



DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT  
ENVIRONMENTAL COORDINATOR  
11511 MAIN ST., P.O. BOX 90012  
BELLEVUE, WA 98009-9012

## DETERMINATION OF NON-SIGNIFICANCE

**PROPONENT:** Craig Chaney, Merrick Lentz Architect

**LOCATION OF PROPOSAL:** 5220 164<sup>th</sup> Avenue SE

**NAME & DESCRIPTION OF PROPOSAL:** Calvary Chapel Eastside

Construction of a 31,942 gross square foot two-story church building with 18,592 square feet of underground parking. The site contains protected slopes exceeding 40% and a Type A wetland.

**FILE NUMBER:** 98-003376 LD

The Environmental Coordinator of the City of Bellevue has determined that this proposal does not have a probable significant adverse impact upon the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(C). This decision was made after the Bellevue Environmental Coordinator reviewed the completed environmental checklist and information filed with the Land Use Division of the Department of Planning & Community Development. This information is available to the public on request.

- There is no comment period for this DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's office by 5:00 p.m. on \_\_\_\_\_.
- This DNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on the DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's Office by 5 p.m. on 5/10/07.
- This DNS is issued under WAC 197-11-340(2) and is subject to a 14-day comment period from the date below. Comments must be submitted by 5 p.m. on \_\_\_\_\_. This DNS is also subject to appeal. A written appeal must be filed in the City Clerk's Office by 5 p.m. on \_\_\_\_\_.

This DNS may be withdrawn at any time if the proposal is modified so that it is likely to have significant adverse environmental impacts; if there is significant new information indicating, or on, a proposals probable significant adverse environmental impacts (unless a non-exempt license has been issued if the proposal is a private project); or if the DNS was procured by misrepresentation or lack of material disclosure.

Cave U Holland  
Environmental Coordinator

April 26, 2007  
Date

**OTHERS TO RECEIVE THIS DOCUMENT:**

State Department of Fish and Wildlife  
State Department of Ecology,  
Army Corps of Engineers  
Attorney General  
Muckleshoot Indian Tribe



**City of Bellevue**  
**Department of Planning and Community Development**  
**Land Use Division Staff Report**

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Proposal Name: Calvary Chapel Eastside

Proposal Address: 5220 164<sup>th</sup> Ave SE

Proposal Description: Application for Design Review and SEPA approval to construct a 31,942 square foot two-story church building with 18,592 square feet of underground parking in the Office zoning district.

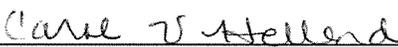
File Number: 98-003376 LD

Applicant: Craig Chaney, Merrick Lentz Architect

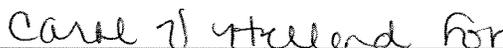
Decisions Included: Process II, Combined Design Review and SEPA

Planner: Matthews Jackson  
Senior Planner

State Environmental Policy Act  
Threshold Determination: **Determination of Non-Significance (DNS)**

  
\_\_\_\_\_  
Carol V. Helland  
Environmental Coordinator

Director's Decision: **Approval with Conditions**

  
\_\_\_\_\_  
Matthew A. Terry, Director  
Dept. of Planning & Community Development

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Notice of Decision Date: April 26, 2007  
Appeal Deadline: May 10, 2007

For information on how to appeal a proposal, visit the Permit Center at City Hall or call (425) 452-6864. Comments on State Environmental Policy Act (SEPA) Determinations can be made with or without appealing the proposal within the noted comment period for a SEPA Determination. Appeal of the Decision must be received in the City Clerk's Office by 5 PM on the date noted for appeal of the decision.

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## CONTENTS

I.	Request/Proposal Description.....	Pg #3
II.	Site Description, Zoning & Land Use Context.....	Pg #4
III.	Consistency with Land Use Code/Zoning Requirements.....	Pg #6
IV.	Public Notice & Comment.....	Pg #7
V.	Technical Review.....	Pg #12
VI.	State Environmental Policy Act (SEPA).....	Pg #15
VII.	Changes to Proposal Due to Staff Review.....	Pg #22
VIII.	Decision Criteria.....	Pg #22
IX.	Decision.....	Pg #24
X.	Conditions of Approval.....	Pg #24

### ATTACHED:

- Exhibit A: Location & Zoning Maps
- Exhibit B: Comprehensive Plan Policies
- Exhibit C: Project Plans & Drawings

## **I. REQUEST/PROPOSAL DESCRIPTION**

The applicant requests Design Review approval to construct a 31,942 gross square foot two-story church building with 18,592 gross square feet of underground parking in the Office zoning district.

### **A. Site Design**

Site improvements have been designed to complement the existing neighborhood context in which the new development will be located. The site is constrained by both steep slopes related to Lewis Creek on the east and a Type A wetland on the south adjacent to Lakemont Boulevard. Existing native vegetation will be retained within the protected areas and their primary buffers. A 20 foot landscape buffer is required along the street frontage adjacent to 164th Avenue SE. This buffer must be planted to meet Transition Area requirements that include a minimum of 5 trees per 1,000 square feet which must be a minimum of 10 feet high at planting. A Transition Area buffer will also be required along the northern property line within the 50 foot landscape buffer adjacent to three existing residential homes.

The new development will provide a total of 259 surface and underground parking stalls which will support an assembly capacity of 570 seats based on the Traffic and Parking Analysis prepared by TDA Inc, dated May 23, 2005 which assumes approximately 2.2 people per vehicle. According to the plans and drawings, proposed assembly capacity is 460 seats. Approximately 234 of the parking stalls will be permanent with 25 special event parking spaces provided within the drive aisle of the west lot.

Visually, the proposed development will appear "tucked" into this site as the finish floor level of the proposed building sits significantly below the road grade of both Lakemont Boulevard and 164<sup>th</sup> Avenue SE. The parking areas are sloped and terraced to minimize the amount of modification to existing topography and the quantity of cut and fill. An existing structure known as the lodge will remain in the northeast corner of the site. (See Exhibit C for a site plan).

### **B. Building Design**

The proposed building will provide a multi-purpose religious facility for Calvary Chapel Eastside including a multi-purpose room, classrooms, offices, and additional support facilities. The building has been designed to comply with the Lakemont design review criteria. Lapped wood siding is the primary wall siding material, while stucco siding is utilized to provide a solid visual base and shingle siding as an accent material in the gables. The stucco base with wood siding above is intended to reduce the vertical scale of the building by providing a successive transition of materials and textures. Extensive use of wall and roofline modulation helps to keep the project at a scale compatible with the surrounding neighborhood. The roofs are sloped and peaked, with hips, gables and overhangs, and the roofing material is gray metal standing seam. The sloped, peaked roofs surround flat roof areas, providing an architecturally screened location for any rooftop equipment, and these features also reduce the size, scale, and massing of the roofline. Trim is used extensively to add interest and detail to the building. The proposed main body colors are muted and subdued, with stronger accent colors used sparingly to add richness and depth to the overall color palette. (See Exhibit C for building elevations)

## II. SITE DESCRIPTION, ZONING, & LAND USE CONTEXT

### A. Site Description

The 4.6 acre (200,740 square feet) site is located at the northeast corner of the intersection of Lakemont Boulevard SE and 164<sup>th</sup> Avenue SE. Significant natural features on the site include a Type A wetland on the southern boundary of the site and Lewis Creek (Type A riparian corridor) with associated steep slopes along the entire eastern boundary. A significant percentage of the total 4.6 acres is encumbered by environmentally sensitive features (Approximately 1 acre or 20%). The construction permits associated with this proposal were submitted and deemed complete prior to the effective date of the recently approved Critical Areas Ordinance. Therefore, this proposal is vested to the critical areas regulations in place on the date of building permit submittal. (See Exhibit A for a vicinity map.)

### B. Zoning

The property is zoned Office and is located in the Transition Area and Sensitive Areas Overlay Districts. Development on the site is subject to the conditions of Concomitant Agreement No. 19689 as described in D.7 and D.11 below. The proposed religious facility is a permitted use in the Office zoning district.

### C. Land Use Context

This site sits among a mixture of residential, commercial, and public/park developments. Following is a summary of these uses which, along with Exhibit A, provide a description of the site context.

**East:** City NGPA (Professional Office) with Chevron beyond (General Commercial)

**West:** Lakemont View Condominiums (R-7.5) Hillside PUD (R-3.5)

**North:** Single Family Residential (R-3.5 and R-5)

**South:** Lewis Creek Park and other City owned open space (Professional Office)

### D. History

1. **May 31, 1983** – Adoption of Newcastle Community Plan by King County allowing development of up to two “villages” in the planning area.
2. **May 5, 1986** – Adoption by City of Bellevue of the Newcastle Subarea Plan (Res. 4735).
3. **July 28, 1986** – King County Council approval of North Village Master Plan development and related zoning and plat approval including conditions on Parcel D.
4. **June 6, 1989** – Resolution 5151 approving amendments to Newcastle Subarea Plan.
5. **October 2, 1989** – Annexation to City of Bellevue of “High Park” (now Lakemont) portion of North Village and establishment of zoning through Ordinances 4059 and 4060.

6. **June 12, 1991** – Reclassification of 2.9 acres (Parcel D) to PO, Professional Office, to allow continuation of Lakemont Development leasing offices.
7. **October 25, 1993** – Reclassification of 3.6 acres (Ordinance 4594), (Parcel D and adjacent Tax Lot 49 to the north) from PO/C and R-3.5/C to Office/C to allow churches without a Conditional Use Permit on Tax Lot 44 and allow increase in maximum allowable building height. Includes Concomitant Agreement No. 19689. Conditions of the Concomitant Agreement include:
  - a. Permitted uses shall be restricted to churches, day nurseries or day care facilities, parking (including park and pool), signs and landscaping as provided in KCC 21.12.
  - b. A 50 foot buffer shall be provided along the east, north and west boundaries as illustrated in Exhibit No. 204 as amended. Vegetation within this buffer area shall be enhanced to meet King County Type I standards.
  - c. The site will be properly screened to assure that no direct headlight illumination from parking areas will be directed toward any abutting single family residentially classified property. Site and facility lighting shall be directed, deflected, or screened in such a manner as to avoid glare or direct illumination upon any adjoining single family residentially classified property.
  - d. The architectural character, scale, massing, exterior materials, and finish of any structures other than churches shall reflect the residential community character.
  - e. Street frontage, parking lot and boundary landscaping shall be provided consistent with NCP Policy N-15 and KCC 21.51.
  - f. Any of the uses listed in condition a, above, which are permitted only as conditional uses in the Office district under the Bellevue Land Use Code will be subject to the conditional use process.
  - g. In the event that any term or clause of this Agreement conflicts with applicable law, such conflicts shall not affect other terms of this Agreement which can be given effect without the conflicting term or clause, and to this end the terms of this Agreement are declared to be severable.
  - h. Any amendment or modification of this Agreement shall be valid only if mutually agreed upon by the City of Bellevue and the Owner (including heirs, assignees and successors to the Owner) in writing and recorded with the King County Department of Records and Elections.
  - i. In addition to all other remedies available to the City by law, the City reserves the right to initiate proceedings to revoke the rezoning of the properties should the Owners fail to comply with any of the terms and conditions of this Agreement.
  - j. The City may at its discretion bring a lawsuit to compel specific performance of the terms and conditions of this Agreement.
  - k. Nothing in this Agreement shall be constructed to restrict the authority of the City to exercise its power to rezone the property in future years.
  - l. This Concomitant Agreement shall be binding on the heirs, assigns and successors of the Owner of the property.

