

# **Appendix B:**

# **The BelRed Corridor Plan**

## **Streetscape Character, Guidelines, and Standards**

AUGUST 2011





## MEMORANDUM

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**TO:** Traffic Management Staff, Capital Projects Staff, Planning Staff, Streets Maintenance Staff

**FROM:** Goran Sparrman, Director

**DATE:** September 13, 2011

**SUBJECT:** Design Manual Update – The BelRed Corridor Plan

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The BelRed Corridor Subarea Plan and rezone, adopted in 2009, and the updated BelRed Land Use Code (LUC 20.25D) established a local street grid system in the BelRed Corridor area that emphasizes continuity, connectivity, and community character.

The Transportation Department's Design Manual, which includes design standards and drawings for construction of transportation infrastructure, is being updated by the incorporation of The BelRed Corridor Plan: Streetscape Character, Guidelines, and Standards, as Appendix B.

These new guidelines and standards should be incorporated for projects currently under design and prior to submittal of a complete application. Many aspects of Appendix B are conceptual and may be modified during the design phase of a project, provided that the intent of the BelRed Corridor Subarea Plan, rezone, and code are maintained.

The effective date of the revised Design Manual is September 13, 2011.

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# Introduction

## Goal

To distill the character of the BelRed Corridor and guide its expression in the streetscape.

The 2008 Bel-Red Subarea Plan establishes a vision for the reshaping of the Bel-Red Corridor into a vibrant and sustainably developed area that accommodates high-technology and other forms of business, provides dense transit-oriented retail and residential development, and restores riparian corridors to provide habitat and open space recreation.

This transformation is spurred by the catalyst of Sound Transit's East Link Light rail alignment, and guided by a new supplement to the Bellevue Land Use Code, *Part 20.25D – Bel-Red*. This new amendment provides guidelines, standards and code requirements for new development in the Corridor, but does not illustrate planned street types nor make specific recommendations about what aspects of the Corridor's character should be expressed through streetscape design.

*Part 20.25D – Bel-Red* identifies a total of five street typologies within the Corridor: Local Streets, Retail Streets, Green Streets, Arterials, and a Transit Boulevard. Arterial design will be developed under direction from the Bellevue Transportation Department, and Transit Boulevard design will be developed under standards produced by the NE 15th/16th Street Project.

This plan, the Bel-Red Corridor Plan/Streetscape Character and Design Guidelines, addresses Local Streets, Retail Streets, and Green Streets. It is intended to supplement *Part 20.25D – Bel-Red*, and accomplish three primary objectives:

- Identify qualities of Bel-Red's past, present and future character that can be expressed graphically in certain elements in the street matched with a complimentary palette of materials;
- Provide new development with a set of guidelines on how a palette of street elements and materials can express Bel-Red's overall character as well as the unique qualities of subdistricts; this expression will occur through unique public art, street furniture, plantings and the configuration of new streets and sidewalks;
- Provide new development with a set of physical design templates for Local, Retail and Green Streets, at a block scale, to guide the arrangement of all elements in the district's rights-of-way; new development will be responsible for construction of much of the district's new street system as projects are implemented.

## How To Use This Document

This document is divided into four major topics: the establishment of the Corridor's character and a design language that conveys that character, a discussion of the role public art will play in creating identifiable, vital places within the Corridor, the development of block prototypes for the different street typologies proposed for the Corridor, and specific street furnishing recommendations that are consistent with the Corridor's character. Specifically:

**Part 2** makes an assessment of the aspects of the BelRed Corridor's character that should be expressed as a basis for design in the Corridor's transformation. These concepts were identified in a series of charettes with representatives of Bellevue City Departments, the Bellevue Arts Commission and the design team. Topics include history, culture, arts, land use, natural features and technology.

**Part 3** examines the role Public Art can play in expressing BelRed's evolving character. Along with the expansion of light rail through the Corridor and catalyst projects called for in code, public art, and specifically the development of an arts district, is seen as a way to jump start the transformation of the Corridor by creating a new degree of vibrancy and community. A study of public art opportunities is presented to identify ways the Corridor's character may be interpreted and where these interpretations may occur to best initiate transformation of the Corridor and compliment anticipated development.

**Part 4** establishes physical design templates for Local, Retail and Green Streets (as identified in Bellevue Land Use Code, Part 20.25D.140). Each typology provides a statement of design intent, a character illustration, typical section, block plan and precedent photos of similar streets to give the developer a sense of the desired character of that street within the Corridor. It is understood that these templates are a starting point and that they will be adapted by developers, with City approval, to fit existing conditions and capitalize on opportunities unique to each development.

**Part 5** provides a palette of streetscape elements for each street typology that supports and articulates the expression of BelRed's character as described in Part 2. The development of the BelRed Corridor will take place over a number of years, and during that time specific products recommended in this document may go out of production, while other suitable products may be developed. This is intended to be a "living/working" design manual in which specific products may be removed or added over time, in both hard copy and web-based versions. Products are presented in a single-page "cut sheet" format to facilitate easy removal or addition.

This document makes design-based recommendations and relies upon the City of Bellevue Transportation Design Manual for technical guidance. Diagrams and drawings in this document are conceptual and do not supersede transportation engineering requirements. Engineering details required to implement unique features for the BelRed corridor must be approved by the City prior to construction. Departures from the technical design and guidelines presented in the Manual will require approval from the City.

**BELRED CORRIDOR**

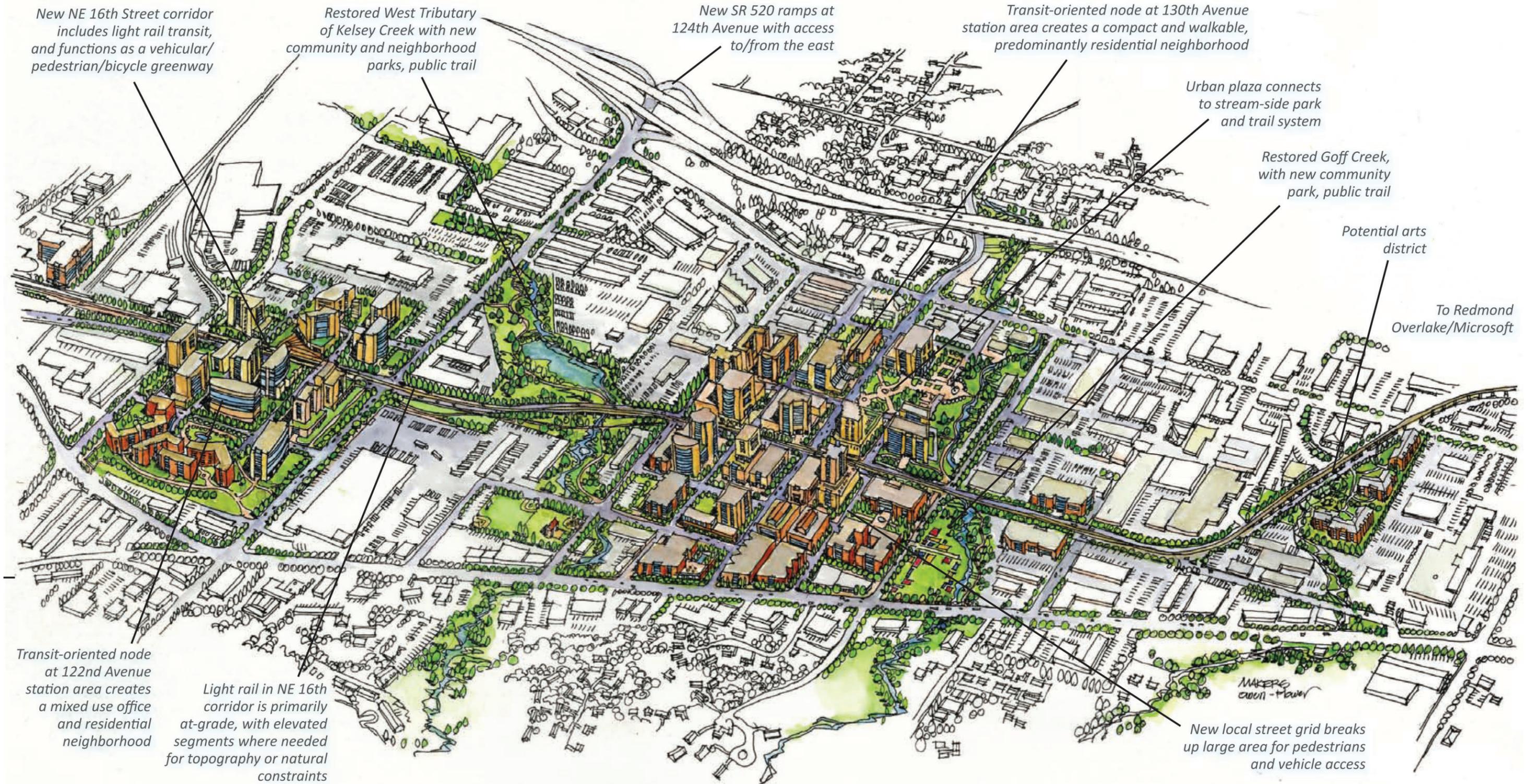


Figure 1.1



Figure 2.1

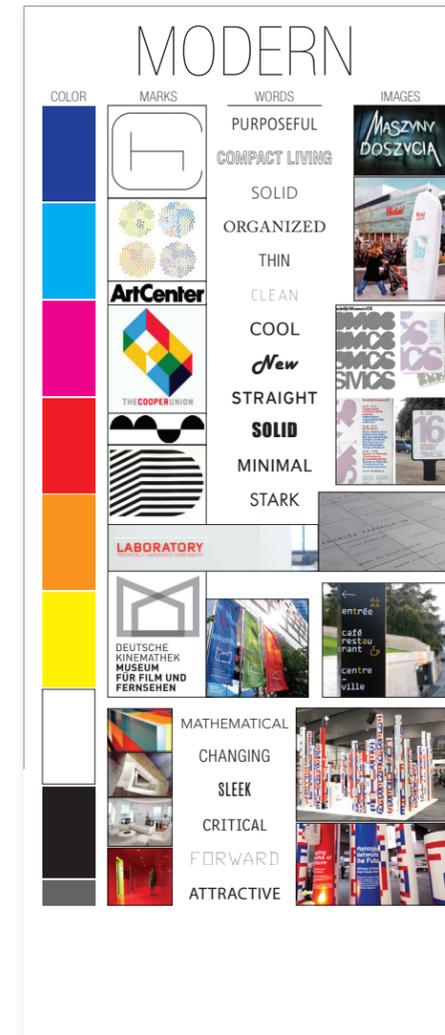


Figure 2.2

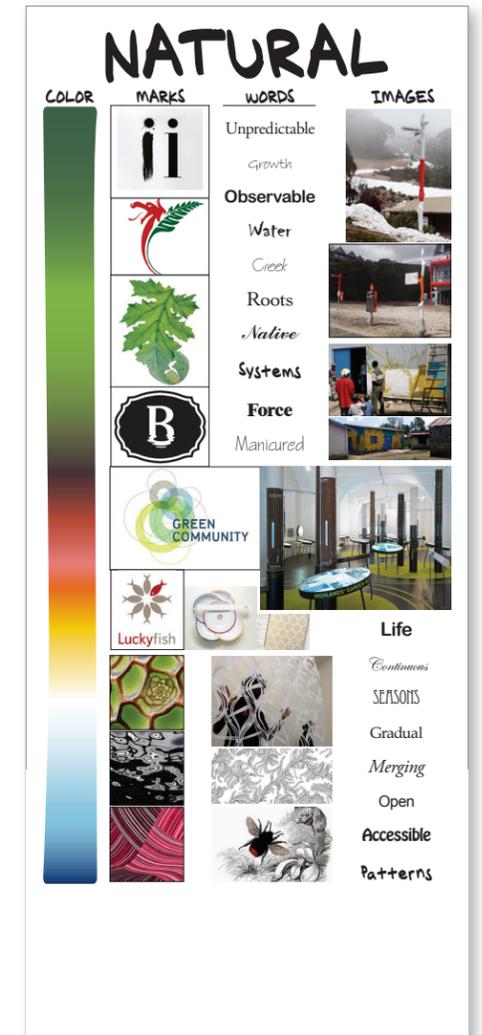


Figure 2.3



Figure 2.4

*BelRed Character Workshop*

These “mood” boards, above, were created to stimulate discussion about the character of the BelRed Corridor. This started the development of a BelRed design language that can be applied to physical features, and printed and digital media.

# Character of the Corridor

## Context

The process of identifying the character of the BelRed Corridor began with an investigation of its context. Three broad themes were identified as particularly relevant to the Corridor's character:

### Industrial

The BelRed Corridor, as it exists today, is the largest industrial/light industrial area in the City of Bellevue. The west end of the Corridor has large industrial sites such as the Safeway distribution facility, the Coca-Cola bottling plant, the Metro yard, and the Cadman plant. To the east of these large industrial sites are smaller light industrial uses, many of which have a retail component to them.

### Natural

The entire Corridor has significant natural underpinnings. It is transected by six water courses. Some already have significant green belts around portions of their course (West Tributary and Valley Creek), while the others are mostly channelized and underground. The BelRed Sub-Area Plan calls for the daylighting of these important natural features, and will reestablish important habitat corridors for fish, fowl, and other wildlife. It is envisioned that these greenbelts will provide a sense of boundary to each of the development nodes in the Corridor. They will be conspicuous as one moves through the corridor, particularly by foot or bicycle as one crosses these areas at-grade, and by rail, as one looks into them from elevated rail structures.

### Modern

The major catalyst for the redevelopment of the Corridor will be the expansion of the Link Light Rail system across the Eastside and through the BelRed Corridor. Stations at the Spring District development and at approximately 130th Avenue Northeast will transform these areas to high-density employment and residential centers with connections to established communities such as downtown Bellevue and Redmond, Overlake, regional centers such as Seattle, and the airport. This vital linkage across the Eastside connects a number of high-technology employers, and it is anticipated that this will make the Corridor an attractive location for these companies to expand to, for smaller start-ups looking for proximity to the larger companies, and for employees of these companies, who come from all corners of the globe, to live.

### Workshop Synthesis Chart

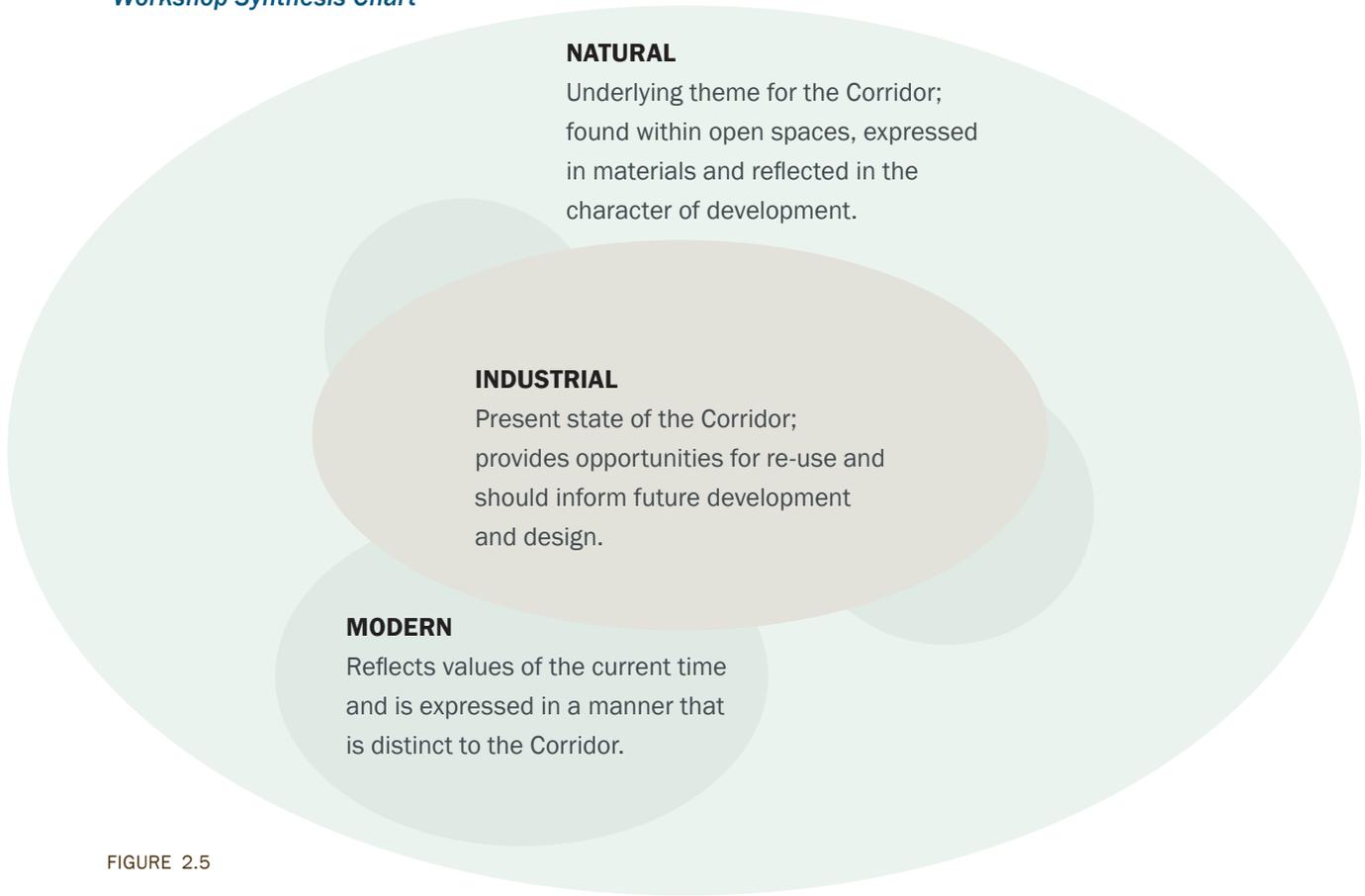


FIGURE 2.5

### BelRed Typographic Application

During the workshop process it was decided that BelRed should be typographically represented without a hyphen and each word capitalized: BelRed.

