



City of Bellevue
Department of Planning and Community Development
Development Services Division Staff Report

Proposal Name: **Eastgate Elementary School**

Proposal Address: 4255 153rd Avenue SE

Proposal Description: To demolish an existing 38,406 square foot structure and remove three portables to construct a two-story 63,036 square foot facility with 98 parking stalls. The existing 5,000 square foot Early Child Care (ECC) building will remain with this application.

File Number: **07-133461 LA**

Applicant: Bellevue School District 405

Decisions Included: Administrative Conditional Use, Process II

Planner: Antoinette Pratt, Senior Planner, (425) 452-5374

State Environmental Policy Act
Threshold Determination: **Determination of Non-Significance Issued August 31, 2007, by Bellevue School District 405.**

Director's Decision: **Approval with Conditions**

Carol V. Holland for

Matthew A. Terry, Director
Department of Planning and Community Development

Application Date: October 4, 2007
Public Notice (500 feet): October 18, 2007
Public Meetings: November 15, 2007
Minimum Comment Period: November 1, 2007
Bulletin Publication Date: **January 31, 2008**
Appeal Deadline: **February 14, 2008**

For information on how to appeal a proposal, visit Development Services at City Hall or call (425) 452-4570. Appeal of the Decision must be made by 5 p.m. on the date noted for appeal of the decision.

I. Request and Project Description

The Bellevue School District (BSD) requests Administrative Conditional Use approval to demolish an existing 38,406 square foot school to construct a two-story 63,036 square foot facility. Three portables will be removed from this site with this proposal. The existing Early Child Care (ECC) Center will remain with this application. It will not be open during construction and the exterior will be modified with this application to match the new facility. Interior modifications are expected as well.

This is the seventh elementary school that the District will demolish as part of their capital facilities upgrade. The District is currently in the process of upgrading a majority of its elementary schools as part of bond measures that were passed by the citizens of Bellevue. The District has conducted studies to determine the cost effectiveness of upgrading its existing facility versus demolition and construction of a new facility. It was determined that a new facility for Eastgate would provide a better finished product—functionally, programmatically, and for maintenance purposes.

One of the purposes of this request is to meet the requirements of State mandate, I-728, which requires schools to reduce the number of students per teacher within the classroom. This request also responds to Resolution 5840, which requires that Eastgate Elementary School, upon redesign, create a facility that not only meets the educational needs of the neighborhood but also focuses on the “recreational, cultural, social, health and human services needs” of the area as well (see Attachment A). The community use of schools is not specific to Eastgate Elementary or to the Bellevue School District. Joint use of schools is beneficial because it reduces the need to construct additional facilities for the local community; thereby, reducing the built environment. The new facility responds to the Districts’ intent that all new elementary schools should be approximately 60,000 to 70,000 square feet. This was established as a target size to accommodate all of the standard and special programs found at the various schools.

The current student population for the 2006-2007 school year is 425 students for Eastgate Elementary. Student population at this school has stayed at this level for several years. Eastgate Elementary was originally constructed in 1955 for 520 students.

Construction is scheduled to begin at the conclusion of the school year in June 2008 with completion estimated August 2009. Bellewood is the District’s swing school where students are located during construction activities. Bellewood is currently being utilized by the Sherwood Forest student body and next year it will be utilized by the students of Eastgate.

II. Site Context and Description

Eastgate Elementary School is bounded by two public streets: SE Newport Way which is located within King County jurisdiction and 152nd Avenue SE which is within Bellevue City limits. The surrounding neighborhood is located within King County and contains single-family residences that were generally constructed in the 1950’s and 1960’s. The homes tend to be single story rambler style with an occasional two-story structure.

The existing school is one story and located on the central and west portions of the site. Paved parking areas are located at the northeast and southeast corners of the site. An existing playfield is located south of the facility. The existing school contains six individual buildings that are connected through external corridors. Three portables are located west of the existing play field. The ECC building is located adjacent to 152nd Avenue SE. This

configuration does not contribute to the safety of the student body as the whole site is readily accessible. The City Parks Department currently schedules the play field for soccer and baseball and will do so again once redevelopment occurs on this site.

Site topography is relatively level where the school is located. Slopes gradually decline from the southwest to northeast. Along the westerly property line, site topography slopes up by 10 feet into the adjacent neighborhood. The southwest corner of the site contains an existing retaining wall that is composed of concrete and a rockery. This wall is located west of the play field. No changes will be made to these man made features.

At the southerly portion of the site, there is an existing culvert and stream that is located at the lower portion of this site (see aerial below). This area will remain undisturbed and continue in its present form. See Section 4E for review of the Critical Areas Report for this area.



Boundary Lines

Eastgate Elementary is composed of five lots (see aerial on next page).



Three of these lots are located within King County and two are located within the City of Bellevue. The existing school straddles three property lines with north-south orientation. The property line where the bulk of the school straddles, is in the process of being removed through a Boundary Line Adjustment. The remaining three property lines will remain in place as the King County Assessor's Department will not allow removal of lots lines between jurisdictions due to differing levying districts. As a condition of approval of this application, the District will be required to sign a Single-Site Agreement with the City so the site can be viewed as one site in lieu of a Boundary Line Adjustment. See Section XI for related condition.

In addition to the lot line issues, in the course of reviewing this application, the District discovered that the notched area between its westerly lots is owned by King County. Over the course of time, the District has installed a stair case so that students traveling from the west can reach the site. Associated landscaping is located here as well. It has maintained this area since the school came in existence in 1955. The District has obtained a Use Agreement with the County to continue usage of this area for the school (see file). They are also petitioning for a vacation so that the notched area can be consolidated into District property.

III. Proposed Site and Building Design

Design Goals

The goal of the Eastgate Elementary School project is to design a 40% larger replacement school that provides an inspiring learning environment and supports and encourages student learning as well as the existing collaborative teaching culture. The school is designed primarily as a two story facility with approximately 40,839 square feet for the first floor and 22,197 square feet for the second floor for a total of 63,036 square feet. By increasing height from one to two stories, it allows for a smaller building footprint to better utilize the site for parking and traffic circulation. See Sections IV.C.2 and IV.D.1 for further discussion regarding parking.

Site Design

The building is located on the site so that the two story administration/library wing is prominent and visible from Newport Way. The approach to the school from Newport Way leads the visitor to the north parking lot. The main entry is raised above the parking lot and is approached by short flights of stairs or by a ramp parallel to the administrative offices. Staff parking and bus drop-off/pick-up are located off 152nd Avenue SE, and a short section of sidewalk connects to the covered play area and a covered canopy leading to the main entry lobby from the south.

While the vehicular circulation is schematically similar to that of the existing school, there are significant improvements. The length of the student drop off area is increased and the separation of bus and vehicular drop-off will result in significantly less congestion at the beginning and end of the school day. West of the multi-purpose building is a fenced service courtyard accessed from SE 43rd Street. This will allow access for food service, waste disposal and recycling to the service elements of the school without crossing student pedestrian or other vehicle routes.

The all-weather playfield is located at the south end of the site, adjacent to a play structure area. There is a separate existing play area to the north for kindergarten and first grade students. The ECC will continue to occupy the corner between Newport Way and 152nd Avenue SE and will receive a face-lift and new mechanical system.

Building Design

One of the primary changes with this design is the elimination of the external breezeways connecting the existing six buildings. The new facility is designed in six clusters that are connected through internal corridors. Courtyards are provided between clusters to provide additional lighting and views. The landscape plans depict seatwall areas for student use along with landscape materials of trees and various understory.

A plaza is proposed along the northeast corner of the administration building which is adjacent to the student drop off zone. This plaza will contain special pavement materials along with seatwalls and landscaped planters. Further east of this plaza is the visitor parking area with ingress/egress from SE Newport Way. The bus loading zone and staff parking lot is located adjacent to the play field and south of the ECC building.

Internally the main entry lobby connects to a hallway leading to the main floor classroom clusters and a stairwell providing access to the library and second floor clusters. When the school is in session, the main doors to the entry lobby will be locked and visitors must first check in at the reception desk in the administration wing. School security is a major priority in

the design due in part to the number of uncontrolled access points onto the campus from the neighborhoods to the west. All exterior doors will be electronically controlled and each door can be locked except during specific times as dictated by the school schedule.

The administration suite is directly off the main entrance and visible to the public. Facing the entry courtyard are the reception area, principal's office and a conference room. In the southeast corner is the faculty lounge. The remainder of the perimeter is taken up with offices for the various counseling functions as well as an office to house PTSA activities. The interior of the suite includes toilet rooms, work and storage rooms, and the nurse area, including an examination area, toilet, cot area and office.

