



DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT
 ENVIRONMENTAL COORDINATOR
 11511 MAIN ST., P.O. BOX 90012
 BELLEVUE, WA 98009-9012

DETERMINATION OF NON-SIGNIFICANCE
 and Adoption of the 2006-2017 Transportation Facilities Plan
 Final Environmental Impact Statement (TFP EIS)
 issued November 30, 2006; and incorporation by reference of
 the Bellevue Place final EIS issued June 10, 1986

PROPONENT:	Kemper Development Company
LOCATION OF PROPOSAL:	10500 NE 8 th Street
DESCRIPTION OF PROPOSAL:	Construct an expansion to the existing Bellevue Place Hyatt Hotel. The hotel expansion (approx. 340,000 gross square feet) will be a 19-story tower with 351 hotel rooms, an exhibition hall, one ballroom, meeting rooms, offices and retail. There will be five levels of underground parking with 516 parking stalls. Site improvements include landscaping and site utilities.

FILE NUMBER: 06-123807-LD

The Environmental Coordinator of the City of Bellevue has determined that this proposal does not have a probable significant adverse impact upon the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(C). This decision was made after the Bellevue Environmental Coordinator reviewed the completed environmental checklist and information filed with the Land Use Division of the Department of Planning & Community Development. This information is available to the public on request.

- There is no comment period for this DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's office by 5:00 p.m. on _____.
- This DNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on the DNS. There is a 14-day appeal period. Only persons who submitted written comments before the DNS was issued may appeal the decision. A written appeal must be filed in the City Clerk's Office by 5 p.m. on **September 13, 2007**.
- This DNS is issued under WAC 197-11-340(2) and is subject to a 14-day comment period from the date below. Comments must be submitted by 5 p.m. on _____. This DNS is also subject to appeal. A written appeal must be filed in the City Clerk's Office by 5 p.m. on _____.

This DNS may be withdrawn at any time if the proposal is modified so that it is likely to have significant adverse environmental impacts; if there is significant new information indicating, or on, a proposals probable significant adverse environmental impacts (unless a non-exempt license has been issued if the proposal is a private project); or if the DNS was procured by misrepresentation or lack of material disclosure.

Carol V. Hurlend
 Environmental Coordinator

August 30, 2007
 Date

OTHERS TO RECEIVE THIS DOCUMENT:

State Department of Fish and Wildlife
State Department of Ecology, Shoreline Planner N.W. Region
Army Corps of Engineers
Attorney General
Muckleshoot Indian Tribe

*Scari
8/30/07*

City of Bellevue Submittal Requirements 27a

ENVIRONMENTAL CHECKLIST 4/18/02

If you need assistance in completing the checklist or have any questions regarding the environmental review process, please visit or call the Permit Center (425-452-6864) between 8 a.m. and 4 p.m., Monday through Friday (Wednesday, 10 to 4). Our TTY number is 425-452-4636.

BACKGROUND INFORMATION

Property Owner:
Kemper Development Company

Proponent:
Kemper Development Company

Contact Person:
Daniel Meyers, Kemper Development Company's Vice President of Design and Construction

Address:
Kemper Development Company
575 Bellevue Square
Bellevue, WA 98004

Phone: 425-646-3660
Fax: 425-460-5811
e-mail: dmeyers@kemperdc.com

Proposal Title:
Bellevue Place Hyatt Expansion

Proposal Location:
(Street address and nearest cross street or intersection) Provide a legal description if available.
The Bellevue Place Hyatt Expansion project would primarily occur on the north central and northwestern portions of the superblock, on lots currently occupied by the Wallace & Wheeler offices, Wendy's Restaurant, and surface parking areas. These lots are located directly north of the existing Hyatt Regency Hotel in Bellevue Place.

The Bellevue Place Expansion site has the following street addresses: 924 and 940 Bellevue Way NE in Bellevue, Washington.

~~Please attach an 8 1/2" x 11" vicinity map that accurately locates the proposal site.~~
See ~~Figure 1.~~

order separately approved permits

will connect at the underground to existing hotel

*10500 NE 8th Street
this project site address
attached to staff report*

Give an accurate, brief description of the proposal's scope and nature:

1. General description:

The proposed expansion would include: a 351-room expansion to the Hyatt Regency Hotel for a total of 723 rooms, with additional ballroom/meeting, office, and retail space. ~~The proposed uses are attached in Figure 2.~~ The expansion would be constructed as a 19-story tower, with 5 levels of below-grade parking on an approximately 1.3-acre site located in the north central portion of the superblock. The existing 3-story Wallace & Wheeler office building, located on the western portion of the superblock, would be demolished and removed during this expansion and replaced with interim surface parking.

1.64 acres

The Project would also include modifications to some of the existing uses in Bellevue Place. Most of this has been accomplished through previously permitted projects in 2005 as "Scope 1 and 2" at Bellevue Place. Second floor renovations include large access to existing ballrooms and meeting rooms at current restaurant space on the second floor of the Wintergarden to the east.