The Ordinance also authorized the plat alteration of Lakemont Div. 2.

- 8. **December 6, 1993** - Adoption of Bellevue Comprehensive Plan including re-adoption of the Newcastle Subarea Plan.
- 9. **March 14, 1994** - Amended Plat of Lakemont Div. 2 recorded with King County Records removing 50 foot landscape buffer from northern boundary of Parcel D.
- 10. **March 18, 1996** – Ordinance 4860 modifies 50 foot landscape buffer to 20 feet along 164<sup>th</sup> Ave SE frontage of Parcel D and Tax Lot 49 (3.6 acres).
- 11. **November 21, 2005** – Ordinance 5638 rezones one acre Tax Lot 44 to Office and modifies Concomitant Agreement to include the property and move 50 foot landscape buffer to northern perimeter of the development site.

**III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS**

**A. General Provisions of the Land Use Code**

**1. Use**

Uses are regulated by Sec. 20.10.400 (Use Charts) and 20.25B (Transition Area Overlay District, Land use Code). The use proposed for this project is permitted in the Office zoning district , however, it is subject to Design Review approval since it is located within the Transition Area Design District.

**2. Dimensional Requirements**

As conditioned, all applicable dimensional requirements of the Land Use Code would be met. Refer to the following table for further information.

	<b>Permitted/Required</b>	<b>Proposed</b>
<b>Height (w/out bonus)</b>	40 feet (30' basic + 10' allowable height increase with basement parking per LUC 20.25.040.A)	34 feet 6 inches from average finish grade elevation to mid-point of the pitched roof.
<b>Lot Coverage</b>	35% (LUC 20.20.010)	10.1%
<b>Setbacks</b>	30 feet from the property line receiving transition (north)(LUC 20.25B.040.B.1) 30 feet (west and south) 25 feet (east)	180 feet 140 feet west, 275 feet south 82 feet east
<b>Parking</b>	(1 stall per 2 sanctuary seats, 476 seat assembly proposed, Per parking and traffic analysis) 238 Stalls	245 Stalls Maximum assembly of 490 seats

<p><b>Landscape</b></p>	<p>Compliance with LUC 20.20.520 and 20.25B.040.C</p>	<p>The landscape plan complies with all Land Use Code landscape requirements. <u>In addition, conditions of approval are included in section X of this report requiring landscape installation and maintenance assurance devices.</u></p>
<p><b>Tree Retention</b></p>	<p>15% diameter inches of existing significant trees on site</p>	<p>Significantly more than the minimum 15% diameter inches of significant trees will be preserved in the steep slope primary buffer, wetlands and buffer, and perimeter landscape areas.</p>
<p><b>Mechanical Equipment</b></p>	<p>Locate on the roof or below grade and visually screen, unless this requirement is modified by the City for projects requiring discretionary approval per LUC 20.20.525.C.5 &amp; 6.</p>	<p>All mechanical equipment will be located on the roof. The sloped, peaked roofs surround flat roof areas, providing an architecturally screened location for any rooftop equipment (refer to plan sheet A2.3).</p>

**IV. PUBLIC NOTICE AND COMMENT**

A Notice of Application and Public Meeting was published in the Eastside Journal on June 11, 1998. Notice was also mailed to each owner of property within 200 feet of the rezone area. In addition, a "Proposed Land Use Action" sign was installed on the site. As the proposal has evolved, notice of this application has been published in the Eastside Journal and King County Journal on May 4, 2000, May 18, 2000, November 18, 2004, December 16, 2004, January 13, 2005, and June 2, 2005.

Public meetings have been held in various locations in the city regarding the proposal on June 15, 1998, May 23, 2000, December 16, 2004, January 21, 2005, and June 14, 2005. The purpose of these meetings was to provide information on the associated rezone and this design review proposal and to solicit comments and concerns of the property owners or other interested parties in the area of the proposal.

Staff has received numerous letters and emails regarding this project. It should be noted that staff has received a significant amount of email in general support of having a new church in the neighborhood. The following summary of specific questions, comments, and responses is relevant to this application.

**1. Question/Comment: Has the church acquired more land since the original Village Master Plan was approved?**

***Response:** The church originally acquired 2.6 acre Parcel D in July of 1995 and one acre Tax Lot 49 in August 1995. The church then purchased one acre Tax Lot 44 in February 1997.*

2. **Question/Comment: The traffic generated by the church will create gridlock on Lakemont Blvd SE and 164<sup>th</sup> Ave SE.**

*Response: The Traffic and Parking Analysis prepared by TDA Inc. provides peak trip generation numbers for the proposed church development. Since the most significant trips will be generated by two Sunday services, the document provides an analysis of total and peak trips on Sundays. The report indicates total trip generation for a Sunday at 632 trips spread out over a period from 8:00 a.m. to 12:00 p.m. However, the new trips, anticipated to be approximately 271 in the Sunday peak hour (10:00 a.m. to 11:00 a.m.), are not expected to significantly change the area's calculated volume to capacity ratio.*

3. **Question/Comment: Traffic and surface parking will have negative light and noise impacts on adjacent residential neighborhoods.**

*Response: A fifty foot landscape buffer will be provided along the entire northern boundary of the proposed development. This buffer will be planted at a density to provide a significant buffer to adjacent neighborhoods. In addition, a minimum five foot fence will be required at the edge of the parking area south of the 50 foot buffer and a six foot fence will be required at the property line. Vegetation and fencing will absorb a substantial amount of the noise generated from vehicles using the subject property. The concomitant agreement contains the following language to strengthen these requirements: "The site will be properly screened to assure that no direct headlight illumination from parking areas will be directed toward any abutting single family residentially classified property. Site and facility lighting shall be directed, deflected, or screened in such a manner as to avoid glare or direct illumination upon any adjoining single family residentially classified property." Compliance with these requirements must be demonstrated before Temporary Certificate of Occupancy for the new facility. Refer to related condition of approval in Section X.*

4. **Question/Comment: The size of the church exceeds what was originally approved as part of the "Village" concept. The size of the church as proposed will generate regional trips that aren't consistent with uses intended to serve the neighborhood.**

*Response: The original proposal for Phase I and II for the church contemplated a total buildout of approximately 82,000 square feet. This scenario did not account for a large wetland located on the southern part of the property. The current proposal would have a gross area of approximately 31,942 square feet and a building footprint of approximately 22,000 square feet. As discussed earlier in this report, the church has acquired 2 additional acres beyond the original 2.6 acres of Parcel D that was included in the master plan. The applicants have argued, and staff concurs that the amount of buildable area available for this proposal is consistent with what was anticipated in the master plan as modified by the city council's approval of the rezone of Tax Lot 49 with Ordinance No. 4594, because the wetland area and its required buffers were not delineated until 2001.*

6. **Question/Comment: The church will provide an opportunity for Lakemont residents to congregate in their own neighborhood, therefore reducing traffic**

**impacts.**

**Response:** *While it is true that the presence of the church in the neighborhood is likely to attract new members from the immediate vicinity, it is likely that there will be proportional increase in membership from outside the neighborhood. The Transportation Department has reviewed this proposal and indicates that the impacts to the transportation system anticipated from approving the new facility can be adequately mitigated.*

7. **Question/Comment: The expense of the land in this neighborhood and the cost to provide infrastructure make it impossible to build a “small” church on this site.**

**Response:** *While it is likely that the costs to purchase property in this neighborhood and provide required infrastructure is significant, it is not one of the criteria used in determining the merits of a design review.*

8. **Question/Comment: Any reduction in landscaping will have a negative impact on adjacent residential development to the north and west.**

**Response:** *With Ordinance No. 4860, the 50 foot buffer required for Parcel D and Tax Lot 49 along the frontage of 164<sup>th</sup> Avenue SE was reduced to 20 feet. This buffer must be planted to meet Transition Area requirements that include a minimum of 5 trees per 1,000 square feet which must be a minimum of 10 feet high at planting. A Transition Area buffer remains in effect along the northern property line within the 50 foot landscape buffer on Tax Lot 49 and Tax Lot 44.*

9. **Question/Comment: The size of the church will have a negative impact on Lewis Creek, the wetland, and wildlife that inhabit the property.**

**Response:** *Both Lewis Creek and the wetland will have 50 foot primary buffers per the requirements of the land use code. No disturbance will be allowed within this area. Stormwater will be collected on site within a vault, treated for water quality, and ultimately released at rate intended to mimic pre-development forested conditions. There will be an expected loss in some of the wildlife activity on the site with development, however, the protected critical areas and their buffers will continue to provide suitable habitat for use by wildlife in the vicinity.*

10. **Question/Comment: Will the existing residence stay on the lot covered in the rezone?**

**Response:** *The church plans to continue to use the structure in its current configuration. The previously approved rezone of the lot that contains this structure will allow it to remain in its existing location and configuration as long as it is not expanded or enlarged within the 50 foot landscape buffer.*

11. **Question/Comment: How is a parking analysis done?**

**Response:** *The applicant was required to review the parking usage of similar*

*facilities in the area by actually counting cars at various times of day. For this proposal, the applicants reviewed parking patterns at other Calvary Chapel facilities as well as the findings from a parking analysis done at First Presbyterian Church of Bellevue.*

12. **Question/Comment: Is there going to be a school at the church?**

**Response:** *There are no plans for a school with this proposal. Calvary Chapel provides ministry within different age groups as indicated by the classrooms in the floor plans. This is not inconsistent with other religious organizations.*

13. **Question/Comment: How can you beautify a large parking area?**

**Response:** *The land use code requires landscape improvements in surface parking areas. For parking areas containing more than 99 parking stalls, the development must provide a minimum of 35 square feet of Type V landscaping for each stall proposed.*

14. **Question/Comment: The records on file show Phase II within the area indicated as wetlands. How can this happen?**

**Response:** *The wetland area and its primary 50 foot buffer are to be maintained in a natural undisturbed state. The proposal under review with this application and the design review has been referred to as Phase I. City codes will not allow any development, unless specifically allowed by the code, within designated critical areas. The church may exercise their right to appeal the City's conclusions with the design review decision. Any future development proposed for this site outside of the scope of this approval will be reviewed for compliance with all current codes and standards including the new critical areas ordinance approved in August 2006.*

15. **Question/Comment: The reduced size of the building responds to community concerns regarding neighborhood scale and traffic generation.**

**Response:** *As mentioned earlier in this report, the original proposal for the church has been reduced from approximately 82,000 gross square feet to 31,942 gross square feet. Requirements for landscaping and architecture to mimic residential development will help this development respond to the character of the neighborhood.*

16. **Question/Comment: Development in the Lewis Creek drainage basin already exceeds acceptable limits. How can additional impervious surface be approved in this area?**

**Response:** *The impervious surface will lead to the increase of volume and rate of discharge of surface water leaving the site. A detention vault has been included for the northern portion of the site in the proposal. The proposed detention vault will temporarily detain storm water runoff and release at pre-developed rates except for the 2 year design storm which will release at ½ the pre-developed runoff rate.*

17. **Question/Comment:** The proposed church is located adjacent to a fault line. How can new construction be allowed close to a fault and steep slopes which could result in structure failure and a potential landslide into Lewis Creek in the event of an earthquake?