Above the administration suite is the library. A central stair leads from the main entry to the library so that students passing by see the library as they enter and move through the school. The library includes a library workroom and a separate book storage room. The library itself is zoned to accommodate various group and independent activities. Across from the check-out desk is a computer area. In the middle of the room are the stacks and at the east end is a classroom area. Along the south, an alcove between the workroom and book storage is designed as a reading area for the younger students.

There is a culture of sharing among the Eastgate faculty that emphasizes collaborative teaching within a grade level structure. Each grade is allocated its own neighborhood cluster of three to four classrooms, and each classroom shares a flex room with the adjoining classroom. Each cluster also includes student toilets. The K-2 classroom clusters are on the first floor with the kindergarten and first grade classrooms opening directly onto the north play area.

From the three classroom clusters, a hallway on each floor leads to the gym and multi-purpose room. Adjacent to the multi-purpose room is the kitchen and a raised stage platform that will double as the orchestra classroom. Connected to the stage via a short ramp is the band classroom. Receiving, electrical and mechanical rooms as well as an emergency storage room are located to the west adjacent to the service courtyard. Along the south side of the gym and multi-purpose room is an assortment of storage rooms for tables, gym equipment, community property plus a physical education office.

Across the hallway running along the north side of the gym and multi-purpose room are two gang toilet rooms and a shower room. The art room, with kiln room, is also accessed from this hallway. Adjacent to the art room is another classroom. These two teaching stations form one side of an internal courtyard, flanked by circulation for the two story classroom clusters on two sides and the main entry lobby on the fourth. This courtyard is designed to accommodate classroom activities, weather permitting, and is accessible directly from the art classroom.

Building articulation is expressed by building offsets and breaks used to keep the structure compatible with adjacent single-family uses. These components allow the structure to be viewed in smaller increments rather than one building mass. The District proposes to use cement fiber board and CMU for their primary building materials. Metal shading is proposed over windows located at various areas to reduce glare. Building colors contain a palette of a warm sienna brown and pale brown. Accent colors will be red and blue. The school administration proposed the blue color to maintain a connection to the existing school which is blue.

The building structure will be steel. Roofs will be low slope (1/4:12) with mechanical penthouses partially recessed above the classroom clusters with exception of the library cluster which will have mechanical equipment located within the building shell. Given the

LAND USE CODE (LUC) REQUIREMENTS

Category	LUC Requirements	Proposal by Applicant
Site Area (R-5 Zoning District)	7,200 square feet	9.58 acres or 417,305 square feet
Lot Coverage(1)	35 percent	12 percent
Impervious Surface(2)	80 percent	44 percent
Building Height(3)	40 feet	30 feet
Building Setbacks Side (north) Front (east) Side (west)(4) Rear (south)	50 feet 20 feet 30 feet 50 feet	50 feet 45 feet 10 feet 1,100 feet
Parking	Unspecified Use	On Site: 25 Staff 39 Parent/visitor 17 Parent pick up/drop off lane 13 Bus lane (upon their exiting) 4 Service stalls Off-Site: 51 stalls (Eastside Congregational Church) Total Provided: 149 stalls
Landscaping Perimeter Buffers North South East West(5)	10 feet 10 feet 10 feet 10 feet	10 feet 600 feet(6) 10 feet 20 feet
Parking lot Landscaping	1,434 square feet	6,550 square feet
Tree Preservation Interior	15% minimum of the existing diameter tree inches= 648.15 diameter inches	2,347 diameter inches or 54% diameter inches remaining
Tree Preservation Perimeter	100% of diameter inches	100%

1 LUC 20.20.740.A.1 reduces lot coverage for schools from 40 percent to 35 percent despite the extra lot coverage gained in this district.

2 LUC 20.20.010, footnote 36 permits new allowed nonresidential uses in residential land use districts to increase impervious surface from 55 to 80 percent.

3 LUC 20.20.740 allows school facilities to increase height by 10 feet beyond the underlying zoning height of 30 feet if mechanical is located within the two story structure and not on the roof. Site size must be larger than 5 acres. This proposal qualifies for this extra height allowance.

4 A single-site agreement is required due to an inability to obtain a Boundary Line Adjustment from King County. This agreement is provided in lieu of such document and must be completed prior to building permit issuance for this project. See Section XI for related condition.

5 Landscaping at the northwest corner is increased to 20 feet because a 10 foot setback is provided in this vicinity.

6 This area is heavily treed and contains the culvert and stream area discussed in Section 4E. It will remain undisturbed with this application.

B. Schools

LUC Chapter 20.20.740 provides development standards for schools in residential districts. The proposal meets the dimensional standards for schools in regards to building setbacks, lot coverage, landscaping, and site and building design guidelines.

C. Landscaping (Alternate Landscape Option)

The District has complied with the landscape standards for schools with exception of parking lot landscaping every 10th stall. As noted above, the minimum requirement for parking lot landscaping is 1,434 square feet. The District is providing 6,500 square feet by creating a rain garden that is heavily vegetated with overstory and understory vegetation within the staff parking lot. The presence of the rain garden will provide screening of the all weather play field that will be completed with this application. The all weather field will be composed of sand with upgraded drainage. Alternatively, the staff parking lot will contain, by itself, 3,537 square feet which is substantially more than would be required by placing two additional landscape islands in this area. The visitor's parking lot area complies and contains the appropriate amount of parking lot island landscaping.

D. Height Requirement

LUC Chapter 20.20.740 permits school facilities to increase the maximum building height from underlying building height of 30 feet to 40 feet if the following parameters could be achieved: 1) No mechanical equipment on the roof and 2) a site size of 5 acres or larger. Building height is 30 feet for this structure; therefore, screened mechanical equipment may be located on the roof with exception of the library cluster which will be 31 feet in height. Mechanical equipment for this cluster will be embedded within the structure.

E. Critical Areas

The site contains steep slopes located approximately 40 feet south of the existing play field. There is also an existing retaining wall located at the southwest corner of the play field. Lastly, there is a Type N stream that resides within a buried 48 inch diameter culvert that is located just north of the steep slope area. Both the upstream and downstream ends of the culvert are located outside the project area. The existing parking and play field will remain but be reconfigured. The proposed two story building is located approximately 340 hundred feet from top of bank.

In compliance with 20.25H.230, the District has submitted a Critical Areas Report by GeoDesign which is dated July 16, 2007, to analyze the critical areas on-site. Geodesign found that there is a short segment of Vasa Creek that has been buried and placed within the 48 inch diameter culvert beneath the property. The culvert extends beneath the southeast corner of the playfield and through the south end of the parking lot. There is an existing catch basin at the end of the school parking lot that collects overflow water and drains it into the buried culvert. To the south of this culvert is an open segment of Vasa Creek that extends to the southerly most boundary of this site and beyond. The existing playfield and parking lot will remain in their general locations. The attached site plan designates the required 10 foot setback from this existing culvert to fulfill LUC 20.25H.075.D.2.b.

GeoDesign found that there are two areas of steep slope: one area is located approximately 40 feet from the play field and the other is an existing concrete/rockery wall that is located at the southwest corner of the play field. Both areas meet the definition of steep slope per LUC 20.25H.120.A.1. A 50 foot buffer would ordinarily be required from top of bank near the play field and a toe of slope setback of 75 feet would be required from the rockery wall.

LUC 20.25H.120.B.2 allows existing development that was established prior to August 1, 2006, to encroach into the critical area buffer established in subsection B.1 of this section. The critical area buffer and structure setback shall be modified to exclude the footprint of the existing structure. Expansion of an existing structure into the critical area buffer shall be allowed only pursuant to the provisions of LUC 20.25H.065. With this application, the play field and parking lot areas will be maintained but reconfigured in this location.

F. Parking Standards and Site Circulation

The Land Use Code 20.20.590 does not define the number parking stalls required for an educational facility. As such, this proposal is classified as an unspecified use per LUC 20.20.590.F.2. To comply with the standards for unspecified uses, the applicant has submitted a Traffic, Parking and Pedestrian Study by Gibson Traffic Consultants (GTC) based upon the existing parking conditions. Site reconnaissance was conducted on April 17, 2007. GTC contacted the City's Transportation Department to have an initial scoping discussion to determine intersections that would potentially be impacted by this proposal. All of the identified intersections are located within King County. See Section VI.D.3.b for King County response.