**BEL-RED**



BelRed

NOT ALL CAPS

FIGURE 2.6

## Character

With this understanding of the context in the BelRed Corridor, a series of workshops were performed as part of the work for this Plan to distill the relevant themes at play in the Corridor into a character statement and discuss how this character might be expressed in the streetscape. It was agreed upon that the Corridor's industrial setting, unique to the Eastside, and the imminent restoration and transformation of the natural systems in the Corridor are strong identifying characteristics that should be emphasized.

Equally important is the fact that BelRed will reinvent itself as one of the newest neighborhoods in the Puget Sound region. With its connectivity to established high technology businesses, it is expected to entice additional high-tech employers looking for space, and a workforce looking for residence. Increasingly, this workforce is being drawn from around the world. All of this suggests that the BelRed story should be forward-looking, and that the design language used to tell that story be contemporary and cosmopolitan.

Great consideration was given during the development of this plan to the nature of contemporary design and what it means in the context of the Corridor. Contemporary design is a reflection of its time and place. Because of the potential for the Corridor to become a very cosmopolitan neighborhood, approaches to design from around the world were considered. With society's increased awareness of sustainability and natural, renewable, and recyclable materials, Asian design motifs, with their emphasis on material and clarity of form seemed particularly appropriate. This approach to motif also draws from the historical context of the Corridor, which was settled by Japanese-American families who practiced agriculture in the area.

Given this context, the Corridor's character might best be expressed through design that references its industrial tradition and natural systems, with clear expression of materials and simple form.

“BelRed is intended to be a vibrant community defined by its creativity, connectivity, and innovation, inspired by its industrial roots, and expressing its underlying natural context through sustainable design.”

# Public Art Considerations

This plan identifies broad opportunities for the incorporation of art into the public realm. Public art will play a role in establishing human scale and creating unique identities at significant sites within the Corridor. The Corridor has the potential to become quite cosmopolitan, and as such, will become both a cultural and intellectual melting pot. This development will occur over generations in a spontaneous manner, and may be spurred by the catalysts of East Link Light Rail and the development of the Spring District.

This plan lays out a number of opportunities for private, public and community stakeholder partnerships and recommends the development of a Public Art Master Plan which will facilitate the development of memorable neighborhoods within the Corridor. These types of partnerships maximize opportunities for extraordinary improvements and places. This document presents a menu of options which illustrate the types, sizes and locations of public art that could enhance the human experience of not only these places, but also the identity of the unique series of neighborhoods within the Corridor. This plan does not make recommendations pertaining to artistic style. Site specific recommendations can only be undertaken within the context of a Public Art Master Plan which is developed through extensive historical research, community and Arts Commission involvement, and interface with Stakeholders.

## Comprehensive Corridor Recommendations

The following comprehensive actions should be taken in the consideration of public art in the Corridor:

- A Public Art Master Plan should be developed that will address logical and significant art opportunities in both private and public lands, and identifies funding and implementation strategies for those projects. These opportunities should encourage the identification of the Spring District, retail and arts districts, and the light rail alignment as unique places defined by their public art.
- A series of meetings should be convened between the artistic community and property owners within the Corridor to discuss developing building stock inventories that will support the creation of an arts district within the corridor.
- These planning exercises should be undertaken with distinct consultant teams so as to infuse each exercise with a specific set of solutions focused on the unique opportunities and identities of each condition and each district-specific team.

A set of detailed actions are presented in appendix c.

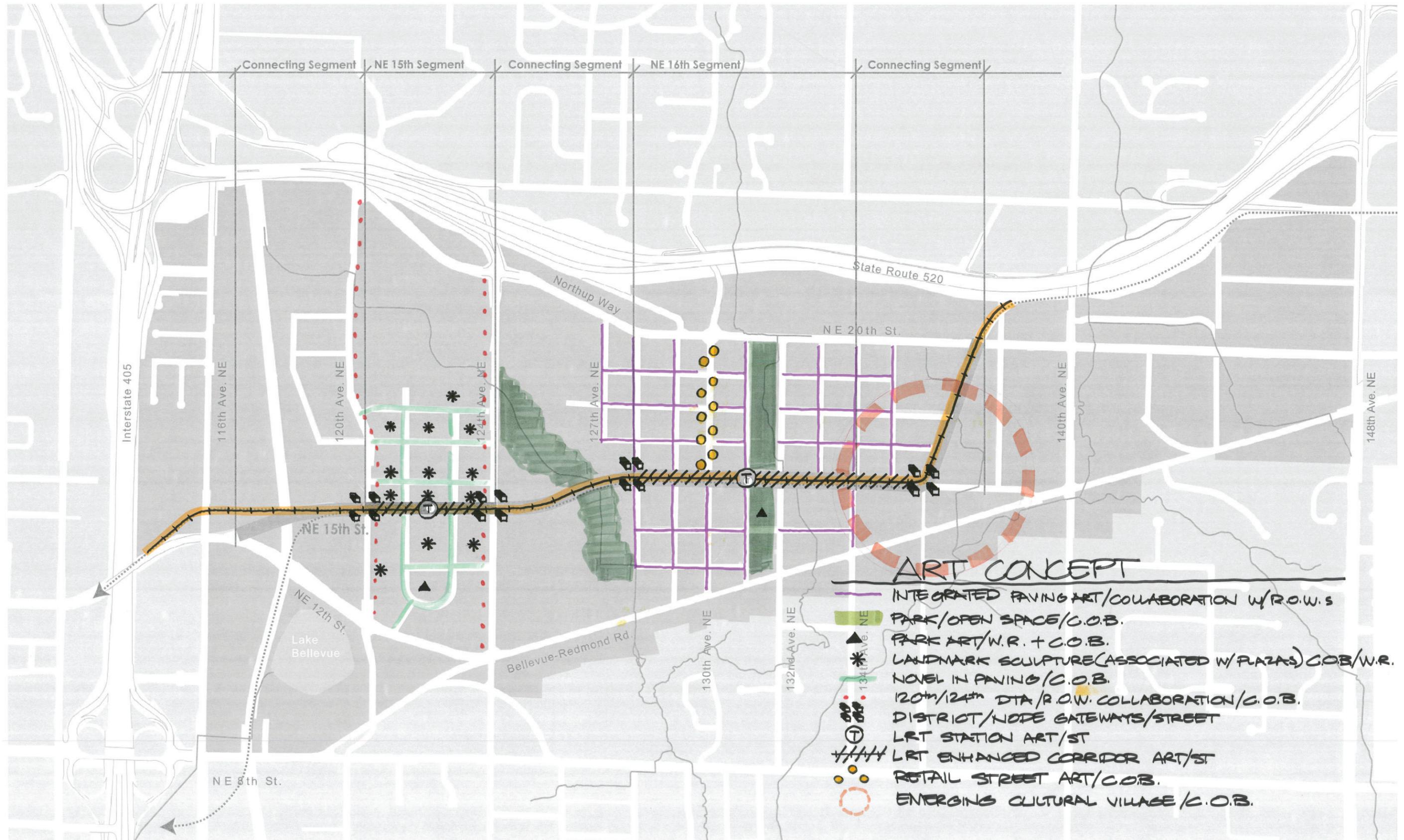
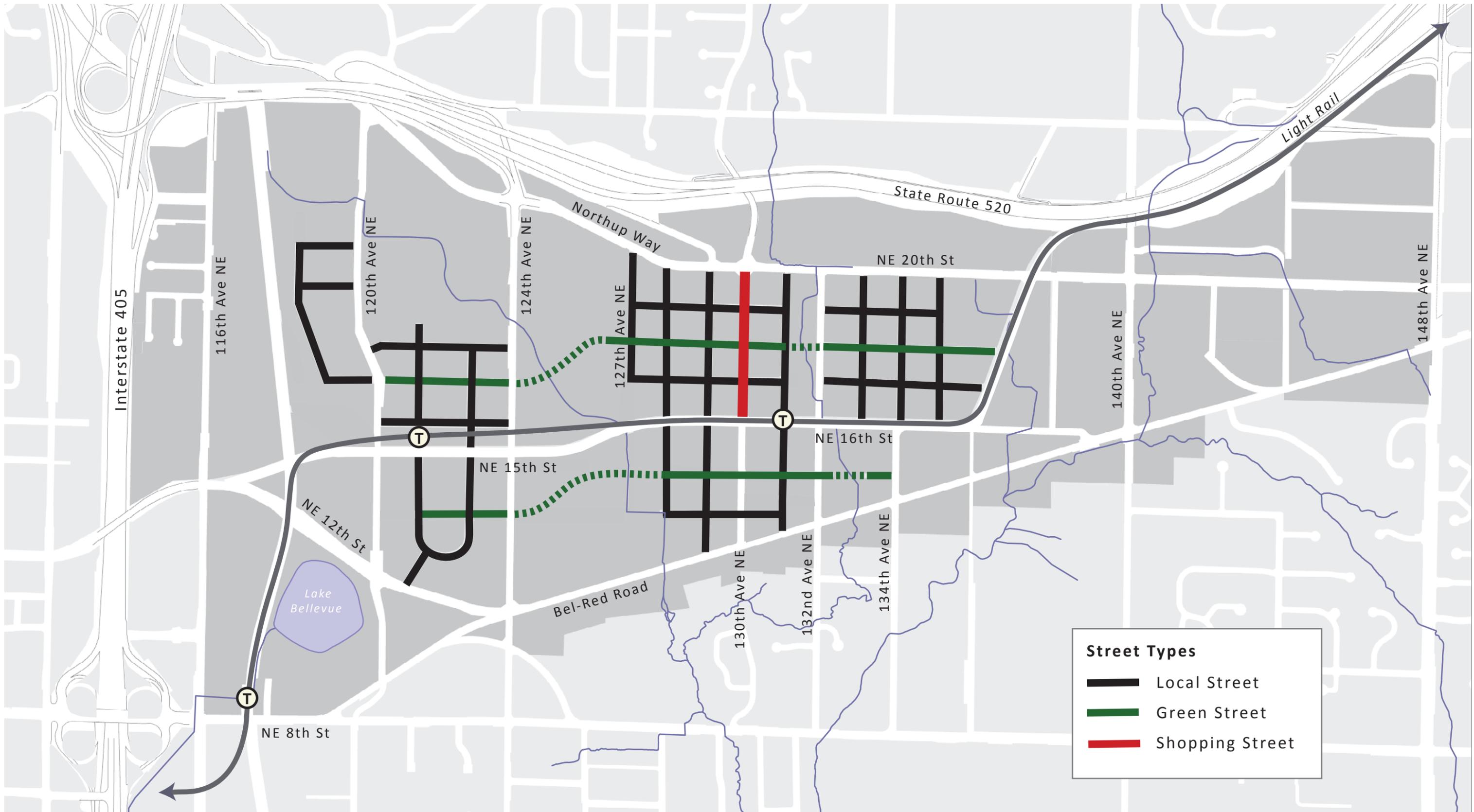


Figure 3.1

Note: Base map for reference only.

# STREET TYPOLOGY MAP

Figure 4.1



# Conceptual Plans & Development Standards

## Street Types

This plan makes design recommendations for three prototypical street types: Local Streets, Retail Streets, and Green Streets. These prototypes assume a generic block length of 300 feet, which is driven by a code stipulated maximum block frontage of 1200 feet. As station area planning evolves at the Corridor's nodes and development occurs, real conditions will dictate layout and design.

The prototypical plans make recommendations for types of street elements to be provided for each street type, general layout of the various zones of the sidewalk, and street parking configuration. These plans, sections and illustrations are meant to be used in conjunction with City of Bellevue Transportation Department Design Manual.

Generally the character of street furniture and other elements may vary from street type to street type. The level of amenity will vary, so that a retail street for example, would have more waste receptacles than a local street.

The BelRed Corridor's density, transit infrastructure, and location on a proposed major bicycle thoroughfare will encourage the use of bicycles as transportation. Local Streets, Retail Streets, and Green Streets in the Corridor are intended to feed the regional bicycle network by making connections to the dedicated bicycle lanes on NE 15th/16th Street. As such, the use of sharrows is encouraged, as directed by the City of Bellevue, in the Corridor where bicycle lanes are not present.

## LOCAL STREETS

The majority of new streets to be built in the Corridor will be Local Streets. These streets are intended to support residential development through their intimate scale, generous landscape and pedestrian furnishings. Their design intends to encourage a sense of neighborhood “ownership” and participation through the relationship of entrances, lobbies and courtyards with social spaces for seating and conversation in the street. Mid-block curb extensions will provide an amenity space for seating, additional landscaping, bike parking as well as a more frequent interval of crossing in the 300 foot blocks. Texture and detail are prioritized in design. These block types are meant to provide quiet juxtaposition to the busier retail streets or transit boulevards that they intersect. The intent is that when you turn the corner from these busier streets, you enter a quieter environment where you are able to “hear the birds sing”.



FIGURE 4.2: Residential building edges can meet the sidewalk.



FIGURE 4.3: A green edge can exist between residential buildings and the sidewalk.



FIGURE 4.4

Mid-Block Crossing

FIGURE 4.7: Connecting streets and residential buildings through large pedestrian-only corridors between buildings.



FIGURE 4.5: Courtyards for residential buildings can be open to the sidewalk.

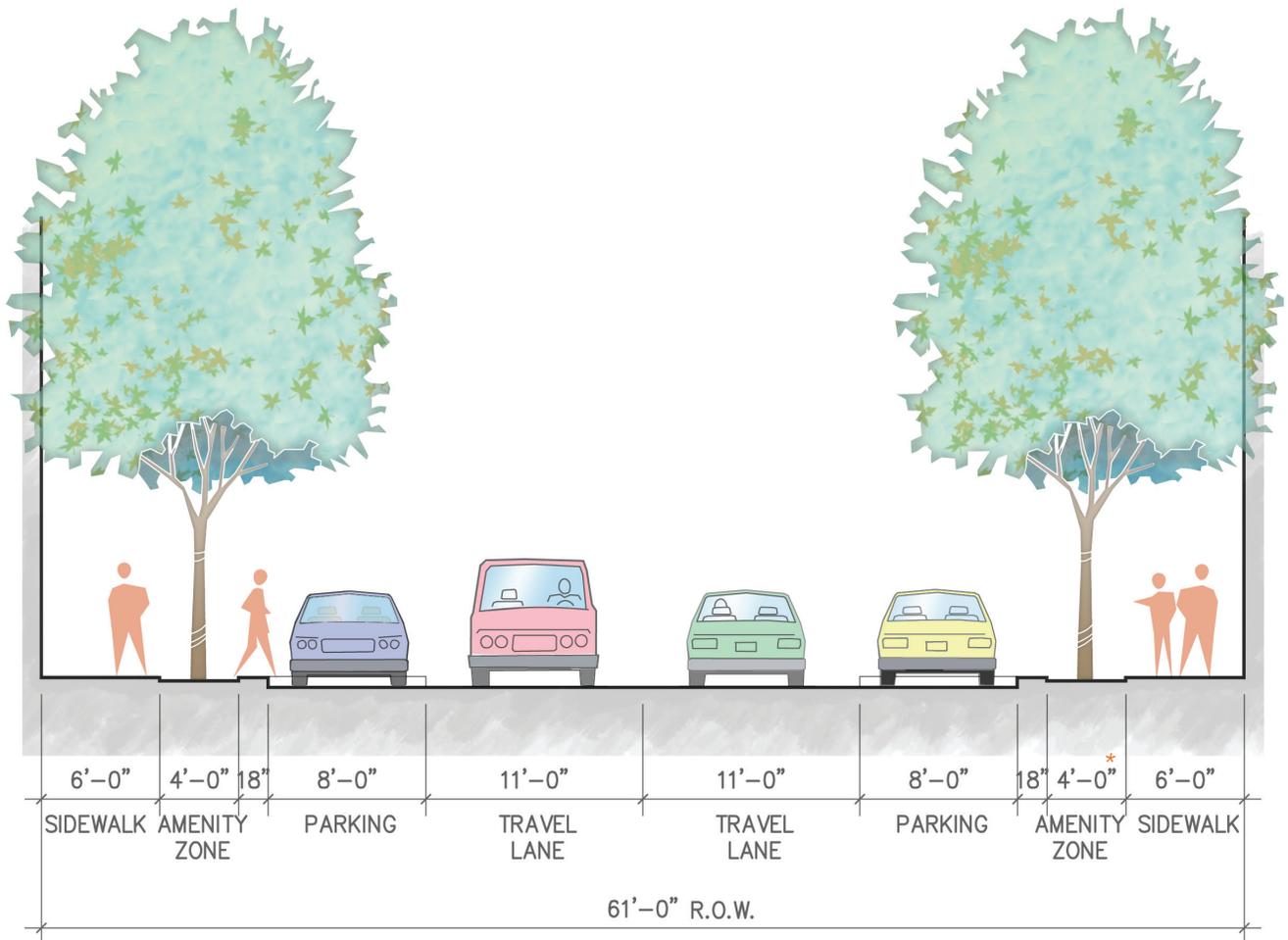


FIGURE 4.6: A courtyard with residential entrance that is gated to the sidewalk.