*Response:* Additional information was requested from the geotechnical engineer regarding the seismic issues associated with this site. The geotechnical report states that the soils overlying the bedrock consist predominantly of dense glacial till-like soils that there is negligible risk for liquefaction to occur at this site during an earthquake. The report indicates that based on soil conditions and local geology, the 2003 International Building Code (IBC) site class should be C for structural design. Additional assessment of negligible risk is discussed in detail in Section VI of this report.

18. **Question/Comment:** The height of the proposed church at 40 feet is higher than the neighboring houses. This will overwhelm the neighborhood and the houses in Vuemont Meadows.

*Response:* In order to achieve a height greater than 30 feet in the office zone, the church has designed a pitched roof form and underground parking. These elements are intended to mitigate the additional height on adjacent properties. Other developments in the area are built to a similar height as the 40 feet proposed for the church and have not had a negative impact on the character of the neighborhood.

19. **Question/Comment:** The amount of vehicles generated by the church will create gridlock conditions. This may impede emergency vehicle passage during heavy traffic. Traffic that will be generated by this development is not consistent with the semi-rural feel of the neighborhood.

*Response:* It is estimated that the church will generate 21 peak period vehicle trips during normal commute hours. This impact is not considered significant and existing transportation infrastructure is available to accommodate these trips. Peak traffic generation for the church is expected during two Sunday services for a 2 and a half hour period from approximately 10 am to 12:30 pm. There are typically few background trips generated during this time period. Methods used for calculating trip generation are consistent with industry standards. The consultants assumed 5% yearly growth in projecting traffic impacts. Traffic in this area is analyzed based on an urban, not rural standard.

20. **Question/Comment:** The environmental impacts of this development on the stream and wildlife are significant. Even with the proposed detention facility, pollution will accumulate on the site. The additional vehicle trips and resulting exhaust fumes will increase CO levels.

*Response:* The proposed development is required to comply with all applicable environmental regulations in city codes and standards. This will result in approximately an acre of the total site area being left undeveloped in order to protect steep slopes associated with Lewis Creek and a wetland that was delineated on the site in 2001. Required stormwater detention and treatment is required to mimic the

predeveloped forested condition with its release rate and pollution levels. As conditioned, the proposal satisfies all applicable environmental codes and standards. Impacts to wildlife who are sensitive to edge effects and human activity can be expected as a result of this development. These impacts will be partially mitigated by the preservation of natural areas that buffer the most sensitive portions of the site. Although impacts to wildlife are expected, they aren't considered significant.

## V. TECHNICAL REVIEW

### A. Planning & Community Development Dept/ Clearing & Grading Division

The Clearing & Grading Division has no comments or requirements for this Design Review. All comments are reserved for construction permit review.

### B. Utilities Department

The Utilities Department has reviewed the proposal and has the following comments:

1. The City of Bellevue water and sewer systems have adequate capacity for this proposal.
2. The Utilities Department approval this application is based on a conceptual utility design review only and there are no implied approvals of the engineering components of the proposal. Final civil engineering of the utility design may require changes to the site layout or size of buildings to accommodate the utilities. The water, sewer and storm drainage shall be designed per the current City of Bellevue Utility Codes, Utility Engineering Standards and the Utility Developer Extension Agreement. All design review, approvals and inspection shall be performed under the Utility Developer Extension Agreement application.

See Section X of this report for Utilities Department related Conditions of Approval.

### C. Transportation Department

#### Site Access

Access to the proposed project will be provided via two driveways on 164<sup>th</sup> Avenue SE. Left turns out of the northern driveway must be prohibited, since that driveway does not meet sight distance requirements to the north. The other driveway on 164<sup>th</sup> Ave shall be the primary access to the site, and shall be located opposite the existing driveway serving the development across the street to the west. Unrestricted turns will be allowed for the primary access on 164<sup>th</sup> Avenue SE. No access will be allowed on Lakemont Blvd. No on-street loading or parking will be allowed in order to protect the flow of through traffic on arterial streets. See Section X for related Condition of Approval.

#### Street Frontage Improvements

In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements with a consistent and attractive appearance, the construction of street frontage improvements is required as a condition of development approval. The design of the improvements must conform with the requirements of the

Americans with Disabilities Act and the Transportation Development Code (BCC 14.60), and the provisions of the Transportation Department Design Manual.

Large commercial and institutional developments are typically required to improve their entire street frontage, unless satisfactory frontage improvements already exist. The Calvary Chapel site has total street frontage of approximately 900 feet for a site totaling 4.62 acres; however, satisfactory frontage improvements already exist on more than half of the frontage. The developer is required to install standard frontage improvements for approximately 430 feet, filling in a gap where frontage improvements are absent on 164<sup>th</sup> Avenue SE. As a comparison, the only other large institutional development in the Cougar Mountain area in recent years is Open Windows School, which was required to install full frontage improvements on approximately 1100 feet of frontage for a site of 7.62 acres. The frontage improvements required of Calvary Chapel are moderate in comparison to Open Windows School.

Specific street frontage requirements include:

1. No new frontage improvements are required on Lakemont Blvd, except that the handicapped ramp at the street intersection must be upgraded by installing a yellow "detectable warning" panel as shown on standard drawing TE-12.
2. On 164<sup>th</sup> Avenue SE, a combined street tree and streetlight plan is required for review and approval prior to completion of engineering and landscape plans. The goal is to provide the optimum number of street trees while not compromising the light and safety provided by streetlights. The streetlight plan and specifications must be based on light level analysis along the frontage on 164<sup>th</sup> Avenue and produced by a licensed engineer with knowledge of city standards and specifications. Appropriate details from the Transportation Department Design Manual and Appendix must be copied exactly in the streetlight plan and specifications.
3. On 164<sup>th</sup> Avenue SE, the existing curb, gutter, sidewalk, and planter strip near the intersection with Lakemont Blvd may remain in place. North of the end of those existing frontage improvements, the developer shall install new curb, gutter, sidewalk, and planter strip with landscaping. The face of the new curb shall typically be 36 feet from the existing curb on the opposite side of the street, except where the street width changes approaching Lakemont Blvd. The eastern half of 164<sup>th</sup> Avenue shall be typically striped with a 12-foot vehicle lane and 5-foot shoulder, which the city intends to designate in the future as a bike lane. Near the north end of the site, the striped lanes, curb, gutter, sidewalk, and planter strip shall taper gradually as needed to match existing improvements farther north. Adjacent to the island at the intersection of 164<sup>th</sup> and Lakemont, the striped lanes may narrow as appropriate to fit within the available space, although the vehicle lane should be not less than ten feet and the shoulder lane should be not less than four feet.
4. The design and appearance of the sidewalk on 164<sup>th</sup> Avenue SE shall comply with the standards and drawings in the Transportation Department Design Manual, including standard drawing TE-11, which must be included in the engineering plans. The new sidewalk shall be eight feet wide, except where it tapers down to match the existing sidewalk north of the site. The eight-foot width may be reduced to six feet if

such a reduction will significantly reduce negative impacts of grading and fill along the frontage.

5. New landscaping, including street trees, within the planter strip on 164<sup>th</sup> Avenue SE, shall be irrigated with a metered water source.
6. The southern driveway on 164<sup>th</sup> Avenue SE shall have an approach width, as defined in standard drawing DEV-6, of 36 feet. The driveway shall be striped for two exit lanes and one entry lane. The northern driveway shall have an approach width of 28 feet, although the driveway may narrow to 24 feet behind the sidewalk. The driveway apron design for both driveways shall be consistent with standard drawing DEV-6. The grade of the driveways shall not exceed seven percent for 30 feet behind the sidewalk. Grade changes in the driveways shall be rounded to prevent vehicles from bottoming out.
7. As much as feasible, no new utility vaults that serve only one development will be allowed within a public sidewalk. Vaults serving a broader public purpose may be located within a public sidewalk.
8. No fixed objects, including fire hydrants, trees, and streetlight poles, are allowed within ten feet of a driveway edge, defined as Point A in standard drawing Dev-6. Fixed objects are defined as anything with breakaway characteristics stronger than a 4-inch by 4-inch wooden post.
9. No new overhead utility lines will be allowed within or across any right of way or sidewalk easement, and existing overhead lines along the frontage on 164<sup>th</sup> Avenue must be relocated underground.
10. The frontage of 164<sup>th</sup> Avenue SE must have a storm drainage system meeting the standards of the Utilities Department.

See Section X for Transportation related Conditions of Approval.

### **Use of the Right of Way**

Applicants often request use of the right of way for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right of Way Use Permit for such activities must be acquired prior to issuance of any construction permit including demolition permit. This permit is issued directly by the Transportation Department. See Section X for related Conditions of Approval.

### **Pavement Restoration**

The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration Program, every street in the City of Bellevue has been examined and placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are, "No Street Cuts Permitted", "Overlay Required", and "Standard Trench Restoration". Each category has different trench restoration

requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that often comes with small strip patching.

Presently, 164<sup>th</sup> Avenue is classified as "Standard Trench Restoration", although it is scheduled to be repaved in 2009. Lakemont Blvd was repaved earlier in 2006. After repaving, streets are classified as "no-cut" for five years. If trenching within a no-cut street cannot be avoided, then extraordinary pavement restoration will be required. See Section X for related Conditions of Approval.

#### **D. Planning & Community Development Dept/ Building Division**

The Building Division has no comments or requirements for this Design Review. All comments are reserved for construction permit review.

#### **E. Fire Department**

The Fire Department has reviewed the proposal and has the following comments:

1. Provide automatic fire sprinklers throughout the building designed per NFPA 13.
2. Provide a Fire Department Connection at least 50 feet from the building at an approved location by the Fire Department.
3. Provide a fire hydrant within 50 feet of the Fire Department Connection.
4. Provide a fire alarm notification system throughout the building designed per NFPA 72.