~~Total parking with added and displaced stalls = 429. There are 5 levels of parking beneath the proposed hotel tower, with a total of 516 spaces. As part of Phase 1, 30 parking spaces would be eliminated in the north surface lot, and 80 spaces would be eliminated in the Wallace & Wheeler surface lot, located in the northwestern portion of the superblock. Thirty-one (31) surface parking spaces would be provided on the surface lot where the existing Wallace & Wheeler building is demolished.~~

Total parking with this project, Wendy's parking lot expansion and existing stalls will be 2,076 parking stalls

2. Acreage of site:

The superblock on which the proposed Bellevue Place Expansion is proposed is 348,751 square feet, or approximately 8.0 acres, in total. The existing Bellevue Place development occupies 148,133 square feet, or approximately 3.4 acres, in the southwestern portion of the superblock. Low-rise office buildings occupy 77,057 square feet, or approximately 1.8 acres in the northeastern portion of the superblock. The Bank of America building occupies 29,973 square feet, or approximately 0.7 acres, in the southeastern corner of the superblock. This project, the Bellevue Place Hyatt Expansion occupies 57,168 square feet, or approximately 1.3 acres, in the northern portion of the superblock. The Phase 2 expansion area.

1.64

3. Number of dwelling units/buildings to be demolished:

None

4. Number of dwelling units/buildings to be constructed:

None Hotel expansion with guest rooms (351 rooms)

5. Square footage of buildings to be demolished:

One existing building totaling 24,679 (Wallace and Wheeler) square feet ~~would be~~ demolished on site to allow development.

has been

approved under separate permits

6. Square footage of buildings to be constructed:

This project would add a total of 603,462 square feet of parking and hotel uses to the existing Bellevue Place development.

7. Quantity of earth movement (in cubic yards):

Phase 1 would require excavating approximately 119,175 cubic yards of soil for the underground parking garage and foundation support system. ✓

8. Proposed land use:

Parking and hotel uses

interior administ. conference rooms, ballrooms, retail, offices for hotel ✓

9. Design features, including building height, number of stories and proposed exterior materials:

The height above average grade of the 18-story hotel tower proposed in Phase 1 would be 220 feet, including 15 feet of roof feature/mechanical screen.

up 15' screening mech. equip.

227.5 ft. (max allowed 235 ft.)

Exterior materials for the lower two to three levels of the proposed building would include precast concrete, painted concrete frames, brick and concrete masonry unit walls and storefront glazing systems. The upper floors of the building would consist of painted concrete frames and walls and storefront glazing systems.

→ to match existing Hyatt building

The proposed project design would be consistent with applicable aspects of the City's Land Use Code, ~~Core Downtown Design District standards~~, zoning regulations, and Design Review requirements (see the **Land Use** and **Aesthetics** sections of this Environmental Checklist for further discussion). *MU*

10. Other

✓

Estimated date of completion of the proposal or timing of phasing:

Phase 1 construction is proposed to start in ^{Fall} Spring/Summer 2007 and be completed in Spring /Summer 2009.

Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

A future construction of 5 floors of underground parking and another residential tower on what is currently the Wendy's lot is planned in the future. No design work or construction dates have been planned at this time. The garage would connect to the garage floor plates planned in this project.

Proposed PACE project will connect to this project. PACE has not yet been submitted - not yet known.

List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

- *Wind Speed at Pedestrian Levels: An Assessment*, by Magnusson Klemencic Assoc. dated 5/14/03.
- *Bellevue Place Draft EIS* (City of Bellevue, March 1986)
- *Bellevue Place Final EIS* (City of Bellevue, June 1986)
- *Phase 1 Environmental Site Assessment* (Hart Crowser, October 2001)
- *Geotechnical Engineering Design Study, Hyatt Hotel Development* (Hart Crowser, October 2001)
- *City of Bellevue 2001-2012 Transportation Facilities Plan Draft EIS* (City of Bellevue, May 2001)
- *City of Bellevue 2001-2012 Transportation Facilities Plan Final EIS* (City of Bellevue, July 2001)
- *COB 2006-2017 TFP EIS issued 11/30/06.*

Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. List dates applied for and file numbers, if known.

None Dept of Ecology

List any government approvals or permits that will be needed for your proposal, if known. If permits have been applied for, list application date and file numbers, if known.