FIGURE 4.8: Buildings could include interior plazas connected to their lobby/public space.

**Local Streets Section**



*\*Note: 6" curb zone with 5'-0" amenity zone where no parking permitted*

FIGURE 4.9

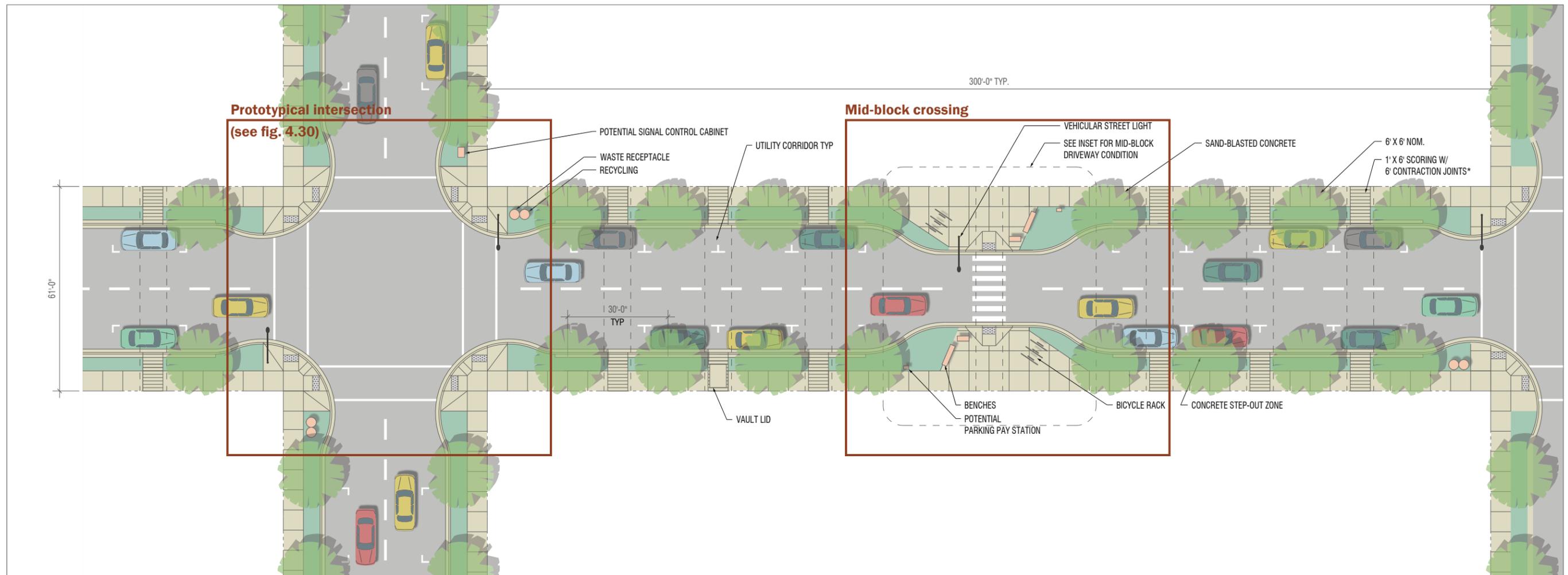


Figure 4.10

**Street Zone Guidelines**

- 6' pedestrian thru-zone.
- 18" concrete curbside step-out zone.
- 6' (max) pedestrian pass-throughs (potential location of future utility corridors)
- 8' +/- curb bulbs at mid-block crossings
- 3' +/- curb bulbs at block corners with 21' radius.

**Paving & Scoring**

- Concrete Joints: Saw-cut or trowel at developer's option.
- Pedestrian pass-throughs: 1' x 6' scored joint pattern over 6' x 6' full-depth contraction joints.

**Lighting**

- Vehicular street lights at middle and corners of blocks, one side of block only, or as required to meet design standards.

**Landscape & Furnishings**

- Trees spaced at 30' on-center in planting strip.
- 4'-0" planting strip.
- Waste receptacles on pads within planting strip, on opposing corners.
- One (potential) parking pay station at every mid-block crossing as shown at edge of sidewalk.
- Benches and bicycle racks located at mid-block crossings.

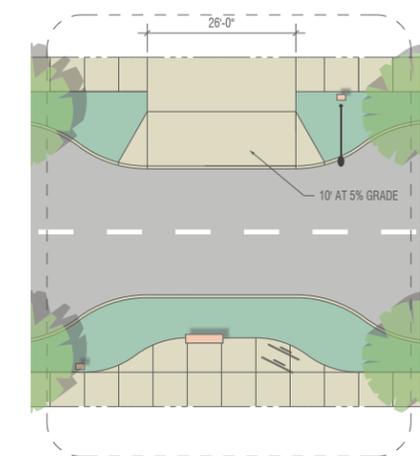
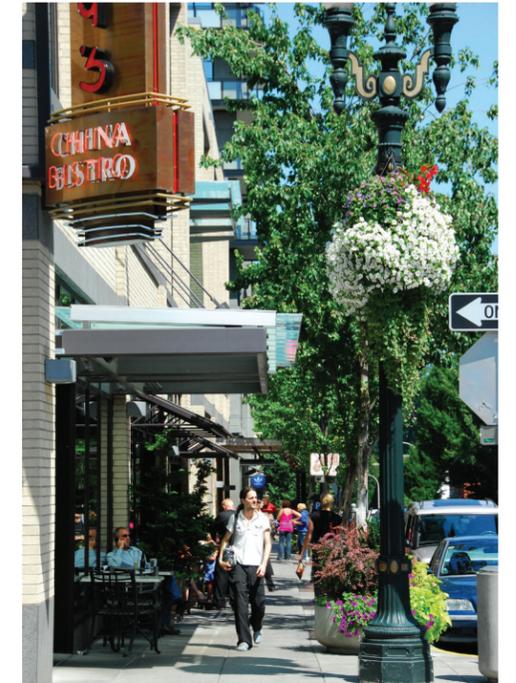


Figure 4.11

## RETAIL STREETS

Retail Streets are intended to be active corridors that support retail by providing wider sidewalks for dining and window shopping, grating trees for extra pedestrian maneuverability, and low furnishings for unobstructed sightlines to store fronts and on-street parking and loading. 130th Avenue NE is designated as a Retail Street in the BelRed Plan. Retail uses here are seen as the type that will serve the emerging high density residential neighborhood and not compete with regional retail in Downtown Bellevue. Smaller scale retail that is pedestrian-friendly will line both sides of 130th Avenue NE. To provide space adequate to serve all needs, a wider right-of-way is proposed. 130th Avenue NE is also a local bicycle corridor that will connect into the larger city-wide bicycle corridor proposed for the NE 15th/16th Street light rail corridor. As such it will include generous bicycle parking in front of businesses.

Because the street trees on Retail Streets will be grated instead of located in large open planters, provisions will need to be made for adequate root and soil volume. A root space protection zone is proposed from the face of adjacent development to the edge of the vehicular travel lane, in which a structural matrix such as Silva Cell will be used to support pavement over a high-quality growing medium.



**Figure 4.12:** Diversity and vibrancy can be found within signage, potted plants, and retail uses spilling onto sidewalk.



**Figure 4.13**

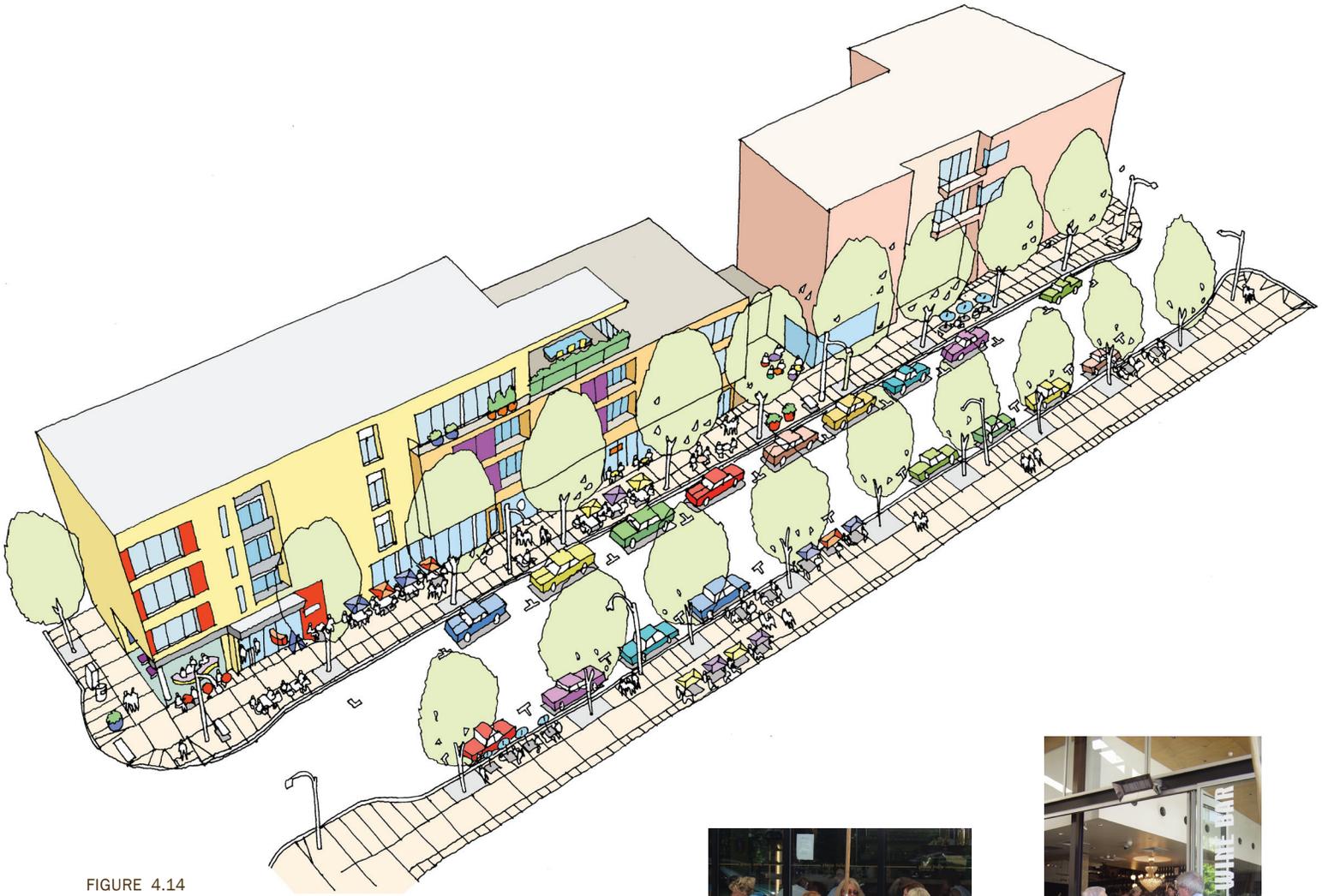


FIGURE 4.14



FIGURE 4.15: Retail edges can open to sidewalks without spilling into the pedestrian zone.



FIGURE 4.17



FIGURE 4.18

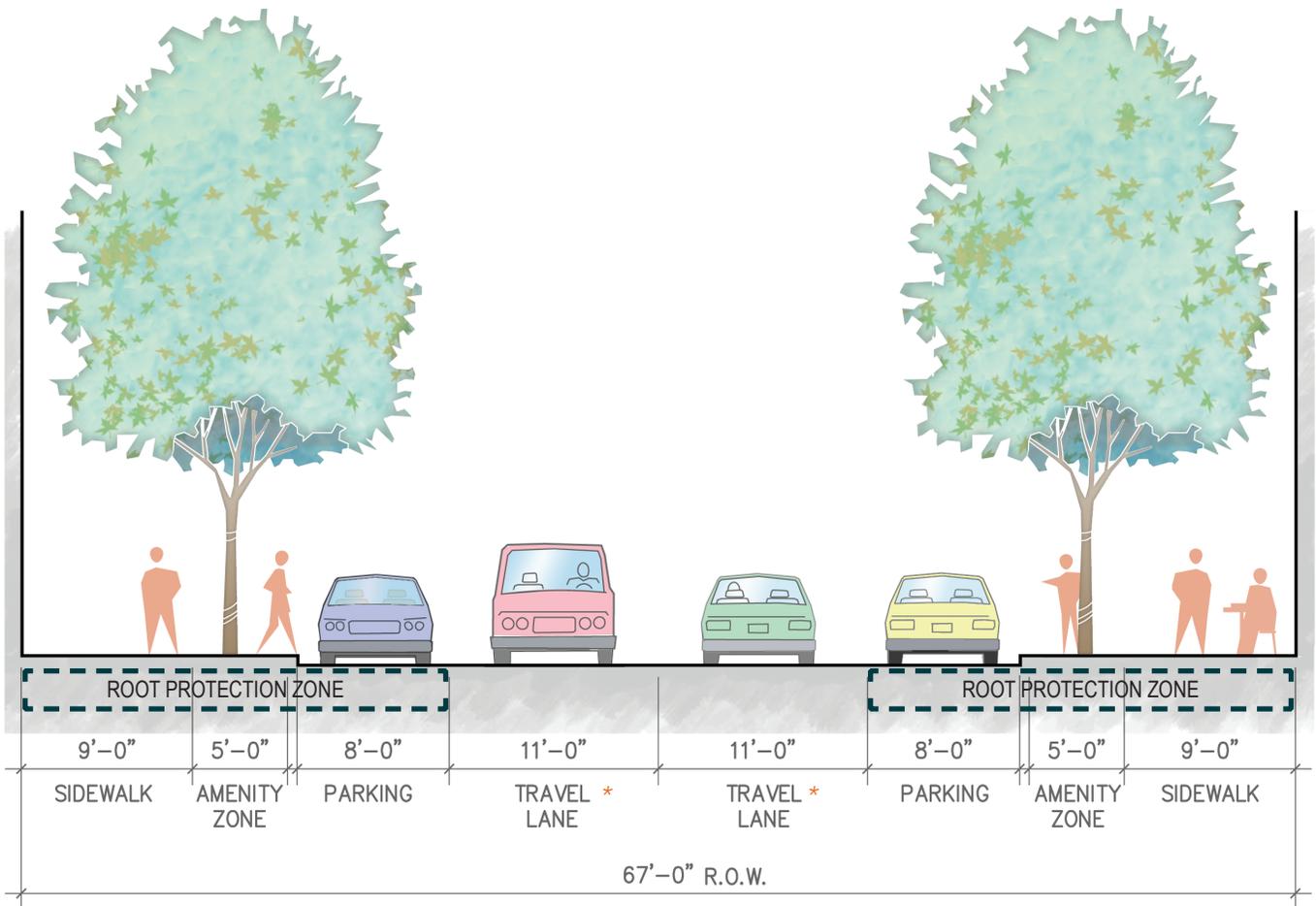


FIGURE 4.16



FIGURE 4.19: Create an active edge with room for tables, strollers, and merchandise.

