2 YD BOX	3 YD BOX	4 YD BOX	6 YD BOX	8 YD BOX	10 YD BOX	2 YD SLANT	3 YD SLANT	4 YD SLANT	6 YD SLANT	8 YD SLANT	10 YD SLANT
Trackbed Quantities #8 1/2" x 31"	53/57	84/57	202/28	147/16	131/14	67	49/53	31/26	23/21	14/16	13/14	6/7
Plastic Lid Standard	37" x 41" Single Wall	37" x 46" Single Wall	37" x 46" Single Wall	37" x 58" Single Wall	37" x 58" Single Wall	37" x 58" Single Wall	37" x 46" Single Wall	37" x 46" Single Wall	37" x 58" Single Wall			
Side Door	N/A	N/A	N/A	30" x 30" Single Wall	30" x 30" Single Wall	30" x 30" Single Wall	N/A	N/A	N/A	N/A	N/A	Optional 30" x 30" Single Wall
Sides	12 GA.											
Front	12 GA.											
Rear	12 GA.											
Bottom	10 GA.											
Pockets	10 GA.											
Bumpers	7 GA. Rounded											
Bottom Channel	(4) Formed Caster Pads	(6) Formed Caster Pads	(2) Formed G-Channel	(2) Formed G-Channel	(2) Formed G-Channel	(2) Formed G-Channel	(4) Formed Caster Pads	(4) Formed Caster Pads	(2) Formed G-Channel	(2) Formed G-Channel	(2) Formed G-Channel	(2) Formed G-Channel
Back Tube	10 GA. Formed G-Channel											
Front Tube	10 GA. Formed G-Channel											
Side Tubes	12 GA. Formed G-Channel											
Drain Plug	1 1/2" Dia. Plastic											
Theo. Weight	500#	625#	775#	1010#	1225#	1350#	531#	645#	765#	955#	1160#	1620#

NOTE: For XHD specifications, consult factory. Unless stated otherwise in writing, container sizes indicated on sales literature, invoices, price list, quotations, and delivery tickets are nominal sizes. Actual volume may vary from nominal sizes. *Shipping quantities based on typical 45' and 55' dropdeck trailers. Actual quantities may vary.

REFUSE COLLECTION DOCUMENT DETAIL B

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BLOCK 14 - GIX

ADMINISTRATIVE DESIGN REVIEW



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D. Torrey	J. Jones
PROJ. NO.:	1500-001-010
DATE:	2016/04/07

DWG.

1

Loading/Refuse Collection Plan

DWG.

C4-101

BLOCK 14 - GIX

**ADMINISTRATIVE
DESIGN
REVIEW**



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PROJ. NO.: **1500-001-010**

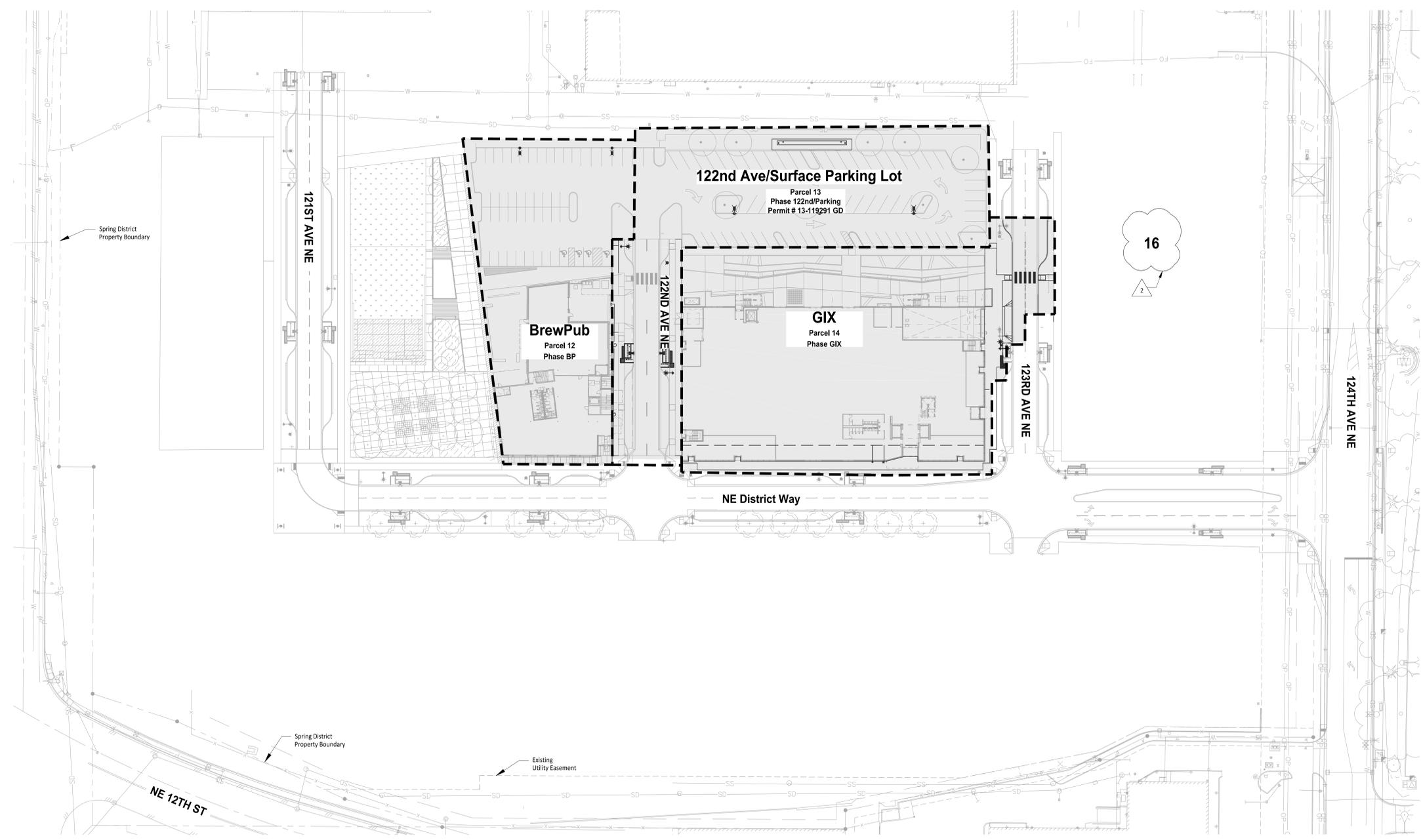
DATE: **2016/04/07**

DWG.

**Clear & Grade
Phasing Plan**

DWG.

C4-301



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BLOCK 14 - GIX

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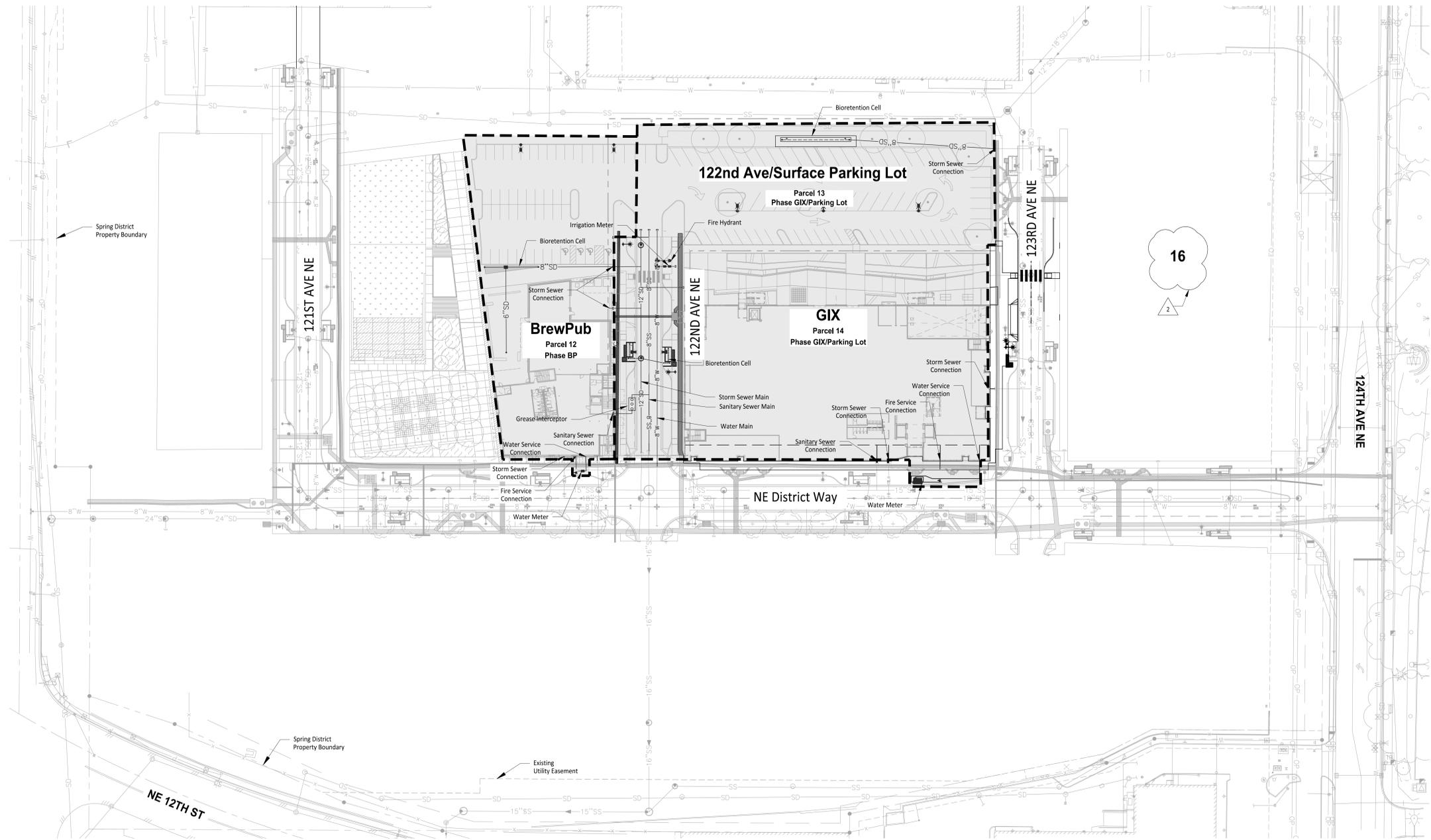
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DWG.

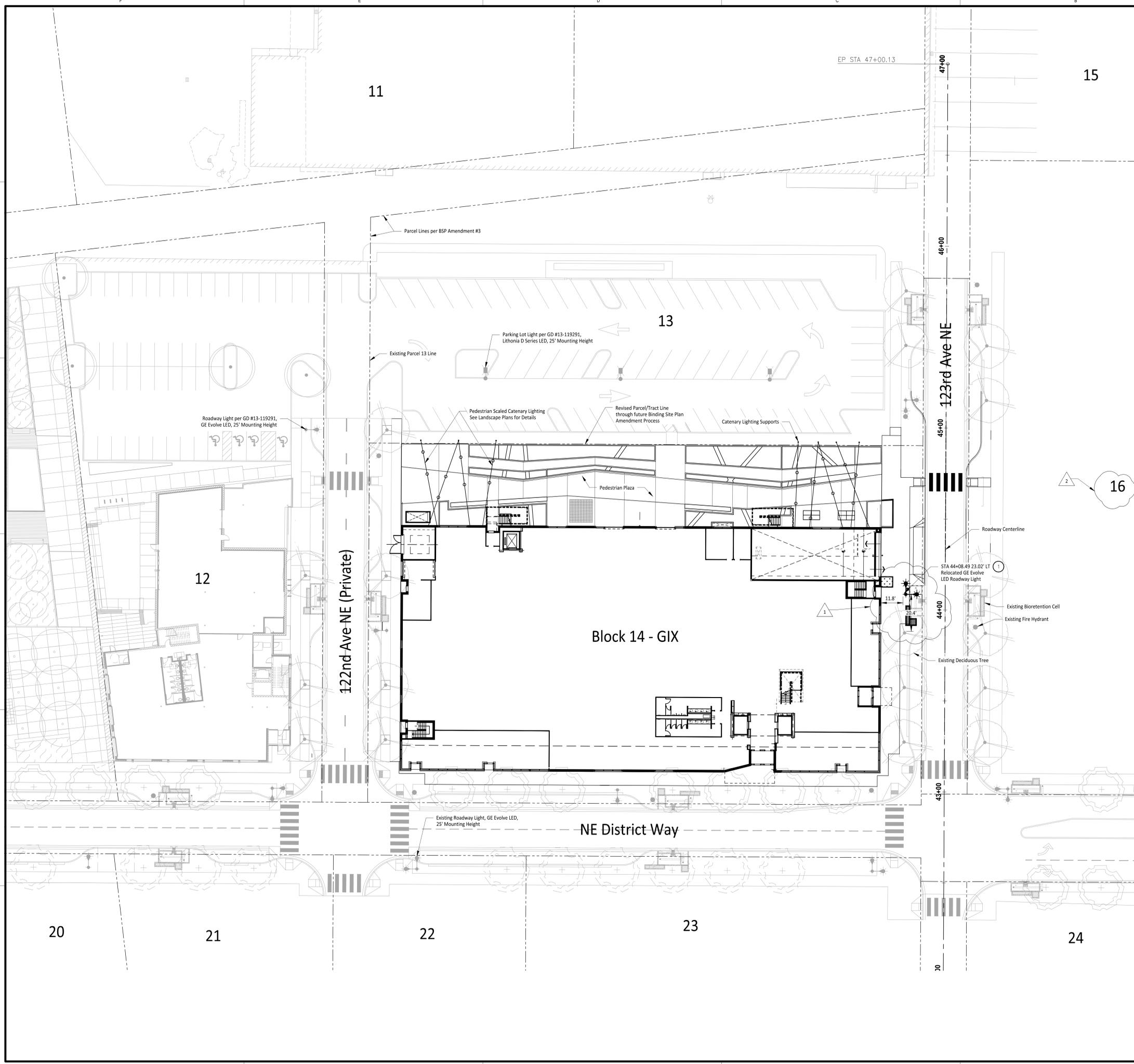
Utility Phasing Plan

DWG.

C4-302



1



CONSTRUCTION NOTES

- 1 Relocate GE Evolve LED Roadway Light. Street Light Foundations installed per City of Bellevue STD. Drawing TSSL-2.

GENERAL NOTES

- 1. Streetlights for NE District Way, 121st Ave NE, and 123rd Ave NE (To be Dedicated ROW's to the City) are designed, permitted, and constructed to City of Bellevue Standard Light Tertiary Street Light Levels.
- 2. For 122nd Ave NE, a private street classified as a local street, designed and permitted under separate permit.