See Section X of this report for Fire Department related Conditions of Approval.

### **VI. STATE ENVIRONMENTAL POLICY ACT (SEPA)**

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the proposal. Therefore, issuance of a Determination of Non-Significance (DNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements, with incorporation by reference of the *2006-2017 Transportation Facilities Plan Final Environmental Impact Statement* (TFP EIS) updated November 2006. This document is available in the Department of Planning and Community Development Records Room, Bellevue City Hall, 450 110<sup>th</sup> Ave NE. Transportation-related impacts associated with the (enter project name here) project are consistent with the potential projected impacts analyzed in the 2006-2017 TFP EIS.

Adverse impacts which are less than significant are usually subject to City Codes or Standards which are intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code Sec. 22.02.140 provides substantive authority to mitigate impacts disclosed through the environmental review process.

## **Earth**

A Preliminary Geotechnical Report was prepared by Terra Associates, Inc, on February 15, 2000, and revised on May 8, 2006. This report indicates that soils at the site consist of several inches of organic surface soils overlying undocumented fill and native dense to very dense fine grained silty sand with gravel. Siltstone/sandstone bedrock was encountered in several test pits and each boring at depths ranging between approximately 4 and 13 feet below the ground surface. This report indicates that the soil and groundwater conditions observed at the site would not preclude development of the property, as proposed. Suitable bearing for standard spread footing foundations would be provided by undisturbed native soil subgrades or compacted structural fill. Special excavation techniques will be required in order to remove bedrock in the lower part of the detention vault excavation.

Development of this site will require 8,700 cubic yards of fill and 6,100 cubic yards of cut. Next export of earth is estimated at 2,600 cubic yards. The environmental checklist indicates an average slope in the development area of 10% – 12% and the steepest slopes approaching 50% are located adjacent to Lewis Creek on the east side of the property. Fill slopes associated with road construction located at the intersection of Lakemont Boulevard and 164<sup>th</sup> Avenue SE.

Moderate to heavy groundwater seepage was found in two test pits and light groundwater seepage was found in several others in both 2000 and 2006 field investigations. This seepage occurred from sandy zones within the glacial soils at depths about five to thirteen feet below existing surface grades. The geotechnical report characterizes the observed groundwater conditions as typical for sites underlain by glacial till and/or bedrock. The occurrence of groundwater seepage and the volume and rate of groundwater flow will tend to be less during summer and early fall.

The geotechnical report states that the soils overlying the bedrock consist predominantly of dense glacial till-like soils that there is negligible risk for liquefaction to occur at this site during an earthquake. The report indicates that based on soil conditions and local geology, the 2003 International Building Code (IBC) site class should be C for structural design. In order to address concerns regarding the seismic safety of this site expressed in public comments from a citizen, staff requested additional information regarding seismic considerations for this site. Terra Associates, Inc. responded to these considerations on December 13, 2006. The following is an excerpt from the Terra Associates, Inc. response:

**1. Provide an opinion on the seismic level of ground motion during a major earthquake.**

**Response:** The USGS zip code earthquake ground motion hazard web page provides ground motion hazard values expressed as a percent of the acceleration of gravity (%g) for any 5 digit integer zip code. The peak ground acceleration (PGA) for the site (zip code area 98006) is about .32g. This value is the PGA with a 10 percent probability of exceedance in 50 years.

Considering the planned development area is underlain by sedimentary bedrock

at relatively shallow depths, we do not expect that significant amplification of the ground movements will occur.

**2. Describe any seismic risks associated with an earthquake such as liquefaction, internal spreading, landslides, or flooding.**

**Response:** As discussed in our referenced geotechnical report dated May 8, 2006, it is our opinion that the risk for liquefaction to occur at this site during an earthquake is negligible. The planned development area is underlain predominantly by dense glacial till-like soils, which overlie sedimentary bedrock at relatively shallow depths. Considering this, and the gentle to moderate surface grades at the site, it is also our opinion that the risk for lateral spreading and landslides to occur at the site during a severe seismic event is negligible.

Based on our study, it is our opinion that conditions do not exist at the site that would create a risk of flooding during a severe seismic event.

**3. Describe how design and construction recommendations will reduce or eliminate the impacts of seismic risks.**

**Response:** The reinforced fill/rockery wall designs presented in our referenced geotechnical report dated May 8, 2006 are based on analyses completed using methods encoded in the computer program MSEW v. 2.0 published by ADAMA Engineering, Inc. The wall designs provide adequate minimum safety factors against instability for both global and internal stability considerations for the static and seismic conditions.

See Section X for a Condition of Approval related to the recommendations included in the geotechnical report.

**Water**

The site is located in the Lewis Creek Drainage Basin. Storm water from the southerly portion of the site drains to the Lakemont Detention Treatment Facility which provides detention, nutrient and conventional pollution mitigation then discharges into Lewis Creek. The northern portion of the site sheet flows to the east and discharges into Lewis Creek. Lewis Creek then flows north and discharges into Lake Sammamish.

Development of the site will increase the amount of impervious surface on site. The impervious surface will lead to the increase of volume and rate of discharge of surface water leaving the site. A detention vault has been included for the northern portion of the site in the proposal. The proposed detention vault will temporarily detain storm water runoff and release at pre-developed rates except for the 2 year design storm which will release at ½ the pre-developed runoff rate.

The proposed development is expected to adversely affect water quality on the site. Pollutants such as sediment, oil, grease, herbicides, pesticides and fertilizers could be expected to enter storm water from the driving surfaces and landscaped areas. This site drains to Lake Sammamish which has been identified as sensitive to nutrient loading.

Dead storage in the vault along with Contech's canister storm water treatment system with the use of perlite as the treatment media is proposed to mitigate for nutrient and conventional pollutant removal prior to releasing storm water from the site. The proposed mitigation is consistent with the City of Bellevue and Washington State Department of Ecology Standards. See Section X for stormwater related conditions of approval.

A Type A wetland is located on the southern portion of Parcel D. This feature was originally delineated and flagged by Talasaea Consultants, LLC on August 2, 2001. Cooke Scientific Services conducted a peer review and verification of the wetland boundaries on September 6, 2001. This review indicated a general agreement with the boundaries identified by Talasaea. The identified wetland was approximately 9,652 square feet in area and is located adjacent to the bank of the ravine leading to Lewis Creek.

Subsequent reports and documentation submitted by the applicant contested the regulatory status of this wetland by suggesting that the wetland was created or enlarged through human intervention potentially as the unintended result of road construction with an upland area. Subsequent peer review by the City's consultant dismissed that contention absent definitive evidence that the primary source of water from a seep in the road grade is a result of failed infrastructure. Therefore, the wetland will be regulated as a Type A wetland per the definition found in LUC 20.50.054.

### **Noise**

Given the project site's close proximity to residential uses, consideration of construction noise will be important. The use of the best available technology to mitigate noise impacts on sensitive receivers needs to be incorporated into the construction phase of this proposal. Refer to Section X of this report for a conditions of approval pertaining to construction noise and the use of the best available technology to mitigate construction noise impacts.

### **Light and Glare**

The concomitant zoning agreement attached to this site requires proper screening to assure that no direct headlight illumination from parking areas will be directed toward any abutting single family residentially classified property. The single family properties to the north of the site will be screened by a sight obscuring wood fence and 50 foot landscape buffer. Exterior building lights and parking area lights shall be appropriately shielded to direct light on the church property and away from adjacent properties. Refer to Section X for a related condition of approval.

### **Transportation**

#### **Long Term Impacts and Mitigation**

The long-term impacts of development projected to occur in the City by 2017 have been addressed in the City's 2006 – 21017 Transportation Facilities Plan Final EIS, published November 2006. The impacts of growth which are projected to occur within the City by

2017 are evaluated on the roadway network assuming that all the transportation improvement projects proposed in the City's current Transportation Facilities Plan are in place. The Transportation Facilities Plan EIS divides the City into Mobility Management Areas (MMAs) for analysis purposes. The Calvary Chapel (an "institutional" land use) lies within MMA # 11, for which no institutional growth has been projected in the TFP EIS. It is recognized that the TFP projections fall short in this area in terms of this proposed land use; however, the TFP is updated every two years at which time land use projections can be updated to meet current growth trends. With this considered, the long-term transportation impacts are fully mitigated by payment of traffic impact fees, as described below.

Most developments are required to pay transportation impact fees for long-term mitigation. However, a 1989 pre-annexation agreement with the developers of High Park stated that off-site traffic mitigation was previously provided for that part of the Calvary property included in High Park. Since the High Park portion of the Calvary property includes the building footprint area, the Transportation Department will not require payment of a transportation impact fee. A copy of the pre-annexation agreement is available in the city's files for the Calvary Chapel development.

### **Mid-Range Impacts and Mitigation**

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more weekday PM peak hour trips undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained. According to the trip generation rates used in the city's transportation impact fee calculations, a religious institution generates an average of 0.66 new weekday PM peak hour trips per 1000 gross square feet. At 31,942 gross square feet, this development would generate approximately 21 new weekday PM peak hour trips. Therefore, concurrency testing is not required.

### **Short Term Operational Impacts and Mitigation**

Operational impact analysis for a new development usually focuses on the PM peak hour of weekdays. However, for a church, operational impacts are much more significant before and after Sunday services and a few large special events, such as weddings. Special events occur intermittently and are not always predictable; therefore, this analysis focuses on Sundays. Issues that were analyzed included trip generation, parking, site access operations, and nearby intersection level of service. The results of the traffic analysis are published in the Traffic and Parking Analysis, dated May 23, 2005, by TDA, Inc. The study is included in the city's files for this development. A summary of the traffic analysis and related issues follows:

- Trip Generation:  
For the PM peak hour on weekdays, the City of Bellevue uses the rate of 0.66 new trips per 1000 square feet for religious institutions. This rate is based on national average data published by the Institute of Transportation Engineers. As mentioned above, this indicates that weekday PM peak hour trip generation would be 21 trips for a church of 31,942 gross square feet.

For Sundays, TDA, Inc and Calvary Chapel staff collected data at several similar churches in the region. The number of people present at each church service and the number of people per vehicle were determined. Those figures were used to estimate that if Calvary Chapel constructs a church with maximum occupancy of 450, then 158 vehicles would be expected to arrive and depart for each regular church service. This assumes 70% church occupancy for a typical service and an average vehicle occupancy of two persons per vehicle (which is relatively conservative). With two services each Sunday, the highest hour on a typical Sunday morning is predicted to have 137 entering trips and 134 exiting trips, for a total of 271 peak hour trips. This assumes some traffic overlap between the two services, and that the traffic peak spreads out to some degree as people arrive early or leave late for various reasons.

For a few peak services each year, church occupancy would reach 100% or close to it. This would raise the trip generation estimate to 225 vehicles per service and up to 387 vehicle trips (in plus out) in the highest hour on a peak Sunday.