There are three access points to this site: one from SE Newport Way and two from 152nd Avenue SE. The site is served by six regular school buses, one English as a Second Language bus and one half-day kindergarten bus for a total of eight buses. There is also a parent drop off/pick up lane that is accessed from SE Newport Way with a capacity for nine vehicles. There is an existing crosswalk located on SE Newport Way that is heavily used by pedestrians. GTC noted that there were 100 pedestrians (parents/children) who utilized this crosswalk which is located north of the school within King County that then connects to Eastgate Congregational Church. The church parking lot contains 51 stalls. Parents park off-site at this location and then walk their children across the street to school. This arrangement has been in place for over 20 years. With this application, the District and the church administration have formalized their existing joint parking arrangement. This agreement has been reviewed and approved by the City and has been filed under King County Recording number 20071128000777. This document is located within the project file.

V. Public Comment

To date, staff has received two e-mails on this project.

- ✓ One letter was received from King County Metro Transit who asked if there were going to be frontage improvements along SE Newport Way to determine if there would be any impacts to an existing Metro bus stop at the north end of the site.

Response: King County has not required frontage improvements to SE Newport Way so the bus stop will not be impacted by this proposal.

- ✓ The second e-mail pertained to the District's ability to conduct its own environmental review.

Response: The Bellevue School District is considered a State agency so is considered an agency of jurisdiction under WAC 197-11-926.

The City held its required public meeting on November 15, 2007. No one from the public attended this meeting but City staff, the District and its consultants.

District Held Public Meetings

The District conducted public meetings with staff from Eastgate Elementary, the adjacent neighborhood and the PTA. Those meetings were well attended and public response to the proposal has been generally favorable. Transportation-related questions and public comments regarding this project came primarily from residents of unincorporated King County, and dealt with concerns about County streets and roads impacted by the project. Such comments focused on signalization and turn lane warrants, school traffic on adjacent residential streets in the unincorporated area, and speed reduction on Newport Way. The only question regarding a City street was about whether queuing on 152nd Avenue south of Newport Way would block the school's outbound driveway at that point.

The School District responded to all public questions via a memo from Gibson Traffic Consultants dated October 2, 2007. The memo was reviewed by City staff. A copy is available in the City's files.

Regarding questions about County streets and roads, Gibson Traffic Consultants recommended the following, and City staff concurred:

- Signalization on Newport Way is not warranted.
- Turn lane installation is not warranted.
- The school should take steps to inform parents not to engage in pick-up and drop-off on nearby residential streets.
- Speed reduction on Newport Way (an arterial) would be problematic, although installation of radar message signs would be useful. On Newport Way, such signs could be installed by or required by King County.

Regarding the question about queuing on 152nd Avenue SE (a City street), Gibson Traffic Consultants concluded that there is a 95% probability that the peak-period queue would not extend past the school's outbound driveway. That is an acceptable situation.

VI. Technical Review

1. Transportation Department

A. Background

The existing student population for Eastgate Elementary School is approximately 425. The school district expects that figure to remain generally stable, but random fluctuation can occur in the community's demographics, leading to an increase in students. In order to accommodate such random fluctuation, the new school will be constructed to accommodate up to 450 students, based on present standards for class size and student / teacher ratios. Those standards are not absolute limits, and it is possible that unpredicted changes in the community's demographics could cause the student population to exceed 450. However, for purposes of transportation analysis and transportation-related conditions of approval for this Administrative Conditional Use application, an upper limit of 450 students is assumed. In any case, growth above the present number of 425 students will be due to random fluctuation of the surrounding community. Reconstruction of the school will not, in itself, generate an increase in students. Therefore, reconstruction of the school will not directly cause a significant change in trip generation.

Predicted traffic operations as well as parking supply and demand are discussed in a Traffic and Parking Study, dated May 2007, by Gibson Traffic Consultants. This document is on file with the

City. Key aspects of the analysis and recommendations are discussed elsewhere in this Staff Report.

B. Existing Transportation Facilities and Services

The school faces SE Newport Way, which is a County road not within Bellevue City limits. According to information provided by Gibson Traffic Consultants, existing traffic volumes on Newport Way are approximately 900 vehicles per hour during both the school's morning and afternoon peak periods. The school's morning peak hour is 8:15 to 9:15 AM and the school's afternoon peak hour is 3:15 to 4:15 PM.

The school site also fronts on 152nd Avenue SE, which is a City street. Existing traffic volumes on 152nd Avenue at the school site are roughly 250 vehicles per hour during both the school's morning and afternoon peak hours.

Presently, the site has two driveways on 152nd Avenue. On Newport Way, the site has a single driveway opening nearly 80 feet wide. The Newport Way access has the potential to be unsafe, because haphazard turning movements are possible within the wide opening.

Presently, some of the school's pick-up and drop-off activity occurs in the parking lot of the Eastgate Congregational Church, which is located across Newport Way from the school site. A mid-block crosswalk with signs and flashers exists to help students cross the street. This part of Newport Way, including the crosswalk, is under King County's jurisdiction.

Standard street frontage improvements (curb, gutter, and sidewalk) do not exist on the site's street frontages. Existing streetlights on the City's part of the frontage (152nd Ave) are adequate for lighting that street.

Two Metro bus routes serve Newport Way at the school site. Route 271 serves the site three to four times per hour during peak commute periods and approximately twice per hour midday. Route 210 serves the site once per hour during peak commute periods.

C. Short-Term Impacts

As is typical of elementary schools in Bellevue, the amount of pick-up and drop-off traffic creates short periods of intense activity. The analysis below discusses trip generation at the site and indicates that the proposed site design will improve the accommodation of pick-up and drop-off traffic, compared to the previous site design.

1. Trip Generation: The school presently has approximately 425 students, a number that is expected to remain relatively stable for the foreseeable future, but could reach 450 or more in some years due to random fluctuation. For this analysis, 450 students was assumed to be the worst case. If the student population ever exceeds 450, it may be necessary to perform additional traffic analysis. See Sections X, XI, and XII for related conditions.

Gibson Traffic Consultants predicted that trip generation for the new school site with 450 students will be approximately 134 vehicles inbound and 127 outbound (261 total) in the morning peak hour (8:15 – 9:15 AM), and 68 vehicles inbound and 80 outbound (148 total) in the afternoon peak hour (3:15 – 4:15 PM). Afternoon vehicle trip generation is significantly lower because some students who are dropped off by parents in the morning use other means to go home in the afternoon. These trip generation figures are derived from actual counts of the school's driveways.

These trip generation figures assume continued use of the Eastgate Congregational Church parking lot on the opposite side of Newport Way for some of the school's pick-up and drop-off activity. Without use of the church parking lot, trips using the school's driveways would be roughly 50 vehicles higher (25 in and 25 out) for each peak hour. Use of the church lot means that the number of trips using the school's driveways is generally lower than for other comparable elementary schools in Bellevue. If the school's use of the church lot is ever lost, then it may be necessary to perform additional traffic analysis. See Sections X, XI, and XII for related conditions.

2. Parking and Pick-Up / Drop-Off Storage: The amount of parking and pick-up/drop-off storage needed at peak times is less than the total trip generation described above, since parent vehicles do not all come and go at once. In order to accommodate the number of vehicles expected to be on-site at one time, the proposed site plan includes the following:
 - a. Total on-site parking provided by the project will increase from 65 spaces to approximately 85. (The future number is approximate because new drop-off parking lanes will not be marked with individual spaces.)
 - b. In addition, some pick-up and drop-off activity can occur within the bus parking zone after buses depart.
 - c. At peak times, the parking lot aisles may also accommodate some additional parent vehicles.
 - d. The site design will provide more efficient access and on-site circulation by improving the haphazard parking and access layout that presently exists in the northern parking lot and by providing circular access in the southern lot.
 - e. Shared use of the Eastgate Congregational Church site will continue to provide up to 51 parking spaces on the north side of Newport Way. Long-term use of the church site for school usage has been assured through a new agreement between the church and the school district.

Thus, the proposed project provides more capacity and greater efficiency for on-site parking and circulation than exists with the current layout. Based on those site plan features, the school district's traffic consultant examined peak demand for parking and pick-up/drop-off storage, and concluded that peak demand for parents and staff can be accommodated in both the morning and afternoon peak periods, with no need for pick-up/drop-off activity on nearby streets. The consultants observed no pick-up and drop-off activity on nearby streets under the previous site layout; therefore, pick-up and drop-off on nearby streets should not be a significant concern with the new site layout.