None applied for at this time. Future approvals as follows:

Administrative Design Review (ADR) approval (City of Bellevue)

- Building permit approval (City of Bellevue)
- Other construction permits (demolition, excavation, right-of-way, utilities, etc.) (City of Bellevue)

Please provide one or more of the following exhibits, if applicable to your proposal. (Please check appropriate box(es) for exhibits submitted with your proposal):

Land Use Reclassification (rezone) Map of existing and proposed zoning

Preliminary Plat or Planned Unit Development Preliminary plat map

Clearing & Grading Permit
Plan of existing and proposed grading
Development plans

Building Permit (or Design Review)
Site plan
Clearing & grading plan

Shoreline Management Permit
Site plan

✓

A. ENVIRONMENTAL ELEMENTS

1. Earth

Responses to the following questions are based on the subsurface explorations and geotechnical engineering design study prepared for the Bellevue Place Expansion by Hart Crowser in October 2001. This study included reviewing existing borings, drilling five new borings and obtaining soil samples, performing geotechnical laboratory tests, and conducting engineering analyses. The existing borings were performed as part of the design review process for the existing Bellevue Place development.

in DR file

- a. **General description of the site:** Flat Rolling Hilly Steep slopes Mountains
Other

20 feet

The ^{1.64} ~~approximately 2-acre~~ project site is relatively flat, with slopes of approximately ^{10%} ~~3%~~ or less. An estimated ~~10-foot~~ change in elevation occurs from the western site boundary downward to the eastern site boundary. The site is largely covered by impervious surfaces consisting of existing buildings and parking areas. Limited areas in landscaping are present on site.

- b. **What is the steepest slope on the site (approximate percent slope)?**

estimated 20 foot elev. change

Existing elevations on the project site vary from ~~161 feet to 175 feet~~; the site datum has been established at approximately 170 feet. The existing topography is relatively flat. The steepest slope on site occurs between the Wendy's Restaurant and the existing Bellevue Place north parking lot. In this area a landscaped strip acts as small retaining wall and creates a localized area of approximately 10% slope.

- c. **What general types of soil are found on the site (for example, clay, sand, gravel, peat, and muck)? If you know the classification of agricultural soils, specify them and note any prime farmland.**

Subsurface soils generally consist of a thin layer of soil fill (at limited locations), overlying soils that are glacially overridden. In order of occurrence with increasing depth, the soils at the site consist of: fill to a depth of 3 to 5 feet (at limited locations); weathered till a few feet thick (at limited locations); glacial till generally extending to elevations of about 120 to 86 feet; and glacially overridden silt, sand, and gravel.

MU zoning district

The project site is located in the City's downtown ~~core~~ area and is not classified as prime farmland.

- d. **Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe.**

There are no known surface indications or history of unstable soils on the project site or in the immediate site vicinity.

- e. **Describe the purpose, type, and approximate quantities of any filling or grading proposed. Indicate source of fill.**

The proposed Bellevue Place Expansion would require demolition and removal of the existing Wallace & Wheeler office building and associated surface parking on site. The proposed project would excavate an area of approximately 60,000 SF (^{1.64} ~~1.38~~ acres) to a maximum elevation of

*already approved under separate permits.
seems complete*

113.5 feet for underground parking. The project would require excavating approximately 119,175 net cubic yards of soil to construct the five levels of underground parking. The excavated material would not be reused on-site as structural fill. Off-site hauling would not occur without prior notification to the City of Bellevue.

In general, the site is well suited for the proposed development. The site soils are suitable for high capacity footings foundations and soldier pile/tieback or soil nail shoring systems. The presence of groundwater at certain locations may require specific control measures (see the following section on **Water** for details).

f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

During initial demolition and removal of paved areas on site, some native soil would be exposed, increasing the potential for erosion due to the silt content of the native soils. Once the excavations for the parking garages have begun, the potential for erosion would be low; the majority of the excavated areas would become closed depressions. Erosion control measures, per the Department of Ecology Best Management Practices (BMPs), would be implemented during construction to reduce the potential for erosion.

Once construction of the project is complete, no significant erosion would be anticipated, since all surfaces would be covered with buildings, paving and landscaping.

g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

The site is currently developed with less than 2 percent pervious surfaces. The proposed expansion would redevelop the site with a zero-lot-line structure and landscaping. The proposed expansion would result in a similar amount of impervious surface coverage to what currently exists.

h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any:

- A temporary erosion control plan would be implemented during construction of the Bellevue Place Expansion. All temporary erosion and sedimentation control measures would be designed and constructed in accordance with the Department of Ecology Best Management Practices (BMPs). BMPs are defined as physical, structural and/or managerial practices that when used singly or in combination, prevent or reduce pollution of stormwater runoff caused by construction activities. The Temporary Erosion and Sedimentation Control plan for the proposal would be designed to protect off-site properties, as well as minimize the quantity of sediment-laden water from entering the public stormwater control system. The system would utilize three weir tanks during construction to settle out sediment;
- The project would incorporate a construction stormwater runoff turbidity monitoring plan in accordance with the City of Bellevue Clearing and Grading Code;
- Measures to address conditions associated with excavation, shoring, foundation design, and construction are contained in the October 2001 Hart Crowser report. These include the following:
 - If soldier piles are used they would be embedded at least 10 feet below the bottom of the excavation to achieve an allowable end bearing resistance;

- Backfill within 18 inches of subgrade walls would consist of free-draining (containing less than 3 percent fines by dry weight) sand or sand and gravel, and,
- All floor slabs would be underlain by at least 6 inches of well-graded, free-draining sand or sand and gravel. Cross drains would be at a 30 to 50-foot spacing; and,
- The presence of groundwater at certain locations may require specific control measures (see the following section on **Water** for details).

per CG
 permit +
 CG inspector
 BCC 23.76 ✓

2. AIR

The City updated its ~~Transportation Facilities Plan~~ (TFP) in August 2001. An EIS was prepared on the updated TFP. The EIS addressed the likely incremental impacts that would result from implementation of the updated TFP. As part of the TFP, the City formulated assumptions about future development in the downtown core. On a broad basis, that EIS addressed the air quality impacts that would result from TFP implementation.

a. What types of emissions to the air would result from the proposal (i.e. dust, automobile odors, and industrial wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known.