- Parking Demand and Supply:

The site plan shows 220 parking spaces, with room for another 24 vehicles parked in aisles for peak events. TDA, Inc estimates that peak parking demand at any one time on a typical Sunday would be 181 vehicles (assuming some overlap between services). Thus, the on-site parking supply will be adequate for most Sundays. For a few peak Sundays each year, parking demand is estimated to be as high as 292 vehicles at one time. Thus, as many as 48 vehicles would need to be accommodated off-site for peak Sundays. The applicant is required to develop a Traffic and Parking Management Plan, which will include a method for dealing with any parking overflow. This plan is discussed in the Mitigation section below.

- Safety and Operational Analysis for Access Points:

City code (BCC 14.60.240) requires 300 feet of sight distance for a commercial driveway on a street with a 30-mph speed limit, such as 164<sup>th</sup> Ave. For the site's northern driveway, the available sight distance to the north is approximately 240 feet, due to the curve of the street. Based on that and other factors, TDA, Inc recommends, and the Transportation Department concurs, that left turns out of the site's northern driveway should be prohibited. The developer will be required to install a right turn arrow in the outbound lane of the northern driveway and a right-turn-only sign adjacent to the driveway. The Traffic and Parking Management Plan required elsewhere in this document shall include a requirement to educate members regarding the reasons for not turning left out of the northern driveway.

TDA, Inc calculated average vehicle delay for the site's access points. The average delay for vehicles exiting to the south via the primary driveway is calculated to be 10.4 seconds per vehicle on a typical Sunday and 15.9 seconds per vehicle on a peak Sunday. Given the number of vehicles predicted to exit via that driveway, the total time to empty the parking lot might cause more vehicles than predicted to exit toward the north. The impacts of this change in trip distribution will be mitigated by educating drivers to not cut through residential neighborhoods. See the Mitigation section below for further discussion.

Some citizens expressed concern that traffic to or from the north might use SE 46<sup>th</sup> Street and 171<sup>st</sup> Avenue SE as an alternate route, cutting through a residential area. TDA, Inc indicated that this is unlikely to be a significant issue, because on Sundays when total traffic on arterials is light, use of low speed residential streets as a cut-through route would not be a beneficial time savings, compared to use of arterial routes. This will be further mitigated by educating church members not to use residential streets for cut-through.

- Level of Service for Nearby Intersections:

TDA, Inc calculated level of service and delay for the intersections of 171<sup>st</sup> Avenue SE and Lakemont Blvd and 164<sup>th</sup> Avenue SE and Lakemont Blvd. Traffic generated by the Calvary Chapel during the church's peak hour on Sunday would not change the level of service at 171<sup>st</sup> and Lakemont, but would change the level of service from level A to level B at 164<sup>th</sup> and Lakemont. At each intersection, church traffic would add about two seconds to the average delay per vehicle. This is not a significant negative impact.

- Other:

Some citizens were concerned about cumulative traffic problems if church traffic and traffic generated by events at Lewis Creek Park coincide. Major weekend events at parks tend to be on Saturdays or Sunday afternoons, while church traffic is primarily Sunday mornings. Therefore, the cumulative impact of church traffic and park traffic should not be a significant issue.

Another citizen concern was about overflow parking on nearby residential streets. According to TDA's analysis, on-site parking will be adequate almost all the time, and the church will develop an organized program for off-site parking at agreed locations for the small number of events for which on-site parking will not be adequate.

- Mitigation:

- a) The sight distance problem on the northern driveway will be mitigated by prohibiting left turns out, and by educating members about the reasons not to turn left out of the northern driveway.
- b) The church will develop and submit for city approval a Traffic and Parking Management Plan, which shall include:
  - o Educating members on how to deal with parking lot congestion.
  - o Training staff and volunteers on parking lot management.
  - o Educating members to not use local streets for parking or as cut-through routes. It is especially important to avoid neighborhood cut-through on local streets for members using streets north of the site.
  - o Educating members about the reasons for not turning left from the northern driveway.
  - o Handling overflow parking, including agreements to use off-site parking lots for peak events, and to use shuttles as needed.

See Section X for related Conditions of Approval.

## VII. CHANGES TO PROPOSAL DUE TO CITY REVIEW

### Site Design

The original proposal contemplated two phases of construction with gross square footage at build out exceeding 80,000 square feet. The wetland located on the site was delineated and classified in 2001. The presence of this wetland and its associated primary and structure setbacks has resulted in a significantly reduced size of the proposed structure. In addition, the buffer along the north boundary of Parcel D was relocated to the north property lines of Tax Lots 44 and 49. This 50 foot buffer will add significant new landscaping to the development.

Parking layout and onsite circulation was modified to address turning movements into and out of the site during peak use and to accommodate fire department access around the structure in the event of fire.

## VIII. DECISION CRITERIA

*The Director may approve, or approve with modifications, an application for Design Review if:*

### **1. The proposal is consistent with the Comprehensive Plan.**

This project is consistent with the Comprehensive Plan, which includes policies that address the goals of site and building design, landscaping, and open space. See Exhibit B for a listing of applicable Comprehensive Plan policies and discussion of how each is addressed by the proposal.

### **2. The proposal complies with the applicable requirements of this Code.**

As conditioned, the proposal complies with applicable requirements of the Land Use Code. Refer to Section III of this report for further information on Land Use Code consistency.

### **3. The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent.**

As conditioned, the proposal complies with all applicable Design Guidelines contained in LUC 20.25B.050, Transition Area Design District. Key points are summarized as follows:

#### **A. Site Design Guidelines**

**Site development should maximize the retention of existing significant vegetation in order to soften the visual impact on adjacent residential uses (LUC 20.25B.050.A.3).**

Approximately an acre of the 4.6 acre site will be undisturbed in its natural state due to the presence of a Type A wetland, Lewis Creek, and steep slopes. This will result in the retention of significant vegetation on the site with development than might have

otherwise been required. Additional significant trees will be retained within the required enhanced landscape buffer along the northern property line adjacent to existing single family residential uses.

**Surrounding vegetation, topography, street patterns, parking configuration, and building massing should be considered in order to result in a compatible fit between the proposed development and existing residential development (LUC 20.25B.050.A.4).**

The proposed building and site development respond to the context of the site as well as existing residential development. Residential development in the vicinity ranges from single family detached residential structures on lots ranging from 5,000 square feet to 37,000 square feet to a 78 unit condominium development in three structures. Significant landscaping and wood fence will screen the new facility from the only existing residential properties that share a common property line with the church. The new church is setback significantly from the property lines and will be no closer than 180 feet to the closest residential structure.

#### **B. Building Design Guidelines**

**Building surfaces facing abutting residential districts should be clad with materials which are similar to or compatible with surrounding uses, and which minimize reflected lighting (LUC 20.25B.050.B.1).**

The building surfaces are proposed to be clad with green and tan lapped wood siding material with tan stucco siding used to provide a solid visual base. The proposed colors are muted and subdued, with a few stronger accent colors. All proposed materials are non-reflective and compatible with abutting residential districts.

**Building facades should incorporate elements such as stepbacks, offsets, angled facets, deep roof overhangs, recesses and other architectural features which serve to break down the scale (LUC 20.25B.050.B.2).**

The proposed stucco base and wood siding above is intended to reduce the vertical scale of the buildings by providing a transition between materials and textures. Extensive use of wall and roofline modulation has been incorporated into the building design to break down the scale and mass of the structure.

**Pitched roof forms are preferred in order to enhance the compatibility with nearby residential uses (LUC 20.25B.050.B.3).**

The sloped, peaked roofs proposed with this structure comply with this guideline.

**Materials and colors used on the building facades should be compatible with nearby residential buildings and the surrounding natural environment (LUC 20.25B.050.B.5).**

The green and tan wood siding and eldorado stone accents proposed for this structure respond to the both the existing neighborhood character and nature backdrop for this

development.

**4. The proposal is compatible with, and responds to, the existing or intended character, appearance, and quality of development and physical characteristics of the subject property and immediate vicinity.**

As described throughout this report, the proposal has been designed to complement the character and quality of development in the vicinity as well as the physical characteristics of the site. The materials and architectural form of the proposed structure is of a quality consistent with existing development in the area. The preservation of significant environmental features on the site including a wetland, stream, and steep slopes adequately responds to the physical characteristics of the site.

**5. The proposal will be served by adequate public facilities including streets, fire protection, and utilities.**

All required public services and facilities are available to the site.

**IX. DECISION**

After conducting the various administrative reviews associated with the proposal, including applicable Land Use consistency, SEPA and City Code & Standard compliance reviews, the Director of Planning and Community Development does hereby APPROVE WITH CONDITIONS the subject proposal.

**X. CONDITIONS OF APPROVAL:**

The following conditions are imposed on the applicant under the authority referenced:

**A. GENERAL CONDITIONS: The following conditions apply to all phases of development.**

**1. CONSTRUCTION HOURS:**

Noise related to construction is allowed from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020C.1 when necessary to accommodate construction which cannot be undertaken during exempt hours. Prolonged exposure to noise created by extended hour construction activity is likely to have a significant impact on inhabitants of surrounding residential properties during the proposed timeline for construction. In order to minimize detriment on residential uses in the immediate vicinity of the project, the Contractor shall not rely on City issuance of a blanket exemption from the Noise Control Code during the construction period. Allowances for short term work outside of normal construction hours shall be limited and will be reviewed on a case by case basis to verify necessity and ensure appropriate noise

mitigation is utilized to protect surrounding uses and properties. Written requests for exemption from the Noise Control Code must be submitted two weeks prior to the scheduled onset of extended hour construction activity. Such request shall include a noise analysis prepared by a noise consultant, including recommendations for achieving the noise limitations of the Noise Ordinance for new residential construction.

Authority: Bellevue City Code 9.18.040  
Reviewer: Matthews Jackson, Planning and Community Development Department

**2. VEHICULAR ACCESS RESTRICTIONS**

Access to this site via the southern driveway on 164<sup>th</sup> Avenue SE will not be restricted. For the northern driveway on 164<sup>th</sup>, outbound traffic will be restricted to allow right turns only, but inbound traffic will not be restricted. The restriction will be implemented by installing a right turn arrow in the outbound lane and a right-turn-only sign adjacent to the driveway. No access will be allowed to or from Lakemont Blvd.

Authority: Bellevue City Code 14.60.150  
Reviewer: Carl Wilson, Transportation Department

**3. PRELIMINARY DESIGN APPROVAL**

The Utilities Department approval of the Design Review application is based on the preliminary utility design only. Final civil engineering of the utility design may require changes to the site layout to accommodate the utilities.

Authority: Bellevue City Code 24.02, 24.04, 24.06  
Reviewer: Don Rust, Utilities Department

**4. UTILITY CODES AND ENGINEERING STANDARDS**

The water, sewer, and storm drainage systems shall be designed per the current City of Bellevue Utility Codes and Utility Engineering Standards. Utilities Department design review plan approval and field inspection is performed under the Developer Extension Agreement and Utilities Permit Processes.