Compared to other elementary schools, the City has received very few complaints about traffic congestion or spillover parking for the existing Eastgate Elementary School site. Based on that and the analysis above, the Transportation Department staff expects that the improved site plan will be adequate to accommodate the expected demand for parking and pick-up and drop-off traffic, assuming the following:

- The student population will not exceed 450.
- The school will provide on-site traffic monitoring and management.
- The school will provide information as needed to inform parents about preferred parking and circulation behavior.
- Shared use of the Eastgate Congregational Church parking lot will continue. If the church parking lot ever becomes unavailable for use by the school, it will be necessary for the school district to make other provisions at that time to accommodate the need for parking and pick-up and drop-off activity.

D. On-Site Improvements, Traffic Management, Frontage Improvements, and Pavement Restoration as Short-Term Mitigation

On-Site Improvements: The proposed project will add approximately 20 new on-site parking spaces, and will allow parent usage of the bus zone after buses depart. Site plan improvements will improve safety and efficiency at the access points and improve on-site circulation. These improvements, plus continued use of shared parking at the Eastgate Congregational Church site, will largely eliminate any tendency for off-site spillover of pick-up and drop-off activity. On-site signs, pavement markings, and pedestrian improvements are needed to improve safety and traffic flow. See Sections X, XI, and XII for related conditions.

1. Traffic Management: In spite of the improved on-site parking capacity and circulation, pick-up and drop-off activity during brief periods within the peak hours is typically so intense that additional traffic management efforts will be needed. The school district must implement an on-going program to monitor and manage on-site parking and circulation with the goal of preventing any on-site parking and circulation problems from spilling off the site. Said program must include at least the following:
 - Provide an on-site traffic monitor whose duties will include managing pick-up and drop-off traffic to improve safety and reduce impacts on City streets.
 - At the beginning of each school year and as needed throughout each school year, the school district shall provide information to parents regarding proper traffic behavior and safety during pick-up and drop-off periods.
 - Prior to issuance of the initial certificate of occupancy for this project, the school district shall submit a policy plan for City review and approval. This policy plan shall include a statement of the school district's on-going commitment to manage on-site parking and circulation and an outline of how the school district will implement traffic monitoring and the provision of related information to parents. See Sections X, XI, and XII for related conditions.

2. Street Frontage Improvements: On 152nd Avenue SE, the City intends to construct a street improvement project along the school's frontage. The project, identified as PW-W/B-74 in the City's Capital Investment Program Plan (CIP), is intended to begin design in early 2008, with construction scheduled from late 2008 into 2010. The project will install curb, gutter, sidewalk, and possibly a bike lane on the western edge of 152nd Avenue. Because the City's project is already scheduled and funded, and because reconstruction of the school is not expected to generate an increase in traffic, the City will not require the school district to construct or pay for any frontage improvements on 152nd Avenue, except for trench or pavement restoration that may be necessitated by the school's construction work and improved driveway connections for the revised parking lots. However, in lieu of the school district providing standard street frontage improvements on the school's frontage along 152nd Avenue, the school district will contribute, at no charge to the City, whatever reasonable amount of right of way or easement may be needed for the City to construct and use project PW-W/B-74. Preliminary indications are that the City's project will need little or no new right of way. It is likely that some areas of sidewalk easement, slope or wall easement, temporary construction easement, and rights of entry will be needed. As part of the design of the City's project, the City will do any necessary survey work to identify said right of way or easement areas.

In addition to the above information regarding the City's CIP project, the Transportation Department has determined that the following transportation improvements are the minimum required for pedestrian and traffic safety, good traffic operations, and ADA compliance.

a. 152nd Avenue SE:

- i. The school district's design and construction work along 152nd Avenue must be coordinated with the design and construction of the City's CIP project as much as feasible, given the schedules of the two projects. This applies especially to the design and construction of driveways as described below. In addition, proper coordination of any trenching and pavement restoration on 152nd could nullify the need for the school district to provide a pavement grind and overlay.
- ii. Driveway locations shall be approximately as shown on plans submitted to the City on November 26, 2007.
- iii. Driveway grades shall not exceed 7% for 30 feet behind the approximate edge of the future sidewalk, and shall not exceed 15% at any point. Grade changes shall be rounded so that vehicles do not bottom out. Elevations confirming the driveway grades shall be shown on the final plans.
- iv. Driveway approaches on 152nd Avenue shall be constructed per standard drawing DEV-7B, with asphalt radii and no curb, if the exact location of the City's curb, gutter, and sidewalk have not been determined prior to construction of the school's new driveways. If the exact location of the City's curb, gutter, and sidewalk have been determined, then the school's driveway approaches shall be constructed by the school district per standard drawing DEV-5, or with curb radii and handicapped ramps, as determined during later phases of the design process. The width of both driveway approaches shall be sufficient to accommodate the turning movements of school buses. Drawings of bus turning movements confirming the adequacy of the driveway approach width must be submitted by the school district prior to approval of the school's engineering plans.
- v. Signage and pavement markings for the driveways and on-site circulation, including "do not enter" signs for the northern driveway on 152nd Avenue, must be provided in order to enhance traffic operations and safety. Specific design details, including AASHTO designations for standard signs, must be included in the final engineering plans, to the satisfaction of the City of Bellevue Transportation Department. Prior to the completion of construction work, the exact locations of signs and markings will be determined in the field by the Transportation Department inspector.
- vi. Confirm ADA compliance and route desirability for any new sidewalks and ramps from the street into the site.

b. Sight Lines at Driveways

Landscaping, signage, etc. adjacent to driveways shall not block sight distance triangles required by BCC 14.60.240 and 241.

c. Newport Way:

The proposed development was reviewed by the King County Department of Transportation. The proposed revisions within the King County right of way along Newport Way will require the developer to obtain a right of way use permit from the King County Department of Development and Environmental Services (contact Mazen Haidar at (206) 296-7133). That permit will require the revised driveway connection to Newport Way to meet county standards for a three-lane driveway, and possibly other requirements. Generally, County standards call for two eleven-foot exit lanes and one thirteen-foot entry lane. All work shall be reviewed, approved, and inspected through an associated King County Right-of-Way Use permit.

3. **Trench and Pavement Restoration:** According to Bellevue City Code 14.60.250, whenever work done by a developer requires cutting into the street surface, the City requires the surface and subgrade to be restored according to engineering principles documented in Transportation Department Design Manual, Section 21 and shown on standard drawings ROW-1 through ROW-5. Presently, 152nd Avenue SE is classified as “overlay required” near the school site. Any trenching or damage to the street surface requires a grind and overlay at least 50 feet long for the full width of any affected lane. The final engineering plans must show the extent and nature of required trench and pavement restoration, including copies of the appropriate detail drawings. However, the City’s CIP project on 152nd will probably include installation of new pavement on that street. It may be possible to coordinate the school project with the City’s project to eliminate the need for the school project to do a grind and overlay on 152nd. Such coordination should be examined as the design of the City’s project progresses. The exact extent of any grind and overlay will be determined in the project’s right of way use permit prior to construction. See below for specific Conditions of Approval.

Any trench and pavement restoration in Newport Way must meet the requirements of King County.

E. Concurrency

The State Growth Management Act and the Bellevue Traffic Standards Code (BCC 14.10) require concurrency testing for development projects under certain conditions. Concurrency testing is based on a comparison of traffic (including that generated by the project) to street capacity that exists or will exist within six years. Bellevue’s concurrency testing focuses on the PM peak hour; which, for concurrency purposes, is defined as the highest hour between 4 PM and 6 PM. Elementary schools typically have very low trip generation during that time. The City’s trip generation data for elementary schools indicate a rate of 0.01 PM peak hour trips per student. Based on 450 students, the site would generate approximately five total PM peak hour trips. Bellevue City Code 14.10 requires concurrency testing for any development project that would generate 30 or more new PM peak hour trips. Thus, the proposed project is well below the threshold for requiring a concurrency test.

F. Long-Term Impacts

The City evaluates the long-term traffic impacts of a project by comparing the future land use predicted for a project site to the assumptions made in the Environmental Impact Statement for the City’s Transportation Facilities Plan, which was most recently updated and adopted in November 2006. The Eastgate Elementary School lies within Traffic Analysis Zone (TAZ) 158, which was predicted to contain 40,000 square feet of institutional development by the year 2017. The Eastgate Elementary School, which is the only known institutional use in that TAZ, presently has 42,500 square feet; approximately the same as the TAZ assumption for the year 2017. The school is planned to increase to a total of 62,000 square feet. However, the increase in square footage is to accommodate more and better services, not to accommodate an increase in the number of students. Trip generation for a school is generally based on the number of students. Therefore, trip generation at the school site is not expected to increase as a result of the school’s reconstruction, and the school’s traffic impact will continue to be consistent with the land use assumptions of the Environmental Impact Statement for the Transportation Facilities Plan. Thus, long-term area-wide traffic impacts of the proposed project are not an issue.

