
Addendum to
**Environmental Impact
Statement
Bel-Red Corridor Project**

Prepared for
City of Bellevue, Washington

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- 1 Bel-Red Subarea Plan Draft
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- 3 Bel-Red Subarea Design Guidelines

Introduction

Project Background

In January 2007, the City of Bellevue published a programmatic Draft Environmental Impact Statement (DEIS) for the Bel-Red Corridor Project. The DEIS evaluated three action alternatives and a no-action alternative for redevelopment in the corridor. Each alternative represented a mix of land use changes and transportation improvements that could achieve the City's planning objectives. The DEIS analyzed the impacts of increases in development intensity and density and identified changes to the City's Comprehensive Plan (City of Bellevue, 2008a), subarea plans (City of Bellevue, 1988, 1993a, 1993b), and Land Use Code (City of Bellevue, 1975) that would be needed to implement the proposed changes. The draft *Bel-Red Subarea Plan*, the draft Land Use Code Amendments, and the Bel-Red Subarea Design Guidelines are provided as Attachments 1, 2, and 3 to this document.

In July 2007, the City issued the Final EIS (FEIS) for the project. The FEIS introduced a Preliminary Preferred Alternative (PPA), which included components of all the action alternatives but most closely resembled Alternative 3 from the DEIS. The PPA was identified by the Bel-Red Steering Committee after consideration of the DEIS results and public comment on the DEIS. The Steering Committee presented its final recommendations, which were consistent with the PPA, to the Bellevue City Council on September 24, 2007, in the *Bel-Red Corridor Project Final Report*.

With the framework of the Bel-Red plan established, work began to develop the detailed strategies needed to implement the changes. The Council asked five of the city's boards and commissions to craft new comprehensive plan policies and land use code regulations to implement the new plan (City of Bellevue, 2008b). On February 27, 2008, four boards and commissions presented their policy recommendations for Bel-Red to the Planning Commission. A staff report and draft *Bel-Red Subarea Plan*, zoning, and Land Use Code Amendments were published May 8, 2008. On May 28, 2008, the Planning Commission conducted a public hearing and considered a wide range of public comments on the draft. Following deliberation over six meetings and a detailed review of many issues, the Planning Commission made a recommendation to the City Council on July 23, 2008, to approve the *Bel-Red Subarea Plan* and Land Use Code, and related comprehensive plan amendments.

The City Council reviewed the Planning Commission's recommendation, and at the October 6, 2008, meeting, provided feedback for staff to incorporate. Staff returned to Council on January 5, 2009, with recommended modifications. Council discussed the proposed land use plan map for the Bel-Red Subarea and a proposal for transfer of development rights on January 20, 2009.

The City Council is anticipated to make its decision on approval of the *Bel-Red Subarea Plan*, Land Use Code amendments, and Comprehensive Plan amendments in the first quarter of 2009.

During development of the new *Bel-Red Subarea Plan* and Land Use Code amendments, the generalized land use categories evaluated in the EIS were defined in greater detail, and several minor changes were made to the PPA. Also, since publication of the FEIS, the City has made some changes to the traffic modeling to reflect updated assumptions and some changes in the proposed transportation projects. This addendum to the Bel-Red Corridor Project FEIS serves to document the changes since the FEIS was published and evaluate their potential impacts. None of the changes have been determined to significantly increase the level of effects described in the original EIS. The following section describes the changes evaluated in this addendum.

Proposed Changes Evaluated in this Addendum

As noted above, the changes between the FEIS PPA and the proposed subarea plan center on two primary areas: land use and transportation. The changes in each area that are evaluated in this addendum are described below.

Land Use

The proposed land use changes in the draft *Bel-Red Subarea Plan* differ very little from the PPA analyzed in the FEIS. Figure 1-1 depicts the PPA from the FEIS. Figure 1-2 depicts the proposed *Bel-Red Subarea Plan* land use designations. The overall number of residential units and amount of commercial square footage proposed are identical in the PPA and the draft *Bel-Red Subarea Plan*. The primary differences include:

- An increase in allowable floor area ratio (FAR) in the higher-intensity development nodes from 2.5 in the PPA up to 4.0, or up to 4.5 if park and open space areas are provided elsewhere within the subarea. An increase in allowable FAR in two of the non-node land use designations, from 1.0 to 2.0.
- Land uses and development nodes based on parcel boundaries rather than the more general “bubble” areas described in the FEIS.
- A more refined gradation of land use designations, particularly mixed-use designations.
- Addition of a medical institution designation in the area adjacent to Overlake Hospital (this action was included in the FEIS addendum dated July 17, 2008, and is mentioned here for reference).
- Small shifts in land use district areas.

Nodes are areas of sufficient intensity, amenities, and mix of uses to support a high level of pedestrian activity, where transportation facilities will support development. Areas of more intense color for a land use designation indicate node locations.
See Figure 1-2

Other than these changes, overall land use assumptions from the PPA remain the same as described in the FEIS. In particular, while building form and massing is likely to differ somewhat from the PPA due to the higher FARs, neither the proposed employment and housing levels nor the distribution of land uses would change.

Land use effects of the proposed subarea plan are compared to those of the PPA in Chapter 2 of this addendum.

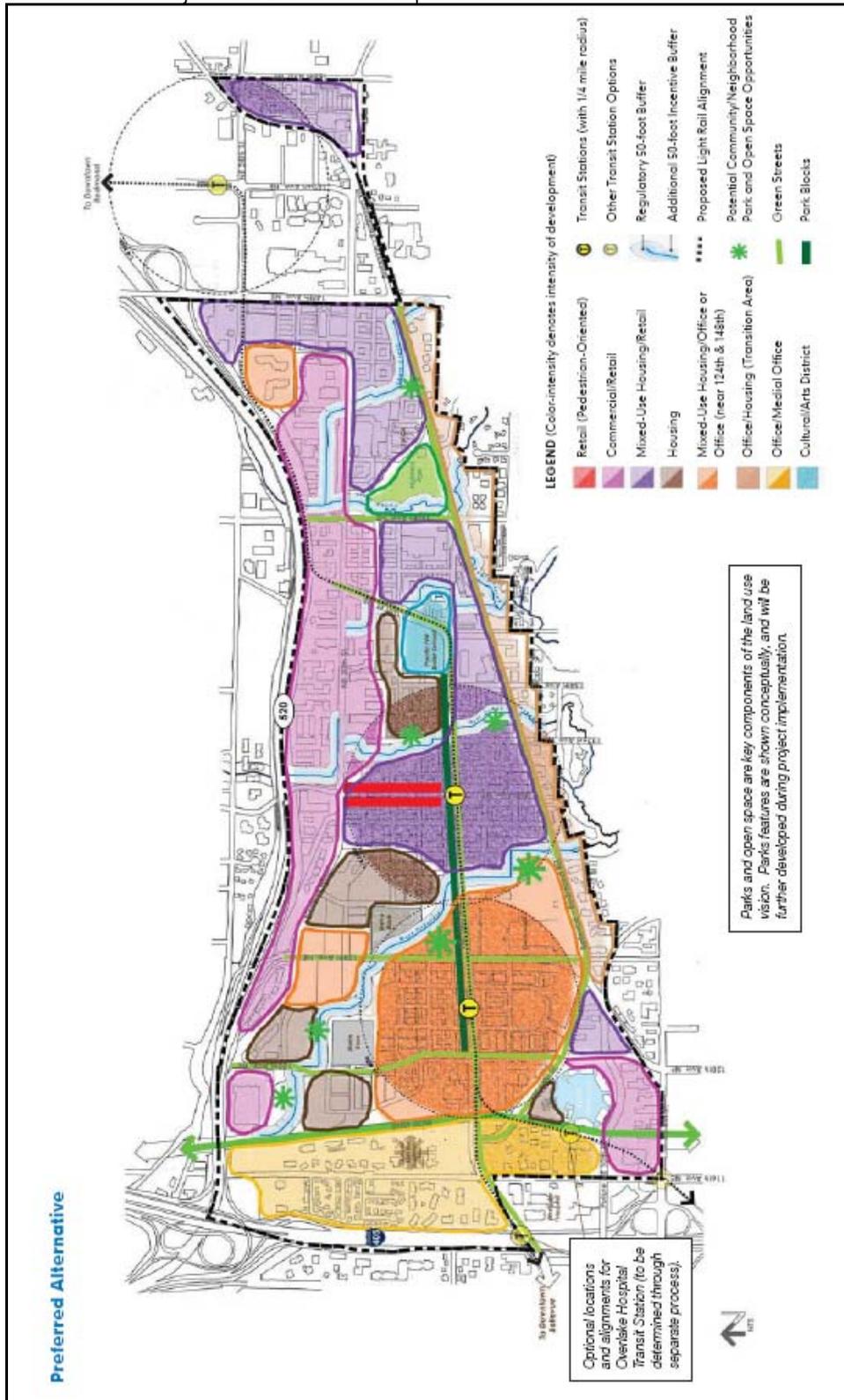
Transportation

Several updates to the FEIS traffic modeling have been made to reflect updated assumptions and changes in the set of proposed transportation improvement projects evaluated in the PPA. These include:

- Removal of the new SR 520 ramps at the 124th Avenue NE interchange from the No-Action Alternative . (This project was erroneously included in the No-Action Alternative in the Draft and Final EIS. This project is part of the 2030 transportation system in both the PPA and the draft *Bel-Red Subarea Plan*.)
- Do not add a second southbound through lane on 116th Avenue NE, from Northup Way to NE 12th Street (Project R-4 in FEIS).
- Widen 120th Avenue NE from two to five lanes, from Northup Way to Bel-Red Road (Project R-5 in FEIS) .
- Do not construct the NE 10th Street extension from 116th Avenue NE to 124th Avenue NE (Project R-11 in FEIS).
- Do not construct a two-lane roadway connection from 124th Avenue NE, north Bel-Red Road and under NE 12th Street, to Old Bel-Red Road/NE 10th Street extension (new improvement identified by the City of Bellevue).
- Do not construct the new NE 11th Street roadway, east of 116th Avenue NE, between 116th Avenue NE and NE 12th Street/Bel-Red Road (new improvement identified by the City of Bellevue) within the 2030 planning horizon, but retain this project in the *Bel-Red Subarea Plan* for future consideration.
- Revise the project description for 130th Avenue NE to be a two-lane, pedestrian-oriented street with on-street parking north of NE 16th Street, and a three-lane street south of NE 16th Street (Project R-7 in FEIS).
- Add the six intersection improvements to the modified No-Action Alternative, as described in Chapter 3.
- Eliminate 11 intersection improvements from the PPA, as described in Chapter 3.

Overall, traffic volumes and levels of service from the updated modeling effort are very similar to those described in the FEIS for the PPA. For the No Action Alternative, traffic volumes increased on some roadways and levels of service degraded at some intersections, due to the removal of one major capacity improvement (new ramps at 124th Avenue NE and SR 520). See Chapter 3 for more details about the traffic modeling and transportation effects.

FIGURE 1-1
Bel-Red Corridor Project Final Environmental Impact Statement Preferred Alternative



Land Use and Aesthetics

Introduction

This chapter describes the differences between the proposed land uses in the recommended *Bel-Red Subarea Plan* (City of Bellevue, 2008) and the Preliminary Preferred Alternative analyzed in the Bel-Red Corridor Project FEIS. This chapter also analyzes how the land use and visual effects of new development or redevelopment according to the proposed changes differ from those of the PPA in the FEIS. Key elements of relevant land use plans and policies and project consistency with these plans and policies are also analyzed for the land use changes in the proposed subarea plan.

Existing conditions and the No-Action Alternative remain as discussed in the Bel-Red Corridor Project DEIS. Within the DEIS, the description of the No-Action Alternative can be found in Section 2, pages 2-3 and 2-4. Existing conditions are organized by topic area and are located in the DEIS as follows:

- Air Quality: 3-1
- Watershed Processes: 4-11
- Environmental Hazards: 6-3
- Land Use: 7-2
- Population, Housing, and Economics: 8-3
- Aesthetics: 9-2
- Transportation: 10-6
- Public Services and Utilities: 11-2

Typically, adoption of a plan document is reviewed as a “nonproject” action under SEPA so that its broad environmental impacts can be assessed before adoption. Individual projects are then reviewed to determine the precise nature and significance of their impacts and whether those impacts are consistent with the overall planning goals of the local jurisdiction.

For this study, the nonproject action will be the City’s adoption of the *Bel-Red Subarea Plan* and other amendments to Bellevue’s Comprehensive Plan, including the Transportation Element, the Glossary, the *Crossroads Subarea Plan*, and the *Wilburton/NE 8th Street Subarea Plan*. Amendments to the Land Use Code consistent with the *Bel-Red Subarea Plan* will also be adopted as part of this action. Because this addendum evaluates a planning process under the Growth Management Act (GMA), it is prepared in accordance with Washington Administrative Code 197-11-235 and will become part of an integrated GMA document as described in those regulations.

The pertinent state, regional, and local plans and policies are the same as those discussed in the DEIS (Chapter 7, page 7-9) and are not repeated here.

Proposed Changes from the Preliminary Preferred Alternative

The proposed land use changes in the *Bel-Red Subarea Plan* differ very little from the PPA analyzed in the FEIS. Figure 1-1 depicts the PPA from the FEIS, and Figure 1-2 shows the *Bel-Red Subarea Plan* land use designations. The overall number of residential units and commercial square footage proposed is identical. The primary differences include:

- Adding a medical institution designation in the area adjacent to Overlake Hospital, as discussed in the prior addendum.
- Providing a more refined gradation of land use designations.
- Small shifts in land use district areas.
- Designating land uses and development nodes based on parcel boundaries rather than the more general “bubble” areas described in the FEIS.
- Providing for an increase in floor area ratio (FAR) in the nodes from 2.5 in the PPA up to 4.0. An additional FAR increase to 4.5 FAR could be achieved beyond that allowed with the FAR incentive system with dedication of park and open space lands within the subarea.
- Increasing maximum FAR in the Commercial Residential and Residential areas outside of nodes from 1.0 to 2.0.

Impacts

This section describes only those potential impacts of the land use designation changes in the proposed *Bel-Red Subarea Plan* that differ from the impacts of the Preliminary Preferred Alternative analyzed in the FEIS. Figure 1-2 depicts the proposed *Bel-Red Subarea Plan* land use map.

Operational Impacts

Types of land use impacts that could potentially occur under the proposed *Bel-Red Subarea Plan* relate to land use conversions, activity level changes (i.e., noise, traffic, and pedestrian activity), and compatibility of new land uses in the study area with existing on-site uses and surrounding land uses. In general, uses proposed under the subarea plan would have very similar impacts to those in the PPA. Visual changes would be limited to increased building bulk within the core of development nodes resulting from the FAR increase from 2.5 to up to 4.0 or 4.5, which could occur with park and open space internal transfers, and from 1.0 to up to 2.0 in the Commercial Residential and Residential designations outside of nodes. Overall, these effects will not differ significantly from those discussed in the DEIS and FEIS because the *Bel-Red Subarea Plan* does not change the 2030 development program for the subarea.

Similar to the PPA, the proposed subarea plan land uses would allow for substantial amounts of new employment and housing in this portion of the city compared to current conditions. Over time, portions of the Bel-Red Subarea would change from an area of

low-rise industrial buildings and retail development to a range of mid-rise, medium-density uses, including office, residential, retail, and service uses.

Proposed Subarea Plan Land Use Changes

The following discusses the land use changes in the proposed *Bel-Red Subarea Plan* that differ from the land use in the PPA and the potential land use and visual impacts that could result from the changes.

Medical Institution. As discussed in the July 17, 2008, addendum, the Medical Institution designation in the area northeast of Overlake Hospital and Group Health, west of the BNSF corridor and east of 116th Avenue NE, would allow an area for less intensive hospital and ambulatory health care center uses in close proximity to the hospital campus.

The PPA in the FEIS as well as Alternatives 1, 2, and 3 in the DEIS, designated this area as Medical Office, with development intensity up to four to six stories. Please refer to the July 17, 2008, addendum for a discussion of the Medical Institution designation.

Land Use Map Changes. The *Bel-Red Subarea Plan* uses land use designation terminology that differs slightly from that used in the FEIS. Table 2-1 shows the equivalent terms in the two documents. The designations used in the proposed subarea plan are more refined (i.e., include greater detail on types of land uses) than those in the FEIS, as the subarea plan is the next step toward implementing the land use changes. However, the more refined designations are consistent with the broader categories in the FEIS. Land use category names not listed in the table below are the same in both the documents.

TABLE 2-1
Land Use Category Comparison

FEIS	Proposed Subarea Plan
Commercial/Retail	General Commercial
Mixed-use Housing/Retail Higher Intensity	Residential Commercial 1, 2, and 3
Mixed-use Housing/Retail Lower Intensity	Commercial Residential
Housing	Residential
Mixed-Use Housing/Office	Office Residential and Office Residential 1 and 2

The Mixed-use Housing/Retail area from the FEIS is now defined in greater detail with the Residential Commercial 1, 2, and 3 and Commercial Residential districts in the proposed subarea plan. The Residential Commercial 1 and 2 areas form the 130th Avenue NE node, with Residential Commercial 1 denoting higher intensity development in the core of the node. Residential Commercial 3 is located on 156th Avenue NE in the eastern portion of the Bel-Red Subarea. Residential Commercial and Commercial Residential are both mixed-use designations, with the potential for combining uses within one building. However, the Residential Commercial designation indicates an emphasis on residential uses, while Commercial Residential indicates an emphasis on retail and service uses. As depicted in Figure 1-2, the refinement of the land uses also includes delineation of land use areas by

parcel lines, which will facilitate plan implementation as specific properties are redeveloped.

The proposed subarea plan expands the Commercial Residential area north of NE 20th Street at the 140th Avenue NE intersection and to the east. This area was previously a Commercial/Retail area. The Commercial Residential designation provides for a mix of housing, retail, and service uses, often within the same building, with commercial uses on the ground floor. This district emphasizes retail and service uses, whereas the Commercial/Retail (General Commercial under the proposed subarea plan) designation does not provide for a mix of uses and represents the same development intensity as what occurs in the area today. This change could support slightly increased development intensity in the Commercial Residential areas than would occur in the Commercial/Retail or General Commercial area near 140th Avenue. The Commercial/Retail area (General Commercial in the subarea plan) would extend farther to the south than described in the PPA, east of the 130th Avenue NE node and west of 136th Place NE, removing a small area previously designated for housing in the PPA.

These shifts in proposed land use are not anticipated to create additional impacts beyond those described in the DEIS and FEIS, as the 2030 development program for the subarea is not changed. The general focus on commercial development would continue, with the primary change limited to a shift of areas that provide for residential development. Visual impacts beyond those described in the FEIS are also unlikely, as land use intensity and scale in the subarea plan are not dramatically different from the PPA. Although slightly higher development intensity could occur in the Commercial Residential area designation in the subarea plan than proposed under the Commercial/Retail designation in the PPA, the visual change would be minor. In addition, a greater degree of visual unity would be likely with redevelopment under the subarea plan as residential and commercial structures are developed as mixed uses adjacent to commercial areas. Future development would also be subject to design guidelines and codes created for the subarea.

The land use map changes related to matching land use district boundaries to parcel lines would have no impact, as the overall amount of development and general locations of proposed land uses would not change.

The King County Metro East Bus Base facilities are not shown on the subarea plan map as they are on the PPA map. Instead, these areas are designated as Office Residential and Residential. However, as discussed in the FEIS, provisions are made for the continuation and expansion of existing uses that are not consistent with the new plan. Therefore, the proposed change does not affect the Metro facilities.

Floor Area Ratio Increase. The PPA allowed a FAR of up to 2.5 in nodes. The proposed subarea plan allows a FAR in the nodes of up to 4.0 or 4.5 with park and open space internal transfers from elsewhere within the subarea. Maximum FAR in the Commercial Residential and Residential designations outside of nodes would also increase from 1.0 to 2.0. A building height increase over that discussed in the FEIS for these land use designations is not proposed. This change would allow for bulkier buildings on sites where developers provide for other public needs, such as open space, affordable housing, and stream restoration, but a floor-plate limit is proposed to address this impact. Because the overall square footage of development and the number of residential units under the 2030

development program for the subarea plan would not change from the PPA, no additional impacts beyond those discussed in the DEIS and FEIS are anticipated.

Visual impacts resulting from the FAR increase in the nodes would be incrementally different from those of the PPA. The proposed subarea plan and the PPA would each generate a notable increase in scale, height, and visual texture in the subarea compared to the No-Action Alternative. However, existing land uses have relatively low visual quality, and viewers in these areas – particularly the areas currently in industrial use – have a low sensitivity to visual change. In addition, development under the proposed subarea plan, as well as under the PPA, has the potential for increased visual quality through visually unified development and high-quality architecture. Future development would adhere to design guidelines and codes established for the subarea.

Consistency with Plans and Policies

Because the proposed changes do not change the overall 2030 development program in the subarea, the discussion of consistency with land use plans and policies in the DEIS and FEIS remains relevant and is not repeated here. By increasing the maximum FAR in nodes from 2.5 up to 4.0 or 4.5, the proposed subarea plan would allow higher-intensity development in the nodes at transit stations than the PPA, further encouraging transit use and preservation of open space consistent with many state, regional, and local goals and policies.

Construction Impacts

Land Use and Aesthetics

The land use and visual impacts for construction of projects consistent with the proposed subarea plan would not differ from those anticipated under the PPA. Redeveloping land and constructing transportation improvements could result in short-term temporary impacts on nearby land uses, including increases in noise, dust, and traffic. Visual impacts can also be expected with the altering of existing visual elements as well as the presence of construction equipment, materials, signs, and staging areas. Construction activities would occur over time during the planning period and at different locations within the study area, resulting in temporary impacts at specific adjacent land uses when site construction is taking place. The Bel-Red Corridor Project DEIS provides additional discussion of construction impacts related to each topic area. During project-level SEPA review and permitting for these specific projects, the City would identify and commit to measures to reduce these impacts on adjacent land uses, such as limiting construction hours and designating construction traffic access routes away from residential areas both within and adjacent to the Bel-Red Subarea.

Land Use Displacements Due to Roadway Improvements. Due to changes in specific transportation improvement elements of the plan, the number of buildings displaced would be reduced from approximately 30 commercial and 3 residential buildings to 19 or 20 commercial buildings and no residential buildings. Elimination of the proposed projects to widen 116th Avenue NE and to extend NE 10th Street would avoid the displacements of twelve commercial properties and three residences that were associated with these projects. However, three to four additional buildings would be displaced by the newly proposed extension of NE 11th Street, which was not included in the FEIS.

Mitigation Measures

There are no additional land use or aesthetic impacts beyond those identified in the DEIS and FEIS. Therefore, the mitigation measures identified in the DEIS and FEIS remain applicable and no additional mitigation measures are necessary.

Unavoidable Adverse Impacts

There are no unavoidable adverse impacts other than displacement of businesses and possibly residences in the Bel-Red Subarea, as discussed in the DEIS and FEIS.

Transportation

Introduction

This chapter documents the results of the updates to the transportation analysis since the publication of the Bel-Red Corridor Project Final Environmental Impact Statement (FEIS), July 19, 2007. The analysis was updated to evaluate the effects of changes since the FEIS in the following areas:

- Updated land use information, as described in Chapter 2 of this Addendum, and associated changes in traffic volumes
- Screening analysis for several of the proposed transportation improvements identified in the FEIS
- Updates to the travel demand model used for the FEIS
- Proposed changes to the Traffic Standards Code , described here and analyzed in the 2009–2020 Transportation Facilities Plan Draft Environmental Impact Statement, published January 22, 2009

This chapter documents the changes since the FEIS was published and compares the results of the new analysis with the results described in the FEIS for the No-Action Alternative and the Preliminary Preferred Alternative (PPA).

Refer to Chapter 10 of the Draft EIS (DEIS), January 25, 2007, for documentation of the transportation analysis regarding existing conditions and the original three action alternatives. Refer to Appendix A of the FEIS for previous analysis discussion and results for the PPA.

Unless there were substantive changes in assumptions, methodologies, or results, the transportation analysis documented in the DEIS and FEIS is still valid. Therefore, most of the text from the DEIS and FEIS is not repeated in this chapter. Only sections with changes or new data are included in this document.

Changes to the No-Action and Preliminary Preferred Alternatives since the FEIS

Travel Demand Model Update

Since the publication of the FEIS, the City of Bellevue has studied and finalized a list of transportation projects needed to support the *Bel-Red Subarea Plan* and analyzed when these improvements could be phased in over time.

The Bellevue Kirkland Redmond (BKR) travel demand model was used to develop 2030 p.m. peak hour forecasts for the modified set of transportation improvements. These changes are described in the sections that follow. The analysis described in this document tested the proposed Bel-Red transportation projects in the BKR model to assess their performance, as well as to identify specific proposed intersection turn-lane improvements that could be eliminated given the revised transportation updates.

In addition to the changes noted above, it was discovered after the publication of the FEIS that an improvement was unintentionally and incorrectly included in the BKR model for the original No-Action Alternative. The improvement project was the ramps to and from the east on SR 520 at 124th Avenue NE (Project R-13 in the FEIS). This project has been removed from the travel demand model for the No-Action Alternative, and the level of service (LOS) analysis contained in this chapter has been updated to reflect the removal of this project for the modified No-Action Alternative.

Transportation Improvements Screening

On the basis of further deliberations on the subarea plan, and subsequent analysis and consideration, several projects in the FEIS were reevaluated. Four improvement projects proposed in the FEIS, and two newly identified projects were screened in preparation for the *Bel-Red Subarea Plan* to determine whether they were necessary to support mobility and achieve the new LOS standard for the Bel-Red/Northrup Mobility Management Area (MMA). The screening was completed using the BKR model, based on the demand predicted for each of the following proposed improvements:

- Add a second southbound through lane on 116th Avenue NE, from Northrup Way to NE 12th Street (Project R-4 in the FEIS)
- Widen 120th Avenue NE from two to five lanes, from Northrup Way to Bel-Red Road (Project R-5 in the FEIS)
- Construct a NE 10th Street extension, from 116th Avenue NE to 124th Avenue NE (Project R-11 in the FEIS)
- Revise the project description for 130th Avenue NE to be a two-lane, pedestrian-oriented street with on-street parking north of NE 16th Street, and a three-lane street south of NE 16th Street (Project R-7 in the FEIS, which had described this project as a four-lane arterial)
- Construct a two-lane roadway connection from 124th Avenue NE, north of Bel-Red Road and under NE 12th Street, to Old Bel-Red Road/NE 10th Street Extension (new improvement identified by the City of Bellevue)
- Construct a new NE 11th Street roadway, east of 116th Avenue NE, between 116th Avenue NE and NE 12th Street/Bel-Red Road (Project R-14, new improvement identified by the City of Bellevue)

On the basis of this screening process, three of these six proposed projects were eliminated, while the remaining three projects will become part of the transportation improvement list for the *Bel-Red Subarea Plan*. The three projects that were carried forward are the following:

- 120th Avenue NE widening project
- NE 11th Street connection
- 130th Avenue NE two to three-lanes

An explanation of the screening outcome of the three eliminated projects follows.

- **Second southbound through lane on 116th Avenue NE:** On the basis of the travel demand model update completed after the FEIS, revised 2030 traffic volumes on 116th Avenue NE (north of NE 12th Street) are forecasted to decrease by almost 30 percent compared with the FEIS results (from 1,044 vehicles to 733 vehicles, southbound), most notably in the southbound direction. As a result, it was determined that Project R-4 (construct a second southbound through-lane on 116th Avenue E, from Northup Way to NE 12th Street) was no longer needed to accommodate traffic demand in 2030 for the Bel-Red Subarea.
- **NE 10th Street Extension:** Operational results from the NE 10th Street extension project were compared with delays and intersection LOS resulting from the NE 11th Street connection project, which was not evaluated in the FEIS. Overall, delay and intersection LOS were similar for the two projects. However, the extension of NE 10th Street (Project R-11) between 116th Avenue NE and 124th Avenue NE would involve significant right-of-way acquisition, making this option considerably more costly than the NE 11th Street extension, without substantial traffic-operations benefits. Therefore, the NE 10th Street extension project was removed from consideration, while the NE 11th Street project replaced it from a long-range capacity standpoint.
- **Two-lane connection from 124th Avenue NE, north of Bel-Red Road and under NE 12th Street to Old Bel-Red Road/NE 10th Street Extension:** This project, not originally part of the PPA in the FEIS, was eliminated from further consideration based on the low forecasted traffic demand for this roadway in 2030. Its intent was to replace Project I-3 in the FEIS at the intersection of Bel-Red Road and 124th Avenue NE. However, it was determined that the cost of the new roadway would exceed the cost of the intersection improvements (Project I-3), with no additional benefit.

The remaining three projects, the 120th Avenue NE widening project, NE 11th Street connection, and redefined 130th Avenue NE project were generally found to have sufficient traffic demand in the updated BKR forecasting model to provide a meaningful level of improvement.

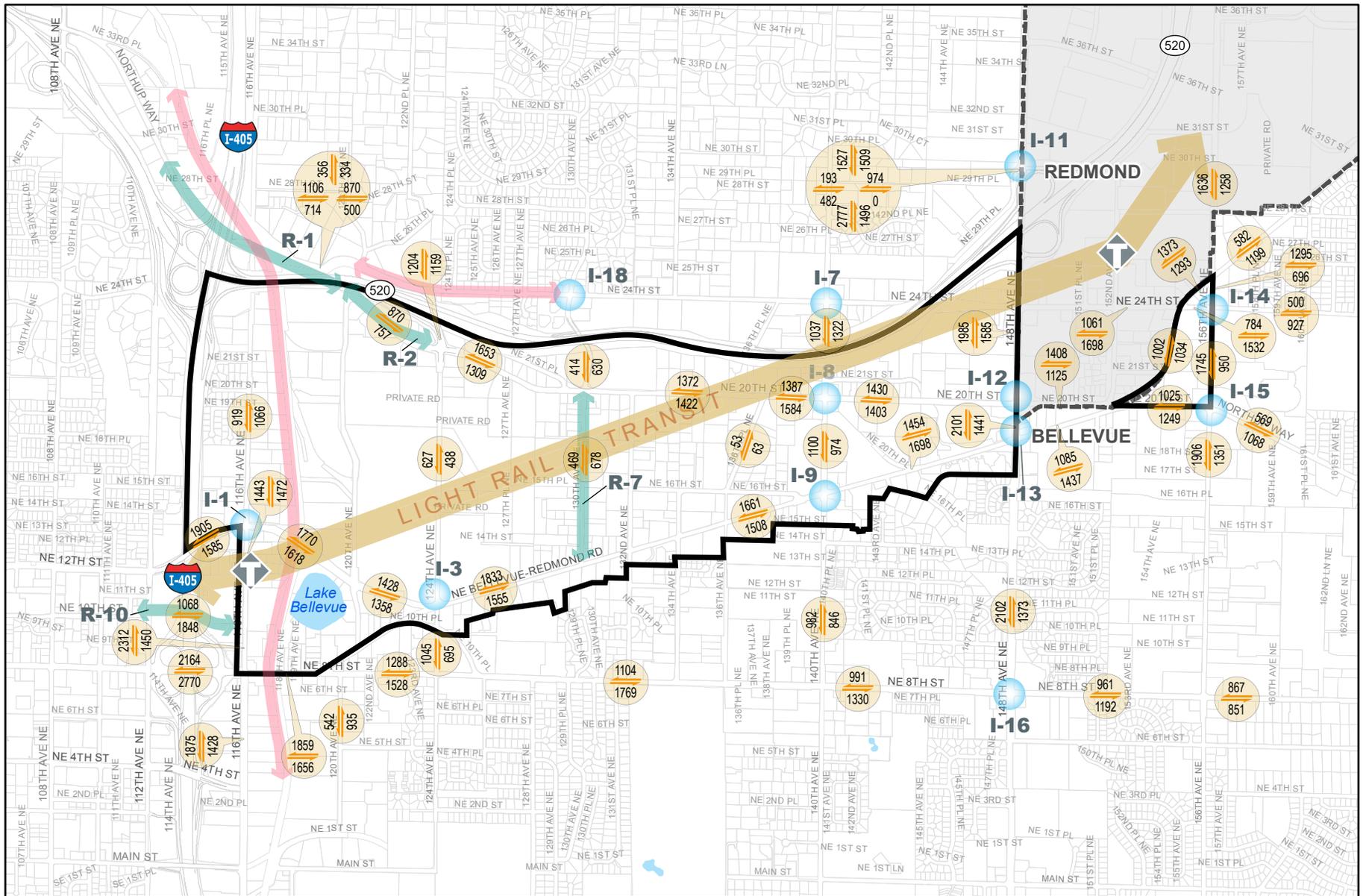
Future Traffic Volumes

Revised 2030 p.m. peak hour traffic volumes for the modified No-Action and *Bel-Red Subarea Plan* alternatives were forecasted using the BKR model (which included updated land use, as documented in Chapter 2) and the transportation network assumptions listed in the previous section. These updated future traffic volumes are illustrated in Figures 3-2 and 3-4, respectively, in the Impacts section of this chapter. The 2030 p.m. peak hour traffic volumes

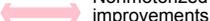
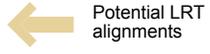
for the original No-Action Alternative and PPA documented in the FEIS are also provided as Figures 3-1 and 3-3, respectively, for comparison purposes.

Intersection Traffic Operations

The same intersections in and in proximity to the Bel-Red Corridor that were analyzed in the FEIS for the No-Action Alternative and PPA were reanalyzed using new turning-movement traffic forecasts as described in the previous section. Fifty-three intersections were analyzed for the modified No-Action Alternative and 58 intersections consistent with the assumptions in the *Bel-Red Subarea Plan*. These results are presented in Figures 3-7 and 3-8, and they are discussed in the Impacts section of this chapter. For comparison purposes, the 2030 p.m. peak LOS results for the original No-Action Alternative and PPA documented in the FEIS are also provided as Figures 3-5 and 3-6 (also in the Impacts section).



LEGEND

-  Bel-Red Corridor
-  Intersection improvements
-  2030 PM peak-hour traffic volumes
-  Arterial improvements
-  Nonmotorized improvements
-  Potential LRT alignments
-  Potential LRT station locations; number of and specific locations will require additional analysis.

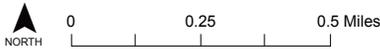
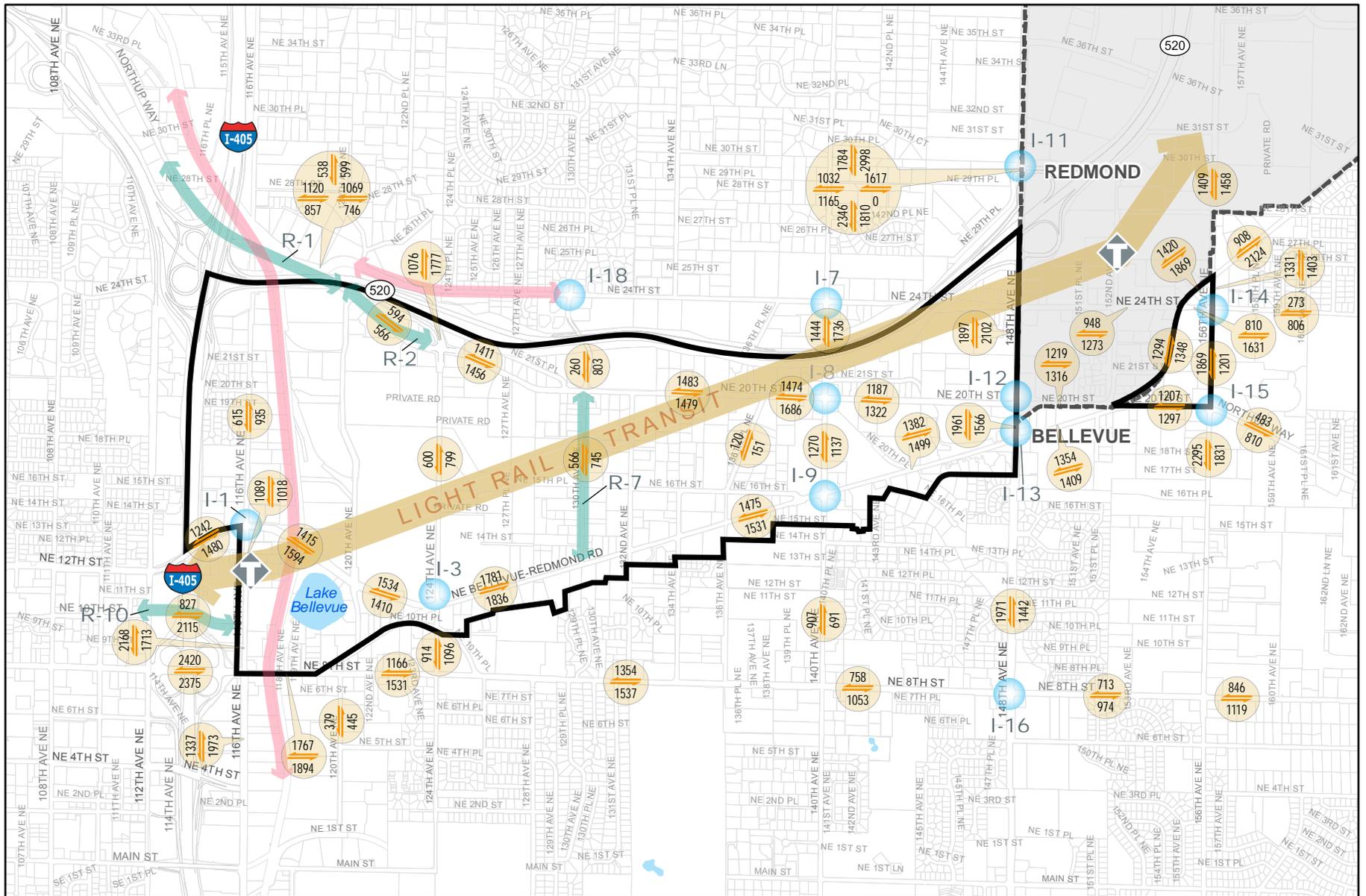


Figure 3-1
No-Action Alternative
Transportation Improvements and
2030 PM Peak-Hour Traffic Volumes
 Bel-Red Corridor Final EIS Addendum



LEGEND

- Bel-Red Corridor
- ↔ Arterial improvements
- Intersection improvements
- ↔ Nonmotorized improvements
- 2164
—
2770 2030 PM peak-hour traffic volumes
- ← Potential LRT alignments
- ⬇ Potential LRT station locations; number of and specific locations will require additional analysis.

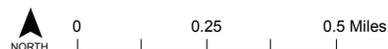
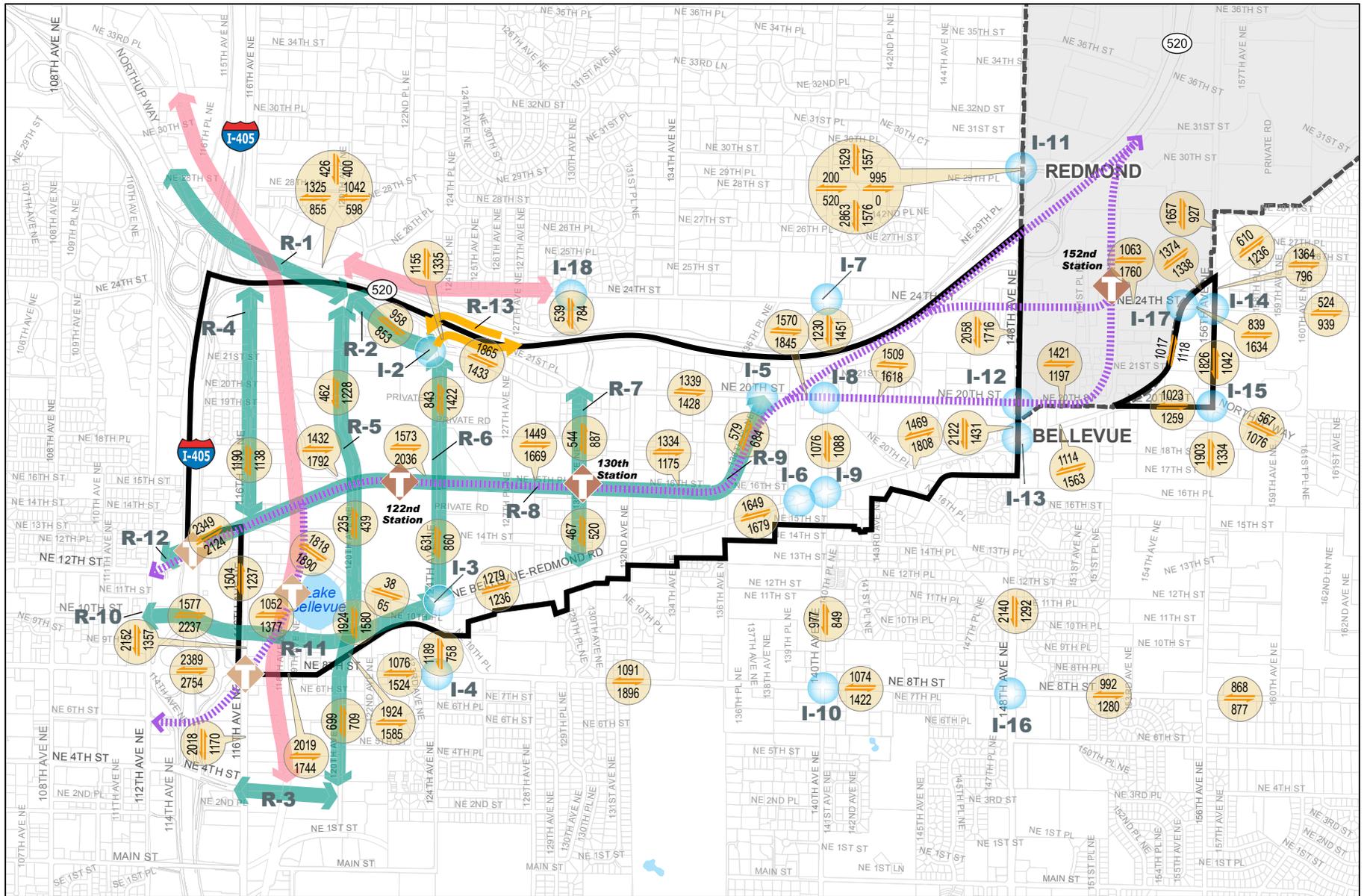
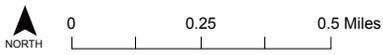


Figure 3-2
Modified No-Action Alternative
Transportation Improvements and
2030 PM Peak-Hour Traffic Volumes
 Bel-Red Corridor Final EIS Addendum

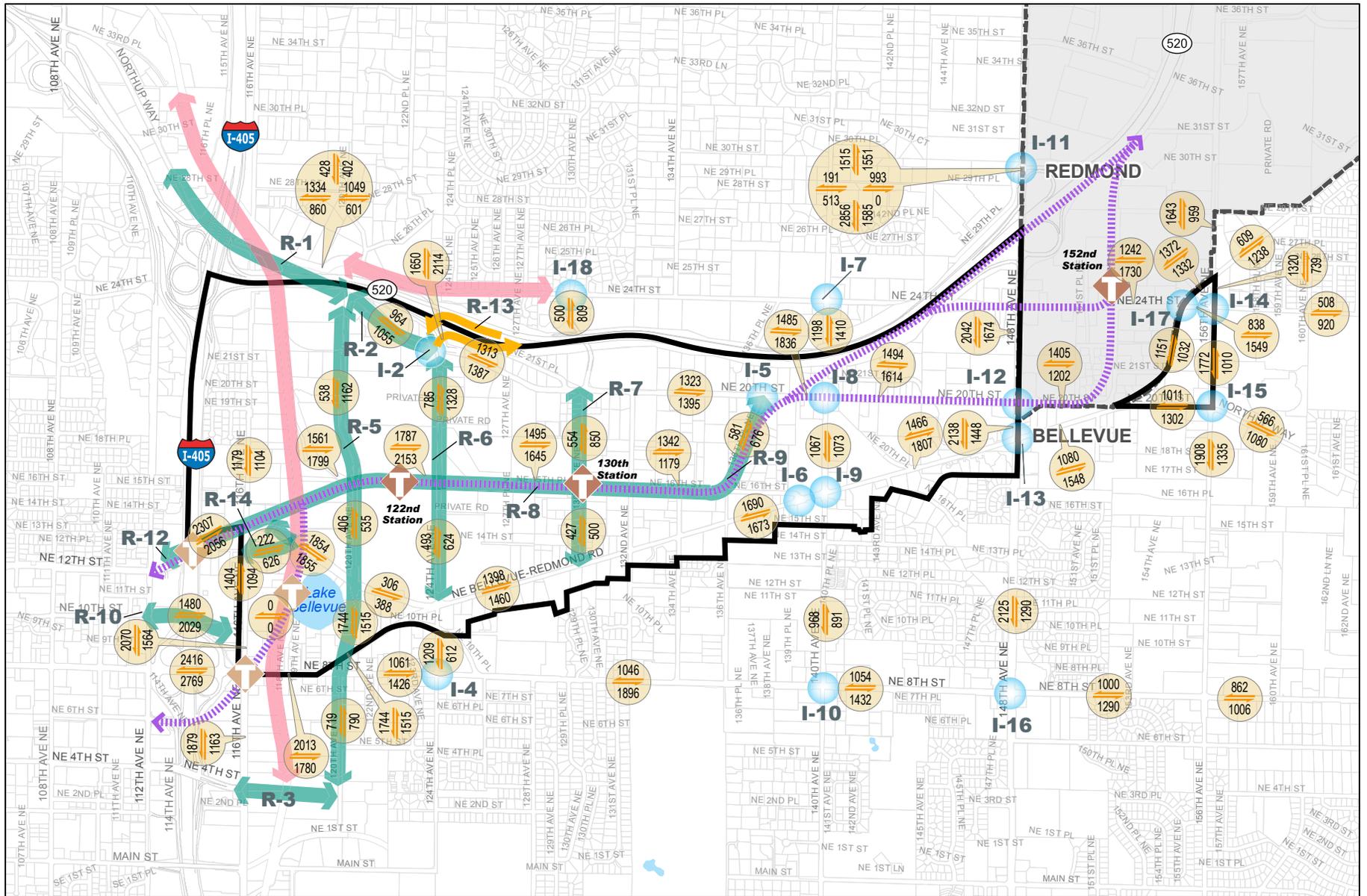


LEGEND

- Bel-Red Corridor
- Intersection improvements
- 2164
2770 2030 PM peak-hour traffic volumes
- New freeway access
- Arterial improvements
- Nonmotorized improvements
- ⋯ Potential LRT alignments
- ⬇ Potential LRT station locations; number of and specific locations will require additional analysis by others.



**Figure 3-3
Preliminary Preferred Alternative
Transportation Improvements and
2030 PM Peak-Hour Traffic Volumes**
Bel-Red Corridor Final EIS Addendum



LEGEND

-  Bel-Red Corridor
-  Intersection improvements
-  2030 PM peak-hour traffic volumes
-  New freeway access
-  Arterial improvements
-  Nonmotorized improvements
-  Potential LRT alignments
-  Potential LRT station locations; number of and specific locations will require additional analysis by others.

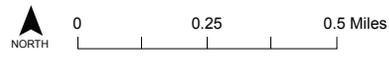


Figure 3-4
Bel-Red Subarea Plan
Transportation Improvements and
2030 PM Peak-Hour Traffic Volumes
 Bel-Red Corridor Final EIS Addendum

Transportation Policy and Code Changes

Since the FEIS was issued, the City of Bellevue has proposed to modify its Traffic Standards Code to reflect changes to the MMA boundaries, their associated intersections, and the LOS standards for each area (Sections 14.10.030, 14.10.060, and Table TR.1 of the Transportation Element of the Comprehensive Plan). These code changes are proposed to be consistent with the recommended changes to the Transportation Element. They have been documented and analyzed in the 2009–2020 Transportation Facilities Plan Draft Environmental Impact Statement published on January 22, 2009.

Impacts

This section describes only those potential impacts of the transportation changes in the proposed *Bel-Red Subarea Plan* that differ from the impacts of the PPA analyzed in the FEIS. This section also addresses the differences between the original and modified No-Action Alternatives and their potential impacts to the Bel-Red Corridor transportation system.

Operational Impacts

The operational impacts described in this section generally focus on only the measurable differences between the following:

- FEIS No-Action and modified No-Action Alternative
- Modified No-Action Alternative and the FEIS PPA
- FEIS PPA and the *Bel-Red Subarea Plan*

Planned Roadway Projects and Improvements

As a result of the change in the larger set of transportation improvements, some of the proposed intersection-level improvements identified for both the No-Action Alternative and the PPA in the FEIS either are no longer needed or have changed. The intersection improvements often involve double left-turn lanes where single left-turn lanes exist today, or separate right-turn lanes where shared right/through movements occur today at intersection approaches. These measures to add capacity often helped to improve intersection operations and lowered overall delay. However, intersection improvements could also result in adjacent property impacts, increased pedestrian crossing distance, and offset urban design enhancements, and therefore were analyzed also from this perspective. Table 3-1 presents an updated list of intersection improvements for the modified No-Action Alternative and the Bel-Red Corridor Final EIS Addendum, compared with the FEIS original No-Action Alternative and PPA. Many of the intersections analyzed in the FEIS and the Addendum are outside the Bel-Red Subarea but are important to the function of the overall transportation system. A general discussion of changes in intersection operations is provided below for justification of the change in the improvement; corridor-wide intersection LOS operations are further described in the *Intersection Traffic Operations* section of this chapter.

TABLE 3-1
Comparison of Proposed Intersection Improvements

Int. No.	Street Names		Intersection Improvements	FEIS No-Action Alternative	Mod. No-Action Alternative	FEIS PPA	FEIS Addendum Analysis
25	112th Avenue NE	NE 12th Street	Double WBL	X	X	X	X
			Separate WBR			X	X
			Separate NBR	X	X	X	
29	116th Avenue NE	NE 12th Street	Double WBL	X	X	X	X
			Double EBL	X	X	X	X
32	120th Avenue NE	NE 12th Street	Separate NBT		X		
			Separate NBT/R		X		
34	124th Avenue NE	Bel-Red Road	Shared NBL/T			X	X
			Double WBL			X	
35	124th Avenue NE	NE 8th Street	Double SBL		X	X	X
39	140th Avenue NE	NE 20th Street	Double EBL	X	X	X	X
			Double WBL	X	X	X	
			Separate SBR	X	X	X	
			Separate EBR			X	X
40	140th Avenue NE	Bel-Red Road	Separate EBR	X	X	X	X
			Separate WBR	X	X	X	X
			Separate NBR	X	X		
			Double EBL			X	X
41	140th Avenue NE	NE 8th Street	Additional NBT			X	X
			Additional SBT			X	X
47	148th Avenue NE	NE 20th Street	Double NBL	X	X	X	
			Double SBL	X	X	X	
			Separate SBR	X	X	X	X
			Separate WBR			X	X
48	148th Avenue NE	Bel-Red Road	Double EBL	X	X	X	X
			Separate WBR	X	X		
49	148th Avenue NE	NE 8th Street	Double NBL	X	X	X	X
			Double SBL	X	X	X	X
			Separate WBR	X	X		
51	148th Avenue NE	Lake Hills Blvd	Separate WBL			X	X
			Separate EBR		X		
59	Bel-Red Road	NE 24th Street	Separate SBR			X	X
61	156th Avenue NE	NE 24th Street	Separate EBR	X	X	X	X
			Double NBL			X	
62	156th Avenue NE	Northup Way NE	Double NBL	X	X	X	X
			Double SBL	X	X	X	
			Additional EBT	X	X	X	X
64	140th Avenue NE	NE 24th Street	Separate NBR	X	X	X	X
			Separate SBR	X	X		
			Separate EBR			X	X
68	130th Avenue NE	NE 20th Street	Separate EBR		X	X	
			Separate WBR		X	X	
74	Bellevue Way NE	Northup Way NE	Double WBL	X	X	X	X
			Separate NBR	X	X	X	X
			Separate SBR	X	X	X	X
75	164th Avenue NE	NE 24th Street	Separate EBR			X	
77	130th Avenue NE	NE 24th Street	Separate WBL	X	X	X	X
78	108th Avenue NE	Northup Way NE	Separate WBR	X	X	X	X
79	148th Avenue NE	NE 40th Street	Separate NBR	X	X	X	X

TABLE 3-1
Comparison of Proposed Intersection Improvements

Int. No.	Street Names		Intersection Improvements	FEIS No-Action Alternative	Mod. No-Action Alternative	FEIS PPA	FEIS Addendum Analysis
87	164th Avenue NE	NE 8th Street	Separate SBR Separate EBR	X	X	X	X
88	124th Avenue NE	Northup Way NE	WB channelization ¹		X		
131	116th Avenue NE	SE 1st Street	Separate WBL	X	X		
138	Bel-Red Road	NE 40th Street	Separate EBR	X	X	X	X
139	116th Avenue NE	NE 4th Street	Double EBL Double WBL Separate NBR Separate SBR			X X X X	X X X X
188	148th Avenue NE	NE 29th Place	Double WBL ² Separate WBT Separate EBL Separate EBR	X X X X	X X X X	X X X X	X X X X
231	Bel-Red Road	NE 30th Street	Signalize	X	X	X	X
233	120th Avenue NE	NE 8th Street	Double EBL Separate SBR Separate NBR			X X X	X X X
255	156th Avenue NE	NE 51st Street	Separate EBL Separate WBL	X X	X X	X	
901	112th Avenue NE	NE 10th Street	Double NBL Double WBL	X	X	X X	
904	120th Avenue NE	NE 16th Street	Separate SBR Separate WBR			X X	X X
905	124th Avenue NE	NE 16th Street	Separate NBR Separate SBR Double WBL Double EBL	X X		X X X X	X X X X
906	130th Avenue NE	NE 16th Street	Separate NBR			X	
907	136th Place NE	NE 20th Street	Double WBL			X	X
Total Number of Intersection Improvements:				44	49	67	52

NOTES:

EBL = eastbound left

WBL = westbound left

NBL = northbound left

SBL = southbound left

LT = left turn

TH-RT = through, right turn shared

NBR = northbound right

SBR = southbound right

EBT = eastbound through

WBT = westbound through

RT = right turn

LT-TH = left turn, through shared

EBR = eastbound right

WBR = westbound right

NBT = northbound through

SBT = southbound through

TH = through

¹ Change WB from LT, TH, TH, and RT to LT, TH, TH-RT, and RT² Change WB shared LT-TH lane to double lefts and single through lane

Modified No Action Alternative Channelization Changes. As a result of the removal of the two ramp connections to SR 520 at 124th Avenue NE for the No-Action Alternative, six new intersection improvements were needed to improve operations for the No-Action Alternative as originally described in the FEIS. These intersection improvements for the modified No-Action Alternative are discussed in the text that follows:

- For the intersection of 120th Avenue NE and NE 12th Street, changes to the northbound channelization as described in Table 3-1, and the addition of a short receiving lane on the north leg, improve the intersection operation from LOS F to LOS E.
- Adding a southbound double left-turn pocket at the intersection of 124th Avenue NE and NE 8th Street improves this intersection's operation from LOS E to LOS D.
- For the intersection of 148th Avenue NE and Lake Hills Boulevard, adding an eastbound right-turn pocket improves intersection operations from LOS E to LOS D.
- Adding eastbound and westbound right-turn pockets at 130th Avenue NE and NE 20th Street improves this intersection's operations from LOS E to LOS D.
- Modifying the channelization for the westbound approach for 124th Avenue NE and Northup Way NE improves intersection operations from LOS E to LOS D.
- At the intersection of 116th Avenue NE and NE 4th Street, adding a southbound right-turn pocket improves this intersection's operations from LOS E to LOS D.