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BLOCK 14 - GIX

**ADMINISTRATIVE
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PROJ. NO.: **1500-001-010**

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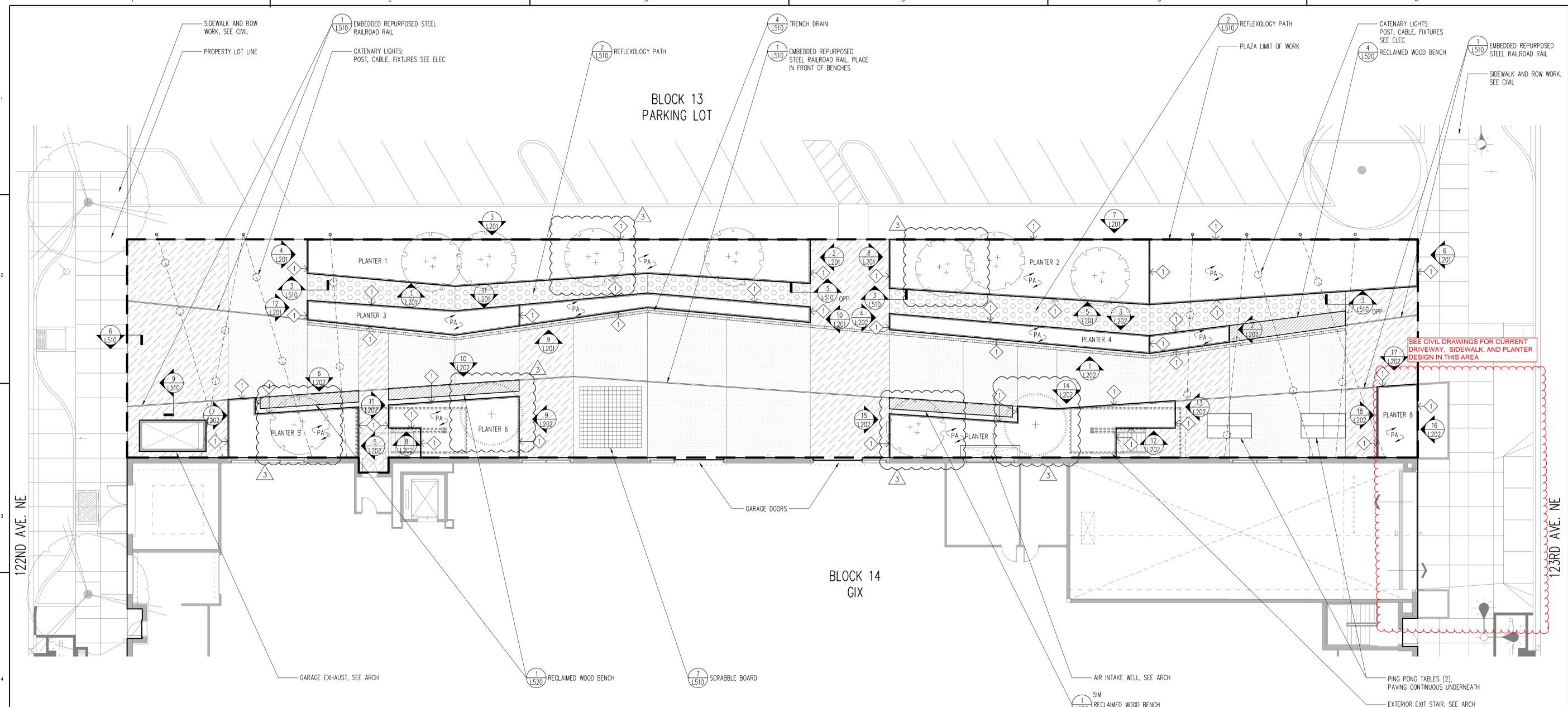
DWG.

Preliminary
Illumination Plan

DWG.

C5-101





1 MATERIALS PLAN
SCALE: 1/8" = 1'-0"



SITE FURNISHING SCHEDULE

SYMBOL	DESCRIPTION	DETAIL	NOTES	SPEC
	PING PONG TABLE		PRECAST CONCRETE TABLE, PROVIDED BY OWNER, INSTALLED BY CONTRACTOR	
	RECLAIMED WOOD BENCH	SEE PLAN	RECLAIMED WOOD OVER PIP CONCRETE BASE, WOOD SIZE AND DETAILS TBD	062013
	TRENCH DRAIN	4 L510	CAST IRON GRATE COVER, SEE MEP FOR DRAIN	129300
	EMBEDDED REPURPOSED STEEL RAILROAD RAIL	1 L510		055001
	CATENARY LIGHTS, POSTS, CABLES, AND FIXTURES		SEE, ELEC	129300

MATERIAL SCHEDULE

SYMBOL / TYPE	DESCRIPTION	DETAIL	NOTES	SPEC
PT-1	PIP CONCRETE, EXPOSED AGGREGATE FINISH	6 L510	SEE STRUCT	
PT-2	PIP CONCRETE, LIGHT BROOM FINISH	6 L510	SEE STRUCT	
PT-3	REFLEXOLOGY PATH: MORTAR SET SMOOTH RIVER STONES	2 L510		321440
PT-4	SCRABBLE BOARD: MORTAR SET PRECAST PAVERS OR SIMILAR, FLUSH TO GRADE	7 L510	(5) DIFFERENT PAVER COLORS AND/OR FINISHES, PROVIDED BY OWNER, INSTALLED BY CONTRACTOR	321440

WALL AND EDGE SCHEDULE

SYMBOL	DESCRIPTION	DETAIL	NOTES	SPEC
1 L500	PIP CONCRETE WALL WITH CUSTOM FORMLINER FINISH	1 L500	REFER TO L151 FOR WALL HEIGHTS, LEADERS POINT TO FINISH FACE OF WALL	

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SEATTLE, WA 98101

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BLOCK 14 - GIX

ADMINISTRATIVE DESIGN REVIEW

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3	4-8-2016	ADR REVISION 3

DRAWN BY: **GGN**
PROJ. ARCH: **GGN**
PROJ. NO.: **100908.11**
DATE: **4-8-2016**

DWG. **MATERIALS PLAN**
DWG. **L121**



BLOCK 13
PARKING LOT

BLOCK 14
GIX

122ND AVE. NE

123RD AVE. NE

1 PLANTING PLAN
SCALE: 1/8"=1'-0"



PLANTING NOTES

- IF CONFLICTS ARISE BETWEEN PLANT QUANTITIES AND PLANS, CONTRACTOR TO CONTACT THE LANDSCAPE ARCHITECT FOR RESOLUTION.
- ALL PLACEMENT TO BE VERIFIED IN FIELD BY LANDSCAPE ARCHITECT PRIOR TO FINAL INSTALLATION.
- ALL PLANTING AREA TO BE IRRIGATED WITH AUTOMATED IRRIGATION SYSTEM WITH CONTROLLER, BACKFLOW PREVENTER, AND ZONE CONTROLS, PROVIDING 100% PLANT/IRRIGATION COVERAGE.
- SEE CIVIL FOR ROW TREES.

DECIDUOUS TREE SCHEDULE

SYMBOL	QTY	SCIENTIFIC NAME	COMMON NAME	SIZE	SPACING	NOTE	SPEC
	9	ACER CIRCINATUM	VINE MAPLE	3-5 STEMS 1"-1.5" CALIPER PER STEM	AS SHOWN IN PLAN, LANDSCAPE ARCHITECT TO PLACE IN FILED		329000
	2	CERCIDIPHYLLUM JAPONICUM	KATSURA	2"-4" CALIPER	AS SHOWN IN PLAN, LANDSCAPE ARCHITECT TO PLACE IN FILED		329000

PLANTING SCHEDULE - SHRUB

SYMBOL	AREA	TYPE	QTY.	SCIENTIFIC NAME	COMMON NAME	SIZE	SPACING	NOTE	SPEC
	626 SQ. FT.	AZALEAS		RHODODENDRON 'BLUE DANUBE'	BLUE DANUBE AZALEA	2 GAL	24" O.C.	CONTRACTOR TO VERIFY AVAILABILITY OF VARIETIES LISTED; ARCHITECT TO SELECT MIX OF 3-5 VARIETIES FOR FINAL INSTALLATION; TRIANGULAR SPACING	329000
				RHODODENDRON 'BLUE DIAMOND'	BLUE DIAMOND AZALEA	2 GAL	24" O.C.		329000
				RHODODENDRON 'GIRARD DWARF LAVENDER'	GIRARD DWARF LAVENDER AZALEA	2 GAL	24" O.C.		329000
				RHODODENDRON 'GIRARD'S PURPLE'	GIRARD'S PURPLE AZALEA	2 GAL	24" O.C.		329000
				RHODODENDRON 'HERBERT'	HERBERT AZALEA	2 GAL	24" O.C.		329000
				RHODODENDRON 'KARENS'	KAREN AZALEA	2 GAL	24" O.C.		329000
				RHODODENDRON 'PURPLE GEM'	PURPLE GEM AZALEA	2 GAL	24" O.C.		329000
				RHODODENDRON 'ROBLEX'	AUTUMN LILY ENCORE AZALEA	2 GAL	24" O.C.		329000
				RHODODENDRON 'VIOLET MIST'	VIOLET MIST AZALEA	2 GAL	24" O.C.		329000
					2657 SQ. FT.	FERNS			DRYOPTERIS CELSA
DRYOPTERIS ERYTHROSORA 'BRILLIANCE'	AUTUMN FERN	2 GAL	24" O.C.					329000	
POLYSTICHUM SETIFERUM	SOFT SHIELD FERN	2 GAL	24" O.C.					329000	
	280 SQ. FT.	HOSTAS						HOSTA 'BLUE MOUSE EARS'	BLUE MOUSE EARS HOSTA
				HOSTA 'HADSPEN BLUE'	HADSPEN BLUE PLANTAIN LILY	1 GAL	18" O.C.	329000	
				HOSTA 'HALCYON'	HALCYON PLANTAIN LILY	1 GAL	18" O.C.	329000	

BLOCK 14 - GIX

ADMINISTRATIVE DESIGN REVIEW

WRIGHT
RUNSTAD
& COMPANY

nbbj
223 YALE AVENUE NORTH
SEATTLE, WASHINGTON 98109
PHONE 206 223 5555
FAX 206 223 5000

GUSTAFSON GUTHRIE NICHOL
1932 FIRST AVENUE, SUITE 700
SEATTLE, WA 98101

AGENCY APPROVAL:

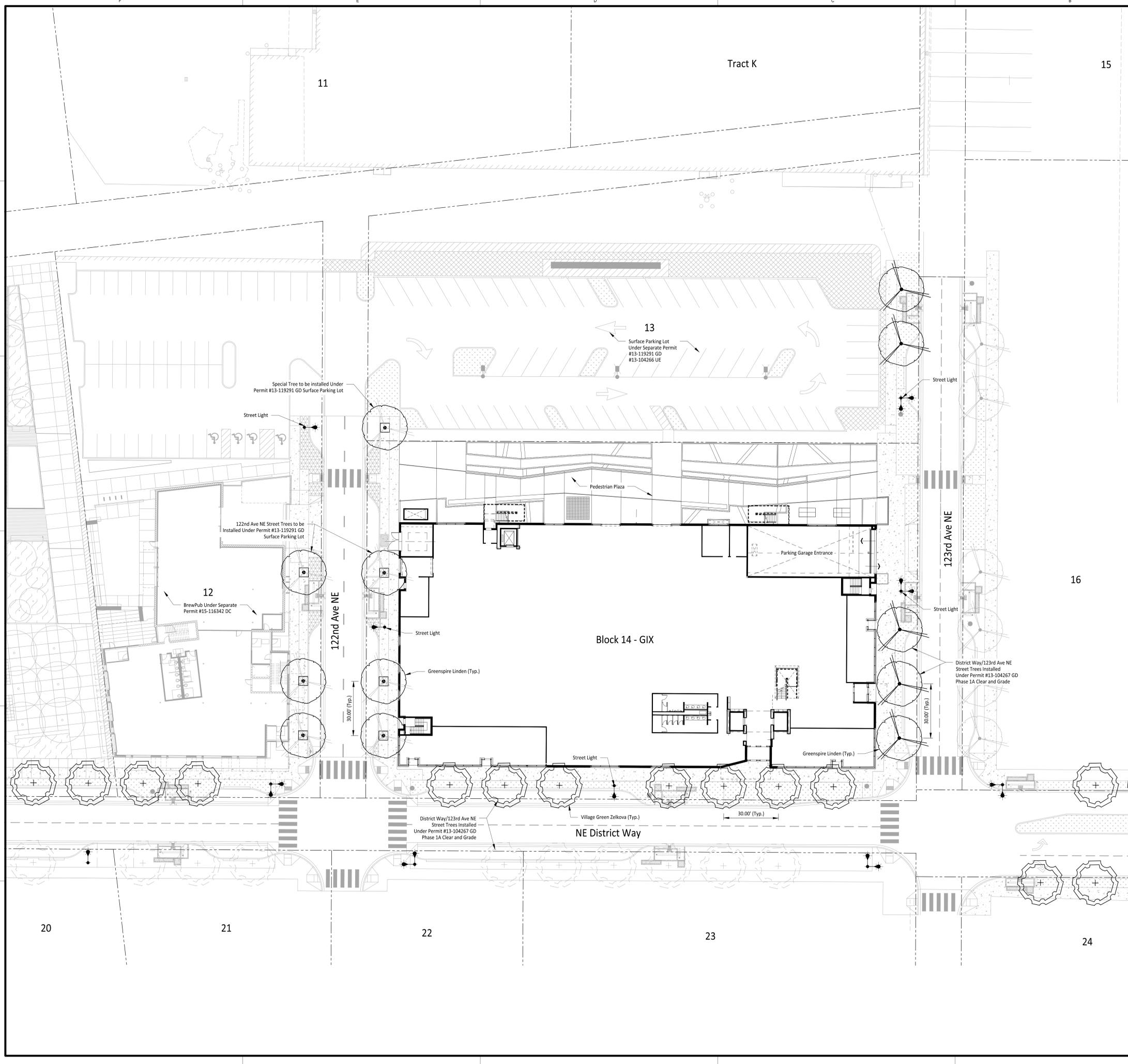
ISSUE

MARK	DATE	DESCRIPTION
3	4-8-2016	ADR REVISION 3

DRAWN BY: **GGN**
PROJ. ARCH: **GGN**
PROJ. NO.: **100908.11**
DATE: **4-8-2016**

DWG. **PLANTING PLAN**

DWG. **L161**



General Notes

- Parcel 12 BrewPub shown as Existing Condition, permitted under separate permits, to be constructed, and opened prior to completion of Proposed GIX Building.
- Parcel 13 Surface Parking Lot and 122nd Ave NE shown as Existing Condition, permitted under separate permits, to be constructed after Mass Excavation of Proposed GIX Building, and opened prior to completion of BrewPub/GIX Buildings.
- Site Conditions of BrewPub, Surface Parking Lot, and 122nd prior to construction is Concrete Slab from demolished Warehouse Building.