**Retail Streets Section**



*Note: Retail Streets shall accommodate bicycle traffic, typical section to be determined.*

*\* Add 5' 0" for on-street bicycle facility*

FIGURE 4.20

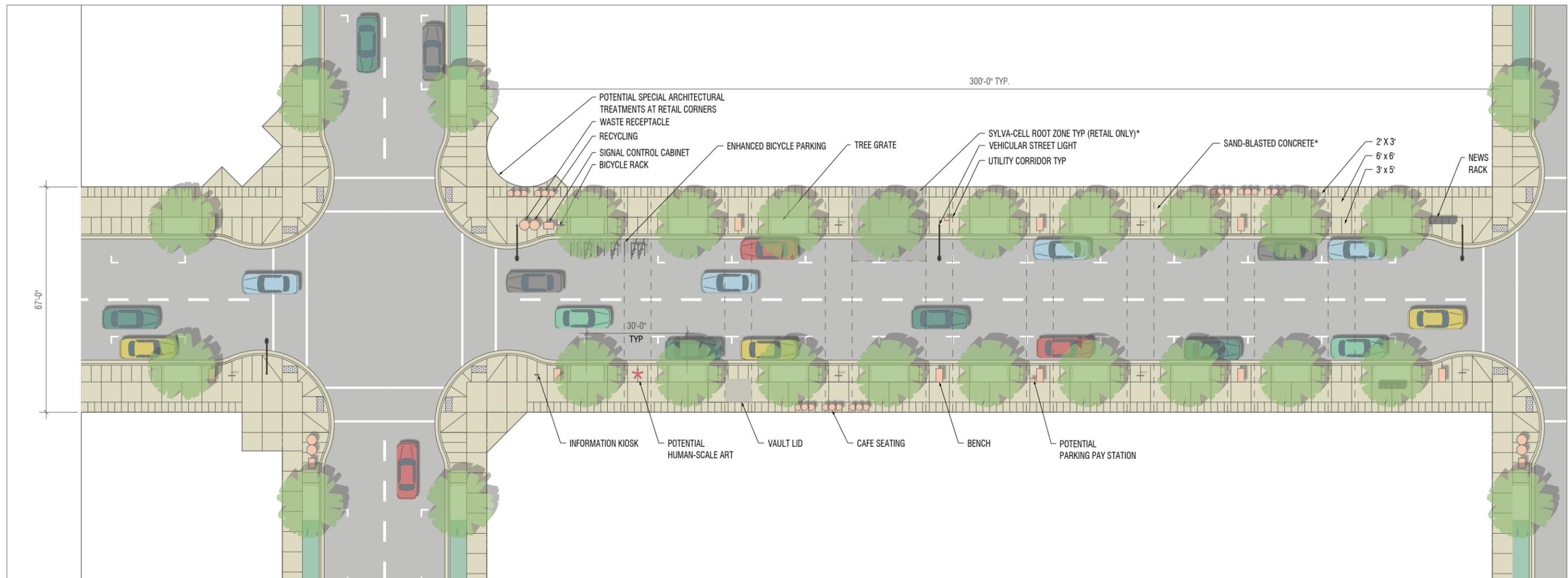


Figure 4.21

**Street Zone Guidelines**

- 3' cafe seating zone.
- 6' pedestrian thru-zone.
- 18" concrete curbside step-out zone.
- 3' curb bulbs at block corners with 21' radius.

**Paving & Scoring**

- Concrete Joints: Saw-cut or trowel at developer's option.
- 2' x 3' concrete joints at cafe seating zone.
- 3' x 5' concrete joints at planter strip zone.

**Lighting**

- Vehicular street lights at middle and corners of blocks, one side of block only, or as required to meet design standards.
- Building lighting by developer.

**Landscape & Furnishings**

- Trees spaced at 30' on center in planting strip.
- 5' x 10' planters with tree grates.
- Waste receptacles within amenity zone, on opposite corners.
- One (potential) parking pay station at every mid-block as shown at edge of sidewalk.
- Benches and bicycle racks distributed through street at random interval as shown. Minimum 3 benches and 6 bike racks per block face

## GREEN STREETS

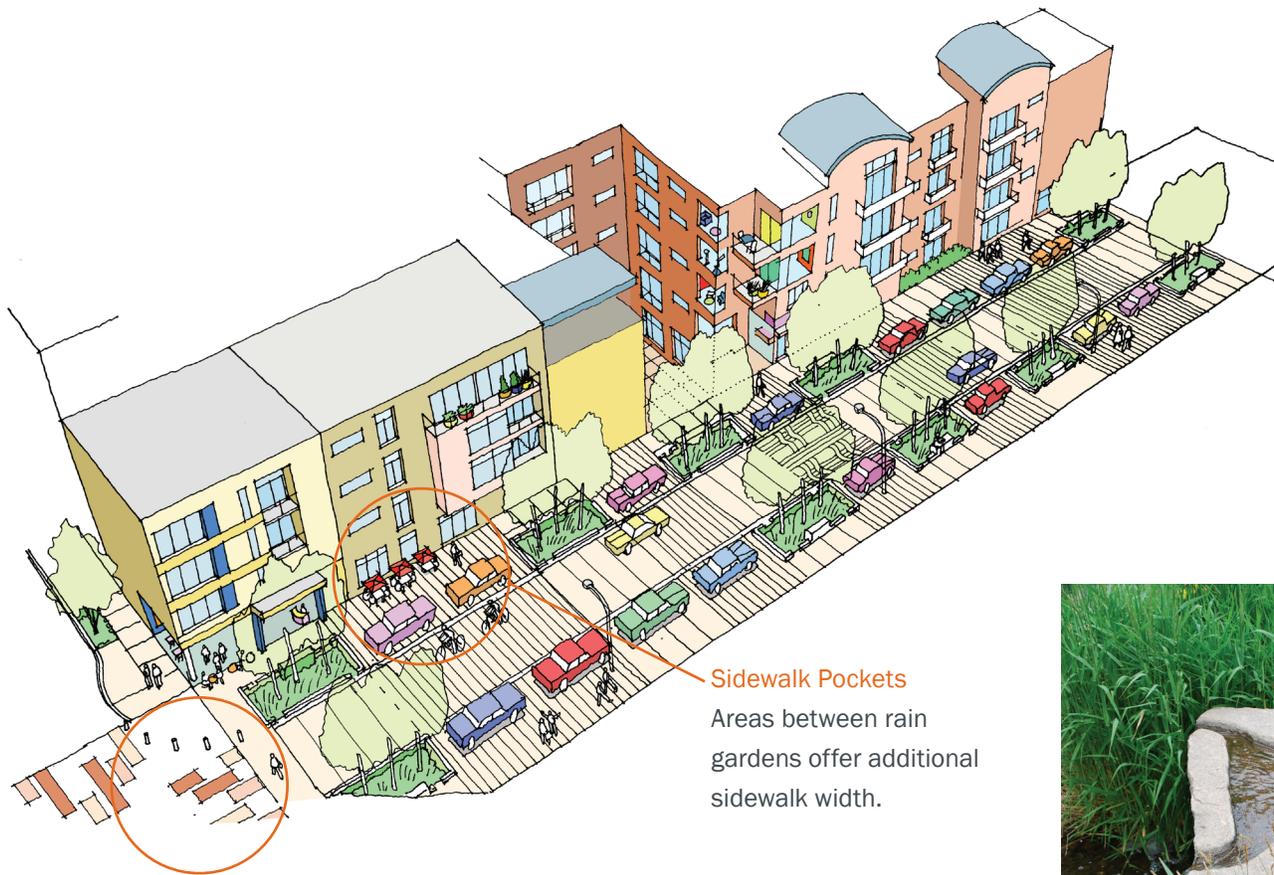
Green Streets are seen as a specific type of local street that supports intensive residential uses, has a traffic-calmed character that is attractive to pedestrians and bicyclists and because of their east-west alignment, act as green connective corridors between subdistricts and riparian open spaces. At intersections on the edge of subdistricts, Green Streets transition to trails as they cross the riparian corridors. The emphasis of the Green Street typology is to put pedestrians and bicycles on equal or greater priority with minor, local automotive traffic, and to employ natural systems to assist with storm water management.

The Green Streets are curbsless environments with paving enhancements that feel plaza-like and could allow temporary closure for a pedestrian-oriented day festival or event. The street is punctuated by asymmetrically placed rain gardens in line with the parking bays. Trees are clumped into irregular groves within rain gardens, reinforcing a more natural extension of landscape from the riparian areas into the neighborhood street grid.

Storm water is conveyed to the rain garden planters along a crease in the pavement which feeds small cascades into the basins. The rain gardens will remove pollutants and suspended solids before returning water to the aquifer. In heavy rainfall overflow structures convey water to the storm sewer system to avoid flooding.



Figure 4.22: A curbsless rich pedestrian environment attracting babies, birds, bikes, and bees



**Sidewalk Pockets**

Areas between rain gardens offer additional sidewalk width.

FIGURE 4.23

**Park Intersections**

Where Green Streets intersect green open spaces; materials are extended through the intersection and into adjacent open space to create an integrated and seamless park entrance.



FIGURE 4.27: Embracing stormwater as a street amenity provides opportunities for fountains and artistic channelization to bring people closer to the water.



FIGURE 4.26: An environment for bikes.



FIGURE 4.24: Rain gardens provide unique color and texture to the streetscape.



FIGURE 4.25: Interconnected infiltration planters for water retention, infiltration, evaporation and transpiration.



FIGURE 4.28: Water is cleaned of suspended solids, heavy metals, nitrogen and phosphorous before entering the sewer system.

**Green Streets Section**

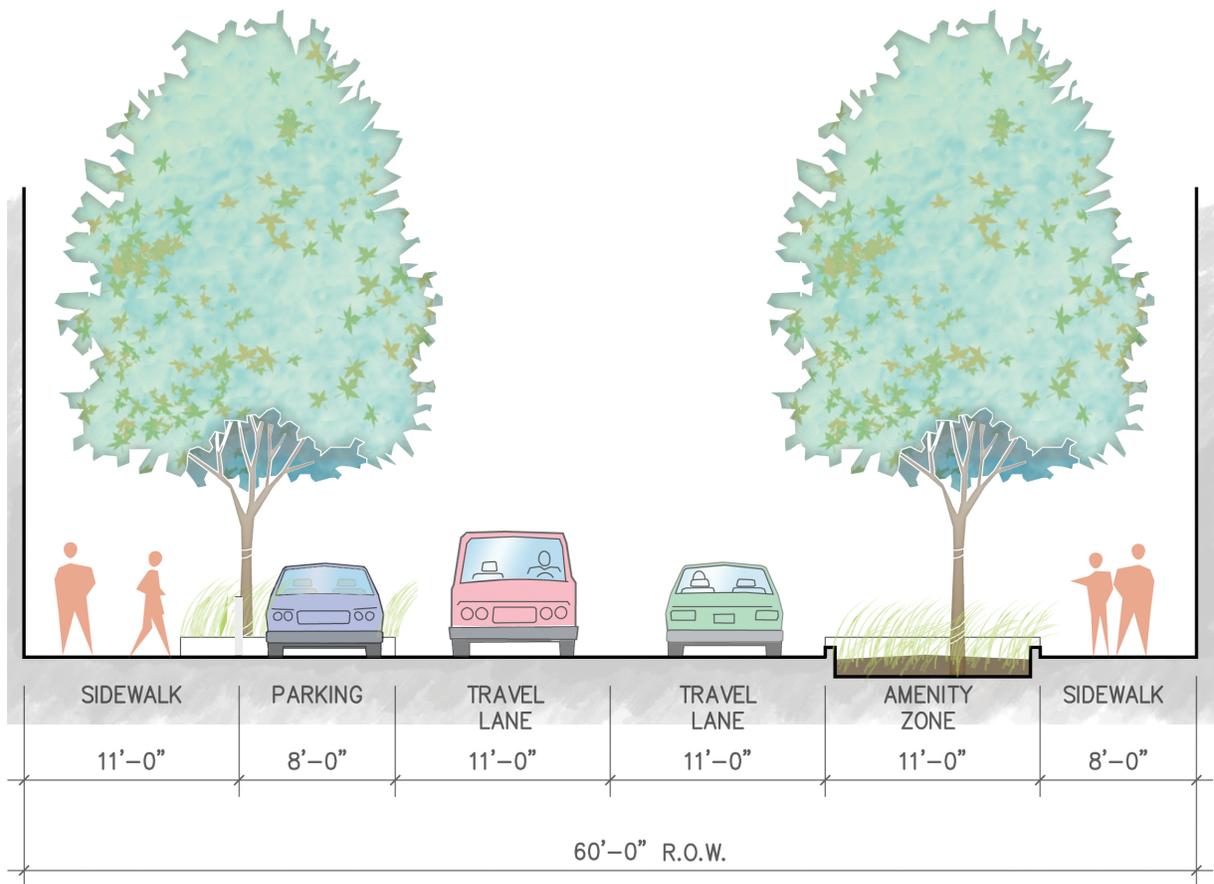


FIGURE 4.29

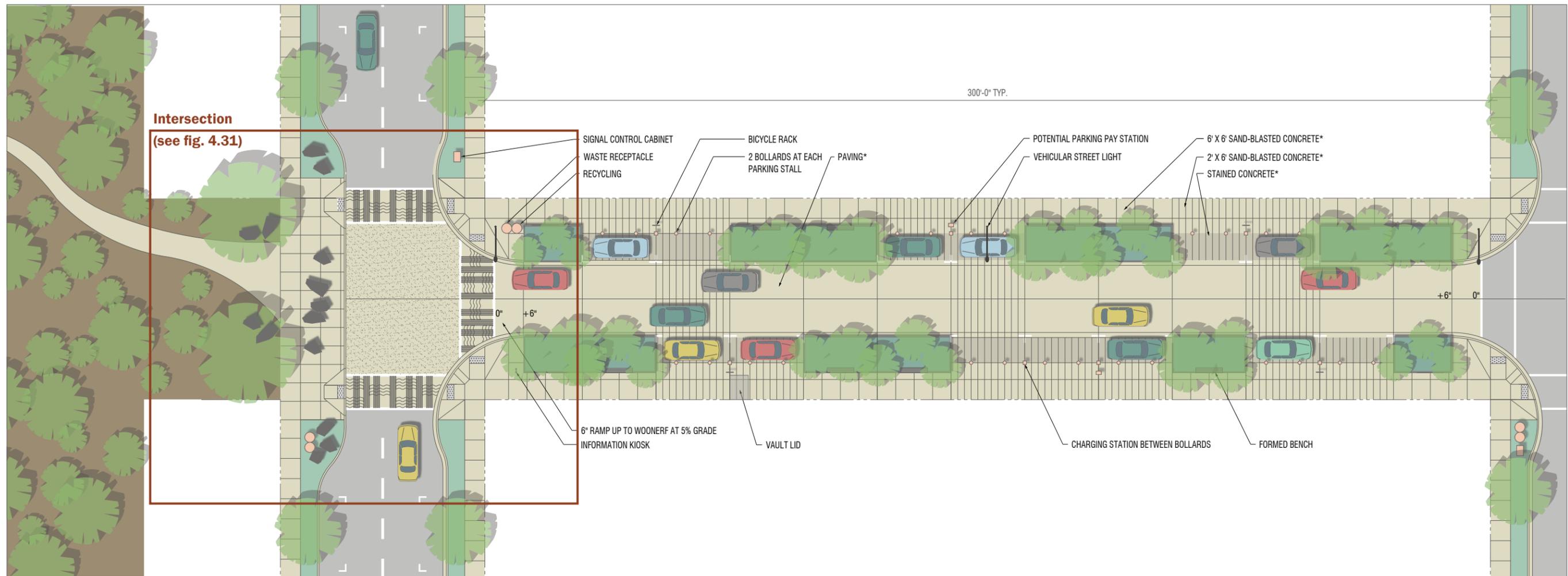


Figure 4.30

**Street Zone Guidelines**

Street, planting zone, and sidewalk are all coplanar. 6" ramp up from perpendicular streets into woonerf environment.  
3' curb bulbs at block corners with 21' radius

**Paving & Scoring**

Concrete Joints: Saw-cut or trowel at developer's option.  
2' x 6' scored joint pattern over 6' x 6' full-depth contraction joints.  
Travel lanes and sidewalk are concrete.  
Parking bays to be charcoal gray concrete.

**Lighting**

Vehicular street lights at center and corners of blocks, one side of block only, or as required to meet design standards.

**Landscape & Furnishings**

Trees planted within raingarden planters at random spacing and number.  
Waste receptacles on opposite corners of intersections.  
One (potential) parking pay station at mid-block as shown at edge of pedestrian thru zone.  
Bicycle racks and potential vehicle charging stations shown at intersection of two parking spaces, typical.  
Two bollards placed at each parking stall to delineate parking zones from sidewalk.  
Found seating integral with raingarden planters (see detail in appendix).

## INTERSECTIONS

The prototypical BelRed intersection features shallow curb bulbs that provide queuing space and shortened crossing distances while still allowing emergency vehicles to make turns without mounting the curb.

Special intersections are proposed where the Green Streets transition to trail as they enter the riparian corridors. These gateways can be framed by large trees, and announced with native boulders which also block vehicular traffic.