G. Transportation Impact Fee

Bellevue City Code 22.16.070.B.2 exempts publicly funded educational institutions from the City's transportation impact fee requirements.

See Sections X, XI, and XII for related conditions.

2. Utilities Department

The Utilities Department reviewed the conceptual design only. Changes to the site layout may be required to accommodate the utilities after utility engineering is approved. All design review, plan approval, and field inspection shall be performed under the Developer Extension Agreements. See Section XI for conditions. At the time of writing this staff report, the applicant had submitted the required Developer Extension Agreements for water and storm.

3. Fire Department

The Fire Department has approved this proposal based upon the completion of these items prior to Temporary Certificate of Occupancy or Certificate of Occupancy for this project: They are as follows:

- Fire and sprinkler alarms
- Fire department connection
- Standpipe outlets
- Fire access
- Elevator connection
- Detention pipes in access road
- Gates
- Know Box

See Sections XI and XII for related conditions to above items.

4. Parks Department

The Parks Department has reviewed and approved the submitted proposal.

VII. State Environmental Policy Act (SEPA)

The Bellevue School District is an agency with SEPA jurisdiction, which permits the District to complete their own environmental determinations. The District has chosen to exercise this right for this project. A Determination of Non-Significance (DNS) was issued on August 31, 2007 with an appeal period ending September 16, 2007. A copy of this DNS can be located within the project file.

VIII. Applicable Decision Criteria

Administrative Conditional Use: The Director may approve or approve with modifications an application for Administrative Conditional Use if it complies with the decision criteria of Land Use Code Section 20.30E.140. After conducting the various administrative reviews of this project, including Comprehensive plan goals and policies and the Land Use Code provisions, the following conclusions are made with regard to the Administrative Conditional Use decision criteria:

1. The Administrative Conditional Use is consistent with the Comprehensive Plan.

This proposal is located within the Newcastle Subarea. The Comprehensive Plan designation for this site is Single-Family--High, which is consistent with the zoning classification of R-5 for this property. This proposal is consistent with the Comprehensive Plan's goals and policies.

The following are applicable Comprehensive Plan policies for the Newcastle subarea:

General Land Use Policy

Policy S-NC-8: Participate, as a partner with King County, in the decision making process for issues in the Subarea related to the provision of services, annexation, development review, and construction inspection.

Response: City of Bellevue staff has contacted King County to determine if they have development review comments on this proposal. Although this site is within the City of Bellevue, it is surrounded by adjacent properties located within King County jurisdiction. County staff has provided recommendations for City staff to consider with this proposal. See Section VI.D.3.b to review County comments.

The District has operated a school at this location since 1955 and is not proposing a change of use but will maintain this site as an elementary school to serve the surrounding neighborhood. This application does not increase the intensity of school use on this site beyond what is already occurring now but provides minimal capacity to accommodate student demand that may occur in the future. Given these considerations, this proposal is consistent with the policy above.

Community Focal Point Policy

Policy HS-9: Encourage cooperation with the school district in the development and utilization of schools as a focal point for the identification of needs and delivery of services to children and families.

Response: Eastgate Elementary serves as a focal point for the community. The school is not only used for educational purposes for also for human services delivery as well. It is also the recreational hub for the neighborhood as families can bring their children after school hours to utilize the outdoor play equipment and covered play area.

Partnership and Cooperation Policies

Policy S-NC-56: Require that park facilities, built as part of a private development, be consistent with the City of Bellevue park standards.

Policy HS-8: "Encourage the development of partnerships among the City, schools, human services providers, and others to address the needs of children and families within the school setting.

Response: The City of Bellevue Parks and Human Services Department and the Bellevue School District have developed a partnership of shared services for neighborhood community. This has been formally implemented through Resolution 5840 (see Attachment A). Parks often schedules a myriad of after school sports activities in BSD facilities—particularly for use of existing sports fields. Parks has reviewed this proposal and has determined that the new sports

field will be beneficial to the community as there will be a backstop and soccer field for public use after school. This proposal complies with both of the policies above.

Policy TR-38: "Require mitigation to provide safety and site access, and to mitigate neighborhood impacts as needed to address the effects of development."

Policy TR-46: "Maintain and enhance safety for all users of the roadway network using measures such as.....Improve the opportunities for pedestrians to safely cross streets at intersection and mid-block locations....."

Response: The Comprehensive Plan policies above support improved parking and circulation to accommodate pick-up and drop-off vehicles, implementation of policies to improve pick-up and drop-off activity, and installation of street and pedestrian improvements to improve safety. Such improvements help provide an adequate street system by reducing conflicts and improving safety.

- 2. The design is compatible with and responds to the existing or intended character, appearance, quality of development and physical characteristics of the subject property and immediate vicinity.**

See Section III for a description of the site and building design. The proposal fulfills this policy as it has been sensitively designed to blend in with the adjacent neighborhood. The proposed colors and materials will complement adjacent single-family development.

- 3. The Administrative Conditional Use will be served by adequate public facilities, including streets, fire protection and utilities.**

The site will be served by adequate public facilities including fire protection and utilities. See Sections X, XI, and XII for related conditions.

- 4. The Administrative Conditional Use will not be materially detrimental to uses or property in the immediate vicinity of the subject property.**

As conditioned, the demolition of the existing school and construction of a new facility will not be detrimental to the adjacent neighborhood. The new structure has been designed to consolidate the building footprint and internalize exterior corridors through construction of a two-story facility.

This application eliminates three portables to allow more efficient use of space for this site. As a nonresidential use within a residential neighborhood, the District has designed an attractive facility to complement the adjacent neighborhood while avoiding an "institutional" effect. The new facility will create a civic focal point for the neighborhood as a place not only to educate their children but to also meet and congregate together as a community.

As conditioned, short term impacts related to noise generation as a result of the construction will be minimized. Normal construction hours are from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. Exceptions to the construction hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020C.1 when necessary to accommodate construction on schools which cannot be undertaken during exempt hours. However, prolonged exposure to noise created by extended hour construction activity is likely to have a significant impact on inhabitants of surrounding residential properties during the proposed timeline for construction that extends from June 2008 to August 2009. In order to minimize detriment on residential uses in the immediate vicinity of the Eastgate Elementary

School, the District and the Contractor should not rely on City issuance of a blanket exemption from the Noise Control Code during the pendency of the construction period. Allowances for short term work outside of normal construction hours will be limited and will be reviewed on a case by case basis to verify necessity and ensure appropriate noise mitigation is utilized to protect surrounding uses and properties. If expanded hours are necessary to accommodate a specific component of the school construction, **the District must apply for a separate noise permit for review and approval by staff.** See Section XI for related condition.

5. The Administrative Conditional Use complies with the applicable requirements of this Code.

School facilities are permitted to be located within residential zones as an Administrative Conditional Use. As conditioned, the proposal meets the requirements of the LUC. See Sections X, XI and XI for related conditions.

IX. Decision of the Director

After conducting the various administrative reviews associated with this proposal, including applicable Land Use consistency, and City Code and Standard compliance reviews, the Director of Planning and Community Development does hereby recommend **APPROVAL WITH CONDITIONS:**

X. General Conditions of Approval:

- 1. Re-Examine Traffic and Parking if Students Exceed 450:** If the student population exceeds 450 at any time during the life of this Administrative Conditional Use Permit, then the City may require additional traffic analysis and may require the school district to obtain additional parking or implement additional transportation management programs, if necessary to mitigate the transportation or parking impacts of the higher student population.

Reviewer: Carl Wilson, 425-452-4228
Authority: LUC 20.30E.165, 170, 175

- 2. Reexamine Traffic and Parking if Church Parking is Lost:** If the school's use of the Eastgate Congregational Church parking lot is lost at any time during the life of this Administrative Conditional Use Permit, then the City may require additional traffic analysis and may require the school district to obtain additional parking or implement additional transportation management programs, if necessary to mitigate the impacts of the loss of parking.

Reviewer: Carl Wilson, 425-452-4228
Authority: LUC 20.30E.165, 170, 175

- 3. Dedication of Easement / Right of Way on 152nd Avenue SE:** In lieu of constructing curb, gutter, sidewalk, and street widening on 152nd Avenue SE, the Bellevue School District agrees to dedicate whatever reasonable amount of right of way or easement area will be needed for the City's CIP project on 152nd Avenue. Any survey work needed for said dedications will be done by the City. Said dedications must be complete before the City begins construction on the CIP project, but no later than initial occupancy for the new school building.