**WRIGHT
RUNSTAD
& COMPANY**

nbbj
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SEATTLE, WASHINGTON 98109
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FAX 206 223 5000

JVTEAM
PO BOX 2066
SUMNER, WASHINGTON 98390
PHONE 206 596 2020

AGENCY APPROVAL:

BLOCK 14 - GIX

**ADMINISTRATIVE
DESIGN
REVIEW**



ISSUE

MARK	DATE	DESCRIPTION
1	01-11-16	Design Review Revisions
2	04-07-16	Design Review Revisions

DRAWN BY: **D. Torrey** PROJ. ENG.: **J. Jones**
PROJ. NO.: **1500-001-010**
DATE: **2016/04/07**



Street Trees

DWG. **L700**

AGENCY APPROVAL:

BLOCK 14 - G1X

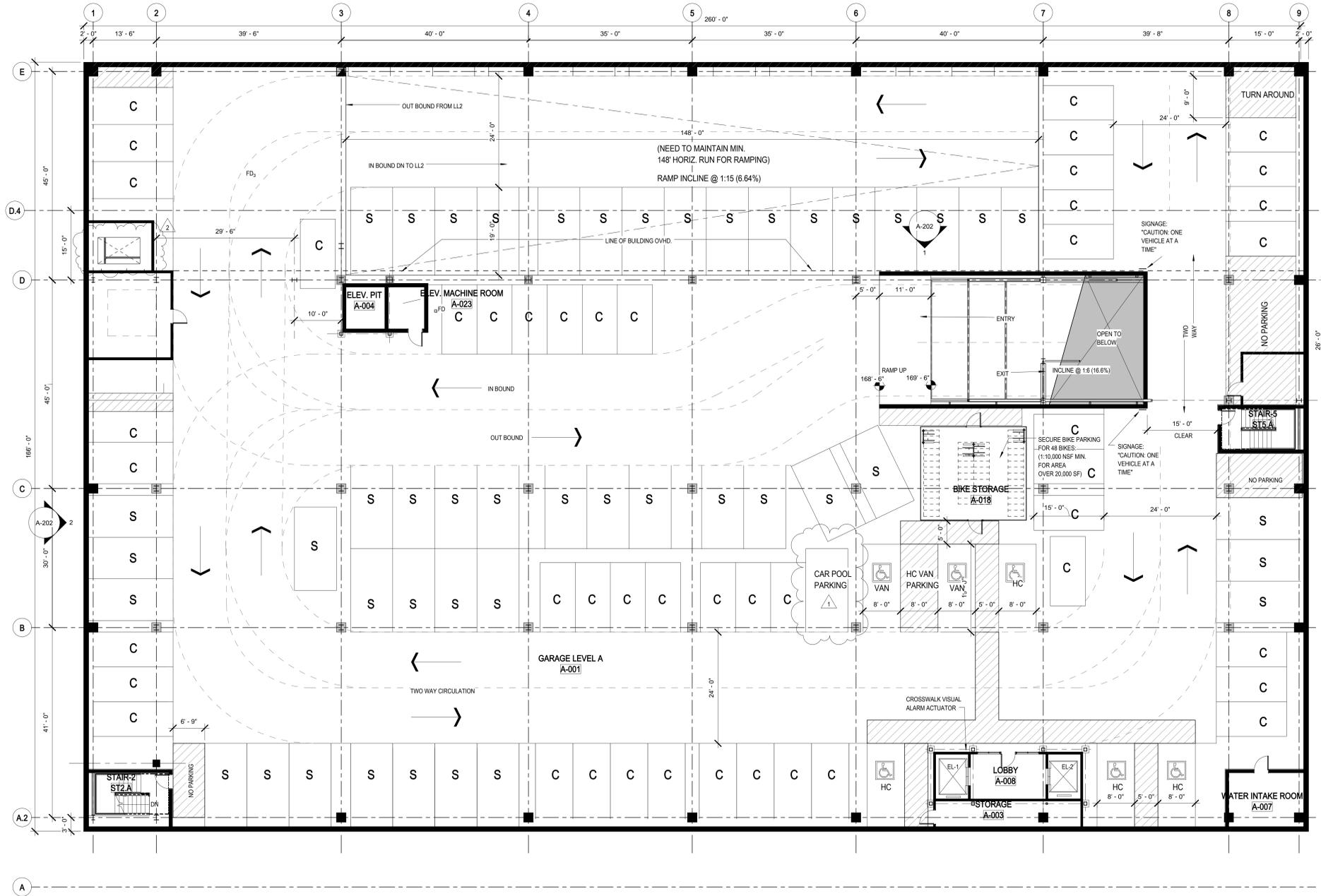
ADMINISTRATIVE
DESIGN REVIEW

ISSUE		
MARK	DATE	DESCRIPTION
2	03-11-2016	ADR REV 2
1	02/01/2016	ADR REV 1

DRAWN BY:	PROJ. ARCH:
NBBJ	NBBJ
PROJ. NO.:	100908.11
DATE:	03/11/2016

DWG.
GARAGE LEVEL A

DWG.
A-00A



A: 51 STANDARD 46 COMPACT
 B: 54 STANDARD 53 COMPACT
TOTAL: 105 STANDARD 99 COMPACT
 SECURE BIKE PARKING FOR MIN. 12 BIKES

2 HC VANS 3 HC STALLS
 2 HC VANS 4 HC STALLS
 2 HC VANS 7 HC STALLS

① GARAGE LEVEL A ADR
 3/32" = 1'-0"



AGENCY APPROVAL:

BLOCK 14 - G1X

ADMINISTRATIVE
DESIGN REVIEW

ISSUE

MARK	DATE	DESCRIPTION
2	03/11/2016	ADR REV 2
1	02/01/2016	ADR REV 1

DRAWN BY: **NBBJ** PROJ. ARCH: **NBBJ**

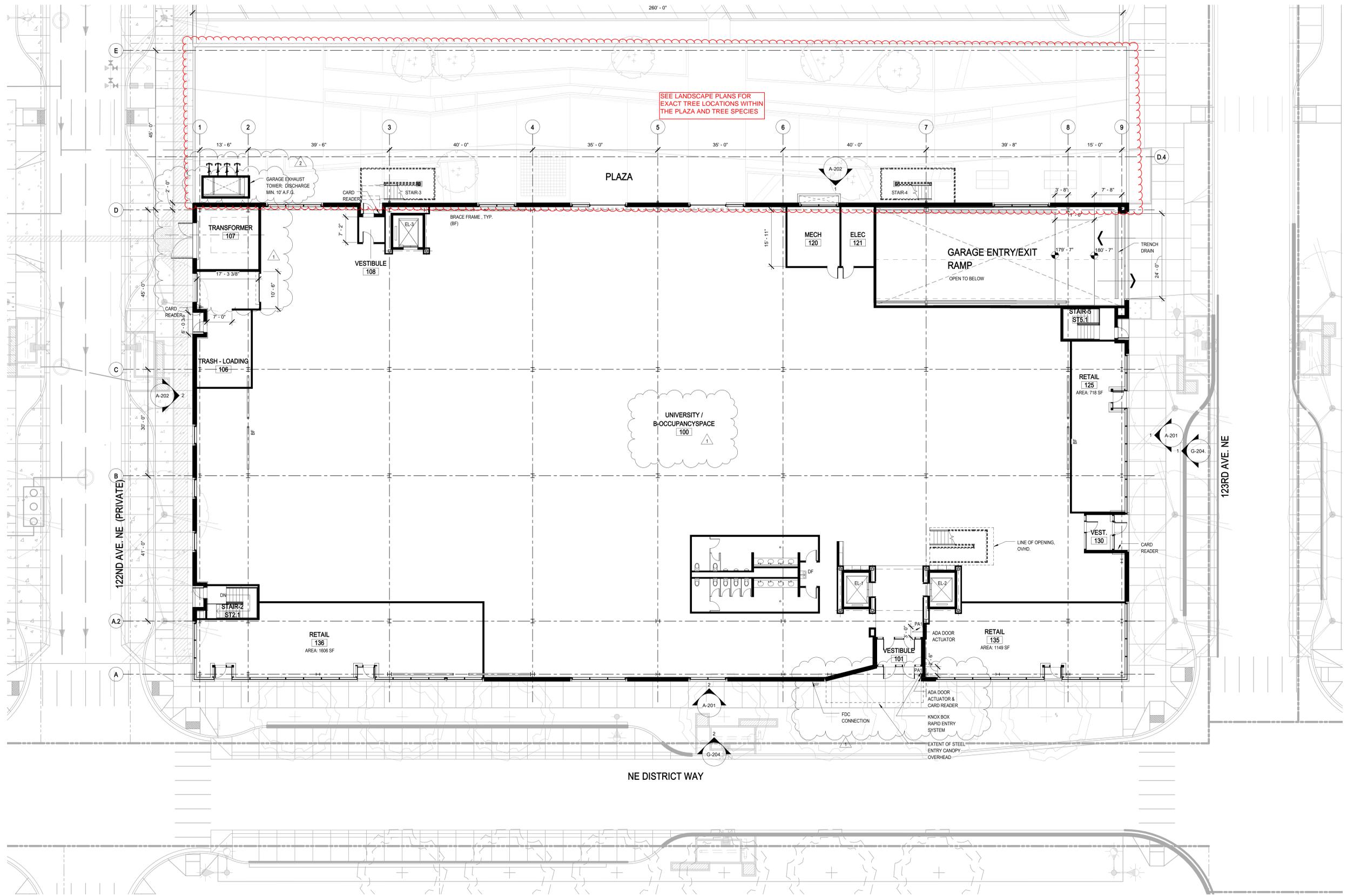
PROJ. NO.: **100908.11**

DATE: **03/11/2016**

DWG. **FIRST FLOOR PLAN**

DWG.

A-101



1 FIRST FLOOR PLAN
3/32" = 1'-0"



AGENCY APPROVAL:

BLOCK 14 - G1X

ADMINISTRATIVE
DESIGN REVIEW

ISSUE

MARK	DATE	DESCRIPTION
2	03-11-2016	ADR REV 2
1	02/01/2016	ADR REV 1

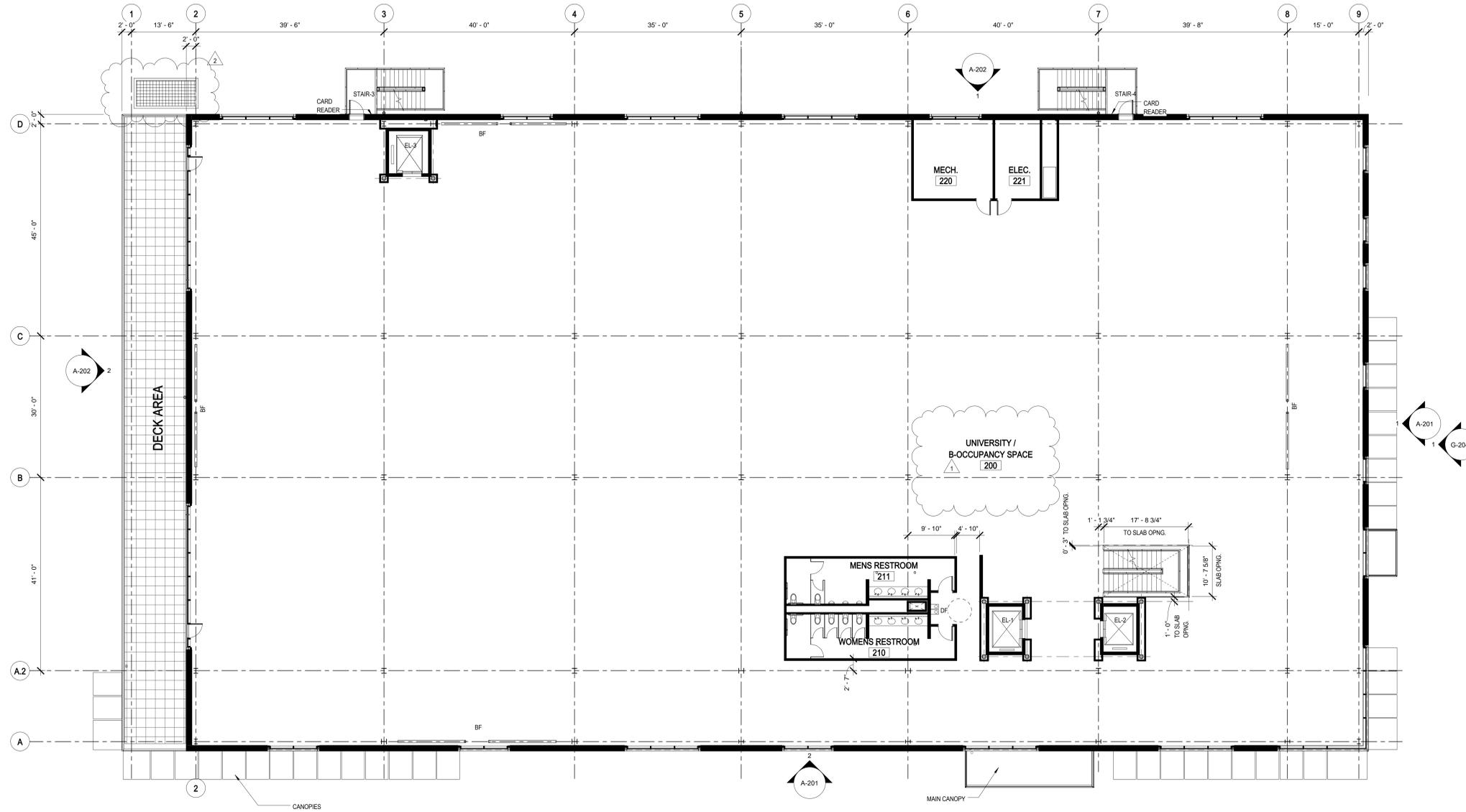
DRAWN BY: **NBBJ** PROJ. ARCH: **NBBJ**

PROJ. NO.: **100908.11**

DATE: **03/11/2016**

DWG.
**SECOND FLOOR
PLAN**

DWG.
A-102



1 LEVEL 2 PLAN ADR
3/32" = 1'-0"



COLOR AND MATERIAL LEGEND



AGENCY APPROVAL:

BLOCK 14 - G1X

ADMINISTRATIVE
DESIGN REVIEW

ISSUE

MARK	DATE	DESCRIPTION

1 02/01/2016 ADR REV 1

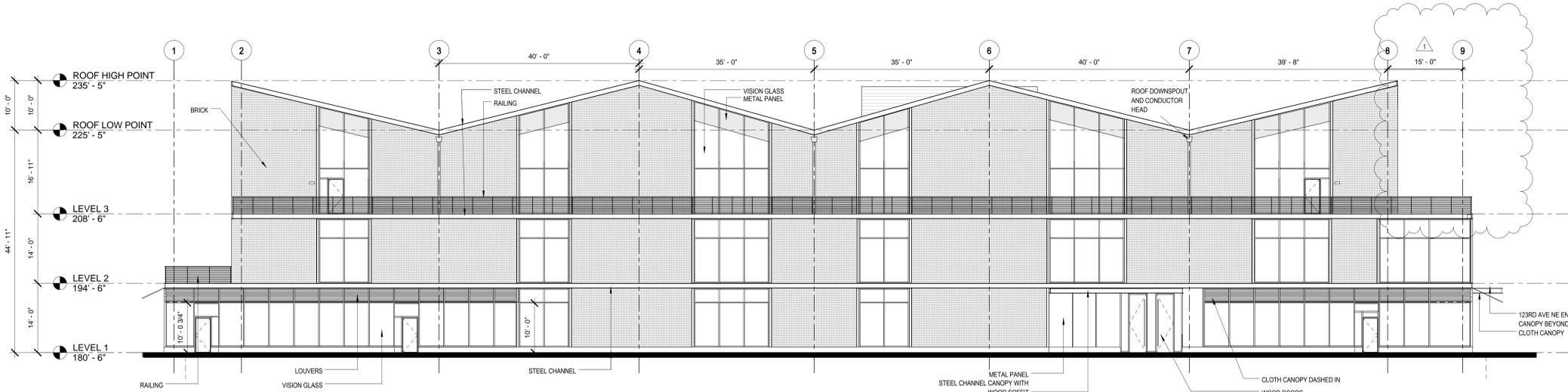
DRAWN BY: **NBBJ** PROJ. ARCH: **NBBJ**

PROJ. NO.: **100908.11**

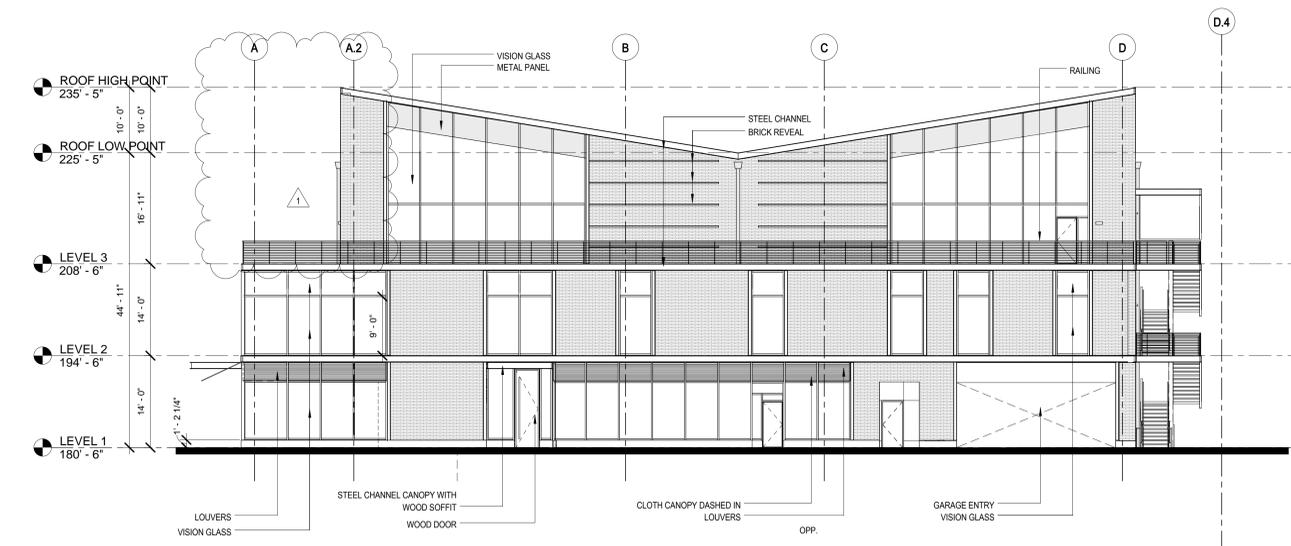
DATE: **02/01/2016**

DWG. **EXTERIOR ELEVATIONS**

DWG. **A-201**



2 BUILDING ELEVATION - SOUTH
3/32" = 1'-0"



1 BUILDING ELEVATION - EAST
3/32" = 1'-0"

GENERAL SHEET NOTES

1. DOOR HARDWARE AND KNOX BOXES WILL BE PROVIDED PER APPLICABLE CODE REQUIREMENTS.

AGENCY APPROVAL:

BLOCK 14 - G1X

**ADMINISTRATIVE
DESIGN REVIEW**

ISSUE

MARK	DATE	DESCRIPTION
2	03-11-2016	ADR REV 2
1	02/01/2016	ADR REV 1

DRAWN BY: **NBBJ** | PROJ. ARCH: **NBBJ**

PROJ. NO.: **100908.11**

DATE: **03/11/2016**

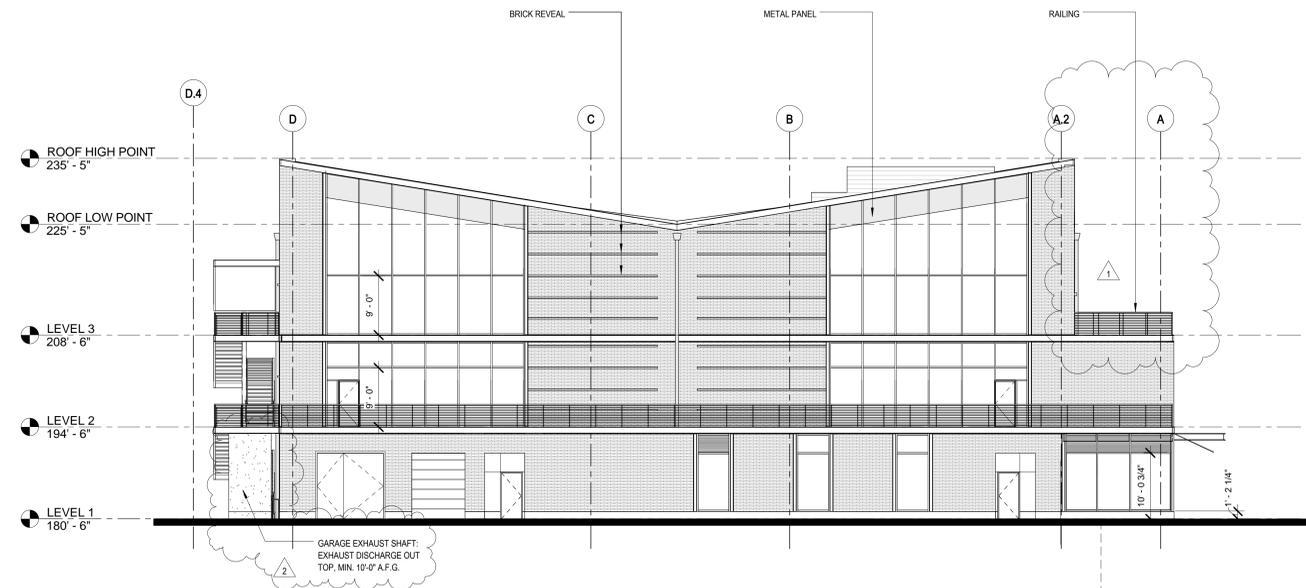
DWG. **EXTERIOR ELEVATIONS**

DWG. **A-202**

COLOR AND MATERIAL LEGEND



1 BUILDING ELEVATION - NORTH
3/32" = 1'-0"



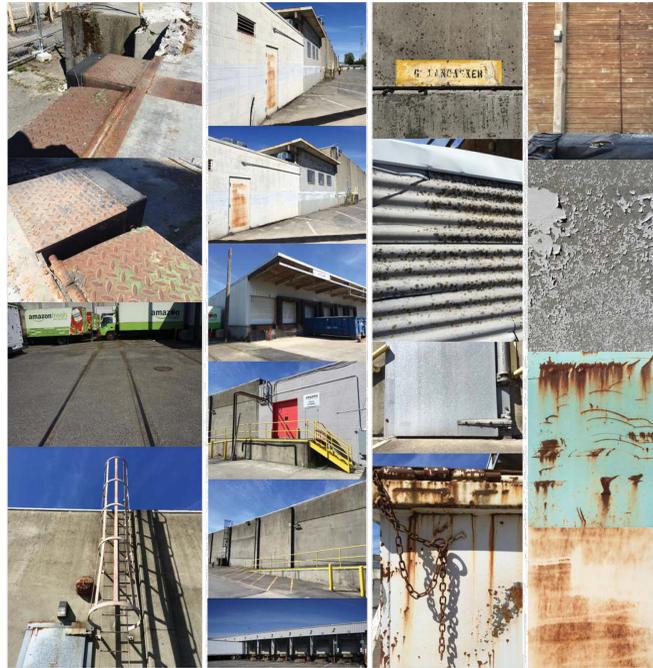
2 BUILDING ELEVATION - WEST
3/32" = 1'-0"

GENERAL SHEET NOTES

1. DOOR HARDWARE AND KNOX BOXES WILL BE PROVIDED PER APPLICABLE CODE REQUIREMENTS.

Concrete Wall FormLiner

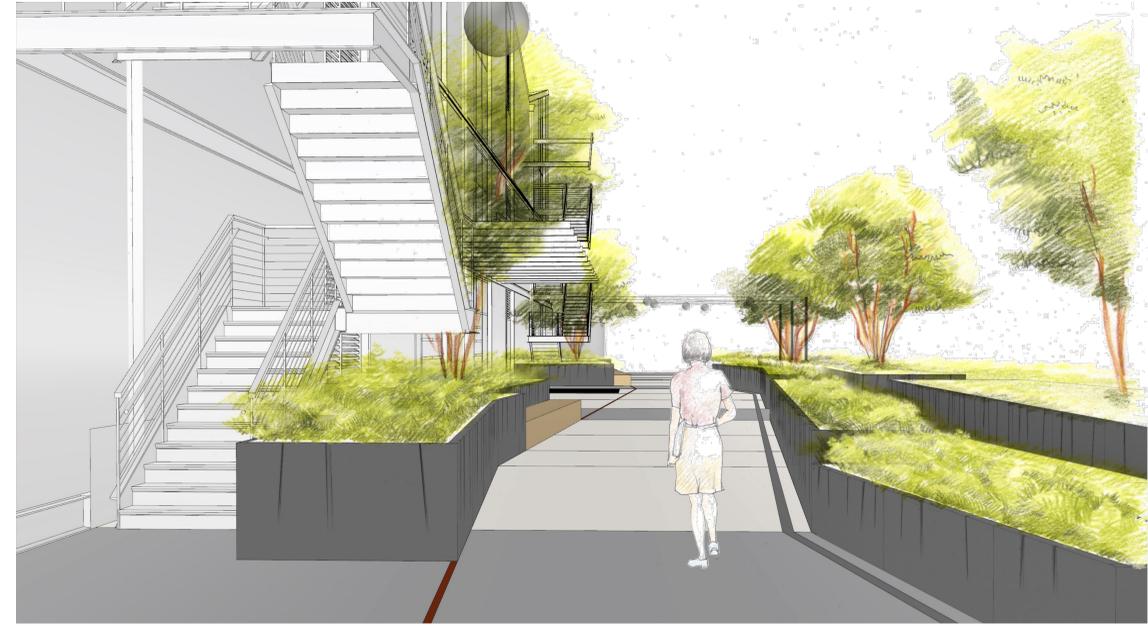
Salvaged/Found objects



GIX Plaza
GUSTAFSON GUTHRIE NICHOL

February 11, 2016

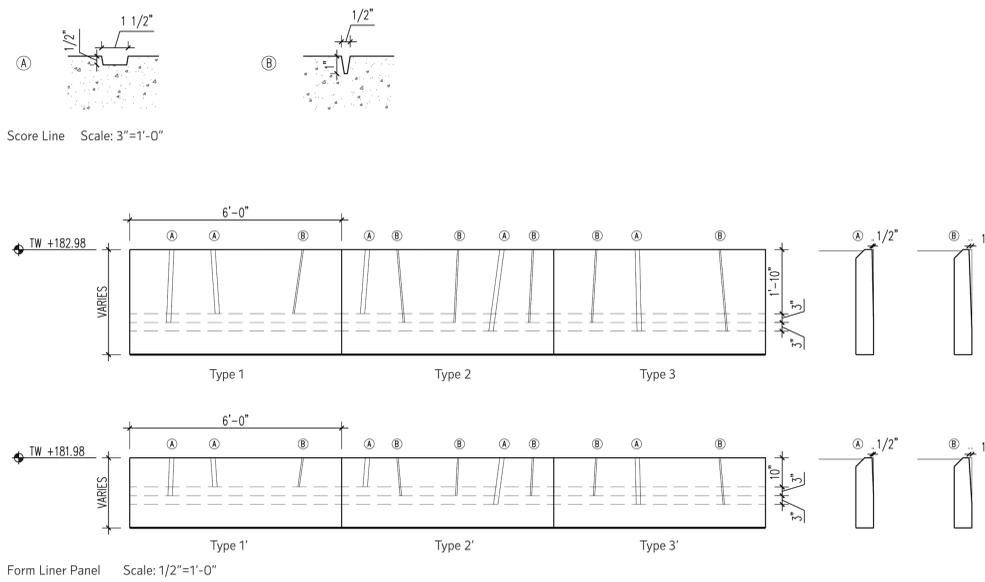
View 1



GIX Plaza
GUSTAFSON GUTHRIE NICHOL

February 11, 2016

Formliner Details



GIX Plaza
GUSTAFSON GUTHRIE NICHOL

February 11, 2016

View 2



GIX Plaza
GUSTAFSON GUTHRIE NICHOL

February 11, 2016

AGENCY APPROVAL:

BLOCK 14 - GIX

ADMINISTRATIVE
DESIGN
REVIEW



ISSUE

MARK	DATE	DESCRIPTION
1	01-11-16	Design Review Revisions
2	04-07-16	Design Review Revisions

DRAWN BY: D. Torrey

PROJ. ENG.: J. Jones

PROJ. NO.: 1500-001-010

DATE: 2016/04/07

DWG.

Exterior Details

A-203

View 3

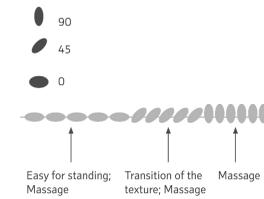


GIX Plaza
GUSTAFSON GUTHRIE NICHOL

February 11, 2016

Option 01 Continuity and Textual Transition

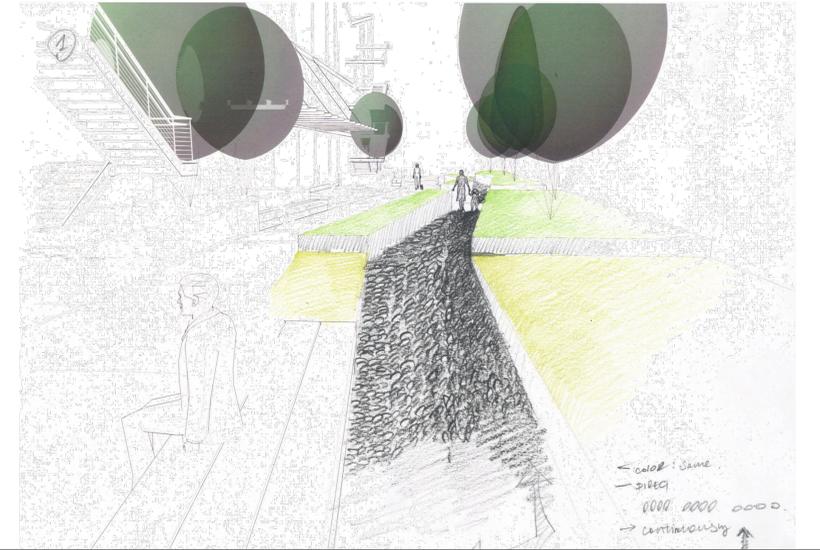
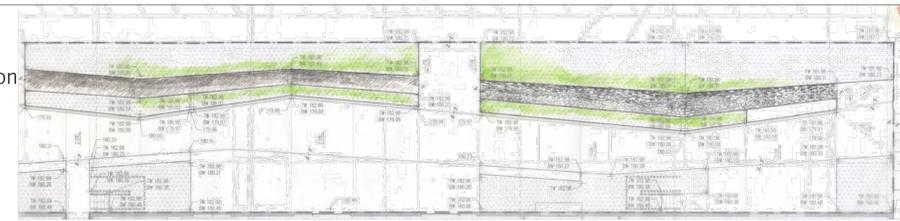
Size: Same
Color: Same
Layout: Transitional



Reference



GIX Plaza
GUSTAFSON GUTHRIE NICHOL



OPTION 1 textured concrete



WRIGHT RUNSTAD & COMPANY nbbj

THE SPRING DISTRICT BLOCK 13 + 14
03.03.2016

WRIGHT RUNSTAD & COMPANY

nbbj
223 YALE AVENUE NORTH
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PHONE 206 223 5555
FAX 206 223 5000

JMTEAM
PO BOX 2066
SUMNER, WASHINGTON 98390
PHONE 206 596 2020

AGENCY APPROVAL:

BLOCK 14 - GIX

ADMINISTRATIVE DESIGN REVIEW



ISSUE

MARK	DATE	DESCRIPTION
1	01-11-16	Design Review Revisions
2	04-07-16	Design Review Revisions