### Prototypical Intersection

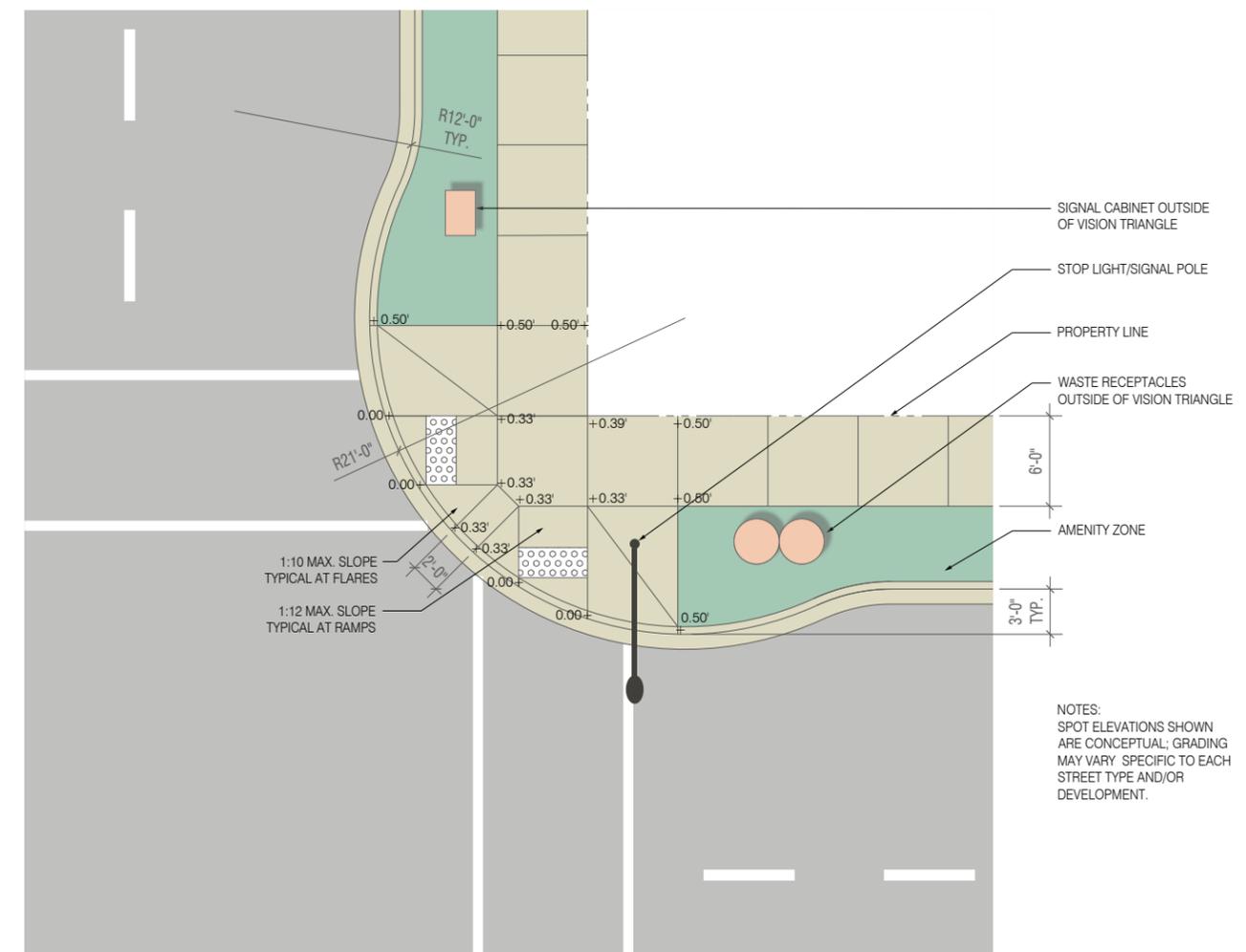


Figure 4.31

Special Intersection at Green Streets

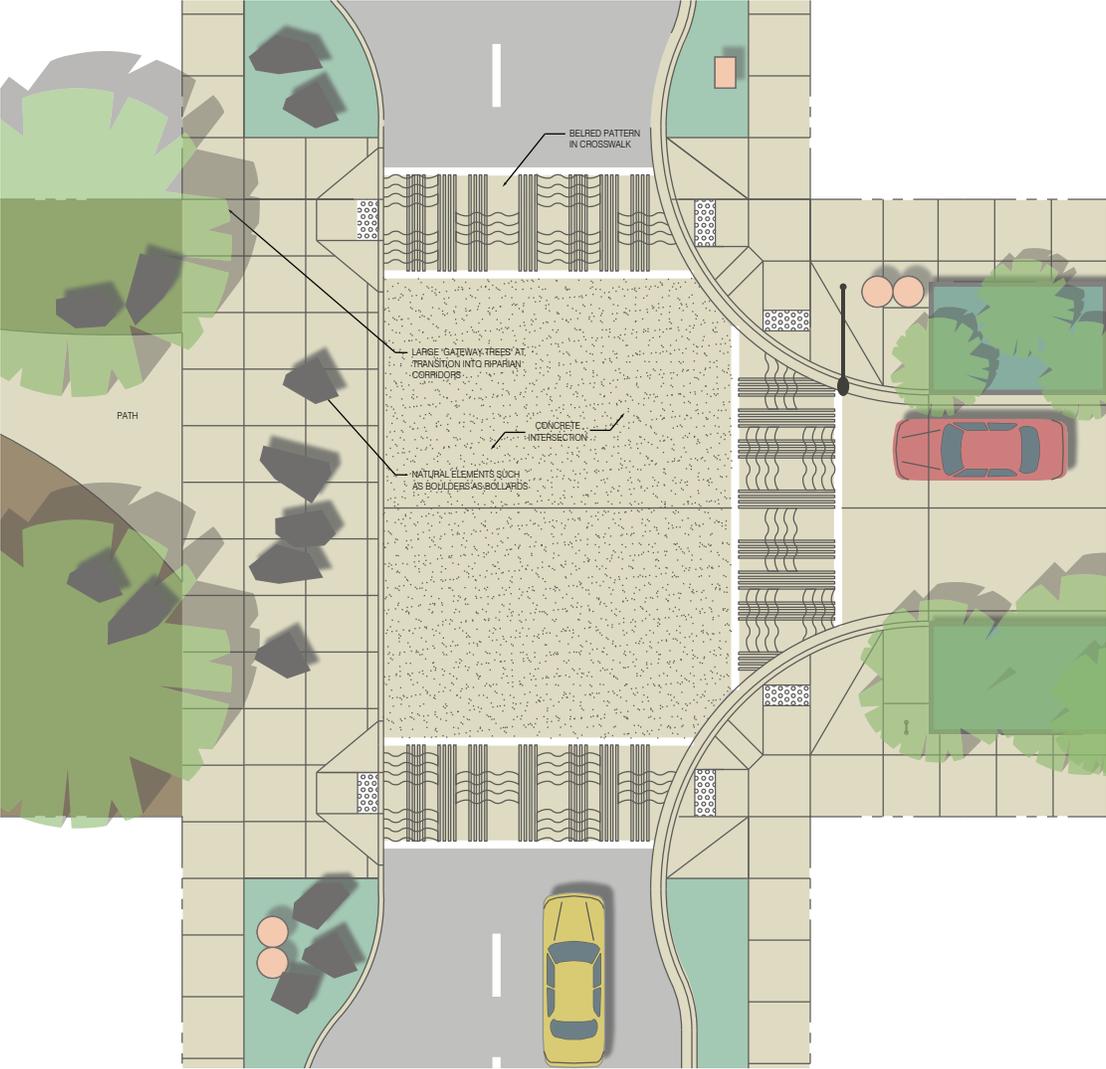


FIGURE 4.32



# Streetscape Elements

## General

In the selection of elements for use in the right-of-way, preference should be given to furnishings that express a mix of industrial and natural character, have simple clean, simple lines, and express the character of their materials. When possible, fasteners, welds, and flanges should be visible.

Materials: drawing from natural or industrial motifs:

- steel
- wood
- concrete
- stone

Finishes: preference should be given to natural or industrial finishes and conditions:

- Corten steel
- galvanized steel
- natural/clear stains or unfinished

Colors: in Corridor-related graphics, or on materials that need to be painted or otherwise coated, colors should come from a traditional industrial range of color tones:



Installation: streetscape elements may be either embed-mounted or surface mounted.

### In This Section

Seating: Backless and “Backed” Benches

Waste Receptacles

Bollards

Bicycle Racks

Vehicular-Scale Lighting

Pedestrian-Scale Lighting

Pedestrian Pavement

Tree Grates

Street Tree Root Zone Protection

Newspaper Boxes

Power and Signal Cabinets

Vault Lids, Manholes, and Grates

Wayfinding



## SEATING

Seating may be either in the form of benches or “found seating”.

Benches on Local Streets and Green Streets shall have backs.

Benches on retail streets shall be backless.

Found seating is encouraged along development frontages. A cast in place concrete seating block is recommended at rain garden cells on Green Streets (*see detail in appendix d.*).





### **Backless Benches**

*Company:* **Nu by Landscape Forms**  
*Dimensions:* 26" x 32" x 102"  
*Materials:* Jarrah seat, galvanized steel frame  
  
*Contact:* [timg@landscapeforms.com](mailto:timg@landscapeforms.com)



**PREFERRED**  
**Backless Bench**



*Company:* **Wood Bloc Bench by Factory Furniture**

*Dimensions:* 6'-5" x 1'-5" x 1'-6"

*Materials:* FSC certified tropical hardwood timber on stainless steel frame, natural finish

*Contact:* mail@obrienandsons.com





*Company:* **Argo by Hess America**

*Dimensions:* 3' x 7'4" x 1'10"

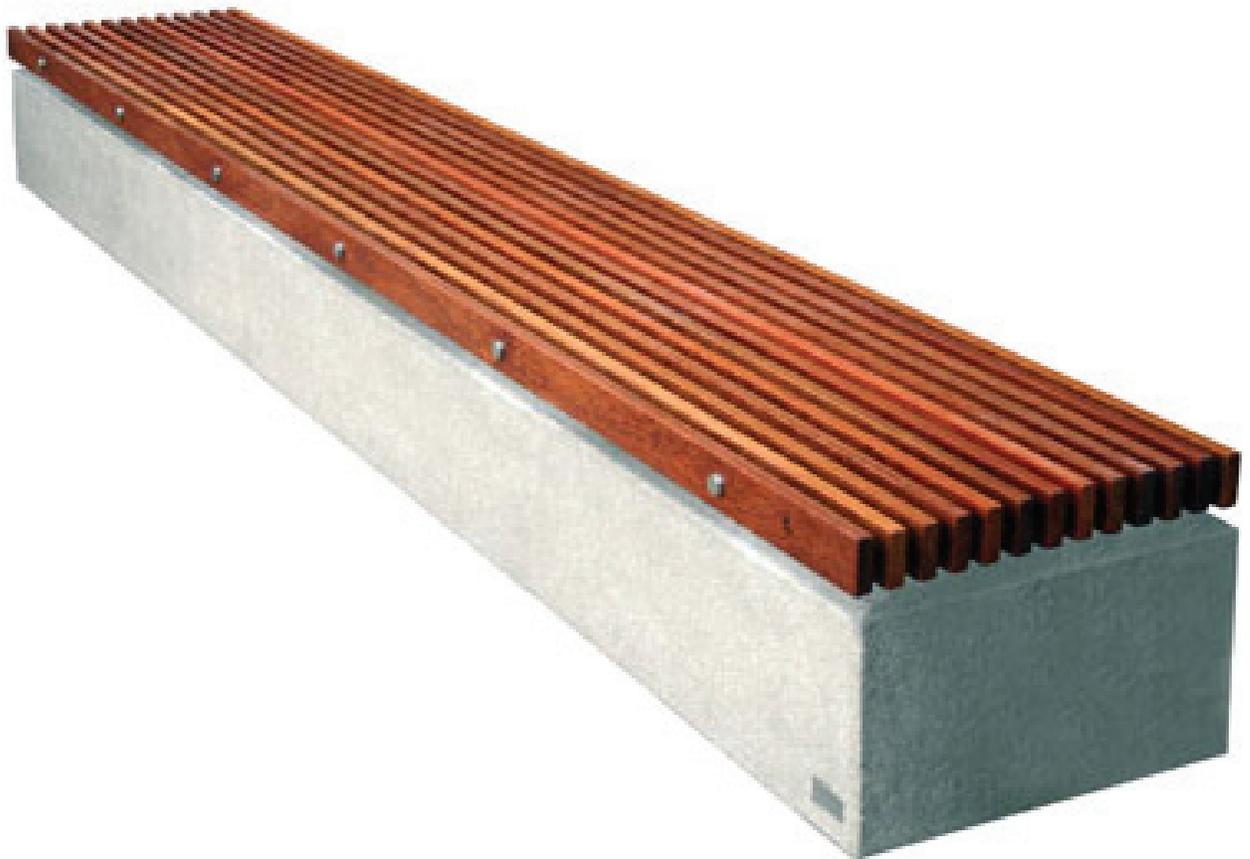
*Materials:* Compressed beech planks, steel frame, hot-dip galvanized and painted

*Contact:* Pacific Lighting Systems Inc. / 206.323.2200





*Company:* **SOCA by Factory Furniture**  
*Dimensions:* Variable length x 27" x 18"  
*Materials:* Concrete and FSC certified tropical hardwood  
  
*Contact:* mail@obrienandsons.com





*Company:* **Broadmoor Bench by Urban Hardwoods**

*Dimensions:* 176" x 30" x 19"

*Materials:* Salvaged timber

*Contact:* [bryan@urbanhardwoods.com](mailto:bryan@urbanhardwoods.com)



**PREFERRED**  
**Green Street Park Entrance**



**“Backed” Benches**

*Company:* **Nu by Landscape Forms**  
*Dimensions:* 26” x 32” x 102”  
*Materials:* Jarrah seat, galvanized steel frame  
  
*Contact:* [timg@landscapeforms.com](mailto:timg@landscapeforms.com)



**PREFERRED**  
**“Backed” Bench**



*Company:* **Promenade Modul Bench by Union**  
*Dimensions:* 260cm (or 300) x 48cm x 46cm  
*Materials:* Steel frame, hot-dip galvanized and color-coated gray  
  
*Contact:* [info@crestcontracts.co.uk](mailto:info@crestcontracts.co.uk)





*Company:* **Bancal by Landscape Forms**

*Dimensions:* 88" x 23" x 31"

*Materials:* lpe seat and back, steel frame

*Contact:* [timg@landscapeforms.com](mailto:timg@landscapeforms.com)





*Company:* **SOCA by Factory Furniture**  
*Dimensions:* variable length x 27" x 18"  
*Materials:* concrete and FSC certified tropical hardwood

*Contact:* mail@obrienandsons.com





*Company:* **Trapezio by Landscape Forms**

*Dimensions:* 212" x 32" x 27"

*Materials:* Unfinished Alaskan Yellow Cedar on hot-dip galvanized frame

*Contact:* [timg@landscapeforms.com](mailto:timg@landscapeforms.com)





## WASTE RECEPTACLES

Containers for trash and mixed recycling are to be provided.

Receptacles should have lids, or be otherwise closed to the sky to prevent accumulation of rainwater.

Side-opening or tilting receptacles are preferred for easier removal of waste bags.





*Company:* **ROUND Bin by Factory Furniture**

*Dimensions:* 19 gallons

*Materials:* aluminium top, stainless steel body

*Contact:* mail@obrienandsons.com



**PREFERRED**  
**Waste Receptacle**



*Company:* **Ratio Litter Bin by Union**  
*Dimensions:* 475mm x 350mm x 1500mm  
*Materials:* Stainless steel or hot-dipped galvanized.

*Contact:* [info@crestcontracts.co.uk](mailto:info@crestcontracts.co.uk)





*Company:* **T-55 by Urban Accessories**

*Dimensions:*

*Materials:* Steel

*Contact:* Architecreation / 206.932.4730





*Company:* **Steely Can by Landscape Forms**

*Dimensions:* 39 gallons

*Materials:* Stainless steel

*Contact:* [timg@landscapeforms.com](mailto:timg@landscapeforms.com)





## **BOLLARDS**

*Company:* **Toro by Hess America**

*Dimensions:* 36"

*Materials:* painted steel

*Contact:* Pacific Lighting Systems Inc. / 206.323.2200



***PREFERRED***  
***Bollard***



*Company:* **R-8400 Bollard by Reliance Foundry Co.**

*Dimensions:* 36" tall, 9-1/4" diameter

*Materials:* Powder-coated steel

*Contact:* [info@bollards.ca](mailto:info@bollards.ca)





## **BICYCLE RACKS**

Bicycle racks may be vinyl coated.

Racks should have two points of contact with bikes.

For use on sidewalks and in enhanced bicycle parking on Retail Streets.