The proposed project would result in localized increases in emissions to the air primarily as a result of construction activity and vehicular traffic. Potential impacts from truck traffic would primarily be from any tracking of earth and dust out of the site and onto public streets where it could be mixed into the air by passing traffic. No burning of land debris would be allowed or required for proposed site development.

Following development of the proposed Bellevue Place Expansion project, there would be no significant direct sources of emissions to the air. However, increased vehicular traffic that would occur as a result of the project would generate emissions of carbon monoxide and contribute to concentrations of ozone.

Air quality in Bellevue is regulated by three agencies: the U.S. Environmental Protection Agency (EPA), Washington State Dept. of Ecology (DOE), and the Puget Sound Air Pollution Control Agency (PSAPCA). EPA sets national standards and exercises oversight authority for DOE and PSAPCA; DOE has responsibility of mobile sources; and PSAPCA has local responsibility for regulation and permitting of stationary sources and construction emissions. The proposed Bellevue Place Expansion project would comply with applicable demolition, new construction and point source emission requirements of PSAPCA. ✓

b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.

Vehicular traffic in downtown Bellevue is a source of carbon monoxide emissions in the area. Such traffic would not be anticipated to affect the proposed project. ✓

c. Proposed measures to reduce or control emissions or other impacts to the air, if any:

- Dust control measures would include watering construction areas, covering stockpiles and vehicle loads, and sweeping paved areas. No chemical dust suppressants would be used;
- Dump trucks would have adequate freeboard or would cover loads to prevent spillage and dust generation; and
- Development would likely require a demolition permit from PSAPCA; therefore, compliance with PSAPCA's regulations concerning demolition and construction would be assured.

Construction dust
 suppressant measures
 per BCC 23.76 ✓

3. WATER

a. Surface

- (1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

No surface water body occurs on or in the immediate vicinity of the site.

- (2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If Yes, please describe and attach available plans.

The project would not require any work in or adjacent to any surface water body.

- (3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

No filling/dredging would be required.

- (4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

The proposal would not require surface water withdrawals or diversions.

- (5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.

The project site is not located within a 100-year floodplain.

- (6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

No discharge of waste material is anticipated.

b. Ground

- (1) Will ground water be withdrawn, or will water be discharged to ground water? Give general description.

Groundwater was encountered at elevation 91 feet in one of the five new borings drilled for the Bellevue Place Expansion project (see the **Earth** section for details). Of the existing borings, groundwater was encountered in three of the five borings at elevations ranging from about 87 to 107 feet. These groundwater levels are perched zones of water within the glacial soils.

Seeps and perched groundwater zones may be encountered during excavation for the Bellevue Place Expansion. The maximum excavation level of construction would be to elevation 113.5 feet. Therefore, excavations for the proposed expansion are not anticipated to encounter a groundwater table, although perched groundwater could be present.

While groundwater is not expected to be a significant issue during construction, groundwater

seepage from perched zones and probable precipitation runoff during excavations would likely require temporary drainage or other groundwater control (e.g., ditches, sump pumps, etc.) to maintain the excavation during construction.

- (2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals....; agricultural; etc.) Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

No discharge of waste materials into the groundwater is anticipated as a result of the proposed action. Stormwater runoff and sewage discharge from the project would be connected to the existing City of Bellevue systems adjacent to the site.

c. Water Runoff (Including storm water)

- (1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

The proposed project site is located within the Meydenbauer drainage basin. A 30-inch stormwater drainage line currently exists in NE 10th Street and a 48-inch stormwater drainage line currently exists in 106th Avenue NE. The City stormwater control system is routed to Meydenbauer Bay approximately 2,600 feet downstream of the project site. No adverse impacts to the existing storm drainage system downstream of the site are anticipated with the proposed development.

Stormwater control improvements associated with the proposed development would be designed and constructed in accordance with the City of Bellevue's Utility Engineering Standards (1996). For the Bellevue Place Expansion, stormwater runoff from impervious surfaces would be collected and conveyed to catch basins and would connect directly to the City stormwater control system in NE 10th Street. Alternatively, if there is insufficient capacity in the NE 10th Street system, the project could connect to the 48-inch stormwater drainage line downstream in 106th Avenue NE. Runoff from pollution generating impervious surfaces (i.e., parking areas and driveways) would receive water quality treatment via a coalescing plate vault prior to discharge to the City stormwater control system. Detention is not required or proposed for the project.