FEIS Addendum Intersection Channelization Analysis Results. Since the publication of the FEIS, some recommended intersection improvements for the PPA have also been revised. These changes are discussed below. A determination of the value of roadway capacity improvements was considered relative to other factors such as property impacts, pedestrian crossing distance, stormwater management, and overall urban design. Where the impacts of intersection widening would be great, and where an acceptable level of service could be achieved without adding turn lanes, those previously recommended turn lanes were eliminated.

For the following intersections, proposed double left-turn lanes in the PPA as identified in the FEIS are recommended not to be included, but even with the change, the intersection would still operate at the same level of service as was documented in the FEIS. A more detailed discussion of the operations at each of these intersections follows.

- **124th Avenue NE and Bel-Red Road – westbound left.** At the intersection of 124th Avenue NE and Bel-Red Road, double left-turn lanes were proposed in the PPA to accommodate high volumes of westbound traffic. The proposed double left-turn lanes were reduced to a single left-turn lane, and the overall LOS would remain at LOS E.
- **140th Avenue NE and NE 20th Street – westbound left.** At the 140th Avenue NE and NE 20th Street intersection, removal of the westbound double left-turn lane would not affect operations, as the LOS would remain at LOS E with or without the second westbound left-turn lane, as shown in Table 3-3, later in this chapter.

- **156th Avenue NE and NE 24th Street – northbound left.** Without the proposed double left-turn lanes, the intersection of 156th Avenue NE and NE 24th Street would continue to operate acceptably at LOS D.
- **156th Avenue NE and Northup Way – southbound left.** Similarly, if just the single southbound left-turn lane was retained at the intersection of 156th Avenue NE and Northup Way, the average delay at this intersection would increase by 5 seconds per vehicle compared with the FEIS PPA, but would be less than the unmitigated or mitigated modified No-Action Alternative.

As a result of this analysis, all four of these FEIS improvements are recommended to be removed from further consideration.

For the following intersections, proposed right-turn lane mitigation was originally identified in the PPA, but has been eliminated based on the change in demand. At all of these intersections, operations with or without the proposed improvement would remain the same or provide better level of service as was documented in the FEIS for the PPA. Therefore, elimination of the earlier recommended mitigation does not adversely affect the operations of the transportation system. These intersections are as follows:

- 140th Avenue NE and NE 20th Street – southbound right
- 130th Avenue NE and NE 20th Street – eastbound right and westbound right
- 164th Avenue NE and NE 24th Street – eastbound right
- 130th Avenue NE and NE 16th Street – northbound right

As part of the future network described in the FEIS PPA, the 148th Avenue NE and NE 20th Street intersection was proposed to include double left-turn lanes on each approach. The 148th Avenue NE corridor (Bel-Red Road to NE 29th Place) will be evaluated further to identify a corridor-wide improvement. Until this study is completed, however, it is recommended that the northbound and southbound double left-turn lanes at the intersection of 148th Avenue NE and NE 20th Street not be included. The removal of the second left-turn lane on these two approaches worsens operations at this intersection from LOS E to LOS F, with a 27 percent increase in average delay expected.

At the intersection of NE 12th Street and 112th Avenue NE, a separate northbound right-turn lane was assumed as an additional intersection improvement in the FEIS PPA. However, this improvement was subsequently identified as impractical because of right-of-way constraints and potential conflicts with an underground parking garage. This intersection would operate at LOS F with or without the northbound right-turn lane.

Also included in the FEIS were the double westbound and double northbound left-turn lane improvements at NE 10th Street and 112th Avenue NE. These double lanes would provide additional capacity when the NE 10th Street bridge crossing to 116th Avenue NE is complete. This intersection is currently under construction and does not include these double left turn lanes. Because of potential conflicts with stormwater detention facilities and right-of-way limits, it is not likely these proposed improvements would be built in the future. The impact to operations at this intersection would be a degradation in LOS from E to F.

While not in the City of Bellevue, it was also determined that a separate eastbound left-turn lane would no longer be needed at the intersection of 156th Avenue NE and NE 51st Street, which was assumed as part of the FEIS PPA. By removing this eastbound left-turn, this intersection would continue to operate at an acceptable LOS D.

No additional changes were made at the remaining intersections that were proposed to have channelization improvements in the FEIS.

In summary, the following changes were made to the transportation improvements included in the FEIS PPA:

- Do not add a second southbound through lane on 116th Avenue NE, from Northup Way to NE 12th Street (Project R-4 in FEIS)
- Widen 120th Avenue NE from two to five lanes, from Northup Way to Bel-Red Road (Project R-5 in FEIS)
- Do not construct the NE 10th Street extension from 116th Avenue NE to 124th Avenue NE (Project R-11 in FEIS)
- Do not construct a two-lane roadway connection from 124th Avenue NE, north Bel-Red Road and under NE 12th Street, to Old Bel-Red Road/NE 10th Street extension (new improvement identified by the City of Bellevue)
- Do not construct the new NE 11th Street roadway, east of 116th Avenue NE, between 116th Avenue NE and NE 12th Street/Bel-Red Road (new improvement identified by the City of Bellevue) within the 2030 planning horizon, but retain this project in the *Bel-Red Subarea Plan* for future consideration.
- Revise the project description for 130th Avenue NE to be a two-lane, pedestrian-oriented street with on-street parking north of NE 16th Street, and a three-lane street south of NE 16th Street (Project R-7 in FEIS)
- Add the six intersection improvements to the modified No-Action Alternative, as described above
- Eliminate 11 intersection improvements from the PPA, as described above

Roadway Traffic Volumes

Figures 3-1 and 3-2 illustrate the p.m. peak-hour roadway link traffic volumes for the 2030 original No-Action Alternative and the modified No-Action Alternative, respectively. Figures 3-3, and 3-4 illustrate the p.m. peak-hour roadway link traffic volumes for the 2030 FEIS PPA and *Bel-Red Subarea Plan*, respectively.

Table 3-2 summarizes the daily person work trips (with one trip end being travel to or from home) by mode for the modified No-Action Alternative, the FEIS PPA and the *Bel-Red Subarea Plan*. As shown in Table 3-2, total daily home-based work person trips would grow under the *Bel-Red Subarea Plan* relative to the No-Action Alternative and the modified No-Action Alternative; this is consistent with the earlier FEIS analysis comparing the PPA with the No-Action Alternative.

TABLE 3-2
Daily Home-Based Work Person Trips by Mode in the Bel-Red Corridor Study Area

	No-Action Alternative ¹		Preliminary Preferred Alternative		<i>Bel-Red Subarea Plan</i>	
	Trips	Share	Trips	Share	Trips	Share
Transit via Walk	2,397	8%	9,288	17%	8,962	16%
Transit via Park and Ride	356	1%	1,324	2%	1,410	3%
Drive Alone	27,655	86%	42,118	76%	42,243	76%
Shared Ride	1,669	5%	2,504	5%	2,501	5%
Total	32,077	100%	55,234	100%	55,116	100%

¹ No change in values between the original (FEIS) No-Action Alternative and the modified No-Action Alternative.

In the modified No-Action Alternative, 86 percent of home-based work person trips are drive alone, 5 percent are ride sharing, and 9 percent are trips by transit, including trips on transit from walking to and from park-and-ride lots. These values did not change from the original No-Action Alternative in the FEIS.

The important points to be drawn from Table 3-2 are that the *Bel-Red Subarea Plan* work trips (55,116 trips) would be consistent with work trips projected in the FEIS for the PPA (55,234 trips), as would be expected because the development program and the land use distribution would be the same. Additionally, drive-alone work trips would also be a comparable percentage of all trips under the *Bel-Red Subarea Plan* relative to the PPA, as would transit-related trips. There is no change to the mode share for transit, drive alone, and shared ride for the *Bel-Red Subarea Plan* compared with the PPA.

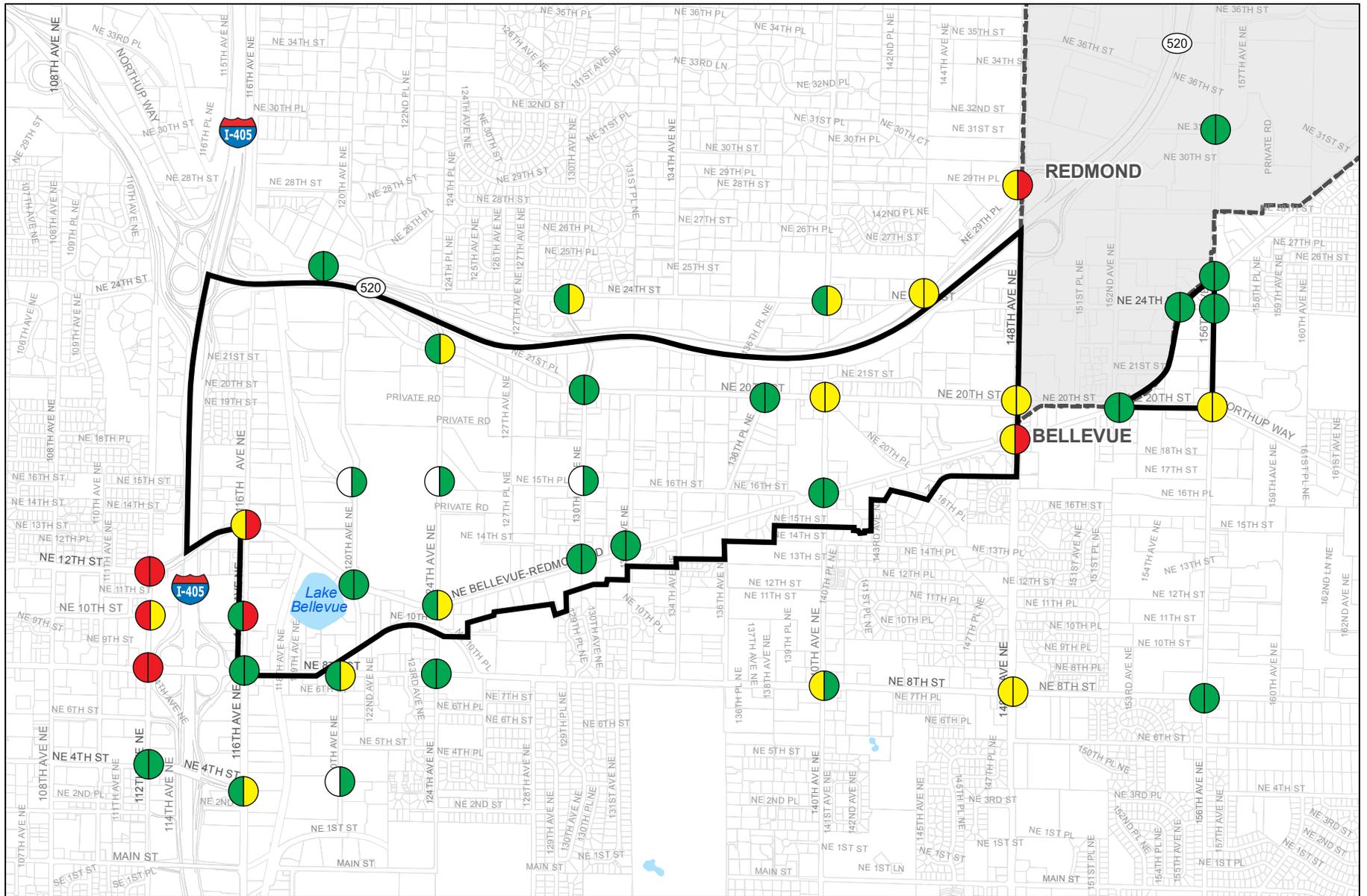
Intersection Traffic Operations

Figures 3-5 and 3-6 summarize the updated 2030 p.m. peak-hour intersection LOS and average delays for the original No-Action Alternative and the PPA. The modified No-Action Alternative and the intersection LOS and average delays analyzed in the Addendum are illustrated in Figures 3-7 and 3-8, respectively. Table 3-3 lists these LOS and delay results. The intersections expected to operate at LOS F in 2030 are shown in bold and italics. The results of the updated LOS analysis were further reviewed and summarized to compare the number of intersections operating at LOS E and F and overall corridor-wide average intersection delays; Table 3-4 summarizes the results of these comparisons.

Modified No Action Alternative Operational Comparison. As shown in Table 3-4, the modified No-Action Alternative would result in 31 intersections operating at LOS E or F during the PM peak hour in 2030, assuming only funded improvements as compared with 25 intersections operating at LOS E or F documented in the FEIS for the No-Action Alternative. The reason for the increase in LOS E and F intersections is due to the removal of the interchange improvement at SR 520/124th Avenue NE. Average intersection delays would increase substantially over existing conditions.

As described in the previous section, additional intersection mitigation improvements, over and above those identified for the No-Action Alternative, were identified to allow an equal comparison with the action alternatives. These intersection improvements are listed in Table 3-1. With these additional intersection improvements in place, average intersection operations for the modified No-Action Alternative would improve. The number of intersections operating at LOS E or F for the modified No-Action Alternative would be reduced from 31 to 19. In general, most of the intersections that would operate at LOS F would improve to LOS E or better.

Compared with the original No-Action Alternative results in the FEIS, the number of intersections operating at LOS E or F under the modified No-Action Alternative would increase by 6 intersections for unmitigated conditions, and by 1 intersection with mitigated conditions. The average delay for the modified No-Action Alternative would increase by approximately 84 seconds (unmitigated) and by 7 seconds (mitigated) from the original No-Action Alternative.

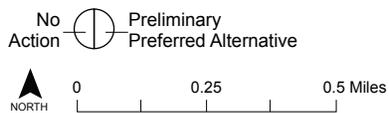


LEGEND

- Bel-RedCorridor
- Roadway
- City boundary
- Parcel
- Lake

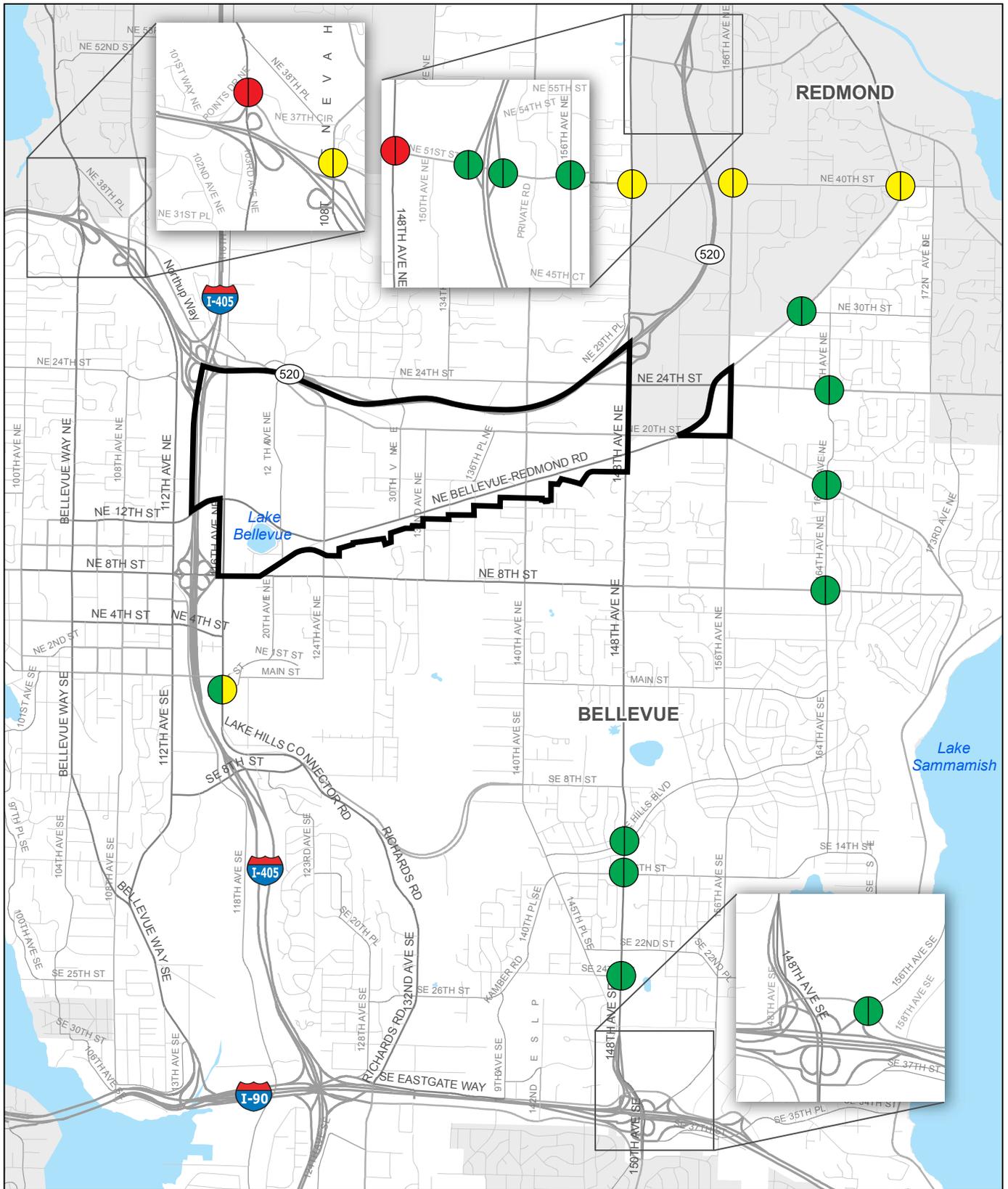
Intersections level of service (LOS):

- LOS A, B, C, or D
- LOS E
- LOS F
- Not applicable
- Preliminary
- Preferred Alternative



**Figure 3-5
Future (2030)
LOS Conditions**

Bel-Red Corridor Final EIS Addendum



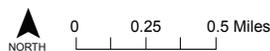
LEGEND

Bel-Red Corridor

Intersections level of service (LOS):

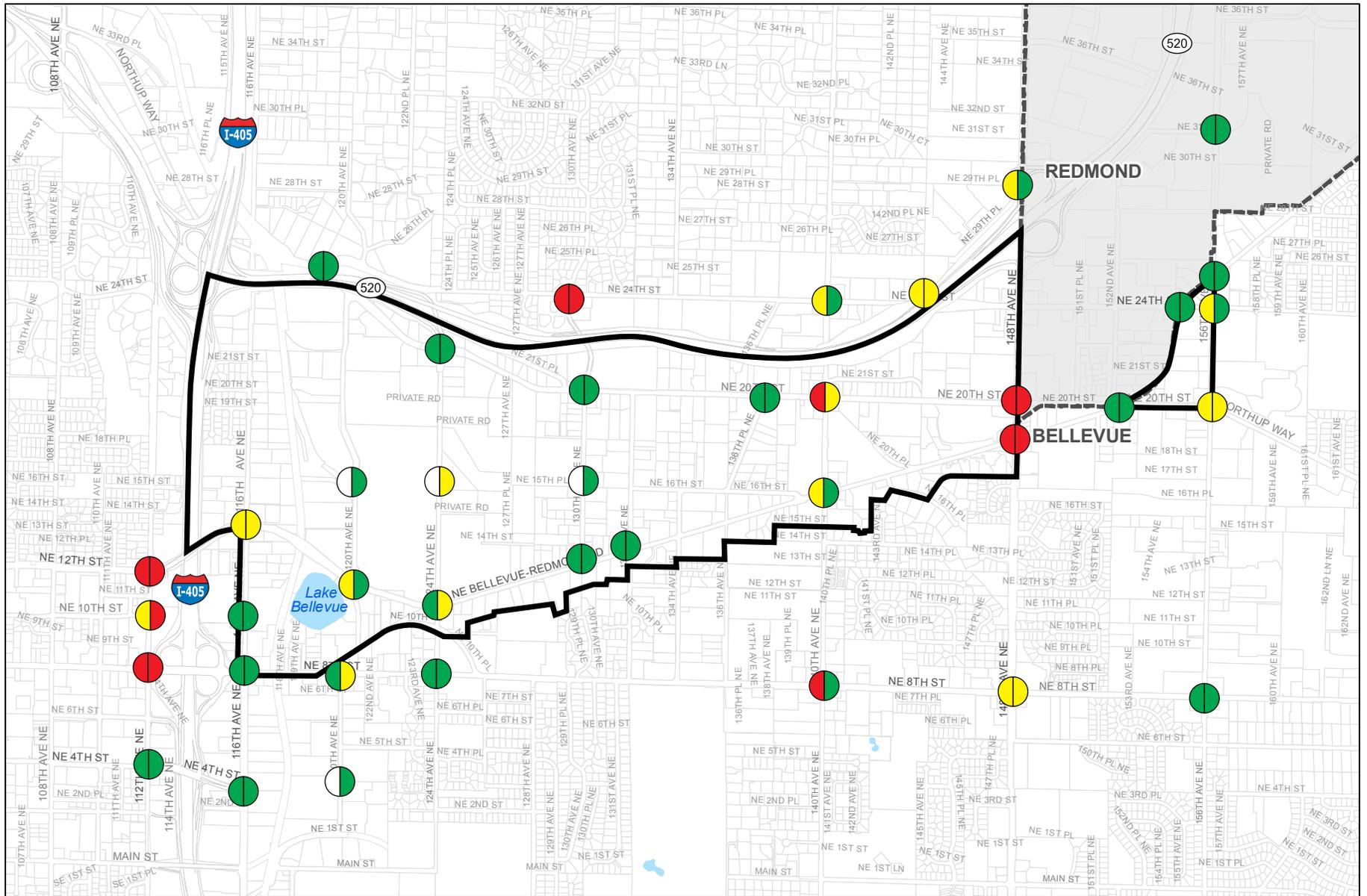
- LOS A, B, C, or D
- LOS E
- LOS F

No Preliminary
Action Preferred Alternative



**Figure 3-6
Future (2030) LOS
Conditions**

Bel-Red Corridor Final EIS Addendum

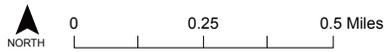


LEGEND

- Bel-Red Corridor
- Roadway
- Parcel
- City boundary
- Lake

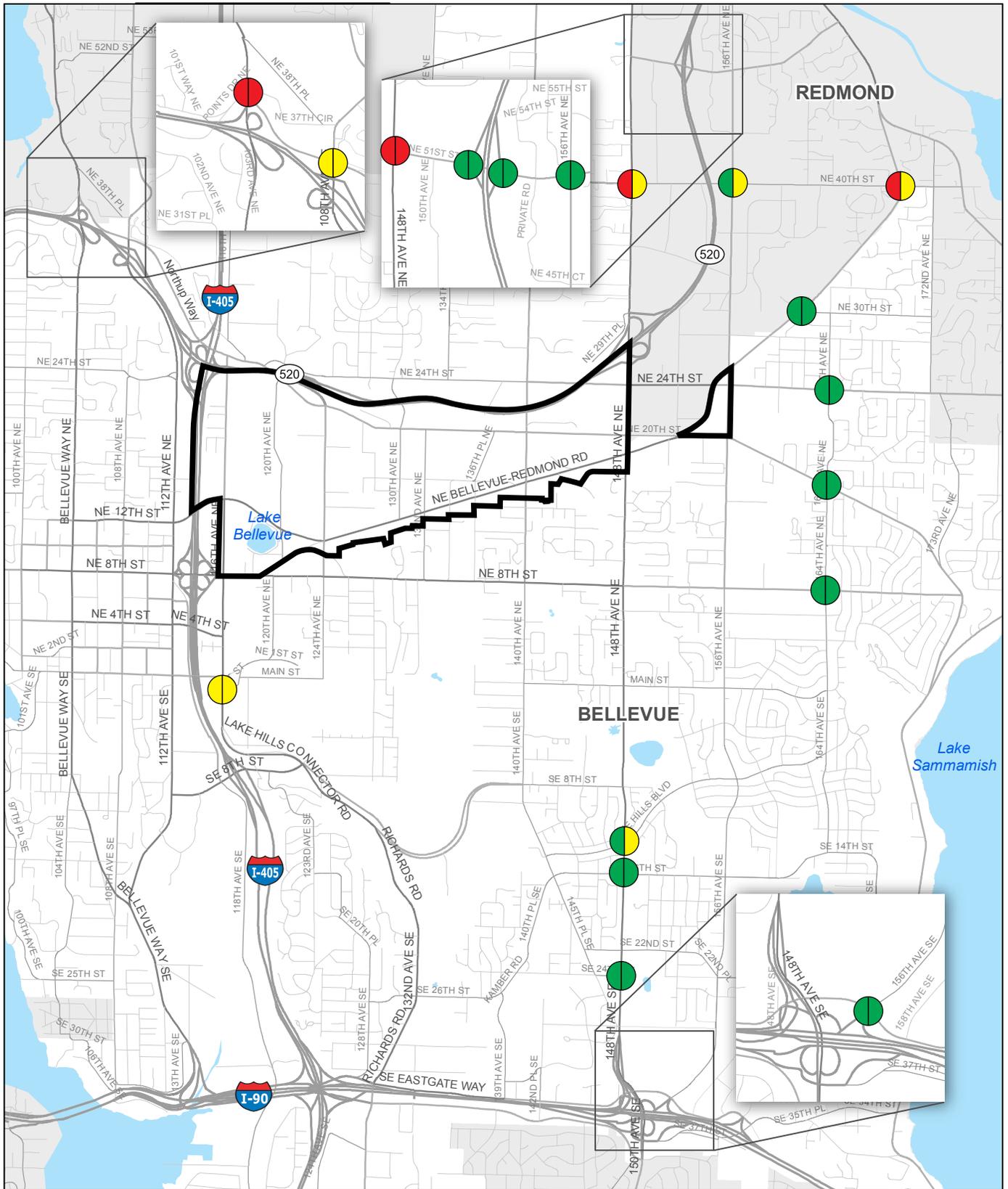
Intersections level of service (LOS):

- LOS A, B, C, or D
- LOS E
- LOS F
- Not applicable
- Modified No Action
- Bel-Red Subarea Plan



**Figure 3-7
Future (2030)
LOS Conditions**

Bel-Red Corridor Final EIS Addendum



LEGEND

Bel-Red Corridor

Intersections level of service (LOS):

- LOS A, B, C, or D
- LOS E
- LOS F

Modified No Action Bel-Red Subarea Plan

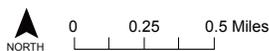


Figure 3-8
Future (2030) LOS Conditions
 Bel-Red Corridor Final EIS Addendum

TABLE 3-3
2030 PM Peak-Hour Intersection LOS and Delay

Int. No.	Streets		FEIS No-Action Alternative ¹		Mod. No-Action Alternative ¹		FEIS PPA		FEIS Addendum Analysis ²	
			LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay
25	112th Avenue NE	NE 12th Street	<i>F (F)</i>	136 (94)	<i>F (F)</i>	114 (81)	<i>F</i>	106	<i>F</i>	173
26	112th Avenue NE	NE 8th Street	<i>F</i>	115	<i>F</i>	124	<i>F</i>	129	<i>F</i>	>180
29 ³	116th Avenue NE	NE 12th Street	<i>F (E)</i>	97 (71)	<i>F (E)</i>	90 (74)	<i>F</i>	92	E	75
30	116th Avenue NE	NE 8th Street	D	51	D	51	D	41	D	50
32 ³	120th Avenue NE	NE 12th Street	C	35	<i>F (E)</i>	84 (70)	B	18	C	25
34 ³	124th Avenue NE	Bel-Red Road	D	38	E (D)	58 (49)	E	75	E	74
35	124th Avenue NE	NE 8th Street	D	45	E (D)	62 (42)	D	51	D	51
37 ³	130th Avenue NE	Bel-Red Road	C	23	C	24	D	41	B	13
38	132nd Avenue NE	Bel-Red Road	C	26	C	35	D	41	B	14
39 ³	140th Avenue NE	NE 20th Street	<i>F (E)</i>	103 (63)	<i>F (F)</i>	137 (86)	E	80	E	74
40 ³	140th Avenue NE	Bel-Red Road	E (D)	66 (53)	<i>F (E)</i>	82 (56)	D	50	D	42
41	140th Avenue NE	NE 8th Street	E	71	<i>F</i>	84	D	46	D	37
47 ³	148th Avenue NE	NE 20th Street	<i>F (E)</i>	109 (68)	<i>F (F)</i>	134 (91)	E	70	<i>F</i>	89
48 ³	148th Avenue NE	Bel-Red Road	<i>F (E)</i>	93 (80)	<i>F (F)</i>	111 (92)	<i>F</i>	92	<i>F</i>	87
49	148th Avenue NE	NE 8th Street	<i>F (E)</i>	88 (65)	<i>F (E)</i>	95 (75)	E	77	E	78
51	148th Avenue NE	Lk. Hills Blvd.	D	50	E (D)	63 (49)	D	54	E	66
52	148th Avenue NE	SE 16th Street	C	24	C	31	C	25	C	27
55	148th Avenue SE	SE 24th Street	D	46	D	53	D	49	D	49
58	Bel-Red Road	NE 20th Street	D	45	D	45	C	46	C	37
59 ³	Bel-Red Road	NE 24th Street	C	33	D	48	D	38	D	52
60 ³	156th Avenue NE	Bel-Red Road	D	51	D	51	D	46	C	30
61 ³	156th Avenue NE	NE 24th Street	<i>F (D)</i>	90 (52)	<i>F (E)</i>	143 (79)	D	50	D	55
62	156th Avenue NE	Northup Way NE	<i>F (E)</i>	116 (63)	<i>F (E)</i>	128 (76)	E	64	E	69
63	156th Avenue NE	NE 8th Street	D	53	D	53	D	40	D	47
64	140th Avenue NE	NE 24th Street	E (D)	73 (53)	<i>F (E)</i>	101 (69)	E	56	D	43
68 ³	130th Avenue NE	NE 20th Street	D	50	E (D)	61 (53)	D	52	D	39
72	112th Avenue NE	NE 4th Street	D	41	D	41	D	44	D	43
74	Bellevue Way NE	Northup Way NE	<i>F (F)</i>	129 (86)	<i>F (F)</i>	134 (104)	<i>F</i>	105	<i>F</i>	85
75	164th Avenue NE	NE 24th Street	D	43	D	43	C	34	D	48
76	164th Avenue NE	Northup Way NE	D	43	D	54	D	42	D	41
77	130th Avenue NE	NE 24th Street	<i>F (D)</i>	84 (33)	<i>F (F)</i>	108 (71)	E	65	<i>F</i>	55
78	108th Avenue NE	Northup Way NE	<i>F (E)</i>	83 (58)	E (E)	79 (63)	E	75	E	76
79	148th Avenue NE	NE 40th Street	<i>F (E)</i>	94 (68)	<i>F (F)</i>	159 (129)	E	73	E	68
86	156th Avenue SE	SE Eastgate Way	D	49	D	51	D	48	D	48
87	164th Avenue NE	NE 8th Street	<i>F (D)</i>	81 (45)	<i>F (D)</i>	94 (56)	D	54	D	37
88 ³	124th Avenue NE	Northup Way NE	D	51	E (D)	76 (51)	E	68	D	36

TABLE 3-3
2030 PM Peak-Hour Intersection LOS and Delay

Int. No.	Streets		FEIS No-Action Alternative ¹		Mod. No-Action Alternative ¹		FEIS PPA		FEIS Addendum Analysis ²	
			LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay
118	Northup Way	NE 24th Street	C	29	C	29	C	32	B	15
131	116th Avenue NE	SE 1st Street	E (D)	69 (47)	F (E)	101 (57)	E	73	E	59
138	Bel-Red Road	NE 40th Street	F (E)	87 (70)	F (F)	110 (89)	E	72	E	77
139	116th Avenue NE	NE 4th Street	D	55	E (D)	61 (40)	E	60	D	52
188	148th Avenue NE	NE 29th Place	F (E)	108 (72)	F (E)	171 (58)	F	86	D	48
189	NE 29th Place	NE 24th Street	E	62	E	68	E	66	E	70
231	Bel-Red Road	NE 30th Street	C	33	F (D)	>180 (41)	D	36	C	27
233	120th Avenue NE	NE 8th Street	C	22	B	14	E	79	E	70
239	156th Avenue NE	NE 40th Street	E	56	D	53	E	56	E	56
249	148th Avenue NE	NE 51st Street	F	>180	F	>180	F	>180	F	>180
250	SR 520 SB ramps	NE 51st Street	B	12	B	12	B	12	B	12
251	SR 520 NB ramps	NE 51st Street	C	35	C	35	C	34	C	34
255	156th Avenue NE	NE 51st Street	F (D)	73 (48)	D (D)	46 (43)	C	28	D	52
264	156th Avenue NE	NE 31st Street	D	44	D	44	D	51	D	38
901	112th Avenue NE	NE 10th Street	F (F)	118 (88)	F (E)	100 (77)	E	77	F	172
902	116th Avenue NE	NE 10th Street	C	28	D	36	F	102	C	32
904	120th Avenue NE	NE 16th Street	---	---	---	---	D	48	D	55
905	124th Avenue NE	NE 16th Street	---	---	---	---	D	55	E	68
906	130th Avenue NE	NE 16th Street	---	---	---	---	D	48	D	50
907	136th Place NE	NE 20th Street	C	34	C	34	D	46	C	30
908	120th Avenue NE	NE 4th Street	---	---	---	---	C	21	A	9

Source: Analyses performed by CH2M HILL in 2006, 2007, and 2008.

NOTE: The intersections expected to operate at LOS F in 2030 are shown in bold and italics.

¹ Numbers in parentheses are the results with additional intersection channelization improvements identified in this Addendum that are beyond those specified in the Bel-Red/Overlake Transportation Study, the City of Bellevue *Transportation Facilities Plan*, and the NE 10th Street Extension and Overlake Hospital Medical Center Expansion Environmental Impact Statement (EIS).

² Level of service (LOS) and delay results shown are based on the assumption that intersection improvements listed in Table 3-1 will be made, to optimize operational performance for the alternatives. Additional analysis will be undertaken to determine whether these intersection improvements will be implemented.

³ Intersections located within the Bel-Red/Northup mixed commercial/residential area (MMA 12).

TABLE 3-4
Corridorwide Comparison of 2030 PM Peak-Hour Intersection LOS and Delay Results

Criteria	2030 Alternatives	
	No-Action ¹	Preliminary Preferred
All 58 Intersections (FEIS)		
Number of intersections operating at LOS E	6 (13)	17
Number of intersections operating at LOS F	19 (5)	8
Total intersections operating at LOS E or F	25 (18)	25
Average intersection delay per vehicle (in seconds)	67 (54.3)	59.8
All 58 Intersections (Addendum)	Modified No-Action¹	FEIS Addendum²
Number of intersections operating at LOS E	8 (11)	14
Number of intersections operating at LOS F	23 (8)	8
Total intersections operating at LOS E or F	31 (19)	22
Average intersection delay per vehicle (in seconds)	150.9 (61.7)	58.9

Source: Analyses performed by CH2M HILL in 2006, 2007, and 2008.

¹ Numbers in parentheses are the results with additional intersection mitigation improvements beyond those specified in the Bel-Red/Overlake Transportation Study, the City of Bellevue *Transportation Facilities Plan*, and the NE 10th Street Extension and Overlake Hospital Medical Center Expansion Environmental Impact Statement (EIS).

² Level of service (LOS) and delay results shown are based on the assumption that additional intersection improvements listed in Table 3-1 will be made, to optimize operational performance for the alternatives. Additional analysis will be undertaken to determine whether these intersection improvements will be implemented.

Bel-Red Subarea Plan Operational Comparison. For the FEIS Addendum analysis of intersection improvements, the number of intersections operating at LOS E or F would be similar to the mitigated modified No-Action Alternative. This is consistent with expectations, as the plan's greater levels of development would be offset by the proposed transportation improvements. The overall average delay per intersection for the *Bel-Red Subarea Plan* would be slightly lower (about 3 seconds) than the mitigated modified No-Action Alternative, but significantly lower (92 seconds) than the unmitigated modified No-Action Alternative. In addition, despite the various changes to the network described earlier in this Addendum, the updated LOS analysis illustrates very little change in the total number of intersections operating at LOS E or F for the *Bel-Red Subarea Plan*, when compared with the PPA results in the FEIS. There are three fewer LOS E intersections, and overall the average delay for the *Bel-Red Subarea Plan*, compared with the PPA, is only marginally lower.

Transportation System Performance

This section summarizes how the overall transportation system would perform under the modified No-Action Alternative and the updated *Bel-Red Subarea Plan* analysis in comparison with the results presented in the FEIS. Vehicle miles traveled (VMT), vehicle hours traveled (VHT), and average speed are three common system performance measures.

Because the BKR model covers the entire Puget Sound region, a subarea system was created to measure the performance of the Bel-Red Corridor study area and nearby surroundings. The VMT, VHT, and average speed of all vehicles traveling in the Bel-Red Corridor were summarized for the 2030 p.m. peak hour.

As shown in Table 3-5, the *Bel-Red Subarea Plan's* VHT and VMT are 5.5 percent and 3.3 percent higher, respectively, than the Modified No-Action Alternative. This reflects increased travel demand generated from growth in population and employment, as well as planned network improvements in the Bel-Red Subarea.

TABLE 3-5
Bel-Red Corridor Study Vehicle Hours Traveled, Vehicle Miles Traveled, and Average Speed Summary

	Original No-Action Alternative	Modified No-Action Alternative	Preliminary Preferred Alternative	<i>Bel-Red Subarea Plan</i>
Vehicle hours traveled	10,118	10,332	10,903	10,902
Vehicle miles traveled	200,131	200,443	207,208	206,917
Average speed (miles per hour)	19.96	19.90	19.80	19.71
Change from No-Action ¹ (percent)				
Vehicle hours traveled	0	2.1	7.8	5.5
Vehicle miles traveled	0	1.6	3.5	3.3
Average speed (percent change)	0 %	-0.3%	-0.8%	-1.0%

Source: BKR Model, and analyses performed by CH2M HILL in 2006 and 2007.

¹Percent change for the Modified No Action and Preliminary Preferred Alternative is calculated compared with the Original No Action Alternative, whereas the percent change for the Bel-Red Subarea Plan is calculated compared with the Modified No Action Alternative.

The increases in VHT and VMT for the *Bel-Red Subarea Plan* relative to the PPA are similar to those found in the FEIS, and marginally better. The *Bel-Red Subarea Plan*, similar to the FEIS PPA, shows a slightly greater increase in VHT than VMT when compared with the Modified No-Action Alternative, resulting in a decrease of 1.0 percent in average speed.

Mitigation Measures

This section describes the proposed measures that the City of Bellevue could implement to minimize impacts during project operation.

Roadways

The transportation system improvements listed in Table A-1 of the FEIS, in effect, serve as mitigation for the PPA, although these improvements have been included in all the previously documented operational analysis. New transportation system improvements for the *Bel-Red Subarea Plan*, compared with the FEIS PPA, are listed below. With these improvements, no further mitigation is needed:

- Although the construction of the new NE 11th Street roadway, east of 116th Avenue NE, between 116th Avenue NE and NE 12th Street/Bel-Red Road (new improvement identified by the City of Bellevue) would not be implemented within the 2030 planning horizon, this improvement would be included in the *Bel-Red Subarea Plan* for future consideration. (Project R-14)
- Widen 120th Avenue NE from two to five lanes, from Northup Way to Bel-Red Road. (Project R-5)
- Revise the 130th Avenue NE project description to be a two-lane, pedestrian-oriented street with on-street parking north of NE 16th Street, and a three-lane street south of NE 16th Street. (Project R-7)

In addition, the intersection channelization improvements listed in Table 3-2 are recommended as additional spot improvements were assumed to be in place by 2030 through implementation of transportation facilities plans. These intersection improvements were evaluated as part of the modified No-Action Alternative and this FEIS Addendum. Future (2030) LOS analysis results listed in Table 3-5 and shown in Figures 3-7 and 3-8 assume that these intersection improvements would be in place.

Unavoidable Adverse Impacts

Increases in traffic volumes and corresponding increases in congestion (including intersections projected to operate at LOS F) would occur in the Bel-Red Corridor, surrounding neighborhoods, and the regional transportation system with both the modified No-Action Alternative and the *Bel-Red Subarea Plan*. These increases would be due in part to regional factors, including economic growth and land use changes in areas outside of Bellevue.

CHAPTER 4

References

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City of Bellevue. 1975. *Bellevue Land Use Code, Title 20*. Available at: <http://www.bellevuewa.gov/bellcode/blvlucnt.html>. Updated through March 3, 2008.

ATTACHMENT 1

Bel-Red Subarea Plan Draft

ATTACHMENT 1

CITY OF BELLEVUE, WASHINGTON

ORDINANCE NO. _____

AN ORDINANCE relating to the Comprehensive Plan of the City of Bellevue, as required and adopted pursuant to the Growth Management Act of 1990, as amended (Chapter 36.70A RCW); adopting 2008 amendments to the Comprehensive Plan and known as the Bel-Red CPA: amending the Bel-Red/Northup Subarea Plan by replacing it with new text, policies, and maps and adding transportation, parks and open space, and environment and trail project lists; amending the boundaries of the Crossroads and Wilburton/NE 8th Street Subarea Plans to align with the new Bel-Red Subarea Plan; amend the Transportation Element to add new mobility management policy related to the BNSF corridor, and amendments to the Mobility Management Areas and the Level of Service standard in Figure TR.1 and Table TR.1 respectively; and add new definitions to the Glossary.

WHEREAS, on August 1, 2005, the City Council initiated the Bel-Red Corridor Project, for purposes of addressing the economic presence of this aging economic area and reexamining land use in the corridor in light of economic challenges facing Bellevue in the years ahead as identified in the 2004 Update of the Comprehensive Plan; and

WHEREAS, the City Council established major goals and objectives for Bel-Red in order to work with the community to plan and manage change rather than accommodate the inevitable change in a haphazard, piecemeal way; and

WHEREAS, these goals included identifying a preferred long-term land use vision for the Bel-Red corridor that provides clear and deliberate direction for the area's future; enhances the economic vitality of this area and of the larger city, complements Downtown Bellevue and other employment centers in the city; strongly integrates land use and transportation systems in an environmentally sustainable manner; devises a multi-modal transportation system for the area that accommodates future growth, enhances overall mobility, and mitigates impacts on adjoining areas; evaluates the impact and opportunities presented by Sound Transit's East Link light rail system through the area on both land use and transportation, and identifies a preferred light rail route and station locations through this corridor in order to coordinate future route and station decisions with Sound Transit; identifies community and neighborhood amenities that will complement the preferred land use vision for the area and serve the broader

community, and protects adjoining areas from impacts of land use and transportation changes in the study area; and

WHEREAS, Sound Transit's proposed East Link light rail system has provided an opportunity for the City to re-think the relationship between land use and transportation in the Bel-Red area; and

WHEREAS, the City has engaged in a multi-year planning process for the Bel-Red area that has included the work of the Bel-Red Steering Committee, the Planning Commission and other City boards and commissions. This work resulted in an extensive update to the Bel-Red/Northrup Subarea Plan, now known as the Bel-Red Subarea Plan (Plan) and associated amendments to support the vision developed in the Plan; and

WHEREAS, the City Council envisions Bel-Red in 2030 as an area that is unique within the city, where thriving businesses will be adjacent to, and sometimes mixed with, livable neighborhoods, all served by a multi-modal transportation system that connects the area to the greater city and region; that the area will also be distinguished by environmental and community amenities serving residents and employees in the area, as well as residents from surrounding neighborhoods and the entire city; and that the area will transition gracefully over time, with existing businesses being accommodated while new types of development occur as conditions warrant; and

WHEREAS, the Plan is consistent with Bellevue's community and economic development strategy to pursue redevelopment and reinvestment in older commercial areas of the city; and

WHEREAS, the Plan supports and is supported by a light rail route between downtown Bellevue and Overlake, traversing the Bel-Red Subarea on an alignment along approximately NE 15th/16th Street, with stations at planned development nodes; and

WHEREAS, the Plan includes a concentration of the majority of growth in the Bel-Red Subarea into a series of mixed-use, pedestrian-friendly and transit-oriented development nodes around the anticipated light rail transit stations; and

WHEREAS, in November, 2008, the voters of the three-county Sound Transit district approved Proposition 1, which includes funding for Sound Transit's East Link light rail light line through the Bel-Red Subarea, connecting that area with Downtown Bellevue (and Mercer Island and Seattle) to the west and with the Overlake Area of Redmond to the northeast; and

WHEREAS, Sound Transit is now implementing Proposition 1 and is preparing to issue a Final Environmental Impact Statement studying the potential

alignments and stations of the East Link, including the alignments and stations in the Bel-Red area; and

WHEREAS, the Plan identifies necessary transportation projects and supports actions to preserve necessary rights-of-way for the transportation projects identified in the Plan; and

WHEREAS, the Planning Commission held a public hearing on May 28, 2008, with regard to the Bel-Red CPA; and

WHEREAS, on July 23, 2008, the Planning Commission recommended that the City Council approve such proposed amendments; and

WHEREAS, the City Council has considered the Bel-Red CPA concurrently with the other 2008 amendments; and

WHEREAS, the City Council finds that the Bel-Red CPA satisfies the decision criteria established in Part 20.30(I) of the Land Use Code; and

WHEREAS, the City of Bellevue has complied with the requirements of the State Environmental Policy Act (Chapter 43.21C RCW) and the City Environmental Procedures Code (Chapter 22.02 BCC), including preparation of the Draft Environmental Impact Statement dated January 25, 2007, the Final Environmental Impact Statement dated July 19, 2007, and an Addendum dated February 12, 2009; now, therefore,

THE CITY COUNCIL OF THE CITY OF BELLEVUE, WASHINGTON, DOES HEREBY ORDAIN AS FOLLOWS:

Section 1. Amendments. The Bel-Red/Northup Subarea Plan contained in the City of Bellevue's Comprehensive Plan is hereby renamed the Bel-Red Subarea Plan and amended as set forth in Attachment D1 and by this reference fully incorporated herein.

Section 2. Amendments. The Crossroads and Wilburton/NE 8th Street Subarea Plan maps as contained in the City of Bellevue's Comprehensive Plan are hereby amended as set forth in Attachments D2 and D3 and by this reference fully incorporated herein.

Section 3. Amendments. The Transportation Element of the City of Bellevue's Comprehensive Plan is hereby amended as follows:

POLICY TR-34.1. Recognize the transportation and recreation uses under consideration for the BNSF rail corridor when considering public and private improvements adjacent to and across the corridor and preserve the opportunity for future multi-modal transportation use and access.

Section 4. Amendments. Transportation Element Figure TR.1 of the Comprehensive Plan-Mobility Management Areas, is hereby amended as set forth in Attachment D4.

Section 5. Amendments. Transportation Element Table TR.1 of the Comprehensive Plan-Area Mobility Targets, is hereby amended as set forth in Attachment D5.

Section 6. Amendments. The Glossary of the City of Bellevue's Comprehensive Plan is hereby amended as follows:

Bel-Red – A series of land use designations applicable to the Bel-Red Subarea that provide for the location of office, residential, commercial, and existing uses. The Bel-Red designations include:

Bel-Red Commercial/Residential (BR-CR)

A land use designation in the Bel-Red Subarea that provides for a mix of housing, retail, office and service uses, with an emphasis on retail and service uses. Multiple uses are encouraged on individual sites, in individual buildings, and in the district as a whole.

Bel-Red General Commercial (BR-GC)

A land use designation in the Bel-Red Subarea that provides for a wide variety of business activities that provide goods and services to other businesses and the general public.

Bel-Red Medical Office (BR-MO)

A land use designation in the Bel-Red Subarea that provides for office uses, with an emphasis on medical office.

Bel-Red Medical Office Node (BR-MO-1)

A land use designation in the Bel-Red Subarea that provides for medium intensity office uses within the core of a nodal area, with an emphasis on medical office. The district is limited in extent in order to provide the level of intensity appropriate for areas in close proximity to the highest levels of transit service within the Bel-Red area.

Bel-Red Office/Residential (BR-OR)

A land use designation in the Bel-Red Subarea that provides for a mix of office, housing and retail uses, with office as the predominant use.

Bel-Red Office/Residential Node 1 (BR-OR-1)

A land use designation in the Bel-Red Subarea that provides for a mix of office, housing and retail uses within the core of a nodal area, with office as the predominant use. The district is limited in extent in order to provide the

level of intensity appropriate for areas in close proximity to the highest levels of transit service within the Bel-Red area.

Bel-Red Office/Residential Node 2 (BR-OR-2)

A land use designation in the Bel-Red Subarea that provides for a mix of office, housing and retail uses, with office as the predominant use. The district is located within a node but outside the node's core, and building heights provide for a transition between the node's core and areas outside the node.

Bel-Red Office/Residential Transition (BR-ORT)

A land use designation in the Bel-Red Subarea that provides for low-intensity office and uses and low density multifamily residential dwellings, developed in such a manner as to provide a buffer between residential and more intensively developed properties.

Bel-Red Residential (BR-R)

A land use designation in the Bel-Red Subarea that provides for residential uses. Limited retail and service uses are permitted secondary to residential use, in order to provide the amenity of shopping and services within easy walking distance of residential structures.

Bel-Red Residential/Commercial Node 1 (BR-RC-1)

A land use designation in the Bel-Red Subarea that provides for a mix of housing, retail and services in this nodal area, with an emphasis on housing. The district is limited in extent in order to provide the level of intensity appropriate for areas in close proximity to the highest levels of transit service within the Bel-Red area.

Bel-Red Residential/Commercial Node 2 (BR-RC-2)

A land use designation in the Bel-Red Subarea that provides for a mix of housing, retail and services. The district is located within a node but outside the node's core, and building heights provide for a transition between the node's core and areas outside the node.

Bel-Red Residential/Commercial Node 3 (BR-RC-3)

A land use designation in the Bel-Red Subarea that provides for a mix of housing, retail, and services, with an emphasis on housing. The district is located within a node but in close proximity to mature, stable neighborhoods, and is thus appropriate for transitional heights.

Section 7. Amendments. The Glossary of the City of Bellevue's Comprehensive Plan is hereby amended as follows:

Office- A land use designation that provides for the location of business, financial, administrative and professional uses.

Discussion: A low intensity office is an office building or office portion of a building with a floor area ratio (FAR) of 0.5 or less and where the following sliding scale is used: At 0.5 FAR, no office building or portion of a building may exceed 50,000 square feet of gross floor area; at 0.3 FAR, 100,000 square feet; at 0.1 FAR, 150,000 square feet.