*Company:* **Ring by Landscape Forms**

*Dimensions:* 2 bikes each

*Materials:* Stainless steel

*Contact:* [timg@landscapeforms.com](mailto:timg@landscapeforms.com)



**PREFERRED**  
**Bicycle Rack**



*Company:* **Mipos Bicycle Rack by Crest**  
*Dimensions:* 500mm x 50mm x 1200mm  
*Materials:* Stainless steel or hot-dipped galvanized

*Contact:* [info@crestcontracts.co.uk](mailto:info@crestcontracts.co.uk)





*Company:* **Ultra Bicycle Stand by Crest**  
*Dimensions:* 1000mm x 80mm x 900mm  
*Materials:* Stainless steel or hot-dipped galvanized

*Contact:* [info@crestcontracts.co.uk](mailto:info@crestcontracts.co.uk)





## TREE GRATES

Tree grates are not to be used on Local Streets or Green Streets.  
Tree grates are required on Retail Streets.

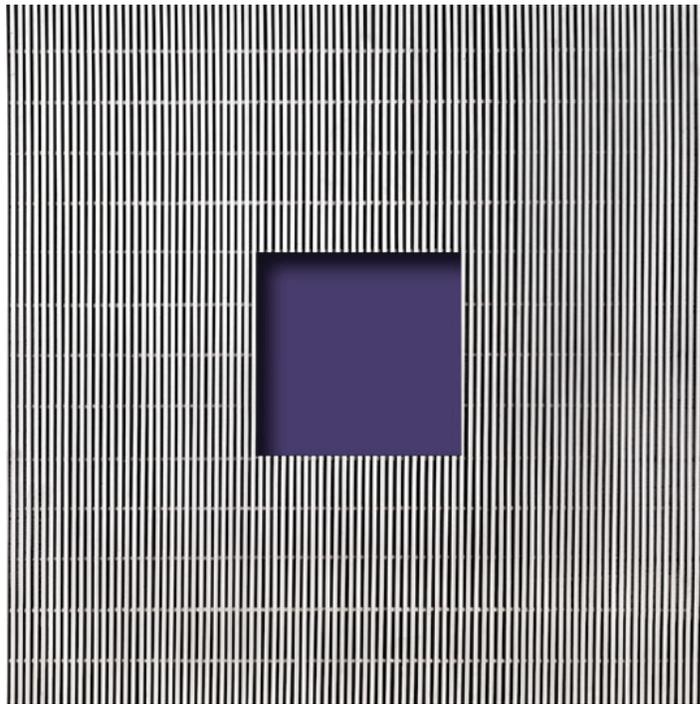
Grates shall be ADA compliant walkable surfaces

*Company:* **Jamison by Urban Accessories**

*Dimensions:* 5' x 10'

*Materials:* Aluminum

*Contact:* Architecreation / 206.932.4730



**PREFERRED**  
**Tree Grate**



## **STREET TREE ROOT ZONE PROTECTION**

Street trees on Retail Streets (i.e. street trees in grates) shall have their root zones protected by a three dimensional structural matrix product such as Silva Cell.

Minimum depth of the system shall be 32". Width and length of installation per prototypical plans in this document.





## NEWSPAPER BOXES

Newspaper boxes are to be provided on Retail Streets.

Boxes are to consolidate newspapers from various vendors to reduce clutter on the sidewalk.

Boxes should accommodate a minimum of four newspapers.





## **POWER AND SIGNAL CABINETS**

Utility-related cabinets that occur in the right-of-way should not call attention to themselves, and therefore should not be decorated.

Cabinets should have a high quality metal finish.





## VAULT LIDS, MANHOLES, AND GRATES

An opportunity exists to apply a BelRed graphic to lids and grates in the right-of-way. This can be implemented after Corridor graphics are formalized.





## WAYFINDING

Unique wayfinding elements that feature a BelRed graphic and typeface may be provided. These can be in the form of signage and kiosks, and/or through embedded information and patterns in the sidewalk. Signage and wayfinding standards should be developed after Corridor graphics are formalized.





## VEHICULAR-SCALE LIGHTING

Vehicular-scale lighting shall be GE Evolve LED Series Area Light - Medium Thin Profile, Dimmable, Mounting Option 'B', color to be coordinated with pole.

Product ID: EAMT

Pole shall be round, height and spacing dependent on conditions specific to development. Finish to be 3 mils of polyester powder topcoat over 3 mils of epoxy powder primer, for a total thickness of 6 mils. Color to be black or dark bronze.

Other fixtures may be approved on a case-by-case basis.

Proposed fixtures shall be in keeping with the modern/industrial design themes proposed in this document. The fixture must be tested to LM-79 and LM-80 standards and shall be manufactured in the USA or as otherwise allowed by funding restrictions.



**PREFERRED**  
*Vehicular-Scale Light Fixture*



## **PEDESTRIAN-SCALE LIGHTING**

Pole-mounted pedestrian-scale lighting is not required for development in the BelRed Corridor.

Lighting of the pedestrian realm is to be provided by fixtures mounted to adjacent development.

Pedestrian realm lighting where adjacent development is set back from the sidewalk is to be coordinated with the City of Bellevue Transportation Department on a case-by-case basis.

## **PEDESTRIAN PAVEMENT**

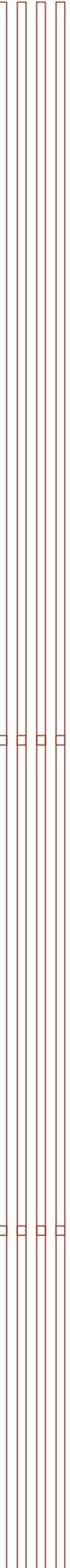
Pedestrian pavements are to be cast-in-place concrete. Unit pavers are not to be used within the right-of-way.

Finishes are to comply with the Transportation Design Manual, except at areas of accent pavement where concrete may have a medium sandblast finish and/or saw-cut joints.

Accent areas of concrete pavement may be colored integrally. Top-dressed color may be used in addition to integral color, but not in lieu of it.



# Appendix



## a. Graphic Considerations

As the identity of the Corridor continues to evolve, a graphic may be developed to further enhance and make identifiable the character of the Corridor. A typeface may also be selected so that Corridor's character can be expressed in print. The simplicity and clarity of Asian graphic motifs are a logical fit, particularly in their ability to be arranged in repeatable patterns. This repeatability is desirable in its ability to be applied across a number of surfaces of varying scale. These graphics may be applied in the streetscape to wayfinding components, vault and manhole covers, pavement at intersections, and electronic media to reinforce the sense of place.

### Color Selection

Colors are pulled from the inspirational photos below, showcasing the beautiful side of Industry. These colors represent an industrial mood with the undertones of modern vibrance.

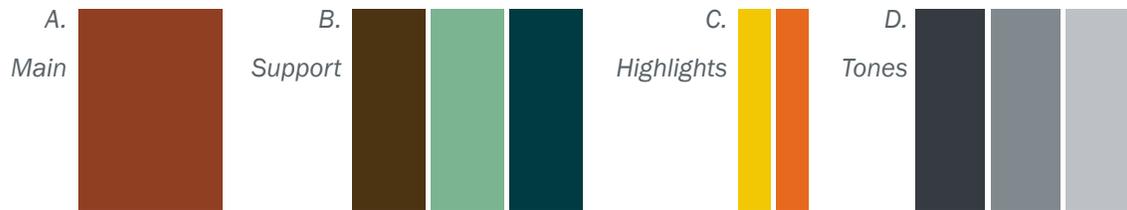


FIGURE 6.1

Color Inspiration images.



FIGURE 6.2

### Font Selection and Preferred Typographic Applications

A number of fonts were discussed during the workshop process for this plan. Preference was expressed for a very clean, modern-looking typography. This font would be utilized alongside the graphic, in signage, and any other media representing BelRed.

Font Selected: **Franklin Gothic**

Typographic Application:

**BelRed**

FIGURE 6.3

BelRed

FIGURE 6.4

### Graphic Inspiration

Easy modification and repetition were seen as desirable qualities for the graphic. The graphic may be applied to printed and digital media and to physical forms in the streetscape. In the example below, a single fish can take on many forms of logo or pattern.

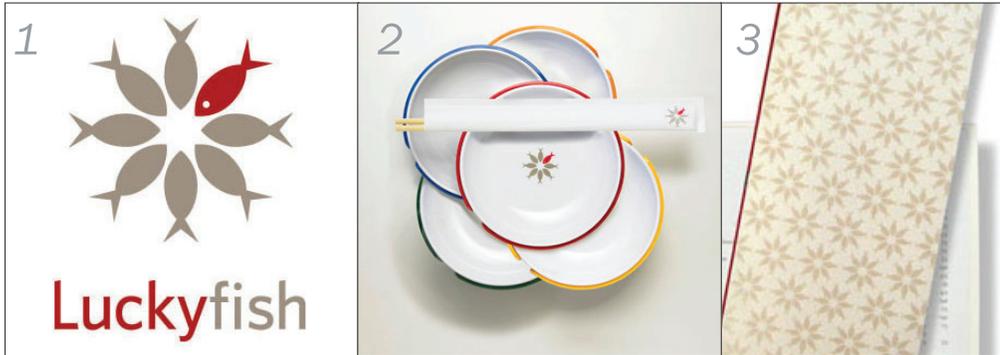


FIGURE 6.5

Graphic inspirations that incorporates modern, industrial and natural themes in visually unique ways.

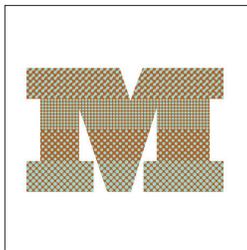


FIGURE 6.6

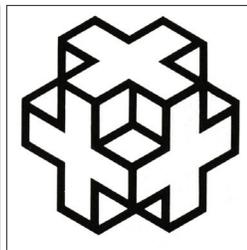


FIGURE 6.7



FIGURE 6.8

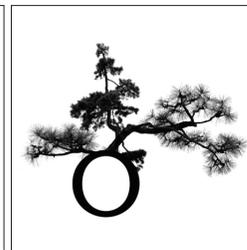


FIGURE 6.9

### Pattern Inspiration

Pattern is an important element of the BelRed graphic, expressing BelRed's identity when appropriate and creating a BelRed "pattern language".

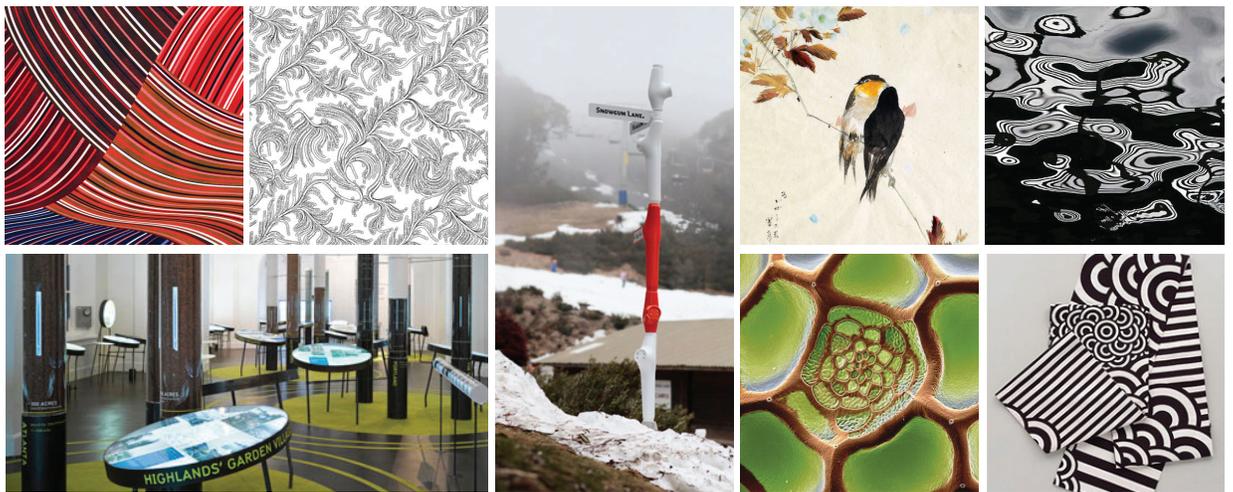


FIGURE 6.10

### Pattern and Graphic Possibilities

Most of the options explored in the workshops represent all three of BelRed's contexts: Industrial, Modern, and Natural. Modern is shown through the typographic application of font style and weight. The graphic expresses the Natural and/or Industrial context of the Corridor.

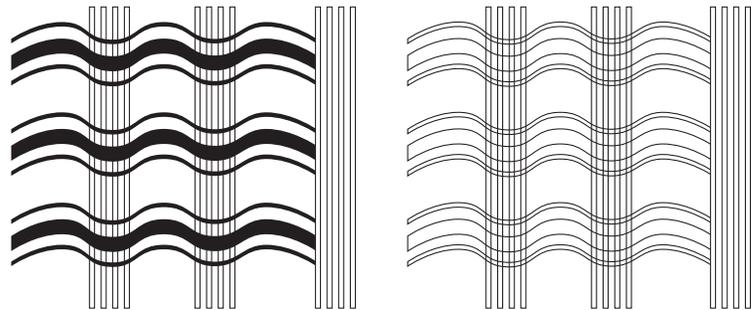
### Graphic Options Explored



#### *Industrial and Natural*

*This pattern merges the repetition and regularity of industry to the sinuous forms of BelRed's riparian corridors.*

FIGURE 6.11



#### *Industrial and Natural*

*Branches represent the interconnected nodes of BelRed in a form appropriate to the Corridor's system of creeks, parks, and wetlands.*

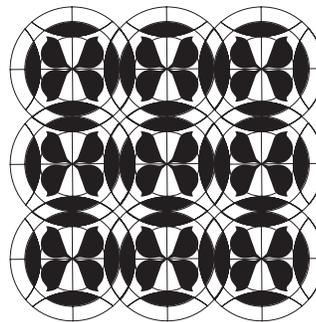
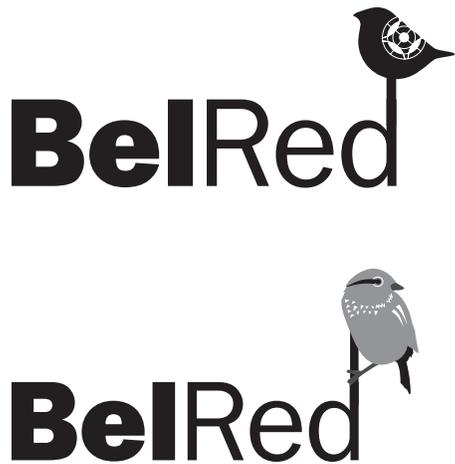
FIGURE 6.12



**Natural**

The leaf represents the natural context of the corridor as well as its growth as a place.

FIGURE 6.13



**Natural and Industrial**

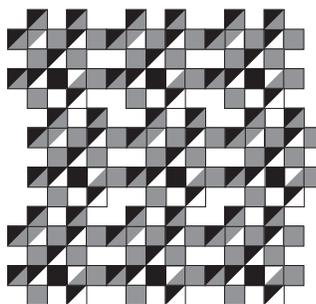
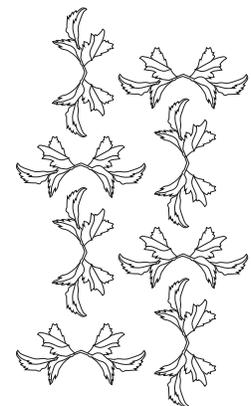
The bird is a representation of the corridor's Japanese history and its restored future as habitat for the Song Sparrow. The pattern is industrial in character but also evocative of ripples in water.

FIGURE 6.14

**Natural**

The plant represents new growth. The growth of BelRed through development, transit, and creation of green spaces.

FIGURE 6.15



**Industrial**

The triangles, squares, and hard angles represent the industrial and modern aspects of the Corridor to the extreme.

FIGURE 6.16

## b. Graphic Application

### Examples of Wayfinding

Wayfinding should be inclusive to all ages with preferably some sort of interaction. Application should be identifiable but not dominating or “Disneyland-ish”, keeping to the context of each street type.