Storm water control per COB reg. UE permit, BCC 24.04

- (2) Could waste materials enter ground or surface waters? If so, generally describe.

If materials are spilled during construction (gas/diesel, etc), they would have the potential to enter perched groundwater levels during excavation for the parking garages during both Phase 1 and 2. A spill prevention program would be implemented during construction per code requirements to minimize the potential for impacts.

DOE Storm water Managt Manual for Project Sand

d. Proposed measures to reduce or control surface, ground, and runoff water impacts, if any:

- If groundwater seepage and precipitation runoff occur during excavations, temporary drainage or other groundwater control (e.g., ditches, sump pumps, etc.) would be employed to maintain the excavation during construction;
- Spill prevention, containment, and cleanup would be provided to reduce/control potential

impacts to the aquifer/groundwater per Chapter 5 of the Utility Engineering Standards;

- Stormwater control improvements associated with the proposed development would be designed and constructed in accordance with the City of Bellevue's Utility Engineering Standards (1996); and,
- Paved surfaces would be sloped so that drainage flows away from structures. Roof drainage would be collected in large tight lines for diversion into the stormwater control system.

*Standard
erosion
control
measures
per
CO permit +
geotech. review.
BCC 23.76*

4. Plants

a. Check or circle types of vegetation found on the site:

- deciduous tree: alder, maple, aspen, other
- evergreen tree: fir, cedar, pine, other
- shrubs
- grass
- N/A pasture
- N/A crop or grain
- N/A wet soil plants: cattail, buttercup, bulrush, skunk cabbage, other
- N/A water plants: water lily, eelgrass, milfoil, other
- N/A other types of vegetation

Limited vegetation is present on site. The project site currently contains approximately 98 percent impervious surfaces. Sparse landscaping (including small shrubs, ground cover, and several trees) is present in the landscaping strips that divide existing parking and drive areas. ✓

b. What kind and amount of vegetation will be removed or altered?

Most of the existing minor areas of landscaping would be removed to accommodate the proposed building and parking lot areas. The proposed expansion would require removal of the existing row of cottonwood trees along the west margin of the drop off area to the north of the Wintergarden. A landscape plan has been prepared for the Administrative Design Review submittal to the City of Bellevue. ✓

c. List threatened or endangered species known to be on or near the site.

There are no known threatened or endangered plant species on or proximate to the project site. ✓

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

- A landscape plan will be submitted to the City as part of the Administrative Design Review package; ✓

- Street trees would be planted along NE 10th Street and Bellevue Way NE; the types and spacing of these trees would adhere to the City of Bellevue Land Use Code requirements (LUC 20.25A.040). Street frontage plantings would include flowering broadleaved shrubs and groundcovers, with accent plantings of flowering annuals and perennials;
- Large-scale deciduous shade trees and street frontage screening would be integrated into the new temporary surface parking area proposed on the Wallace & Wheeler property; and,
- All new plantings would be irrigated with an automatic irrigation system. Year-round maintenance of all plantings would be provided.

5. ANIMALS

- a. Check or circle any birds and animals which have been observed on or near the site or are known to be on or near the site:

Birds: hawk, heron, eagle, songbirds, other: Songbirds primarily
 Mammals: deer, bear, elk, beaver, other Small mammals such as squirrels
 Fish: bass, salmon, trout, herring, shellfish, other:

- b. List any threatened or endangered species known to be on or near the site.

There are no known threatened or endangered species on the project site nor is there any known data that indicates that the Downtown Bellevue area serves as breeding, resting or roosting locations for threatened or endangered species.

- c. Is the site part of a migration route? If so, explain.

The project site is not part of any known migration route.

- d. Proposed measures to preserve or enhance wildlife, if any:

The project site is developed under existing conditions and is located in the Downtown ^{All zoning} ~~core~~ district. Therefore, the project does not include any measures to preserve or enhance wildlife, nor are any warranted.

6. Energy and Natural Resources

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy need? Describe whether it will be used for heating, manufacturing, etc.

The proposed Bellevue Place Expansion project would require energy for lighting, heating, ventilation, air conditioning and associated functions. The project would depend primarily upon electricity for lighting and heating. Electricity would also be used for domestic water heating, general internal and exterior lighting, telephone equipment, LAN equipment and control systems, sound and audiovisual equipment and video systems.

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe.

No.

- c. **What kinds of energy conservation features are included in the plans of the proposal? List other proposed measures to reduce or control energy impacts, if any:**

Energy conservation features would include the use of insulation, per City of Bellevue Code. The expansion would include HVAC systems with heat recovery features, automatic energy management systems, airlock entrances, and energy efficient light fixtures.

7. Environmental Health

- a. **Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe.**

No environmental health hazards are anticipated as a result of the proposal. Transport and disposal of petroleum, lead, asbestos, or other hazardous material- containing wastes encountered during demolition activities would be conducted in accordance with applicable state and federal regulations and guidelines. Operation of the proposed buildings would not result in the use or storage of substances presenting unusual risk to health.