To provide significant permanent open space and to allow a shift of development potential, office intensity of no more than 0.5 is allowed on properties designated OLB-OS. In this district, the sliding FAR scale does not apply.

A medium intensity office is a building of at least 0.5 FAR but not exceeding 3.0 FAR. A high intensity office is a building of at least 3.0 FAR.

Medium and high intensity offices are only located in the Downtown with the following exceptions: The 1993 Factoria annexation agreement created special circumstances for development in Factoria from 1993-2000 to permit office buildings of higher intensities; and office limit for FAR for the area north of Factoria Mall can be increased up to 0.75 consistent with Ordinance 5799 and Policy S-FA-30.1. Medium intensity office is also permitted in the Bel-Red Subarea, where extensive area-wide land use and transportation planning has shown how medium intensity office uses can be accommodated, while helping to create the transit-supportive form envisioned by the Bel-Red Subarea Plan.

Section 8. Effective Date. This ordinance shall take effect and be in force five days after its passage and legal publication. This ordinance, the Bel-Red Subarea Plan, the Transportation Element, the Glossary, and the city's Comprehensive Plan shall be available for public inspection in the office of the City Clerk.

Passed by the City Council this ____ day of _____, 2009, and signed in authentication of its passage this ____ day of _____, 2009.

(SEAL)

Grant S. Degginger, Mayor

Approved as to form:
Lori M. Riordan, City Attorney

Attest:

Myrna L. Basich, City Clerk

Published: _____

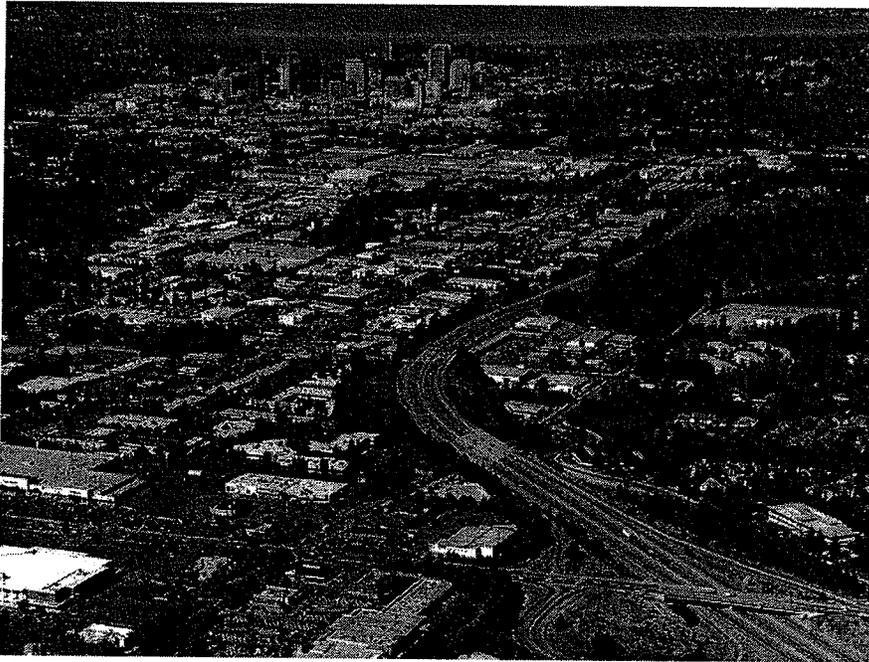
Bel-Red Subarea Plan

GOAL:

To develop a sustainable urban development pattern that dramatically reshapes the future of the Bel-Red Subarea, while allowing the area to transition gracefully from its past.

OVERVIEW

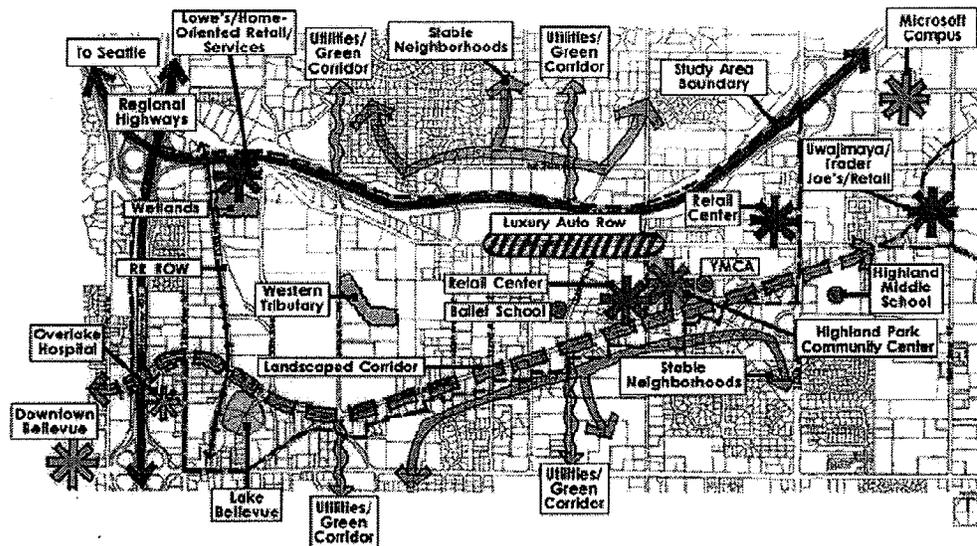
As one of Bellevue's major employment areas, the Bel-Red Subarea historically included a large share of the City's land zoned for light industrial and commercial uses. Particularly on the west end, land uses include sprawling, large lot warehouses and distribution buildings, with acres of surface parking. The transportation network is sparse and discontinuous, with little in the way of a street grid. Six streams run through the area, all heavily impacted by past development, but remarkably still provide some functioning fish habitat. The area has one major city recreational facility (Highland Park and Community Center), but no substantial neighborhood parks or trails.



View of Bel-Red Subarea looking west

In recent years, Bel-Red's light industrial past has seen significant declines, and the area has been in transition. Several large employers, such as Safeway distribution and the King County Journal, moved out or greatly reduced their operations in the area. Between 1995 and 2004, employment dropped by more than five percent in the Bel-Red area, while increasing by twenty percent in Bellevue as a whole.

Meanwhile, areas on both the east and west sides of Bel-Red have transformed dramatically, and other changes are on the horizon. Downtown Bellevue to the west has developed into a dynamic high-rise employment and residential center, connected across I-405 to a major, multiple institution medical district. To the east, Redmond's Overlake area has become a prominent high technology employment center. The introduction of light rail, under consideration by Sound Transit as a way to connect Downtown Bellevue to Overlake by way of Bel-Red, is another potential change. Light rail and other forms of high capacity transit provide a key opportunity to re-think the land use and transportation relationship in the Bel-Red area, with this new transportation investment as a potential catalyst for redevelopment that takes on a much more pedestrian-friendly and transit-supportive character.



Assessment of existing assets

With these considerations in mind, the City undertook a major planning effort to update the vision and plan for the future of Bel-Red. Guided by a citizens steering committee and with extensive public involvement, this new Bel-Red Subarea Plan is the result. The overall thrust of this Plan is to guide Bel-Red on a new path, toward a model of compact, mixed use, "smart growth," a path to set Bel-Red on an economically and environmentally sustainable course for the future, and to take on a new role within the city and the Puget Sound Region.



Bel-Red Steering Committee process

This Subarea Plan informs a range of actions, by both public and private entities. It establishes a vision and overall policy framework to convey the City's intention for the future of Bel-Red, creating a sense of the area's potential and helping to inspire decisions by many actors. The Plan will be implemented through a combination of development regulations and incentives, capital investments, and other public and private strategies.

Bel-Red Subarea Vision

The Bel-Red corridor in 2030 will be an area that is unique within the city of Bellevue and the entire Puget Sound region. It will be an area where thriving businesses will be adjacent to, and sometimes mixed with, livable neighborhoods, all served by a multi-modal transportation system that connects the area to the greater city and region. The area will also be distinguished by environmental and community amenities that will serve residents and employees in the area, as well as residents from surrounding neighborhoods and the entire city. The area will transition gracefully over time, with existing businesses being accommodated while new types of development will occur as conditions warrant.

Specifically, the area will be distinguished by the following attributes:

- ***A thriving economy:*** Bel-Red will be home to major employers, types of businesses and employment sectors unique to this part of Bellevue, and services that are important to the local community.
- ***Vibrant, diverse and walkable neighborhoods:*** Bel-Red will contain a broad range of housing types to meet the needs of a diverse population of varied income levels. Neighborhoods will have a pedestrian friendly and walkable character, with convenient access to shopping, jobs, and community amenities, and will also be well connected to the larger city and region.
- ***A comprehensive, connected parks and open space system:*** Bel-Red will have a park system that serves residents, employees, and visitors of the area, and provides recreation and open space benefits for residents from surrounding neighborhoods as well. System components will include trails along stream corridors, urban and natural open space areas, community facilities, community and neighborhood parks, and cultural/arts features.
- ***Environmental improvements:*** Redevelopment of the corridor will provide opportunities for major environmental enhancements, including improving riparian corridors, adding trees and green spaces, and providing a more environmentally sensitive approach to managing storm water and other natural resources.
- ***A multi-modal transportation system:*** Bel-Red's transportation system will take maximum advantage of its proximity to Downtown Bellevue and Overlake by providing convenient access and short travel times within and outside the corridor for drivers, transit riders, vanpools and access vans, bicyclists, and pedestrians, while minimizing spillover traffic impacts on adjoining neighborhoods.
- ***A sense of place:*** Bel-Red, and the neighborhoods within it, will have a character that is different from Downtown Bellevue, Overlake, or other Bellevue neighborhoods. The area will build on its industrial past, and recall its natural, agricultural and ethnic heritage, while incorporating new development types that offer a unique experience for residents

and employees. Public art and a distinctive cultural environment will also add to Bel-Red's character.

- ***A unique cultural environment:*** Bel-Red will offer a culture-rich environment unique to the Eastside with an arts district focusing on arts education and production. Public art and artists living and working in the area will add flavor to Bel-Red's character. Open studios, art walks and artful expression in buildings and infrastructure will contribute to a lively sense of place.
- ***Appropriate scale of development:*** Development and redevelopment in Bel-Red should complement, not compete with, Downtown Bellevue, and should provide graceful transitions in scale in areas adjacent to residential neighborhoods.
- ***Timing of development:*** As the Bel-Red corridor redevelops over time, provision of transportation and other infrastructure and public amenities (such as parks) should occur concurrently with or in advance of development.
- ***Sustainability:*** New neighborhoods, buildings, streetscapes, parks and open space systems, environmental enhancements, and transportation facilities will also be planned, designed and developed using state-of-the-art techniques to enhance the natural and built environment and create a more livable community.

General

By taking advantage of the Bel-Red area's unique location, and the opportunities associated with high capacity transit, this Plan steers Bel-Red onto a new path. Departing from its low intensity, industrial past, Bel-Red's future will become a model of smart growth and sustainability, the type of planning needed in this era of global climate change. It will offer outstanding opportunities for living and working, while occupying a unique economic niche for Bellevue and the Eastside. Clearly this entails a major transformation for parts of the area, but change will happen over time, with some areas changing earlier and other areas remaining very much the same for years to come.

POLICIES

POLICY S-BR-1. Implement the Bel-Red Subarea Plan in a manner that integrates the three dimensions of sustainability: sustainable economy, society, and environment; meeting the needs of the present without compromising the needs of future generations.

Discussion: In essence, the term "sustainability" conveys one critical thought and intention: that the current generation meets its needs and desires in a manner that allows for future generations to have as rich opportunities to meet their own needs and desires. It means making decisions with the long term, and many generations, in mind. It requires a balancing act, considering the environment, the economy, and the social system, to ensure that all three are taken into account in critical decisions.

POLICY S-BR-2. Promote a differentiated economic niche for Bel-Red, retaining many existing businesses while attracting new businesses in a form not found elsewhere in

Bellevue. Take advantage of opportunities afforded by Bel-Red's strategic location between Downtown Bellevue and Redmond's Overlake employment center, as well as the opportunities brought about by light rail and high capacity transit coming through the area.

POLICY S-BR-3. Reduce and mitigate the environmental and transportation spillover impacts of new development, and work to continually enhance environmental conditions in the area, through a combination of development regulations and incentives, public investments, and other public and private strategies.

POLICY S-BR-4. Develop and implement a phased approach to new commercial development, so that transportation, open space, and other infrastructure is in place or committed to serve the needs of growth. This may include establishment of a year 2030 commercial development limitation consistent with the terms of an interlocal agreement with the city of Redmond, for the purpose of coordinating land use and mitigation between the two cities.

Land Use

GOAL:

To develop a land use pattern that is environmentally sustainable and economically vibrant, and that creates distinctive new commercial and residential neighborhoods for the Eastside.

The Subarea Plan provides an opportunity to re-cast land use in a more sustainable, pedestrian-friendly and transit-oriented form, while accommodating existing uses and minimizing impacts on nearby neighborhoods. The intent of Subarea Plan is to be a model of responsible growth, and to create a land use type and form that is distinctive for Bellevue and the Eastside. The Bel-Red Land Use Plan map is shown as Figure S-BR.1.

A major theme of the Subarea Plan is the "nodal" development pattern, which concentrates future development in the vicinity of potential future light rail stations.

Nodes are envisioned to be areas of sufficient development intensity, amenities, recreation opportunities, and mix of uses that support a high level of pedestrian activity. The decision to focus new employment-generating and higher density residential uses in the nodes is intended to link development areas to locations where planned transportation facilities will support development, and to protect residential neighborhoods located to the north, south and east of the corridor from cut-through traffic.

Land uses within nodes can reach higher development intensity levels and heights than in the surrounding parts of the Subarea. Buildings that are larger and higher than what the base zoning would allow, both inside and outside of nodes, can only be achieved through participation in an incentive system that will provide a range of public amenities.



Existing light industrial use

The Subarea Plan recognizes the economic value and the benefits to the community of the many existing light industrial and service uses in the area. The planned land use designations retain areas for many of these commercial uses to continue. In areas where the land use designation has changed to support nodal, mixed use or residential development, the plan seeks an appropriate transition. Existing light industrial uses and services uses with an industrial character, that were conforming under the previous zoning, are accommodated without making them non-conforming, although new light industrial uses will not be allowed. Other services uses that are compatible with planned new uses are encouraged to continue, and new commercial uses are allowed.

POLICIES

POLICY S-BR-5. Develop land uses consistent with the Bel-Red Land Use Plan map (Figure S-BR.1).

POLICY S-BR-6. Concentrate the majority of future Bel-Red growth into a series of mixed use, pedestrian-friendly and transit-oriented development nodes, with higher density and height therein, as enabled through a land use incentive system. Within each node, provide for tiered building heights, with maximums at the center.

POLICY S-BR-7. Implement a land use incentive system that makes available additional floor area ratio (FAR) and height in exchange for infrastructure and amenities that contribute to the public good.

Discussion: As in the Downtown Subarea, a land use incentive system is a cornerstone of implementing this Plan. Base as-of-right FARs and heights are established at levels significantly below the maximums, with higher levels achieved only by providing public benefits through features such as public open space, trails, environmental enhancements, affordable housing, and other public amenities.

POLICY S-BR-8. Encourage mixed use development, promoting opportunities to live, work, shop, and recreate within close proximity.

POLICY S-BR-9. Accommodate existing light industrial uses that were legally established as of the date of this Plan's adoption, by allowing for their continued operation, expansion including site expansion outside of nodes, and re-building if destroyed. Preclude new light industrial uses from being established, and discontinued light industrial uses from being re-established, with the exception that light industrial uses of limited size (less than 20,000 square feet) are appropriate outside transit nodes and stand-alone residential areas.

Discussion: This policy is to be implemented through the City's land use regulations. Whereas light industrial uses were prevalent in Bel-Red's past, the vision for the Subarea's mixed use, commercial and residential neighborhoods portends an evolution toward a different future. While it is appropriate to support existing light industrial uses, this policy precludes new ones from being established, in order to support a gradual transition to the desired land use and development pattern.

POLICY S-BR-10. Accommodate the continued operation of existing, and allow new, service uses that are compatible with planned future land uses. Accommodate existing service uses that are less compatible with residential and higher intensity, mixed use development (i.e., those that create noise, odor, fumes, aesthetic or other impacts), but preclude the new establishment of these types of service uses in transit nodes and in stand-alone residential areas.

Discussion: This policy is to be implemented through the City's land use regulations. The services sector is quite broad, and includes uses such as health care, business and professional office, household repair, and auto repair. Many of these service uses have characteristics of general retail, are compatible with mixed use commercial and residential, and are encouraged in Bel-Red's future. A smaller subset of service uses, such as auto repair, auto dealers and boat dealers (particularly their service/repair components) and towing, display characteristics similar to light industrial uses. These types of uses are less compatible with transit nodes and stand-alone residential areas, and thus new uses of this type are precluded in these areas.

POLICY S-BR-11. Encourage commercial and residential building siting and design to incorporate stream corridors as a significant on-site amenity, while helping to restore and enhance the ecological functions of these corridors, through the use of development regulations and incentives.

POLICY S-BR-12. Develop and implement landscaping standards that promote sustainable design, and encourage natural drainage practices where both appropriate and feasible.

POLICY S-BR-13. Provide graceful edges and transitions between new land uses and established light industrial areas, and between new development and neighborhoods in adjacent subareas.

Discussion: The transition between new mixed use and residential developments and existing Bel-Red light industrial uses is a special challenge. The siting and design of new buildings should be accomplished in a manner that minimizes conflicts with

existing industrial uses, for the sake of creating a desirable working and living environment for new tenants. Redevelopment of Bel-Red also creates the challenge to ensure that new development is a good neighbor with existing neighborhoods near Bel-Red. Development regulations and design guidelines should address both of these challenges.

Key cross references, Land Use:

- *General Policy S-BR-4, regarding land use phasing.*

Urban Design

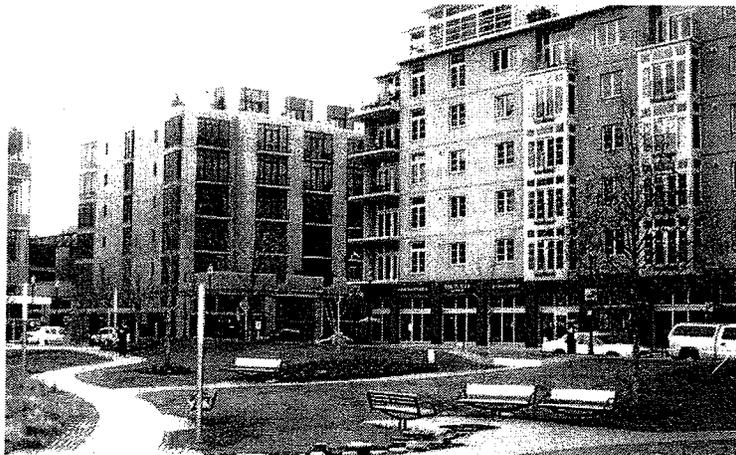
GOAL:

To achieve a design character that results in aesthetically beautiful, distinctive, and long-lasting places that evoke a strong sense of Bellevue and the Northwest, and a dynamic public realm that encourages social interaction.

Past development of the Bel-Red Subarea did not allow for strong attention to the field of urban design. Development was loosely guided and rather discontinuous, with little attention to architectural form, connectivity, and the public realm. By contrast, transformation of Bel-Red into the dynamic mixed use place envisioned by this Plan will require a great deal of attention to high quality and sensitive urban design.

For new mixed use neighborhoods to successfully attract residents, employees, and visitors, the design of the built environment will need to be pedestrian-friendly, integrated with transit and mobility options, and include deliberate “place-making.” Building and site design and materials will need to reflect the vision for the Subarea and provide quality and permanence.

The urban design process is also an opportunity to integrate environmental components with new development, where trees, landscaping and open space not only improve aesthetics, but also work to enhance the function of the natural environment, such as by helping manage stormwater flows.



Development of mixed-use neighborhoods adjacent to green space

POLICIES

POLICY S-BR-14. Use design guidelines to promote pedestrian-friendly and transit-oriented design, ensure quality and a sense of permanence, promote environmental sustainability, and create a distinct sense of place. Conduct design review for all mixed use, office and residentially designated areas of the Subarea. Apply additional depth and attention to the details of design review within transit-oriented development nodes.

Discussion: Design review should pay special attention to creating a pedestrian-friendly environment, by helping to create vibrant, interesting, safe, walkable and interconnected sites.

POLICY S-BR-15. Integrate transit in the design of public and private developments, so that the form and connectivity of the built environment support travel choices.

Discussion: Features such as transit stops near major buildings, building entrances oriented toward transit stops, and direct pedestrian connections between buildings and transit help to integrate transit and land use, improving the pedestrian environment and supporting travel choices.

POLICY S-BR-16. Encourage place-making and a dynamic public realm by integrating publicly accessible plazas, open spaces and other gathering places with development, in public and private projects.

POLICY S-BR-17. Promote environmentally sensitive design in public and private projects, including practices such as the US Green Building Council LEED certification of buildings, use of natural drainage systems, water conservation measures, and other practices.

POLICY S-BR-18. Encourage diversity in the built environment through a variety of building heights and forms, building articulation and modulation. Encourage building rooflines and floorplates that break down the scale of buildings, help to differentiate Bel-Red from Downtown, and enhance the architectural variety of the area.

POLICY S-BR-19. Encourage the use of building materials that are of high quality and durability, are appropriate for the area climate, and that have a sense of permanence.

POLICY S-BR-20. Reinforce the area's sense of place and Northwest provenance by encouraging innovative building and site design, construction techniques and materials that reflect the industrial roots of the area while emphasizing the emerging urban character of Bel-Red.

POLICY S-BR-21. Protect solar access to public spaces and important views from public spaces, as defined through the land use regulatory framework.

POLICY S-BR-22. Promote parking design and management that supports local uses in a manner compatible with the area's urban design, transit and pedestrian orientation, including:

- a. Encourage shared parking;

- b. Encourage structured parking as opposed to surface parking, particularly in identified development nodes;
- c. Prohibit surface parking between buildings and sidewalks where appropriate, and provide visual screening and/or landscaping relief of surface parking where it occurs; and
- d. Allow reduction of parking supply in transit development nodes.

POLICY S-BR-23. Reinforce neighborhood character and identity through the use of gateways and neighborhood signage. Prohibit signage that is out of scale with or detracts from the public realm.

POLICY S-BR-24. Encourage private and public use of public art to enrich design aesthetics and add character, identity and a sense of place.

POLICY S-BR-25. Design and develop an outstanding street environment that promotes streets as key urban places, sensitive to their context and providing an interesting and aesthetically rich experience. Apply a street hierarchy with design guidelines and street standards that provides an appropriate combination of the following elements:

- a. Strong consideration of character and aesthetics in the design and implementation of all street projects;
- b. Integration of open space and landscaping, including street trees;
- c. Environmentally sensitive practices, including natural drainage systems where appropriate;
- d. Sidewalk development standards that promote pedestrian functionality and interest, and avoid obstructions;
- e. Ground floor differentiation, including preferred uses, visual and physical access;
- f. Mid-block pedestrian crossings; and
- g. On-street parking, where it contributes to pedestrian convenience and safety.

Environment

GOAL:

To redevelop the Bel-Red area as a model of environmental sustainability, realizing opportunities provided by new development to achieve significant improvements over current conditions.

Environmental sustainability is one of the overarching principles and hallmarks of this Plan. As noted in the General Policies section above, this suggests a long-term orientation, and an intention to meet current needs in ways that leave a rich legacy for future generations. This goal can be expressed vividly and consistently by integrating stream and wetland rehabilitation, innovative stormwater management techniques, public art, trails, and other social amenities into redevelopment. Natural areas rehabilitation, energy and resource conservation, and diverse transportation choices are fundamental in the redevelopment vision

for this area. This will provide multiple benefits, from improvements in natural functions and habitat to healthier and more beautiful places for Bellevue residents and businesses. Special attention in this section is placed on improving stream corridors, integrating environmental and social elements, and implementing emerging technologies that improve stormwater management.

Stream Corridor Restoration

With a predominance of industrial and commercial uses in the Subarea, natural areas in Bel-Red are not obvious to the casual observer. Nonetheless, a number of streams and natural areas exist, providing the opportunity to restore some natural systems and substantially enhance the area's character. Surface water resources in the Subarea include seven drainage basins that encompass six streams, a pond, and two wetlands functioning as regional stormwater treatment facilities.

In general, these basins exhibit the typical characteristics of urbanized watersheds. Their streams suffered severe impacts during the development that occurred in the area in the 1960s and 1970s, prior to the advent of current environmental regulations. The types of impacts and stream constraints include fish passage barriers, impervious surfaces, buildings adjacent to streams, and piped stream segments. However, most of the streams within the Bel-Red Subarea do support fish, including salmon, at locations within and/or downstream of the Subarea.



Assessment of streams

This Plan embraces the concept of removing fish passage barriers, daylighting piped stream segments, and restoring riparian habitat to transform these streams into community and environmental amenities. Restoring a width of 50 to 100 feet of natural streamside vegetation and incorporating a public trail network will enhance fish and wildlife habitat, improve water

quality, increase open space and neighborhood buffering, linking local and regional trails, encourage social gathering, and other lasting benefits. Beautiful and vibrant stream corridors will encourage the community and new development to refocus on the streams as integral natural treasures in an urban environment. Integrating public parks and stream corridors with public spaces in private development will provide seamless connection of natural resources, education, art, neighborhoods, commerce, and aesthetics. Based on parcel size and initial redevelopment interests, the West Tributary and Goff Creek corridors are anticipated to be early opportunities for enhancement, though rehabilitation opportunities have been identified for other streams as well. Achieving this vision will require wider riparian corridors than currently exist, plus acquiring and developing “opportunity areas” where elements of the parks and transportation systems interface with the streams.

Natural Drainage Practices

Private-sector redevelopment, transportation projects and new parks all present opportunities for incorporating “green infrastructure.” In particular, natural drainage practices, sometimes referred to as low-impact development, manage rainwater runoff in a manner that allows it to infiltrate into the soil on site, or even to be used on site, rather than running off site into nearby streams. As redevelopment occurs, innovative techniques that improve stormwater management will be encouraged.

Natural drainage practices include features such as rain gardens, pervious pavement, vegetated roofs, amended soils and rain recycling. In many cases they are more cost effective, environmentally sound, and aesthetically pleasing than traditional stormwater treatment and storage measures. Although specific natural drainage practices may not be feasible in all settings, such as sites constrained by poorly permeable soils, opportunities for improving stormwater management can typically be identified. These practices do not remove the need for conventional detention to handle large storm events, but they may reduce the needed size of these facilities. A variety and increasing number of stormwater management options are available. A customized approach may be needed for individual sites to attain the environmental and visual amenities identified in the vision for the Bel-Red area.

POLICIES

POLICY S-BR-26. Promote the rehabilitation of streams and their adjacent riparian corridors, through a combination of public investments and private development incentives, as a means to enhance the natural environment and provide multiple public benefits. Provide land use incentives to achieve stream protection and rehabilitation that goes beyond what can be achieved through application of the city’s critical area regulations, including building setbacks of up to 100 feet in specific locations, to provide enhanced stream buffers.

Discussion: Bel-Red’s six streams are a major part of the area’s natural legacy. Rehabilitation of the streams will bear multiple benefits for the environment, for Bel-Red properties where nearby streams can become a significant area amenity, and for the wider Bellevue community. These benefits include improved water quality, habitat enhancement, stormwater and flood control, human use buffering, aesthetic enhancements, noise attenuation, and potential trail access where easements are available.

POLICY S-BR-27. Protect and enhance wetlands and other designated critical areas in Bel-Red, through the use of development regulations, incentives, and possibly public funds.

Discussion: Special attention is needed if Bel-Red's critical areas are to be protected and restored, given that much Bel-Red development took place before standards were adopted to identify and protect these sites.

POLICY S-BR-28. Encourage natural drainage practices where feasible in public and private projects, as an alternative to traditional stormwater treatment and control. Allow natural drainage practices to offset traditional treatment and control standards to the extent practicable, and provide other incentives to promote their use if needed.

POLICY S-BR-29. Explore stormwater basin planning as a future step. Such planning might evaluate coordinated stormwater treatment and detention across multiple properties and may provide for broader environmental benefits as compared to a site-by-site approach.

POLICY S-BR-30. Consider opportunities for off-site or coordinated stormwater mitigation, as a means to provide for a more effective approach to detention needs, and potentially achieve broader environmental benefits.

Discussion: In some cases, providing combined offsite stormwater mitigation may help achieve some of the broader objectives of this Plan. For example, several properties could combine stormwater management requirements on the West Tributary by expanding an existing wetland providing fish and wildlife benefits, increasing open space, as well as providing stormwater detention and water quality treatment. Opportunities for off-site mitigation will be determined on a case-by-case basis, determined by overall ecological and public improvements over on-site.

POLICY S-BR-31. Encourage the use of "green infrastructure," the design and development of infrastructure projects in a manner that deliberately achieves multiple environmental and other public benefits.

Discussion: Traditional infrastructure projects often were designed to solve a single functional problem, such as flood control. The concept of "green infrastructure" suggests that many infrastructure projects have the potential to provide multiple community benefits, such as enhancing neighborhood character and aesthetics, reducing urban heat reducing greenhouse gases and providing access and open space. If these multiple benefits are to be realized, they must be considered starting with early design work forward through all phases of project development.

POLICY S-BR-32. Incorporate environmental education and interpretation into public and private projects, where appropriate.

POLICY S-BR-33. Provide for density transfers within the Bel-Red Subarea as a means to help achieve stream corridor and open space objectives.

POLICY S-BR-34. Actively consider the Bel-Red Subarea as a potential receiving site for regional Transfer of Development Rights (TDRs), as a means to achieve conservation of rural resource lands outside the Countywide Urban Growth Boundary.

Discussion: Transfer of Development Rights is a market mechanism for land conservation. Development rights from sensitive sites needing protection (sending sites) are purchased by and transferred to urban sites where the density can be responsibly accommodated (receiving sites). Since private funding for such TDR purchases is among a long list of desired public amenities, regional TDRs should be explored in light of the full array of public mitigations and benefits desired from private development.

Key cross references, Environment:

- *Urban Design Policy S-BR-17, regarding environmentally sensitive site and building design*

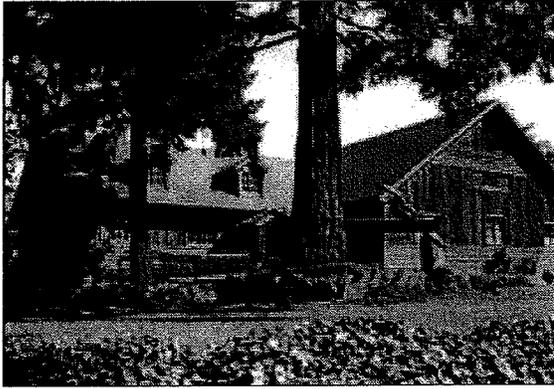
Parks and Open Space

GOAL:

To create a robust, aesthetically beautiful, and functional parks and open space system that serves the needs of Bel-Red and the broader community, and that connects with and complements the larger Bellevue parks and open space system.

Bel-Red's legacy of light industrial and commercial uses have left little in the way of existing park and open spaces, with the notable exception of Highland Park and Community Center. As the area evolves, a comprehensive and connected system of parks, open space and trails will be needed to provide for the recreation and enjoyment of residents, employees, and visitors to new Bel-Red neighborhoods. These new parks will also complement and connect to the citywide parks system, for the benefit of surrounding neighborhoods and the broader community.

The Subarea Plan envisions a park system with an array of elements that cannot be neatly summarized in a suburban level of service standard: community and neighborhood parks, riparian corridor open spaces, open spaces developed as part of natural drainage treatments and designed as public amenities, passive and active plazas and other urban open spaces, and a comprehensive trail system on and off roads, including along stream corridors. Beautiful and functional open spaces will help transform the area, support new residential and commercial uses, improve the environment, and be an asset for the broader Bellevue community.



Existing Highland Park and Community Center

In addition to a range of community-oriented park, open space and trail opportunities, Bel-Red may be a suitable location for a potential major recreation facility that would meet an identified citywide need for both indoor and outdoor recreation. If this citywide facility were to locate in Bel-Red, it could also serve as a community park for the new neighborhoods in this area.

POLICIES

POLICY S-BR-35. Create a robust, aesthetically beautiful and functional parks and open space system that serves the needs of residents, employees, visitors, surrounding neighborhoods, and the entire community. This system should connect with and complement the citywide parks and open space system, and include the following:

- a. Provide neighborhood parks and smaller “pocket” parks with convenient access to all neighborhoods;
- b. Provide a community park serving Bel-Red residents, employees, and citywide residents, comparable in size and utility to the Highland Community Park, in the western portion of the study area. The site might be accomplished through lidding of the Metro transit bus base, and would be able to accommodate indoor and outdoor recreational facilities;
- c. Locate neighborhood and community parks along stream corridors, linked through a series of trails and other open spaces;
- d. Develop a “linear park” series of open spaces spanning the extended NE 16th Street multi-modal corridor through the middle of the Subarea; and
- e. Centrally locate a large civic plaza (public square) within the pedestrian-oriented 130th Avenue NE development node.

POLICY S-BR-36. Consider Bel-Red a suitable area to locate a potential major recreational facility to meet identified citywide needs for both indoor and outdoor recreation and sports facilities.

POLICY S-BR-37. Integrate natural resources management with Bel-Red park system development and operation:

- a. Incorporate natural drainage practices into park development to provide community amenities and watershed benefits, where appropriate and feasible;
- b. Integrate the park and open space system with the riparian corridors to achieve multiple benefits, including enhanced fish and wildlife habitat, improved stormwater management, trail connections, passive recreation, and environmental education; and
- c. Manage upland park areas to protect habitat and restore natural functions.

POLICY S-BR-38. Provide an interconnected system of non-motorized trails for mobility within the study area, connected to the larger, regional trail system, including Marymoor Park and Bridle Trails State Park. The system will emphasize recreational use and provide transportation benefits as well:

- a. Provide non-motorized trails along the stream corridors to connect parks and other land uses, and links to the trail systems along the BNSF Corridor and NE 16th Street. Initial emphasis will focus on the West Tributary and Goff Creek stream systems;
- b. Provide a major east-west multi-purpose trail along the NE 16th Street corridor. The corridor will include a generous right-of-way width to accommodate significant green infrastructure, and to create a high-quality pedestrian and bicycle environment and auto-free access between neighborhoods. It will connect to other local and regional trail systems and to light-rail stations, and provide opportunities to incorporate gateway features; and
- c. Support the development of a regional north-south multi-use trail along the BNSF Corridor that could coexist with potential long-term future rail use, linking to the NE 16th Street regional trail and other local trail access points.

POLICY S-BR-39. Promote the development of “green streets” throughout the corridor, with an abundance of street trees and areas of landscaping to improve and reduce the amount of stormwater runoff, be aesthetically pleasing, and provide an attractive pedestrian experience.

Key cross references, Parks and Open Space

- *Arts and Culture Policy S-BR-45, regarding the creation of a cultural arts district*

Housing

GOAL:

To encourage Bel-Red redevelopment to result in a diversity of housing types and prices, including a significant share of “workforce housing.”

Housing policy has many linkages to a truly sustainable future, with economic, social and environmental dimensions. Having a close-in supply of housing of types and prices that match available jobs can contribute both to Bel-Red’s economic competitiveness and its potential to meet the needs of families who want to live on the Eastside. Establishing housing near Bel-Red’s jobs and services will also reduce trip lengths and allow for a higher share of

trips by transit, bicycling and walking, with environmental benefits in energy conservation and air quality. This Plan's intent to create the potential for 5,000 new housing units in an area that today has almost no housing is a remarkable opportunity to make progress in all three of these dimensions.

The above housing outcomes entail very significant challenges. New urban residential neighborhoods will require an array of public and private investments in open space and other amenities needed to support high quality, livable places. Providing for affordable and workforce housing may be an even bigger challenge.

Housing affordability is a citywide issue, and should be considered in a comprehensive strategy of which Bel-Red is a part. This Plan creates the potential for thousands of new housing units, and provides opportunities not found in other parts of the city to create entirely new residential and mixed use neighborhoods in close proximity to jobs, services and transit. It is critical to consider an approach to housing affordability up-front, at the time this new development potential is being created, or the opportunity for a coherent strategy may be lost. This Plan establishes a multi-faceted strategy to address housing creation and housing affordability through a mix of land use designations, development regulations and incentives, direct public investments, and other public and private approaches.

POLICIES

POLICY S-BR-40. Encourage a diversity of housing types, from high density, multi-story housing in transit nodes, to medium density housing outside nodes, to other innovative housing forms, such as live/work and work/live units.

POLICY S-BR-41. Promote owner and rental affordability in Bel-Red's new housing stock, with a policy target that 50% of new units be affordable for households earning 120% of median income or less. This overall target should include 10 to 20% of new units affordable to low income households (those earning up to 50% of area median income), another 10 to 20% of new units affordable to moderate income households (those earning up to 80% area median income), and another 10 to 20% affordable as an additional segment of "workforce housing" (for households earning up to 120% of median income). These targets will be addressed through a combination of development regulations and incentives, public investments, and other public and private strategies, such as employer-assisted housing and short-term property tax exemptions for multi-family housing.

Discussion: The policy targets are not expected to be met by each Bel-Red housing development. Rather, they are intended to be met over time, across the subarea as a whole, through a combination of public and private strategies. The overall supply of affordable housing will be monitored and if policy targets are not being achieved, affordable housing tools and incentives should be adjusted, or new more effective strategies adopted.

POLICY S-BR-42. Monitor the supply of affordable housing in Bel-Red and make adjustments to implementation strategies, including development regulations and incentives, as needed to meet the identified targets. A review of housing affordability strategies should occur three to five years after the 2009 adoption of the Subarea Plan.

POLICY S-BR-43. Integrate the strategy for promoting housing affordability in the Bel-Red area with the citywide approach of which Bel-Red is a part.

Arts and Culture

GOAL:

To identify and realize opportunities to integrate arts and cultural works and attractions into Bel-Red.

Arts and culture contribute to healthy individuals, neighborhoods, communities and society. In Bel-Red, arts and culture can play a significant role in enhancing the quality of life, vibrancy, and attractiveness of the area, helping to attract creative professionals to live and work here, and benefiting the broader community. Accordingly, this Plan calls for public art, cultural uses and complementary activities to be broadly integrated throughout the redevelopment of the Subarea.

One specific concept is the creation of a cultural/arts district near 136th Place NE, in the vicinity of the existing Pacific Northwest Ballet School. Such a district could include adaptive reuse of other existing buildings for additional classroom, rehearsal and, smaller performance spaces. This district has the potential to be a very significant neighborhood and community amenity, to be used by individual artists, educational institutions, and nonprofit organizations. Uses might also include the production of ceramics, sculpture, blown glass, and others arts and crafts.

POLICIES

POLICY S-BR-44. Encourage artists, arts and cultural organizations, and arts related uses to locate in the Bel-Red area, capitalizing on proximity to existing arts uses and availability of suitable spaces. Encourage major arts and education institutions to locate in the Bel-Red area.

POLICY S-BR-45. Encourage the development of an arts/cultural district near 136th Place NE, as shown on the Bel-Red Land Use Plan map, Figure S-BR.1.

Discussion: This arts/cultural district would build upon existing arts and cultural uses, and expand out to include additional opportunities for arts education, rehearsal, performance, and production activities. Land use incentives and public investments would be used to help achieve these outcomes.

POLICY S-BR-46. Provide funding for public art in the Bel-Red area.

POLICY S-BR-47. Promote public art, cultural activities and urban design that reflect the Bel-Red area's character, heritage, and ecology, with emphasis in the following areas:

- a. Integration of public art into publicly accessible spaces, both privately and publicly developed;
- b. Inclusion of public art in transportation projects, including transit projects; and

- c. Incorporation of public art in interpretation and celebration of stream corridor improvements and other environmental enhancements

POLICY S-BR-48. Promote flexible live and work space for artists in the Bel-Red area by encouraging the creation of live/work and work/live units, in adaptive reuse of existing buildings or in new development.

POLICY S-BR-49. Encourage arts uses as appropriate transitional uses for existing buildings or areas, as Bel-Red evolves over time.

POLICY S-BR-50. Explore opportunities for traditional and technology based media to create a range of artistic expression that is tied to and complements the area's economic development.

Discussion: The Bel-Red area's proximity to high technology uses may create significant opportunities for innovative artistic expression, tying artist activities to the area's broader economic development.

Transportation

GOAL:

To create a more complete, connected, and well balanced transportation system, while protecting neighborhoods from spillover traffic impacts and while ensuring that transportation investments contribute to the area's sense of place and sustainability.

The Bel-Red area has a sparse and discontinuous transportation system. The few arterials in the western portion of the Subarea were designed to serve low intensity, light industrial uses and therefore lack connected sidewalks or mature streetscapes. Arterials in the eastern portion of the subarea carry commuter traffic and provide local access to retail and service uses. This system provides limited connections for east-west travel, few transit options, and minimal facilities for pedestrians and bicyclists.

To support the Bel-Red land use vision, this Plan includes improvements to the transportation system on all fronts. These will provide better vehicular, transit and non-motorized connections within the Subarea and to other parts of the City and region.

Transportation improvements will occur consistent with this Plan's emphasis on sustainability and on making Bel-Red a highly livable place. Providing travel choices is a key element of sustainability. Efforts to reduce commuter trip demand, encourage ride-sharing, walking and bicycling, and promote transit service will further pursue this end. Designing transportation projects as "green infrastructure;" i.e., to incorporate open space, place-making elements, and environmental design such as natural drainage practices, is another element of this Plan's focus on sustainability.

POLICIES

Overall Transportation Policies

POLICY S-BR-51. Support the Bel-Red Subarea Land Use Plan with a multi-modal transportation system consistent with Figure S-BR.2 (map of system improvements), that provides enhanced, multi-modal travel connections within the Bel-Red Subarea, and to other parts of the City and region.

POLICY S-BR-52. Incorporate natural drainage practices and other opportunities to enhance the natural environment into transportation projects where appropriate, effective and feasible.

Discussion: Transportation projects may include significant opportunities for environmental enhancements, such as improvements in stormwater management or removal of existing fish passage barriers.

POLICY S-BR-53. Identify and preserve necessary rights of way for the transportation projects identified in this Plan by ensuring that proposed site and building development plans are compatible with the planned transportation system. (See Table BR-1, Bel-Red Transportation Project List.)

Roadways

Bel-Red Subarea roadway system policies promote arterial improvements including the major NE 16th Street multi-modal corridor, plus local streets to connect uses and enhance the character of Bel-Red in a new grid system with smaller block sizes. Traffic analysis shows that transportation capacity improvements are needed mostly in the western part of the Subarea, because much of the potential employment and residential growth is planned for this area.

POLICY S-BR-54. Design and develop arterial improvements, including added vehicular capacity, transit facilities, and non-motorized components, to serve travel demand generated by the Bel-Red Land Use Plan in addition to citywide and regional travel demand.

POLICY S-BR-55. Extend and expand NE 16th Street as a multi-modal corridor that includes vehicular, high capacity transit, and non-motorized travel modes to serve east-west trip demand across the Bel-Red area, while incorporating significant urban open spaces, and environmentally sensitive design features.

Discussion: The expansion of NE 16th Street is a lynchpin project for Bel-Red. The extended corridor will be the key east-west arterial connection, tying together much of the new Bel-Red land use. It is also the City's desired location for light rail and high capacity transit, and for major new pedestrian and bicycle access across the Bel-Red area. The vision for the corridor is ambitious, incorporating all these transportation modes, and including a "linear park" series of open spaces spanning the corridor. These improvements will be designed in a manner that protects wetlands and other critical areas, and to the extent feasible, incorporates natural drainage practices and other elements of environmentally sensitive design.

POLICY S-BR-56. Develop local streets to establish a new grid system with smaller block sizes, particularly in development nodes; emphasizing continuity, connectivity and community character. Minimize crossings of streams and wetlands by local streets; use environmentally friendly pedestrian and bicycle crossings where needed to provide local connectivity.

POLICY S-BR-57. Encourage garage and service vehicle access via local and secondary streets and alleys. Limit access points along arterial streets.

POLICY S-BR-58. Work with the Washington State Department of Transportation to improve connections to State Route 520 by completing the interchange at 124th Avenue NE, to allow access to and from the east, and to design the project to minimize impacts to adjacent residential neighborhoods.

POLICY S-BR-59. Design Bel-Red arterials and local streets in a manner that contributes to community character, open space, and environmental enhancements.

POLICY S-BR-60. Include on-street parking where it contributes to the pedestrian environment and other elements of the desired neighborhood character.

POLICY S-BR-61. Incorporate public access where appropriate and environmental restoration components, such as removal of fish passage barriers and the creation of wildlife passages, in the design and development of roadways that intersect stream corridors.

Pedestrian and Bicycle System

Within the Bel-Red Subarea many segments of roadway have no sidewalks. Bicycle accommodation is limited to shared roadway facilities as there are no exclusive bicycle lanes.

Creating a multi-modal transportation system to support a mixed use environment requires excellent pedestrian and bicycle access, including access for wheelchairs, strollers and other wheeled pedestrian devices. The Subarea Plan offers the potential to create transit-rich development nodes and neighborhoods in which walking and bicycling opportunities abound. A wide range of improvements to the non-motorized transportation system is anticipated; from sidewalks and mid-block pedestrian connections, to trails along riparian corridors and the Burlington Northern Santa Fe (BNSF) railroad corridor, and new neighborhoods and roadways that are designed from the start with pedestrians and bicyclists in mind. Proposed bicycle facilities would serve all types of users, from novice to expert, and all types of trips, including recreation and commuting. Links to local parks and connections to the regional system are part of the vision.

As a major future arterial and transit corridor, an extension of NE 16th Street is also planned to be the primary component of the east-west non-motorized transportation system. Providing a high quality pedestrian and bicycling environment along this arterial will create non-motorized linkages between neighborhoods, regional trail systems, and light rail stations with limited or minimal auto conflicts. The east-west pedestrian and bicycle corridor is also intended to link the various north-south trails and sidewalks.

In the policies below, “pedestrian and bicycle” is used to include the range of pedestrian and bicycle-like types of travel, such as wheelchairs, strollers, and mobility scooters.

POLICY S-BR-62. Include pedestrian and bicycle facilities in the design of arterials and local streets.

POLICY S-BR-63. Improve pedestrian connectivity and the quality of the pedestrian environment with a comprehensive sidewalk and trail system, including through-block pedestrian connections, and mid-block crossings. Include pedestrian amenities such as pedestrian-scaled lighting, seating, transit shelters, and weather protection.

POLICY S-BR-64. Develop a multi-use trail system throughout the Subarea that provides both local and regional connections, including major non-motorized facilities along the extended NE 16th Street, along the West Tributary of Kelsey Creek and other stream corridors, and incorporating the planned regional trail along the BNSF railroad corridor.

POLICY S-BR-65. Develop multiple access points to the planned BNSF corridor multi-use trail.

POLICY S-BR-66. Provide grade-separated road crossings of the proposed trail system along the West Tributary of Kelsey Creek, and of other separate trails where feasible.

Transit

Past transit service in the Bel-Red Subarea was quite limited, and generally available only on the area’s perimeter. Regional transit investments, such as King County Metro’s “Transit Now” program will provide rapid ride bus service in the vicinity of the subarea, along NE 8th Street and 156th Avenue NE. Future land use will be served by a greatly enhanced transit system, including both conventional and high capacity transit service, and an integrated land use pattern that promotes travel choices.

The Subarea Plan envisions a light rail route between downtown Bellevue and Overlake, traversing the Bel-Red Subarea on an alignment along approximately NE 15th/16th Street. Such an alignment with stations at planned development nodes maximizes the potential to create transit-oriented, mixed-use neighborhoods, and greatly increases ridership and performance of the light rail system. Light rail stations in the vicinity of Overlake Hospital Medical Center, 122nd Avenue NE, 130th Avenue NE, and at 152nd Avenue NE in Redmond’s Overlake area, would allow for about three-quarters of future Bel-Red residents and employees to have convenient, walkable access to the stations. When light rail is operating, “feeder” bus service can be reallocated to connect other City neighborhoods with the Bel-Red light rail stations. A park and ride in the vicinity of the 130th Avenue station would enhance local access to the system.



Light rail transit

Light rail transit is an exciting opportunity and part of the preferred vision for Bel-Red. The “nodal” development pattern, and mix and intensity of use envisioned will greatly promote travel choices of all types, including rubber-tired bus transit. The City will work with Metro and other conventional transit providers to greatly enhance service to Bel-Red, with a special emphasis on supporting the emerging development nodes identified above. This will be critical both to ensure that transit serves the area in the shorter term (before light rail is developed), and in the longer term, to feed light rail stations.

POLICY S-BR-67. Work with King County Metro and other transit providers to serve emerging new land uses in the Bel-Red Subarea, and to connect to and support future light rail or alternative forms of high capacity transit, including:

- a. Enhance conventional transit service throughout the Bel-Red Subarea;
- b. Provide a level and type of service that, to the extent feasible, emulates high capacity transit in future light rail station areas, as an interim measure in advance of light rail operations; and
- c. Provide transit feeder service from other Bellevue neighborhoods into transit-oriented development nodes at future light rail stations.

POLICY S-BR-68. Work with Sound Transit to realize the City’s preferred light rail route, alignment and station locations, as shown in Figure S-BR.2. Support the development of light rail stations in the vicinity of Overlake Hospital Medical Center, 122nd Avenue NE/NE 16th Street, and 130th Avenue NE/NE 16th Street.

POLICY S-BR-69. Include transit-supportive improvements, such as transit shelters, wayfinding signage, and other features through a combination of public investments and a regulatory framework applicable to private development.

POLICY S-BR-70. Work with Sound Transit to determine the need for a future light rail maintenance facility in Bel-Red, and if needed, to locate it where compatible with planned land uses and transportation facilities and services.

Transportation Demand Management

By closely integrating land use and transportation planning, this Subarea Plan goes a long way toward promoting travel choices. The Plan supports a mix and intensity of land uses that

allows for fewer and shorter trips, and promotes a balanced, multi-modal transportation system that creates an environment that allows for more trips to be convenient for transit and non-motorized options. Education and outreach efforts can help users become aware of and take advantage of these travel options.

POLICY S-BR-71. Implement a transportation demand management program to reduce Bel-Red single-occupancy trip demand, and increase the share of trips utilizing transit, carpools and vanpools, and pedestrian and bicycle options.

POLICY S-BR-72. Support the development of a Transportation Management Association in the Bel-Red Subarea to assist employers in providing commute options for employees.

POLICY S-BR-73. Manage the parking supply and consider establishing maximum parking requirements to encourage the use of transit, car/van pool, and non-motorized commute options.

POLICY S-BR-74. Promote the development and management of parking supply to encourage the use of transit, car/van pool, and non-motorized commute options, recognizing that in areas with compact, transit-oriented design, a surplus of parking and/or poorly designed parking detracts from the pedestrian environment and the ability of the area to maximize travel choices.

Neighborhood Traffic Calming/Mitigation

This Plan supports on-going efforts to minimize neighborhood cut-through traffic and speeding associated with Bel-Red development. Mitigation for potential traffic impacts on arterials must create a roadway character that alerts drivers that they are in a neighborhood and encourages them to drive accordingly. Medians, special paving, and other options to address traffic volume and speed on arterials will be developed in coordination with the affected neighborhoods.

For non-arterial streets, traffic-calming devices and traffic control measures will be considered on a case-by-case basis, applying the City's Neighborhood Traffic Control Program criteria within residential neighborhoods in and around the Bel-Red Corridor.

POLICY S-BR-75. Develop and implement strategies to minimize neighborhood cut-through traffic associated with Bel-Red development. Monitor the outcomes of these efforts and make adjustments as needed to ensure their effectiveness.

Key Cross-References, Transportation:

- *Urban Design Policy BR-22, regarding parking policies to promote pedestrian-friendly and transit-oriented design, particularly in development nodes*
- *Citywide Comprehensive Plan Transportation Element, Chart TR.1, establishing Bel-Red Subarea mode split targets and arterial LOS standard*
- *Citywide Comprehensive Plan Transportation Element, Transportation Demand Management and High Capacity Transit policies*

Interjurisdictional Coordination

GOAL:

To provide for effective coordination and mitigation of impacts that cross jurisdictional boundaries

Many interactions occur between the Bel-Red Subarea and the adjacent Overlake area in the city of Redmond, particularly with regard to travel patterns. The two cities have a long history of working together to address cross-jurisdictional impacts, and the Bel-Red Subarea Plan continues to promote this level of interjurisdictional coordination. The intent is to ensure that any cumulative impacts of the Bel-Red and Overlake plans are identified and mitigated, and also that there is synergy between the plans in terms of light rail and transit implementation, streetscape design, pedestrian and bicycle facilities, and other issues.