Authority: Bellevue City Code 24.02, 24.04, 24.06  
Reviewer: Don Rust, Utilities Department

- B. PRIOR TO CLEARING & GRADING PERMIT: The following conditions are imposed to ensure compliance with the relevant decision criteria and Code requirements and to mitigate adverse environmental impacts not addressed through applicable Code provisions. These conditions must be complied with on plans submitted with the Clearing & Grading or Demolition permit application:**

**1. COMPLIANCE WITH BELLEVUE CITY CODES AND ORDINANCES**

Compliance with all applicable Bellevue City Codes and Ordinances including but not limited to the following is required:

Clearing and Grading Code - BCC 23.76	Savina Uzinow, 425-452-7860
Bellevue Development Standards	"
Transportation Code - BCC 14.60	Carl Wilson, 425-452-4228
Trans. Improvement Program - BCC.22.16	"
Right-of-Way Use Permit - BCC 14.30	Jon Regalia, 425-452-4599
Bellevue Utilities Code - BCC Title 24	Don Rust, 425-452-4856
Construction Codes - BCC Title 23	Douglas Beck, 425-452-4563
Land Use Code - BCC Title 20	Matt Jackson, 425-452-2729
Sign Code - BCC Title 22B	"
Noise Control - BCC 9.18	"
Uniform Fire Code - BCC 23.11	Adrian Jones, 425-452-6032

**2. USE OF BEST AVAILABLE NOISE ABATEMENT TECHNOLOGY**

The use of best available noise abatement technology consistent with feasibility is required during construction to mitigate construction noise impacts to surrounding uses.

Authority: Bellevue City Code 9.18.020F  
Reviewer: Matthews Jackson, Planning and Community Development Department

**3. RIGHT-OF-WAY USE PERMIT**

Prior to issuance of any construction or clearing and grading permit, the applicant shall apply for applicable right-of-way use permits from the City's Transportation Department, which may include:

- a) Designated truck hauling routes.
- b) Truck loading/unloading activities.
- c) Location of construction fences.
- d) Hours of construction and hauling.
- e) Requirements for leasing of right of way or pedestrian easements.
- f) Provisions for street sweeping, excavation and construction.
- g) Location of construction signing and pedestrian detour routes.
- h) All other construction activities as they affect the public street system.

In addition, the applicant shall submit for review and approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevents access. General materials storage and contractor convenience are not reasons for preventing access.

**The applicant will secure sufficient off-street parking for construction workers before the issuance of a clearing and grading, building, a foundation or demolition permit.**

Authority: Bellevue City Code 11.70 & 14.30  
Reviewer: Jon Regalia, Transportation Department

#### **4. CIVIL ENGINEERING PLANS – TRANSPORTATION**

Civil engineering plans produced by a qualified engineer must be approved by the Transportation Department prior to issuance of the clearing and grading permit. The design of all street frontage improvements and driveway accesses must be in conformance with the requirements of the Americans with Disabilities Act, the Transportation Development Code, the provisions of the Transportation Department Design Manual, and specific requirements stated elsewhere in this document. All relevant standard drawings from the Transportation Department Design Manual shall be copied exactly into the final engineering plans. Specific requirements for the engineering plans include:

- a) Traffic signs and markings, including the fog line on 164<sup>th</sup> Avenue.
- b) Curb location to accommodate required street cross slope and width, including the specified vehicle lane and shoulder widths.
- c) Curb, gutter, sidewalk, planter strip, handicapped ramp, and driveway approach design. (The engineering plans shall be the controlling document on the design of these features; architectural and landscape plans must conform to the engineering plans as needed.)
- d) Installation of streetlights and related equipment. Street tree location must be coordinated.
- e) Sight distance. (Show the required sight triangles and include any sight obstructions, including those off-site.) Mature vegetation shall not create a sight distance obstruction.
- f) Location of fixed objects in the sidewalk or near the driveway approach.
- g) Trench restoration within any street right of way.

Authority: Bellevue City Code 14.60; Transportation Department Design Manual  
Reviewer: Carl Wilson, Transportation Department

#### **5. GEOTECHNICAL RECOMMENDATIONS**

The recommendations included in the Preliminary Geotechnical Report prepared by Terra Associates, Inc. dated February 15, 2000 and revised May 8, 2006 must be incorporated into all applicable construction permits. These recommendations include, but are not limited to, site preparation and grading, excavations, foundations, retaining walls, stormwater detention vaults, rockeries, drainage, and utility installation.

Authority: Bellevue City Code 23.76, Land Use Code 20.25H  
Reviewer: Matthews Jackson, Planning and Community Development Department  
Savina Uzinow, Planning and Community Development Department

**C. PRIOR TO BUILDING PERMIT: The following conditions are required by City Code. Unless specified otherwise below, these conditions must be complied with on plans submitted with the Building permit application:**

**D.**

**1. PROJECT SIGNAGE**

A sign master plan package in compliance with the Sign Code shall be submitted to the Permit Center for review and approval as an application for a Sign Permit with Land Use Exemption from Design Review.

Authority: Bellevue City Code 22B.025  
Reviewer: Matthews Jackson, Planning and Community Development Department

**2. MECHANICAL EQUIPMENT SCREENING**

Show the location of each piece of mechanical equipment, including communication equipment such as satellite dishes, and demonstrate that screening is provided so that these items are not visible from adjacent streets, public sidewalks, or the surrounding buildings. Mechanical equipment which is located on the roof shall be incorporated into the pitched or stepped roof form, and not appear as a separate penthouse or box.

Authority: Land Use Code LUC 20.20.650 and 20.25B.040.E  
Reviewer: Matthews Jackson, Planning and Community Development Department

**3. FIRE AND LIFE SAFETY**

The Building Permit Drawings shall include the following:

- a. Provide a Fire Hydrant within 50 feet of the Fire Department Connection.
- b. Provide automatic fire sprinklers throughout the building designed per NFPA 13.
- c. Provide a Fire Department Connection at least 50 feet from the building at an approved location by the Fire Department.
- d. Provide a fire alarm notification system throughout the building.

Authority: NFPA 13, Bellevue Fire Department Standards, International Fire Code, Bellevue City Code 23.11.903.4.2  
Reviewer: Adrian Jones, Fire Department

**4. BUILDING AND SITE PLANS – TRANSPORTATION**

Building plans, landscaping plans, and architectural site plans must be consistent with the transportation-related items included in the engineering plans approved with the clearing and grading permit.

Authority: Bellevue City Code 14.60.060, 110, 120, 150, 180, 181, 190, 240, 241  
Reviewer: Carl Wilson, Transportation Department

**D. PRIOR TO TCO: The following conditions are required by City Code and supported by City Policy. The conditions shall be complied with prior to issuance of the Temporary Certificate of Occupancy (TCO):**

**1. LANDSCAPE INSTALLATION ASSURANCE DEVICE**

All site landscaping shall be 100% complete per the plan approved by the City. Alternatively, the applicant shall submit the following: 1) a red-marked plan identifying which landscape areas are incomplete; 2) an estimate for the total cost to complete these areas; and 3) a notarized Assignment of Savings dedicated to the City for 150% of the estimated cost to complete these areas per the approved Landscape Plan. The assurance device will be released upon complete installation, inspection approval and rededication of the plaza property to the City filed with King County Records Office.

Authority: Land Use Code 20.40.490  
Reviewer: Matthews Jackson, Planning and Community Development Department

**2. LANDSCAPE MAINTENANCE ASSURANCE DEVICE**

File with the Department of Planning & Community Development a landscape maintenance assurance device for a one-year period in the form of an assignment of savings or letter of credit for 20% of the cost of labor and materials for all required landscaping.

Authority: Land Use Code 20.40.490  
Reviewer: Matthews Jackson, Planning and Community Development Department

**3. STREET FRONTAGE IMPROVEMENTS**

All street frontage improvements and other required transportation elements, including street lighting, must be constructed by the applicant as shown on the approved engineering plans and accepted by the City Inspector. Bonding or other types of assurance devices will not be accepted in lieu of construction, unless the city requires a delay.

Authority: Bellevue City Code 14.60.060, 110, 120, 150, 180, 181, 190, 210, 240, 241; Transportation Department Design Manual; and Transportation Department Design Manual Standard Drawings.  
Reviewer: Carl Wilson, Transportation Department

**4. PAVEMENT RESTORATION**

Pavement restoration associated with street frontage improvements or to repair damaged street surfaces shall be provided as follows:

a) 164<sup>th</sup> Avenue SE: The City plans to resurface this street in the year 2009, after which a 5-year no-street-cut moratorium will be in effect. All necessary trenching and street cuts must be completed by that time, using standard trench restoration methods.

b) Lakemont Blvd: This street was repaved in 2006, and a 5-year no-cut moratorium is now in effect. If trenching in Lakemont Blvd cannot be avoided, then extraordinary trench restoration will be required. Such restoration will include a grind and overlay at

least 50 feet long for the full width of any affected lane. Specific grind and overlay requirements will be determined in a right of way use permit.

Authority: Bellevue City Code 14.60. 250; Design Manual Design Standard #21  
Reviewer: Jon Regalia, Transportation Department

## **5. TRAFFIC AND PARKING MANAGEMENT PLAN**

The applicant shall submit for city review and approval a Traffic and Parking Management Plan. The Plan shall include:

- a) Training church members for efficient parking usage and parking lot management.
- b) Educating members to not use local streets for parking or as cut-through routes. It is especially important to avoid neighborhood cut-through on local streets if exiting delay increases the tendency of drivers to exit toward the north.
- c) Handling overflow parking, including agreements to use off-site parking lots for peak events, and to use shuttles as needed. Agreements with off-site parking lot owners must be documented prior to occupancy of the Chapel.

The applicant shall agree to abide by the Plan and to modify the Plan as needed in order to minimize the development's traffic impacts within residential neighborhoods and to minimize conflicts on adjacent streets.

Authority: SEPA: Comprehensive Plan Policies TR-14, TR-17, TR-38  
Reviewer: Matthews Jackson, Planning and Community Development Department  
Carl Wilson, Transportation Department

## **6. LIGHT MITIGATION**

The applicant shall submit for city review and approval a light mitigation plan that includes information regarding proposed lighting fixtures, landscaping, and/or fencing that is intended to direct, deflect, or shield light from vehicle headlights and the site to avoid glare or direct illumination upon any adjoining single family residentially property.