- (1) **Describe special emergency services that might be required.**

Fire, police, and emergency medical services would be required at a level consistent with comparable office or commercial buildings. It is anticipated that no special services would be required.

- (2) **Proposed measures to reduce or control environmental health hazards, if any.**

The Bellevue Place Expansion project would meet all local, state and federal environmental health hazard requirements, including the following:

- The contractor would prepare and follow a health and safety plan, as well as a spill control and cleanup plan prior to and during construction of the project; and
- Transport and disposal of petroleum, lead, asbestos, or other hazardous material - containing wastes would be conducted in accordance with applicable state and federal regulations and guidelines.

b. Noise

- (1) **What types of noise exist in the area which may affect your project (for example, traffic, equipment, operation, other)?**

The predominant source of ambient noise surrounding the project site and downtown area is from vehicular traffic. Existing noise in the area would not significantly affect proposed uses.

- (2) **What types and levels of noise would be created by or associated with the project on a short-term or long-term basis (for example, traffic, construction, operation, other)? Indicate what hours noise would come from the site.**

Construction of the Bellevue Place Expansion project would generate short-term construction-related noise. Noise associated with demolition, site preparation and

6pm
6pm
Truck traffic noise + construction noise - per BCC 9.18
generators, transformers
construction activity would be the most intense noise type and would be of the shortest duration. Construction related activities in the downtown area are typically permitted Monday through Friday in the early morning hours (i.e., 7 AM) up to 10:00 PM in the evening, and on Saturdays between 9:00 AM and 5:00 PM by the City of Bellevue. Long term noise indirectly related to the project would be primarily associated with increases in vehicular traffic; such increases are not expected to be significant.

(3) Proposed measures to reduce or control noise impacts, if any:

- Subject to City of Bellevue discretion, construction-related activity would be limited to specific hours Monday through Saturday to avoid conflicts with peak traffic volumes and residents in the project vicinity;
 - Noise control measures would include proper muffling of diesel, gasoline, and air-powered equipment prior to construction;
 - Construction noise from the site would remain in compliance with maximum permissible environmental levels, as set forth in WAC and as adopted by the Bellevue City Code (9.18.010); and,
 - The general contractor would submit a noise suppression mitigation plan to the City for review and approval prior to the start of construction.
- BCC 9.18

8. Land and Shoreline Use

a. What is the current use of the site and adjacent properties?

The area is currently occupied by the north surface parking lot for the existing Bellevue Place development, and a portion of the surface parking area for the Wallace & Wheeler Building.

The project site is bordered on the north by NE 10th Street; on the south by the existing Bellevue Place mixed-use development and the Bank of America office building; on the east by existing low-rise office buildings; and on the west by Bellevue Way. Land uses in the site vicinity include low-, mid- and high-rise buildings containing office, retail, commercial, and hotel uses. To the north of the site across NE 10th Street and to the west of the site across Bellevue Way are low-rise retail uses. To the south of the site across NE 8th Street is Lincoln Square, a mixed-use development including retail, office, movie theater, hotel, and residential uses. The Bellevue Square regional shopping mall is located to the southwest of the project site.

b. Has the site been used for agriculture? If so, describe.

No.

c. Describe any structures on the site.

Two buildings currently occupy the project site: the three-story, 24,679-square foot Wallace and Wheeler building and the one-story, 2,975-square foot Wendy's restaurant.

on adjacent site - not a part of subject DR approval

d. Will any structures be demolished? If so, what?

Only the Wallace and Wheeler building is scheduled for demolition with the Bellevue Place Expansion. The Wendy's building will remain.

separate from this project

e. What is the current zoning classification of the site?

The project is located in the City of Bellevue's Downtown Mixed Use (DNTN-MU) zone ~~and the Downtown Core Design District.~~

f. What is the current comprehensive plan designation of the site?

Development in downtown Bellevue is guided by the goals and policies of the ~~Central Business District (CBD)~~ ^{Downtown} Subarea Plan. The majority of the superblock on which the Bellevue Place Expansion would occur is designated as Multiple Use on the land use diagram in the ~~CBD~~ ^{Downtown} Subarea Plan. Edges of the Retail Focus and Office Focus overlap the southern portion of the block.

g. If applicable, what is the current shoreline master program designation of the site?

N/A

h. Has any part of the site been classified as an "environmentally sensitive" area? If so, specify.

No

i. Approximately how many people would reside or work in the completed project?

A total of approximately 417 people would be expected to work in the complete project. No residents other than "temporary" hotel guests.

j. Approximately how many people would the completed project displace?

~~Significant employment displacement is not anticipated. The businesses operating within the Wallace & Wheeler building and Wendy's could relocate in the downtown vicinity.~~ ^{None - Vacant property}
Separate from this DE approval.

k. Proposed measures to avoid or reduce displacement impacts, if any:

New development would increase the employment opportunities associated with the expanded hotel and added retail space would result in the Downtown area.

l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

- The proposed expansion project is consistent with existing plans and policies of the City of Bellevue;
- Proposed new landscaping (i.e., street trees) would soften the appearance of the proposed buildings and temporary surface parking areas, particularly from adjacent roadways; and,
- Pedestrian mid-block access would be provided through the building to connect the existing Wintergarden with NE 10th Street.
- *Design of building materials consistent w/ existing Hyatt Hotel.*

9. Housing

a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing.