In addition to coordination with Redmond, implementing the Bel-Red vision will require ongoing coordination with Sound Transit, King County Metro, and Washington State Department of Transportation (WSDOT), to ensure that transportation facilities and services are implemented over time.

POLICIES

POLICY S-BR-76. Continue to coordinate with the City of Redmond on the planning and implementation of land use and facilities that have cross-jurisdictional impacts, and work together to mitigate such impacts. Coordination may include but is not limited to systematic sharing of information, joint advocacy for regional investments benefiting the two areas, and financial contributions toward facilities that mitigate interjurisdictional impacts.

Discussion: An updated interlocal agreement is an appropriate mechanism for formalizing the terms for coordination between the two cities. Such an agreement should identify appropriate mitigation for the East Bellevue area that considers: 1) the combined effects of land use development in the Bel-Red area of Bellevue and the Overlake area of Redmond; and 2) the strong neighborhood preference to avoid projects that add general north-south capacity through East Bellevue neighborhoods.

POLICY S-BR-I77. Coordinate with state and regional transportation and transit agencies (WSDOT, Sound Transit and King County Metro) on planning and providing transportation projects and services needed to implement this Subarea Plan.

Key cross references, Interjurisdictional Coordination

- *General Policy S-BR-4, regarding growth phasing and growth caps*

Implementation

GOAL:

To ensure an ongoing and effective process, with the right tools available, to result in effective implementation of the Bel-Red subarea vision

The Bel-Red Subarea Plan is an ambitious vision that calls for a substantial departure from the area's past. In addition to sweeping land use changes in portions of the area, the plan calls for very significant investments in transportation, parks, and environmental improvements. Much of these investments will need to come from properties undergoing redevelopment. Some of this will occur as redevelopment brings sites up to newer land use and urban design standards. Some will occur with land use incentives, allowing greater building intensities or height in exchange for development contributions to infrastructure and amenities. A city funding strategy will likely also play a role, by utilizing both existing revenue sources and new financing tools.

The transformation will not happen overnight, but rather will likely unfold over a number of decades. Successful implementation will require a coordinated strategy, with ongoing commitment, a full array of tools, and dedication over time.

POLICIES

POLICY S-BR-78. Implement the Bel-Red Subarea Plan through a combination of development regulations and incentives, capital investments, and other public and private strategies.

Discussion: Some improvements anticipated by this Subarea Plan will be provided through land use development regulations that enable new land uses to occur. Other improvements will be provided through a land use incentive system that allows for larger and taller buildings than can occur under the basic zoning, in exchange for the provision of certain public amenities. Yet other improvements will be included in a public financial strategy, as described below.

POLICY S-BR-79. Establish a financial strategy that provides for the costs of public infrastructure and amenities needed to support Bel-Red development and identified as specific projects in this Plan to be born by various parties based on their relative shares of impacts and benefits. Cause developing properties to contribute the principal share of needed public improvements, to the extent they are the primary beneficiary of such improvements. Implement existing and new financial tools as needed to carry out this strategy.

Discussion: Redevelopment of the Bel-Red Subarea requires a broad array of investments in public infrastructure and amenities. Various classes of properties, including developing properties, existing properties, and the general City should share these costs based upon the relative share of benefits they receive.

POLICY S-BR-80. Reserve the right-of-way needed for the Bel-Red public infrastructure and amenities identified in this Plan as early as practicable.

POLICY S-BR-81. Allow for private development to voluntarily advance the building of public infrastructure improvements, in order to enable land use to proceed in sync with the availability of needed infrastructure; consistent with the phasing approach set forth in Policy S-BR-4. Such investment may be repaid in part by latecomers agreement or similar device.

POLICY S-BR-82. Require the dedication and improvement of local streets at the time of development, consistent with Figure S-BR.2, to enable development of frontage improvements and a local street pattern that provides for access and loading, and improved transportation connectivity.

POLICY S-BR-83. Encourage master planned developments and other processes that better coordinate and integrate this Subarea Plan's objectives rather than individual development of small parcels. Consider mandatory use of master planned development approval for large sites, to facilitate site planning for vehicle access and pedestrian needs.

POLICY S-BR-84. Encourage business and property owners to support the ongoing development of the Bel-Red area through a business improvement organization.

POLICY S-BR-85. Monitor and assess the implementation of the Bel-Red Subarea Plan and make adjustments to assure success of providing affordable housing, parks and open space, a mix of uses, and other aspects of the Bel-Red vision. A review of the implementation of the Bel-Red Subarea Plan should occur approximately five years after the Plan's initial 2009 adoption and periodically thereafter.

Neighborhoods/Districts

GOAL:

To support the development of new Bel-Red neighborhoods that achieves distinctive and high quality character.

Bel-Red is a large Subarea, with the potential for developing a number of distinct neighborhoods. These include the "development nodes" around future light rail stations, areas of mixed use and relatively dense development where public and private improvements are expected to create a pedestrian-friendly and transit-supportive environment. Additional neighborhoods will form outside the development nodes. Some will be fairly consistent with past development; others will be a significant departure. The intent of this Plan is to capitalize on these various land use patterns within Bel-Red, to break down the large expanse of the Subarea and create a series of neighborhoods, each with its own distinct character and sense of place.

Development Nodes

The nodal development pattern concentrates development in the vicinity of potential future light rail stations, generally within a quarter-mile radius. Development nodes are located in the vicinity of Overlake Hospital, at 122nd Avenue NE, at 130th Avenue NE, and at 152nd Avenue NE (with a station in Redmond). These nodes would be mixed-use in nature, with a

high level of pedestrian access and amenities. Land use intensities in these nodes can reach a maximum development intensity of 4.0 FAR, but only through participation in an incentive system that provides public amenities in exchange for higher densities. Maximum building heights vary by development node, with the highest allowed heights near the center of the nodes. As with intensity, these maximum heights above the base zoning height limits are allowed only through participation in the amenity incentive system.

POLICIES

Node in the Vicinity of the Medical Institution District

POLICY S-BR-86. Provide for office uses in this area, with an emphasis on medical office. Initially higher intensities are limited to an area adjacent and to the east of Overlake Hospital, and heights in this area may reach 150 feet. Sound Transit is contemplating an additional light rail station on either the northwest or southeast side of the Overlake Hospital Medical Center and Group Health Ambulatory Care Center campus. When that station location is determined, an area of additional development intensity and height may be designated through a subsequent Comprehensive Plan amendment.

Discussion: This is an area directly adjacent to Overlake Hospital Medical Center and the Group Health Ambulatory Care Center, by far the largest medical complex on the Eastside, and just south of the planned Seattle Children's Hospital facility. In addition to these institutional uses, the area is characterized by a wide range of medical office uses that complement the medical campus.

At the time of the adoption of this Subarea Plan in 2009, multiple locations were being considered for a light rail station to serve the Medical Institution District, including on the northwest side of Overlake Hospital and immediately east of Whole Foods. When a final station location decision is made by Sound Transit, expected in 2010, the City may consider the need for additional land use planning in that area, which may warrant amendments to the Subarea Plan.

Note: See policy S-BR-K6, which also applies to this medical office nodal area.

Node at 122nd Avenue NE

POLICY S-BR-87. Provide for a mix of office, housing and retail uses in this area, with office as the predominant use. Potential height in the center of this node may reach 150 feet, and up to 125 feet in the perimeter.

Discussion: This office-supportive node is located on the west side of the subarea, where it is better served by regional transportation infrastructure and has lesser potential for traffic impacts on East Bellevue neighborhoods. Housing and retail uses should be part of the mix of this neighborhood, to avoid a sterile office-only area and to create a balance of activities. Parks, open spaces, and recreational opportunities are envisioned as part of this urban environment. The Master Development Plan process will be used as a review mechanism for larger developments.

Node at 130th Avenue NE

POLICY S-BR-88. Provide for a mix of housing, retail and services in this area, with an emphasis on housing; include a pedestrian-oriented retail area along 130th Avenue NE. Potential height in the center of this node may reach 150 feet, and up to 125 feet in the perimeter.

Discussion: This area is envisioned as a predominantly residential neighborhood with an active retail street at its core. With its focus on residential uses, this area represents a key opportunity to develop a range of housing types and densities. An urban plaza located near 130th Avenue NE will serve as a "town square."

Node at 156th Avenue NE, Near the Overlake Transit Station Location in Redmond

POLICY S-BR-89. Provide for a mix of housing and retail uses in this area. Potential heights may reach 70 feet in this area, with a limitation of 45 feet along 156th Avenue NE.

Discussion: This mixed use node is on the edge of Redmond's designated Overlake neighborhood, and is within the walkable area of the planned Overlake transit station at 152nd Avenue NE.

Land Uses Outside Nodes

Medical Office and Medical Institution Area Along 116th Avenue NE

POLICY S-BR-90. Provide for office uses in this area, with an emphasis on medical office, and with an FAR up to 1.0.

POLICY S-BR-91. Provide for major medical institution development within the area bounded by NE 8th and NE 16th Streets, and I-405 and the Burlington Northern Railroad right of way.

Discussion: Sites in this area that are proposed to include medical institution uses are appropriate to be rezoned Medical Institution. Special dimensional standards are appropriate to accommodate the needs of major medical institutions and related uses, provided those facilities provide a high quality of design that recognizes this area as a prominent community landmark. Building and site design should create a visual identity for those facilities that contribute to the streetscape, are compatible with adjacent and nearby neighborhoods, and are sensitive to views from the freeway and views of the skyline. Design should also create a visually pleasing and safe environment for the public, incorporating features such as public spaces, gateways, streetscape improvements, and safe pedestrian linkages.

Retail Along the Central Portion of the NE 20th Street Corridor

POLICY S-BR-92. Provide for retail uses in this area, with FARs and heights not to exceed the standards for the General Commercial designation that is applicable city-wide. Encourage little significant land use change in this area, which is not contemplated for more intense development.

Area South of Bel-Red Road

POLICY S-BR-93. Provide for a mix of office and housing in this area, with low-intensity buildings of one or two stories serving as an appropriate buffer between the uses north of Bel-Red Road and the single-family residential neighborhoods to the south. While office should remain a focus, housing in this area is also a suitable transition use.

Mixed Use Retail/Housing Areas (area just west of 148th Avenue, area near Highland Park and Community Center, and the triangular area south of NE 12th Street, east of 120th Avenue NE)

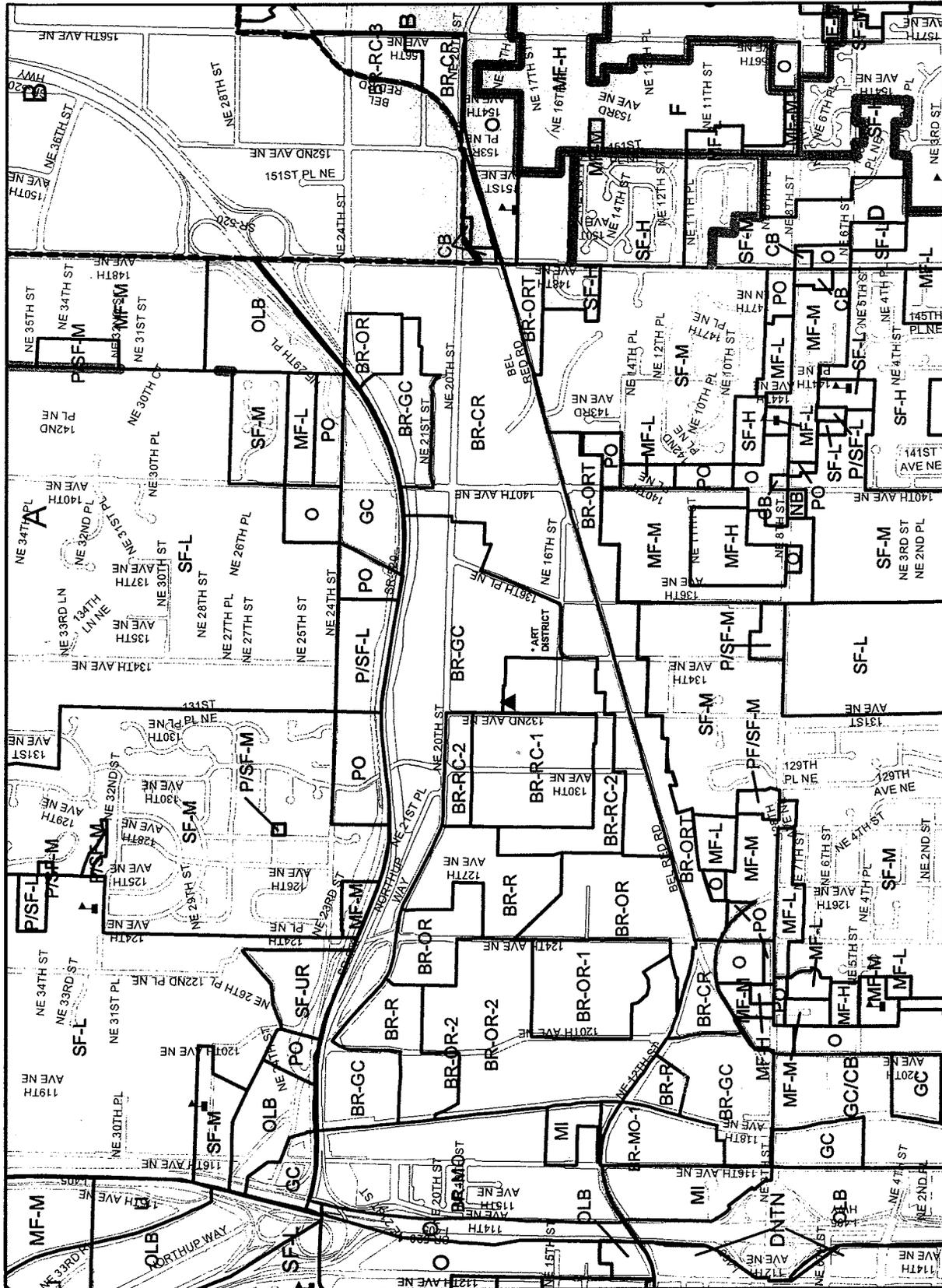
POLICY S-BR-94. Promote additional development of retail uses in these areas, together with mixed use development that incorporates housing. Allow maximum building heights up to 70 feet through the incentive system.

Housing Emphasis Area (area north of the Metro east base along the south end of NE 20th Street, north of the West Tributary, and north of Lake Bellevue)

POLICY S-BR-95. Promote stand-alone housing in these areas, taking advantage of view corridors to the south and west, as well as potential improvements to the West Tributary stream corridor.

Area South of NE 12th Street and Contiguous to Lake Bellevue

POLICY S-BR-96. Provide for retail, commercial, and residential uses in this area. Allow maximum building heights up to 45 feet in this area.



**FIGURE S-BR.1
Bel-Red Land Use Plan**



- SF Single Family
- MF Multi Family
- L Low Density
- M Medium Density
- H High Density
- UR Urban Residential

- PO Professional Office
- O Office
- OLB Office, Limited Business
- OLB-OS Office, Open Space
- NB Neighborhood Business
- CB Community Business

- GC General Commercial
- LI Light Industrial
- PF Public Facility
- P Park

- ▲ Fire Stations
- ⌚ Public Schools
- ▬ Planning Districts
- ▬▬▬ Bellevue City Limits (2008)
- Lakes

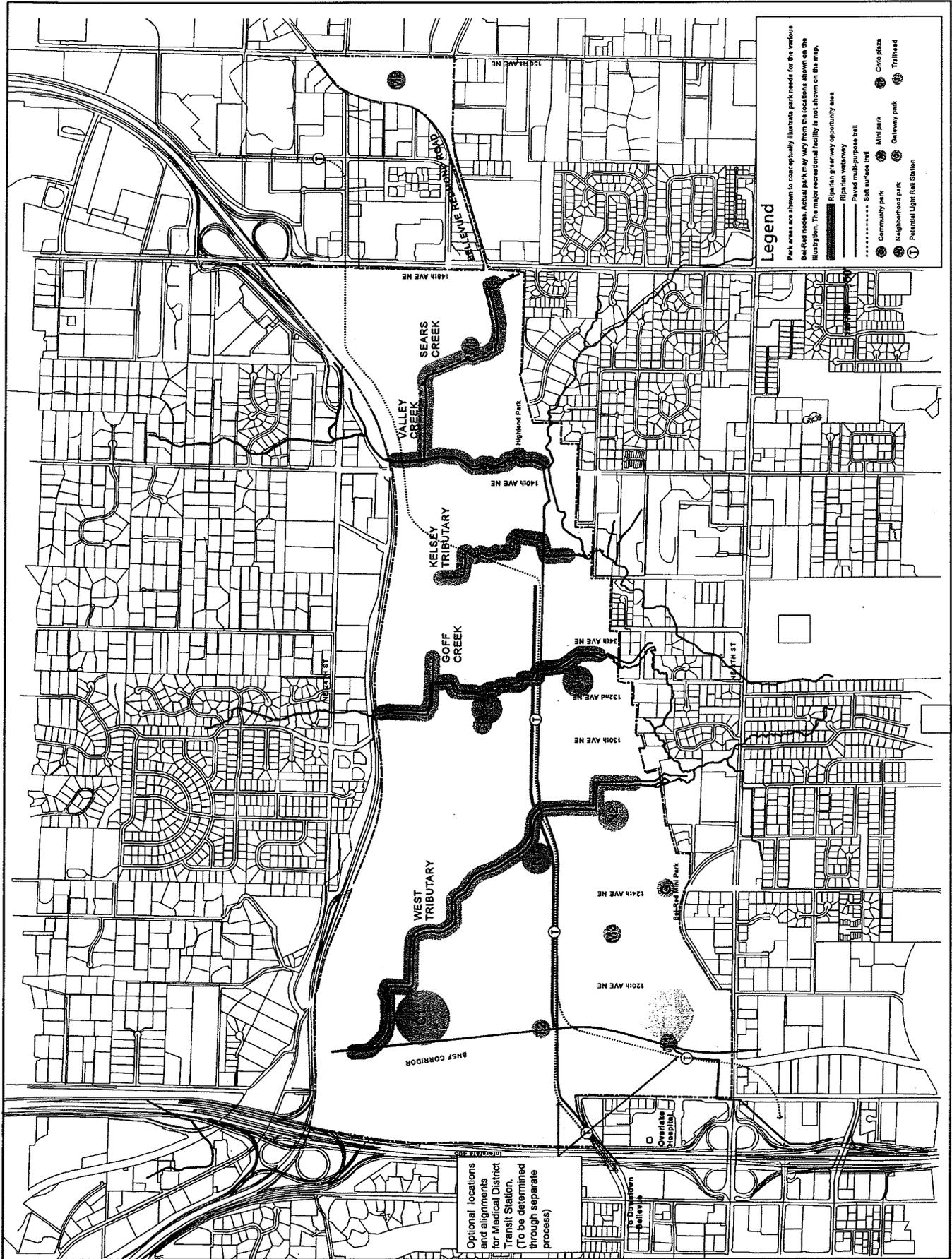


Figure S-BR.3

Bel-Red Parks and Open Space Plan



Table 1 Bel-Red Transportation Project List

Arterial Project	Description of Arterial Improvement
101	Northup Way between 120th Avenue NE and 124th Avenue NE 5-lanes Add eastbound lane Sidewalks both sides Bicycle lanes
102	Northup Way west of 120th Avenue NE 3-lanes Sidewalks both sides Bicycle lanes
103	116th Avenue NE 5-lanes between NE 12th Street and approximately NE 16th Street – across the frontage of the Children’s Hospital site
104	120th Avenue NE 5-lanes Connect directly to NE 8th Street on the south Sidewalks both sides Bike lanes both sides – connection to the State Route 520 Trail on the north
105	124th Avenue NE 5-lanes Connect with SR 520 and proposed ramps to and from the east Connect with Bel-Red Road with an improved intersection Sidewalks both sides
106	130th Avenue NE 2-lanes, pedestrian-oriented street north of NE 16th Street On-street parking north of NE 16th Street 3-lanes south of NE 16th Street
107	NE 15th/16th Corridor from 116th Avenue NE to 136th Place NE Multi-modal corridor incorporating east-west arterial capacity (2 through-lanes in each direction, potentially with an interim improvement limited to a single through-lane in each direction); light rail guideways and stations; urban sidewalks; a bicycle trail/pathway with regional trail connections; and “green” elements, including urban open spaces, tree canopy and landscaping features, and natural drainage features where practical.

Discussion: The new NE 15th/16th Corridor is critically important both to the functioning and character of the Bel-Red Subarea. This project provides new east-west capacity for both vehicles and light rail. It provides a high quality pedestrian and bicycle facility that connects Bel-Red to Downtown, open spaces, north-south trails, and the larger Bellevue parks system. The quality and character of this centrally located corridor will also have a formative impact on the ambiance of the entire Bel-Red Subarea. Thus the design and implementation of this project should carefully incorporate the above elements in a manner that promotes the desired urban design and character of Bel-Red, with special sensitivity to width, human scale, walkability, and pedestrian-friendly design.

Table 1 Bel-Red Transportation Project List

Arterial Project	Description of Arterial Improvement
108	136th Place NE 3-lanes (2 southbound, 1 northbound) Light rail running above grade in the median Sidewalks both sides Bike lanes both sides
109	NE 11th/NE 12th Street 4/5 lanes 116th Avenue NE to NE 12th Street

NE 4th Street and NE 6th Street improvement projects are included in the East Bellevue Transportation Facilities Plan

Table 2 Bel-Red Parks and Open Space Project List

Project	Map ID	Preliminary Descriptions of Park Improvements	Acres
201	N-1	Neighborhood Park – Along West Trib, adjacent to NE 16th Street Parkway. Integrates major trail systems and regional detention, with neighborhood park facilities. Includes 1 acre remnant transportation acquisition.	7
202	N-2	Neighborhood Park – Along West Tributary, adjacent to Bel-Red Road. Integrates major trail and neighborhood park facilities. Includes 3 acres Neighborhood Park / 1 acre riparian.	4
203	N-3/CP	Neighborhood Park/Civic Plaza – Along Goff Creek, north of NE 16th Street between 130th Avenue NE and 132nd Avenue NE. Integrates approximately 1-acre urban plaza along pedestrian oriented retail street with 3-acre neighborhood park and trail facilities along 1-acre riparian corridor.	5
204	N-4	Neighborhood Park – Along Goff Creek, between Bel-Red Road and NE 16th Street. Integrates trail system along riparian corridor with neighborhood park facilities. Includes 3 acres Neighborhood Park / 1 acre riparian	4
205	G	Gateway Park – Near the intersection of 124th Avenue NE and Bel-Red Road. Integrates mini park facilities and potential art component near major road intersection to serve as gateway to the Bel-Red Corridor.	1
206	T-1	Trail Head – Located adjacent to BNSF at Lake Bellevue. Integrates a trailhead with mini park facilities at BNSF regional trail and Lake Bellevue.	1
207	T-2	Trail Head – Located at the BNSF regional trail corridor and the NE 15th/16th Parkway. Integrates a trailhead with mini park facilities at the crossing of the two major multi purpose trails.	1
208	M-1	Mini Park – Along Sears Creek west of 148th Avenue NE, between Bel-Red Road and NE 16th Street. Integrates trial system and riparian corridor with mini park facilities.	1
209	M-2	Mini Park – Centrally located between Bel-Red Road, 156th Avenue NE, and NE 20th Street. Incorporates mini park facilities to serve node.	1
210	M-3	Mini Park – Located between 120th Avenue NE and 124 Avenue NE and south of NE 15th/16th Street. Provides park facilities to serve residential and commercial mixed use development	1
211	C-1	Community Park – Along West Tributary adjacent to 120th Avenue NE. Incorporates trailhead and trail system along West Tributary and BNSF with community park facilities. Includes 10-acre acquisition through purchase for community park facilities and 7-acre open space dedication by adjacent owner .	17
212	N/A	Major Indoor Recreation Facility – 150,000 square feet – Components could include sports, health and fitness facilities and major aquatic center, together with companion outdoor sports facilities. Location not shown on map.	10

Table 3 Bel-Red Environmental and Trail Project List

Project	Description of Environmental and Trail Improvements	Size/ Length
301	West Trib Riparian Corridor & neighborhood park Restore 100-foot width native plant riparian corridor on each side of the West Tributary from the BNSF right of way to Bel-Red Road with a paved ped-bike path (10 feet) on one side of the stream. Improve fish passage, and rehabilitate in-stream habitat.	6350 lf stream 27 acre riparian corridor
302	Goff Creek Riparian Corridor Develop a 50 foot wide native plant and social amenity riparian corridor on each side, integrating natural drainage practices, 8-foot paved/boardwalk trail, art and place-making features as part of redevelopment north of NE 16th Street. Realign and daylight Goff Creek with additional floodplain storm water capacity. Restore fish passage.	4200 lf 1.5 acre
303	Kelsey Creek Trib Riparian Corridor Restore a 50-foot average width native plant and social amenity riparian corridor on each side. Integrate natural drainage practices, soft surface pathway, art and place-making features in corridor. Restore fish passage.	2000 lf 12 acre
304	Valley Creek Riparian Corridor Restore an 100-foot average width native plant riparian corridor on each side between Highland Community Park and State Route 520. Include paved multi-use trail/boardwalk on one side.	500 lf 3 acre
305	Sears Creek Riparian Corridor Restore a 50-foot average width native plant riparian corridor along each side of Sears Creek. Rehabilitate salmon habitat, channel capacity, restore riparian functions and provide soft surface pedestrian pathway. Daylight piped area of stream.	1300 lf 8.5 acre

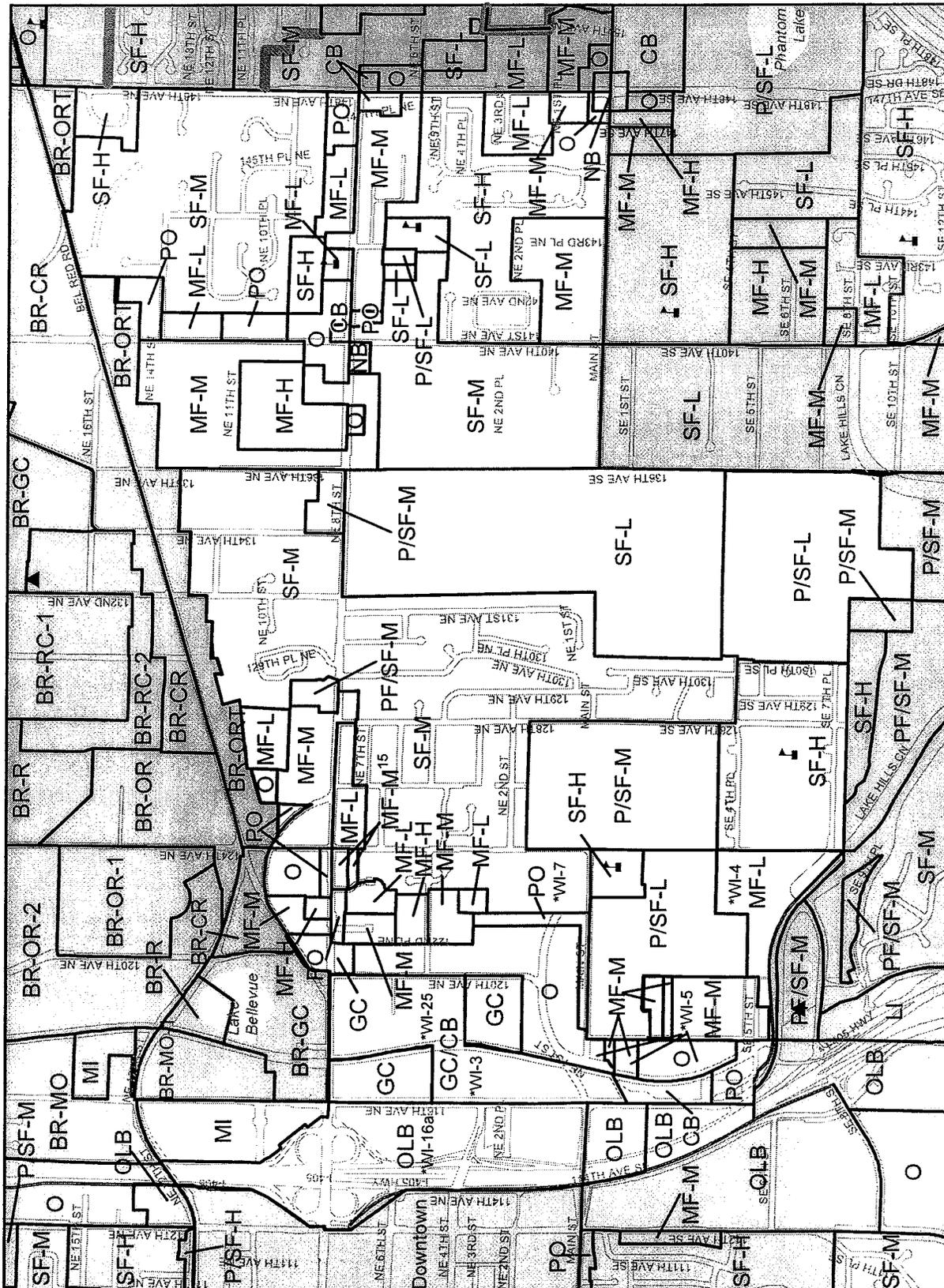


FIGURE S-WI.1
Wilburton/NE8th Land Use Plan

- SF Single Family
- MF Multi Family
- L Low Density
- M Medium Density
- H High Density
- UR Urban Residential

- PO Professional Office
- O Office
- OLB Office, Limited Business
- OLB-OS Office, Open Space
- NB Neighborhood Business
- CB Community Business

- GC General Commercial
- LI Light Industrial
- PF Public Facility
- P Park

- ¹⁵R-15 density limit
- ▲ Fire Stations
- ▤ Public Schools
- ▨ Planning Districts
- ▧ Public Facility
- ▩ Bellevue City Limits (2008)
- Lakes



ATTACHMENT D4

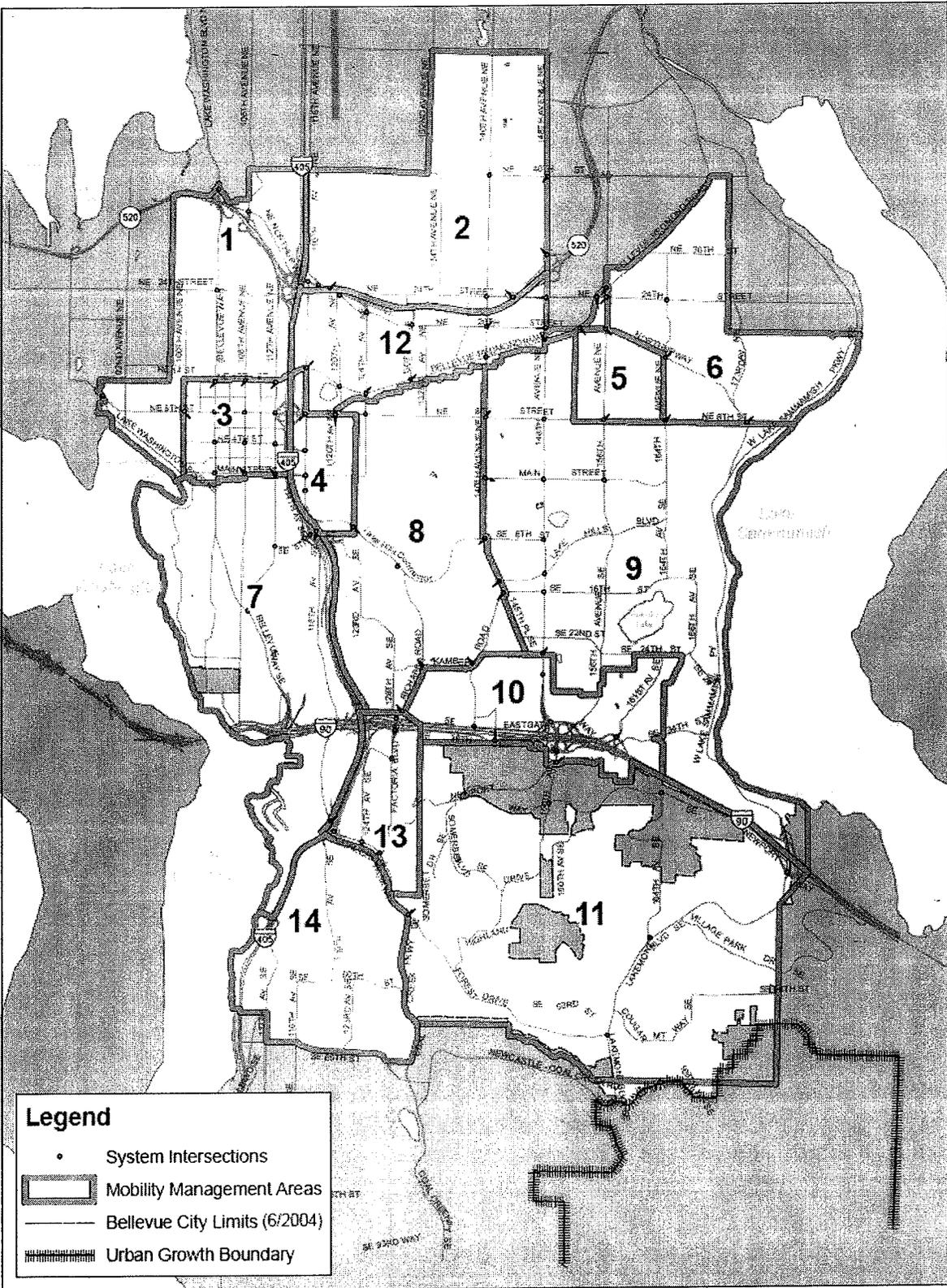


FIGURE TR.1
Mobility Management Areas

ATTACHMENT D5

*Table TR.1 Area Mobility Targets
(reference Policies TR-21, TR-36)*

AREA	ROADWAY (Area Average Level of Service) See TABLE TR.2 for Descriptions	MODE SPLIT INCLUDING RIDESHARING (% commute trips by modes other than SOV)	
		2002	2005 Target
REGIONAL CENTER			
3 - Downtown	E+	32	40
MIXED COMMERCIAL			
RESIDENTIAL AREAS			
4 - Wilburton	D		
5 - Crossroads	D-	19	25
10 - Eastgate	D	26	35
12 - Bel-Red/Northup	E+	20	25
13 - Factoria	E+	15	20
RESIDENTIAL GROUP 1*			
1 - N. Bellevue	D+	N/A for residential areas	
7 - S. Bellevue	D+		
8 - Richards Valley	D+		
9 - E. Bellevue	D+		
RESIDENTIAL GROUP 2*			
2 - Bridle Trails	C	N/A for residential areas	
6 - NE Bellevue	C		
11 - Newcastle	C		
14 - Newport	C		

* Groupings based on street patterns, transit serviceability, topography, development patterns, & land use objectives (see Policy TR-21).

ATTACHMENT 2

Land Use Code Amendments Draft

Part 20.25D Bel-Red

20.25D.010 General.

A. Applicability.

1. This Part 20.25D, Bel-Red (BR) contains requirements, standards, and guidelines that apply to development and activity within the Bel-Red Land Use Districts. Except to the extent expressly provided in this Part 20.25D and as referenced in paragraph 2 below, the provisions of the Land Use Code, other development codes, the city development standards, and all other applicable codes and ordinances shall apply to development and activities in the Bel-Red Land Use Districts. Except within the Critical Areas Overlay District (Part 20.25H LUC), where there is a conflict between the Bel-Red Land Use District regulations and the Land Use Code and other city ordinances, the Bel-Red Land Use District regulations shall govern. Where there is a conflict between the Bel-Red Land Use District regulations and the Critical Areas Overlay District, the Critical Areas Overlay District shall govern. Where the requirements of the Bel-Red Land Use District exceed those of the Land Use Code and other city ordinances, the Bel-Red Land Use District regulations shall govern.
2. The following general development requirements of Chapter 20.20 LUC do not apply in the Bel-Red Land Use Districts:
 - a. LUC 20.20.005;
 - b. LUC 20.20.010;
 - c. LUC 20.20.012;
 - d. LUC 20.20.015
 - e. LUC 20.20.017;
 - f. LUC 20.20.018;
 - g. LUC 20.20.030 (provided, this section applies in the ORT Land Use District);
 - h. LUC 20.20.060;
 - i. LUC 20.20.070;
 - j. LUC 20.20.125 (provided, this section applies in the ORT Land Use District);
 - k. LUC 20.20.128;
 - l. LUC 20.20.135;

- m. LUC 20.20.190;
- n. LUC 20.20.250;
- o. LUC 20.20.400;
- p. LUC 20.20.520;
- q. LUC 20.20.560;
- r. LUC 20.20.720;
- s. LUC 20.20.760;
- t. LUC 20.20.800; and,
- u. LUC 20.20.890.

B. Land Use Districts.

Bel-Red is divided into twelve (12) land use districts to facilitate its transformation into a vibrant and sustainable, pedestrian oriented mixed use neighborhood with vital businesses. The transformation will include multimodal transit systems that connect to the greater city and region. The geographic location of Bel-Red is legally described in LUC 20.50.012.

The purpose and intent of the Bel-Red Land Use Districts is described in LUC 20.10.375 and application of requirements, standards, and guidelines to a specific property may differ depending upon which of the following districts a property is located.

1. Bel-Red-Medical Office (BR-MO).
2. Bel-Red-Medical Office Node 1 (BR-MO-1).
3. Bel-Red-Office/Residential (BR-OR).
4. Bel-Red-Office/Residential Node 1 (BR-OR-1).
5. Bel-Red-Office/Residential Node 2 (BR-OR-2).
6. Bel-Red-Residential/Commercial Node 1 (BR-RC-1).
7. Bel-Red-Residential/Commercial Node 2 (BR-RC-2).
8. Bel-Red-Residential/Commercial Node 3 (BR-RC-3).
9. Bel-Red-Commercial/Residential (BR-CR).

10. Bel-Red-Residential (BR-R).
11. Bel-Red-General Commercial (BR-GC).
12. Bel-Red-Office/Residential Transition (BR-ORT).

20.25D.020 Definitions Specific to Bel-Red.

A. Bel-Red Definitions.

The following definitions are specific to the Bel-Red land use districts and shall have the following meaning:

BROTS. An interlocal agreement between the cities of Bellevue and Redmond regarding land use planning and the funding and construction of transportation improvements in the Bel-Red/Overlake Transportation Study Area, as adopted by Resolution No. 6353 and subsequently amended.

Build-to Lines. A location along a designated block or right-of-way where a building must be constructed. The build-to line is the property line unless designated otherwise by an adopted street design.

Curb Extension. A section of sidewalk that projects into the street at an intersection or midblock crossing that reduces the crossing width of a street or right-of-way for pedestrians.

Average Finished Grade Along Façade. Proposed grade after development as measured along a building façade from perpendicular wall to perpendicular wall including offsets, bays, and other minor modulating treatments not more than five feet deep.

Façade Length. The length of a building from perpendicular wall to perpendicular wall including offsets, bays, and other minor modulating treatments not more than five feet deep.

Gross SF/Floor. Floor area in square feet within the surrounding exterior walls measured from the interior wall surface and including openings in the floor plate such as vent shafts, stairwells, and interior atriums.

Natural Drainage Practices. Techniques such as rain gardens, pervious pavement, vegetated roofs, and amended soils that manage stormwater runoff in a manner that improves the quality of runoff and more closely mimics natural drainage flows and rates than traditional stormwater techniques.

Node. An area, or district where planned transportation facilities will support sufficient development intensity, amenities, recreation opportunities, and a mix of uses that foster a high level of pedestrian activity.

Project Limit. A lot, portion of a lot, or combination of lots or portions of lots treated as a single development parcel for purposes of the Land Use Code. A project limit may cross a right-of-way as long as the project limit results in a cohesive design and the Master Development Plan process is used.

Required Ground Floor Uses. Retail and commercial activities or a combination thereof as permitted by 20.25D.070 that are required to be located on the ground floor. Ground floor uses shall be located as indicated in LUC Figure 20.25D.120.A.

Tower. That portion of a building that is in excess of 40 ft above average finished grade for any building with a maximum allowable height of 70 feet or greater.

Tree Well. A tree planting area, generally within a paved surface area.

Work-Live Unit. A commercial building or tenant space that includes a functionally related and integrated residential unit. Employees and walk-in trade are permitted.

B. General Definitions not applicable to Bel-Red are noted in the text of the general definitions contained in Chapter 20.50 LUC.

20.25D.030 Review Required.

A. Applicable Review and Guidelines.

The Director shall use this Part 20.25D LUC and the Bel-Red Subarea Design Guidelines (LUC 20.25D.140 as currently adopted or subsequently amended or superseded) in reviewing an application for Master Development Plan or Design Review approval in the Bel-Red Use Districts. The Community Retail Design Guidelines of Part 20.25I LUC and Transition Area Design Guidelines of Part 20.25B LUC shall not apply to applications for development in the Bel-Red Land Use Districts.

B. Master Development Plans.

1. Projects located within a node. An applicant for a project with multiple buildings located within a single project limit shall submit a Master Development Plan for approval by the Director pursuant to Part 20.30V LUC. An applicant for a single building project may submit a Master Development Plan for approval by the Director pursuant to Part 20.30V LUC. The Director may, through the Master Development Plan process, approve a proposal that varies from the specific requirements as set forth in paragraph C.2 below.
2. Projects located outside of a node. An applicant may submit a Master Development Plan for approval by the Director pursuant to Part 20.30V LUC. The Director may, through the Master Development Plan process, approve a

proposal that varies from the specific requirements as set forth in paragraph C.2 below.

3. This requirement shall not apply to the placement of additional structures associated with existing conditions.
4. For the purposes of this section, the project limit may be drawn to encompass right-of-way that bisects a site, provided the Director finds that the following connectivity criteria can be met:
 - a. A system of corner and mid-block crossings shall be provided to functionally connect on-site pedestrian paths across the bisecting right-of-way within the proposed project limit.
 - b. Pedestrian paths shall be provided to connect all buildings and right-of-way crossings located within the proposed project limit.
 - c. Visual connections shall be provided between all buildings located within the project limit by minimizing topographic variation and through use of vegetation and outdoor spaces.
 - d. Only right-of-way meeting the requirements of LUC 20.25D.080.D may be included in the land area located within the proposed project limit for the purpose of computing maximum FAR.

C. Design Review Required.

1. Process Required. With the exception of the Bel-Red General Commercial district (BR-GC) all development within the Bel-Red Land Use Districts shall be reviewed by the Director through Design Review (Part 20.30F LUC). An applicant may submit a Design Review for approval by the Director pursuant to Part 20.30F LUC for a development proposal located in the Bel-Red General Commercial that includes deviations permitted by paragraph 2 below.
2. Deviations. The Director may, through the Design Review process, approve a proposal that varies from the specific requirements set forth in LUC Section 20.25D.130 if the applicant demonstrates that the resulting design will be more consistent with the purpose and intent of the code. Deviation from the following dimensional requirements is not permitted, except pursuant to 20.25D.080.B, or unless a variance is obtained under Part 20.30G LUC:
 - a. Floor plate maximums;
 - b. Minimum setbacks/stepbacks;
 - c. Impervious surface/lot coverage percentages; and
 - d. Maximum floor area ratio.

No deviation from Maximum Building Height is permitted in any Bel-Red Land Use District.

D. Procedural Merger.

Within a Bel-Red Land Use District, any administrative decision required by this Part 20.25D or by the Land Use Code, including but not limited to the following, may be applied for and reviewed as a single Process II Administrative Decision, pursuant to LUC 20.35.200-250:

1. Master Development Plan, Part 20.30V LUC;
2. Administrative Conditional Use Permit, Part 20.30E LUC;
3. Design Review, Part 20.30F LUC;
4. Variance, Part 20.30G LUC; and
5. Critical Areas Land Use Permit, Part 20.30P LUC.

20.25D.035 Recording Required.

Following approval of a Master Development Plan or Design Review and any subsequent modifications thereto, the applicant shall record the plans and conditions that constitute the approval with the King County Division of Records and Elections or its successor agency. Components of the approval required to be recorded include but are not limited to the applicable conditions of approval, total amount (square footage) of floor area earned through the FAR Amenity Incentive System, or floor area earned through the Special Dedication of right-of-way, parks or open space. A copy of the recorded document shall be provided to the city for inclusion in the project file.

20.25D.040 Phasing of Intensity.

This section provides limits on development based on (1) the provision of public improvements to the Bel-Red Subarea, and (2) the Interlocal Agreement between the cities of Bellevue and Redmond for the Bel-Red/Overlake Transportation Study Area (otherwise referred to as BROTS).

A. BROTS Interlocal Agreement Limitation.

Development in the Bel-Red Land Use Districts shall be subject to the provisions of BROTS, as originally executed to cap commercial development at a total of 12.2 million square feet through 2012, or as thereafter amended or superseded.

B. Bel-Red Development Limitation.

Development as measured in the BROTS Interlocal Agreement in the Bel-Red Land Use Districts shall not exceed 4,500,000 square feet until light rail is provided to the Bel-Red Subarea.

20.25D.050 Permitted Uses.

A. Permitted Uses.

Specific categories of uses are listed in Chart 20.25D.070 LUC. LUC 20.25D.050.B explains Chart 20.25D.070 and describes the applicable review procedures. The use chart description and interpretation provisions of 20.10.400 do not apply to the Bel-Red Land Use Districts.

B. Use Chart Described.

In Chart 20.25D.070, land use classifications and standard Land Use Code reference numbers are listed on the vertical axis. City of Bellevue land use districts are shown on the horizontal axis.

1. If no symbol appears in the box at the intersection of the column and the row, the use is not allowed in that district, except for short-term uses, which are regulated under Part 20.30M LUC (Temporary Use Permits), subordinate uses which are regulated under LUC Section 20.20.840, and existing uses which are regulated under LUC Section 20.25D.060.
2. If the symbol "P" appears in the box at the intersection of the column and row, the use is permitted subject to applicable general requirements of Chapter 20.20 LUC for the use and the district specific requirements of this Part 20.25D LUC.
3. If the symbol "C" appears in the box at the intersection of the column and the row, the use is permitted subject to the Conditional Use provisions specified in Part 20.30B or 20.30C LUC in addition to any applicable general requirements for the use and land use district.
4. If the symbol "A" appears in the box at the intersection of the column and the row, the use is permitted subject to the Administrative Conditional Use provisions as specified in Part 20.30E LUC in addition to any applicable general requirements for the use and land use district.
5. If a number appears in the box at the intersection of the column and the row, the use is permitted through the applicable review process and subject to the special limitations indicated in the corresponding Notes.
6. If a "/" appears in the box at the intersection of the column and the row:

- a. The process or note indicated in front of the slash (i.e., P/ or 1/ applies outside of the node described in the column; and
- b. The process or note indicated behind the slash (i.e., /P or /1) applies inside the node described in the column.

20.25D.060 Existing Conditions.

A. Purpose.

Many existing uses and developments would not be allowed pursuant to the Bel-Red Land Use District Charts (refer to LUC Section 20.25D.070) and district specific standards and guidelines in Part 20.25D that are in effect to implement the policies of the Bel-Red Subarea Plan. The purpose of this section is to allow the continued operation of existing light industrial and service uses and development that were legally established when the Bel-Red Subarea Plan was adopted on [insert Plan adoption date]. An additional purpose of this section is to allow limited expansion of existing uses and structures that is compatible with residential and higher intensity mixed use development that was introduced with the adoption of the Plan.

B. Types of Existing Conditions - Definitions.

A site may be considered an existing condition because it contains either an existing use or existing development as defined in this paragraph and based on documentation provided pursuant to paragraph D of this section.

1. Existing Use. The use of a structure or land which was permitted when established, in existence on [insert Plan adoption date] and not discontinued or destroyed, but is not otherwise allowed under LUC 20.25D.070.
2. Existing Development. A structure or site development which was permitted when established, in existence on [insert Plan adoption date] and not discontinued or destroyed, but does not otherwise comply with Part 20.25D LUC.

C. Applicability.

1. This Section 20.25D.060 LUC applies only to existing conditions occurring within a Bel-Red Land Use District.
2. The nonconforming provisions of LUC 20.20.070 and 20.20.560 do not apply within the Bel-Red Land Use Districts.
3. Expansions or modifications of an existing use or development shall comply with any applicable requirements of Part 20.25H LUC – Critical Areas Overlay District. In the event of a conflict between this Part 20.25D LUC and Part 20.25H LUC, the requirements of Part 20.25H LUC Critical Areas Overlay District shall control.

4. Modifications to signs associated with an existing use or development shall comply with any applicable requirements of the Sign Code, Title 22B.10 BCC.

D. Documentation.

The applicant shall submit documentation, which shows that the existing condition was permitted when established and has been maintained over time. The Director shall determine based on Paragraph 1 and 2 below whether the documentation is adequate to support a determination that the use and development constitute an existing condition under the terms of this section. The Director may waive the requirement for documentation when an existing condition has been clearly established.

1. Existing Condition Permitted when Established. Documentation that the condition was permitted when established includes, but is not limited to the following:
 - a. Building, land use or other development permits; or
 - b. Land Use Codes or Land Use District Maps.
2. Existing Condition Maintained Over Time. Documentation that the existing condition was maintained over time, and not discontinued or destroyed as described in this Section 20.25D.060 includes, but is not limited to the following:
 - a. Utility bills;
 - b. Income tax records;
 - c. Business licenses;
 - d. Listings in telephone or business directories;
 - e. Advertisements in dated publications;
 - f. Building, land use or other development permits;
 - g. Insurance policies;
 - h. Leases; and
 - i. Dated aerial photos.
3. Appeal of Director Determination. The Director determination of whether a use constitutes an existing condition may be appealed pursuant to LUC Section 20.35.250 Appeal of Process II decisions.

E. Regulations Applicable to all Existing Conditions.

1. Ownership. The status of an existing condition is not affected by changes in ownership.
2. Maintenance. Normal maintenance and repairs associated with existing conditions are allowed.

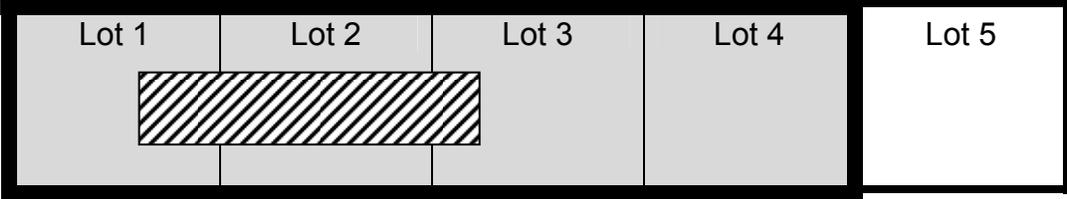
F. Regulations Applicable to Existing Uses.

1. Operations.
 - a. Existing Uses May Continue to Operate. Operations associated with an existing use may continue, subject to the provisions of this Section F.
 - b. Existing Uses – Hours of Operation. The hours of operation associated with an existing use located in land use districts which permit residential uses may only extend into the period of 9:00 p.m. to 6:00 a.m. subject to Administrative Conditional Use approval. Existing uses which currently operate between these hours may continue without ACU approval, as long as the hours of operation between 9:00 p.m. and 6:00 a.m. are not expanded.
2. Expansions. Existing uses may expand under certain circumstances as described in this paragraph:
 - a. Expansions of Floor Area. Floor area associated with existing uses may be expanded in conformance with this code.
 - b. Expansions of Exterior Improved Areas. Exterior improved areas associated with an existing use may be expanded by increasing the amount of land used. Exterior areas supporting the existing use may be expanded.
 - c. Limitations on Expansion.
 - i. No expansion of hazards. No expansion in operations shall be permitted that increases the use or on-site quantity of flammable or hazardous constituents (e.g, compressed gases, industrial liquids, etc.), or that increases the amount of waste generated or stored that is subject to the Washington Hazardous Waste Management Regulations Chapter 70.105.210 RCW as currently adopted or subsequently amended or superseded. The Director may in consultation with the Fire Marshal modify the requirements of this paragraph if the Director determines that the expansion will not increase the threat to human health and the environment over the pre-expansion condition.
 - ii. Expansions within Nodes (BR-MO-1, BR-OR-1 and 2, BR-RC-1, 2, and 3) and Residential Land Use Districts (BR-R). Refer to LUC 20.25D.060.F.2.c Figure 1 below. Floor area or exterior improvements

associated with an existing use may be expanded when proposed within the limits of property held in a single ownership in existence on [insert Plan adoption date] pursuant to an Administrative Conditional Use approval.

20.25D.060.F.2.c - Figure 1

Expansions within nodes and residential Land Use Districts



-  Limits of property held in a single ownership as of (date)
-  Area occupied by existing use as of (date)
-  Area where existing use may expand in conformance with this code

iii. Expansions outside Nodes and in Non-Residential Land Use Districts (BR-MO, BR-OR, BR-GC, BR-CR, and BR-ORT). Refer to LUC 20.25D.060.F.2.c Figure 2 below. Floor area or exterior improvements associated with an existing use may be expanded beyond limits of property held within a single ownership in existence on [insert Plan adoption date] pursuant to an Administrative Conditional Use approval and the following limitations:

- (1) The property proposed for expansion is abutting at least one of the property lines of the existing use as they existed on [insert Plan adoption date].
- (2) The regulations applicable to the property proposed for expansion would have allowed the use as of [insert Plan adoption date].