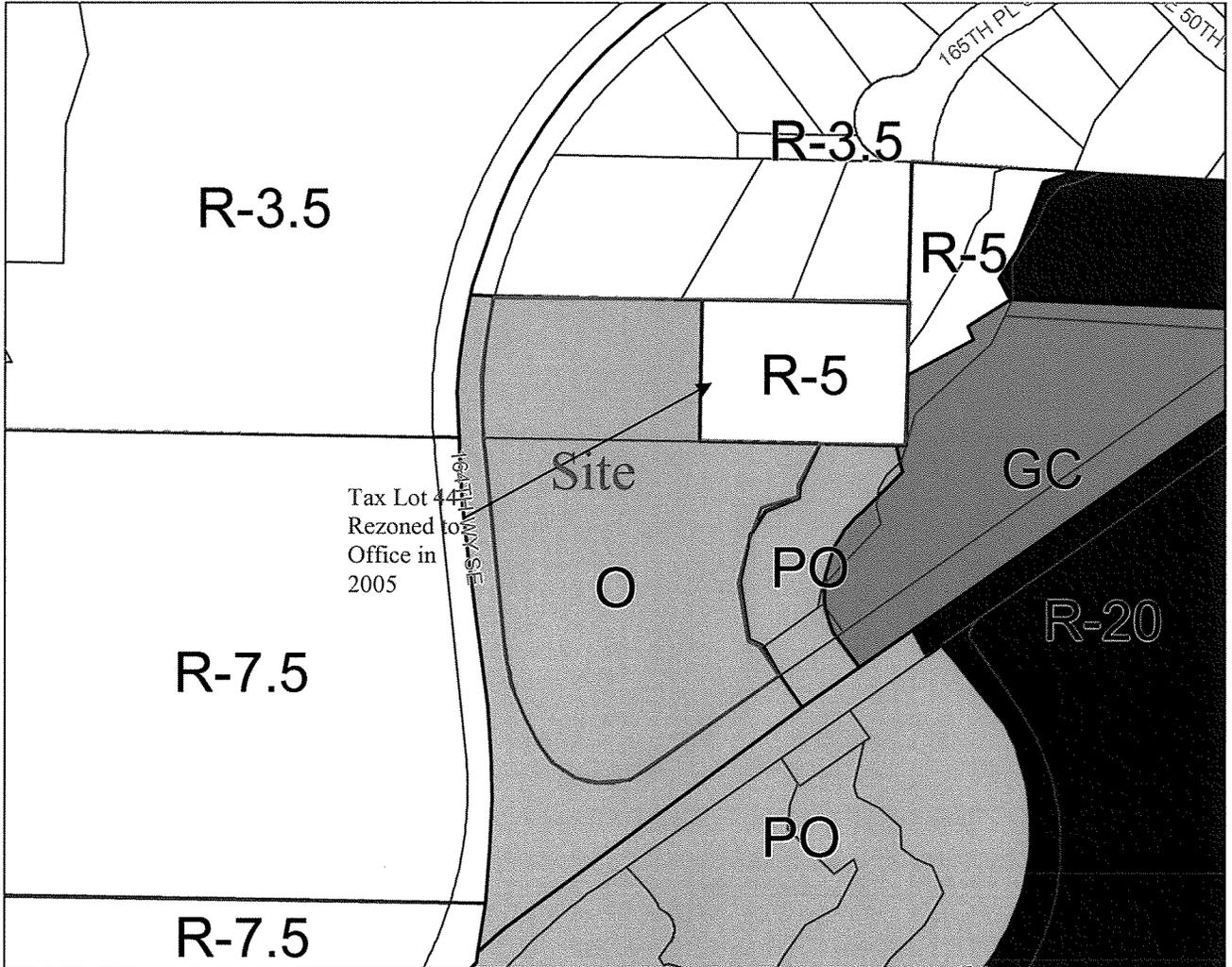
Authority: Land Use Code 20.30F.145.d  
Reviewer: Matthews Jackson, Planning and Community Development Department

# Exhibit A

## Location Maps



# Zoning Map



# **Exhibit B**

## **Comprehensive Plan Policies**

## Comprehensive Plan Policies

Following are land use policies from the City's Comprehensive Plan that are pertinent to, and supportive of, the project proposal:

***Policy LU-22:*** *Protect residential areas from the impacts of non-residential uses not of a scale not appropriate to the neighborhood.* The size of the church has been significantly reduced since the original submittal in 1998. The presence of a wetland and its required buffers required a reduction in the amount of buildable area available to the new facility. The architectural form is characteristic of the northwest in color and materials, and building articulation reduces the bulk and scale of the building in response to existing neighborhood character.

***Policy CP-6:*** *Encourage and emphasize open communication between developers and neighbors on compatibility issues.* During the long evolution of this proposal there have been numerous public meetings intended to inform both staff and the proponents of community concerns. Many of these issues have been considered and the proposal has been designed to address these issues where appropriate.

***Policy UD-12:*** *Place and design outdoor lighting around buildings, in parking lots and along streets to prevent excessive glare into residential neighborhoods.* The applicant will be required to provide a light mitigation plan that is intended to reduce light and glare impacts on adjacent residential properties.

***Policy S-NC-9:*** *Require complete topographic surveys, soil reports, drainage reports and habitat evaluation for projects in areas identified as sensitive due to their environmental characteristics.* Topographic surveys, soil reports, drainage reports, wetland delineations, and geotechnical studies were required in support of this proposal due to the presence of steep slopes and a wetland on the property.

***Policy S-NC-33:*** *Maintain or enhance the natural hydraulic and habitat functions of streams, lakes and wetlands. The functions to be preserved or enhanced include storm water storage and conveyance, groundwater recharge and fish and wildlife habitat.* The proposal will be required to detain and treat stormwater from impervious surfaces on the site. The rate of stormwater release is required to approximate the predeveloped forested condition. The wetland located on the property will be preserved as will a primary buffer of 50 feet. This will allow continual recharge of the wetland as well as preserve its hydraulic connection to Lewis Creek. Wildlife habitat along the banks and slope of Lewis Creek and the wetland area will be preserved with the new development.

# **Exhibit C**

## **Project Plans & Drawings**



PROJECT NAME



**CALVARY  
CHAPEL  
EASTSIDE**  
5220 164th AVE. SE  
BELLEVUE, WA

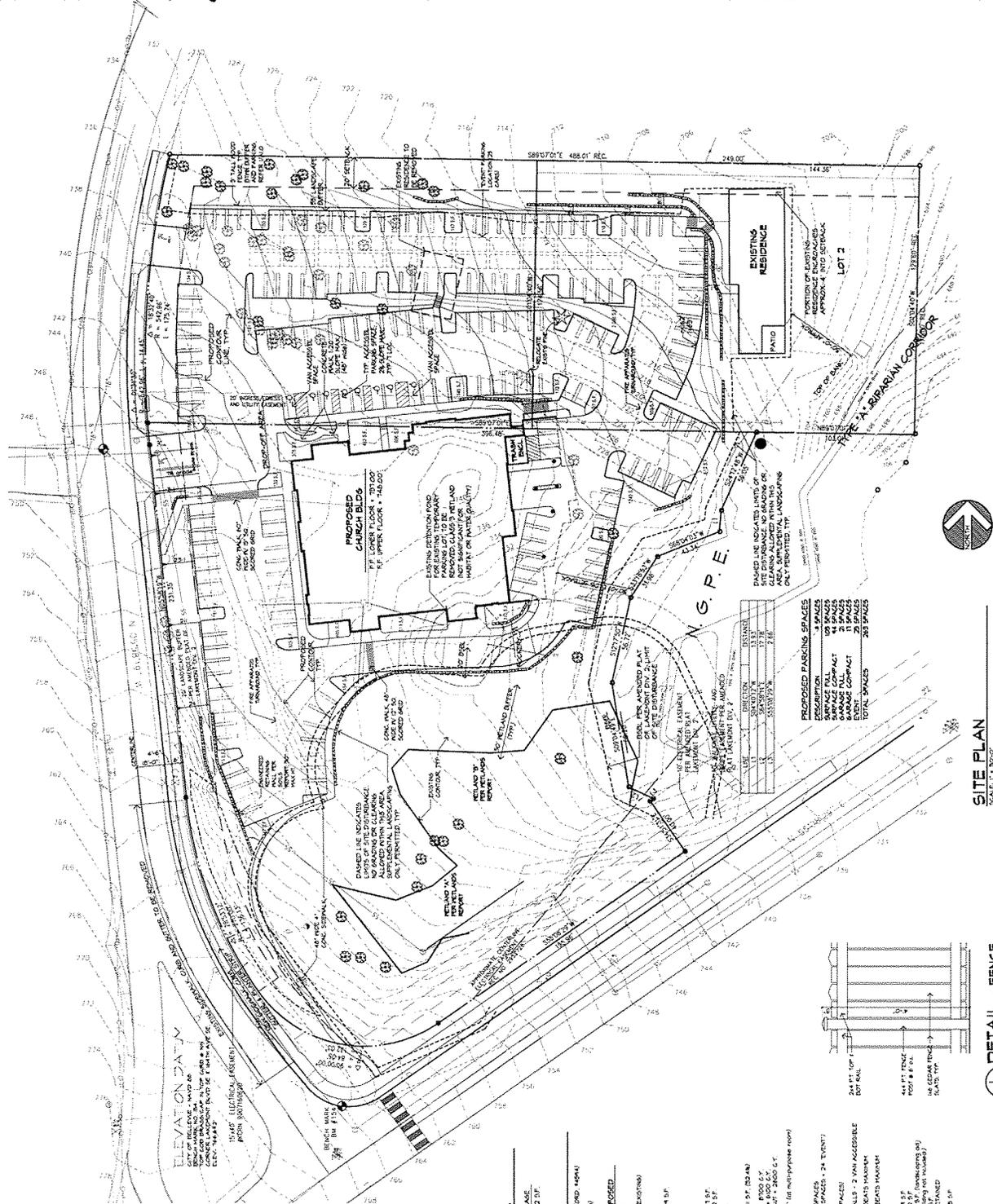
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**SITE PLAN  
SITE NOTES**

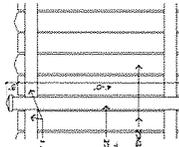
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**A.I.O.**