None

- b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.

None

- c. Proposed measures to reduce or control housing impacts, if any:

None

10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

The 19-story hotel tower proposed would be 220 feet in height above average grade, plus 15 feet of roof feature/mechanical screen. The height of this building would be within the maximum 220 feet above average grade allowed by the City of Bellevue height regulations. *227.5 (incl. 15' ht. above for rooftop mech. equip. screening) Max per code = 235' (incl. 15' rooftop mech. equip. screening)*

Exterior materials for the lower two to three levels of the proposed building would include precast concrete, painted concrete frames, brick and concrete masonry unit walls and storefront glazing systems. The upper floors of the building would consist of painted concrete frames and walls and storefront glazing systems.

- b. What views in the immediate vicinity would be altered or obstructed?

The current proposal was encompassed by the overall development analyzed in the 1986 *Bellevue Place EIS*. In particular views of an overall development encompassing development of similar height and bulk to the current proposal were analyzed in that document. Buildings assumed in that analysis were 220 feet in height above average grade, the maximum allowed by the City of Bellevue height regulations. As indicated in that analysis, middle views of the project site (those observed from across the street or at a distance of a block or so) would be of zero-lot line structures and landscaping, typical of those that generally occur in downtown Bellevue. Distant views of the Cascades from some nearby residences are already blocked by existing high-rise buildings. The Bellevue Place Expansion would fill in additional portions of the northwest Bellevue skyline. ~~Overall, no significant new impacts to views would be expected as a result of the proposal.~~ The proposed development would continue the existing pattern of development in the area.

Some western view of WA Sq. to east may be blocked - WA Sq. still under construct.

- c. Proposed measures to reduce or control aesthetic impacts, if any:

Landscape plans will be submitted to the City of Bellevue as part of the Administrative Design Review process. In compliance with City Code requirements (LUC 20.25A.040), street trees would be included in the proposed landscaping design.

11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

Typical light sources for the development would include interior lighting, pedestrian-level lighting along sidewalks, parking lot lighting, entryway lighting, and illuminated signs. Specific

information on building lighting would be provided as part of the Administrative Design Review and building permit processes.

b. Could light or glare from the finished project be a safety hazard or interfere with views?

Glazing of the buildings exterior windows would be non-reflective. Light and glare from the finished project would not be expected to cause safety hazards or interfere with views from adjacent commercial properties or streets. ✓

c. What existing off-site sources of light or glare may affect your proposal?

Light and glare from other commercial buildings and traffic headlights are produced in the site area. Off-site sources of light or glare would not affect the proposal. ✓

d. Proposed measures to reduce or control light or glare impacts, if any:

- All exterior lights would include non-glare fixtures and horizontal cutoffs to minimize potential spillover impacts to adjacent properties; and ✓
- A non-reflective glazing system would be installed as part of the exterior window system.

12. Recreation

a. What designated and informal recreational opportunities are in the immediate vicinity?

Public recreational facilities and spaces within two miles of the project site include:

- 1) Lake Washington (several local waterfront parks)
- 2) Major Public Open Space (Compass Plaza) at N.E. 6th St. and 106th Ave. N.E.
- 3) Ashwood Plaza and Ashwood Court playfield (2.3 acres west of the Bellevue Regional Library at the intersection of 110th Avenue NE and NE 10th Street) ✓
- 4) McCormick Park strip (6.8 acres on the north side of NE 12th Street)

Bellevue Square and Lincoln Square, privately owned retail shopping malls, are another area attraction located to the south and southwest of the project site.

b. Would the proposed project displace any existing recreational uses? If so, describe.

No existing park or recreational use would be displaced.

c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

- Pedestrian sidewalks would continue to be provided along Bellevue Way and along NE 10th Street under the proposal.
- Pedestrian mid-block access would be provided through the building to connect the existing Wintergarden with NE 10th Street.
- The existing Bellevue Place development includes an athletic club for the use of hotel patrons and employees. ✓

13. Historic and Cultural Preservation

- a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? If so, generally describe.

None have been identified.

- b. Generally describe any landmarks or evidence of historic, archeological, scientific, or cultural importance known to be on or next to the site.

None are known.

- c. Proposed measures to reduce or control impacts, if any:

None are proposed or necessary.