Reviewer: Carl Wilson, 425-452-4228
Authority: BCC 14.60.060, 090, 100, 110, 181

XI. Conditions Required Prior to Issuance of Clear and Grade and Building Permits:

1. **Signs:** If any change is made to the existing sign, a separate sign package shall be submitted to PCD for staff review and approval. Any proposed sign shall be architecturally compatible with the existing building.

Authority: BCC 22B.10.040.B.1,2
Reviewer: Antoinette Pratt, (425) 452-5374

2. **Final Engineering Plans:** Prior to issuance of the project's clearing and grading permit, the engineering plans must be complete and must correctly show all street frontage improvements and related access improvements. All transportation improvements must meet the requirements of City Code, the Transportation Department Design Manual, the Americans with Disabilities Act, and requirements stated elsewhere in this Staff Report, especially as stated in Section VI.1.D. Plans submitted on November 26, 2007 are satisfactory for approval of the Administrative Conditional Use permit. However, further engineering review for the Clear and Grade or Building permits and coordination with the City's CIP project may necessitate changes to engineering plans or details. Standard detail drawings from the Design Manual must be included in the final engineering plans as appropriate. Requirements include the following:
 - a. Design and construction of improvements along 152nd Avenue SE must be coordinated with the City's CIP project as much as feasible, given the schedules of the two projects.
 - b. The school district is responsible for constructing driveway connections to 152nd Avenue SE, as determined during engineering review. Driveway connections must have sufficient width to accommodate bus turning movements.
 - c. Parking lots and on-site drive aisles shall be constructed per the final engineering plans.
 - d. In order to control traffic flow and improve safety, traffic control signs and pavement markings shall be installed per City, state, and MUTCD standards, as appropriate. Such features must be included in the final engineering plans. Exact locations will be determined in the field by the Transportation Department inspector.
 - e. No new overhead utility lines will be allowed along or across any City street right of way. Existing overhead lines may need to be undergrounded, depending on the conditions of the right of way use permit for the relevant utility company.
 - f. All sidewalks, asphalt walkways, ramps, driveway crossings, and crosswalks must be ADA compliant.
 - g. Storm drainage treatment for transportation facilities must meet Utility Department requirements, as appropriate.
 - h. Any construction work or traffic revisions within the right of way of Newport Way must meet the requirements of King County.

Reviewer: Carl Wilson, 425-452-4228
Authority: BCC 14.60.110, 150, 180, 181, 190, 210, 240, 241, and 250

- 3. Trench and Pavement Restoration:** Presently, 152nd Avenue SE is classified as “overlay required” near the school site. Any trenching or damage to the street surface requires a grind and overlay at least 50 feet long for the full width of any affected lane. The final engineering plans must show the extent and nature of required trench and pavement restoration, including copies of the appropriate detail drawings. However, the City’s CIP project on 152nd will probably include installation of new pavement on that street. It may be possible to coordinate the school project with the City’s project to eliminate the need for the school project to do a grind and overlay on 152nd. Such coordination should be examined as the design of the City’s project progresses. The exact extent of any grind and overlay will be determined in the project’s right of way use permit prior to construction. Any trench and pavement restoration in Newport Way must meet the requirements of King County.

Reviewer: Carl Wilson, (425) 452-4228
Authority: BCC 14.60.250

- 4. Right of Way Use Permit:** The applicant is required to apply for a right-of-way use permit from the City of Bellevue Transportation Department, Right of Way division, before the issuance of any clearing and grading, building, foundation, or demolition permit. In some cases, more than one right of way use permit may be required, such as one for hauling and one for construction work within the right of way. A right of way use permit regulates activity within the City right of way, including but not limited to the following:

- a. Designated truck hauling routes.
- b. Truck loading and unloading activities.
- c. Hours of construction and hauling.
- d. Continuity of pedestrian facilities.
- e. Temporary traffic control and pedestrian detour routing for construction activities.
- f. Street sweeping and maintenance during excavation and construction.
- g. Location of construction fences.
- h. Parking for construction workers.
- i. Construction vehicles, equipment, and materials in the right of way.
- j. All other construction activities as they affect the public street system.

Additionally, the clearing and grading permit requires a calculation of the amount of material to be imported/exported.

Reviewer: Jon Regalia, 425-452-4599
Authority: BCC 14.30 and 14.60.250

- 5. Final Utilities Approval:** The Utilities Department approval of the Administrative Conditional Use application is based on the preliminary utility design. Final civil engineering of the utility design may require changes to the site layout to accommodate the utilities.

Authority: BCC Title 24.02, 24.04, 24.06
Reviewer: Mark Dewey, (425) 452-6179

- 6. Developer Extension Agreement:** The water, sewer, and storm drainage systems shall be designed per the current City of Bellevue Utility Codes and Utility Engineering Standards. Utilities Department design review, plan approval, and field inspection is performed under the Developer Extension Agreement and Utilities Permit Processes.

Authority: BCC Title 24.02, 24.04, 24.06
Reviewer: Mark Dewey, (425) 452-6179

- 7. Demolition:** Demolition/construction shall conform to the requirements of the IFC. Fencing or other method shall be used to keep non-construction personnel out of the demolition/construction area.

Authority: International Fire Code (IFC), Chapter 14
Reviewer: Adrian Jones, (425) 452-6032

- 8. Early Child Care Center:** The sprinklers in the Early Child Care Center shall remain in service during construction.

Authority: IBC 903
Reviewer: Adrian Jones, (425) 452-6032

- 9. Detention pipes:** The detention and water quality vaults in the parking lot area shall be capable of supporting fire apparatus with a gross weight of 64,000 lbs. (rear axle=48,000 lbs and front axle=19,000 lbs) and shall support the weight of the ladder truck outrigger which is 45,000 lbs over an 18 inch square.

Authority: BFDSS 6-3 & Development Information Sheet
Reviewer: Adrian Jones, (425) 452-6032

- 10. Construction Hours:** Normal construction hours are from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. No deliveries shall be scheduled prior to 7:00 a.m. to 6:00 p.m. as well. Exceptions to the construction hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020C.1 when necessary to accommodate construction on schools which cannot be undertaken during exempt hours. No blanket exemption exists. Allowances for short term work outside of normal construction hours shall be limited and will be reviewed on a case by case basis to verify necessity and ensure appropriate noise mitigation is utilized to protect surrounding uses and properties. If expanded hours are necessary to accommodate a specific component of the school construction, If expanded hours are necessary to accommodate a specific component of the school construction, the District must apply for a separate noise permit for review and approval by staff. In this time period, the site shall be posted on all street frontages prior to the start of construction activity.

Authority: BCC 9.18.040
Reviewer: Antoinette Pratt (425) 452-5374

- 11. Single Site Agreement:** A single-site agreement shall be signed prior to the issuance of a building permit to mitigate setback requirements along the western property line in lieu of a Boundary Line Adjustment.

Authority: LUC 20.20.010, Dimensional Standards
Reviewer: Antoinette Pratt (425) 452-5374

XII. Conditions Required Prior to Temporary Certificate of Occupancy:

- 1. On-Site Parking, Circulation Improvements and Monitoring:** The school district shall implement on-site parking and circulation improvements as shown in the final site plan and engineering plans. On-site circulation and safety shall be enhanced with pavement markings,

directional signs, and stop signs as needed. The school district shall implement the use of one or more on-site traffic monitors whose duties will include managing pick-up and drop-off traffic to improve safety and reduce impacts on City streets. At the beginning of each school year and as needed throughout each school year, the school district shall provide information to parents regarding proper traffic behavior and safety during pick-up and drop-off periods.

Prior to issuance of a temporary certificate of occupancy for this project, the school district shall submit a policy plan for City review and approval. This policy plan shall outline how the school district will implement traffic monitoring, on-site traffic management, and the provision of related information to parents and students. The goal of the plan shall be to prevent any on-site parking and circulation problems from spilling off the site. The document shall state the school district's commitment to achieving that goal on an on-going basis, including changing the plan as needed and keeping on-site staff informed. The policy plan shall be signed by a person with authority to make such a commitment for the school district.