20.25D.060.F.2.c - Figure 2

Expansion outside nodes and in non-residential Land Use Districts



-  Limits of property held in a single ownership as of (date)
-  Area occupied by existing use as of (date)
-  Area where existing use may expand in conformance with this code

3. Loss of Existing Use Status.

- a. **Discontinuance.** If an existing use of a structure or exterior improved area is discontinued for a period of 12 months, any subsequent use shall thereafter conform to the regulations of the district in which it is located.
- b. **Accidental Destruction.** When a structure containing an existing use is damaged by fire or other causes beyond the control of the owner, the use may be re-established. The structure may be repaired and/or reconstructed in accordance with applicable city codes.
- c. **Intentional Destruction.** When a structure containing an existing use is intentionally damaged by fire or other causes within the control of the owner of that structure that houses the existing use, the re-establishment of the existing use is prohibited.
- d. **Relinquishment.** An existing use is relinquished when the existing use is replaced with an allowed use pursuant to LUC 20.25D.070. Upon relinquishment, the existing use rights no longer apply and the existing use may not be re-established.

G. Regulations Applicable to Existing Development.

1. **Existing Development May Remain.** Existing development may remain unless specifically limited by the terms of this paragraph.
2. **Permitted Alterations to Existing Development.** Existing development may be altered, provided that the alteration conforms to city codes and the existing development conforms to proportional compliance requirements contained in paragraph 3 below.

- a. Three Year Period. Alterations made within a three year period will be viewed as a single change for the purposes of determining required improvements.
 - b. Value of Changes. The value of alterations is determined by the Director based on the entire project and not individual permits. The Director shall promulgate rules for determining the value of alterations in the context of LUC 20.25D.060.
3. Proportional Compliance. An existing development associated with an existing, permitted, or conditional use, may be altered consistent with the requirements set forth below:
- a. Threshold Triggering Required Improvements. The standards of this paragraph shall be met when the value of the proposed changes to an existing development exceed \$150,000 as of [insert Plan adoption date]. The threshold established here will be reviewed annually, and, effective January 1 of each year, may be administratively increased or decreased by an adjustment to reflect the current published annual change in the Seattle Consumer Price Index for Wager Earners and Clerical Workers as needed in order to maintain accurate construction costs for the region. The following alterations and improvements do not count toward the threshold:
 - i. Alterations required as a result of a fire prevention inspection;
 - ii. Alterations related to the removal of architectural barriers as required by the Americans with Disabilities Act, or the Washington State Building Code (RCW 19.27), now or as hereafter amended;
 - iii. Alterations required for the seismic retrofit of existing structures;
 - iv. Improvements to on-site stormwater management facilities in conformance with Chapter 24.06 BCC, now or as hereafter amended;
 - v. Alterations that reduce offsite impacts (including but not limited to noise, odors, dust, and other particulate emissions); and
 - vi. Alterations that meet LEED, Energystar or other industry recognized standard that results in improved mechanical system or operational efficiency.
 - b. Required Improvements. When alterations meet the threshold in subsection a above existing development shall be brought toward compliance in the following areas:
 - i. Landscape development requirements as set forth in LUC 20.25D.100 and LUC 20.20.520 and required landscape treatments as set forth in LUC 20.25D.130;

- ii. Circulation and internal walkway requirements, as set forth in LUC 20.25D.120 and LUC 20.20.590;
 - iii. Surface parking lot landscaping as set forth in LUC 20.25D.100 and LUC 20.20.520 standards that apply to the site; and
 - iv. Required paving of surface parking, outdoor storage, and retail display areas.
- c. Timing and Cost of Required Improvements.
- i. Required improvements shall be made as part of the alteration that triggered the required improvements;
 - ii. The value of required improvements shall be limited to 20 percent of the value of the proposed alteration. The applicant shall submit evidence as required by the Director that shows the value of proposed improvements associated with any alteration; and
 - iii. Required improvements shall be made in order of priority listed in paragraph 3.b above unless a deviation in priority order is approved by the Director as necessary to accommodate a function that is an essential component of the existing development.

4. Loss of Existing Development Status.

- a. Discontinuance. If an existing development is discontinued or abandoned for a period of 12 months, any subsequent development shall thereafter conform to the regulation of the district in which it is located.
- b. Accidental Destruction. When an existing development is damaged by fire or other causes beyond the control of the owner, the existing development may be re-constructed. The existing development may be repaired and/or reconstructed in its original configuration. Changes to the footprint and exterior proposed as part of the repair and/or reconstruction must conform to this code.
- c. Intentional Destruction. When an existing development is intentionally damaged by fire or other causes within the control of the owner of the existing development, reconstruction of the existing development is prohibited.

20.25D.070 Land Use Charts.

The following charts apply to Bel-Red. The use charts contained in LUC 20.10.440 do not apply within the Bel-Red land use districts.

Chart 20.25D.070 Residential Uses in Bel-Red Land Use Districts

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Residential – Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
1	Residential (1) (5)							
	Single-Family Dwelling		P 2/		P 2	3	P 2, 3	P
	Two to Four Dwelling Units Per Structure		P/P	P	P	3	P 3	P
	Five or More Dwelling Units Per Structure		P/P	P	P	3	P 3	P
12	Group Quarters: (5) Dormitories, Fraternal Houses, Excluding Military and Correctional Institutions, and Excluding Secure Community Transition Facilities		/P	P				
13 15	Hotels and Motels	P/P	P/P	P		P	P	
	Congregate Care Senior Housing (1)	P/P	P/P	P	P		P	
6516	Nursing Home (1)	P/P	P/P	P	P		P	
	Assisted Living (1)	P/P	P/P	P	P		P	
	Accessory Dwelling Unit (4)	P/	P/		P		P	P

Existing uses in the Bel-Red District are regulated pursuant to 20.25D.060.

Key

P – Permitted Use

C – Conditional Use (see Parts 20.30B and 20.30C)

A – Administrative Conditional Use (see Part 20.30E)

Notes: Uses in land use districts – Residential

- (1) An agreement shall be recorded with the King County Department of Records and Elections restricting senior citizen dwellings, congregate care senior housing, or assisted living to remain as senior housing for the life of the project.
- (2) A minimum density of 10 units per acre shall be achieved for new single family dwelling units. This requirement does not apply to work-live units.
- (3) Work-live units are the only housing permitted in BR-GC.
- (4) Accessory dwelling units may be established in existing or permitted (P) single family structures subject to the performance criteria of LUC 20.20.120.
- (5) Bicycle parking shall be provided pursuant to 20.25D.120.G.

Chart 20.25D.070 Manufacturing Uses in Bel-Red Land Use Districts

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Manufacturing-Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
2 and 3	Manufacturing (1, 2, 5)							
21	Food and Beverage Products Mfg. (3)					P 4	P 4	
22	Textile Products Mfg.					P 4	P 4	
23	Apparel, Fabric, Accessories, and Leather Goods Mfg.					P 4	P 4	
24	Lumber and Wood Products Mfg.							
25	Furniture and Fixtures Mfg.					P 4	P 4	
26	Paper Products Mfg.					P4	P4	
27	Printing, Publishing, and Allied Industries					P		
28	Chemicals and Related Products Mfg.							
31	Rubber Products Mfg.							
314	Misc. Plastic Products Mfg.					P 4	P 4	
321 322 324 325 327	Light Stone, Clay, and Glass Products Mfg.; Glass, Pottery, and China Ceramic Products, Stone Cutting and Engraving (except Concrete Batch Plant)					P 4	P 4	
	Concrete Batch Plant							
329	Handcrafted Products Mfg.					P 4	P 4	
3427	Computers, Office Machines, and Equipment Mfg.					P 4		

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Manufacturing-Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
3433	Electrical Equipment Mfg.; Appliances, Lighting, Radio, TV Communications, Equipment and Component Parts							
3434								
3435							P 4	
3436								
3437								
3491	Fabricated Metal Products Mfg.;							
3492								
3493								
3495							P 4	
3497								
35	Measuring, Analyzing and Controlling Instruments, Photographic, Medical and Optical Goods; Watches and Clocks Mfg.;	P					P 4	P 4
3997	Signs and Advertising Display Mfg.						P	
3999	Misc. Light Fabrication Assembly and Mfg. Not Elsewhere Classified						P	

Existing uses in the Bel-Red District are regulated pursuant to 20.25D.060.

Key P – Permitted Use

C – Conditional Use (see Parts 20.30B and 20.30C)

A – Administrative Conditional Use (see Part 20.30E)

Notes: Uses in land use districts – Manufacturing

- (1) Permitted manufacturing uses shall not include primary metal industries such as foundries, smelters, blast furnaces, rolling mills, and concrete batch plants. The Director may in consultation with the Fire Marshal modify the requirements of this note for a limited scale manufacturing use if the Director determines that the modification will not create an unreasonable threat to human health and the environment. A limited scale manufacturing use is one that is size restricted to 20,000 sf or less.
- (2) The manufacture of flammable, dangerous or explosive materials is excluded.
- (3) Microbrewery manufacturing is permitted as a component of an eating and drinking establishment; provided, that the manufacturing use occupies not more than 50 percent of the total square footage of the combined establishment.
- (4) The manufacturing use located in this land use district shall not exceed 20,000 sf.
- (5) New outdoor storage associated with this permitted manufacturing use shall comply with applicable performance criteria for that use and the Landscape Development Requirements as set forth in LUC 20.25D.100.

Chart 20.25D.070 Transportation and Utilities Uses in Bel-Red Land Use Districts

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Transportation and Utilities – Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
4	Transportation, Communications and Utilities							
41	Rail Transportation: Right-of-Way, Yards, Terminals, Maintenance Shops	C/C	C/C	C	C	C	C	C
42 4291	Motor Vehicle Transportation: Bus Terminals, Taxi Headquarters					C		
4214 422	Motor Vehicle Transportation: Maintenance Garages and Motor Freight Services	C/	C/			C		
43	Aircraft Transportation: Airports, Fields, Terminals, Heliports, Storage and Maintenance	C 1	C 1					
	Accessory Parking (2,3)	P/P	P/P	P	P 4	P	P	P
46	Auto Parking Commercial Lots and Garages (5)	/P	/P	P				
	Park and Ride (6)		C/C	C				
475	Radio and Television Broadcasting Studios							
485	Solid Waste Disposal (7)							
	Highway and Street Right-of-Way (8)	P	P/P	P	P	P	P	P
	Utility Facility	C	C/C	C	C	C	C	C
	Local Utility System	P	P/P	P	P	P	P	P
	Regional Utility System	C	C/C	C	C	C	C	C
	On and Off-Site Hazardous Waste Treatment and Storage Facilities							

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Transportation and Utilities – Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
	Essential Public Facility (9)	C	C/C	C	C	C	C	C
	Wireless Communication Facility (WCF): (without WCF Support Structures)	10, 11, and 12	10, 11, and 12	10, 11, and 12	10, 11, and 12	10, 11, and 12	10, 11, and 12	10, 11, and 12
	Communication, Broadcast and Relay Towers Including WCF Support Structures (Freestanding)	10, 11	10, 11	10-, 11	10, 11	10, 11	10, 11	10, 11
	Satellite Dishes (13)	P	P/P	P	P	P	P	P
	Electrical Utility Facility (14)	A C/A C	A C/A C	A C	A C	A C	A C	A C

Existing uses in the Bel-Red District are regulated pursuant to 20.25D.060.

Key P – Permitted Use

C – Conditional Use (see Parts 20.30B and 20.30C)

A – Administrative Conditional Use (see Part 20.30E)

Notes: Uses in land use districts – Transportation and Utilities

- (1) Aircraft transportation is limited to only heliports and is regulated under the terms of LUC 20.20.450.
- (2) Accessory parking requires approval through the review process required for the primary land use which it serves pursuant to LUC Chart 20.25D.070.
- (3) The location of an off-site parking facility shall be approved by the Director. See LUC 20.25D.120.
- (4) Accessory parking is not permitted in the BR-R land use district as accessory to any use that is not permitted in BR-R.
- (5) Commercial lots and garages are only permitted to accommodate short-term parking (four hours or less). Parking structures are required to meet the performance standards contained in LUC 20.25D.120.D.

- (6) A park and pool lot or other carpool facility is regulated as a park and ride. A park and ride providing no more than 50 parking spaces, and utilizing the parking area of an established use shall be regulated as an accessory use under LUC 20.20.200. Any other park and ride requires a conditional use permit.
- (7) Solid waste disposal facilities may be continued as an existing use pursuant to LUC 20.25D.060, provided all requirements in LUC 20.20.820 are met.
- (8) Design is required to meet the standards contained in LUC 20.25D.130 and the 2008 Transportation Department Design Manual as currently adopted or subsequently amended or superseded.
- (9) Refer to LUC 20.20.350 for general requirements applicable to Essential Public Facilities (EPF).
- (10) Wireless communication facilities (WCFs) are not permitted on residential structures, sites developed with a residential use, or on undeveloped sites located in the BR-R land use district. This note does not prohibit locating a WCF on nonresidential structures (i.e., churches, schools, public facility structures, utility poles, etc.) or in public rights-of-way in any BR land use district.
- (11) Refer to LUC 20.20.195 for general requirements applicable to wireless communication facilities and other communication, broadcast, and relay facilities.
- (12) Antenna and associated equipment used to transmit or receive fixed wireless signals when located at a fixed customer location are permitted in all land use districts and are exempt from the requirements of LUC 20.20.010, 20.20.195 and 20.20.525 so long as the antenna and equipment comply with 47 C.F.R. 1.400, now or as hereafter amended. A building permit may be required to ensure safe installation of the antenna and equipment.
- (13) Refer to LUC 20.20.730 for general requirements applicable to Large Satellite Dishes. In BR-OR, BR-OR-1, BR-OR-2, BR-RC-1, BR-RC-2, BR-RC-3, BR-CR, and BR-R, only the provisions of LUC 20.20.730.B and C shall apply.
- (14) For the definition of Electrical Utility Facility see LUC 20.50.018 and for reference to applicable development regulations relating to electrical utility facilities see LUC 20.20.255. For new or expanding electrical utility facilities proposed on sensitive sites as described by Figure UT.5a of the Utilities Element of the Comprehensive Plan, the applicant shall obtain Conditional Use Permit approval under Part 20.30B LUC, complete an alternative siting analysis as described in LUC 20.20.255.D and comply with decision criteria and design standards set forth in LUC 20.20.255. For expansions of electrical utility facilities not proposed on sensitive sites as described by Figure UT.5a, the applicant shall obtain Administrative Conditional Use Permit approval under Part 20.30E LUC and comply with decision criteria and design standards set forth in LUC 20.20.255.

Chart 20.25D.070 Wholesale and Retail Uses in Bel-Red Land Use Districts

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Wholesale and Retail -Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
5	Trade (Wholesale and Retail) (1, 7)							
51	Wholesale Trade: General Merchandise, Products, Supplies, Materials and Equipment except the following:					P 2		
5111 5156 5157 5191 5192	Wholesale Trade: Motor Vehicles, Primary and Structural Metals, Bulk Petroleum							
5193	Scrap Waste Materials, Livestock							
	Recycling Centers		P/			P	P	
521 522 523 524	Lumber and Other Bulky Building Materials Including Preassembled Products		P 3/			P	P 3	
5251	Hardware Paint, Tile and Wallpaper (Retail)		P 3/	P 3	P 4	P	P 3	
5252	Farm Equipment							
53	General Merchandise: Dry Goods, Variety and Dept. Stores (Retail)		P/P 2	P 2	P 4	P	P	
54	Food and Convenience Store (Retail) (5,6)	P/P	P/P	P	P 4	P	P	P4
5511	Autos, Motorcycles (Retail)					P 7	A 7	
	Trucks, Recreational					P 2		

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Wholesale and Retail -Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
	Vehicles (Retail)							
	Boats (Retail)					P 2		
552	Automotive and Marine Accessories (Retail)					P 2		
553	Gasoline Service Stations (8)		P/			P	P	
56	Apparel and Accessories (Retail)	P/P 2	P/P	P	P 2	P	P	
57	Furniture, Home Furnishing (Retail)		P/P	P		P	P	
58	Eating and Drinking Establishments (3, 6)		P/ P 9	P 9	P 4	P	P	P
59	Misc. Retail Trade: Drugs, Liquor, Antiques, Books, Sporting Goods, Jewelry, Florist, Photo Supplies, Video Rentals and Computer Supplies		P/ P 3	P 3	P 4	P	P 2	P
	Adult Retail Establishments (10)		P/P			P	P	
5961	Farm Supplies, Hay, Grain, Feed and Fencing, etc. (Retail)					P 2		
596	Retail Fuel Yards							
5996	Garden Supplies, Small Trees, Shrubs, Flowers, Ground Cover, Horticultural Nurseries and Light Supplies and Tools (11)		P/ P 3	P 3	P 4	P	P	
5999	Pet Shop (Retail, Pet Day Care, and Grooming) (12)			P 3	P 4	P	P	
	Computers and Electronics (Retail)		P/ P 3	P 3		P	P	

Existing uses in the Bel-Red District are regulated pursuant to 20.25D.060.

Key P – Permitted Use

C – Conditional Use (see Parts 20.30B and 20.30C)

A – Administrative Conditional Use (see Part 20.30E)

Notes: Uses in land use districts – Wholesale and Retail

- (1) Wholesale trade excludes tank farms.
- (2) This individual use is limited in size to no greater than 20,000 sf per establishment.
- (3) Any new business which combines two or more permitted retail sales uses and also includes subordinate retail sales uses shall be limited in size to 50,000 sf. This Note does not apply to uses that exceed the size limit, but are documented existing uses pursuant to LUC 20.25D.060.
- (4) Within a project limit, all miscellaneous retail uses combined shall not exceed 10,000 sf and each individual use shall not exceed 3,000 sf.
- (5) Food and Convenience Stores (Retail) shall contain at least 75 percent square footage of retail food sales not for consumption on premises.
- (6) Drive-in windows are not allowed except as regulated by Section 20.25D.060, Existing Conditions.
- (7) See LUC 20.25D.090 Automobile Sales, Leasing, and Rental for applicable performance criteria.
- (8) Gasoline service stations may include convenience stores.
- (9) Microbrewery manufacturing is permitted as a component of an eating and drinking establishment; provided, that the manufacturing use occupies not more than 50 percent of the total square footage of the combined establishment.
- (10) Adult retail establishments are subject to the regulations for adult entertainment uses in LUC 20.20.127.
- (11) Garden Supplies excludes items such as large trees, rock, and bulk supplies which require special handling equipment.
- (12) See LUC 20.20.130 for general requirements applicable to this use.

Chart 20.25D.070 Services Uses in Bel-Red Land Use Districts

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Services - Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
6	Services							
61	Finance, Insurance, Real Estate Services (11)	P 10/ P 10	P/P	P	P 1	P	P	P
62	Personal Services: Laundry, Dry Cleaning, Barber and Beauty, Photography Studio and Shoe Repair	P/P	P/P	P	P1	P	P	P
6241	Funeral and Crematory Services	C/C				C		
6262	Cemeteries							
629	Child Care Services (2, 3)							
	Family Child Care Home in Single Family Residence	P/			P	P	P	P
	Child Day Care Center	P/P	P/P	P	A 4	P	P	
	Adult Day Care	P/P	P/P	P	A 4	P	P	
63	Business Services, Duplicating and Blue Printing, Steno, Advertising (Except Outdoor), Travel Agencies and Employment (11)	P/P	P/P	P		P	P	P
634	Building Maintenance and Pest Control Services					P	P	

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Services - Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
637	Warehousing and Storage Services, Excluding Stockyards		P/			P		
639	Rental and Leasing Services: Cars, Trucks, Trailers, Furniture and Tools (5)					P		
641	Auto Repair and Washing Services		P/P 6	P 6		P	P	
649	Repair Services: Watch, TV, Electrical, Computer, Upholstery					P	P 7	
	Professional Services: Medical Clinics and Other Health Care Related Services (11)	P/P	P/P	P 4		P	P 7	P
	Professional Services: Other (11)		P/P			P	P 7	P
6513	Hospitals (11)	/C						
66	Contract Construction Services: Building Construction, Plumbing, Paving and Landscape					P		
671	Governmental Services: Executive, Legislative, Administrative and Judicial Functions (11)	A/A	A/A				A	A

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Services - Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
672 673	Governmental Services: Protective Functions and Related Activities Excluding Maintenance Shops (11)	A/A	A/A	A	A	A	A	A
	Limited Governmental Services: Executive and Administrative, Legislative and Protective Functions (11)	P/P	P/P	P	P 8	P	P	P
674 675	Military and Correctional Institutions	/C				C		
	Secure Community Transition Facility							
681	Education: Primary and Secondary (9, 11)	/A	A/A	A	A	A	A	A
682	Universities and Colleges (11)	A/P	A/P	P		A	A	
683	Special Schools: (11) Vocational, Trade, Art, Music, Driving, Barber and Beauty Schools	P/P	P/P	P	P 7	P	P	P 7
691	Religious Activities	P/P	P/P	P	A	P	A	A
692 (A)	Professional and Labor Organizations Fraternal Lodge	A/A	A/A	A		P	A	A
692 (B)	Social Service Providers	P/P	P/P	P	P	P	P 7	P
	Administrative Office – General (11)	P 10 / P 10	P/P	P 10		P	P 10	P

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Services - Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
	Computer Programming, Data Processing and Other Computer Related Services			P 10		P 10	P	P
	Research, Development and Testing Services			P 10		P 10	P	P

Existing uses in the Bel-Red District are regulated pursuant to 20.25D.060.

Key P – Permitted Use

C – Conditional Use (see Parts 20.30B and 20.30C)

A – Administrative Conditional Use (see Part 20.30E)

Notes: Uses in land use districts – Services

- (1) All service uses combined shall not exceed 10,000 sf and each individual service use shall not exceed 3,000 sf.
- (2) Refer to Chapter 20.50 LUC for definitions of child care service, family child care home, and child day care center.
- (3) A child care service may be located in a community facility in any land use district pursuant to LUC 20.20.170.E.
- (4) This use is limited in size to no greater than 10,000 sf.
- (5) See LUC 20.25D.090 Automobile Sales, Leasing, and Rental for applicable performance criteria.
- (6) Auto repair and washing services are permitted as a subordinate use pursuant to LUC 20.20.840 only if located in a structured parking area.
- (7) This use is limited in size to no greater than 20,000 sf.

- (8) Uses are limited to community police stations of 1,500 sf or less.
- (9) Refer to LUC 20.20.740 for general requirements applicable to public and private schools. Schools located in the BR-ORT land use district are considered to be in a Transition Area for the purposes of applying the above referenced section. All Bel-Red land use districts are considered to be non-residential for the purposes of applying LUC 20.20.740.
- (10) Limited to 0.5 floor area ratio (FAR).
- (11) Bicycle parking shall be provided pursuant to 20.25D.120.G.

Chart 20.25D.070 Recreation Uses in Bel-Red Districts

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Recreation – Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
7	Cultural Entertainment and Recreation							
711	Library, Museum	P	P	P	A 1	P	P	
7113	Art Gallery		P/P	P	P 2	P	P	
712	Nature Exhibitions: Aquariums, Botanical Gardens and Zoos (3)					C	C	
7212	Public Assembly (Indoor): Sports, Arenas, Auditoriums and Exhibition Halls but excluding School Facilities							
7214			A/A			A	A	
7222								
7231								
7232								
7212	Motion Picture, Theaters, Night Clubs, Dance Halls and Teen Clubs		P/P	P		P	P	
7214								
7218								
7213	Drive-In Theaters							
	Adult Theaters (4)		P/P			P		

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Recreation – Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
7223 73	Public Assembly (Outdoor): Fairgrounds and Amusement Parks, Miniature Golf, Golf Driving Ranges, Go- Cart Tracks, BMX Tracks and Skateboard Tracks					A	A	
73	Commercial Amusements: Video Arcades, Electronic Games			A		A	A	
7411 7413 7422 7423 7424 7441 7449	Recreation Activities: Golf Courses, Tennis Courts, Community Clubs, Athletic Fields, Play Fields, Recreation Centers, Swimming Beaches and Pools (5)	A/	A/A	A		A	A	
744	Marinas, Yacht Clubs							
7414 7415 7417 7425 7413	Recreation Activities: Skating, Bowling, Gymnasiums, Athletic Clubs, Health Clubs		P/P	P		P	P	

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Recreation – Bel-Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
7491 7515	Camping Sites and Hunting Clubs							
76	Private Leisure and Open Space Areas Excluding Recreation Activities Above	P	P/P	P	C	P	P	P
	Private Park	A	A/A	A	A	A	A	
	Stables and Riding Academies						A	
	Boarding or Commercial Kennels					A 6	A 6	
	Public Park	P	P/P	P	P 7	A	A	P 7

Existing uses in the Bel-Red District are regulated pursuant to 20.25D.060.

Key P – Permitted Use

C – Conditional Use (see Parts 20.30B and 20.30C)

A – Administrative Conditional Use (see Part 20.30E)

Notes: Uses in land use districts – Recreation

- (1) This individual use is limited in size to no greater than 20,000 sf.
- (2) Within a project limit, all art gallery uses combined shall not exceed 10,000 sf and each individual recreation use shall not exceed 3,000 sf. This size limitation does not apply to art gallery uses located in a city park facility. Refer to Recreation Note 7 for permitting process applicable to city park uses.
- (3) Excludes zoos.
- (4) Adult Theaters are subject to the regulations for adult entertainment uses in LUC 20.20.127.

- (5) For carnivals, see LUC 20.20.160.
- (6) See LUC 20.20.130 for general requirements applicable to this use.
- (7) Public parks are generally permitted in all districts. However, the following types of uses or facilities in public parks require administrative conditional use approval when located in the BR-R or BR-ORT land use districts: lighted sports and play fields, sports and play fields with amplified sound, and community recreation centers. Nonrecreation uses in public parks located in the BR Land Use Districts require conditional use approval, except that the permit requirements for wireless communication facilities shall be as set forth in LUC 20.20.195. For purposes of this note, “nonrecreation use” means a commercial, social service or residential use located on park property but not functionally related to park programs and activities.

Chart 20.25D.070 Resource Uses in the Bel-Red Districts

STD LAND USE CODE REF	LAND USE CLASSIFICATION	Resources - Bel Red Districts						
		Bel-Red Medical Office /Node	Bel-Red Office Residential /Nodes	Bel-Red Residential Comm. Nodes	Bel-Red Residential	Bel-Red General Commercial	Bel-Red Commercial Residential	Bel-Red Office Residential Transition
		BR- MO/ MO-1	BR- OR/ OR-1 OR-2	BR- RC-1 RC-2 RC-3	BR- R	BR- GC	BR- CR	BR- ORT
8	Resource Production (Minerals, Plants, Animals Including Pets and Related Services)							
81	Agriculture, Production of Food and Fiber Crops, Dairies, Livestock and Fowl, Excluding Hogs					P 1	P 1	
821	Agricultural Processing							
8221	Veterinary Clinic and Hospital (2)		P/P	P		P	P	P
8222	Poultry Hatcheries							
83	Forestry, Tree Farms and Timber Production							
8421	Fish Hatcheries							
85	Mining, Quarrying (Including Sand and Gravel), Oil and Gas Extraction							

Existing uses in the Bel-Red District are regulated pursuant to 20.25D.060.

Key P – Permitted Use

C – Conditional Use (see Parts 20.30B and 20.30C)

A – Administrative Conditional Use (see Part 20.30E)

Notes: Uses in land use districts – Resources

- (1) Agriculture production is limited to the production of food and fiber crops.
- (2) See LUC 20.20.130 for general requirements applicable to this use.

20.25D.080 Dimensional Requirements.

A. General.

Paragraph A of this section (Chart 20.25D.080.A Dimensional Requirements in Bel-Red Districts) sets forth the dimensional requirements for each land use district in the Bel-Red Subarea. The Dimensional Requirements of Chart 20.20.010 do not apply in the Bel-Red land use districts. Each structure, development, or activity in a Bel-Red Land Use District shall comply with these requirements except as otherwise provided in this section. If a number appears in a box at the intersection of a column and a row, the dimensional requirement is subject to the special limitation indicated in the corresponding Note.

Chart 20.25D.080.A Dimensional Requirement in Bel-Red Districts

Bel-Red Land Use District (19)	Tower Type (1) (17) (19)	Minimum Setbacks/Stepbacks (3) (5) (7) (10) (15)			Gross SF/Floor Above 40 ft (gsf/f) (13) (15) (16) (20) (21)	Gross SF/Floor Above 80 Ft (gsf/f) (13) (15) (16) (20) (21)	Maximum Impervious Surface/ Lot Coverage (7)(19)	Building Height (5)		Floor Area Ratio (5) (10)	
		Front	Rear	Side				Base	Max.	Base	Max.
MO-1 OR-1 RC-1	Nonresidential Residential	0 (2)	0	0	28,000 28,000/12,000	28,000 9,000	75%	45	150	1.0	4.0
OR-2 RC-2	Nonresidential Residential	0 (2)	0	0	28,000 28,000/12,000	28,000 9,000	75%	45	125	1.0	4.0
RC-3	Nonresidential Residential	0 (2)	0 (14)	0 (14)	28,000 28,000	NA	75%	45 (13)	70 (13)	1.0	4.0
CR	Nonresidential Residential	0 (2)	0 (14)	0 (14)	28,000 28,000	NA	75%	45 (13)	70 (13)	1.0	2.0
R	Nonresidential Residential	0 (2)	0	0	NA	NA	75%	30	45	1.0	2.0
MO OR	Nonresidential Residential	0 (2)	0	0	28,000 28,000	NA	75%	70	70	1.0	1.0
GC	Nonresidential Residential	0 (2)	0	0	NA	NA	75%	45	45	1.0	1.0
ORT	Nonresidential Residential	20	30	20	NA	NA	75%	45(12)	45 (12)	0.75	0.75
All	Parking (13)				NA	NA	75%	30	30	0.5	0.5

Notes: Chart 20.25D.080.A Dimensional Requirement in Bel-Red Districts

- (1) For the purposes of this chart, a tower is determined to be nonresidential or residential if more than 50 percent of the gross floor area of the tower is devoted to that use. This distinction does not apply to the requirement for providing FAR amenities per LUC 20.25D.
- (2) If a building is within 15 feet of the front property line, the building shall incorporate a 15-foot deep stepback in that façade at a height no more than 40 feet above the average finished grade along that façade.
- (3) All rear and side yards shall contain landscaping as required by LUC 20.25D.110 and 20.20.520.
- (4) The maximum building height and FAR may be achieved only by participation in the FAR Amenity System LUC 20.25D.090.
- (5) A building façade on any street identified as a Required Sidewalk-Oriented Development pursuant to LUC 20.25D.120.C shall incorporate a 15-foot deep stepback in that façade at a height no more than 40 feet above the average finished grade along that façade.
- (6) Impervious Surface/Lot Coverage is calculated after subtracting all critical areas and critical area buffers; provided, that coal mine hazards (20.25H.130) and habitat associated with species of local importance (20.25H.150) shall not be subtracted. See LUC 20.20.460 for exceptions and performance standards relating to impervious surface.
- (7) See LUC 20.20.030 for designation and measurement of setbacks.
- (8) See LUC 20.25H.035 for additional critical area setbacks.
- (9) See LUC 20.25H.045 for calculation of density/intensity on sites in the Critical Areas Overlay District.
- (10) Certain non-critical area setbacks on sites in the Critical Areas Overlay District may be modified pursuant to LUC 20.25H.040.
- (11) Maximum building height in the BR-ORT land use district shall be measured from average existing grade. See LUC 20.25D.130.D.4.d for additional transition edge development requirements.
- (12) The ground floor of a parking structure shall include Required Ground Floor Uses pursuant to LUC 20.25D.130.A.
- (13) Maximum building height west of 156th Avenue NE. Maximum building height located within 50 feet of the back of sidewalk along 156th Ave NE shall not exceed 45 feet as measured from the sidewalk grade adjacent to the building front.

Maximum building height located greater than 50 feet from the back of sidewalk along 156th Ave NE, shall not exceed 70 feet as measured from average existing grade.

- (14) Where building height exceeds 80 feet, all floors above 40 feet shall include a minimum side setback of 25 feet and a minimum rear setback of 25 feet as measured from the property line. Where building heights exceed 80 feet, a minimum separation of 50 feet for buildings on the same site shall be maintained for all floors above 40 feet. See paragraph B of this section for exceptions to this minimum setback/stepback dimensions.
- (15) Maximum façade lengths west of 156th Avenue NE. The façade of any single building fronting on 156th Ave NE shall not exceed a maximum length of 150 feet. All buildings or portions of buildings located above the sidewalk grade measured adjacent to 156th Ave NE shall include a minimum building separation of 40 feet. The required minimum separation shall provide in a continuous building separation corridor that extends from 156th Ave NE to Bel-Red Road.
- (16) Gross square feet per floor may be averaged for floor plates located above 40 feet.
- (17) For the purposes of determining tower type, hotels and motels shall be considered nonresidential.
- (18) For parking structure minimum setbacks refer to specific land use district.
- (19) All new development in the OR-1 and OR-2 land use districts shall provide a minimum of 20 percent of the total project gross square feet as residential use for development on any site of five acres or greater pursuant to a phasing plan, Part 20.30V LUC. Notwithstanding any other provisions of this code, a phased development is not required to provide the proportional minimum of residential use required per phase, provided that the 20 percent residential use requirement is met prior to occupancy of the final phase.
- (20) The 28,000 gsf/f above 40 feet applies only to towers that do not exceed 80 feet. For residential towers, the maximum gross square feet per floor above 40 feet and at or below 80 feet is 12,000 gsf/f. For residential towers the maximum gross square feet per floor above 80 feet is 9,000 gsf/f.
- (21) Denotes roof height above average finished grade.

B. Exceptions to Dimensional Requirements.

1. Floor Plate Exceptions.

- a. The Director may allow the connection of floor plates above 40 feet such that those floor plates exceed the gross square feet per floor above 40 feet consistent with the following limitations:

- i. The connection shall be to allow for safe and efficient building exiting patterns. The connecting floor area shall include required exiting corridor area and may include the area associated with dwelling units or other building uses;
 - ii. The connection shall occur on no more than three floor levels above 40 feet; and
 - iii. The alternative design shall result in a building mass that gives the appearance of separate and distinct building elements.
 - b. Gross square feet/floor above 80 feet may be increased to 10,000 gsf/f if all required affordable housing is provided within the building and not through payment of a fee in lieu.
- 2. Setback/Stepback Exceptions.
 - a. Marquees, awnings, or bay windows that comply with the requirements of the Bel-Red Subarea Design Guidelines adopted pursuant to LUC 20.25D.140 may be permitted to extend over the public right-of-way upon approval of the Director.
 - b. The Director may allow modifications to the requirements of Note 16 of Chart 20.25D.080.A consistent with the following limitations:
 - i. The applicant can demonstrate that the resulting design will be more consistent with the Bel-Red Subarea Design Guidelines adopted pursuant to LUC 20.25D.140; and
 - ii. The building design, with the modification, shall maintain a minimum spacing between towers of not less than 50 feet.
 - c. Minor building elements as defined pursuant to LUC 20.20.025.C may intrude into any setback or stepback required pursuant to LUC Chart 20.25D.080.A, provided that the applicant can demonstrate that the resulting design will be more consistent with the Bel-Red Subarea Design Guidelines adopted pursuant to LUC 20.25D.140.
- 3. Floor Area Ratio Exceptions.
 - a. Per the FAR Amenity Incentive System LUC 20.25D.150, floor areas dedicated to affordable housing, public restrooms, and child care/nonprofit uses shall not be counted for the purpose of calculating FAR.
 - b. Each square foot of ground floor retail and enclosed plaza meeting the criteria set forth below shall not be counted for the purpose of calculating FAR:

- i. Ground Floor Retail Uses: Ground floor retail uses as defined in LUC 20.25D.120.A shall not be counted provided the following limitations are met:
 - (1) The ground floor retail shall only be eligible for exception if located within a multi-story, multi-use building;
 - (2) The ground floor retail shall meet the requirements set forth in LUC 20.25D.120.A Bel-Red Subarea Design Standards applicable to retail uses;
 - (3) Outside the nodes the maximum depth of tenant space eligible for exception is 60 feet; and
 - (4) Inside the nodes ground floor retail shall not be counted.
- ii. Enclosed Plazas shall not be counted provided the following limitations are met:
 - (1) The enclosed plaza shall be clearly visible and accessible from the public right-of-way;
 - (2) The enclosed plaza shall coordinate with and complement ground floor retail uses to the maximum extent feasible;
 - (3) At least 10% of the surface area of the enclosed plaza shall be landscaped;
 - (4) The enclosed plaza shall contain at least one sitting space for each 100 sf of plaza;
 - (5) The enclosed plaza shall be a minimum size of 1,000 sf;
 - (6) Only 4,000 sf of the enclosed plaza shall be excepted for the purpose of calculating FAR;
 - (7) The enclosed plaza shall have a minimum horizontal dimension (width and depth) of no less than 20 feet;
 - (8) The enclosed plaza shall be signed as "Public Access" and open to the public from 7 a.m. to 9 p.m. daily or during business hours, whichever is longer. The sign for the enclosed plaza shall be visible from the public right-of-way; and
 - (9) Any use or feature for the exclusive use of the building users or tenants shall be counted for the purpose of calculating FAR.

C. Impervious Surface/Lot Coverage.

1. Underground buildings as defined in LUC 20.50.050 are not structures for the purpose of calculating floor area.
2. Buildings constructed partially below grade and not higher than 30 inches above existing or finished grade, whichever is lower, are not structures for the purpose of calculating lot coverage subject to the following conditions:
 - a. The 30-inch height limit shall be met at all points along the building excluding those areas necessary to provide reasonable ingress and egress to the underground portions of the building; and
 - b. The rooftop of any underground building shall be screened from abutting properties with 10 feet of Type II landscaping as described in LUC 20.20.520.G.2; except that the required trees shall be a minimum of 10 feet in height at planting. Alternatively, if a use is proposed for the rooftop, the rooftop may be landscaped consistent with the planting requirements for the specific use that is proposed and for the land use district in which the use is located. All landscaping shall comply with standards set forth in LUC 20.20.520.
3. Buildings constructed partially below grade and not higher than 30 inches above average finished grade are not structures for the purpose of calculating impervious surface; provided, that the rooftop of the building shall be landscaped consistent with the City of Bellevue's Utilities Department Engineering Standards, chapter D9, now or as hereafter amended, for the building roof area as approved by the Director..

D. Floor Area Ratio Earned from Special Dedications and Transfers.

1. General. Land that is dedicated to the City of Bellevue for right-of-way, parks, or open space, without compensation to the owner may be used for the purpose of computing maximum FAR notwithstanding the definition of floor area ratio in LUC 20.50.020; provided that the requirements of Paragraph D.2 or D.3 are met. Any dedication for which a transfer is earned under this LUC 20.25D.080.D is not eligible to earn additional bonus under LUC Chart 20.25D.090.C.2.
2. Right-of-Way Special Dedications. The special dedication provisions of LUC paragraph 20.30V.170.B do not apply in the Bel-Red subarea.
 - a. Eligible Right-of-Way Identified in an Adopted Plan or Document. A property owner may make a special dedication by conveying land identified for right-of-way acquisition in a Transportation Facilities Plan of the Comprehensive Plan, the Transportation Facilities Plan adopted by the City Council, the Capital Investment Program Plan or the street development standards of LUC 20.25D.130 by an instrument approved by the City Attorney.

- b. Eligible Right-of-Way Not Otherwise Identified. A property owner may make a special dedication by conveying land identified by the Director of Transportation as necessary for safety or operational improvement projects by an instrument approved by the City Attorney.
- c. Floor Area Earned. The floor area available to transfer shall be equal to that permitted through the base and maximum FAR limits that apply to the square footage of the area to be dedicated in conformance with this paragraph D.2 and shall be included in the project limit for the purpose of computing maximum FAR. The transfer FAR shall be calculated based on applicable land use district regulations for the dedication area and may include FAR earned through the amenity incentive system if permitted by the land use district.

3. Parks and Open Space Transfers.

- a. Eligible Park or Open Space identified within the Bel-Red Subarea Plan. A property owner may earn floor area for transfer to a different site by conveying land identified for park or open space in the Bel-Red Subarea Plan by an instrument approved by the City Attorney.
- b. Floor Area Earned. The floor area available to transfer shall be equal to that permitted through the base and maximum FAR limits that apply to the square footage of the area to be dedicated in conformance with paragraph D.3 and shall be added to the allowed floor area of the project for the purpose of computing maximum FAR. The transfer FAR shall be calculated based on applicable land use district regulations for the dedication area and may include FAR earned through the amenity incentive system if permitted by the land use district.

Dedication area in Square feet	X	Base FAR applicable to dedication area + FAR earned through amenity incentive system, not to exceed maximum FAR applicable to dedication area	=	Total Transferable Floor Area
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c. Use of Transferable Floor Area.

- i. Limitation on Location. Transferable floor area shall only be used in the following Bel-Red land use districts (MO-1, OR-1, OR-2, RC-1, RC-2, CR, and R).
- ii. Limitation on Transfer.

Transferable floor area shall not be used to achieve an effective site FAR of greater than 0.5 above the maximum FAR permitted for the land use district.

Hypothetical example: A 0.5 acre site, zoned CR, is dedicated to the city for park space. The CR zoning district has a base FAR of 1.0 and a maximum FAR of 2.0. Therefore, up to 21,780 sf (0.5 acres x 43,560 sf) of floor area could be transferred to a development site in Bel-Red at the base FAR, or up to 43,560 sq. ft. could be transferred if additional FAR is earned through participation in the amenity incentive system.

If the receiving site is 10 acres with a maximum FAR of 4.0, its allowed floor area, with participation in the amenity incentive system, would be 1,742,400 sf (10 acres x 43,560 sf x 4.0 FAR). By transferring additional floor area from the above sending site, the maximum floor area could be 1,785,960 sf (1,742,400 + 43,560 sf). The transferred area could not exceed an additional 0.5 FAR for the receiving site, or 217,800 sf.

4. Notwithstanding any provision of this Code, no transfer of floor area occurs when all property is included in one project limit

20.25D.090 FAR Amenity Incentive System.

A. General.

A development within a project limit may exceed the base FAR and base building height permitted pursuant to Chart 20.25D.080.A for development within a Bel-Red Land Use District only if it complies with the requirements of this section. In no case may the development within a project limit exceed the maximum floor area ratio permitted for the district except pursuant to LUC 20.25D.080.D. Each unit of measurement (square feet, linear feet, etc.) may only be used to gain one floor area ratio bonus, except where specifically provided otherwise. Any dedication for which a transfer is earned under LUC 20.25D.080.D is not eligible to earn additional bonus under this Section.

B. Required Review.

The Director may approve an amenity which complies with paragraph C below if:

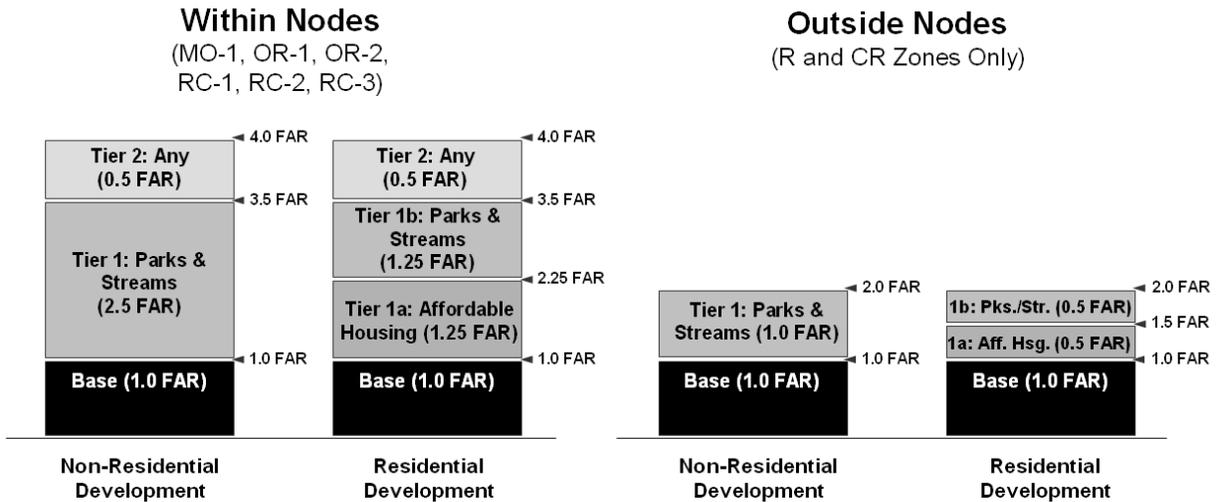
1. The design criteria established for the amenity have been met; and
2. A public benefit will be derived from the development of the proposed amenity in the proposed location.

C. Specific Requirements.

1. Participation in the FAR Amenity System shall comply with LUC Chart 20.25D.090.C FAR Amenity Incentive System provided below.

2. In a multi-building development within a single project limit, amenities may be allocated among all buildings within the project limit; provided, that such allocation shall be approved by the Director through a Master Development Plan. If construction of the multi-building development is to be phased, no phase may depend on the future construction of amenities.
3. Development within a project limit may only exceed its base FAR limit and base building height by providing FAR amenities as described in the LUC Chart 20.25D.090.C and this paragraph. To achieve the maximum FAR for the district, amenities from Tier 1 and Tier 2 must be provided at the specified ratio for every additional square foot of building area to be built beyond the base limit.
 - a. A development within a single project limit must first fully utilize Tier 1 amenity bonuses before using Tier 2 amenity bonuses.
 - b. A residential development or a residential portion of a development must utilize Tier 1 amenities in the following order:
 - i. Tier 1a, affordable housing as described in LUC Chart 20.25D.090.C.1 below; and
 - ii. Tier 1b, parks or stream restoration as described in LUC Chart 20.25D.090.C.2.
 - c. After fulfilling Tier 1, a development may utilize any of the amenity bonus types, whether from Tier 1 or 2.4. Tier 1 equals 2.5 FAR above the base in the MO-1, OR-1, OR-2, RC-1, RC-2, and RC-3 districts and Tier 1a and 1b each equal 1.25 FAR, where applicable.5. Tier 2 equals 0.5 FAR above Tier 1 in the MO-1, OR-1, OR-2, RC-1, RC-2, and RC-3 districts.
4. Tier 1 equals 1.0 FAR above the base in the CR and R districts and Tier 1a and 1b each equal 0.5 FAR, where applicable. No Tier 2 bonuses are permitted in the CR and R districts.

Example of Tiered FAR Amenity Allocation Figure 20.25D.090.C



Note: Tier 1 bonus(es) must be fulfilled prior to pursuing Tier 2 bonus. Where applicable, Tier 1a bonus must be fulfilled prior to pursuing Tier 1b bonus.

5. In-lieu fees shall be used to develop the amenity for which the in-lieu fee was paid to earn bonus FAR.
6. In-lieu fees shall be assessed and collected at building permit issuance.

7. Chart 20.25D.090.C FAR Amenity Incentive System

FAR AMENITY STANDARDS			
AMENITY (1)	APPLICABLE ZONES AND BONUS (3)		DESIGN CRITERIA
	MO-1, OR-1, OR-2, RC-1, RC-2, RC-3, CR, R		
TIER ONE			
<p>1. AFFORDABLE HOUSING* Threshold bonus for residential/ mixed-use development at up to 80% median income level for rental and up to 100% income level for ownership. (2) *Floor area shall not be counted for the purpose of calculating FAR.</p>	<p>Rental: 4.6 sf bonus building area per 1 sf of affordable rental housing 80% median income. Owner: 7.2 sf bonus building area per 1 sf of ownership affordable housing at 100% median income. Fee-in-lieu for Tier 1 residential: \$18 per sf bonus area. Fee-in-lieu for commercial and Tier 2: \$15 per sf bonus area.</p>	<p>Note: Delete this column</p>	<p>1. May be integrated into the same building as market rate housing, or in a stand-alone building on-site. 2. Design shall be generally consistent with associated market rate housing, provided that unit size, amenities, and interior finishes may vary from market units; and further provided that the bedroom mix and exterior finishes shall be comparable to the market rate units.</p>
<p>2. PARKS DEDICATION Dedication of land for park purposes that is provided by the developer consistent with the Bel-Red Subarea Plan generalized locations and size requirements.</p>	<p>3.0 sf bonus building area per 1 sf of parks dedication. Fee-in-lieu: \$15 per 1 sf bonus area.</p>	<p>Note: Delete this column</p>	<p>1. Bonused parks dedications shall be consistent with the Bel-Red Subarea Plan identified locations and sizes. 2. Bonused parks dedications do not need to be contiguous with the site for which development is proposed. 3. Parks dedications shall include FAR associated with the dedicated area and shall be provided in a form satisfactory to the city.</p>

<p>3. PARK IMPROVEMENTS Improvements made on private property to function as parks or improvements made to city-owned community, neighborhood, and mini-parks.</p>	<p>2.7 sf bonus building area per 1 sf of new park, (does not include land value). Fee-in-lieu \$15 per 1 sf bonus area.</p>	<p>Note: Delete this column</p>	<ol style="list-style-type: none"> 1. Improvements made on private property to function as bonused parks must be consistent with the Bel-Red Subarea Plan.. 2. Improvements made on private property to function as bonused parks do not need to be contiguous with the site for which development is proposed. 3. Improvements made on private property to function as bonused parks must be signed and function as part of the Bellevue parks system, including the ability to be programmed by the city. 4. Owners of private property improved to function as a park shall provide an easement in a form satisfactory to the city that allows for public access and maintenance. 5. Parks improvements are constructed by the developer consistent with applicable City plans.
<p>4. STREAM RESTORATION Stream restoration bonus to be applied for improvements above and beyond city's Critical Areas provisions, part 20.25H. Projects must in Bel-Red Subarea Plan, and may include removal of fish passage barriers, daylighting piped stream segments, restoration of natural streamside vegetation, and enhanced fish and wildlife habitat.</p>	<p>66.7 sf building area bonus per \$1,000 stream restoration, (does not include land value). Fee-in-lieu \$15 per 1 sf bonus area. Fee-in-lieu is not subject to the minimum 10,000 sf requirement.</p>		<ol style="list-style-type: none"> 1. A restoration plan shall be prepared by a qualified professional and approved by the city. 2. Must be coordinated with public trail system to the greatest extent possible. 3. Improvements and plans to support those improvements shall be in addition to that required by LUC 20.25H. Critical Areas Overlay District. 4. Minimum 10,000 sf. 5. May occur on-site or off-site. 6. Owner shall provide easement allowing City of Bellevue access for maintenance and monitoring.

TIER TWO

<p>5. CHILD CARE/NONPROFIT SPACE* Floor area dedicated to child care, non-profit groups whose purpose is to provide community or social services, or non-profit groups whose purpose is to provide arts/cultural uses.</p> <p>*Floor area shall not be counted for the purpose of calculating FAR</p>	<p>13.7 sf bonus building area per 1 sf of non-profit/community service space. Fee-in-lieu at \$15 per 1 sf bonus area (or higher rate).</p>	<p>Note: Delete this column</p>	<ol style="list-style-type: none"> 1. Childcare services must comply with the requirements of LUC 20.20.170. 2. Non-profit arts/cultural uses include art education, rehearsal, and performance, art production, and artist live/work space. 3. Space shall be used in manner described for the life of the project. 4. Documentation of nonprofit status shall be provided. 5. Bonused space may be used for other uses in this category with approval from the Director. 6. Director may approve a buy-out of space originally dedicated to child care/non-profit space at the prevailing fee-in-lieu rate if applicant shows good faith efforts to locate eligible tenant. If buy-out option is approved, covenant required in number 3 above shall be released.
<p>6. PUBLIC RESTROOMS* A room or rooms containing toilets and lavatories for the use of the general public, with only limited control for purposes of personal safety.</p> <p>*Floor area shall not be counted for the purpose of calculating FAR.</p>	<p>16.7 sf bonus building area per 1 sf of public restroom space.</p>	<p>Note: Delete this column</p>	<ol style="list-style-type: none"> 1. Shall be located on the ground level of the building. 2. Shall be open for use by the public during normal business hours. 3. Exterior of building shall be signed to identify location and public access of facility. 4. Maintenance of the facility is the obligation of the owner of that portion of the building within which the restroom is located for the life of the project.