FIGURE 6.17

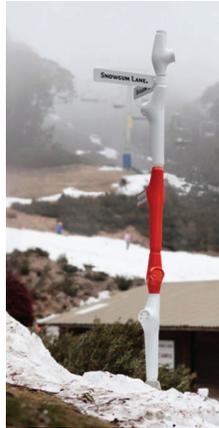


FIGURE 6.18



FIGURE 6.19

### Examples of Pattern Applications



FIGURE 6.20



FIGURE 6.21



FIGURE 6.22



FIGURE 6.23

## GRAPHIC APPLICATION POSSIBILITIES

### Manhole Cover

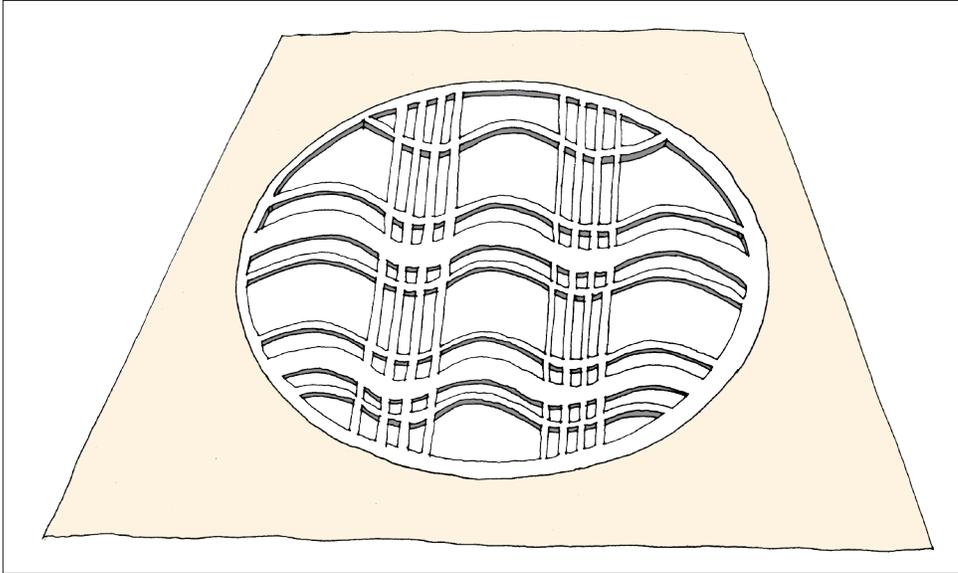


FIGURE 6.24

### Wayfinding Kiosk

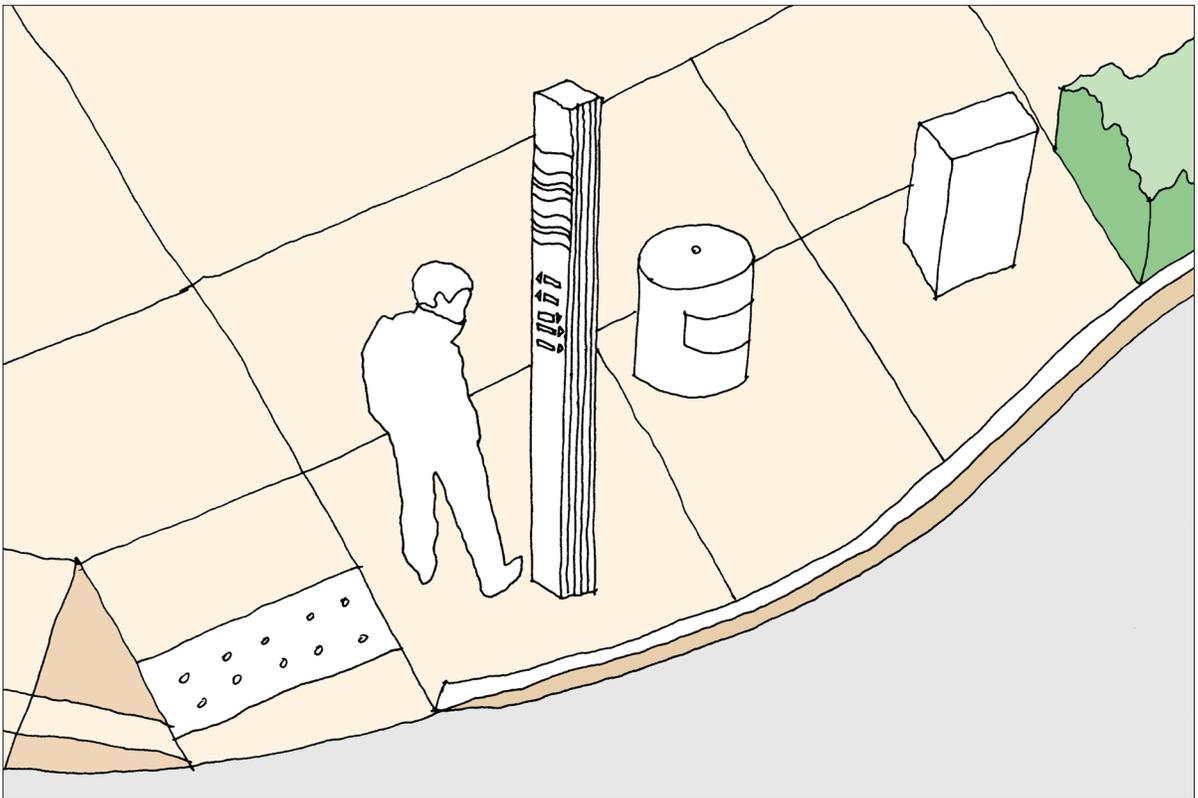


FIGURE 6.25

### Sidewalk Inlay

The example shown below is just one way a BelRed graphic inlay could be applied to pavement in the Corridor. The diagrams below show other placement possibilities.

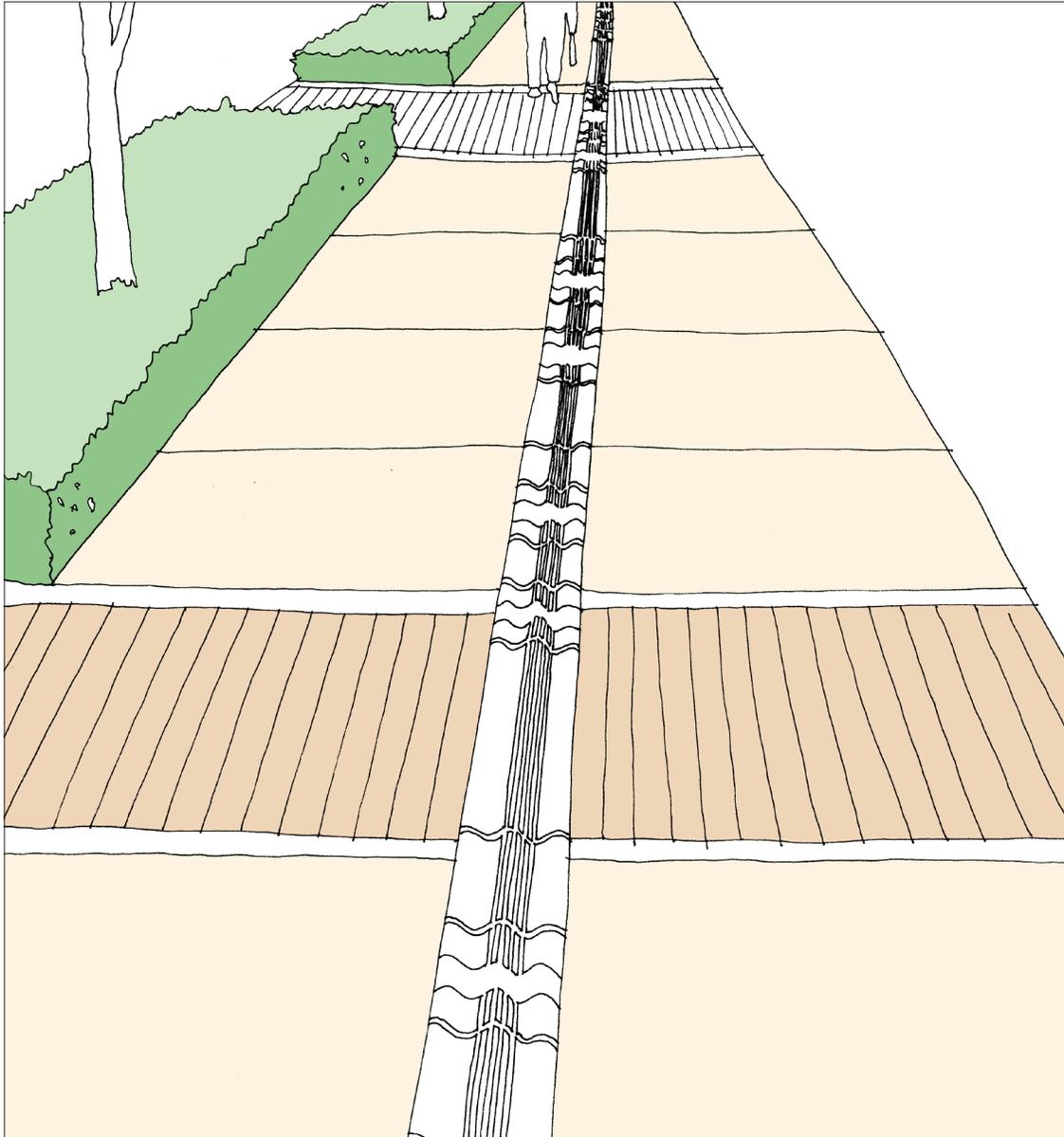


FIGURE 6.26

### Green Streets

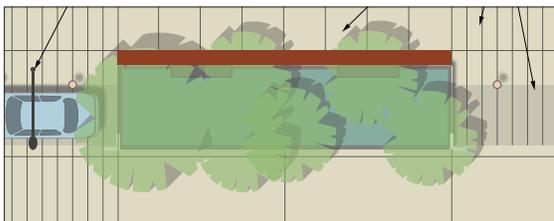


FIGURE 6.27

### Retail Streets

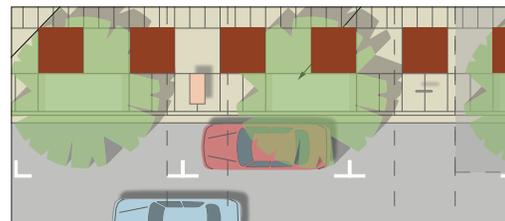


FIGURE 6.28

**Walkway Wayfinding**

*These graphics could be a permanent inlay or a temporary application.*

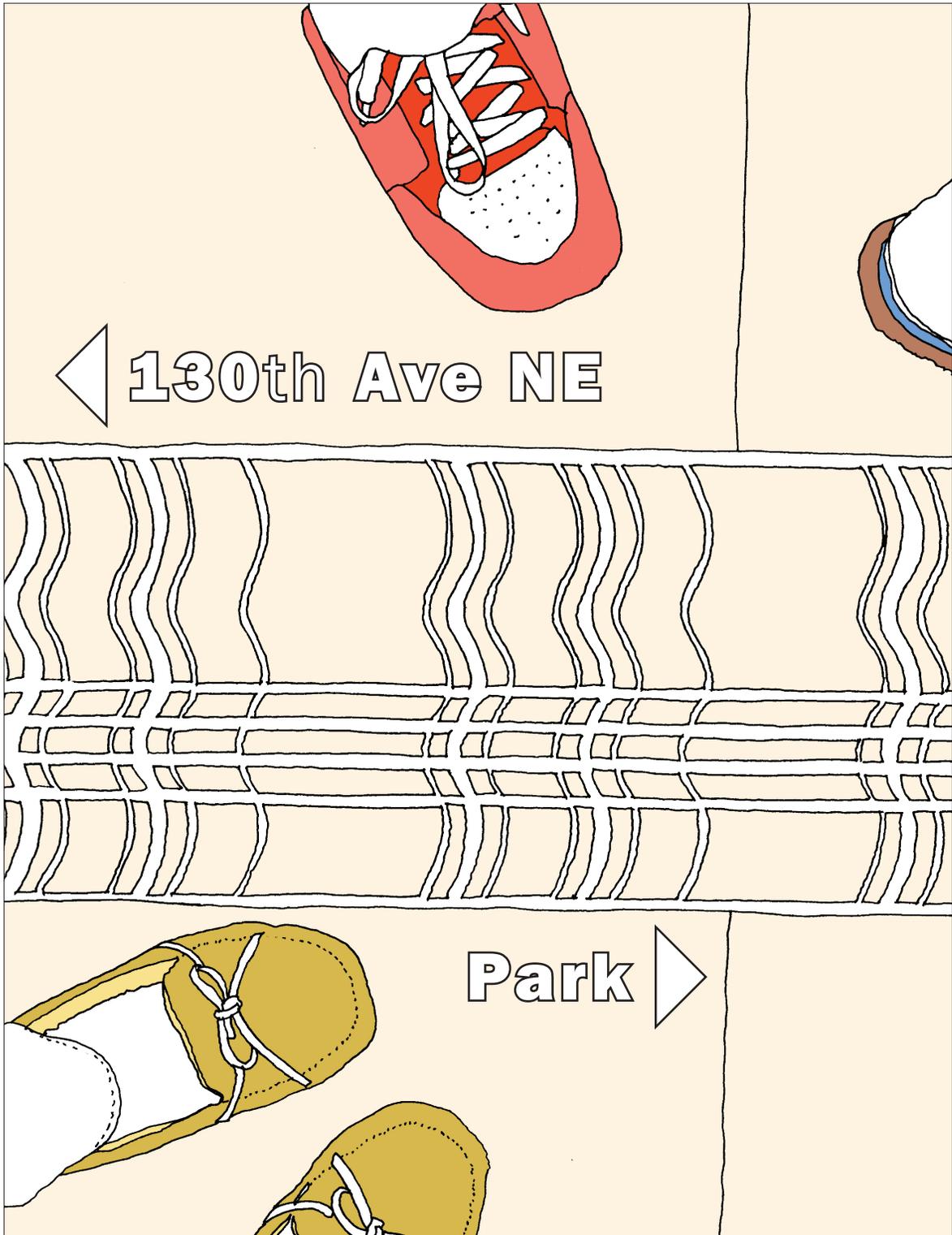


FIGURE 6.29

## Public Art Considerations

Several nodes, or districts, will develop along the Corridor – the Spring District, a residential and retail district at 130th Avenue Northeast, and an Arts District east of Goff Creek. Public art will play a role in creating unique identities for each district, as well as for the Corridor as a whole.



FIGURE 6.30: A rich heritage of the region offers opportunities to celebrate our earliest and diverse cultures.



FIGURE 6.31: Public art works of significant size provide an intermediate scale between humans and buildings, and create landmarks.



FIGURE 6.32: Conceptual artworks encourage us to think of our past, our lives, our futures and the relationship of cities.

### SPRING DISTRICT: PARKS, OPEN SPACES AND PLACES TO GATHER

The recommendations made below pertain to a series of open spaces and public right-of-ways envisioned in the Spring District Concept Plan. These recommendations, and their underlying concepts, should be carried forward and adjusted to the Spring District design as it evolves.

#### Park Adjacent to Residential Development

This generous open space has opportunities to provide intimate as well as larger gathering spaces. Appropriately scaled sculptural works can provide a sense of scale, identity, place making and wonderment.

**Recommendation:** A diversity of works should be commissioned for the Half-Round Park - works that utilize light, a formal sculpture garden complete with figurative works, that spring forth from native cultures, etc should be considered. Conceptual drivers for the work should be focused on content.

#### Entry to Sports Field

The potential exists for this entry to be one of the most widely used pedestrian accesses in and out of the development.

**Recommendation:** An appropriately scaled cultural amenity should be commissioned at this location. Consideration should be given to collaborations between landscape architects and artists, native historians and horticulturists, etc. The focus for this work should be the experience one has in the movement through it rather than drive-by art.

#### Entry Plazas

The Spring District envisions opportunity scaled, open plazas that create a welcome mat at each building's front door, clearly defining the transition from public to private and from outside to inside. These entry plazas have the opportunity to become a discernable and diverse collection of urban scaled "front yards". A public art program can more fully define place and assist with wayfinding. Each front yard can define a unique experience, and significant works of public art will reinforce the identities of these places.

#### Recommendation:

The proposed Spring District Public Art Master Plan should identify scope, process, budget, design criteria and conceptual intent for each plaza project, and if desired, an overarching theme for the district. Each project should be fully integrated in its setting and engineering, and appropriate for the site in which it resides. These works will only be successful within the district if they are of a scale to create a significant presence within the development.



FIGURE 6.33: *Not all public art need be permanent. Many artists working today seek out opportunities to engage the public in a real-time setting.*

### Temporal Projects

Art need not be a permanent thing, nor is it always visually based. Poetry slams, string quartets, film exhibitions and dance performances can do much to build a special quality within communities.

#### Recommendation:

The Spring District Public Art Master Plan should discuss the potential to create specific locations within the development where these events might take place, and then develop an ongoing structure of programming and funding to facilitate their development and occurrence. Opportunities for interior as well as exterior spaces should be explored.

## SPRING DISTRICT: RIGHTS OF WAYS

### 120th and 124th Avenues

The treatment of the Spring District will dictate to what degree it is integrated to the Corridor as a whole.

#### Recommendation:

To ensure that these edges remain permeable, inviting and linked to the extended community, a Design Team Artist should be hired to collaborate with the Spring District design team to develop the landscaping and right-of-way treatments.

### Sidewalks Internal to the Spring District

The Spring District may be developed in phases, but public art can help create a cohesive sense of place over the course of its development. Such art elements, located on or in the sidewalks, may be fine-grained and ever-evolving, permeating the entire district.

#### Recommendation:

A regional author may be commissioned to write a novel, portions of which may be etched in granite panels and pavers set into the sidewalks of the Spring District. The panels may convey a sentence, or sometimes a series of pages, to add a sense of narrative discovery and wonder to the District. In conjunction with text in the sidewalks a web site may be created which includes the entire novel for download onto personal electronic devices such as phones or pads, allowing the novel to include illustrations, new chapters, etc. This text may find its way into the entry plazas and other open spaces in the District as well.