**PROPOSED PARKING SPACES**

DESCRIPTION	NO. SPACES
4 SPACES	4
10 SPACES	10
COMPACT	3
BARREL RACK	20
EVENT SPACES	203
<b>TOTAL SPACES</b>	<b>240</b>



**SITE PLAN**  
SCALE: 1/8"=1'-0"

**DETAIL - FENCE**  
SCALE: 3/4"=1'-0"

**BUILDING FLOOR AREA**

USE	AREA (SQ. FT.)	AREA (SQ. FT.)	AREA (SQ. FT.)
OFFICE	1440		
REAR			
MECHANICAL			
TOTAL	1440		

**SITE PLAN NOTES**

- 1. SITE AREA: 20,244 SQ. FT. (480' x 420')
- 2. TOTAL LOT AREA: 20,244 SQ. FT.
- 3. TOTAL IMPROVED AREA: 6,820 SQ. FT. (34.2% IMPROVEMENT)
- 4. TOTAL UNIMPROVED AREA: 13,424 SQ. FT. (65.8% UNIMPROVED)
- 5. TOTAL FLOOR AREA: 1440 SQ. FT.
- 6. TOTAL GARAGE AREA: 1780 SQ. FT.
- 7. TOTAL PARKING SPACES: 240 SPACES
- 8. TOTAL LANDSCAPING AREA: 10,000 SQ. FT.
- 9. TOTAL SITE DISTURBANCE AREA: 10,000 SQ. FT.
- 10. TOTAL SITE DISTURBANCE PERCENTAGE: 49.4%
- 11. TOTAL SITE DISTURBANCE PERCENTAGE PER LOT: 49.4%
- 12. TOTAL SITE DISTURBANCE PERCENTAGE PER LOT: 49.4%
- 13. TOTAL SITE DISTURBANCE PERCENTAGE PER LOT: 49.4%
- 14. TOTAL SITE DISTURBANCE PERCENTAGE PER LOT: 49.4%
- 15. TOTAL SITE DISTURBANCE PERCENTAGE PER LOT: 49.4%
- 16. TOTAL SITE DISTURBANCE PERCENTAGE PER LOT: 49.4%
- 17. TOTAL SITE DISTURBANCE PERCENTAGE PER LOT: 49.4%
- 18. TOTAL SITE DISTURBANCE PERCENTAGE PER LOT: 49.4%
- 19. TOTAL SITE DISTURBANCE PERCENTAGE PER LOT: 49.4%
- 20. TOTAL SITE DISTURBANCE PERCENTAGE PER LOT: 49.4%





PROJECT NAME



**CALVARY  
 CHAPEL  
 EASTSIDE**

5220 164th AVE. SE  
 BELLEVUE, WA

**DESIGN REVIEW  
 EXTERIOR  
 ELEVATIONS**

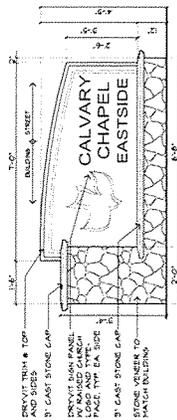
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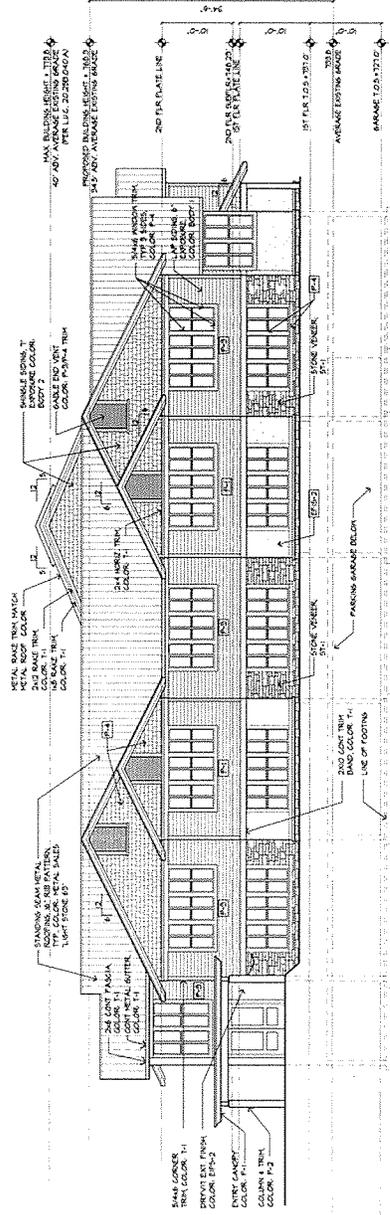
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 ISSUED DATE: DESCRIPTION

**AS.O**

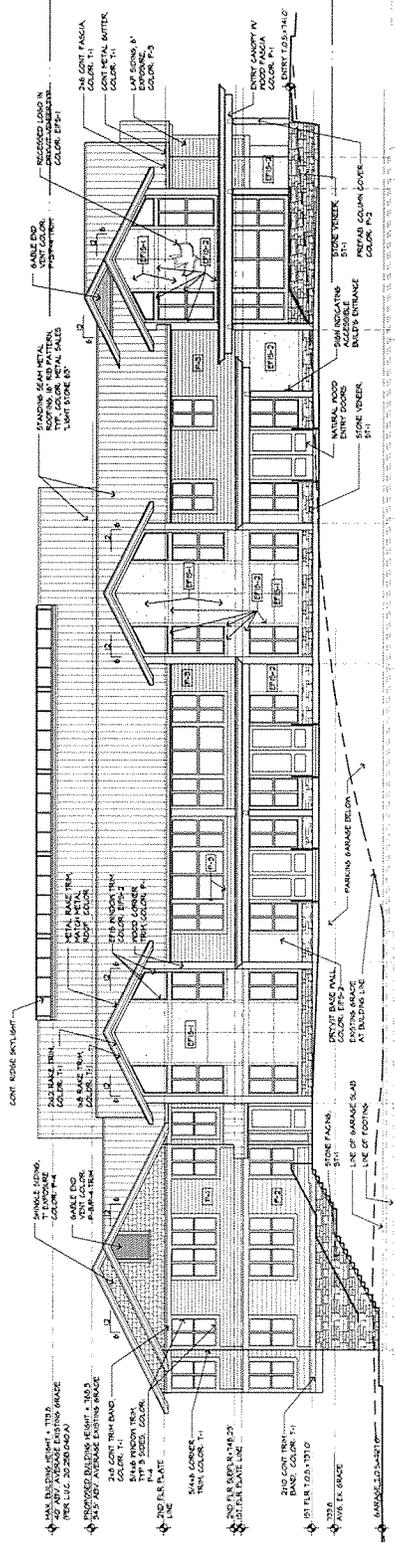


**1 CONCEPTUAL SIGN ELEV.**  
 SCALE: 1/8" = 1'-0"

TAG	MANUF.	COLOR
P-1	ICI PAINTS	#848 'OVSTER WHITE'
P-2	ICI PAINTS	#423 'WORSTED TAN'
P-3	ICI PAINTS	#1083 'WEST INDIES'
P-4	ICI PAINTS	#1100 'SOUTHERN VERANDA'
T-1	ICI PAINTS	#2016 'WHITE WHISPER'
EFS-1	DRYVIT	#333 'MONEY TAST'
EFS-2	DRYVIT	#110 'VAN DYKE'
EFS-3	DRYVIT	#568 'OVSTER SHELL'
ST-1	ELDORADO STONE	'MOUNTAIN LEDGE' SIERRA



**WEST ELEVATION**  
 SCALE: 1/8" = 1'-0"



**NORTH ELEVATION**  
 SCALE: 1/8" = 1'-0"



PROJECT NAME



**CALVARY  
 CHAPEL  
 EASTSIDE**  
 5220 164th AVE. SE  
 BELLEVUE, WA

**DESIGN REVIEW**

DRAWING TITLE

**EXTERIOR  
 ELEVATIONS**

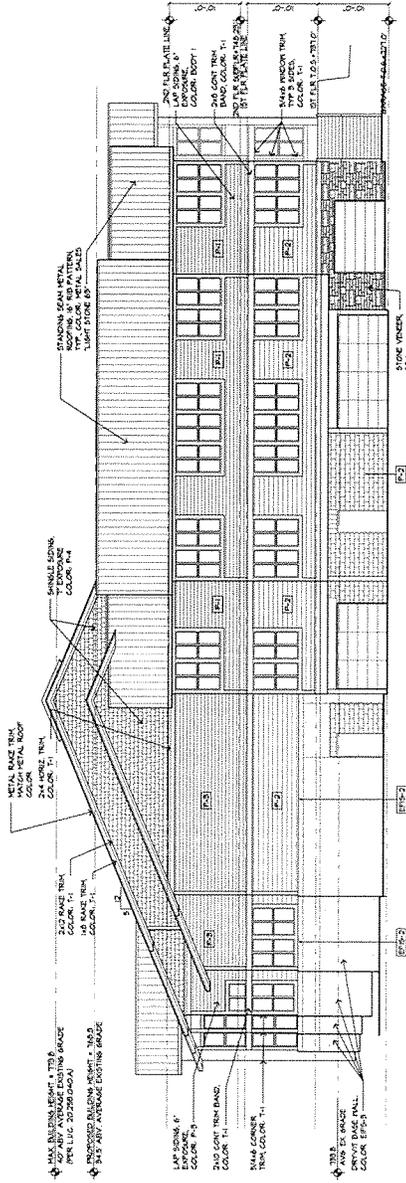
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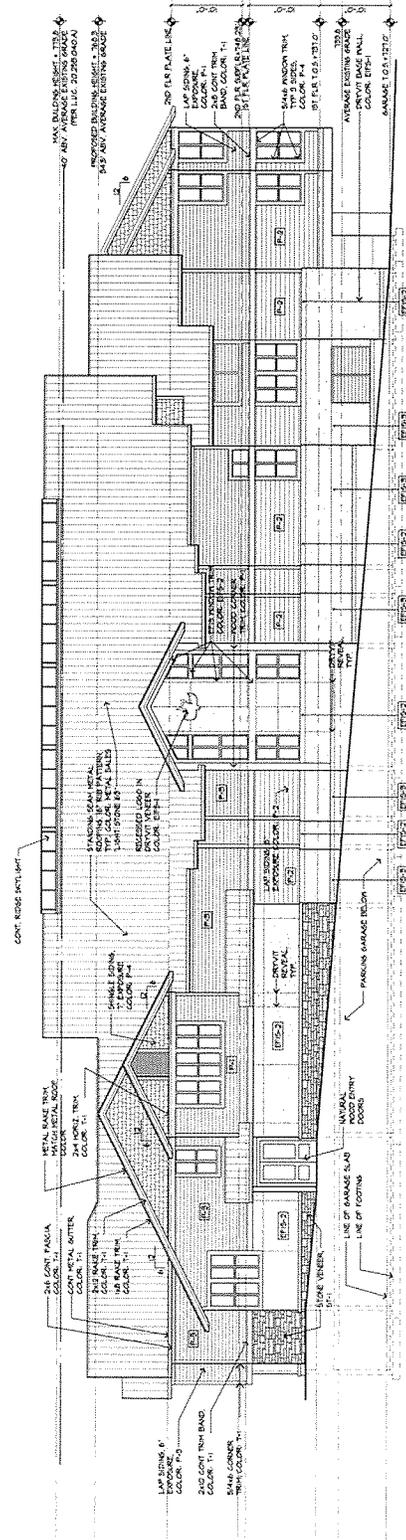
DESCRIPTION

**A3.1**



**EAST ELEVATION**

SCALE: 1/8" = 1'-0"



**SOUTH ELEVATION**

SCALE: 1/8" = 1'-0"