<p>7. PUBLIC ART Sculpture, mural, water feature or other artwork that is located outside of or on a building and fully accessible to the general public. Includes building-related art treatments for awnings, doors, planters, etc., as well as fountains, cascades, and reflection ponds.</p>	<p>66.7 sf building area bonus per \$1,000 artwork.</p>	<p>Note: Delete this column</p>	<ol style="list-style-type: none"> 1. Shall be permanent and displayed outside of or on the building in areas open to the general public and/or any adjacent public right-of-way, perimeter sidewalk or pedestrian way. 2. May be an object or integrated feature of the building's exterior. 3. Art must be accepted by the Bellevue Arts Commission per the Public Art Selection Criteria. 4. Value of art to be determined through appraisal accepted by Bellevue Arts Program. 5. Maintenance of the art is the obligation of the owner of that portion of the site where the public art is located for the life of the project.
<p>8. PUBLIC ACCESS TO OUTDOOR PLAZA A continuous open space, predominantly open above, and designed predominantly for use by people as opposed to serving specifically as a setting for a building.</p>	<p>2.3 sf bonus building area per 1 sf of outdoor plaza.</p>	<p>Note: Delete this column</p>	<ol style="list-style-type: none"> 1. Shall abut and be within 3 ft in elevation of a perimeter sidewalk or pedestrian connection so as to be visually and physically accessible. 2. Shall provide protection from adverse wind, wherever practical. 3. At least 20% of the plaza surface area shall be landscaped. 4. Shall provide at least one sitting space for each 100 sf of plaza. 5. Shall be enclosed on at least two sides by a structure or by landscaping which creates a wall effect. 6. Minimum size is 1,500 sf 7. Maximum size is 15,000 sf 8. Minimum horizontal dimension is 20 ft. 9. Shall provide opportunities for penetration of sunlight in majority of plaza between 11am. and 2 p.m. 10. Shall not be used for parking, loading or vehicular access. 11. Shall be signed "Public Access" and open to the public from 7 a.m. to 9.p.m daily or during business hours, whichever is longer."

<p>9. LEED GOLD OR PLATINUM CERTIFICATION</p>	<p>0.13 FAR bonus for LEED Gold 0.33 for LEED Platinum.</p>	<p>Note: Delete this column</p>	<p>1. Building shall meet minimum criteria for LEED certification in chosen category. 2. A performance bond equivalent to the value of the bonus shall be provided to the city by the developer. In the event the project does not achieve the planned rating, all or part of the money shall be used for environmental improvements identified by the city.</p>
<p>10. ACTIVE RECREATION AREA* An area which provides active recreational facilities for tenants of the development of which it is a part and for the general public. Does not include health or athletic clubs. *Floor area shall not be counted for the purpose of calculating FAR.</p>	<p>9.7 sf bonus building area per 1 sf of active recreation area. 66.7 sf building area bonus per \$1,000 active recreation improvement (does not include land value).</p>	<p>Note: Delete this column</p>	<p>1. May not be used for parking or storage. 2. May be located out of doors, on top of, or within a structure. 3. Recreational facilities include, but are not limited to, sport courts, child play areas, and exercise rooms. 4. May be fee-for-use but not exclusively by membership.</p>
<p>11. NATURAL DRAINAGE FEATURES Low impact development techniques that improve natural drainage features such as rain gardens, pervious pavement, vegetated roof, and amended soils.</p>	<p>0.7 bonus building area per 1 sf of effective natural drainage feature.</p>	<p>Note: Delete this column</p>	<p>1. Shall meet criteria of the Bellevue Natural Drainage Practices Manual – fact sheet U1-U5. 2. Underlying soil condition and infiltration rate must be appropriate for the feature. 3. Requirement for large storm events as determined by Bellevue Utilities Department shall be met. 4. Maintenance of the natural drainage features is the obligation of the property owner for the life of the project.</p>
<p>12. REGIONAL TRANSFER OF DEVELOPMENT RIGHTS (TDRs) Transfer of development rights to designated Bel-Red areas that achieve conservation of rural resource lands outside the Countywide Urban Growth Boundary.</p>	<p>(Reserved for future updates)</p>	<p>Note: Delete this column</p>	<p>(Reserved)</p>

Notes: Chart 20.25D.090.C FAR Amenity Incentive System

- (1) Measured in square feet of permitted development for each qualifying developed square foot of amenity unless otherwise noted.

- (2) An agreement in a form approved by the city must be recorded with King County Department of Records and Elections requiring affordable housing square footage that is provided under this section to remain affordable housing for the life of the project. This agreement shall be a covenant running with the land, binding on the assigns, heirs, and successors of the applicant.
- (3) Where a bonus is earned by payment of a fee-in-lieu, the fee-in-lieu amount established in Chart 20.25D.090.C as of [insert Plan adoption date] will be reviewed annually, and, effective January 1 of each year, may be administratively increased or decreased by an adjustment to reflect the current published annual change in the Seattle Consumer Price Index for Wager Earners and Clerical Workers as needed in order to maintain accurate construction costs for the region.

20.25D.100 Automobile and Motorcycle Sales, Leasing, and Rental.

The following decision criteria, in addition to the criteria in LUC 20.30E.140, apply to an Administrative Conditional Use Permit application for the leasing, rental, and/or retail sale of automobiles and/or motorcycles in the BR land use districts:

1. The applicant shall provide adequate off-street unloading area for vehicle carriers;
2. Limitations on Outside Storage and Displays
 - a. Inside Nodes: No outside storage or display is permitted between the building and public right-of-way designated for required sidewalk-oriented development (refer to LUC 20.25D.130.C);
 - b. Properties abutting the north side of Bel-Red Road: No outside storage or display is permitted between the building and Bel-Red Road; and
3. The provisions of LUC 20.20.135 shall not apply within Bel-Red.

20.25D.110 Landscape Development, Outdoor Storage, Retail Display, and Fence Standards.

A. General.

1. Applicability. The provisions of LUC 20.20.520 Paragraphs A, D, E, G, I, J, K, and L apply to development in the BR Land Use Districts in addition to the provisions contained in this section.
2. Review Required. The Director shall review the proposed landscape development, outdoor storage, retail display, and fencing and may approve a proposed structure, alteration, site development, use, or occupancy only if the requirements of this section are met, subject to the provisions of LUC 20.25D.060 for existing conditions.

B. Street Frontage Landscape Development Requirements.

- 1. Purpose/Intent.** Landscape development, including retention of significant trees, as required by this section is necessary to maintain and protect property values, to enhance the visual appearance of the Bel-Red Subarea, to preserve the natural wooded character of the Pacific Northwest, to promote utilization of natural systems, to reduce the impacts of development on the storm drainage system and water resources, to provide a better transition between the various land use districts in the Bel-Red Subarea and to enhance the pedestrian environment.

- 2. Where Required.** The following street frontage landscape development requirements apply, except where the development is regulated by LUC 20.25D.130.B or C. See Figure 20.25D.110, Required Landscape Treatments for the streets mentioned below.

Required Landscape Treatments

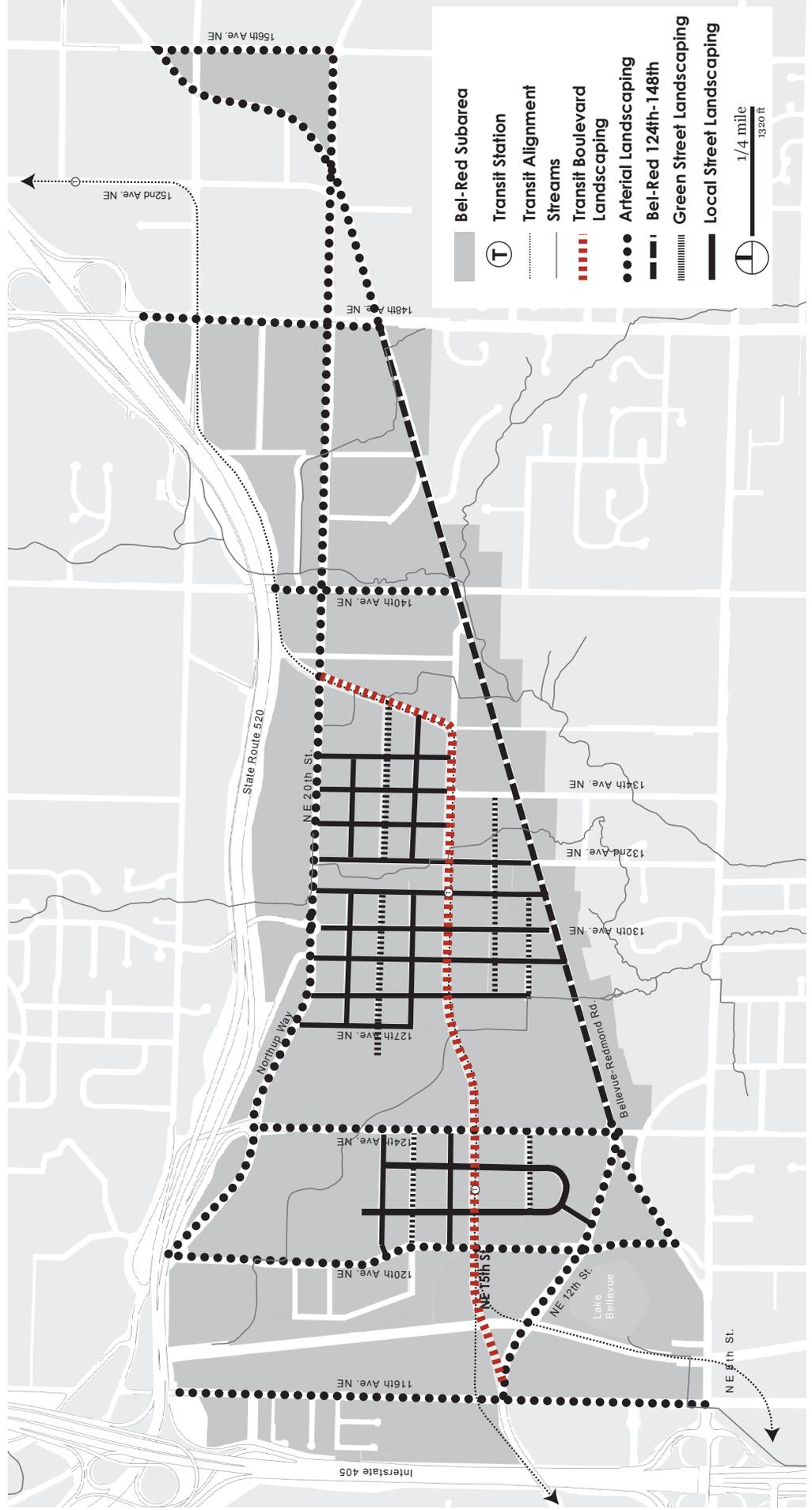


Figure 20.25D.110: Required Landscape Treatments

3. Applicable Standards.

- a. Transit Boulevard Landscaping: NE15th/16th Street:
 - i. Corridor Design approval through Transportation Department.
- b. 130th Avenue Shopping Street north of NE 15th/16th:
 - i. Corridor design approval through Transportation Department.
 - ii. Dimensions: Sidewalk: 9 ft
Planting: 5 ft
- c. Bel-Red Road from 124th to 148th
 - i. Dimensions: Sidewalk: 8 ft
Planting: 20 ft
 - ii. On both sides of Bel-Red Road the required landscaping shall be provided except where a plaza or gateway meeting the requirements of the Bel-Red Design Guidelines, LUC 20.25S.150, abut the sidewalk.
 - iii. The sidewalk may meander into the planting area only where necessary to retain significant trees. In such case, the overall dimension of sidewalk and sidewalk planting shall total 28 feet.
 - iv. Planting
 - (1) All significant trees shall be maintained between the street and existing or proposed development pursuant to Paragraph A above and each of the following standards shall be met:
 - (2) A minimum of five trees (evergreen and deciduous) per 1,000 sf of planting area shall be provided. No more than 40 percent shall be deciduous. Evergreen trees shall be a minimum height of 10 feet at planting. Deciduous trees shall have a minimum caliper of 2-1/2 inches.
 - (3) Evergreen shrubs shall be provided at a minimum spacing of three feet on center; shrubs shall be a minimum height of 42 inches at planting.
 - (4) Living groundcover shall be provided as necessary to cover the landscape area within a minimum of three years.
 - (5) At least 50 percent of the plantings shall be native species.
- d. Arterial Landscaping includes 116th Avenue NE, 120th Avenue NE, 130th Avenue NE south of NE 15th/16th, 148th Avenue NE, 156th Avenue NE, Bel-Red Rd east of 148th Avenue NE
 - i. Dimensions: Sidewalk 6 ft
Planting 5 ft
 - ii. Street Trees:

(1) Street trees with a minimum caliper of 2-1/2 inches shall be provided at a maximum spacing of 30 feet on center in tree wells a minimum of five feet wide and 10 feet long.

(2) Summit Ash - *Fraxinus pennsylvanica* "Summit". The Summit Ash trees will be planted symmetrically (from mid-block outwards to end of block).

(3) Greenspire Linden - *Tilia cordata* "Greenspire" located at the end of block.

iii. Tree Well Groundcover Plantings: Groundcover shall be provided as necessary to cover the landscape area within a minimum of three years.

(1) Mondo Grass

(2) Creeping Lily Turf

(3) Lemon Daylily - *Hemerocallis flava*

(4) Salal - *Gaultheria shallon*

e. Local Street Landscaping

i. Dimensions: Sidewalk: 6 ft

Planting: 5 ft

ii. Street Tree Planting List. Street trees shall have a minimum 2-1/2-inch caliper. The trees shall be planted symmetrically (from mid-block outwards to end of block).

(1) Village Green Zelkova - *Zelkova serrulata* "Village Green"

(2) Greenspire Linden - *Tilia cordata* "Greenspire"

iii. Tree Well Groundcover Plantings.

(1) Lemon Daylily

(2) Kinnickinnick - *Arctostaphylos uva-ursi*

f. Green Street Landscaping

i. Dimensions: Sidewalk: 6 ft

Plantings: 6 ft

ii. Stormwater planters pursuant to Directors Administrative Rule guidelines for Low Impact Development.

iii Street Trees:

(1) Street trees with a minimum caliper of 2-1/2 inches shall be provided at a maximum spacing of 30 feet on center

(2) Black Tupelo *Nyssa sylvatica*

(3) American Hornbeam *Carpinus caroliniana*

(4) Eastern Redbud *Cercis canadensis*

(5) Dacquemontii Birch *Betula jacquemontii*

C. Perimeter Landscape Development for land use districts

- 1. Purpose Intent.** Landscape development, including retention of significant trees, as required by this section is necessary to create visual separation between different land use districts.
- 2. Where Required.** A 20 foot landscape buffer shall be provided along the interior property line of a district abutting BR-R and BR-ORT Land Use Districts.
- 3. Applicable Standards.**
 - a. Evergreen and deciduous trees shall be provided at a maximum spacing of 20-feet on center. No more than 30 percent shall be deciduous. Trees shall be a minimum height of 10 feet at planting.
 - b. Evergreen shrubs shall be provided at a minimum spacing of three feet on center. Shrubs shall be a minimum of two-gallon in size at planting.
 - c. Living ground cover shall be provided as necessary to cover the entire remaining area within a minimum of three years.
 - d. No portion may be paved except for vehicular entrance drives and required trails or other pedestrian connections, and these features should be minimized to the extent feasible.

D. Interior Property Line Development

- 1. Purpose/Intent.** The landscape development required by this section is necessary to provide visual separation of uses so as to soften the appearance of parking areas and building elevations.
- 2. Where Required.** A 10 foot landscape buffer shall be provided along an interior property not regulated elsewhere.
- 3. Applicable Standard.**
 - a. Evergreen and deciduous trees, with no more than 50 percent being deciduous, a minimum of six feet in height, and planted at intervals no greater than 30 feet on center; and
 - b. If planted to buffer a building elevation, shrubs, a minimum of three and one-half feet in height, and living ground cover planted so that the ground will be covered within three years; or
 - c. If planted to buffer a parking area, access, or site development other than a building, any of the following alternatives may be used unless otherwise noted:

- i. Shrubs, a minimum of three and one-half feet in height and living ground cover must be planted so that the ground will be covered within three years.
- ii. Earth-mounding, an average of three and one-half feet in height, planted with shrubs or living ground cover so that the ground will be covered within three years. This alternative may not be used in a Downtown Land Use District.
- iii. A combination of earth-mounding and shrubs to produce a visual barrier at least three and one-half feet in height.

E. Curb Extension Planting.

1. Purpose/Intent. Landscape development as required by this section is necessary to enhance the visual appearance of the Bel-Red Subarea, to reduce the impacts of development on the storm drainage system and water resources to enhance the pedestrian environment in the Bel-Red Subarea.

2. Where Required: Refer to Transportation Department Development Standards for curb extension design standards and generalized locations.

a. Type I. Curb Extension Planting – refer to symbol on map

b. Type II. Curb Extension Planting – refer to symbol on map

3. Applicable Standard.

a. General Standards applicable to all curb extension planting types

- i. Groundcover and shrubs shall be provided as necessary to cover the landscape area width within three years.
- ii. Shrubs shall be a minimum of one gallon at the time of planting to cover the landscape area within a minimum of three years. li Shrubs. A combination of the following shrubs is required at any curb extension unless modified below. All specified shrubs shall be a minimum of one gallon at the time of planting.

- (1) Tall Oregon Grape - *Mahonia aquafolium*
- (2) Creeping Oregon Grape - *Mahonia repens*
- (3) Salal - *Gaultheria shallon*
- (4) Golden Princess Spiraea - *Spiraea japonica*
“Golden Princess”
- (5) Western Swordfern - *Polystichum munitum*
- (6) Lemon Daylily - *Hemerocallis flava*
- (7) David Viburnum - *Viburnum davidii*

b. Type I Curb Extension Planting.

- i. Trees

- (1) Vine Maple - *Acer circinatum* with 2-3 trees with a minimum 3/4 inch caliper planted within each curb extension.
- (2) *Styrax japonicus* "Japanese snowdrop"
- (3) *Cercidiphyllum japonicum* "Katsura"
- (4) *Cercis canadensis alba* "Redbud"

c. Type II Curb Extension Planting

i. Trees

- (1) Minimum 2-inch caliper
- (2) Coral Bark Maple - *Acer Palmatum* "Sango-kaku"
- (3) Dogwood *cornus kousa*
- (4) Lemon Daylily - *Hemerocallis flava*
- (5) David Viburnum - *Viburnum davidii*

F. Significant Tree Retention and Pruning.

Tree retention requirements of LUC 20.20.900 shall apply in addition to the requirements set forth below.

1. In the landscape areas required pursuant to paragraphs B and C above, all significant trees shall be retained that do not constitute a safety hazard as determined by the Director and consistent with the guidelines of the International Society of Arboriculture.
2. Select Tree Pruning. Pruning of existing trees within the 20-foot wide landscape buffer on the north and south sides of Bel-Red Road shall be performed in accordance with guidelines established by the Director for each of the following pruning techniques: canopy reduction; canopy cleaning; canopy thinning; canopy raising or lifting; structural pruning; and canopy restoration. Pruning shall be performed in a manner that ensures continued survival of the vegetation.
3. Pruning or removal of significant trees within Parks and Community Services easements is prohibited except as performed by the City of Bellevue if restricted by the terms of an easement.

G. Screening of Retail Display, Parking Areas, Vehicular Access, and Outdoor Storage Associated with Manufacturing Uses.

Between the sidewalk and the subject property 10 feet of Type III landscaping is required to screen a surface vehicular access, parking area, new retail display, or new outdoor storage associated with an existing or permitted manufacturing use. An alternative design may be approved through Alternative Landscape Option, LUC 20.20.520.J.

H. Fences.

1. No fence shall be permitted to violate the sight obstruction restrictions at street intersections. (See BCC 14.60.240 now or as hereafter amended.)
2. Any fence which exceeds eight feet in height requires a building permit and shall conform to the International Building Code, as adopted by the City of Bellevue now or as subsequently amended or superseded.
3. Height shall be measured from finished grade at the exterior side of the fence. No person shall construct a berm upon which to build a fence unless the total height of the berm plus the fence does not exceed the maximum height allowable for the fence if the berm was not present.
4. Prohibited Fences. The following types of fences are prohibited:
 - a. Barbed wire.
 - b. Electric fences.
 - c. Chain link fences are not permitted on any street frontage in any land use district except as follows:
 - i. To secure a construction site or area during the period of construction, site alteration, or other modification;
 - ii. In connection with any approved temporary or special event use; or
 - iii. As a component of an existing development pursuant to LUC 20.25D.060.

20.25D.120 Parking, Circulation, and Internal Walkway Requirements.

A. General.

1. General. The provisions of LUC 20.20.590 Paragraphs C, E, G, H, I, and K apply to development in the BR Land Use Districts in addition to the provisions contained in this section.
2. Review Required. The Director shall review the proposed parking, circulation, and walkways and may approve the proposed structure, alteration, site development, use, or occupancy only if the requirements of this section are met, subject to the provisions of LUC 20.25D.060 for existing conditions.

B. Minimum/Maximum Parking Requirements by Use – Specified Uses.

1. Number of Parking Stalls. The requirements of this section for the number of parking stalls apply to each new use and to each new tenant.

2. Parking Standards for Bel-Red - Chart 20.25D.120.B.2

Chart 20.25D.120.B.2 - PARKING STANDARDS FOR BEL-RED (6) (7)					
Use	Unit of Measure	MO-1, OR-1, OR-2, RC-1, RC-2, RC-3		MO, RC, CR, GC, R, ORT	
		Min.	Max.	Min.	Max.
a. Financial institution	Per 1,000 nsf	2.0	3.0/3.5 (1)	3.0	4.0
b. Manufacturing/assembly	Per 1,000 nsf	1.0	2.0	2.0	4.0
c. Home furnishing-retail and major appliances-retail	Per 1,000 nsf	1.5	3.0	1.5	3.0
d. Manufacturing/assembly (other than high technology/light industry)	Per 1,000 nsf	1.0	1.5	1.5	2.0
e. Office: Business services/professional services/general office	Per 1,000 nsf	2.0	3.0/3.5 (1)	3.0	4.0
f. Office: Medical/dental/health related services	Per 1,000 nsf	3.5	4.0/4.5 (1)	4.0	5.0
g. Residential (5)	Per unit	0.75	2.0	1.0	2.0
h. Restaurant and bar (3)	Per 1,000 nsf	5.0 (4)	15.0	10.0	20.0
i. Retail, personal service, shopping center	Per 1,000 nsf	2.5 (4)	4.5	3.0	5.0
j. Retail and personal service in mixed-use development (2,3)	Per 1,000 nsf	2.0	3.5	3.0	4.5
k. Senior housing: Nursing home	Per patient bed	0.25	0.75	0.25	1.0
l. Senior housing: Senior citizen dwelling or congregate care	Per living unit	0.25	1.0	0.5	1.25
m. Wholesale, warehouse	Per 1,000 nsf	1.5	2.0	1.5	No max.

Notes applicable to parking standards for Bel-Red (Chart 20.25D.120.B.2):

- (1) The maximum parking ratio for financial institutions and office uses in LUC Chart 20.25D.120.B.2.a and e may be increased from 3.0 to 3.5 per 1,000 nsf and in Chart 20.25D.120.B.2.f from 4.0 to 4.5 per 1,000 nsf as follows:
 - (a) For off-site parking, the additional 0.5 per 1,000 nsf increment shall be provided in an interim surface parking configuration no more than 500 feet away from the site. The interim parking will have a sunset clause of ten (10) years, or such other period approved through a phasing plan, Part 20.30V; or
 - (b) For on-site parking, the additional 0.5 per 1,000 nsf increment may be constructed on-site if it is part of an approved phasing plan, Part 20.30V LUC, and dedicated for a portion of the parking requirement for a future phase of the project.
- (2) If retail and personal service space in a mixed-use development exceeds 25 percent of the net square footage of the development, the retail, personal service, shopping center parking requirements in Chart 20.25D.120.B.2.i apply to the entire retail and personal service space.
- (3) If restaurant and/or bar uses exceed 25 percent of the total net square footage of a retail, shopping center, or mixed-use development, the restaurant and bar requirements in Chart 20.25D.120.B.2.h apply to the entire restaurant and/or bar space.
- (4) Inside nodes, no parking is required for retail and restaurant and/or bar uses under 2,000 nsf when the use is: directly adjacent to a public on-street parking supply of at least 20 spaces within 500 feet, or within 1,000 feet of a public parking garage, or within 500 feet of a light rail or bus rapid transit station.
- (5) The minimum requirement for up to and including one bedroom apartment units available to persons earning 60 percent or less than the median income as determined by the United States Department of Housing and Urban Development for the Seattle Metropolitan Statistical Area is 0.25 stalls per unit. An agreement to restrict the rental or sale of any such units to an individual earning 60 percent or less of the median income shall be recorded with King County.
- (6) Vanpool/Carpool Facilities. The applicant shall provide a vanpool/carpool loading facility that is outside of required driveway or parking aisle widths and that is contained within the required parking and circulation areas. The facility shall be adjacent to an entrance door to the structure served by the parking, or as nearly so as possible after barrier free access parking has been provided, and shall be consistent with all applicable design guidelines.
- (7) Tandem/stacked parking stalls. The applicant may use tandem/stacked parking stalls to exceed the minimum parking requirement, so long as the maximum parking requirement is not exceeded.

C. Off-Site Accessory Parking.

The Director may approve off-site accessory parking to meet the minimum and up to the maximum parking allowed to serve a specific use if the following criteria are met.

1. Adequate visitor parking is provided on the subject property;
2. Adequate pedestrian, van, or shuttle connections between the sites exists;
3. On-site signage is provided regarding accessory parking location;
4. The use being served is within 500 ft of a light rail, bus, or bus transit station; and
5. The off-site parking is within ¼ mile of the use being served.

D. Parking Structure Performance Standards.

The Director may approve a proposal for a parking structure through Design Review if the following criteria are met:

1. Driveway openings are limited to those needed to adequately serve the facility;
2. Exposed parking on the roof of a structure shall be screened by a parapet or other solid screening that equals or exceeds the height of the vehicles;
3. Safe pedestrian connection between the parking structure and the public right-of-way exists;
4. Unfinished ceilings visible from the public right-of-way shall be substantially screened from view; and
5. Lighting shall utilize cut-off shields to prevent spillover upon adjacent uses and the right-of-way and to conceal the light source.

E. Phased Parking.

The property owner may install the required parking spaces in phases pursuant to a phasing plan, Part 20.30V LUC. Each phased parking installation must include the approved minimum to meet the parking requirements for the completed phases of the development for which the parking is provided. The phasing schedule must specifically indicate when all parking approved pursuant to this section will be provided.

1. Location. Phased parking may be located off -site if the criteria of paragraph C above are met.

2. Assurance Device. The Director may require an assurance device pursuant to LUC 20.40.490 to ensure compliance with the requirement and intent of paragraph C above.

F. Director's Authority to Modify Required Parking.

1. The Director may modify the minimum or maximum parking ratio for any use in LUC 20.25D.120.B as follows:
 - a. The modified parking ratio is supported by a parking demand analysis including but not limited to:
 - i. Documentation supplied by the applicant regarding actual parking demand for the proposed use; or
 - ii. Evidence in available planning and technical studies relating to the proposed use; or
 - iii. Required parking for the proposed use as determined by other comparable jurisdictions.
 - b. The proposal does not result in any adverse impact beyond the site; and
 - c. A shared parking agreement is executed pursuant to 20.20.590.I. Use of Shared Parking.
2. Periodic Review. The Director may require periodic review of the reduced parking supply to ensure the terms of the approval are being met.
3. Assurance Device. The Director may require an assurance device pursuant to LUC 20.40.490 to insure compliance with the requirement and intent of paragraph F.1 of this section.

G. Bicycle Parking.

Office, residential, institutional, retail, and education uses are required to provide bicycle parking pursuant to the following standards:

1. Ratio.
 - a. 1 space per 10,000 nsf for non-residential uses greater than 20,000 nsf.
 - b. 1 space per every 10 dwelling units for residential uses.
2. Location. Minimum bicycle parking requirement shall be provided on-site.
3. Covered spaces. At least 50 percent of required parking shall be protected from rainfall by cover.

4. Racks. The rack(s) shall be securely anchored and a bicycle six feet long can be securely held with its frame supported so the bicycle cannot be pushed or fall in a manner that will damage the wheels or components.
5. Size requirement. Each required bicycle parking space shall be accessible without moving another bicycle.

H. Unspecified Uses.

The Director shall establish the minimum number of parking spaces required and may establish the maximum number of parking spaces allowed for any use not specified in LUC 20.25D.120.B. The Director may consider but is not limited to the following in establishing parking requirements for an unspecified use:

1. Documentation supplied by the applicant regarding actual parking demand for the proposed use; or
2. Evidence in available planning and technical studies relating to the proposed use; or
3. Required parking for the proposed use as determined by other comparable jurisdictions.

20.25D.130 Bel-Red Development Standards.

The Bel-Red Development Standards establish regulatory requirements for new structures, development, or activities and remodels or expansions on public and private parcels within the Bel-Red Subarea. These standards support and strengthen existing and planned infrastructure while establishing a hierarchy of use and a highly identifiable Subarea character.

A. Required Ground Floor Uses.

1. Purpose/Intent. Ground floor retail and commercial uses are an essential component of active and vital streets as well as transit station areas. Ground floor uses create a vibrant “18-hour” pedestrian environment where neighborhood services are within an easy walk, bike or transit trip.
2. Where Required. Figure 20.25D.130.A identifies those streets where ground floor retail and ground floor commercial uses are required for buildings frontages.
3. Applicable Standards for Ground Floor Retail Uses.
 - a. Ground floor retail uses shall satisfy the intent of paragraph 1 above and include uses such as:
 - i. Eating and drinking establishments;

- ii. Merchandise sales (including but not limited to grocery, food retail, art, and clothing); or
 - iii. Personal services (including but not limited to laundry and beauty services).
 - b. Continuous retail storefronts shall be provided for 100 percent of the building frontage on a designated street.
 - c. Interruptions in storefronts shall be limited to residential lobbies and required emergency access. Lobby interruptions shall be limited to 25 percent of the building frontage.
 - d. All other uses are prohibited on the ground floor.
4. Applicable Standards for Ground Floor Commercial Uses.
- a. Ground floor commercial uses shall satisfy the intent of paragraph 1 above and include all ground floor retail uses permitted in paragraph A.3 of this section, and financial, real estate, insurance services, and hotels.
 - b. In the 122nd Avenue Node, at least 50 percent of the street level building edges shall incorporate ground floor commercial uses. The required ground floor commercial uses may be interrupted by lobby entrances to office, hotel or residential buildings and required emergency access. Lobby interruptions shall be limited to 25 percent of the building frontage.
 - c. In the 130th Avenue Node, at least 100 percent of the street level building edges shall incorporate ground floor commercial uses. The required ground floor commercial uses may be interrupted by residential lobbies or work-live units. Lobby interruptions shall be limited to 25 percent of the building frontage.

Required Ground Floor Uses

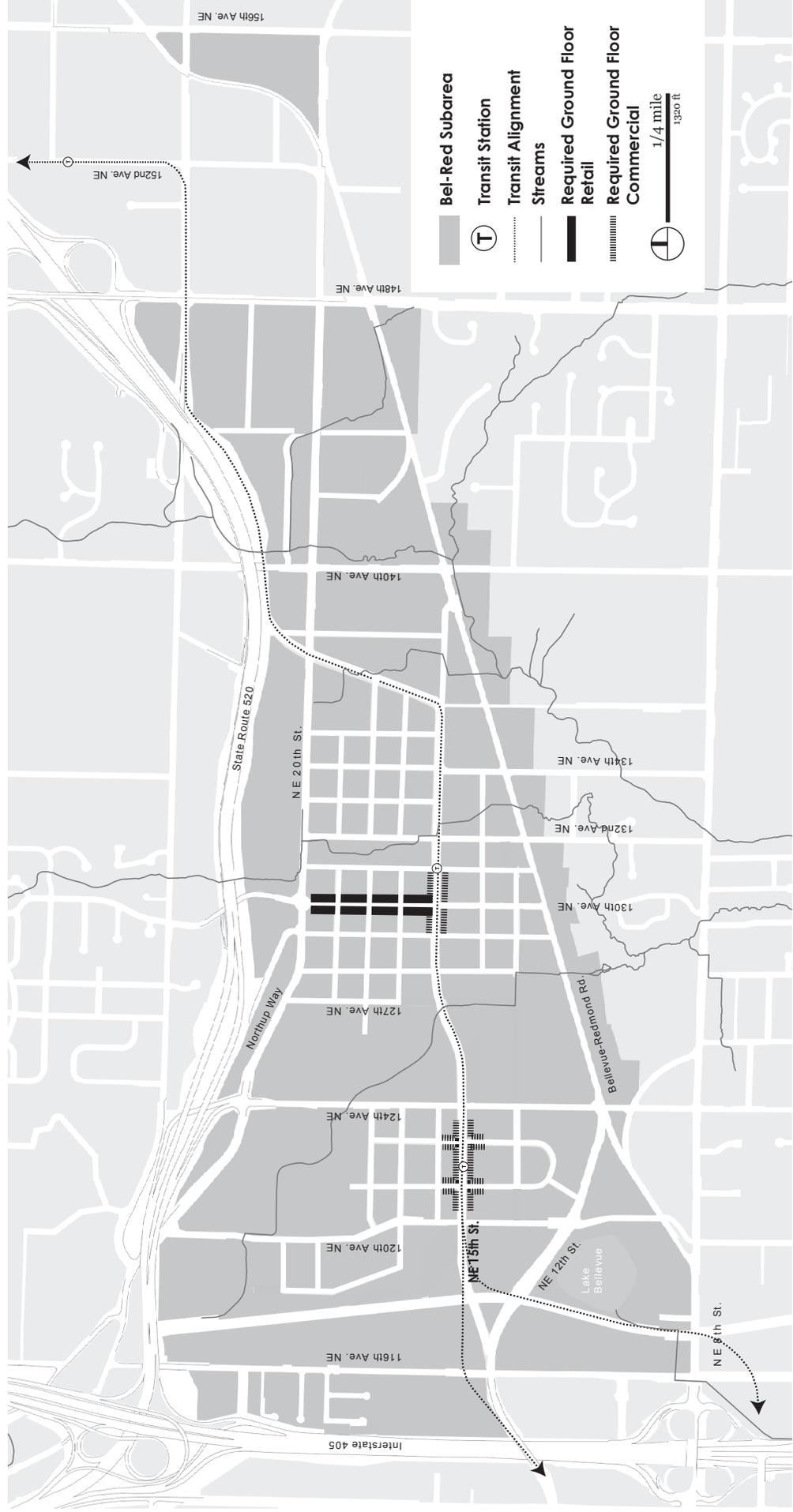


Figure 20.25D.130.A: Required Ground Floor Uses

B. Required Build-to Lines.

1. Purpose/Intent. Build-to lines occur along select block faces to help establish a continuous “street wall” providing a sense of enclosure and visual interest for pedestrians. Build-to-lines also contribute to the economic viability of retail and commercial uses by providing direct pedestrian access and visual exposure to potential drive-by customers.
2. Where Required. Figure 20.25D.130.B identifies locations of required build-to lines where the ground-floor facades must be built to the back of sidewalk.
3. Applicable Standards for required build-to lines.
 - a. Except as provided in paragraphs d, e, and f below, buildings shall satisfy the intent of paragraph 1 above and be constructed along the entire block length where indicated in Figure 20.25D.130.B .
 - b. Entrances to buildings may be recessed a maximum of five feet behind the build-to line.
 - c. Windows and walls may be recessed up to 18 inches if columns, pilasters, walls or other architectural fenestration tie them together with the rest of the building frontage.
 - d. In the 122nd Avenue Node, required build-to lines may be interrupted only to accommodate public or private plazas, parks, or other publicly accessible spaces. Interruptions in the required build-to lines shall be limited to a maximum of 25 percent of the total project frontage on an individual block.
 - e. In the 130th Avenue Node, required build-to lines may be interrupted only to accommodate public or private plazas, parks, or other publicly accessible spaces. Interruptions in the required build-to lines shall be limited to 15 percent of the total project frontage on an individual block.
 - f. Vehicular access is limited pursuant to LUC section 20.25D.140.F regarding restricted driveway access.
 - g. Surface parking is not permitted between the sidewalk and the building wall.

Required Build-to-Lines

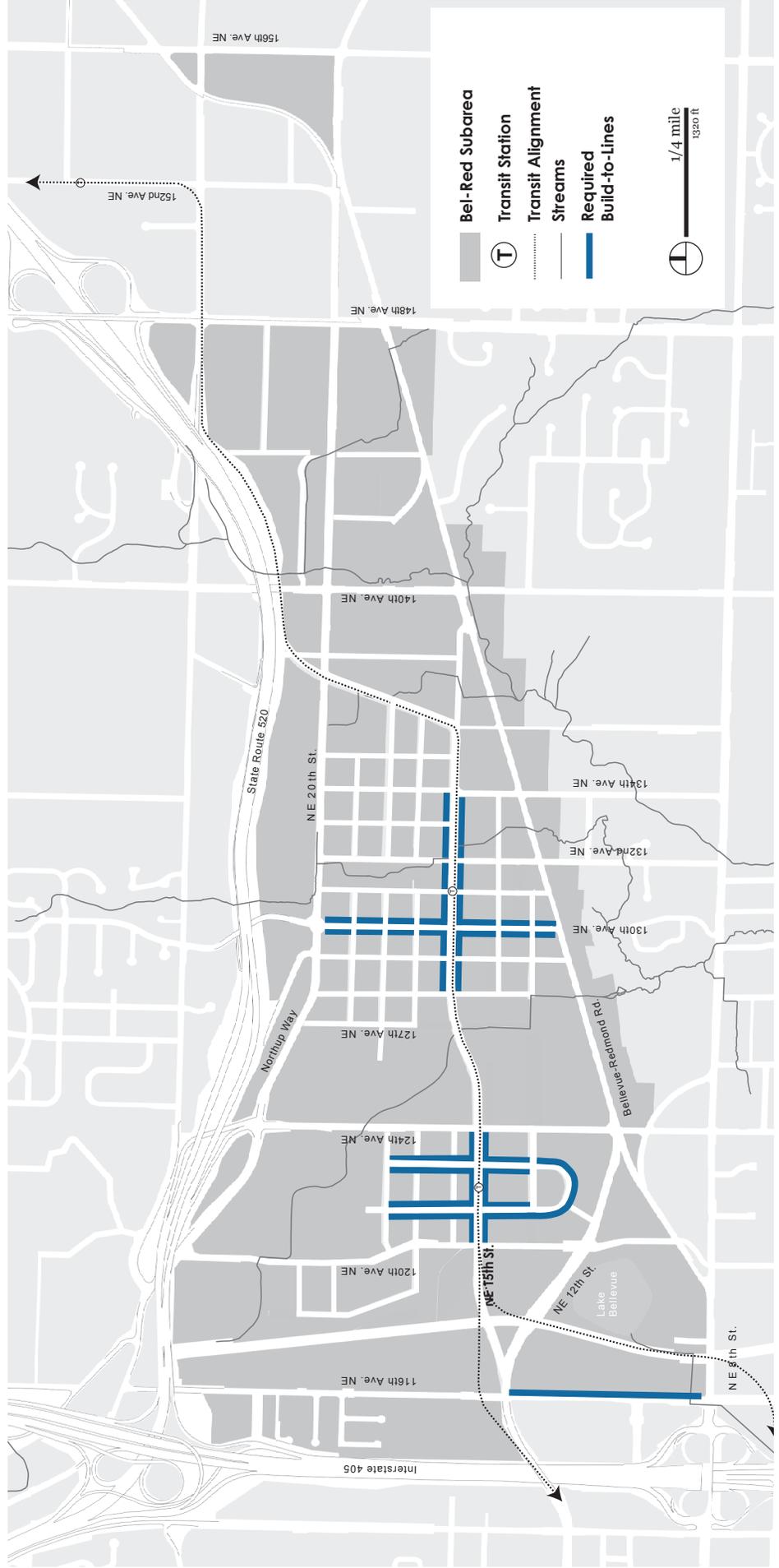


Figure 20.25D.130.B: Required Build-to-Lines

C. Required Sidewalk-Oriented Development

1. Purpose/Intent. Sidewalk-oriented development is characterized as ground floor building frontages with direct entries from the sidewalk and/or building frontages with a high degree of transparency and activating land use. This increased visual and physical interaction between the people inside and outside the buildings results in “eyes on the street” while creating a greater degree of safety and a vibrant public realm.
2. Where Required. Figure 20.25D.130.C identifies where sidewalk-oriented development is required.
3. Applicable Standards for Ground Floor Retail and Commercial Uses.
 - a. On street frontages identified in Figure 20.25A.130.A, a minimum of 100 percent of ground floor facades shall be transparent glass or screens that satisfy the intent of paragraph 1 above. Tinted, reflective, or other types of glass or window treatments that diminish transparency are prohibited.
 - b. Interruptions in the transparent glass or screens shall be limited to a maximum of 30 percent of the sidewalk-oriented development.
 - c. Primary entrances to all ground floor uses shall be oriented to the public right-of-way. Doors shall not be separated from adjacent public sidewalks by steps or ramps except where no feasible alternative exists.
4. Applicable Standards for Other Uses.
 - a. Primary entrances shall be oriented toward the public right-of-way or open space.
 - b. Doors may be separated from adjacent public right-of-way or open space by a maximum of 36 inches grade separation.
 - c. A minimum of 40 percent of ground floor facades shall be transparent glass or screens.

Required Sidewalk-Oriented Development

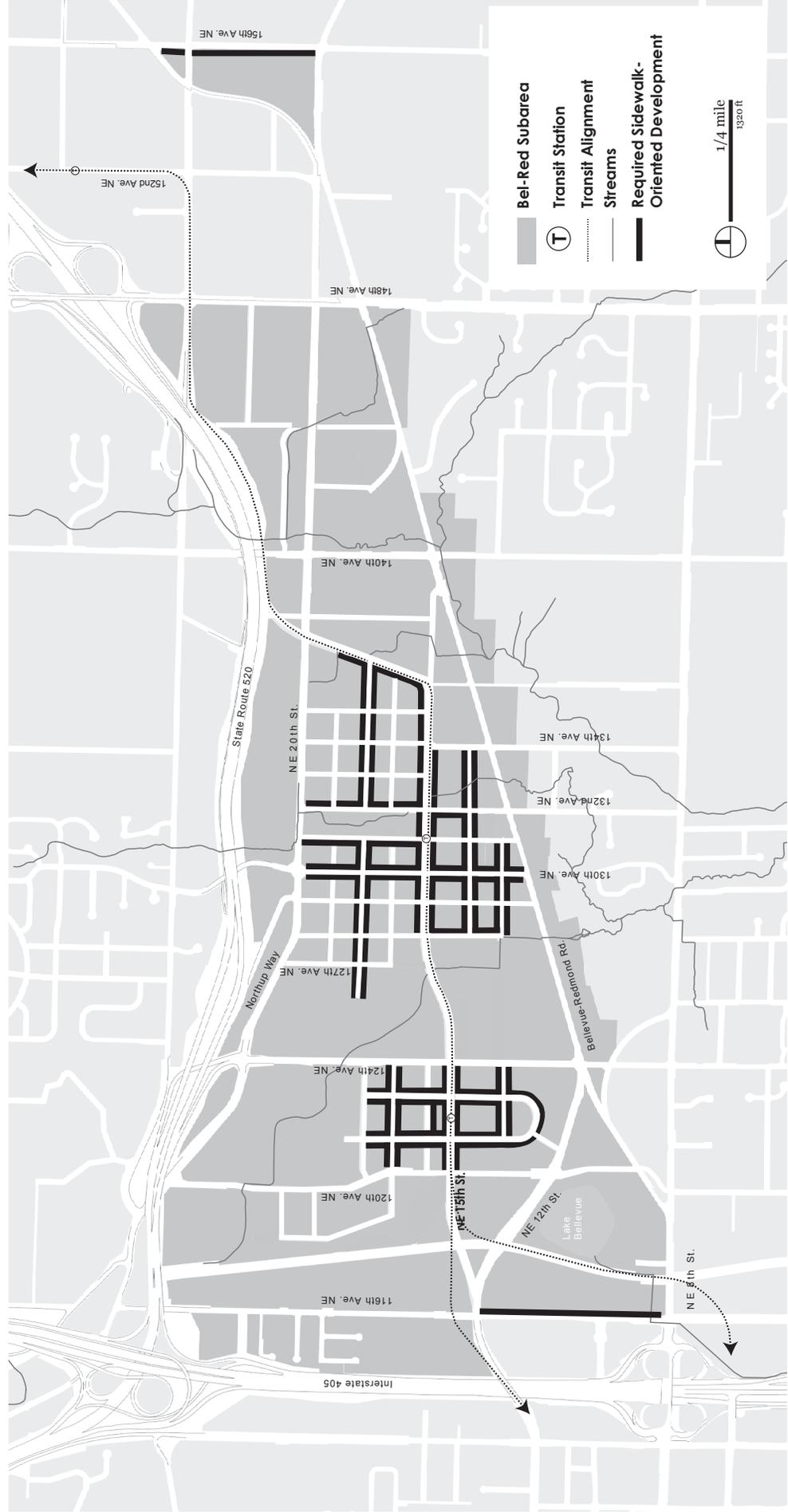


Figure 20.25D.130.C Required Sidewalk-Oriented Development

D. Required Transition Edge Development.

1. Purpose/Intent. Transition edge development requires elements incorporated into the site and building design that soften the impact of BR-ORT, a more intensive land use district, where it is adjacent to single-family uses.
2. Where Required. All buildings on the south side of Bel-Red Road between 124th Avenue NE and 148th Ave NE where abutting property is zoned single-family residential, R-1 – R 7.5.
3. Applicable Standards for Site Design.
 - a. Surface parking lots shall be screened from street level views and from ground level views of an abutting residential district per LUC 20.25D.110.B. In appropriate circumstances, surface parking lots should be located away from adjacent residential properties.
 - b. Mechanical equipment that is located on the roof shall be incorporated into a pitched or stepped roof form, and not appear as a separate penthouse or box.
 - c. All refuse and recycling containers shall be contained within structures enclosed on all four sides and utilize lids made of molded plastic or other sound buffering material. The containers shall be located on a side of the building facing away from the abutting residential properties, but not between the street and the subject building.
4. Applicable Standards for Building Design.
 - a. Building facades shall incorporate elements including but not limited to stepbacks, offsets, roof overhangs, and recesses with a minimum depth of 18 inches. Incorporated recess and offset elements should generally occur along the building façade at intervals no greater than 30 feet.
 - b. A building façade visible from abutting residential properties shall not exceed 150 feet.
 - c. A primary structure shall be a minimum of 20 feet from another primary structure, provided this dimension may be modified pursuant to LUC 20.25H.040 on sites in the Critical Areas Overlay District.
 - d. The maximum building height of 45 feet above average finished grade may be reached only when incorporating pitched or stepped roof forms.
 - e. Communication dishes greater than one meter (3.28 feet) in diameter shall not be visible from adjacent residential districts.
 - f. Natural materials and neutral colors shall be used.

5. Signs.

- a. Building design shall provide for architecturally integrated signage consistent with the scale and architecture of the building, and signage shall be installed so as not to obscure any architectural detail of the building.
- b. Signs shall meet the requirements of Chapter 22B.10 BCC, Bellevue Sign Code.
- c. Signs shall be located so that they are not visible from abutting single family land use districts.

E. Prohibited Materials.

To ensure the Bel-Red District contains high-quality buildings of durable and sustainable materials the following materials are prohibited on facades visible from the public right-of-way within the Bel-Red District unless expressly approved through Design Review:

1. Synthetic stucco;
2. Unfinished metal or plastic storefront window systems;
3. Unfinished concrete or cinder block;
4. Aluminum, plastic, or vinyl siding;
5. Simulated materials such as river rock, or other faux cladding;
6. Clapboard, cementitious lap and shingle, or other types of residential siding;
7. Architectural foam detailing; and
8. Applied simulated divided light window systems.

20.25D.140 Bel-Red Street Development Standards.

The Bel-Red street development standards are a hierarchy of emphasis and design treatment for public areas within the Bel-Red District. These standards ensure that a consistent, high-quality public realm is developed throughout the district and that the unique qualities of Bel-Red are enhanced.

A. Required Local Streets.

1. Purpose/Intent. The intent of the local streets grid is to introduce a public right-of-way system that improves mobility by increasing access for local vehicular and pedestrian traffic throughout the Bel-Red District.

2. Where Required. Figure 20.25D.140.A identifies the general location of new local streets. The Director may approve modifications to the local street grid to respond to specific site conditions, property ownership, and phasing considerations; provided that the modified local street grid satisfies the intent of paragraph 1 above and meets the applicable standards below.
3. Applicable Standards.
 - a. The total perimeter distance of a block shall not exceed 1,200 feet. For the purpose of measuring this dimension, a block may be bordered by a right-of-way, an alley with pedestrian facilities, a private roadway with pedestrian facilities, or a pedestrian street.
 - b. All streets shall be accessible to the public at all times. Gateways or other means of restricting access are prohibited.
 - c. Street design details, including roadway sections and engineering, shall receive all approvals required pursuant to city codes and standards, including but not limited to transportation and utility codes and development standards, now or as hereafter adopted.