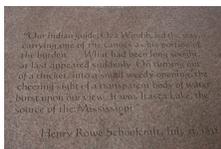


FIGURE 6.34: *Text and language art can bring a sense of narrative to the public realm.*



## TRANSITIONS BETWEEN DISTRICTS: LIGHT RAIL CORRIDOR AND STATIONS

### Transitions Between Districts

Sound Transit has successfully incorporated public art at its existing rail stations and alignments. There will be similar opportunities within the BelRed Corridor for public art that announces significant transitions between districts within the Corridor. Transitions between the Spring District, 130th District, and the arts district may be announced through elements such as arches, gates, and lighting.

#### Recommendation:

Meet with the Sound Transit Art Program Director to develop a cooperative plan that leads to the creation of these transitional elements.



FIGURE 6.35: Works of scale within the transit alignment announce the station and create wayfinding elements within the district.

### Light Rail Stations

Art can be a wayfinding device for locating light rail stations. Additionally, art can be integrated directly into the station architecture and system elements so that the station reflects the character of the neighborhood it serves. The Spring District station, with its associated “cut”, will present an entirely different station condition than its sister at 130th Avenue Northeast, and accordingly should present a different station character.

#### Recommendation:

Meet with the Sound Transit Art Program Director to develop a cooperative set of Public Art Design Criteria that addresses the particular opportunities at each station in the Corridor. Input from Spring District development team should inform the discussion around the Spring District station.

## THE RESIDENTIAL / RETAIL DISTRICT AT 130<sup>TH</sup> AVENUE NORTHEAST

### 130th Avenue Northeast

It is anticipated that the retail activity along 130th Avenue Northeast will spill out of eateries and stores onto the sidewalk to create a diverse, colorful and lively environment. The street experience will be intimate like a table for two, well crafted like a fine pair of Italian shoes and fun like a children’s bookstore. Every 100 feet is different and provides an ever-unfolding experience. Art has a role to play in this environment in adding a layer of non-commercial content and continuity.

#### Recommendation:

Multiple highly crafted/detailed human-scaled works of art should be commissioned to be set into the sidewalk furnishing zones along 130th Avenue Northeast. Sometimes on pedestals, sometimes at-grade, sometimes in conjunction with plantings or benches - these works will help to enrich this street’s activity and help define its unique identity.



FIGURE 6.36: Artwork within the shopping environment provides a scale and richness to the retail experience.

## THE ARTS DISTRICT: AN EVOLVING ARTS VILLAGE

### Arts District



FIGURE 6.37: *The intimate character of Goff Creek can create unique surprises that support the development of an arts village.*

One of the goals for the BelRed Corridor is the development and growth of an arts district. This is envisioned as happening in the area immediately east of Goff Creek in the proximity of the Pacific Northwest Ballet School. The growth of a cultural district generally evolves over generations and is driven by tangibles, such as availability of cheap space, but also by intangibles such as places for socialization, the personalities of its residents, and their ability to attract like-minded people to the area. The physical characteristics of this area of the Corridor, with its low-rise structures and tall evergreen trees, suggest that the district character be intimate and village-like. A number of activities, such as weekend open studios, craft festivals, and jazz performances may be employed to increase not only the use of the district by artists but also visitation by citizens and tourists over time.

### Development and Programming

#### Recommendations:



FIGURE 6.38: *Existing buildings within the district can be easily re-vitalized to create studio space.*

Convene a group of local artists who might wish to locate in this new neighborhood and facilitate their organization into a working-group.

Develop a name for the district.

The development of a modest artist residency program should be considered. Examine other such programs - Pilchuck, Caldera and Playa - Oregon, Ucross -Wyoming, Anderson Ranch - Colorado, etc.



FIGURE 6.39: *An arts district should embrace all who create - the glass artists, the poet, the painter, etc.*

A study group should be convened to visit other such artistic communities of this implied scale - Cannon Beach Oregon, Emeryville, California, Roosevelt District in Phoenix, etc., to glean an understanding of the possibilities for such a community.

Map and inventory properties which are underutilized within the district and then develop some logical, targeted nodes where critical mass cultural uses might be achieved.

Explore the development of a fund which would provide low interest loans or grants to artists who purchase property in the district (*the City of Phoenix program is an example*).

Explore the development of a fund which would provide grants to subsidize rental fees for artists who locate within the district.



FIGURE 6.40: *The existing landscape provides a wonderful stage for an emerging arts district - giving it an identity which is unique to the region.*

### **Landscape**

Inventory and protect all significant trees.

Established landscape standards for the arts district which emphasis native plants and casual character.

### **Public Art**

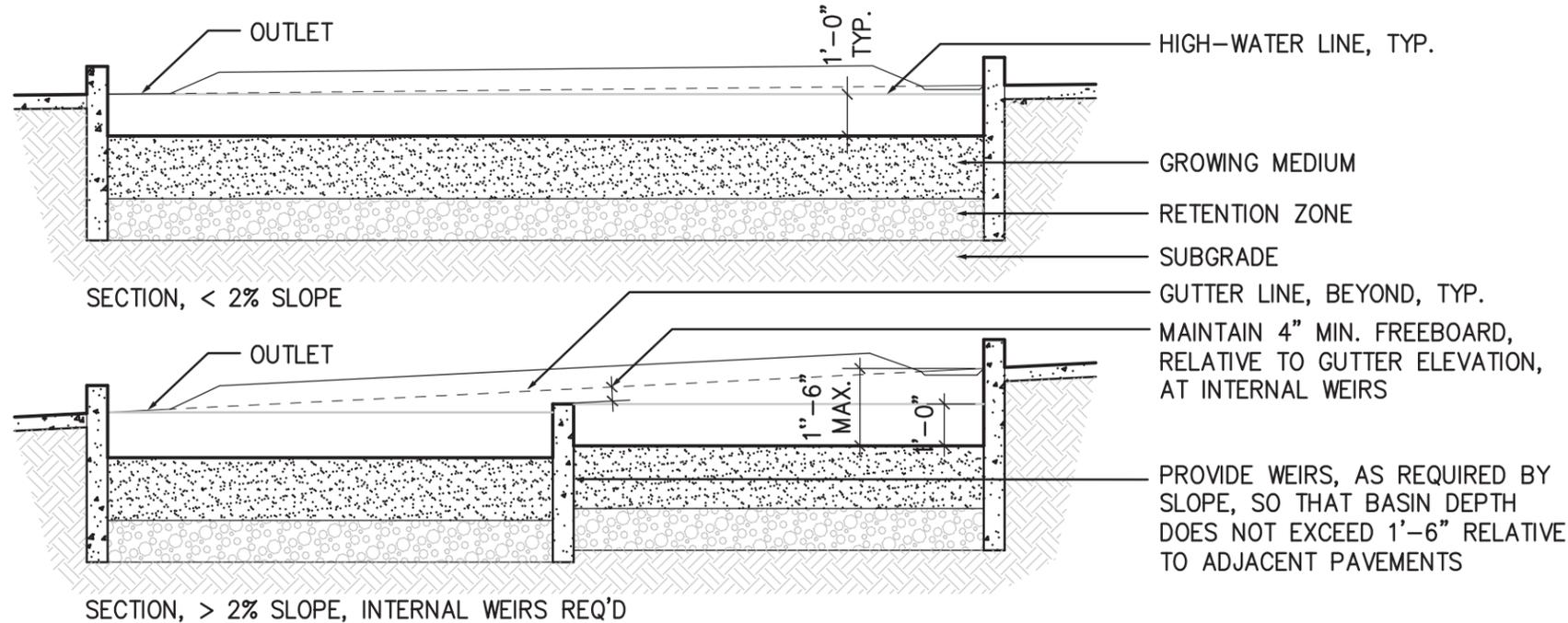
Public art in the art district should favor temporary projects over permanent projects to facilitate the growth of the district. When improvements are made to right-of-ways in the arts district, locations should be identified for temporary display of district-curated art works.

Identify sites in the district for event-driven culture.

Review all existing and anticipated open spaces to determine ownership and potential to include public art work.

Identify opportunities for public art on Green Streets and at intersections.

d.



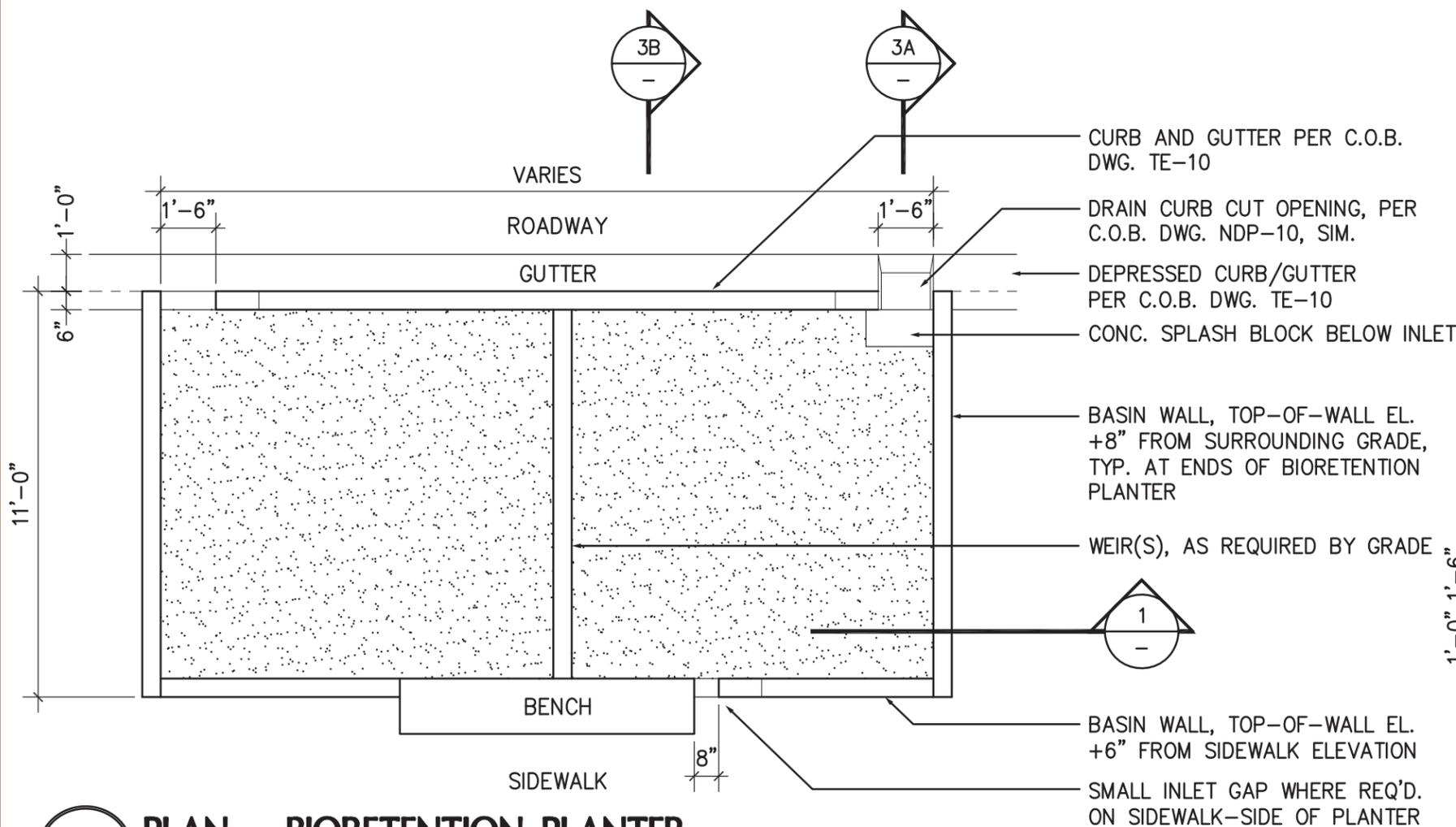
**TYPICAL BIORETENTION PLANTER ON GREEN STREETS**

Figure 4.32

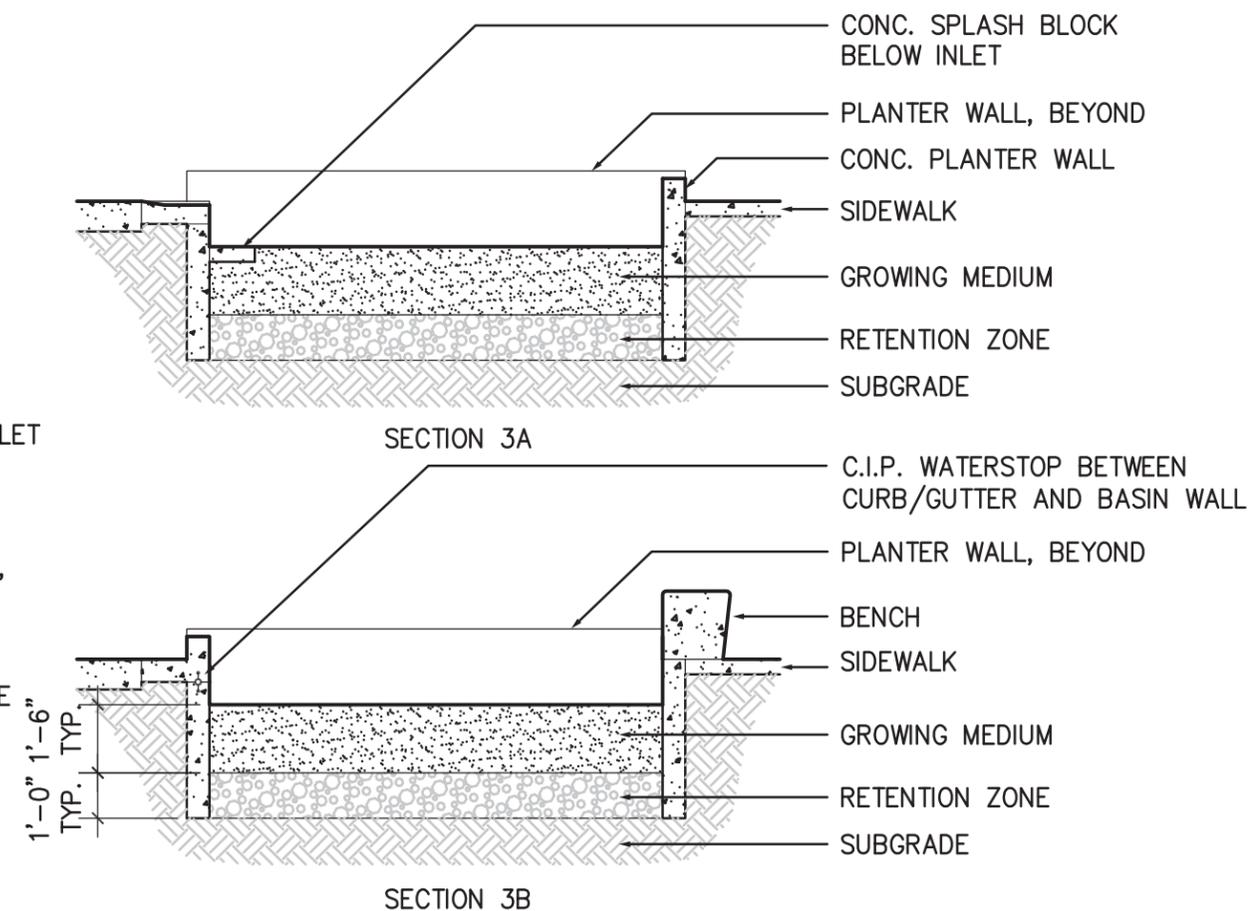
**NOTES:**

1. THESE DRAWINGS ARE CONCEPTUAL ONLY AND ARE NOT FOR CONSTRUCTION. BIORETENTION PLANTER DESIGN AND ENGINEERING IS TO BE PROVIDED ON A PROJECT-SPECIFIC BASIS.
2. BIORETENTION PLANTER PER C.O.B. DWG. NDP-4A, SIMILAR.
3. PLANTER TO FEATURE OVERFLOW OUTLET AT GUTTER IN LIEU OF OVERFLOW RISER.

**1 SECTION, LONGITUDINAL - BIORETENTION PLANTER**  
SCALE: 1/4" = 1'-0"



**2 PLAN - BIORETENTION PLANTER**  
SCALE: 1/4" = 1'-0"



**3 SECTION, TRANSVERSE - BIORETENTION PLANTER**  
SCALE: 1/4" = 1'-0"







CITY OF BELLEVUE  
TRANSPORTATION DEPARTMENT

**Carl Wilson**  
Senior Development Review Engineer  
P 425.452.4228  
F 425.452.5225



ZGF ARCHITECTS, LLP

**David Grant**  
206.521.3516  
david.grant@zgf.com