DRAWN BY: D. Torrey PROJ. ENG.: J. Jones

PROJ. NO.: 1500-001-010

DATE: 2016/04/07

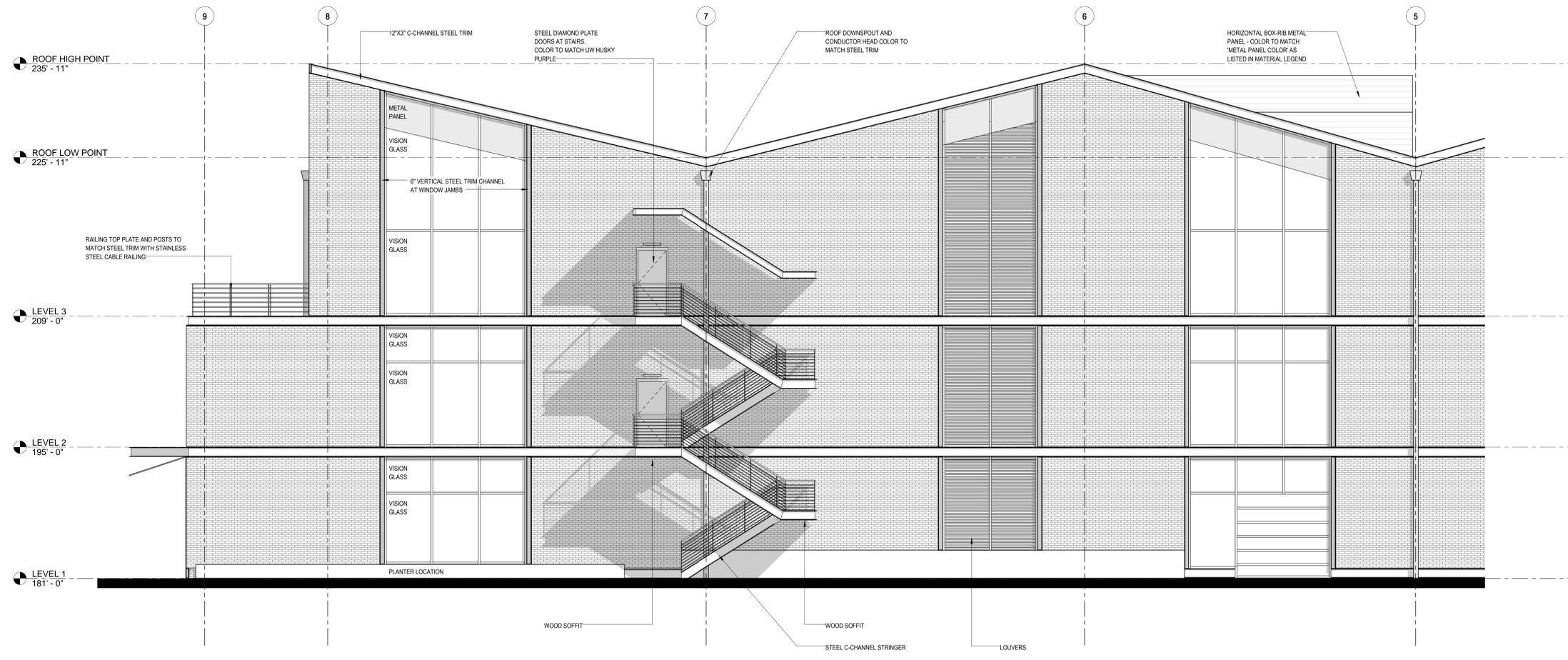
DWG.

Exterior Details

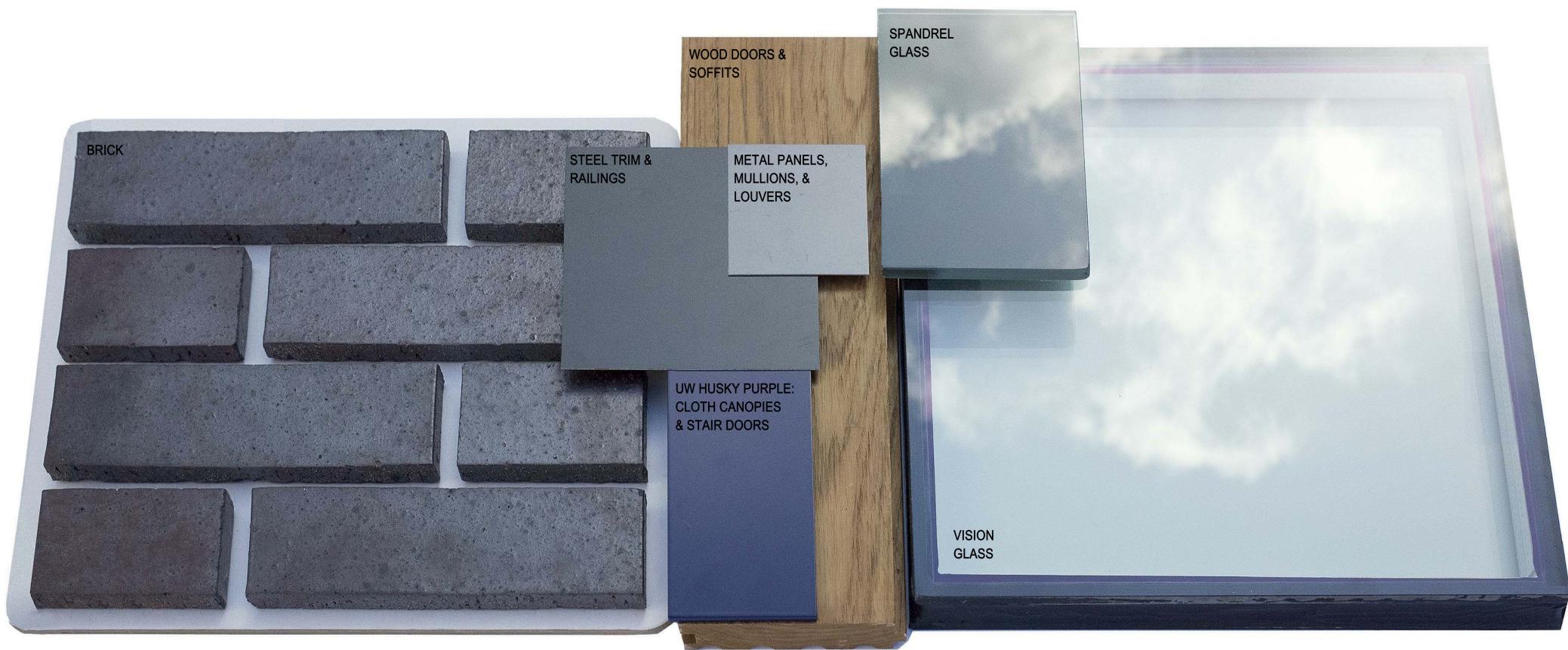
DWG.
A-204



10/29/2015 7:12:57 AM
 C:\Users\palderson\Documents\RevitLocalFiles\nbbj_SPRING-DISTRICT_BLOCK13and14_CENTRAL_20151023_palderson.RVT



ENLARGED NORTH ELEVATION
 3/16" = 1'-0"



COLOR AND MATERIAL LEGEND

- BRICK: NORMAN RUNNING BOND. COLOR: DARK IRON SPOT.
- METAL PANEL COLOR: DURANAR SUNSTORM SILVER GRAY
- LOUVERS & MULLIONS: DURANAR SUNSTORM SILVER GRAY
- STEEL TRIM: DURACON S600 BLACK ONYX
- VISION GLASS: VIRACON VE1-2M
- SPANDREL GLASS: VIRACON VE1-2M WITH VIRASPRAN SUBDUED GRAY
- CLOTH CANOPY COLOR: UW HUSKY PURPLE
- WOOD CANOPIES & DOORS COLOR: [WOOD SAMPLE]
- CABLE RAILING COLOR: [METAL SAMPLE]
- GARAGE DOOR: HORIZONTAL MESH DURACON S600 BLACK ONYX
- CONCRETE: [CONCRETE SAMPLE]

WRIGHT RUNSTAD & COMPANY

nbbj

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 SEATTLE, WASHINGTON 98109
 PHONE 206 223 5555
 FAX 206 223 5000

AGENCY APPROVAL:

BLOCK 14 - G1X

ADMINISTRATIVE DESIGN REVIEW

ISSUE

MARK	DATE	DESCRIPTION

DRAWN BY:	PROJ. ARCH:
NBBJ	NBBJ
PROJ. NO.:	100908.11
DATE:	10/29/2015

DWG. ENLARGED ELEVATION & EXTERIOR MATERIALS

A-410

A. Background [\[help\]](#)

1. Name of proposed project, if applicable: [\[help\]](#)
Global Innovation Exchange (GIX) Building, on Parcel 14 of The Spring District
2. Name of applicant: [\[help\]](#)
Wright Runstad & Company
3. Address and phone number of applicant and contact person: [\[help\]](#)
Tara Howard, Wright Runstad & Company, 1201 Third Avenue, Suite 2700, Seattle, WA 98101, (206) 447-9000
4. Date checklist prepared: [\[help\]](#)
October 29, 2015
5. Agency requesting checklist: [\[help\]](#)
City of Bellevue
6. Proposed timing or schedule (including phasing, if applicable): [\[help\]](#)
Construction of the proposal is expected to begin in spring 2016 with completion in fall 2017.

Subject to change.

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. [\[help\]](#) **Phase 1B per most recent MDP Amendment**
The proposal is within ~~Phase 1A~~ of the Spring District. Future development connected to this proposal includes the full build-out of the Spring District, in accordance with the Master Development Plan Approval with Conditions (May 3, 2012) and appeal approval affirmed on October 8, 2012 by the Hearings Examiner.

Future development will be completed in phases, generally moving from the south to north side of the Spring District property. These anticipated phases are generally described next.

- Phase 1A - this project area includes the southern 14 acres of the Spring District. The site infrastructure to serve this first phase is generally complete, as well as a park. A residential development by others on Parcels 18-21 is currently under construction and office and retail development is permitted on Parcel 16. In addition, an office building and brewpub on Parcel 12 is under Design Review.
- Phase 2 includes City roadway improvements, the arrival of the Sound Transit Light Rail Transit (LRT) station, and the permanent placement of the active and passive park spaces;
- Phase 3 includes additional office and retail space;
- Phase 4 adds a landmark hotel that will provide an additional entry to the LRT station;
- Phase 5 adds development north of NE 16th Street including residential and office/retail space;
- Phase 6 adds the final office building and residential complex on the north side of the property.

Recent MDP amendment #15-129285-LJ has modified phasing of Phase 1

**LT
5/12/16**

Each phase of development will go through Design Review with the City of Bellevue and is subject to applicable regulations and policies in effect at the time of application.

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal. [\[help\]](#)

An FEIS for the BelRed Corridor Project was issued by the City of Bellevue in July of 2007. The FEIS designates a Preferred Alternative, identified by the BelRed Steering Committee in May 2007, which would increase density in the western half of the BelRed Corridor by including three closely spaced development nodes in the vicinity of Overlake Hospital Medical Center (OHMC), 122nd, and 130th Avenues NE.

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. [\[help\]](#)

No known applications.

**Yes - #13-104267-GD - Master Site Infrastructure permit for MDP Phase 1A
#13-119291-GD - Parking lot on Parcel 13 and 122nd Avenue NE construction**

10. List any government approvals or permits that will be needed for your proposal, if known. [\[help\]](#)

In addition to the Administration Design Review in accordance with the Master Development Plan, the development will require local permits, including Clearing & Grading, Utilities, and Building Permits. The site is within the project area of Phase 1A, which is covered under a Washington State Department of Ecology NPDES Permit.

**Phase 1A for
infrastructure**

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.)

[\[help\]](#)

The proposal includes the construction of the Global Innovation Exchange (GIX) building on Parcel 14 of the Spring District. The building will contain approximately ~~66,000~~ square feet (SF) of university and approximately ~~3,400~~ SF of retail space located on the ground floor. In addition, there will be approximately 5,500 square feet of deck spaces on the exterior of the building.

Parking will be accommodated in two levels of underground garage with approximately ~~200~~ spaces, with an entry/exit from 123rd Avenue NE. Loading/unloading, refuse collection and service will access from 122nd Avenue NE.

210 stalls

**90,390 GSF
office/university/parking**

3,473 GSF retail

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist. [\[help\]](#)

The Spring District, Bellevue, King County, WA. 1209 124th Avenue NE,

**LT
5/12/16**

located to the North of NE 12th Street, East of 120th Avenue NE, and West of 124th Avenue NE. King County Parcel number 7933300040, Unit 14 of the Spring District.

B. Environmental Elements [\[help\]](#)

1. Earth [\[help\]](#)

- a. General description of the site: [\[help\]](#) (select one): Flat, rolling, hilly, steep slopes, mountainous, other: *Click here to enter text.*
- b. What is the steepest slope on the site (approximate percent slope)? [\[help\]](#)
The proposal area is flat and consists of a concrete slab, which was the floor of a warehouse on the site for approximately 50 years.
- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils. [\[help\]](#)
The land has been developed since the late 1950's and does not include any prime farmland. A geotechnical engineering report dated (Hart Crowser, 2015) confirms the likelihood of up to five feet of fill under the existing concrete floor slab. The fill is believed to be very dense glacial soils typically consisting of gravelly to very gravelly, silty to very silty sand.
- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. [\[help\]](#)
There are no indications of or history of unstable soils in the immediate vicinity.
- e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill. [\[help\]](#)
Proposed earthwork includes the excavation of approximately 55,000 CY of material for construction of the underground parking garage. The excavated material will be disposed of off-site at a proper disposal site.
- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe. [\[help\]](#)
As with all construction activities, there is the possibility of erosion associated with the clearing and construction of the proposal site. The excavation and grading of the proposal area has the potential to cause erosion if construction stormwater were not properly managed.
- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)? [\[help\]](#)
Currently, the proposal site is 100-percent impervious as it is covered by a concrete slab from the warehouse that was demolished in 2013.

See
Geotech
Report
prepared by
Hart
Crowser
dated
10/27/15

LT
5/12/16

87 %

After construction of the Parcel 14 GIX Building, the proposal site will be approximately ~~88~~-percent impervious. Per the BelRed code and Master Development Plan Conditions of Approval, the Spring District site cannot exceed 75-percent impervious lot coverage site-wide. See the accompanying Impervious Lot Coverage Memo (JM TEAM, 2015) for an updated ~~Phase 11~~ impervious lot coverage calculation.

For approved/in construction projects

h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any: [\[help\]](#)

The project proponent will prepare and implement a construction stormwater pollution prevention plan (CSWPPP) per Washington State Department of Ecology requirements and a Temporary Erosion and Sediment Control (TESC) per Bellevue City Code 23.76.

Project
subject to
clear and
grade code
BCC 23.76

The plans will identify Best Management Practices (BMPs) to minimize stormwater flows, prevent soil erosion, capture water-borne sediment from exposed soils, and protect water quality from on-site pollutant sources. These BMPs include an erosion control plan prepared in accordance with City of Bellevue standards and the Stormwater Management Manual for Western Washington. The City of Bellevue Storm and Surface Water Engineering Standards provides guidance to prevent erosion downstream of construction sites. In accordance with the City's NPDES permit, a Certified Erosion Control Lead (CERCL) will be on-site during construction.

Some measures that may be implemented during construction to manage source control and runoff conveyance and treatment include: road/parking area stabilization, wheel wash, dust control, concrete handling, construction timing, erosion control fencing, outlet protection, silt fencing, sediment traps, and construction stormwater chemical treatment. Additional devices and methods may be employed to ensure the erosion potential is minimized.

2. Air [\[help\]](#)

a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known. [\[help\]](#)

During construction, emissions to the air will be released by construction vehicles and heavy equipment. Following construction, emissions from vehicle traffic within the development will be released. Construction would temporarily increase dust and vehicle emissions near the construction area. Mitigation would include using BMPs to control dust, covering exposed soils, and requiring idling vehicles to be shut off.