Required Local Streets

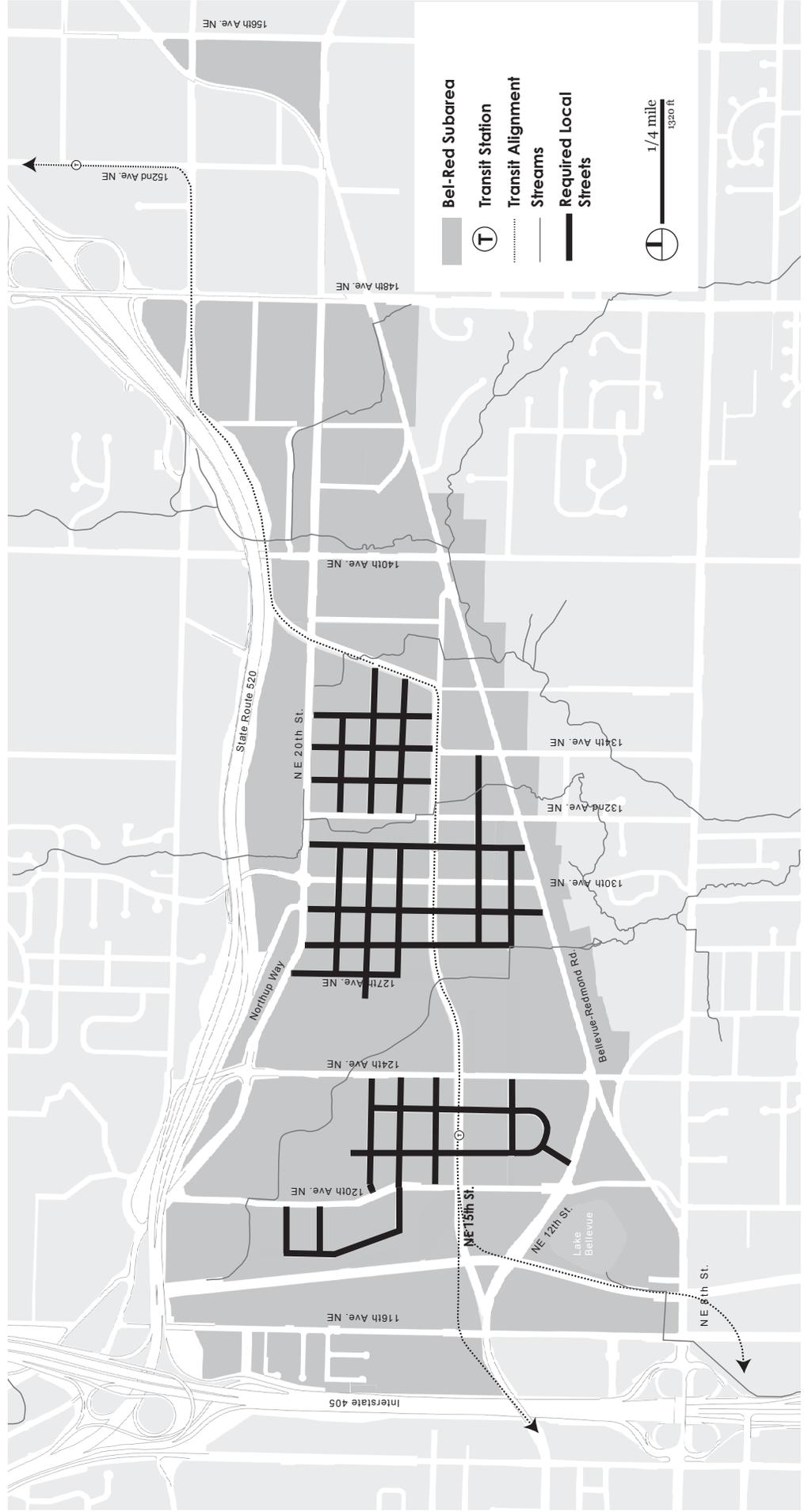


Figure 20.25D.140A: Required New Local Streets

B. 15th/16th Transit Boulevard.

1. Purpose/Intent. The NE 15th/16th Transit Boulevard serves as the symbolic and functional thread that knits the Bel-Red District together. It connects the most intense area of development and includes vehicular, light rail, and non-motorized travel modes. It is also intended to incorporate significant urban open spaces and environmentally sensitive design features.
2. Where Required. Figure 20.25D.140.B identifies the general location for the NE 15th/16th Transit Boulevard. The Director may approve the final location of the Boulevard to respond to specific site conditions, property ownership, and phasing considerations; provided that the final location satisfies the intent of paragraph 1 above and meets the applicable standards below.
3. Street design details, including roadway sections and engineering, shall receive all approvals required pursuant to city codes and standards, including but not limited to transportation and utility codes and development standards, now or as hereafter adopted

NE 15th/16th Transit Boulevard

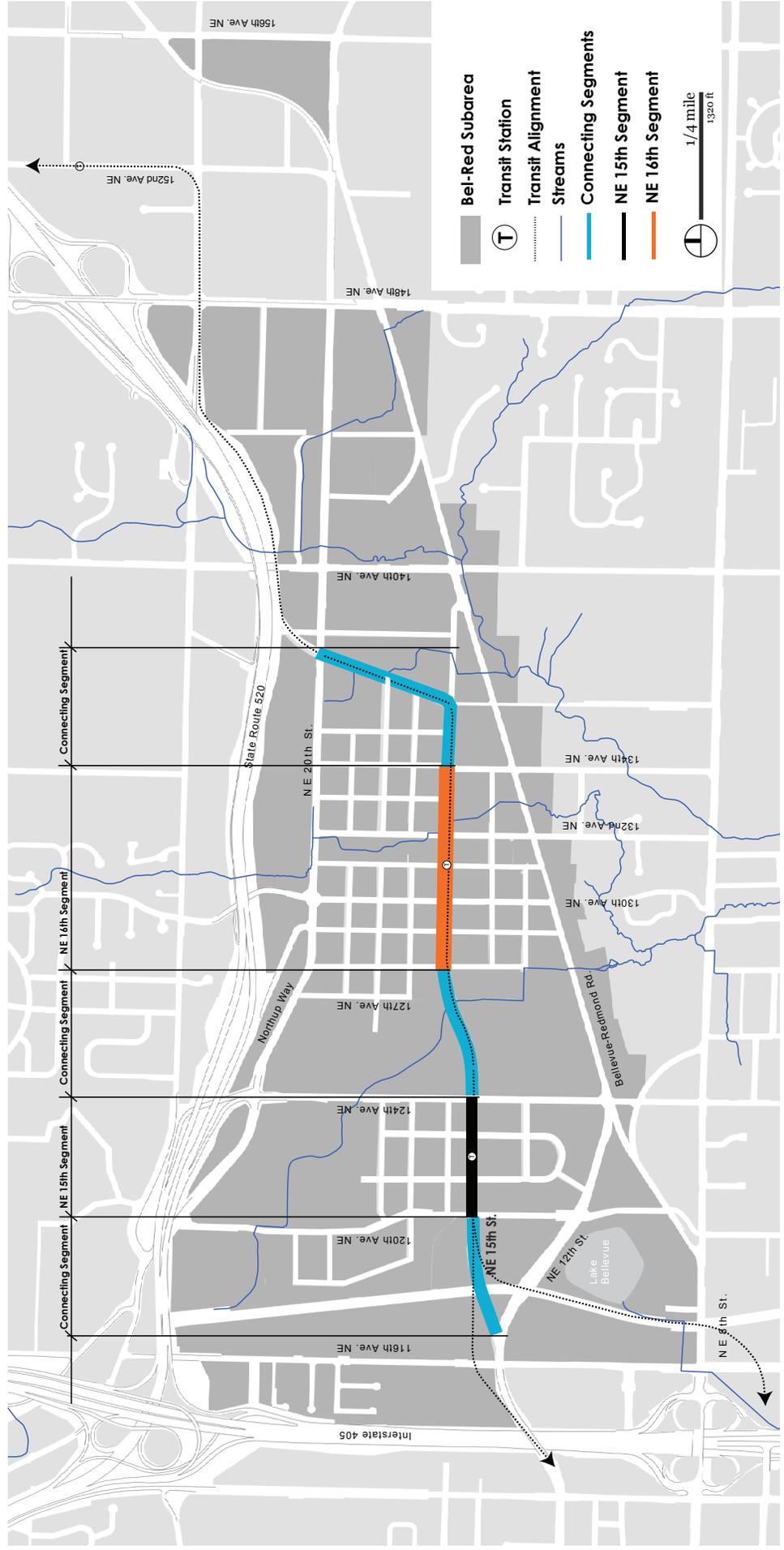


Figure 20.25D.140.B : NE 15th/16th Transit Boulevard Segments

C. 130th Avenue Shopping Street.

1. Purpose/Intent. 130th Avenue NE north of the NE 15th/16th Transit Boulevard is envisioned as a centralized neighborhood-serving retail street. Design is intended to make the pedestrian experience a priority and foster a vital retail environment by using uniform design features.
2. Where Required. The 130th Avenue Shopping Street extends from NE 15th/16th Transit Boulevard to Northup Way/NE 20th. Figure 20.25D.140.C identifies the general location of the 130th Avenue Shopping Street. The Director may approve the final location of the Shopping Street to respond to specific site conditions, property ownership, and phasing considerations; provided that the final location satisfies the intent of paragraph 1 above and meets the applicable standards below.
3. Applicable Standards.
 - a. Sidewalks shall be a minimum of 14 feet 6 inches from face of curb to face of building or open space.
 - b. Street design details, including roadway sections and engineering, shall receive all approvals required pursuant to city codes and standards, including but not limited to transportation and utility codes and development standards, now or as hereafter adopted

130th Avenue Shopping Street

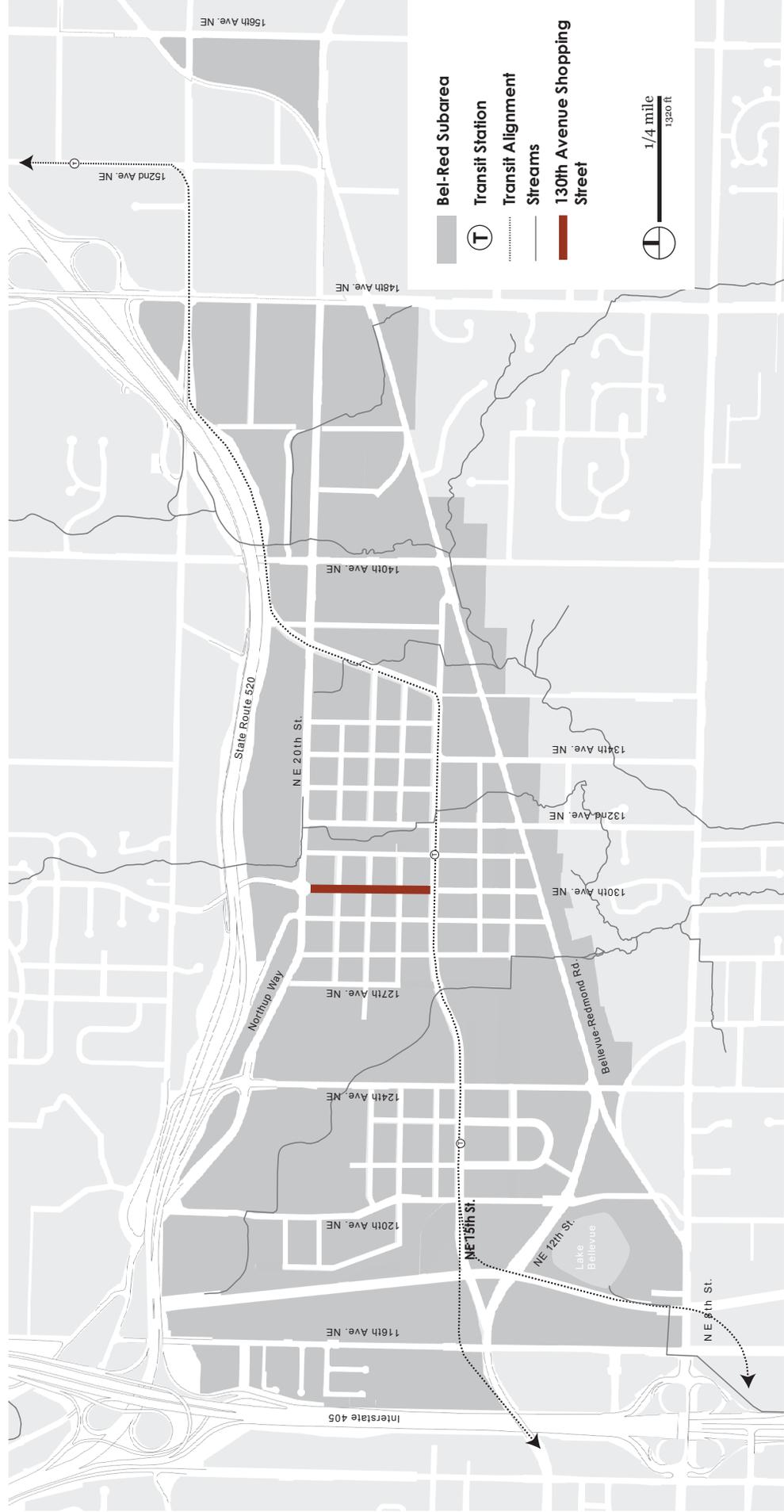


Figure 20.25D.140.C: 130th Avenue Shopping Street

D. Green Streets.

1. Purpose/Intent. Green Streets utilize natural drainage systems to improve and reduce the amount of stormwater runoff at its source.
2. Where Required. Green Streets shall be generally located between stream corridors and trail connections. Figure 20.25D.140.D indicates the general location of Green Streets. The Director may approve the final location of Green Streets to respond to specific site conditions, property ownership, and phasing considerations; provided that the final location satisfies the intent of paragraph 1 above and meets the applicable standards below.
3. Applicable Standards.
 - a. See LUC 20.25D.110.A for Green Streets natural drainage planting requirements.
 - b. Street design details, including roadway sections and engineering, shall receive all approvals required pursuant to city codes and standards, including but not limited to transportation and utility codes and development standards, now or as hereafter adopted

Green Streets

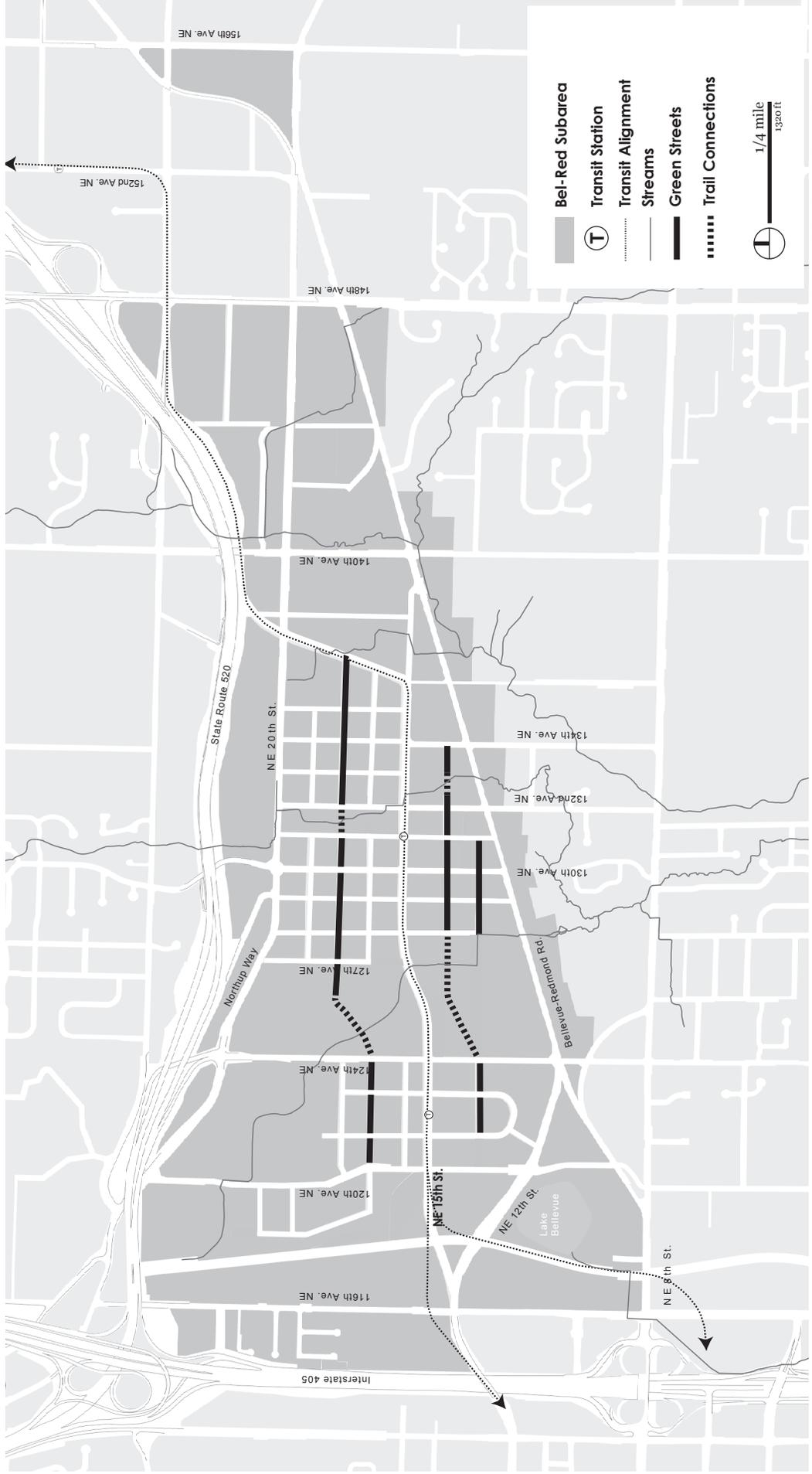


Figure 20.25D.140.D: Green Streets

E. Required On-Street Parking.

1. Purpose/Intent. On-street parking can contribute to the pedestrian environment, be a great benefit to retail uses, and enhance elements of neighborhood character.
2. Where Required. Required locations include the portions of the NE 15th/16th Transit Boulevard adjacent to retail and commercial uses, along the 130th Avenue Shopping Street, and for some of the local streets within the transit nodes. Figure 20.25D.130.E indicates the block faces where on-street parking is required. The Director may approve the final location of on-street parking to respond to specific site conditions, property ownership, and phasing considerations; provided that the final locations satisfy the intent of paragraph 1 above and meets the applicable standards below.
3. Applicable Standards. Parking design details shall receive all approvals required pursuant to city codes and standards, including but not limited to transportation and utility codes and development standards, now or as hereafter adopted.

Required On-Street Parking

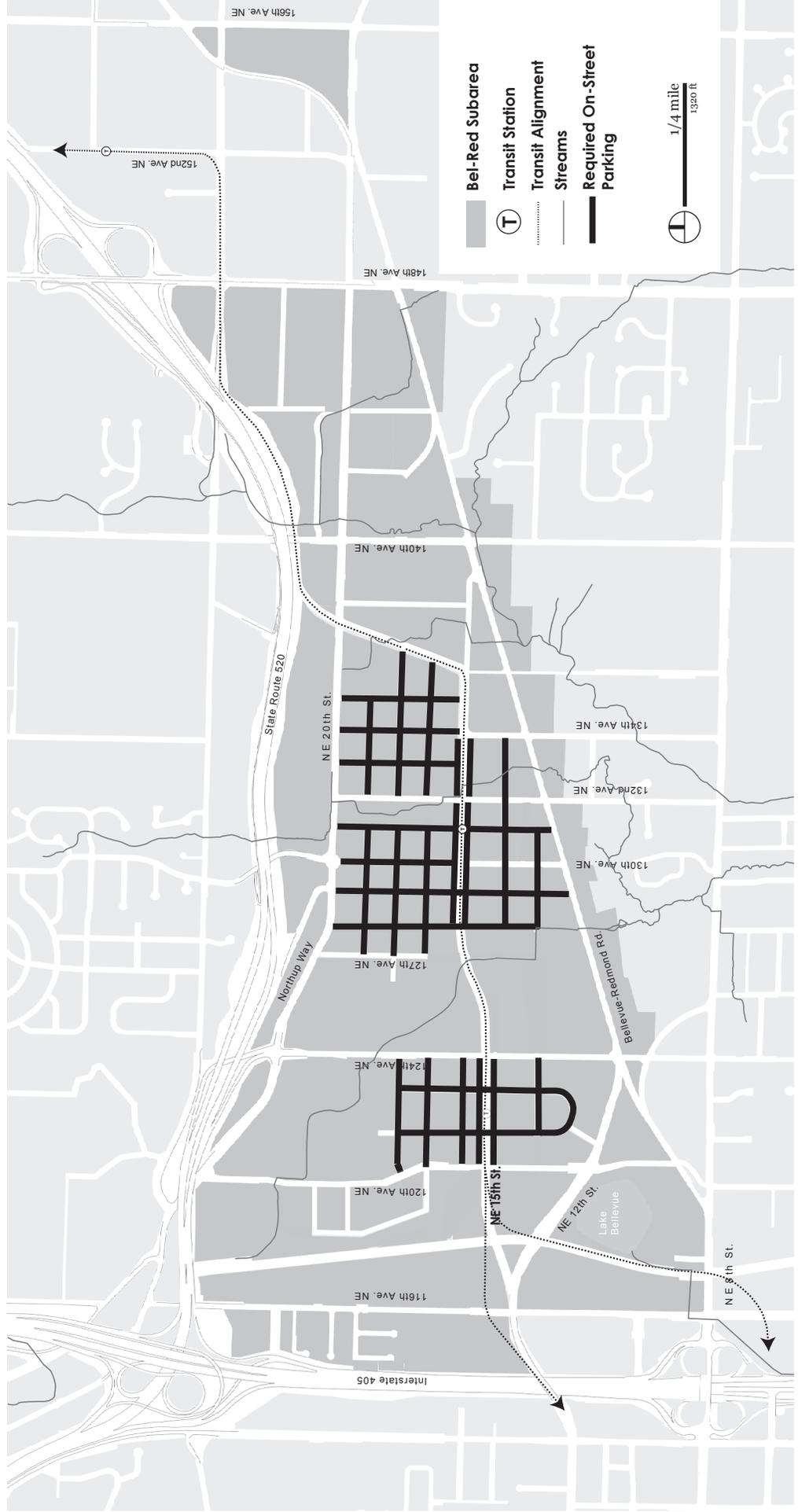


Figure 20.25D.140.E: Required On-Street Parking

F. Restricted Driveway Access.

1. Purpose/Intent. Driveway access locations along certain streets is restricted in order to minimize conflicts with transit, autos, bicycles, and pedestrians and to reinforce active edge continuity along commercial and retail frontages.
2. Where Restricted. Driveway access is generally prohibited along the NE 15th/16th Transit Boulevard and the 130th Shopping Street as shown in Figure 20.25D.140.F.
3. Applicable Standards.
 - a. Except where no feasible alternative access exists, vehicular access to properties abutting the rights-of-way identified in paragraph 2 above, shall be from other arterials, local streets, and alleys off of these designated corridors. The Director may approve vehicular access in these restricted areas to respond to specific site conditions, property ownership, and phasing considerations; provided that the final locations satisfy the intent of paragraph 1 above and meets the applicable standards below.
 - b. The Director may approve on-street loading and service locations when the off-street loading space required by LUC paragraph 20.20.590.K.4 cannot reasonably be designed to satisfy the intent of paragraph 1 above. Designated on-street loading and service locations shall receive all approvals required pursuant to city codes and standards, including but not limited to transportation and utility codes and development standards, now or as hereafter adopted.

Restricted Driveway Access

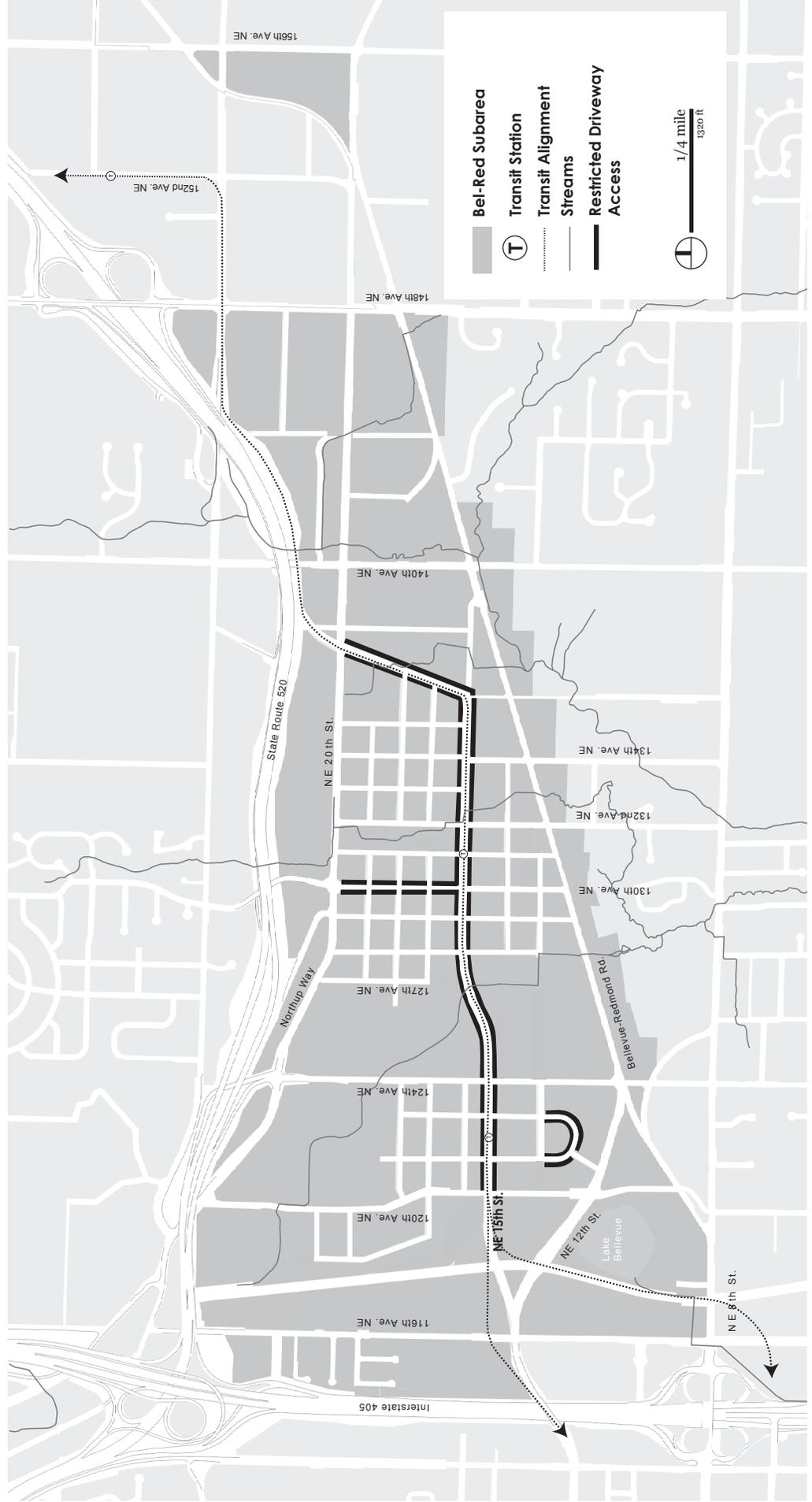


Figure 20.25D.140.F: Restricted Driveway Access

G. Required Street Furniture.

1. Purpose/Intent. Street furniture elements support a consistent and uniform street character, reinforce the identity of a district, and minimize conflicts with pedestrian mobility.
2. Where Required. Street furniture elements are required along streets identified in Figure 20.25D.140.G.
3. Applicable Standards. Location and specifications shall be approved by the Transportation Department.

20.25D.150 Design Guidelines.

Each development within a Bel-Red Land Use District must comply with the provisions of the document entitled Bel-Red Subarea Design Guidelines dated [insert Plan adoption date], now or as hereafter amended or superseded pursuant to the provisions of that document. The provisions of the Design Guidelines will be applied pursuant to the review requirements of LUC section 20.25D.030.

Land Use Code Amendments of General Applicability

20.10.020 Establishment of land use districts.

Land use districts in the City are hereby established as follows:

District	Designation
Single-Family Residential Estate	R-1
	R-1.8
Single-Family Residential	R-2.5
	R-3.5
	R-4
	R-5
Multifamily Residential	R-7.5*
	R-10
	R-15
	R-20
Professional Office	R-30
	PO
	O
	OLB
Office and Limited Business	OLB-OS
Office and Limited Business-Open Space	LI
Light Industrial	GC
General Commercial	NB
Neighborhood Business	CB
Community Business	
Downtown	
Office District 1	DNTN-O-1
Office District 2	DNTN-O-2
Multiple Use District	DNTN-MU
Residential District	DNTN-R
Old Bellevue District	DNTN-OB

Office and Limited Business District	DNTN-OLB
Evergreen Highlands Design District	EH
Performance Area A	EH-A
Performance Area B	EH-B
Performance Area C	EH-C
Performance Area D	EH-D
Factoria Land Use District 1	F1
Factoria Land Use District 2	F2
Factoria Land Use District 3	F3
Medical Institution District	MI
<u>Bel-Red</u>	
<u>Bel-Red-Medical Office</u>	<u>Bel-Red-MO</u>
<u>Bel-Red-Medical Office Node</u>	<u>Bel-Red-MO-1</u>
<u>Bel-Red Office/Residential</u>	<u>BR-OR</u>
<u>Bel-Red-Office/Residential Node 1</u>	<u>Bel-Red-OR-1</u>
<u>Bel-Red-Office/Residential Node 2</u>	<u>Bel-Red-OR-2</u>
<u>Bel-Red-Residential/Commercial Node 1</u>	<u>Bel-Red-RC-1</u>
<u>Bel-Red-Residential/Commercial Node 2</u>	<u>Bel-Red-RC-2</u>
<u>Bel-Red-Residential/Commercial</u>	<u>Bel-Red-RC-3</u>
<u>Bel-Red-Commercial/Residential</u>	<u>Bel-Red-CR</u>
<u>Bel-Red-Residential</u>	<u>Bel-Red-R</u>
<u>Bel-Red-General Commercial</u>	<u>Bel-Red-GC</u>
<u>Bel-Red-Office/ResidentialTransition</u>	<u>Bel-Red-ORT</u>

20.10.375 Bel-Red.

A. Purpose and Intent.

Bel-Red is a major mixed use employment and residential area characterized by a transit-oriented, nodal development pattern, over time replacing the area's original low intensity light industrial and commercial past. The City will encourage land uses in the Bel-Red area which promote employment, retail and residential opportunities. More intense uses and greater heights are concentrated in designated nodal development areas along the NE 15th/16th corridor; these areas are intended to be served by high capacity transit. New development in these designated nodal areas is expected to have a transit-supportive and pedestrian-friendly form. The entire Bel-Red area will be distinguished by environmental and community amenities that serve residents and employees in the area, as well as nearby neighborhoods and the entire city. New

development is expected to make significant contributions to these amenities, and to the infrastructure needed to support redevelopment.

Redevelopment of the Bel-Red area will occur over decades, and the City encourages a graceful transition of land use over time. Therefore special provisions are appropriate for existing uses that may not be part of the area's long-term envisioned future.

1. Bel-Red-Medical Office (Bel-Red-MO). The purpose of the Bel-Red-MO Land Use District is to provide an area for office uses, with an emphasis on medical office.
2. Bel-Red-Medical Office Node (Bel-Red-MO-1). The purpose of the Bel-Red-MO-1 Land Use District is to provide an area for the most intense medical office uses. The district is located within the core of a nodal area, and is limited in extent in order to provide the level of intensity appropriate for areas in close proximity to the highest levels of transit service within the Bel-Red area.
3. Bel-Red Office/Residential (BR-OR). The purpose of the Bel-Red OR Land Use District is to provide an area for a mix of office, housing and retail uses, with office as the predominant use.
4. Bel-Red-Office/Residential Node 1 (Bel-Red-OR-1). The purpose of the Land Use District is to provide an area for a mix of office, housing and retail uses within the core of a nodal area, with office as the predominant use. The district is limited in extent in order to provide the level of intensity appropriate for areas in close proximity to the highest levels of transit service within the Bel-Red area.
5. Bel-Red-Office/Residential Node 2 (Bel-Red-OR-2). The purpose of the Bel-Red-OR2 Land Use District is to provide an area for a mix of office, housing and retail uses, with office as the predominant use. The district is located within a node but outside the node's core, and building heights provide for a transition between the node's core and areas outside the node.
6. Bel-Red-Residential/Commercial Node 1 (Bel-Red-RC-1). The purpose of the Bel-Red-RC-1 Land Use District is to provide an area for a mix of housing, retail, office and service uses within the core of a nodal area, with an emphasis on housing. The district is limited in extent in order to provide the level of intensity appropriate for areas in close proximity to the highest levels of transit service within the Bel-Red area.
7. Bel-Red-Residential/Commercial Node 2 (Bel-Red-RC-2). The purpose of the Bel-Red-RC-2 Land Use District is to provide an area for a mix of housing, retail, office and service uses. The district is located within a node but outside the node's core, and building heights provide for a transition between the node's core and areas outside the node.

8. Bel-Red-Residential/Commercial (Bel-Red-RC-3). The purpose of the Bel-Red-RC-3 Land Use District is to provide an area for a mix of housing, retail, office and service uses, with an emphasis on housing. The district is located within a node but in close proximity to mature, stable neighborhoods, and is thus appropriate for transitional heights.
9. Bel-Red-Commercial/Residential (Bel-Red-CR). The purpose of the Bel-Red-CR Land Use District is to provide an area for a mix of housing, retail, office and services. Multiple uses are encouraged on individual sites, in individual buildings, and in the district as a whole.
10. Bel-Red-Residential (Bel-Red-R). The purpose of the Bel-Red-R Land Use District is to provide an area for residential uses. Limited retail and service uses are permitted secondary to residential use, in order to provide the amenity of shopping and services within easy walking distance of residential structures.
11. Bel-Red-General Commercial (Bel-Red-GC). The purpose of the Bel-Red-GC Land Use District is to provide an area for a wide variety of business activities that provide goods and services to other businesses and the general public.
12. Bel-Red-Office/Residential Transition (Bel-Red-ORT). The purpose of the Bel-Red-ORT Land Use District is to provide an area for low-intensity office and uses and low density multifamily residential dwellings, developed in such a manner as to provide a buffer between residential and more intensively developed properties.

B. Permitted Uses.

Specific categories of uses are listed in Chart 20.25D.070. LUC 20.25D.050 explains Chart 20.25D.070 and describes the applicable review procedures for Bel-Red. The description of the use chart contained in LUC 20.10.400 and the categories of uses contained in LUC 20.10.440 do not apply to the Bel-Red Land Use Districts.

C. General Development Requirements.

1. Regulations applying to specific structures or activities are found listed alphabetically in Chapter 20.20 LUC; consult the alphabetical Key Word Index in the beginning of that chapter.
2. Bel-Red specific standards and guidelines are found in Chapter 20.25D LUC. All development in Bel-Red shall conform these requirements.

20.10.420 Interpretation of Land Use eCharts by Director of Planning and Community Development—Appeal.

- A. Director's Authority. In the case of a question as to the inclusion or exclusion of a particular proposed use in a particular use category, the Director of Planning and

~~Community Development shall have the authority to make the final determination. The Director of Planning and Community Development shall make the determination according to the characteristics of the operation of the proposed use and based upon the Director's interpretation of the Standard Land Use Coding Manual, and the Standard Industrial Classification Manual and the North American Industry Classification System.~~

~~Examples: A large sales office, where business is conducted by telephone or not on the premises, where no goods other than samples pass into or out of the site, is an office rather than a retail or wholesale use; a shop selling handcrafted items made on the premises where the manufacturing processes are undetectable outside the premises and require no heavy trucking, is a retail use, not a manufacturing use.~~

B. Conflict. In the case of a conflict between the Land Use District Descriptions (contained in LUC 20.10.0180 through 20.10.395)~~general description~~ and the Use Chart, the Use Charts contained in LUC 20.10.440 or Chapter 20.25 LUC chart shall prevail.

C. Appeal. An applicant may appeal the final decision of the Director provided pursuant to paragraph A above by requesting an interpretation of the Use Charts contained in LUC 20.10.440 or Chapter 20.25 LUC relating to the inclusion or exclusion of a proposed use. This request for interpretation of the Land Use Code charts will be processed pursuant to ~~will be processed using Part 20.30K LUC, Interpretation of the Land Use Code.~~

20.10.440 Uses in Land Use District [Add following note to each chart]

Permitted uses in the Bel-Red District (BR) are listed in LUC 20.25D.070.

20.20.005 Chart of dimensional requirements described.

Chart 20.20.010 sets forth the dimensional requirements for each land use district except: the Downtown Land Use Districts, the Evergreen Highlands Design District, the Evergreen Highlands Subarea Transportation Improvement Overlay District, Medical Institution District, ~~and the OLB-OS Land Use District,~~ and the Bel-Red Land Use Districts. All structures and activities in the City not located in the above districts shall conform to the dimensional requirements in Chart 20.20.010. Dimensional requirements for the Downtown Land Use Districts are found in LUC 20.25A.020. Dimensional requirements for the Evergreen Highlands Design District are found in Part 20.25F LUC. Dimensional requirements for the Evergreen Highlands Subarea Transportation Improvement Overlay District are found in Part 20.25G LUC. Dimensional requirements for the Medical Institution District are found in Part 20.25J LUC. Dimensional requirements for the OLB-OS Land Use District are found in LUC 20.25L.030. Dimensional requirements for the Bel-Red Land Use Districts are found in LUC

20.25D.080. Additional special dimensional requirements for designated areas of the City are contained in other parts of the Code as follows:

- A. Part 20.25B LUC – Transition Areas;
- B. Part 20.25C LUC – OLB Districts;
- C. Part 20.25E LUC – Shoreline Overlay District;
- D. Part 20.25H LUC – Critical Areas Overlay District;
- E. Part 20.45A LUC – Platting and Subdivisions;
- F. Part 20.45B LUC – Short Plats and Short Subdivisions.

20.20.010 Uses in land use districts dimensional requirements.

[Add following notes to the general dimensional chart]

NOTE: Dimensional Requirements for Downtown are found in Part 20.25A LUC.
Dimensional Requirements for Evergreen Highlands Design District (EH-A, EH-B, EH-C, EH-D) are found in Part 20.25F LUC.
Dimensional Requirements for Office and Limited Business – Open Space (OLB-OS) are found in Part 20.25L LUC.
Dimensional Requirements for Medical Institution District (MI) are found in Part 20.25J LUC.
Dimensional Requirements for Bel-Red Land Use Districts are found in Part 20.25D LUC.

...

Note (42) Dimensional requirements for the Bel-Red Land Use Districts are found at LUC 20.25D.080.

20.20.070 Lots nonconforming as to area, street frontage, width or depth – Status.

E. This section is not applicable in the Bel-Red Land Use Districts. Refer to LUC 20.25.060 for regulations relating to existing conditions.

20.20.130 Animals Keeping and Services.

A. General.

~~The keeping of animals is~~Animal services as defined in 20.50.010 are subject to the requirements of this section and BCC Title 8, Animal Regulations.

B. Minimum Requirements.

The following chart entitled, "Animal Regulations" sets forth the minimum requirements for certain types of animal ~~the keeping of animals~~.

Animal Regulations

Type of Animal/Use	Maximum Number (1A)	Minimum Lot Size	Minimum Setback (5E)
1. Household Pets* (6)	Dogs, Cats, Rabbits: 3(2B); Fowl: 6(4D); Other: no maximum	No minimum	<p>May not be restrained or enclosed outdoors so that the animal is able to come within 15 feet of a property line. This limitation does not prohibit the keeping of a household pet within the following areas, provided it must be allowed to roam freely therein:</p> <ul style="list-style-type: none"> 1) A lot which is fenced along all lot lines so as to enclose the entire lot, or 2) An enclosed portion of a lot which is bounded by fences along either the entire front lot line or entire rear lot line, and along a portion of both side lot lines, which utilizes the house or primary structure as one side of the enclosure and which may include all or a portion of either or both side yards. * 3) No structure to house the household pet may be within 15 feet of a property line.
2. Small Domestic Animals or Hobby Kennel (6)*	10: 20,000 sq. ft. and an additional 1,500 sq. ft. for each animal; 6: 10,000 sq. ft. and an additional 1,500 sq. ft. for each animal over 6	20,000 sq. ft. or 10,000 sq. ft.	<p>May not be restrained or enclosed outdoors so that the animal is able to come within 25 feet of a property line. This limitation does not prohibit the keeping of a small domestic animal within the following areas, provided it must be allowed to roam freely therein:</p> <ul style="list-style-type: none"> 1) A lot which is fenced along all lot lines so as to enclose the entire lot, or 2) An enclosed portion of a lot

			<p>which is bounded by fences along either the entire front lot line or entire rear lot line, and along a portion of both side lot lines, which utilizes the house or primary structure as one side of the enclosure and which may include all or a portion of either or both side yards.</p> <p>← <u>3</u>) No structure to house the household pet may be within 25 feet of a property line.</p>
3. Large Domestic Animals* <u>(6)</u>	1: each 10,000 sq. ft.	20,000 sq. ft.	<p>May not be restrained or enclosed outdoors so that the animal is able to come within 25 feet of a property line. This limitation does not prohibit the keeping of a large domestic animal within the following areas, provided it must roam freely therein:</p> <p>1) A lot which is fenced along all lot lines so as to enclose the entire lot, or</p> <p>2) An enclosed portion of a lot which is bounded by fences along either the entire front lot line or entire rear lot line, and along a portion of both side lot lines, which utilizes the house or primary structure as one side of the enclosure and which may include all or a portion of either or both side yards.</p> <p>← <u>3</u>) No structure to house the animal may be within 25 feet of a property line.</p>
*Special Regulations: Open pasture, foraging or grazing may extend to the property line.			
4. Bees <u>(7)**</u>	Maximum of four hives, each with one colony: Lots containing 15,000 sq. ft. or less; Maximum of 15	7,200 sq. ft.	Hives must be at least 25 feet from a property line, except when situated 8 feet or more above adjacent ground level, or when situated less than 6 feet above adjacent ground level and behind a solid fence or hedge at

	hives, each with one colony: Lots containing more than 15,000 sq. ft. but less than 35,000 sq. ft.;		least 6 feet high parallel to any property line within 25 feet of the hive and extending at least 25 feet level, or when situated beyond the hive in both directions.
	Maximum of 25 hives, each with one colony: Lots containing 35,000 sq. ft. or more		

****Special Regulations:**

- ~~Must register with the Washington State Department of Agriculture.~~
- ~~Must requeen annually or anytime following swarming or aggressive behavior with a breeder raised queen of suitable docile strain.~~
- ~~Hives must be maintained to avoid overpopulation and minimize swarming.~~
- ~~Must be maintained in a movable frame hive at all times.~~

5. Kennel (3C)(8)	Determined in Conditional Use Process	35,000 sq. ft.	May not be restrained or enclosed outdoors so that the animal is able to come within 50 feet of a property line. This limitation does not prohibit the keeping of an animal within the following areas, provided it must be allowed to roam freely therein: 1) A lot which is fenced along all lot lines so as to enclose the entire lot, or 2) An enclosed portion of a lot which is bounded by fences along either the entire front lot line or entire rear lot line, and along a portion of both side lot lines, which utilizes the house or primary structure as one side of the enclosure and which may include all or a portion of either or both side yards. 3) No structure to house the animal may be within 50 feet of a property line.
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*****Special Regulations: See LUC 20.10.440.**

6.	Determined in	35,000 sq.	— May not be restrained or
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Commercial Stable or Riding Academy (3G) (9) ⁺	Conditional Use Process	ft.	<p>enclosed outdoors so that the animal is able to come within 50 feet of a property line. This limitation does not prohibit the keeping of an animal within the following areas, provided it must be allowed to roam freely therein:</p> <p>1) A lot which is fenced along all lot lines so as to enclose the entire lot, or</p> <p>2) An enclosed portion of a lot which is bounded by fences along either the entire front lot line or entire rear lot line, and along a portion of both side lot lines, which utilizes the house or primary structure as one side of the enclosure and which may include all or a portion of either or both side yards.</p> <p>2 <u>3</u> No structure to house the animal may be within 50 feet of a property line.</p>
<p>+Special Regulations: See LUC 20.10.440. Open pasture, foraging or grazing may extend to the property line.</p>			

Notes: Animal Regulations

- ~~(A)~~(1) Number of adult animals. One unweaned litter of offspring, and foals are not included in the number of animals allowed.
- ~~(B)~~(2)—More than three rabbits are regulated as small domestic animals.
- ~~(C)~~(3)—Requires a Conditional Use Permit.
- ~~(D)~~(4)—More than six fowl are regulated as small domestic animals.
- ~~(E)~~(5) The purpose of these setback requirements is to prohibit the confinement of an animal within specific distances from neighboring property, as by leashing the animal to a stake or placing the animal in an enclosure, but to allow animals to be kept in yards fenced on their perimeter so long as the animal is free to roam within the fenced area.
- (6) Special Regulations: Open pasture, foraging or grazing may extend to the property line.
- (7) Special Regulations:
 - (a) Must register with the Washington State Department of Agriculture or any successor organization responsible for oversight of bee keeping.
 - (b) Must requeen annually or anytime following swarming or aggressive behavior with a breeder raised queen of suitable docile strain.
 - (c) Hives must be maintained to avoid overpopulation and minimize swarming.

- (d) Must be maintained in a movable frame hive at all times.
- (8) Special Regulations: See LUC 20.10.440.
- (9) Special Regulations: See LUC 20.10.440. Open pasture, foraging or grazing may extend to the property line.

C. Prohibited Animals.

The keeping of mink, foxes or hogs is prohibited.

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D. Pet Daycare.

In addition to the development standards applicable to the land use district, including Title 8 and Chapter 9.18 of the BCC, pet daycares are subject to the following requirements:

1. Facilities for the boarding of animals may occupy no more than 25 percent of the gross floor area of the pet daycare center.
2. Applicants shall submit at the time of permit application, written operating procedures, such as those recommended by the American Boarding and Kennel Association (ABKA) or the American Kennel Club (AKC). Such procedures, which are to be followed for the life of the business, shall address the identification and correction of animal behavior that impacts surrounding uses, including excessive barking.

E. Veterinary Services.

In addition to the development standards applicable to the land use district, including Title 8 and Chapter 9.18 of the BCC, veterinary services are subject to the following requirements:

1. A veterinary clinic designed for the treatment and care of pet animals, shall be operated by a registered veterinarian.
2. Animals shall be confined within the exterior walls of the building at all times.
3. Pet daycare services may be allowed as a subordinate use subject to the provisions of LUC 20.20.840 and LUC 20.20.130(D). All pet daycare services shall be isolated by soundproofing from all adjacent property and uses.
4. Walls of interior-court animal runs shall be a minimum of eight feet high. Interior-court animal runs shall be roofed and if there are open air spaces between the top of the wall and the roof, they shall be enclosed with wire mesh.

5. All rooms housing animals shall have ample natural or mechanical ventilation.
6. There shall be no cremation or other disposal of dead animals on the premises.
7. A veterinary clinic or hospital building shall not be located closer than 100 feet to an existing residence, residential district (R-1 through R-30), restaurant, clinic or hospital for humans.
8. The setback required above shall not apply in the case of a residence used by the veterinarian himself, or any caretaker or watchman on the same or an adjoining lot.

20.20.195 Communication, broadcast and relay facilities.

A. Definitions.

The following definitions apply to this section:

1. “Flush mounted” shall mean attached to the face of the support structure or building such that no portion of the antenna extends above the height of the support structure or building. Where a maximum flush mounting distance is given, that distance shall be measured from the outside edge of the support structure or building to the inside edge of the antenna.
2. “Gateway intersection” shall mean those intersections identified on Figure UD-1 in the Comprehensive Plan, now or as hereafter amended.
3. “Nonresidential land use districts” shall mean PO, O, OLB, OLB-OS, LI, GC, NB, CB, F1, F2, F3, EH, MI, and all Downtown land use districts, and all Bel-Red land use districts.
4. “Residential land use districts” shall mean R-1, R-1.8, R-2.5, R-3.5, R-4, R-5, R-7.5, R-10, R-15, R-20 and R-30.
5. “Utility support structure” shall mean street or pedestrian light standards, guy poles, traffic signal standards, and poles or towers supporting electrical, telephone, cable or other similar facilities.

20.20.255 Electrical utility facilities.

D. Alternative Siting Analysis.

In addition to the requirements set forth in Part 20.30B LUC, Part 20.25B LUC (if applicable), and the decision criteria and design standards set forth in this section, the applicant shall identify alternative sites, provide required content showing

analysis relating to identified sites, describe technologies considered, and describe community outreach conducted for proposals relating to new or expanding electrical utility facilities on sensitive sites as described in this section.

1. Alternative Sites Analyzed. Prior to submittal of the application for Conditional Use Permit required pursuant to subsection C of this section, the applicant shall identify not less than three alternative site options to meet the system needs for the proposed new or expanding electrical utility facility. At least one of the alternative sites identified by the applicant shall be located in the land use district to be primarily served by the proposed electrical utility facility.
2. Content of Alternative Siting Analysis. Upon submittal of the Conditional Use Permit required pursuant to subsection C of this section, the applicant shall submit results of the siting analysis which:
 - a. Describe the sites identified in subsection D.1 of this section and the land use districts within which the sites are located.
 - b. Map the location of the sites identified in subsection D.1 of this section and depict the proximity of the sites to Neighborhood Business Land Use Districts, Residential Land Use Districts, and Transition Areas.
 - c. Describe which of the sites analyzed are considered practical or feasible alternatives by the applicant, and which of the sites analyzed are not considered practical or feasible, together with supporting information that justifies the conclusions reached. For sites located within a Neighborhood Business Land Use District, Residential Land Use District, and/or Transition Area (including the Bel-Red Office/Residential Transition (BR-ORT)), the applicant shall:
 - i. Describe whether the electrical utility facility location is a consequence of needs or demands from customers located within the district or area; and
 - ii. Describe whether the operational needs of the applicant require location of the electrical utility facility in the district or area.
 - d. Identify a preferred site from the alternative locations considered for the proposed new or expanding electrical utility facility. The following location selection hierarchy shall be considered during identification of the preferred site alternative: (i) nonresidential land use districts not providing transition, (ii) nonresidential Transition Areas (including the Bel-Red Office/Residential Transition (BR-ORT)), and (iii) residential areas. The applicant may identify a preferred site alternative in a Residential Land Use District or Transition Area (including the Bel-Red Office/Residential Transition (BR-ORT)) upon demonstration that the location has fewer site compatibility impacts than a nonresidential land use district location.

3. Technology Considered for the Preferred Site Alternative. Upon submittal of the Conditional Use Permit required pursuant to subsection C of this section, the applicant shall:
 - a. Describe the range of technologies considered for the proposed electrical utility facility;
 - b. Describe how the proposed electrical utility facility provides reliability to customers served;
 - c. Describe components of the proposed electrical utility facility that relate to system reliability; and
 - d. Describe how the proposed facility includes technology best suited to mitigate impacts on surrounding properties.
4. Community Outreach Conducted. Upon submittal of the Conditional Use Permit application required pursuant to subsection C of this section, the applicant shall provide a description of all methods of community outreach or involvement conducted by the applicant prior to selecting a preferred site for the proposed electrical utility facility.

E. Decision Criteria.

In addition to the requirements set forth in Part 20.30B LUC, Part 20.30E LUC, Part 20.25B LUC (if applicable), and other applicable provisions of this section, all proposals to locate or expand electrical utility facilities shall comply with the following:

1. The proposal is consistent with Puget Sound Energy's System Plan;
2. The design, use, and operation of the electrical utility facility complies with applicable guidelines, rules, regulations or statutes adopted by state law, or any agency or jurisdiction with authority;
3. The applicant shall demonstrate that an operational need exists that requires the location or expansion at the proposed site;
4. The applicant shall demonstrate that the proposed electrical utility facility improves reliability to the customers served and reliability of the system as a whole, as certified by the applicant's licensed engineer;
5. For proposals located on sensitive sites as referenced in Figure UT.5a of the Utility Element of the Comprehensive Plan, the applicant shall demonstrate:
 - a. Compliance with the alternative siting analysis requirements of subsection D of this section;

- b. Where feasible, the preferred site alternative identified in subsection D.2.d of this section is located within the land use district requiring additional service and residential land use districts are avoided when the proposed new or expanded electrical utility facility serves a nonresidential land use district;
6. The proposal shall provide mitigation sufficient to eliminate or minimize long-term impacts to properties located near an electrical utility facility.

20.20.350 Essential public facility (EPF).

A. Applicability.

This section applies to each essential public facility (EPF) within the City except where a specific use is otherwise identified and regulated in the use charts in LUC [20.10.440](#) and [Chapter 20.25 LUC](#). The requirements of this section shall be imposed at the establishment of any such EPF use, and upon any addition or modification to any such EPF use or structure housing that use.

Any EPF specifically identified and regulated in the use charts in LUC [20.10.440](#) and [Chapter 20.25 LUC](#) shall be subject to the permitting procedures and requirements for that use, and shall not be subject to this section.

C. Decision Criteria.

In addition to the decision criteria applicable to any permit required to construct or modify the EPF, the City may approve, or approve with modifications, a proposal to construct or modify an EPF if:

1. The location and design is consistent with any planning document under which the proposing agency, special district or organization operates, as determined by the person or body having authority to interpret such document;
2. The location, design, use and operation of the EPF complies with any applicable guidelines, rules, regulations or statutes adopted by state law, or any agency or jurisdiction with authority;
3. A building which houses all or a majority of an EPF must be compatible with the architectural form of surrounding buildings. This requirement is not applicable to an EPF where significant elements of the facility are not housed in a building or to isolated minor elements such as utility meters;
4. An EPF may be permitted in a Neighborhood Business or residential land use district ([R-1 through R-30](#)), only if there is an operational or other need that requires locating in that district to achieve the purpose or function of the EPF;
5. The City may approve a request to exceed the height limit for the underlying land use district if the applicant demonstrates that:

- a. The requested increase is the minimum necessary for the effective functioning of the EPF; and
 - b. Visual and aesthetic impacts associated with the EPF have been mitigated to the greatest extent technically feasible;
6. If the City determines that the EPF is potentially dangerous to human life, appropriate protective measures may be required.

20.20.460 Impervious surface.

B. Applicability.

The impervious surface limits contained in LUC 20.20.010 and Chapter 20.25 LUC, and the standards of this section, shall be imposed any time a permit, approval, or review including land alteration or land development including subdivisions, short subdivisions or planned unit developments, a change in lot coverage, or a change in the area devoted to parking and circulation is required by this Code, or by the International Building Code.

C. Modifications to Impervious Surface Limits.

The impervious surface limits contained in LUC 20.20.010 and Chapter 20.25 LUC may be modified pursuant to a critical areas report, LUC 20.25H.230, so long as the critical areas report demonstrates that the effective impervious surface on the site does not exceed the limit established in LUC 20.20.010 and Chapter 20.25 LUC.

2. Garages on sites sloping uphill should be placed below the main floor elevation where feasible to reduce grading and to fit structures into existing topography. Garages on sites sloping downhill from the street may be required to be placed as close to the right-of-way as feasible and at or near street grade. Intrusion into the front setback, as provided in LUC 20.20.025.B, may be required. On slopes in excess of 25 percent, driveways shall be designed to minimize disturbance and should provide the most direct connection between the building and the public or private street; and
3. Changes in existing grade outside the building footprint shall be minimized. Excavation shall not exceed 10 feet. Fill shall not exceed five feet subject to the following provisions: all fill in excess of four feet shall be engineered; and engineered fill may be approved in exceptional circumstances to exceed five feet to a maximum of eight feet. Exceptional circumstances are: (1) instances where driveway access would exceed 15 percent slope if additional fill retained by the building foundation is not permitted; or (2) where the five-foot fill maximum generally is observed but limited additional fill is necessary to accommodate localized variations in topography.

F. Existing Impervious Surfaces.

Impervious surfaces legally established on a site prior to August 1, 2006, and which exceed the limits set forth in LUC 20.20.010 and Chapter 20.25 LUC shall not be considered nonconforming. Proposals to increase impervious surface on a site shall conform to the limits of LUC 20.20.010 and Chapter 20.25 LUC; where a site already exceeds the allowed amount of impervious surface, the additional impervious surface shall not be approved unless an equal amount of existing impervious surface is removed such that the net amount of impervious surface is unchanged.