Reviewer: Carl Wilson, 425-452-4228, and Toni Pratt, 425-452-5374
Authority: BCC 20.30E.140 and Comprehensive Plan Policy TR-90

2. **Completion of Transportation Improvements:** Prior to issuance of a temporary certificate of occupancy, all street frontage improvements, pedestrian improvements, and transportation conditions shown on the final engineering plans or required by City codes or standards or by the conditions of approval stated herein must be completed to the satisfaction of the Transportation Department and approved by the Transportation Department's inspector. Landscaping, signage, etc. adjacent to driveways shall not block sight distance triangles.

Reviewer: Carl Wilson, 425-452-4228
Authority: BCC 14.60.110, 14.60.240 and 241

3. **Completion of Work within King County Right of Way on Newport Way:** Prior to issuance of a temporary certificate of occupancy, all street frontage improvements and driveway revisions required by King County within the right of way of Newport Way must be completed to the satisfaction of King County. All work shall be reviewed, approved, and inspected through an associated King County Right-of-Way Use permit.

Reviewer: King County
Authority: King County

4. **Fire Sprinklers/Fire Flow/Fire Hydrants:** Automatic fire sprinklers shall be provided throughout the building along with the necessary fire flow. The applicant shall also be required to provide fire hydrants within 400 feet of all portions of the building.

Authority: IFC 508 and 903, IFC Appendix B and C
Reviewer: Adrian Jones, (425) 452-6032

5. **Fire Alarm Panel and Notification System:** A fire alarm panel shall be located at the entrance to the building along with a fire alarm notification system throughout the building.

Authority: IFC 903, 907 and BCC 5749
Reviewer: Adrian Jones, (425) 452-6032

- 6. Fire Lane and Gates:** No parking fire lanes will be posted on all access roads and shown on the plans. Additionally, all gates shall be approved by the Fire Department.

Authority: IFC 508

Reviewer: Adrian Jones: (425) 452-6032

- 7. Fire Department Connection (FDC):** Provide an FDC connection at an approved location at least 50 feet from the building and within 50 feet of a hydrant.

Authority: IFC 903 and BFDDS

Reviewer: Adrian Jones, (425) 452-6032

- 8. Knox Box:** A Knox box is required at the main entrance of the school.

Authority: IFC 506

Reviewer: Adrian Jones, (425) 452-6032

Attachments

- A. Resolution 5840
- B. Plans and Drawings

ATTACHMENT A
(Resolution 5840)

CITY OF BELLEVUE, WASHINGTON

RESOLUTION NO. 5840

A RESOLUTION adopting a joint resolution with the Bellevue School District to create a partnership to meet the needs of the community by focusing on schools as community resource centers.

WHEREAS, the complexity of community problems points to an increasing need for all governmental units and related public service organizations to mobilize their respective resources for the common purpose of improving the quality of community life; and

WHEREAS, the Bellevue School District and the City of Bellevue have a long history of joint cooperation in using public facilities and developing programs; and

WHEREAS, other related public, non-profit, and private community organizations provide programs and services for community betterment; and

WHEREAS, the limited amount of tax money and other resources available to meet public demands for facilities, programs, and services requires that it be used efficiently; and

WHEREAS, our community has facilities, equipment, and staff organized for the purpose of providing educational opportunities for children and youth; and

WHEREAS, our school and park facilities are a major focal point of this community; and

WHEREAS, one of a community's largest investments--its school buildings--could be used more efficiently to provide educational, recreational, cultural, and service programs for community residents of all ages; and

WHEREAS, great potential social and economic benefit can be derived from cooperation in facility use and program development for the benefit of all citizens; and

WHEREAS, a cohesive strategy to coordinate the efforts of the public, non-profit, and private sectors in developing and coordinating use of these community resources is needed; and

WHEREAS, we believe that there is a direct link between the quality of community life and the ability of the School District successfully to fulfill its mission to provide K-12 education; and

WHEREAS, improving the quality of community life by providing facilities, services and programs is the mission of the City; now, therefore,

THE CITY COUNCIL OF THE CITY OF BELLEVUE, WASHINGTON, DOES
RESOLVE AS FOLLOWS:

Section 1. The City of Bellevue joins the Bellevue School District in adopting the following mutual goal:

To further our common interest in enhancing the quality of community life in Bellevue, the City and School District will work as partners to meet the educational, recreational, cultural, social, health and human services needs of the community by focusing on schools as community resource centers.

Section 2. In furtherance of the goal established in Section 1 of this resolution, the City and the Bellevue School District agree to:

- A. Marshal the resources of the whole community to develop programs and deliver services needed or desired by community residents.
- B. Expand the uses and hours of operation at all public facilities to better meet the needs of the community.
- C. Identify and overcome barriers to joint facility use and program development and support.
- D. Explore ways to institutionalize and fund programs that will support the use of schools as community resource centers.
- E. Approve the City/School District 1995 Joint Work Program that will test and evaluate the use of schools as community resource centers with four pilot projects at several schools in the District's East Attendance Area Community. These projects include:

ORIGINAL

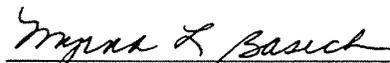
1. A Community School at Phantom Lake Elementary that is using school facilities to provide lifelong learning opportunities for all ages.
2. Joint Middle School Master Planning at Tillicum that is looking at ways to enhance community use and access to school facilities.
3. A Human Services Collaborative that is finding better ways to give children and families access to health and human services at six schools in the attendance area.
4. A Neighborhood Outreach effort at Tillicum and its "feeder elementary schools" that is seeking to involve the local community in identifying needs that could be met at these neighborhood schools.

PASSED by the City Council this 5th day of December,
1994, and signed in authentication of its passage this 5th day of
December, 1994.

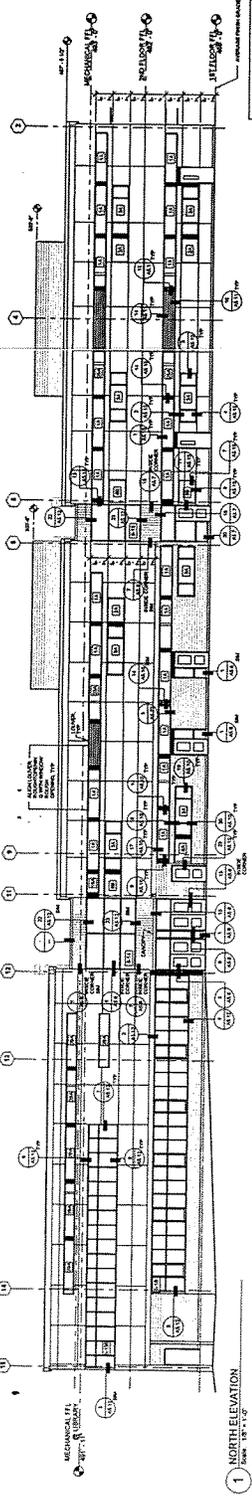
(SEAL)


Donald S. Davidson, DDS, Mayor

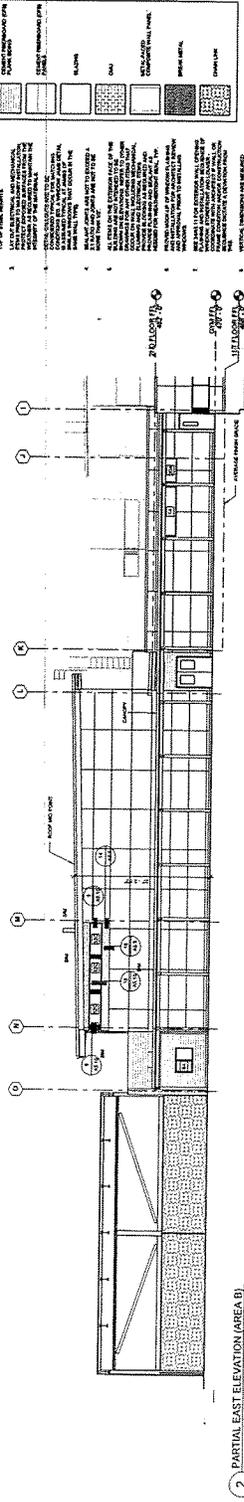
Attest:


Myrna L. Basich, City Clerk

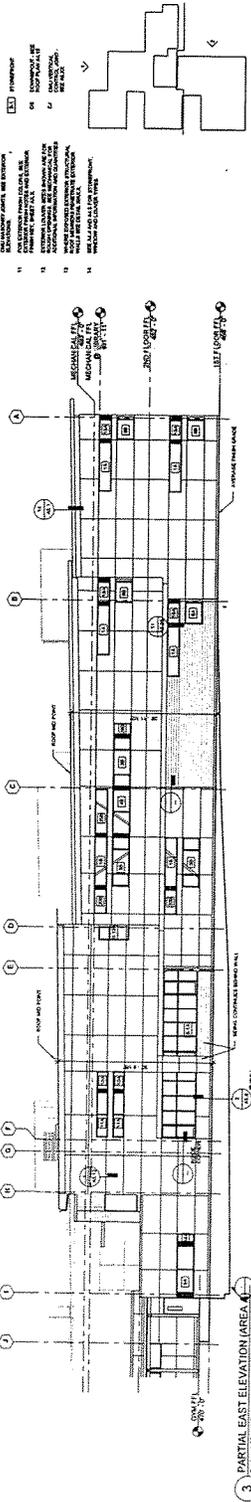
ATTACHMENT B
(Plans and Drawings)



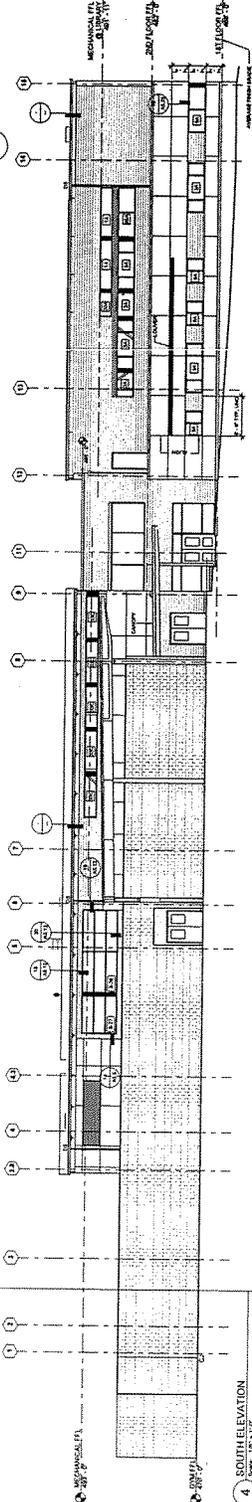
1 NORTH ELEVATION
 Scale: 1/8" = 1'-0"



2 PARTIAL EAST ELEVATION (AREA B)
 Scale: 1/8" = 1'-0"



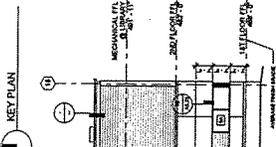
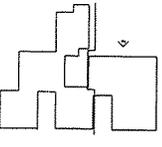
3 PARTIAL EAST ELEVATION (AREA A)
 Scale: 1/8" = 1'-0"



4 SOUTH ELEVATION
 Scale: 1/8" = 1'-0"

EXTERIOR FINISHING MATERIALS

1. 1/2" x 1/2" x 1/2" SQUARE TILE
2. 1/2" x 1/2" x 1/2" SQUARE TILE
3. 1/2" x 1/2" x 1/2" SQUARE TILE
4. 1/2" x 1/2" x 1/2" SQUARE TILE
5. 1/2" x 1/2" x 1/2" SQUARE TILE
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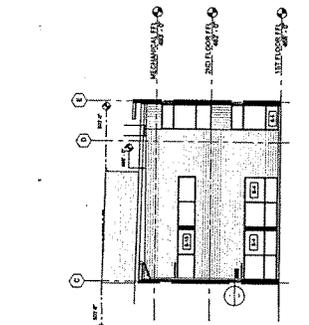
MECHANICAL PLAN

EXTERIOR ELEVATIONS

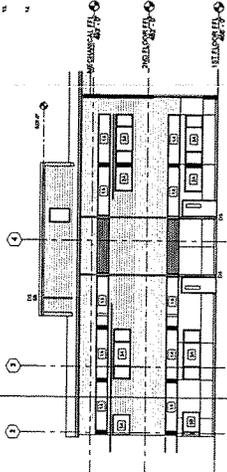
REVISION	DATE	DESCRIPTION
1	11/11/08	ISSUED FOR PERMITS
2	11/11/08	ISSUED FOR PERMITS
3	11/11/08	ISSUED FOR PERMITS
4	11/11/08	ISSUED FOR PERMITS
5	11/11/08	ISSUED FOR PERMITS
6	11/11/08	ISSUED FOR PERMITS
7	11/11/08	ISSUED FOR PERMITS
8	11/11/08	ISSUED FOR PERMITS
9	11/11/08	ISSUED FOR PERMITS
10	11/11/08	ISSUED FOR PERMITS
11	11/11/08	ISSUED FOR PERMITS
12	11/11/08	ISSUED FOR PERMITS
13	11/11/08	ISSUED FOR PERMITS
14	11/11/08	ISSUED FOR PERMITS
15	11/11/08	ISSUED FOR PERMITS
16	11/11/08	ISSUED FOR PERMITS

GENERAL NOTES:

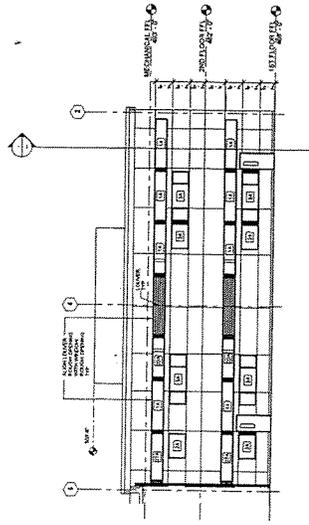
1. ALL ELEVATIONS SHALL BE TO FINISH GRADE UNLESS OTHERWISE NOTED.
2. ALL MATERIALS SHALL BE APPROVED BY THE ARCHITECT AND THE DISTRICT.
3. ALL MATERIALS SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS.
4. ALL MATERIALS SHALL BE INSTALLED ACCORDING TO THE DISTRICT'S SPECIFICATIONS.
5. ALL MATERIALS SHALL BE INSTALLED ACCORDING TO THE DISTRICT'S SPECIFICATIONS.
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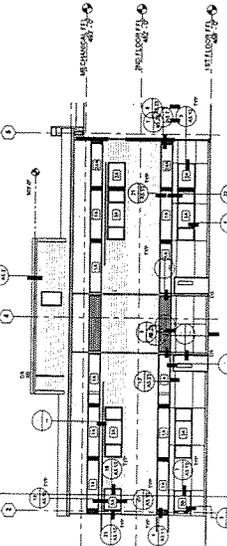
12 NORTH COURTYARD ELEVATION - WEST
 Scale: 1/8" = 1'-0"



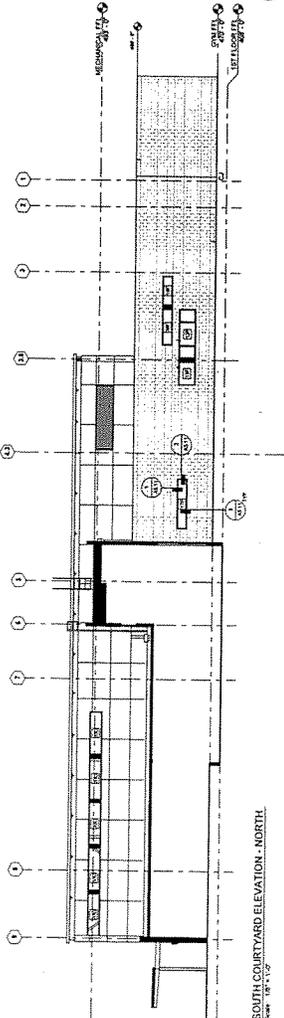
14 SOUTH COURTYARD ELEVATION - SOUTH
 Scale: 1/8" = 1'-0"



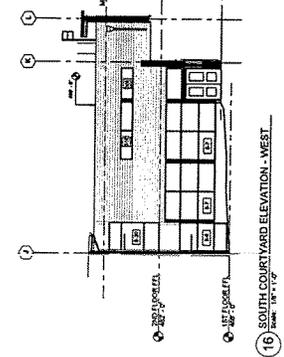
11 NORTH COURTYARD ELEVATION - NORTH
 Scale: 1/8" = 1'-0"



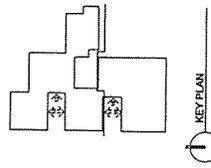
13 NORTH COURTYARD ELEVATION - SOUTH
 Scale: 1/8" = 1'-0"



15 SOUTH COURTYARD ELEVATION - NORTH
 Scale: 1/8" = 1'-0"



16 SOUTH COURTYARD ELEVATION - WEST
 Scale: 1/8" = 1'-0"



KEY PLAN