The BelRed Corridor FEIS (2007) predicts that as a result of increased traffic in the study area (BelRed), carbon monoxide emissions would increase by about 40 percent over the No-Action Alternative, and emissions of particulates would increase by about 30 percent. It also states these emissions are not expected to violate air quality standards. Washington State Department of Ecology (Ecology) has jurisdiction over air quality. This proposal does not trigger the need for a quantitative analysis, as the emissions are below the 25,000 MTCO₂d threshold established by Ecology. However, a qualitative analysis, including mitigation options, is

LT
5/12/16

included in an accompanying Technical Memorandum (JMJ TEAM 2015).

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. [\[help\]](#)

There are no known off-site sources of emissions or odor that would affect this proposal.

- c. Proposed measures to reduce or control emissions or other impacts to air, if any: [\[help\]](#)

The City of Bellevue imposes standard practices as part of its Clearing and Grading permit (Bellevue City Code 23.76). Mitigation will include using BMPs to control dust and vehicle emissions near the construction area. Construction vehicles will be fitted with required, factory-installed emission control devices. To reduce the potential of dust, construction accesses will be covered with rock or aggregate. Dust emissions will also be reduced during construction through the use of spray water as necessary during dry weather conditions and planting disturbed areas with erosion control seed mix as soon as is practical. Material stockpiles will also be covered or watered as necessary to control dust.

The Bel-Red Corridor FEIS states that despite the predicted increase in traffic volumes and emissions, the Bel-Red Corridor redevelopment is not likely to result in any exceedance of the air quality standards. Maintaining traffic flow will reduce vehicles idling and, therefore, reduce pollutant emissions from vehicles.

As described in the Greenhouse Gas Emissions Memorandum, Buildings 16 and 24 will be constructed using adaptive building reuse, sustainably grown and regionally produced projects, and high-performance systems. By selecting durable and less energy consuming building components, the applicant has a proven history of building sustainable, 100-year lifespan structures.

Construction vehicles will be fitted with required, factory-installed emission control devices. To reduce the potential of dust, construction accesses will be covered with rock or aggregate. Dust emissions will also be reduced during construction through the use of spray water as necessary during dry weather conditions and planting disturbed areas with erosion control seed mix as soon as is practical. Material stockpiles will also be covered or watered as necessary to control dust.

3. Water [\[help\]](#)

a. Surface Water:

- 1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into. [\[help\]](#)

The proposal area is more than 400 feet northeast of Lake Bellevue. Lake Bellevue is the receiving water of stormwater runoff from the proposal site. The proposal site is not a major contributor of flow to the lake.

LT
5/12/16

There is a Category III wetland (0.19-acre) located outside of the project area and along 120th Avenue NE. This wetland is entirely within the city's 120th Avenue NE widening project, which is beginning construction in early 2016. The widening project will take the wetland and mitigate for the wetland off-side as part of the city's project. The proposal will not affect this wetland.

Kelsey Creek is located approximately 300 feet northeast of the proposal. The proposal will not affect Kelsey Creek.

2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans. [\[help\]](#)

The proposal will not require work over, in or adjacent to any waters.

3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material. [\[help\]](#)

The proposal will not include fill or dredge materials placed or removed from surface waters or wetlands.

4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known. [\[help\]](#)

The proposal will not require surface water withdrawals or diversions.

5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan. [\[help\]](#)

According to FEMA Flood Insurance Rate Maps, Community Panel numbers 53033C0368F and 53033C0656F (eff. May 16, 1995), the affected geographic area is not within the 100-year floodplain.

6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge. [\[help\]](#)

No waste materials will be discharged to surface waters. Stormwater from pollution-generating surfaces will be collected and treated before being conveyed through approved systems that eventually discharge to Lake Bellevue.

b. Ground Water:

1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known. [\[help\]](#)

This proposal does not involve withdrawals of or discharges to groundwater.

2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals...; agricultural; etc.). Describe the general size of the system, the

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number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve. [\[help\]](#)
This Proposal does not include the discharge of waste materials into the ground from septic tanks or other sources. The proposal will be served by the City of Bellevue's public sewer system.

**Project is subject to
Utility Code BCC 24.06
and any required utility
permits.**

c. Water runoff (including stormwater):

- 1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow?

Will this water flow into other waters? If so, describe. [\[help\]](#)

In compliance with the Washington Department of Ecology Stormwater Management Manual for Western Washington, the proposal is required to provide enhanced stormwater treatment of pollution-generating surfaces. Surface runoff from the parking garage entrance will be collected and treated in a bioretention cell located along 123rd Avenue NE. After treatment, the stormwater will be discharged through an underdrain and leave the site through the stormwater conveyance system. The system will connect to the existing storm drainage system at 120th Avenue NE where it is conveyed to Lake Bellevue.

Non-pollution generating surfaces, including the pedestrian plaza and roof downspouts, will be conveyed directly to the storm drainage system and are not required to provide treatment.

- 2) Could waste materials enter ground or surface waters? If so, generally describe. [\[help\]](#)

It is not anticipated that waste materials will enter ground or surface waters associated with this proposal. As with all projects, there is a possibility of waste materials entering ground or surface waters during construction.

- 3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe. [\[help\]](#)

Stormwater will continue to be discharged to Lake Bellevue. Stormwater from the pollution-generating surface, the parking garage driveway, will be directly to the bioretention cell for treatment before being discharged through the system to Lake Bellevue.

d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any: [\[help\]](#)

The proposal will comply with all applicable requirements of the Drainage Design & Erosion Control Manual and applicable stormwater manual. During construction, contractors will be required to have a Spill Prevention Control and Countermeasure Plans and a Stormwater Pollution Prevention Plan (SWPPP) in place.

The proposal is implementing enhanced stormwater treatment through the use of a bioretention cell for treating stormwater runoff from the driveway entrance off 123rd Avenue NE, which is a pollution-generating surface.

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The proposal is within the Lake Bellevue Stormwater Sub-basin. As stormwater from several sites enters Lake Bellevue, sources of phosphorus levels in the lake were investigated and documented in the Lake Bellevue Water Quality Study and Management Recommendations Report (December 2006). A technical memorandum dated May 23, 2012 and included in the ADR submittal for Phase 1A includes details of this report. The report finds that sources of phosphorous in Lake Bellevue are mostly attributed to internal sources (76%). The Spring District accounts for 26% of the total stormwater runoff within the Lake Bellevue Sub-basin. While the proposal is not a major contributor of flow to Lake Bellevue, it is important to note that the proposal will not re-direct stormwater flows away from Lake Bellevue. The stormwater flow will maintain its historic pattern of entering the lake. The proposal's construction of low impact development techniques will reduce the peak stormwater flow rates to Lake Bellevue by slowing the rate it reaches the lake while not reducing overall flow volumes to the lake.

4. Plants [\[help\]](#)

- a. Check the types of vegetation found on the site: [\[help\]](#)

deciduous tree: alder, maple, aspen, other: *Click here to enter text.*

evergreen tree: fir, cedar, pine, other: *Click here to enter text.*

shrubs

grass

pasture

crop or grain

Orchards, vineyards or other permanent crops.

wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other: *Click here to enter text.*

water plants: water lily, eelgrass, milfoil, other: *Click here to enter text.*

other types of vegetation: *Click here to enter text.*

None - covered 100% with concrete foundation from previously existing structure

- b. What kind and amount of vegetation will be removed or altered? [\[help\]](#)

The proposal does not require the removal of any vegetation.

- c. List threatened and endangered species known to be on or near the site. [\[help\]](#)

There are no threatened or endangered species known to occur on or near the site.

- d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any: [\[help\]](#)

The proposal landscaping will include planter strips as well as bioretention cells to provide stormwater treatment of pollution-generating surfaces.

Streetscape landscaping will meet requirements of LUC 20.25D.110

- e. List all noxious weeds and invasive species known to be on or near the site. [\[help\]](#)

There are no known noxious weeds or invasive species on or near the site.

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5. Animals [\[help\]](#)

- a. List any birds and other animals which have been observed on or near the site or are known to be on or near the site. [\[help\]](#)

Examples include:

birds: hawk, heron, eagle, songbirds, other: *Click here to enter text.*

mammals: deer, bear, elk, beaver, other: *Click here to enter text.*

fish: bass, salmon, trout, herring, shellfish, other: *Click here to enter text.*

- b. List any threatened and endangered species known to be on or near the site. [\[help\]](#)
There are no threatened or endangered species known to occur on or near the site.
- c. Is the site part of a migration route? If so, explain. [\[help\]](#)
Yes, however, most of Western Washington is generally located in the Pacific Flyway for migratory waterfowl.
- d. Proposed measures to preserve or enhance wildlife, if any: [\[help\]](#)
As there is no known wildlife on the site, no preservation measures are needed.
- e. List any invasive animal species known to be on or near the site. [\[help\]](#)
None known.

6. Energy and Natural Resources [\[help\]](#)

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc. [\[help\]](#)
The GIX Building will require electricity and natural gas energy for heating/cooling associated with office and retail use.
- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. [\[help\]](#)
The proposal will not affect the potential use of solar energy by adjacent properties. The proposal will not produce shadows to the north nor shade other adjacent properties.
- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any: [\[help\]](#)
The building will have several low impact development features and will seek Leadership in Energy and Environmental Design (LEED) certification. LED features may include LED lighting, sustainable or renewable materials, and the purchase of local building materials to

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limit truck transit.

7. Environmental Health [\[help\]](#)

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe. [\[help\]](#)

There are no anticipated environmental health hazards associated with this proposal. The project site is entirely covered by a former warehouse's floor slab. Prior to the warehouse's construction in the 1950's, the site was likely agricultural or undeveloped. As with all sites, there may be a risk of spills during construction.

- 1) Describe any known or possible contamination at the site from present or past uses. [\[help\]](#)

There is no known contaminants within the project site. The site is currently and has been covered by a concrete slab since the former warehouse was constructed in the 1950's.

- 2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity. [\[help\]](#)

There are no known hazardous chemicals or underground hazards or transmission pipelines within the proposal site. The site is covered by the concrete slab of the former warehouse. Within the Spring District, in vicinity of the project site, there is a truck shop where vehicle maintenance occurs.

- 3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project. [\[help\]](#)

There are no known toxic or hazardous chemicals involved in the construction or operation of the proposal.

- 4) Describe special emergency services that might be required. [\[help\]](#)

The need for special emergency services is not anticipated. The building use is limited to offices and retail uses. Facilities storing or processing toxic chemicals are not part of this proposal.

- 5) Proposed measures to reduce or control environmental health hazards, if any: [\[help\]](#)

Spill Prevention and Control Plans will be utilized by contractors working on-site during construction.

- b. Noise [\[help\]](#)

- 1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)? [\[help\]](#)

Noise from nearby roadways exists, including freeways I-405 and SR-520 and arterials 124th Avenue NE and NE 12th Street. Noise from these facilities and other surrounding uses is standard roadway noise and will

not affect the proposal.

- 2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)?

Indicate what hours noise would come from the site. [\[help\]](#)

Long-term noise associated with the proposal will be typical vehicle noise from office and retail uses. The BelRed Corridor FEIS states that long-term noise impacts from the BelRed Corridor would be similar to the No-Action Alternative (70 to 72 dBA) in areas proposed for residential development. During construction, the site will produce temporary construction noise.

- 3) Proposed measures to reduce or control noise impacts, if any: [\[help\]](#)

During construction, motorized construction equipment will be properly fitted with mufflers to reduce engine noise associated with short-term construction noise. For long-term noise control, the parking lot will have perimeter landscaping to buffer vehicle noise from adjacent properties, although vehicle noise is typical of any development. The building's usage omits typical noise levels associated with an office/retail building.

Construction noise will be limited to the City's Noise Ordinance BCC 9.18

8. Land and Shoreline Use [\[help\]](#)

- a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe. [\[help\]](#)

The development area currently contains a warehouse floor slab from the warehouse that was demolished in 2013. The Master Development Plan and Binding Site Plan recorded for the property allow for commercial use of the parcel. The site is zoned BelRed Office/Residential (O/R) per the BelRed zoning and code ordinance, 2009. Adjacent properties to the north include an operational distribution warehouse, which is also zoned O/R but is an existing use. The property to the west is under design review for an office and brewpub with associated parking. Across NE District Way to the south/southwest of Parcel 14 is residential development currently under construction. Parcel 16 sits across 123rd Avenue NE to the east is a commercial parcel with a permitted 300,000 SF (approx.) office building with ground-floor retail use.

- b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use? [\[help\]](#)

The site was likely used for agriculture prior to its development as a light industrial warehouse site in the early 1950's. The site has been used for warehouse distribution for the last 60+ years.

- 1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides,

tilling, and harvesting? If so, how: [\[help\]](#)

The proposal will not affect or be affected by nearby farms or forest land operations.

- c. Describe any structures on the site. [\[help\]](#)

There are no permanent structures currently on the site.

- d. Will any structures be demolished? If so, what? [\[help\]](#)

No structures will be demolished as part of this proposal.

- e. What is the current zoning classification of the site? [\[help\]](#)

In 2009, the city rezoned several sites within BelRed, including the entire Spring District property. The proposal site was rezoned from Light Industrial to Office/Residential.

BR-OR-1/BR-OR-2

- f. What is the current comprehensive plan designation of the site? [\[help\]](#)

The current comprehensive plan designation is mixed-use office/residential.

BR-OR-1/BR-OR-2

- g. If applicable, what is the current shoreline master program designation of the site? [\[help\]](#)

Not applicable.

- h. Has any part of the site been classified as a critical area by the city or county? If so, specify. [\[help\]](#)

No, there are no environmentally sensitive areas in the proposal area.

- i. Approximately how many people would reside or work in the completed project? [\[help\]](#)

There is no residence use associated with this proposal. It is unknown how many employees/students can be accommodated in the office space. The retail space will also have employees associated with their operations, however, the exact number of employees is not known at this time.

- j. Approximately how many people would the completed project displace? [\[help\]](#)

The proposal will not displace any residents or workers as the proposal site consists of a concrete slab.

- k. Proposed measures to avoid or reduce displacement impacts, if any: [\[help\]](#)

Not applicable.

- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: [\[help\]](#)

This Proposal is compatible with the City's existing comprehensive plan and the FEIS for the BelRed Corridor Project. Alignment with these plans ensures compatibility with existing and projected land use plans. Any future development that may be proposed within the BelRed Corridor and/or the affected geographic area would be reviewed for compliance

with existing regulations in place at the time of the application.

- m. Proposed measures to ensure the proposal is compatible with nearby agricultural and forest lands of long-term commercial significance, if any: [\[help\]](#)

Not applicable.

9. Housing [\[help\]](#)

- a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing. [\[help\]](#)

No residential housing will be constructed as part of this proposal.

- b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing. [\[help\]](#)

This Proposal will not eliminate any existing housing units as none are currently on-site.

- c. Proposed measures to reduce or control housing impacts, if any: [\[help\]](#)

The Proposal will not have an impact on existing housing units, and therefore no housing impact reduction or control is necessary.

10. Aesthetics [\[help\]](#)

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? [\[help\]](#)

The proposal includes a building with a folded roof, creating a varying building height between 55-foot-tall (at the highest point) and 45-foot tall (at the lowest point). The building will be a three-story structure with office and ground-floor retail uses. Per Bellevue Land Use Code 20.25D, buildings in the BR-OR-1 zoning are allowed to be up to 150 feet tall.

The building design includes a folded roof profile, and vertical expanses of glass to evoke an urban character reminiscent of more vernacular warehouse structures, but with a strong modern aesthetic. Strong, active retail uses at the ground floor and a landscaped plaza at the north side of the building will support pedestrian interest and interaction as well as connection to the nearby park and to future phases of development.