20.20.560 Nonconforming structures, uses and sites.

E. Exceptions.

1. Downtown. The provisions of this section shall not apply in the Downtown Special Overlay District, Part 20.25A LUC. Refer to LUC 20.25A.025 for the requirements for nonconforming uses, structures, and sites located within the Downtown Special Overlay District.
2. Critical Areas Overlay District. The provisions of this section do not apply to structures or sites nonconforming to the requirements of Part 20.25H LUC. Refer to LUC 20.25H.065 for the requirements for such nonconforming structures and sites.
3. Shoreline Overlay District. The provisions of this section do not apply to uses, structures or sites nonconforming to the requirements of Part 20.25E LUC. Refer to LUC 20.25E.055 for the requirements for such nonconforming uses, structures and sites.
4. Bel-Red (BR) Land Use Districts. The provisions of this section do not apply to uses, structures, or sites located in the Bel-Red Land Use Districts. For uses in the Bel-Red Land Use Districts established before [insert Plan adoption date] refer to the existing conditions regulations in LUC 20.25D.060.

20.20.590 Parking, circulation and walkway requirements.

A. Scope.

This section contains standards and design requirements for parking, circulation and internal walkways, except as otherwise provided in Chapter 20.25 LUC for Special and Overlay Districts ~~(e.g., Part 20.25A LUC governs requirements in Downtown Districts).~~

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20.20.620 Produce stands.

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B. Permanent stands for the sale of fruit and vegetables may be erected in zoning districts permitting such retail sales and are exempt from Design Review requirements so long as:

1. Setback requirements are met;
2. The vendor provides safe vehicular and pedestrian ingress and egress from the street to the stand;
3. The site provides parking for retail uses according to the requirements of LUC 20.20.590 for retail uses or district specific parking requirements contained in Chapter 20.25 LUC;
4. The stand meets technical code requirements for utilities and structural integrity;
5. Refuse, recycling, and similar containers are sight-screened and are made of sound-muffling material such as molded plastic;
6. Lighting is designed to prevent spillover of light from the site to surrounding properties; and
7. Where interior property lines abut residentially zoned property, sight-screening in the form of solid board fencing, evergreen plantings, berming, or a combination thereof, to a height of six feet, is provided.

20.20.650 Public utilities – Design and performance standards.

A. Utility Facility Site Standards.

1. Architectural Form and Character. A building which houses all or a majority of a utility facility must be compatible with the architectural form of surrounding buildings. This requirement is not applicable to a utility facility where significant elements of the facility are not housed in a building or to isolated minor elements such as pad mounted transformers, telephone pedestals and metering stations.
2. Screening and Fence Requirements.
 - a. A utility facility must be sight-screened as specified for that use in LUC 20.20.520.F.2 or as required for by district specific landscape standards contained in Chapter 20.25 LUC~~the applicable land use district~~. Alternatively, the provisions of LUC 20.20.520.J may be used.
 - b. If the Director of Planning and Community Development and the Transportation or Utilities Director, as applicable, determine that the utility facility is potentially dangerous to human life, an eight-foot fence may be required.

3. Required Setback.
 - a. Any structure, facility or fence must conform to the setback requirement for structures in the land use district in which the structure, facility or fence is located, except as specified in paragraph A.3.b of this section.
 - b. The minimum side setback for a structure, facility or fence in an R Land Use District is 20 feet.

20.20.730 Large satellite dish antennas.

A. Large Satellite Dish Antennas in Nonresidential Land Use Districts.

Large satellite dish antennas in all nonresidential districts (all land use districts except R-1 through R-30), shall be screened in accordance with the requirements of LUC 20.20.525.C for mechanical equipment screening.

B. Large Satellite Dish Antennas in Residential Land Use Districts.

The requirements for screening of large satellite dish antennas in residential districts (R-1 through R-30) shall depend upon the nature of use and building type of the development.

1. Large satellite dish antennas in any residential development other than detached or single-family attached housing shall be screened in accordance with the requirements of LUC 20.20.525.C for mechanical equipment screening.
2. Large satellite dish antennas in any residential development consisting of detached or single-family attached housing shall be screened in accordance with subsection C of this section.

C. Large satellite dish antennas in any residential development consisting of detached or single-family attached housing as specified in subsection B.2 of this section are permitted subject to the following criteria, provided the Director of ~~Planning and Community Development~~ may modify setback and screening requirements upon proof that strict application of the requirements is infeasible or renders use of an antenna impossible:

1. The antenna shall meet front and side setback requirements for the main building and shall be a minimum of five feet from any rear property line;
2. The antenna shall be a minimum of 10 feet distant from any street right-of-way, vehicular access easement, or private road;
3. No antenna shall be located in a buffer or setback required by the City's critical areas regulations (see Part 20.25H LUC), unless affixed to a structure allowed pursuant to Part 20.25H LUC; and

4. The antenna shall be substantially screened from view from adjacent property and the adjacent public rights-of-way by sight-obstructing landscaping, fencing, on-site structures, or natural topography.

20.20.740 Schools.

A. Public and private schools are permitted as indicated by the chart in LUC 20.10.440 and use charts applicable to specific land use districts contained in Chapter 20.25 LUC, "Education: Primary and Secondary," provided the following standards are met:

1. School buildings in residential districts shall cover not more than 35 percent of their site area.
2. Except as allowed in this section, school buildings in residential districts shall provide 50-foot side and rear yard setbacks. A reduction in the side or rear yard setback may be approved as follows:
 - a. In no event may a side or rear yard setback be less than 30 feet;
 - b. The landscaping required along interior property lines required pursuant to subsection A.5 of this section shall be increased to a minimum of 20 feet in depth;
 - c. No portion of a structure exceeding the building height allowed for the district may be within 50 feet of the side or rear property line; and
 - d. Building entrances or exits, other than required emergency exits, are not located within 50 feet of a side or rear property line.
3. Building Height.
 - a. An increase in building height of up to 10 feet above the maximum building height of the underlying district is allowed for schools so long as:
 - i. No mechanical equipment is located on the roof of any building or portion of building that exceeds the maximum building height of the underlying district; and
 - ii. The school is located on a site of at least five acres.
 - b. An increase in building height above that allowed under subsection A.3.a of this section may be approved if that portion of the structure exceeding the maximum building height of the underlying district satisfies the following criteria:
 - i. The increase in height is necessary to accommodate uses or equipment functionally related to a program offered as part of the

educational programs of the school, for example, for a performing arts center, library or gymnasium;

- ii. That portion of the structure exceeding the maximum building height is set back from any property line a distance equal to 1.5 times the height of that portion of the structure, unless a greater setback is required pursuant to LUC 20.20.010, or the dimensional charts applicable to specific land use districts contained in Chapter 20.25 LUC, or this section;
 - iii. The building and site design minimize the impact of the additional height on the surrounding land uses;
 - iv. The school is located on a site of at least five acres; and
 - v. In no event may the height of a structure or portion of a structure exceed 75 feet.
4. ~~The Director of Planning and Community Development~~ may administratively consider, approve or disapprove the addition of temporary, portable classrooms to existing public schools subject to the criteria set forth in Part 20.30E LUC for an Administrative Conditional Use.
5. Perimeter Landscaping. In lieu of the Perimeter Landscaping Requirements set forth in LUC 20.20.520.F.1, schools shall provide the following landscaping, subject to LUC 20.20.520.F.6:
- a. Schools in Residential Districts or in Transition Areas as Described in Part 20.25B LUC and in the Bel-Red Office/Residential Transition (BR-ORT) Land Use District.
 - i. Ten feet of Type III landscaping along the street frontage, except that 100 percent of the required trees may be planted with deciduous trees; and
 - ii. Ten feet of landscaping meeting the requirements of LUC 20.25B.040.C.2.c along interior property lines, unless more stringent requirements apply pursuant to this section.
 - b. Schools in All Other Districts.
 - i. Ten feet of Type III landscaping along the street frontage, except that 100 percent of the required trees may be planted with deciduous trees; and
 - ii. Ten feet of Type III landscaping along interior property lines.

Alternative landscaping may be approved by the Director of ~~Planning and Community Development~~ if the requirements of LUC 20.20.520.J are met.

6. Vehicular and Pedestrian Circulation. In addition to the requirements of LUC 20.20.590, school vehicular and pedestrian circulation shall satisfy the following criteria:
 - a. Vehicular and bus loading and unloading areas shall be designed to minimize impacts on traffic on public rights-of-way;
 - b. Parking areas shall be designed to minimize conflicts between pedestrian and vehicular movements; and
 - c. Opportunities shall be found for safe, convenient, and pleasant pedestrian connections to existing transit facilities. Where needed, shelters and lay-bys for transit vehicles shall be incorporated into site development.
7. Design Guidelines for Schools in Residential Districts and in Transition Areas. Schools in residential land use districts and in Transition Areas shall meet the site and building design standards set forth in LUC 20.25B.040.D through G and 20.25B.050.
8. Playfields. Schools are not required to provide on- or off-site playfields. Where playfields are provided, however, they must meet the following requirements:
 - a. On-site playfields developed to the limits of their property and which are adjacent to developed private property shall be fenced with an eight-foot-high fence; and
 - b. Travel to and from the school site to any off-site playfield must not be hazardous.

20.20.820 Solid waste disposal.

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B. Decision Criteria.

In addition to the decision criteria applicable to any permit required to construct or modify a solid waste disposal facility pursuant to general use charts contained in LUC 20.10.440 and use charts applicable to specific land use districts contained in Chapter 20.25 LUC, the City may approve, or approve with modifications, a proposal to construct or modify a solid waste disposal facility, provided the following standards are met:

1. All components of the solid waste disposal facility shall be contained within a building that is compatible with the architectural form of surrounding buildings.

This requirement shall not apply to those components of the facility that cannot effectively function inside of a building.

2. A transportation plan may be required. The City shall determine the level of detail to be disclosed in the plan such as estimated trip generation, access routes and surrounding area traffic counts, based on the probable impacts and/or scale of the proposed facility.
3. The facility shall be designed to include design and operation measures to control odor emissions and airborne pollutants, rodents, birds, and other vectors.
4. The location, design, use and operation of the facility shall comply with any applicable guidelines, rules, regulations or statutes adopted by state law, or any agency or jurisdiction with authority.
5. The location and design is consistent with any planning document under which the proposing agency, special district or organization operates, as determined by the person or body having authority to interpret such document.

20.20.840 Subordinate Uses.

A. Purpose.

The purpose of this section is to provide performance standards for subordinate uses, as defined in LUC 20.50.046.

B. Applicability.

The performance standards contained in this section shall be required for subordinate uses, as defined in LUC 20.50.046, whether or not establishment of the use is subject to a permit, approval, or land use review.

C. Performance Standards.

Subordinate uses as defined in LUC 20.50.046 shall be:

1. Located on the same lot or in the same structure as the permitted or principal use;
2. Functionally related to the permitted or principal use; and
3. Generally limited in size to 25 percent of the floor area devoted to the related permitted or principal use.

20.20.900 Tree retention and replacement.

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B. Applicability.

The requirements of this section shall be imposed any time a permit, approval, or review including land alteration or land development including subdivisions, short subdivisions or planned unit developments, a change in lot coverage, or a change in the area devoted to parking and circulation is required by the Bellevue City Code or Land Use Code. Subsection F of this section shall apply to permits for new single-family structures and for additions to impervious surface areas that exceed 20 percent when located on a single-family lot developed with a residential use. Subsection E of this section applies to the removal of any significant trees in the R-1 Land Use District located in the Bridle Trails Subarea.

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D. Retention of Significant Trees for Subdivisions, Short Subdivisions, Planned Unit Development, Change in Lot Coverage, or Change in the Area Devoted to Parking and Circulation, Excluding Areas Located in the R-1 Land Use District in the Bridle Trails Subarea and for New or Expanding Single-Family Structures.

1. Perimeter Landscaping Area. In the required perimeter landscaping area, as set forth in LUC 20.20.520.F.1, the applicant shall retain all significant trees which will not constitute a safety hazard. For properties located in Bel-Red Land Use Districts, refer to perimeter landscape development at LUC 20.25D.110. Area devoted to access and sight areas as defined in the Transportation Code (Chapter 14.06 BCC), and area to be cleared for required roads, utilities, sidewalks, trails, or storm drainage improvements is exempt from this requirement.
2. Site Interior.
 - a. In areas of the site other than the required perimeter landscaping area, the applicant must retain at least 15 percent of the diameter inches of the significant trees existing in this area; provided, that alder and cottonwood trees' diameter inches shall be discounted by a factor of 0.5. In applying the requirement for retention of significant trees, the Director shall consider the preservation of the following types of significant trees a priority:
 - i. Healthy significant trees over 60 feet in height;
 - ii. Significant trees which form a continuous canopy;
 - iii. Significant trees which contribute to the character of the environment, and do not constitute a safety hazard;

- iv. Significant trees which provide winter wind protection or summer shade;
 - v. Groups of significant trees which create a distinctive skyline feature; and
 - vi. Significant trees in areas of steep slopes or adjacent to watercourses or wetlands.
- b. The Director may approve retention of trees which do not meet the definition of significant trees as a contribution toward the sum of the diameter inches required under subsection D.2.a of this section if a group of trees and its associated undergrowth can be preserved.
3. Exemption. The provisions of this subsection which require retention of significant trees are not applicable in any Downtown Land Use District.
 4. The applicant shall utilize tree protection techniques approved by the Director during land alteration and construction in order to provide for the continual healthy life of retained significant trees.
 5. Reduced Parking Bonus. If the proposed landscape plan incorporates the retention of significant trees above that required by this section, the Director may approve a reduction of up to 10 percent of the required number of parking spaces if adequate parking will remain on the subject property, and if land area for the required number of spaces remains available for future development on the subject property.

~~20.20.920 Veterinary clinics or hospitals.~~

~~A veterinary clinic designed for the treatment and care of pet animals, shall be operated by a registered veterinarian. The animals must be confined within the exterior walls of the building at all times. Walls of interior court animal runs shall be a minimum of eight feet high. Interior court animal runs shall be roofed and if there are open air spaces between the top of the wall and the roof, they shall be enclosed with wire mesh. All rooms housing animals shall have ample natural or mechanical ventilation. There shall be no cremation or other disposal of dead animals on the premises. A veterinary clinic or hospital building shall not be located closer than 100 feet to an existing residence, residential district, restaurant, clinic or hospital for humans. The setback required above shall not apply in the case of a residence used by the veterinarian himself, or any caretaker or watchman on the same or an adjoining lot. Overnight keeping of animals shall occupy less than 25 percent of the space in a clinic and shall be isolated by soundproofing from all adjacent property and uses.~~

20.25B.020 Applicability.

A. General.

This chapter applies to any portion of property located in a district designated on the chart below as “Districts providing transition” which is located within 300 feet of property located in a district designated on the chart as “Single-family districts receiving transition” or within 150 feet of property located in a district designated on the chart as “Multifamily districts receiving transition.”

B. Limitations.

1. Where a transition area abuts a portion of I-90, I-405, SR 520, Burlington Northern Railroad right-of-way, or power transmission line which is located in a single-family or multifamily district, the City shall include that portion as part of the required width of the transition area.
2. If the applicant establishes that a minimum 150 feet width of greenbelt or native growth protection easement is permanently dedicated for nonbuildable purposes and is located in a single-family or multifamily district, the City shall include that portion as part of the required width of the transition area.
3. Development within any Downtown Land Use District is not subject to Transition Area Design District requirements (refer to LUC 20.25A.090, Perimeter Design District).
4. Development within the F1 Land Use District is not subject to Transition Area Design District requirements.
5. Development within the OLB-OS Land Use District is not subject to Transition Area Design District requirements where that property receiving transition is developed in a nonresidential use.
6. Development of a wireless communications facility is not subject to Transition Area Design District requirements.
7. Development within the Medical Institution Land Use District is not subject to Transition Area Design District requirements.
8. Development within the Bel-Red Land Use Districts is not subject to the Transition Area Design District requirements unless specifically made applicable pursuant to Part 20.25D LUC.

20.30D.150 Planned Unit Development plan – Decision criteria.

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E. Landscaping within and along the perimeter of the Planned Unit Development is superior to that required by this code, LUC 20.20.520 and landscaping requirements

applicable to specific districts contained in Chapter 20.25 LUC, and enhances the visual compatibility of the development with the surrounding neighborhood; and

20.30D.165 Planned Unit Development plan – Request for modification of zoning requirements.

The applicant may request a modification of the requirements and standards of the Land Use Code as follows:

A. Density.

1. General. The applicant may request a bonus in the number of dwelling units permitted by the underlying land use district (see general dimensional requirements contained in LUC 20.20.010, and district specific requirements contained in Chapter 20.25 LUC or 20.25H.045 for sites in the Critical Areas Overlay District).

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20.30D.170 Planned Unit Development plan – Limitation on authority to modify zoning.

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- B. Any provision of LUC 20.10.440, Uses in Land Use Districts and district specific requirements contained in Chapter 20.25 LUC; or

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Part 20.30F Design Review

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20.30F.145 Decision criteria.

The Director may approve or approve with modifications an application for Design Review if:

- A. The proposal is consistent with the Comprehensive Plan; and
- B. The proposal complies with the applicable requirements of this Code; and
- C. The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent; and

- D. The proposal is compatible with and responds to the existing or intended character, appearance, quality of development and physical characteristics of the subject property and immediate vicinity; ~~and~~
- E. The proposal will be served by adequate public facilities including streets, fire protection, and utilities; and,
- F. The proposal is consistent with any required Master Development Plan approved pursuant to Part 20.30V LUV.

20.30G.150 Limitation on authority.

The decision_maker may not grant a variance to:

- A. The provisions of LUC 20.10.440 or Chapter 20.25 LUC, establishing the allowable uses in each land use district; or

20.50.010 A Definitions.

Adult Day Care. An adult care service use providing regularly scheduled care for a period less than 24 hours. An adult care service use meeting the definition of an Adult Family Home as defined pursuant to RCW 70.128.010(1) does not fall within the scope of this definition.

Affordable Housing. Housing used as the primary residence of an affordable housing qualified household. The price of affordable units is based on that amount a household can afford to pay for housing, when household income is less than 80 percent of the median annual income, adjusted for household size, as determined by the United States Department of Housing and Urban Development for the Seattle Metropolitan Statistical Area, and when the household pays no more than 30 percent of household income for housing expenses. Households with income less than 80 percent of the median annual income, adjusted for household size, may purchase or rent these affordable units. In addition, affordable housing in the Bel-Red Subarea shall include for sale units that only are considered affordable up to 100% of the median annual income. The Director shall issue administrative rules for establishing the sales or rental price of affordable units and the eligibility of residents.

Animal Service. Retail sale, service, or veterinary uses in which health care, pet grooming, pet daycare, or hobby kennel, kennel, or boarding services for animals are provided.

20.50.012 B Definitions.

Boarding. An animal service, which may include hobby kennel, kennel, veterinary, or pet daycare uses.

20.50.014 C Definitions.

Conditional Use. A use, which is compatible with its surrounding area, permitted if approved by a Hearing Body. See LUC 20.10.440 and district specific land use charts contained in Chapter 20.25 LUC.

20.50.024 H Definitions

Health Club. ~~A use featuring exercise, sports and other active physical fitness conditioning with limited subordinate uses.~~ A place of business with equipment and facilities for exercising and improving physical fitness. A broader range of services such as restaurant and meeting rooms are not included, except when subordinate to the permitted use.

20.50.032 L Definitions.

Land Use. The use to which an area of land, or building thereon, is put; human activity taking place thereon. Categories of land uses in this Code are found in Chart 20.10.440 and district specific land use charts contained in Chapter 20.25 LUC.

Lot Coverage. The percentage of a lot which is built upon subject to the exclusions listed in LUC 20.20.012 and the exceptions contained in the dimensional requirements charts contained in LUC 20.20.010 and 20.25A.020. For reference to district specific regulations which govern lot coverage, refer to Chapter 20.25 LUC.

20.50.034 M definitions.

Multiple Function Building or Complex. A building or complex of buildings containing multiple activities such as office, warehousing, manufacturing, or services, as determined by the Director of ~~Planning and Community Development~~ pursuant to LUC 20.10.440 provided this definition does not apply to multiple function buildings or complexes located in Bel-Red Land Use Districts.

20.50.040 P Definitions.

Pet Daycare. An animal service that regularly provides care for animals, which may include boarding or kennel services.

Primary Structure. The structure on a site that houses the principal use. For residential uses, the primary structure houses the dwelling unit(s). For nonresidential uses, the primary structure houses the use undertaken on the site, as classified by LUC 20.10.440 and district specific land use charts contained in Chapter 20.25 LUC. Primary

structures do not include structures that contain only certain functions or equipment that support the principal use, such as sheds, garages, or mechanical equipment structures.

20.50.046 S Definitions.

Special Use. A use which requires a Conditional Use Permit, Administrative Conditional Use Permit, or planned unit development pursuant to LUC 20.10.440 and district specific land use charts contained in Chapter 20.25 LUC.

Subordinate Use. A use which is secondary or incidental to a permitted or principal use. These uses are governed pursuant to LUC Section 20.20.840, located on the same lot or in the same structure as the permitted or principal use, and which is functionally related to the permitted or principal use. Subordinate uses generally are limited in size to 25 percent of the floor area devoted to the related permitted use.

Part 20.25H Critical Areas Overlay District

I. SCOPE AND PURPOSE

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II. DESIGNATION OF CRITICAL AREAS AND DIMENSIONAL STANDARDS

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20.25H.045 Development density/intensity.

A. General.

For development in the Critical Areas Overlay District, the number of dwelling units per acre and the maximum floor area ratio for office space is determined pursuant to this section. The provisions of this section may not be modified through a critical areas report.

B. Dwelling Units per Acre.

The maximum density allowed for a site in the Critical Areas Overlay District is equal to the number of dwelling units per acre as specified in LUC 20.20.010, times the buildable area in acres, plus the dwelling units per acre times the total area of critical area and critical area buffer in acres times the development factor derived from subsection D of this section:

$[(DU/acre)(Buildable\ area\ in\ acres) + (DU/acre)(Total\ critical\ area\ and\ critical\ area\ buffer\ in\ acres)(Development\ factor)] = \text{Maximum dwelling unit potential}$

The Dwelling Unit per Acre calculation contained in this paragraph does not apply in the Bel-Red Land Use Districts.

C. Floor Area Ratio for Office Space.

1. Office floor area outside the Bel-Red Land Use Districts

a. The maximum allowable office floor area for a site which contains a critical area or critical area buffer is equal to 0.5 times the buildable area in square feet plus 0.5 times the total area in critical area and critical area buffer in square feet times the development factor derived from subsection D of this section:

$[(0.5)(Buildable\ area\ in\ square\ feet) + (0.5)(Total\ critical\ area\ and\ critical\ area\ buffer\ in\ square\ feet)(Development\ factor)] = \text{Maximum office development potential}$

2b. A property within the Critical Areas Overlay District is exempt from the sliding scale FAR requirement of LUC 20.20.010, Note 8.

The applicable maximum floor area ratio to the buildable area is 0.5 regardless of building square footage.

2. Floor area within the Bel-Red Land Use Districts

The maximum allowable floor area for a site which contains a critical area or critical area buffer is equal to the maximum FAR for the applicable Bel-Red Land Use District (refer to dimensional requirements of LUC 20.25D.080) times the buildable area in square feet plus the maximum FAR for the applicable Bel-Red Land Use District times the total area in critical area and critical area buffer in square feet times the development factor derived from subsection D of this section:

$$\frac{[(\text{Maximum FAR for the applicable Bel-Red Land Use District})(\text{Buildable area in square feet}) + (\text{Maximum FAR for the applicable Bel-Red Land Use District})(\text{Total critical area and critical area buffer in square feet})](\text{Development factor})}{1} = \text{Maximum development potential within the Bel-Red Land Use Districts.}$$

D. Development Factor.

The development factor is a percent credit to be used in computing the number of allowed dwelling units or the maximum allowed office floor area for a site within the Critical Areas Overlay District. The development factor is determined by figuring the percentage of the total site that is buildable area, divided by 100. The result should be rounded to the nearest hundredth. The following table illustrates the development factor:

Total site (as %)	Critical area and critical area buffer total (as % of total site)	Buildable area (as % of total site)	Development factor (% BA/100)
100	10	90	.9
100	35	65	.65
100	50	50	.5
100	75	25	.25
100	90	10	.1

ATTACHMENT 3

Bel-Red Subarea Design Guidelines

Bel-Red Urban Design Guidelines

A. Introduction

The Bel-Red Subarea Design Guidelines support and complement the community vision described in the Bel-Red Subarea Plan that is part of the city's adopted Comprehensive Plan. The Design Guidelines offer a flexible tool for quality and innovation. They do not prescribe specific design solutions or make rigid requirements. There are many ways to meet a particular guideline. The guidelines are a descriptive template for promoting and improving the urban character of the area without dictating or prescribing a specific style of theme.

Each individual guideline provides the following detail:

- **Intent:** An initial concise statement of the objective of the guideline
- **Guideline:** Explanatory text describing the details of the guideline
- **Recommended:** Textual and photographic examples of recommended development consistent with the intent of the guideline
- **Not Recommended:** Textual and photographic examples of development that does not meet the intent of the guideline

Visual examples are included as models for design and review purposes. They are intended to provide a means to effectively judge a building or project relative to the design criteria; they are not intended to be specific examples to be replicated.

B. Character and Site Guidelines

Purpose. These guidelines address the qualities that make the Bel-Red subarea unique. They consider what makes an area a special, distinct "place," not simply a group of individual buildings and streets.

1. Integrate the Natural Environment

a. Intent

Reinforce linkages and orient buildings to the Bel-Red Subarea's natural and landscaped features.

b. Guideline

Site and building design should capitalize on significant elements of the natural environment, Highland Community Park and planned park and open space, riparian corridors and wetlands. Designs should incorporate open space amenities for residents, employees and visitors. Depending on the location, this may be accomplished through integration of the natural environment with new development or providing a smooth transition between the natural and built environments.

c. Recommended

- i. Active and passive gathering places and walkways oriented toward parks and open, natural spaces
- ii. Clear and convenient public access to open space amenities
- iii. Elements that engage the natural environment where the sight, sound and feel of nature can be directly experienced
- iv. Buildings sited to take maximum advantage of adjacent public amenities
- v. Walkways and plazas paved with high-quality materials (such as brick or stone), and other architectural elements that use materials, colors and forms that are harmonious with the natural surroundings



Open space amenity that allows for the public experience of natural elements

d. Not Recommended

- i. Buildings that turn their back on open space amenities
- ii. Islands of 'native' planting schemes within large, automobile oriented parking lots



Inaccessible islands of planting that do not connect well to the built environment or public amenities

2. Promote Architectural Compatibility

a. Intent

New buildings should contribute to the quality and character of their architectural context.

b. Guideline

Buildings should “fit” with their architectural surroundings – relating to nearby buildings rather than calling attention to themselves through design excesses or novel variations. Architectural elements should enhance not detract from the area’s overall character.

c. Recommended

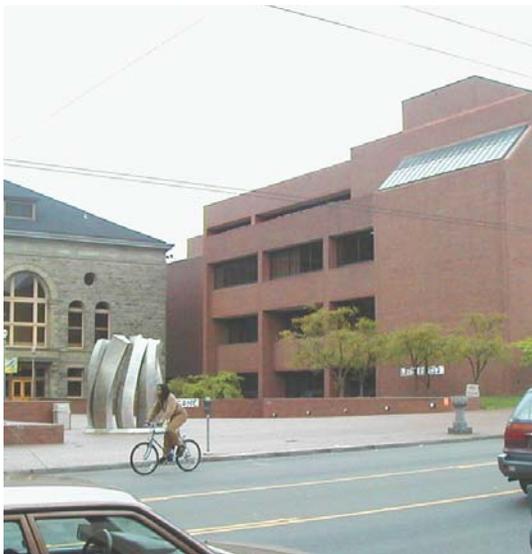
- i. Architectural elements used at a scale and level of detailing proportionate to the size of the building
- ii. Forms, proportions, rhythms, materials, colors and architectural motifs that are suggested by and complement adjacent buildings



Architectural elements fit into local context and overall character of the area

d. Not Recommended

- i. Out-of-scale, over-simplified, cartoon-like or other architectural elements applied without regard to size or use of the element
- ii. Building elements that do not respect the scale, materials, proportions and heights of adjacent high-quality buildings



Building on the right does not respect the scale, materials, proportions and character of adjacent area

3. Establish and Strengthen Gateways

a. Intent

Use architectural and landscape elements to mark transitions and entrances.

b. Guideline

Entrances into and within the Bel-Red Subarea should be celebrated at many levels. Pedestrians, cyclists, transit passengers, and motorists should experience a sense of “entering” or moving into the area as well as entry into unique districts or neighborhoods in the subarea.

c. Recommended

- i. New buildings designed to create gateways, using elements such as arches, arcades, pylons, columns, fountains and bridges
- ii. Signage, landscaping, lighting, or sculptural and artistic elements used to identify a gateway
- iii. Markers or inlaid art treatment in sidewalk paving to strengthen sense of entry into a particular

district or neighborhood

iv. Design elements that indicate a change or separation in transportation modes (i.e., from auto to pedestrian areas, or into transit stations)



Entrances to districts and among modes of transportation emphasized through design elements

d. Not Recommended

i. Gated, private compounds



Detailing of private spaces in lieu of gateway treatment

4. Protect and Enhance Surface Water Resources

a. Intent

Conserve water quality, natural hydrology and habitat, and preserve biodiversity through protection of water bodies and wetlands.

b. Guideline

Natural water systems regulate water supply, provide biological habitat and may provide recreational opportunities. Undeveloped ecosystems absorb the precipitation and convey only a small portion of rainfall as surface runoff. New and infill development should minimize disturbances to the on-site, adjacent, and regional natural water systems.

c. Recommended

- i. Grading and plan layout that captures and slows runoff
- ii. Pervious or semi-pervious surfaces that allow water to infiltrate soil
- iii. On-site landscape-based water treatment methods that treat rainwater runoff from all surfaces, including parking lots, roofs and sidewalks



Aesthetically pleasing development that minimizes adverse impacts to water systems

d. Not Recommended

- i. Buried, piped or culverted stream channels
- ii. Water quality enhancement projects that detract from the urban character of the area



Aesthetically pleasing development that minimizes adverse impacts to water systems

5. Integrate Art

a. Intent

Art in the Bel-Red Subarea should complement the character of a site, building or district as a whole. Art should be integrated into the design of the building or outdoor space.

b. Guideline

Large scale art in both public and private applications should bring focus to an outdoor space while small scale

pieces should bring detail to the pedestrian realm surrounding a building or site. At any scale, art should not overwhelm outdoor spaces or render buildings mere backdrops.

c. Recommended

- i. Artwork designed for and integrated into the building or site
- ii. Functional or interactive artwork
- iii. Durable materials that are vandal-resistant and designed to age well



Public art that complements the built environment and reinforces or creates a distinctive image of a place

d. Not Recommended

- i. Amateur art projects
- ii. Artwork used as advertising
- iii. Display conditions that detract from the artwork



Advertising in lieu of public art or art of poor quality that detracts from the urban character of the area

C. Pedestrian Emphasis Guidelines

Purpose. The pedestrian emphasis guidelines promote an environment where pedestrians are a priority. The highest consideration should be given to the ease and comfort of pedestrian movement and gathering places.

1. Define the Pedestrian Environment

a. Intent

A building should provide a continuous, visually rich pedestrian experience along its ground floor street front.

b. Guideline

The most important part of a building to a pedestrian is its ground floor – the lowest 15 feet of the facade, which a person experiences walking past or entering the building. This “pedestrian experience zone” should provide a sense of enclosure, and a continuous and comfortable street edge for the pedestrian. Ground floor building transparency should foster interaction between the public and private realms.

c. Recommended

- i. Windows that are transparent or have displays at the street level
- ii. Walls that create visual interest by using a variety of forms, colors and compatible cladding materials
- iii. Facades that provide a rhythm by using bays, columns, pilasters or other articulation at the street level
- iv. Signs and lighting at the ground level that complement the human scale



Building edges that maintain strong visual and physical connections to the sidewalk

d. Not Recommended

- i. Blank, flat, nondescript walls that are not articulated by any visual interest or detail at the street level
- ii. Uniform treatment of entire block face



Long, blank facade unconnected visually or physically to the street and sidewalk

2. Enhance the Pedestrian System

a. Intent

Establish the pedestrian as the priority, eliminating pedestrian barriers and ensuring that walking routes are convenient, direct and pleasant.

b. Guideline

Pedestrian routes should be attractive, easy to use and encourage walking and activity. Sidewalks should be continuous, avoiding interruptions such as vehicle curbcuts or changes in direction or grade. The portion of the sidewalk dedicated to walking should be free of barriers such as utility poles, newspaper boxes, cafe tables and chairs, permanent planters, tree grates or other obstructions and clutter.

c. Recommended

- i. Direct pedestrian routes
- ii. Separate pedestrians from visual and other nuisances (e.g. trash dumpsters, loading docks, mechanical equipment, etc.)
- iii. Pedestrian routes that are safely integrated with the street system
- iv. Maintain pedestrian access where rights-of-way have traditionally been located
- v. Parking lot walkways
- vi. Mid-block pedestrian connections



Attractive, safe pedestrian routes that increase walkability and connectivity

d. Not Recommended

- i. Circuitous pedestrian routes
- ii. Pedestrian-only streets Permanent or temporary pedestrian route obstructions
- iii. Interrupted or discontinuous pedestrian routes



Unconnected, discontinuous, overly wide pedestrian-only walkways

3. Protect Pedestrians from the Elements

a. Intent

Provide pedestrians with protection from wind, sun, rain, sleet and snow.

b. Guideline

Awnings and canopies are encouraged along the ground floor of buildings to protect pedestrians from rain and snow and provide shade in summer.

The design of awnings and canopies should be an integral component of the building facade. Awnings should be in proportion to the building and sidewalk, and not so large as to impact street trees, light fixtures or other street furniture.

c. Recommended

- i. Fabric awnings
- ii. Horizontal metal canopies, especially if transom or clerestory windows are above storefront glazing
- iii. Glazed canopies
- iv. Weather protection follows pattern of storefronts



Weather protection that is well integrated with the design of the building

d. Not Recommended

- i. Backlit awnings
- ii. Oversized advertising or tenant signs on awnings



Out of scale canopy made of poor quality materials that serves primarily as signage

4. Create a Variety of Successful Outdoor Spaces

a. Intent

Provide comfortable and inviting outdoor spaces for a variety of activities during all hours and seasons.

b. Guideline

Outdoor gathering spaces should be inviting and maximize opportunities for use. They should be spatially well defined, inviting, secure, easy to maintain. They may be intimate and quiet or active and boisterous. All areas should work well for pedestrians and provide space for special events as well as passive activities.

c. Recommended

- i. Courtyards, squares and plazas with active adjacent ground floor uses
- ii. Buildings surrounding green spaces to give the space visual definition and vitality generated by active ground floor uses
- iii. Trees, shrubs and plants to help define walkways, create transitions from the park to the street and provide visual interest
- iv. Structures, pavilions and seating areas that are easily accessible, and feel safe and secure during day and evening hours
- v. Greenways or pedestrian walkways and courtyards in residential or office development areas



Well defined, comfortable and inviting outdoor spaces that offer varied opportunities for use

d. Not Recommended

- i. Pocket parks, forecourts and plazas without active uses along retail streets
- ii. Outdoor spaces separated from the street by visual barriers or change in grade
- iii. “Leftover” green spaces
- iv. Sunken plazas at disconnected from the edge of the street
- v. Courtyards, squares and plazas adjacent to parking lots and other inhospitable areas without appropriate landscaping



Plaza space adjacent to parking lot and street without appropriate landscaping or buffering

5. Provide Places for Stopping and Viewing

a. Intent

Provide comfortable and inviting places where people can stop to sit, rest and visit.

b. Guideline

People watching, socializing and eating are restful and pleasurable activities for the pedestrian; providing special places where they can do these activities increases the pedestrian's sense of enjoyment.

Seating and resting places can add vitality to the urban environment. People will use available seating in open, well-designed areas, not in secluded or awkward spots.

c. Recommended

- i. Formal (benches) and informal (e.g. wide steps, edges of landscaped planters and low walls) seating areas
- ii. Higher degree of seating areas near active retail establishments (esp. outside eating and drinking establishments and near food vendors)
- iii. Seating adjacent to pedestrian walkways
- iv. Places for stopping and viewing adjacent to and within parks, squares, plazas and courtyards
- v. Sense of separation from vehicular traffic



Comfortable seating near active retail establishments, streets, and outdoor spaces

d. Not Recommended

- i. Seating areas more than three feet above or below street grade
- ii. Seating areas adjacent to loading, service bays or storage areas
- iii. Seating areas that are hidden, secluded, dark or unsecured spaces behind or to the side of buildings
- iv. Seating areas along high vehicle traffic areas



Isolated seating areas separated from the street through grade changes or otherwise

D. Architectural Guidelines

Purpose. The architecture guidelines promote high quality development while reinforcing the area's sense of place and Northwest provenance by encouraging innovative design, construction techniques and materials that reflect the industrial roots of the area while emphasizing the emerging urban character of Bel-Red.

1. Encourage High Quality Materials

a. Intent

Create a sense of permanence in the Bel-Red Subarea through the use of high quality building materials.

b. Guideline

Quality wall materials can provide a sense of permanence and bring life and warmth to a neighborhood. Wall and building materials must enhance the street environment while maintaining compatibility with adjacent buildings. Articulation of wall materials should be bold, with materials that show depth, quality and durability. It should be apparent that the materials have substance and mass, and are not artificial, thin "stage sets" applied only to the building's surface.

c. Recommended

- i. Natural high quality materials such as brick, terracotta, cement stucco, and wood
 - ii. Natural or subdued building colors and limited use of bright accent trims
 - iii. Varied, yet compatible cladding materials
 - iv. Boldly articulated window and storefront trim
- finished concrete, stone,



Durable natural materials of high quality that enhance the street environment and are compatible with local context

d. Not Recommended

Building materials such as:

- i. Simulated rock or brick
- ii. Faux finishes
- iii. Synthetic stucco (EIFS)
- iv. Simulated wood siding, wood veneer, clapboard or other types of residential siding
- v. Aluminum, plastic or vinyl siding
- vi. Corrugated metal siding
- vii. Unfinished concrete or cinder block
- viii. Exposed concrete, metal or plastic



Unarticulated surface treatment without depth, quality or detail

2. Provide Interesting Building Massing

a. Intent

Use scale-defining devices to break up the longitudinal dimensions of buildings, creating a comfortable sense of enclosure by establishing an uninterrupted street edge.

b. Guideline

The length and breadth of a building should be friendly in scale and inviting to the pedestrian. Portions of a large building mass should be broken into smaller, appropriately scaled modules, with changes in plane indicated by

bold projections and indentations. This allows an overly large building to appear as smaller, side-by-side buildings. Vertical and horizontal elements should be used to create a human scale and form a coherent pattern providing visual interest to the pedestrian.

c. Recommended

- i. Break down long expanses of building frontage both horizontally and vertically.
- ii. A vertically articulated tripartite facade division – base, middle and top for buildings over three stories.
- iii. Upper floors set back.
- iv. Vertical articulation of windows, columns and bays.



Well articulated buildings with a clearly defined base, middle and top

d. Not Recommended

- i. Long unbroken volume along street facade.
- ii. Smooth, undifferentiated facade.
- iii. Horizontal banding and emphasized horizontal orientation of building elements – walls, doors and windows.



Unbroken volume of building that does not enhance the street environment and lacks human scale

3. Create Attractive Building Silhouettes and Rooflines

a. Intent

Building rooflines should enliven the pedestrian experience and provide visual interest with details that create forms and shadows.

b. Guideline

A building's silhouette should be compatible with the intended character of the area and enhance the streetscape. In some cases, it may be appropriate to mark an entryway with a distinct form, such as a tower, to emphasize the significance of the building entry. Roof massing should be simple yet detailed and articulated. For example, flat roofs may be appropriate if they have a cornice designed with depth and detail expressing the top of the building wall. Dormers set into sloped roofs may be appropriate. These forms provide visual interest and bring additional living space, light and ventilation to upper floor and attic spaces.

c. Recommended

- i. Dormer windows
- ii. Towers or similar vertical architectural expressions of important building functions such as entries
- iii. Varied roof line heights
- iv. Well-detailed cornices that have significant proportions (height and depth) and create visual interest and shadow lines



Well detailed building silhouettes that create visual interest and enliven the pedestrian's experience of the built environment

d. Not Recommended

- i. Unarticulated rooflines
- ii. Roof forms lacking depth and detail



Unarticulated roof lines

3. Foster Attractive Rooftops

a. Intent

Integrate rooftop elements into the building design. Guideline

Roof shape, surface materials, colors, and penthouse functions should all be integrated into the overall building design. LUC 20.20.525 provides guidance for rooftop mechanical equipment.

b. Recommended

- i. Rooftop penthouse occupied by residential or office spaces
- ii. Rooftop terraces and gardens
- iii. Green roofs that reduce stormwater runoff
- iv. Consolidated and screened mechanical units



Attractive rooftops well integrated into overall building design

c. Not Recommended

- i. Exposed rooftop mechanical or electrical units visible from public spaces
- ii. Telecommunications equipment, including satellite dishes, cell-phone towers or antennae visible from public spaces



Rooftop telecommunications equipment visible from public spaces

4. Promote Welcoming Residential Entries

a. Intent

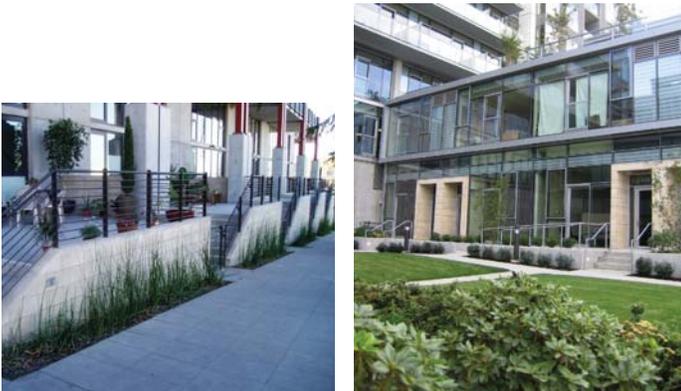
Residential entries should provide a graceful transition between the public and private realms.

b. Guideline

Residential entries should be substantial enough to suggest privacy yet welcoming to those who approach and enter. The overall character of the entry treatments will vary depending on street type. Entries on streets where sidewalk oriented development is required will have a higher degree of transparency, orientation towards the street, and design detail than other portions of the subarea.

c. Recommended

- i. Multi-panel painted doors
- ii. Doors combined with transom windows or side lights
- iii. Durable, high-quality metal door hardware
- iv. Wood solid core doors
- v. Doors accessed from canopy-covered entries
- vi. Moderate change of grade from sidewalk level to entry; more for townhouse-style housing than residential lobbies
- vii. Lobby entries to multi-family buildings providing double or multiple doors



Substantial entries with transparency and design details

d. Not Recommended

- i. Doors raised more than three feet above sidewalk level for townhouse-style housing
- ii. Unarticulated, flush doors or sliding glass doors
- iii. Doors accessed directly from parking lots
- iv. Door glazing with simulated divided lights
- v. Doors glazed with reflective or tinted glazing



Significant grade separation from the street and no transparency

5. Promote Visually Interesting Upper Floor Residential Windows

a. Intent

Upper floor residential windows should create an open and inviting atmosphere that add visual interest and enhances the experience of the building both inside and out.

b. Guideline

The windows of a residential building should be pleasing and coherent. Their size and detailing should be of a human scale with regular spacing and a rhythm of similarly shaped windows.

c. Recommended

- i. Multiple lights or divisions
- ii. Operable windows
- iii. Trim around framed openings
- iv. Windows recessed from building facade, not flush



Upper floor windows with a mixture of depth and articulation

d. Not Recommended

- i. Strips of windows with more of a horizontal than vertical emphasis
- ii. All windows flush with building facade



Simple entrance flush to facade without articulation

6. Design Inviting Retail and Commercial Entries

a. Intent

Design retail and commercial entries to create an open atmosphere that draws customers inside.

b. Guideline

Primary entries to retail and commercial establishments should be transparent, allowing passersby to see the activity within the building and bring life and vitality to the street. Architectural detail should be used to help emphasize the building entry.

c. Recommended

- i. Doors with a minimum of 50% window area
- ii. Building lighting that emphasizes entrances
- iii. Transom, side lights or other window combinations
- iv. Doors combined with special architectural detailing
- v. Double or multiple door entries
- vi. Well-detailed or ornate door hardware
- vii. Large cafe or restaurant doors that open the street to the interior by pivoting, sliding or rolling up overhead



Entry is transparent, opens easily and connects the street to the interior

d. Not Recommended

- i. Primary entry doors that are solid or windowless
- ii. Primary entry doors raised more than three feet above sidewalk level
- iii. Doors flush with the building facade without other articulation or canopy
- iv. Clear anodized aluminum frames
- v. Glazing with simulated divisions
- vi. Reflective, opaque or tinted glazing
- vii. Visual and physical obstructions near the entry



Glazing with simulated divisions that do not connect the sidewalk or street to the interior

7. Encourage Retail Corner Entries

a. Intent

Use corner entries to reinforce intersections as important places for pedestrian interaction and activity.

b. Guideline

Locate entry doors on the corners of retail buildings wherever possible. Entries at 45 degree angles and free of visual obstructions are encouraged.

c. Recommended

- i. Primary building entrance located at corner
- ii. Weather protection, special paving, and building wall lighting, to emphasize corner entry
- iii. Architectural detailing with materials, colors, and finishes that emphasize the corner entry
- iv. Doors with large glass areas with adjacent windows



Location of primary building entrance at corner and emphasized with details and architectural treatment

d. Not Recommended

- i. Visual and physical obstructions, especially utilities and columns



Visual and physical obstructions such as recessed entryway and grade separation

8. Encourage Inviting Ground Floor Retail and Commercial Windows

a. Intent

Use transparency to enhance visual interest and to draw people into retail and commercial uses.

b. Guideline

Retail and commercial uses should use unobstructed windows that add activity and variety at the street level, inviting pedestrians into retail and commercial uses and providing views both in and out.

c. Recommended

- i. Clear window glazing
- ii. Operable windows that open by pivoting, sliding or shuttering for restaurants and cafes
- iii. Painted wood, metal, and tile or stone clad panels below windows

iv. Transom windows



Transparent, visually interesting ground floor windows enhancing sidewalk activity

d. Not Recommended

- i. Exposed or unfinished window frame materials
- ii. Residential-styled bays, multi-paned divided lights, half-round or other similar forms
- iii. Tinted or reflective glazing
- iv. More than 8 feet between mullions
- v. Small scale windows



Tinted or reflective glazing or with no visual or physical connection to the sidewalk

9. Build Compatible Parking Structures

a. Intent

Use design elements so enhance the compatibility of parking structures with the urban streetscape.

b. Guideline

Parking structures should be designed so that their streetscape interface has a consistent form, massing and use of materials with the vision for the area. Any sidewalk facing parking garage frontages should be designed to appear like any other occupied buildings in the area. The horizontal garage form can be broken down by adding more wall surface and usable retail space, while retaining adequate garage ventilation.

c. Recommended

- i. Ground floor retail or other active uses

- ii. Small openings that may be glazed to function as windows
- iii. Stairways, elevators and parking entries and exits that occur at mid-block
- iv. Single auto exit/entry control point to minimize number and width of driveway openings (entry and exit points may be separated)
- v. Vertical expression of building structure
- vi. Cladding to disguise sloped floors from the outside view



Parking garage design resembles mixed use development, limits driveway opening and integrates ground floor retail uses along sidewalk and at building corner

d. Not Recommended

- i. Parked cars on the ground floor that are visible from adjacent sidewalks
- ii. Exposed sloped floors visible from the street
- iii. Stairways, elevators and parking entries and exits occurring at the building's corners, where retail is a more appropriate use
- iv. Horizontal expression of building structure



Clearly visible sloping floors, parked cars and structural expression of structure

E. Lighting Guidelines

Purpose. The lighting of buildings and open spaces should provide security, and also contribute to the character and overall sense of and vitality of the area.

1. Orient Lighting Toward Sidewalks and Public Spaces

a. Intent

Pedestrian-scaled lighting should be used to highlight sidewalks, street trees and other features, and harmonize with other visual elements in the subarea.

b. Guideline

Pedestrian-scaled lighting should be provided along pedestrian walkways and public open spaces. A single fixture type should be used throughout an area with slight variations allowed to identify smaller districts. Fixtures should be visually quiet as to not overpower or dominate the streetscape.

Lighting may also be used to highlight trees and similar features within public and private plazas, courtyards, walkways and other similar outdoor areas to create an inviting and safe ambiance.

c. Recommended

- i. Lighting of district design
- ii. Pole standards black or dark green in color
- iii. Dual-purpose standards (that accommodate pedestrian and vehicular fixtures)
- iv. Standards accommodating banners and hanging flower pots
- v. Lighting to highlight landscape areas
- vi. Fixtures concealed and integrated into the design of buildings or landscape walls and stairways
- vii. Footlighting that illuminates walkways and stairs
- viii. Energy efficient lighting
- ix. Bollard lighting that is directed downward toward walking surfaces
- x. Festive lighting along signature streets on buildings and trees



d. Not Recommended

- i. Flashing or colored lights
- ii. Exposed wires, outlets or other electrical devices that may provide safety hazards and are unsightly
- iii. Exposed light source (bulbs should not be visible)
- iv. Low pressure sodium lamps



Tall auto oriented lighting fixture

2. Integrate Building Lighting

a. Intent

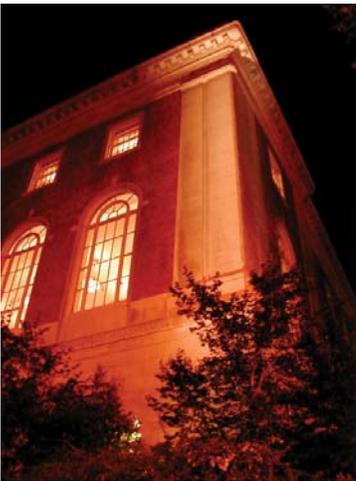
Architectural lighting that enhances and helps articulate building design, including illumination of cornices and entries, uplighting and other effects.

b. Guideline

Exterior lighting of buildings should be an integral component of the facade composition. Lighting should be used to create effects of shadow, relief and outline that add visual interest and highlight aspects of the building. Lighting should not cast glare into residential units or onto adjacent lots or streets in any way that decreases the safety of pedestrians and vehicles.

c. Recommended

- i. Wall-washing lighting fixtures
- ii. Decorative wall sconce and similar architectural lighting fixtures
- iii. Screened uplight fixtures on buildings or integrated with landscape
- iv. Lighting that provides natural color



Screened uplight fixtures on buildings

d. Not Recommended

- Flashing or colored lights
- i. Exposed light source (bulbs should not be visible)
 - ii. Fluorescent tube lighting
 - iii. Security spotlight
 - iv. Low pressure sodium lamps



Exposed security spotlight

F. Sign Guidelines

Purpose. Signs may provide an address, identify a place of business, locate residential buildings or generally offer directions and information. Regardless of their function, signs should be architecturally compatible with and contribute to

the character of the subarea. Signs should be good neighbors – they should not compete with each other or dominate the setting due to inconsistent height, size, shape, number, color, lighting or movement. Signs can contribute significantly to a positive retail and pedestrian environment, improve public safety perceptions and reinforce a sense of place.

1. Consider Size and Placement of Wall Signs

a. Intent

Signs that are sized and placed so that they are compatible with a building’s architectural design and contribute to the character of the subarea.

b. Guideline

Signs should not overwhelm the building or its special architectural features. Signs should not render the building a mere backdrop for advertising or building identification. Signs should be good neighbors – they should not compete with each other or dominate the setting due to inconsistent height, size, shape, number, color, lighting or movement.

c. Recommended

- i. Signs incorporated into the building architecture as embossing, low relief casting or application to wall surfaces
- ii. Signs constructed of individual, three-dimensional letters, as opposed to one single box with cutout flat letters
- iii. Signs may be painted or made with applied metal lettering and graphics
- iv. Signs made of durable and long lasting materials
- v. Signs incorporating lighting as part of their design
- vi. Signs located above storefronts, on columns or on walls flanking doorways



Sign incorporated into design of building façade

d. Not Recommended

- i. The material, size and shape of signs that overwhelm, contrast greatly or adversely impact the architectural quality of the building
- ii. Roof top signs
- iii. Cabinet or bow signs
- iv. Backlit signs
- v. Painted window signs



Sign overwhelms the architectural quality of the building

2. Orient Hanging Signs to Pedestrians

a. Intent

Hanging signs that are oriented to the pedestrian and highly visible from the sidewalk. Hanging signs can contribute significantly to a positive retail and pedestrian environment and reinforce a sense of place.

b. Guideline

Signs should not overwhelm the streetscape. They should be compatible with and complement the building's architecture, including its awnings, canopies, lighting and street furniture.

c. Recommended

- i. Sign lighting that is integrated into the facade of the building
- ii. Signs constructed of high-quality materials and finishes
- iii. Signs attached to the building in a durable fashion

d. Not Recommended

- i. Signs interfering with sight lines, creating a safety hazard or obstructing views



Hanging signs incorporated into the building facade that are oriented to the pedestrian and clearly visible from the sidewalk