- b. What views in the immediate vicinity would be altered or obstructed? [\[help\]](#)

The BelRed Corridor FEIS included a view/visual analysis component. The analysis found that taller buildings on the ridgetop location of The Spring District would be prominently visible from several public vantage points, however, the proposal is a shorter building at approximately 55-feet in height. From City Hall and the western terminus of the SR-520 Trail at NE 24th Street, the proposed building may intersect the distant ridge lines but not block significant views, such as Mount Rainier. At

the public vantage points on BelRed Road and on 124th Avenue NE, the building will not be prominent nor block significant views.

c. Proposed measures to reduce or control aesthetic impacts, if any: [\[help\]](#)

Since the articulated roof of the GIX Building will be visible from neighboring buildings within the Spring District site, the building's roof will not contain exposed mechanical equipment.

11. Light and Glare [\[help\]](#)

a. What type of light or glare will the proposal produce? What time of day would it mainly occur? [\[help\]](#)

The new building along with street lighting and traffic on the roadway network will increase light and glare at night. However, the site was a former warehouse facility with truck traffic so the light and glare is not expected to increase over previous conditions on site.

b. Could light or glare from the finished project be a safety hazard or interfere with views? [\[help\]](#)

It is not anticipated that light or glare from this project will be a safety hazard or interfere with views.

Project
subject to
Light and
Glare
requirements
of LUC
20.50.522

c. What existing off-site sources of light or glare may affect your proposal? [\[help\]](#)

There are no known off-site sources of light or glare that would affect the proposal.

d. Proposed measures to reduce or control light and glare impacts, if any: [\[help\]](#)

Exterior lighting will meet City design standards and cast light downward.

12. Recreation [\[help\]](#)

a. What designated and informal recreational opportunities are in the immediate vicinity? [\[help\]](#)

Wilburton Hill Park and Botanical Gardens and Kelsey Creek Park are located approximately $\frac{3}{4}$ miles to 1 mile from the Spring District site.

A new public park has been established one block west of the subject site, which will be expanded to the north over time.

b. Would the proposed project displace any existing recreational uses? If so, describe. [\[help\]](#)

The development will not displace any existing recreational uses.

c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: [\[help\]](#)

As no impacts on recreational opportunities are proposed, no mitigation is needed.

13. Historic and cultural preservation [\[help\]](#)

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- a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers located on or near the site? If so, specifically describe. [\[help\]](#)

The Washington State Department of Archaeology and Historic Preservation online GIS map tool does not indicate there are any places or objects listed on any registers within the immediate vicinity of the proposal.

- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources. [\[help\]](#)

None known.

- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc.

[\[help\]](#)

Washington State Department of Archaeology and Historic Preservation online GIS map tool.

- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required. [\[help\]](#)

The development will not have any impact on historical or cultural landmarks, therefore no mitigation is proposed.

14. Transportation [\[help\]](#)

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any. [\[help\]](#)

The proposal is generally served by NE District Way, 124th Avenue NE, and 120th Avenue NE. Freeway access includes SR-520 located north of the site and I-405 to the west. Primary access to the proposal will be from 124th Avenue NE via the new NE District Way.

- b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop? [\[help\]](#)

The proposal will not be served directly by public transit, however, King County Metro, serves the vicinity with bus service, including:

- Route MT 226-0: - approximately 0.1 miles from the project site
- Route MT 249-0: approximately 0.3 miles from the project site
- Route MT 672-0, MT 889-0: approximately 0.3 miles from the project site
- King County Rapid Ride B-Line: approximately 0.3 miles from the project site

- c. How many additional parking spaces would the completed project or non-project proposal

have? How many would the project or proposal eliminate? [\[help\]](#)

The Proposal will create approximately ~~200~~ parking stalls in the underground parking garage. The Proposal will not eliminate any parking stalls.

210 stalls

- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private). [\[help\]](#)

The proposal does not require any new roadways or streets. This proposal is part of a Master Development Plan, in which two of the roadways that will front and provide access to this building have been constructed. These roadways include NE District Way and 123rd Avenue NE. The construction of 122^d Avenue NE, which is a private roadway this building will front on its west side, will be constructed under separate permit.

- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe. [\[help\]](#)

The development does not use or occur in the immediate vicinity of current water, rail, or air transportation.

- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and nonpassenger vehicles). What data or transportation models were used to make these estimates? [\[help\]](#)

Based on the City of Bellevue's trip generation rate for commercial use, which is more conservative than university use, the anticipated PM peak hour trips associated with the proposal are as follows:

- Office (university) - 115 trips
- Retail - 8 trips
- Total - 123 trips

- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe. [\[help\]](#)

The proposal will not affect or be affected by the movement of agricultural and forest products on the roads.

- h. Proposed measures to reduce or control transportation impacts, if any: [\[help\]](#)

This proposal is part of a phased Master Development Plan. A Master Development Plan phasing plan revision is underway, which will look at traffic concurrency for transportation network impacts attributed to this and other Spring District proposals.

Design and construction are underway to accommodate increased density planned by the BelRed Corridor Plan and FEIS. City of Bellevue projects adjacent to this proposal include: 120th Avenue NE widening, 124th Avenue NE widening, as well as the design and construction of NE Spring Boulevard.

15. Public Services [\[help\]](#)

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe. [\[help\]](#)

There will be an increase in demand for fire and police protection services associated with two new buildings and existing building remodel. As there will be no residential development as part of this proposal, no additional students will be added to local schools.

- b. Proposed measures to reduce or control direct impacts on public services, if any. [\[help\]](#)

Increased tax base from the buildings will offset the financial impact of the additional public services needed.

16. Utilities [\[help\]](#)

- a. Circle utilities currently available at the site: [\[help\]](#)
electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other

Electricity, natural gas, water, refuse service, telephone and sanitary sewer are or will be available at the site.

- c. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed. [\[help\]](#)

City sewer, storm drainage and water (domestic, fire and irrigation) will be extended to serve the demands of the proposal. Connections to these utilities will be from existing utility stubs in NE District Way and enter mains that run through the Spring District site. These lines exit the Spring District site at either 120th Avenue NE, 124th Avenue NE or NE 12th Street. Telephone service will be provided by a local communications provider and electricity and natural gas will be provided by Puget Sound Energy.

C. Signature [\[help\]](#)

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision

Signature: 

Name of signee: *Joleen Peterson*

Position and Agency/Organization: *JMJ TEAM*

Date Submitted: *February 5, 2016*

TECHNICAL MEMORANDUM

Project: Parcel 14 Global Innovation Exchange
(GIX) The Spring District

From: Joleen Peterson
Justin Jones, PE

RE: Parcel 14 Greenhouse Gas Emissions

Date: October 29, 2015

Summary

A technical memorandum dated March 28, 2012 both qualitatively and quantitatively described the new greenhouse gas (GHG) emissions associated with The Spring District Master Development Plan. As an update to that memo, The Spring District is submitting this GHG emissions memo for Parcel 14 of development. Though the proposal does not meet the Ecology threshold of 25,000 MTCO_{2e} of new emissions to trigger a quantitative analysis, the low impact development methods described in the March 28, 2012 memo apply to the entire Spring District site, including Parcel 14.

Phase 1a – Qualitative Disclosure

Ecology guidance suggests projects expected to produce an average estimate of 10,000 to 25,000 MTCO_{2e} annually should provide a qualitative disclosure of emissions associated with the project.

Parcel 14 includes the construction of the Global Innovation Exchange (GIX)—an approximately 90,000 SF building, including 85,524 SF of office and 3,438 SF of retail space. The expected sources of emissions for GIX include embodied emissions created through the manufacturing, transportation, construction, and disposal of building materials as well as emissions created through landscape disturbance. Emissions associated with the operation of the project include building energy usage and maintenance as well as vehicle trips accessing the site.

Mitigation measures proposed for The Spring District development include transit-oriented development (light rail station, walkable community, multi-use path with regional connection, and mixed-use development), low impact development techniques, which may include: rain gardens, bioretention, porous concrete, and LEED® certified buildings. These techniques can reduce the emissions by at least 11% compared to the no-mitigation option. In addition, through the use of superior building materials and design, the developer anticipates a 100-year lifespan of its buildings, rather than the typical 80-lifespan. This extended lifespan reduces the average annual MTCO_{2e} emissions over the life of the building.

GHG Screening Table

The office and retail use of the Parcel 14 building has been estimated using the King County Department of Development and Environmental Services SEPA GHG Emissions Worksheet, Version 1.7. Using this worksheet, the Parcel 14 building has been screened for Ecology’s thresholds for qualitative and quantitative reporting requirements. No mitigation reductions in MTCO_{2e} emissions were applied to these calculations. See Table 1 below.

Table 1. Ecology GHG Screening – Parcel 14

Building Use	Area	Lifespan Emissions	Estimated Lifespan	Annual Emissions
Office	85,524 SF	115,000 MTCO ₂ e	100 years	1,150 MTCO ₂ e
Retail	3,438 SF	2,600 MTCO ₂ e	100 years	26 MTCO ₂ e
			TOTAL	1,176 MTCO₂e

As shown in the table above, the Parcel 14 GIX Building does not meet the 10,000 MTCO₂e threshold to prompt a qualitative analysis of greenhouse gas emissions.

Instructions: Fill in grey cells only

Line **FAR Incentive System for Bel-Red**

Line		Test	Submittal 1 Office Lot 16	Submittal 2 Resi Lots 18-21	Submittal 3 Resi 2 Lot 22-23	Submittal 4 GIX Lot 14	Submittal 5 Brewpub Lot 12	
4	Non-Residential Development							
5	Aggregate Parcel Size (sf)	100,000	112,373	-	-	59,527	40,038	
6	Total Allowed SF at max FAR (4.0)	400,000	449,492	-	-	238,108	160,152	
7	GFA of Proposed Development	375,000	300,962	-	-	176,843	24,035	
8	Sub: Retail (<i>only if FAR exempt</i>)	75,000	-	-	-	-	-	
9	Sub: Commercial (incl Hotel)	300,000	300,962	-	-	176,843	24,035	
10	FAR	3.75	2.68	-	#DIV/0!	2.97	0.60	
10	Base FAR Allowed (1.0)	100,000	112,373	-	-	59,527	40,038	
11	GFA subject to Incentive System	200,000	188,589	-	-	117,316	-	
12	GFA in Tier 1 (up to 2.5 FAR)	200,000	188,589	-	-	117,316	-	
13	GFA in Tier 2 (up to 0.5 FAR)	75,000	-	-	-	-	-	
14								
15	Residential Development							
16	Aggregate Parcel Size (sf)	100,000	-	110,000	83,367	-	-	
17	Total Allowed SF at max FAR (4.0)	400,000	-	440,000	333,468	-	-	
18	GFA of Proposed Development	375,000	-	298,737	251,004	-	-	
19	FAR	3.75	-	2.72	3.01	-	-	
19	Base FAR Allowed (1.0)	100,000	-	110,000	83,367	-	-	
20	GFA subject to Incentive System	275,000	-	188,737	167,637	-	-	
21	GFA in Tier 1a (up to 1.25 FAR)	125,000	-	137,500	104,209	-	-	
22	GFA in Tier 1b (up to 1.25 FAR)	125,000	-	51,237	63,428	-	-	
23	GFA in Tier 2 (up to 0.5 FAR)	25,000	-	-	-	-	-	
24								
24	Site FAR		2.68	2.72	3.01	2.97	0.60	
25	Overall Average MDP FAR			2.70	2.78	2.81	2.59	

27 **Amenity Fee-in-Lieu Payment Calculation**

28	Gross square feet (GSF) of development		344,363	320,924	261,059	93,863	
29	GSF for DA \$3.75 discount rate available		750,000	405,637	84,713	-	
30	GSF for DA \$3.75 discount rate used		344,363	320,924	84,713	93,863	
31	GSF for DA \$3.75 discount rate remaining		405,637	84,713	-	(93,863)	
32							
33	% Floor Area eligible for DA \$3.75 rate		100%	100%	32%	0%	0%
34	% Floor Area eligible for DA \$4.00 rate		0%	0%	68%	100%	100%
35							
36	Total floor area subject to Amenity System (20.25D.090.C)		188,589	188,737	167,637	117,316	-
37	Total square feet of Amenity System provided on-site		0	0	0	0	0
38	Remaining square feet subject to fee-in-lieu		188,589	188,737	167,637	117,316	-
39							
40	Square feet paid at \$3.75		100% 188,589	100% 188,737	32% 54,398	0% -	0% -
41	Tier 1A [Res]: affordable housing			\$ 515,625.00	\$ 203,992.50	\$ -	\$ -
42	Tier 1 [Cml] or Tier 1B [Res]: parks and streams		\$ 707,208.75	\$ 192,138.75	\$ -	\$ -	\$ -
43							
44	Square feet paid at \$4.00		0%	0% -	68% 113,239	100% 117,316	100% -
45	Tier 1A [Res]: affordable housing			\$ -	\$ 199,243.00	\$ -	\$ -
46	Tier 1 [Cml] or Tier 1B [Res]: parks and streams		\$ -	\$ -	\$ 253,713.00	\$ 469,264.00	\$ -
47							
48	Total fee		\$ 707,208.75	\$ 707,763.75	\$ 656,948.50	\$ 469,264.00	\$ -

Spring Residential 1 GSF per building

Buildi GSF	% of total	Portion of fee-in-lieu
A	62,875	19.6% \$ 138,664.13
B	102,749	32.0% \$ 226,601.99
C	50,370	15.7% \$ 111,085.68
D	80,435	25.1% \$ 177,390.84
E	24,495	7.6% \$ 54,021.12
Total	320,924	\$ 707,763.75

Individual fees per building will be deposited into the Affordable Housing Fund first until the obligation of \$515,625 is met, then remaining funds to Parks & Streams Fund

Spring Residential 2 GSF per building

Buildi GSF	% of total	Portion of fee-in-lieu
A	100,544	38.5% \$ 253,016.48
B	95,637	36.6% \$ 240,668.14
C	64,878	24.9% \$ 163,263.88
Total	261,059	\$ 656,948.50

Individual fees per building will be deposited into the Affordable Housing Fund first until the obligation of \$515,625 is met, then remaining funds to Parks & Streams Fund

MEMORANDUM

DATE: April 5, 2016
TO: Tara Howard, Wright-Runstad
FROM: John Perlic, PE, Parametrix
 Alex Atchison, PE, Parametrix
SUBJECT: Spring District Block 14- Parking Study
CC:
PROJECT NUMBER: 554-4575-017
PROJECT NAME: Spring District

INTRODUCTION

The purpose of this memorandum is to document the parking analysis completed for Block 14 of the Spring District development. The project proposes to construct 80,172 net square feet (nsf) of office and 3,473 nsf of retail within Block 14. Current plans are to lease the office space to the Global Innovation Exchange (GIX), a university partnership between the University of Washington and Tsinghua University. This memorandum provides a summary of Block 14 parking needs based on both office and university land uses. The needs were based on survey data of a similar college in the area and the Institute of Transportation Engineers (ITE) Parking Generation manual, 4th edition.

After completion of East Link and as other phases of the Spring District are developed, additional shared use and walk trips would be expected to increase over time. The following analysis reports the maximum site use for Block 14 only, assuming no shared use or walk trips. This assumption and the resulting parking demand should be reevaluated as subsequent phases are completed.

CITY OF BELLEVUE PARKING CODE

The City of Bellevue parking code provides a range of a minimum parking spaces required to maximum parking spaces allowed. Table 1 summarizes the minimum and maximum City of Bellevue parking requirements for Block 14. The City of Bellevue has no parking requirements for university or college land use.

Table 1. City of Bellevue Parking Code¹

Land Use	City of Bellevue		Block 14
	Minimum - Maximum	Size	Minimum – Maximum
Office	2 – 3 stalls per 1k nsf ²	80,172 sf	160 - 240
Retail	2 – 3.5 stalls per 1k nsf ²	3,473 sf	7 - 12
			Total 167 -252

1) Bellevue LUC 20.25D.120

2) nsf – net square feet

PARKING SURVEY

Since the City of Bellevue has no parking requirements for university or college land use, a parking demand survey for a similar university site was conducted. Parking demand was surveyed at Bellevue College's North Campus. The campus has its own on-site parking lot with a supply of 198 spots. In addition to the on-site parking, Bellevue College also has agreements with nearby businesses to share parking during certain hours of the day. The Bellevue College North Campus has capacity for up to 800 students, however based on conversations with Bellevue College staff, approximately 400 students and 60 staff are present on a typical day.

The survey was conducted on a Wednesday, December 2, 2015 between 9:00 and 11:00 am, corresponding to the peak parking rate for university land use per the ITE Parking Generation manual. Parking counts were taken at both the on-site parking lot and at the adjacent Courtyard Marriott, the off-site parking lot approved for additional daytime parking. The peak parking demand occurred between 10:00 and 11:00 am with a total of 217 vehicles parked in both parking lots. To be conservative it was assumed all cars parked in the Courtyard Marriott parking lot were related to Bellevue College. Daily distributions, taken from the ITE Parking Generation manual for a typical weekday for university land use were applied to the peak parking demand to develop a daily parking demand distribution.

The peak parking demand for the Bellevue College North Campus, based on the survey data, is 5.05 vehicles per school population. The peak parking demand for a university according to the ITE Parking Generation manual is 1.2 vehicles per school population.

BLOCK 14 PARKING ANALYSIS

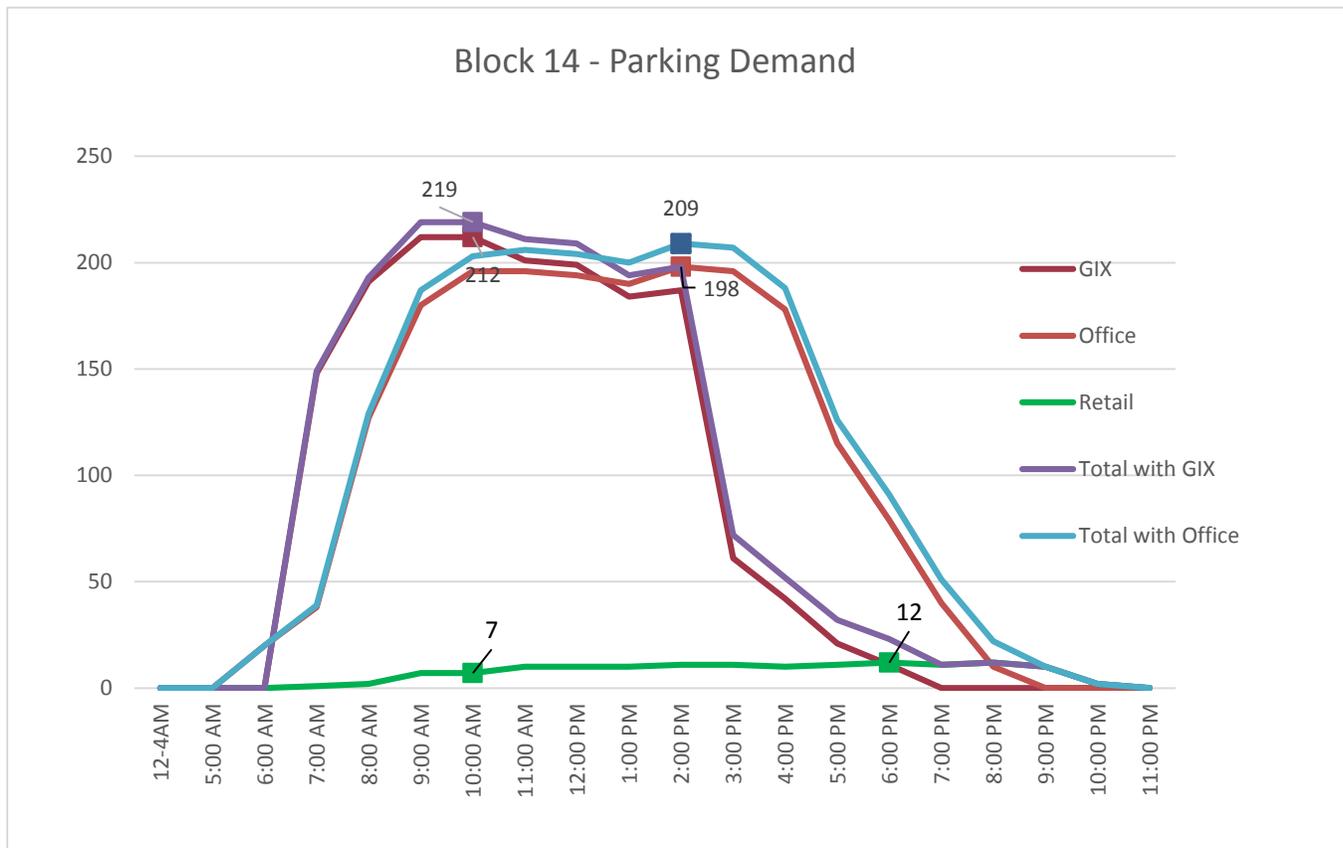
A parking analysis was completed for Block 14 to determine if the proposed parking supply would be sufficient to meet the parking demand of the GIX university land use and retail use. In addition, the parking supply required for office and retail land uses was completed to confirm the proposed parking supply is sufficient. To estimate parking demand for the different land uses, data from the ITE Parking Generation manual and the City of Bellevue was used.

Using the estimated enrollment at GIX of 400 students and assuming a staff size similar to Bellevue College, parking was estimated for a school population of 450. Based on the peak parking demand from the similar university site, estimated parking demand for GIX would range from 42 – 212 vehicles during the peak parking period (8:00am – 4:00 pm), with the peak parking demand estimated to be 212 vehicles.

Based on conversations with City staff, parking demand for the retail land use was estimated using the retail parking ratio found in Bellevue LUC 20.25D.120.B.2.j, which is a minimum of 2.0 parking stalls and a maximum of 3.5 parking stalls per 1,000 square feet. Using this ratio, the retail use would generate up to 12 parked vehicles during the peak hour on a typical weekday. Retail parking peaks between 6:00 and 7:00 pm. Parking demand for retail that coincides with university peak demand would be 7 vehicles. The resulting daily peak parking demand for GIX, retail and their total are shown in Figure 1. The peak parking demand would be 219 vehicles.

For comparison, the peak parking demand for office and retail land uses was estimated. Parking demand for the land use 701 "general office" was used to estimate office parking demand. Using a parking demand of 2.47 vehicles per 1,000 square feet, an office use would generate up to 211 parked vehicles on a typical weekday. The resulting daily parking demand of 209 vehicles for office, retail and their total are shown in Figure 1.

Figure 1: Daily Distribution of Parking Demand



As shown below in Table 2, the parking ratio for Parcel 14, with retail and university land uses, ranges from 49-222 parking stalls. The parking ratio for Parcel 14, with retail and office land uses is higher and ranges from 167 – 252 parking stalls. The project proposes to provide 210 parking stalls, with 7 allocated for retail and 203 for GIX/office use, meeting both the ratio estimated for retail and university land uses as well as the ratio for retail and office land uses.

Table 2. GIX Parking Demand

City of Bellevue Code			Block 14
Land Use	Minimum - Maximum	Size	Minimum – Maximum
University	n/a	450 school population	42 -212 ²
Retail	2 – 3.5 stalls per 1k nsf ³	3,473 sf	7 - 12
Total			49-224
Office	2 – 3 stalls per 1k nsf ²	80,172 sf	160 - 240
Retail	2 – 3.5 stalls per 1k nsf ²	3,438 sf	7 - 12
Total			167 -252

1) Bellevue LUC 20.25D.120
2) Based on parking demand study, Parametrix Dec 2015
3) nsf – net square feet

RECOMMENDATION

Parking for Parcel 14 should be estimated based on retail and office land uses. This insures that if the university land use ceases to exist, Parcel 14 will provide adequate parking for retails and office land uses. The project proposed to provide 210 parking stalls (7 retail/ 203 for GIX/office) , falling within the minimum and maximum range for retail and office land uses, as required by City of Bellevue code.

TECHNICAL MEMORANDUM

Project: Parcel 14 Global Innovation Exchange
(GIX) The Spring District

From: Joleen Peterson
Justin Jones, PE

RE: Impervious Coverage Update

Date: April 05, 2016

Introduction

Per BelRed LUC 20.25D.080 and The Spring District's Master Development Plan approval, The Spring District is not to exceed 75% impervious coverage site-wide at full build-out. At each phase of design review, The Spring District provides an update of the site's impervious surface coverage to track the progress and ensure it meets the 75% impervious requirement.

This memo updates the impervious lot coverage of permitted projects with this Parcel 14 Global Innovation Exchange (GIX) building and parcel design.

Phase 1A

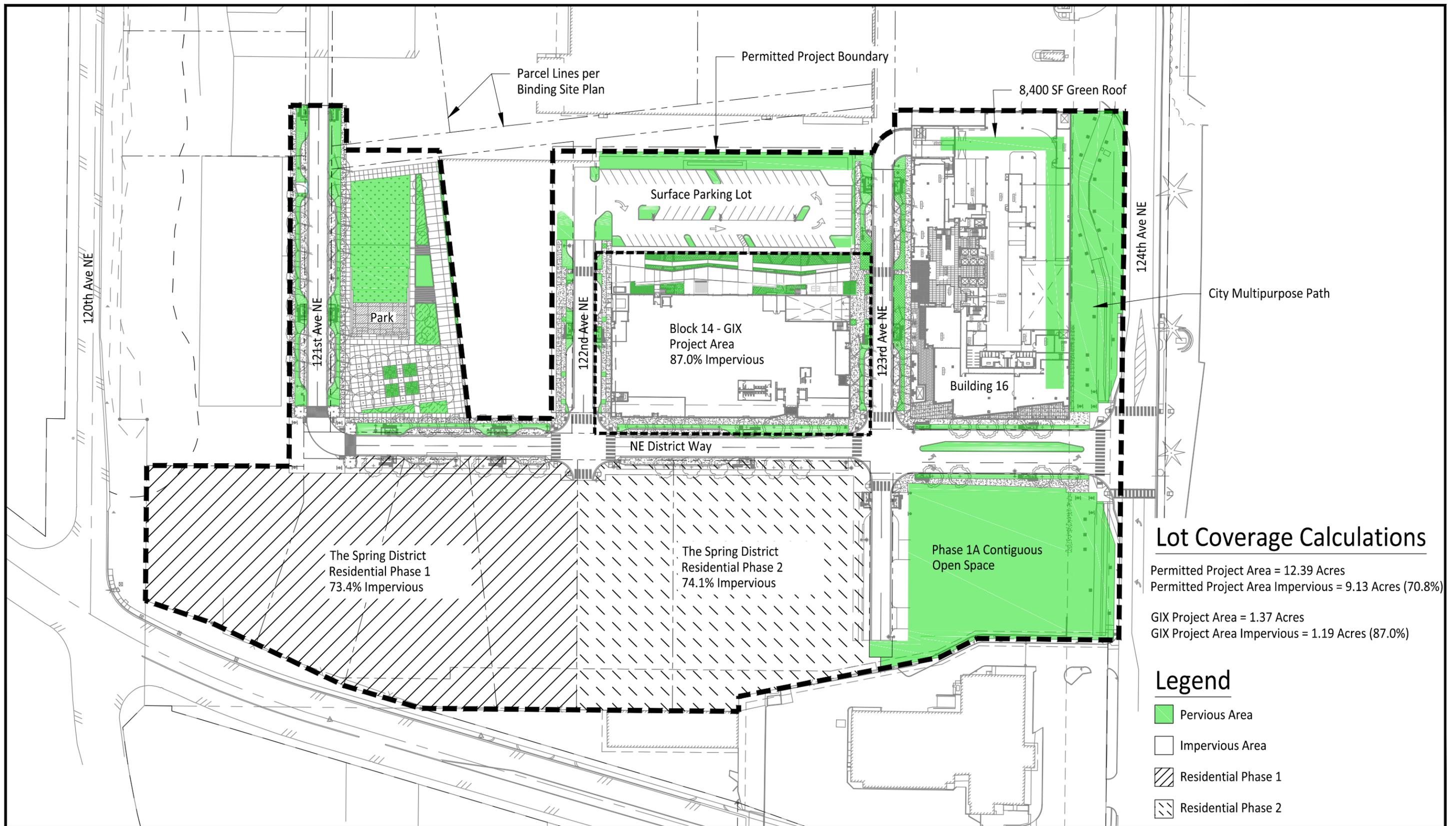
Permitted projects within The Spring District include 12.39 acres, including buildings on Parcels 14 and 16; roads, sidewalks, planter strips and bioretention cells (site infrastructure); park space on Tract C and interim park space on Parcel 24; as well as residential parcels 18-23 extending to the back of curb.

The full buildout of Parcel 14 GIX includes 87.0% impervious surfaces. Updated to reflect this buildout, permitted projects will be covered in 70.8% impervious surfaces. This is within the 75% impervious coverage limit in the BelRed Land Use Code. Pervious areas include the following:

- A pedestrian plaza/walkway with plantings on the north side of the GIX building.
- Tract C Park grass, pervious pavers and trees
- An 8,400 SF green roof on Building 16
- An acre of interim open space on Parcel 24 (to be replaced with a permanent open space/park on Tracts G and J in Phase 1B)
- Planter strips, bioretention cells and parking lot landscape islands

See the attached figure for an illustration of the impervious/pervious surfaces within the permitted projects. It is important to note that during the permitting process for Building 16, the City of Bellevue has agreed to consider the multi-use path along 124th Avenue NE as a pervious surface since the proposed pervious asphalt path was not the city's preferred surfacing.





Lot Coverage Calculations

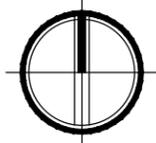
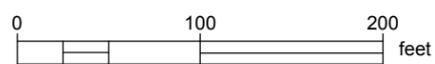
Permitted Project Area = 12.39 Acres
 Permitted Project Area Impervious = 9.13 Acres (70.8%)

GIX Project Area = 1.37 Acres
 GIX Project Area Impervious = 1.19 Acres (87.0%)

Legend

- Pervious Area
- Impervious Area
- Residential Phase 1
- Residential Phase 2

DATE: Apr 05, 2016 FILE: XTSD Phase 1A Lot Coverage



THE **SPRING** DISTRICT

WRIGHT
 RUNSTAD
 & COMPANY



The Spring District
 Permitted Projects
 Lot Coverage

CERTIFICATE OF CONCURRENCY

SPRING DISTRICT PHASE 1B

This certificate documents the Transportation Department Director's decision that the development project at 1227 124th Avenue NE (File No. 11-125943 LP) complies with the requirements of the Traffic Standards Code (BCC 14.10). Under the terms of the Development Agreement for the Spring District, this decision reserves 357 p.m. peak hour trips to Phase 1B of the Spring District Master Development Plan, subject to Process II appeal of either the concurrency determination or the land use decision for the first project in Phase 1B. The concurrency approval issued for Phase 1B shall expire five years from the date of issuance, provided that such five year period shall be extended consistent with the provisions of BCC 14.10.040.F; and further provided that in no event may development in the Spring District consisting of more than 1,000,000 square feet of commercial development be vested at any given time.



Director, Transportation Department

Date

5/5/2016

Certificate No. 102