14. Transportation

~~See attached Tables 1, 2, 3.~~

Responses to the following questions are based on the preliminary transportation study prepared for the proposed Bellevue Place Expansion by TDA Inc. Three (3) pages are attached. This study incorporated assumptions and methodologies that are consistent with the City's transportation analysis efforts from the *City of Bellevue 2001-2012 Transportation Facilities Plan (TFP)*. The study addresses trip generation, distribution and operational impacts associated with the development for the Bellevue Place Expansion. Potential impacts of the current proposal would fall within the range of impacts cited for the development scenario. Expansion of the Bellevue Place development through the superblock was originally contemplated in the mid 1980s. An Environmental Impact Statement (EIS) was prepared in 1986 that addressed both the initial development of Bellevue Place (the existing development as it stands today) and subsequent phases on the northern portion of the superblock proposed in this project. The development levels called for in the current proposal fall within the overall development analyzed in the 1986 EIS. In particular, the vehicular trip generation that would result from development of the current proposal falls within the total trip generation assumed for the entire superblock and analyzed in the prior EIS.

The proposed expansion is part of the overall Bellevue Place development contemplated on the superblock in the mid 1980s. An Environmental Impact Statement was prepared on the entire development in 1986 and a portion of the contemplated development has occurred. The current proposal is encompassed by the overall development analyzed in the 1986 EIS. In particular the potential trip generation and associated transportation impacts of an overall development encompassing development similar to the current proposal were addressed in that document.

The City updated its Transportation Facilities Plan (TFP) in August 2001. The TFP identifies the transportation facilities and improvements that will be needed to accommodate future growth in the City to the year 2012. The City prepared an EIS on the updated TFP that addressed the likely incremental impacts that would result from its implementation. The EIS specifically addressed the transportation and air quality impacts that would result from TFP implementation. In preparing the TFP EIS, the City formulated assumptions about future development in the downtown core. The City assumed more extensive development at Bellevue Place and other downtown properties than has actually occurred or is anticipated to occur by 2012. Therefore, the proposed Bellevue Place Expansion falls within the development assumed in the traffic modeling for the TFP EIS.

See discussion
in staff
report for
Transportation
COB 2006-2017
TFP EIS
issued
11/30/06.

- a. Identify public streets and highways serving the site, and describe proposed access to the existing street system. Show on site plans, if any.

Current classifications and configurations of streets serving the project site are as follows:

- NE 10th Street, a minor arterial, borders the superblock to the north;
- NE 8th Street, a principal arterial, borders the superblock to the south;
- 106th Avenue NE, a minor arterial, borders the superblock to the east; and,
- Bellevue Way NE, a principal arterial, borders the superblock to the west.

The proposed underground parking garage would be an expansion of the existing parking garage beneath Bellevue Place. Primary access to the existing and proposed garage is provided from NE 10th Street and NE 8th Street. A secondary access for hotel valet parking is currently located on Bellevue Way and would be available for the proposed hotel expansion. Trucks and delivery vehicles would use the existing separate underground loading area, which would be expanded under the proposal, using the exclusive driveway from 106th Ave. NE. Passenger drop-off/loading zones would be accessible from all streets.

A total of 11 curb cuts currently exist on the project site's frontage. Under the proposal, there would be a total of 10 curb cuts at the completion of Phase 2.

No new driveways are proposed.

- b. **Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop?**

A total of nine routes travel past Bellevue Place. Transit stops are located on NE 8th Street at Bellevue Way and 106th Ave. NE, and on 106th Ave. NE at NE 10th St. The project site is located approximately 3 blocks to the northwest of the Bellevue Transit Center.

- c. **How many parking spaces would be completed project have? How many would the project eliminate?**

Bellevue Place Hyatt Expansion would provide ⁵¹⁶~~429~~ parking spaces. Five levels of parking beneath the proposed hotel tower, with a total of 516 spaces. As part of Phase 1, 30 parking spaces would be eliminated in the north surface lot, located in the northern portion of the superblock, and 80 spaces would be eliminated in the Wallace & Wheeler surface lot, located in the northwestern portion of the superblock. Thirty-one (31) interim surface parking spaces would be provided on the Wallace & Wheeler parcel.

See pg 2 for total Bellevue Place

The proposed on-site parking supply is sufficient to meet typical daily project demands. Special events, such as a large mid-day ballroom event, may create spillover parking. For the occasions when special event parking demand exceeds supply, options such as valet parking or off-site parking would be employed.

A tunnel to Lincoln Square is being planned to help share parking.

- d. **Will the proposal require any new roads or streets, or improvements to existing roads or streets, not including driveways? If so, generally describe (indicate whether public or private).**

No new roads or streets, or improvements to existing roads or streets would be required as a result of the proposal.

- e. **Will the project use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.**

No.

- f. **How many vehicular trips per day would be generated by the completed project? If known, indicate when peak volumes would occur.**

~~Please see attached tables for trip generation.~~

See Transp. analysis in st. rept.

- g. **Proposed measures to reduce or control transportation impacts, if any:**

Preliminary review of the development indicated the proposed scenario would fulfill concurrency requirements throughout the City of Bellevue. If the project's land use program remains unchanged, and if the City's traffic is not updated in the interim, this preliminary concurrency analysis will become the official concurrency analysis.

~~Under the proposed development scenario, the Bellevue Place Expansion is not expected to create any significant traffic operational impacts requiring mitigation. Intersection levels of service would not change from background conditions in 2006 with the addition of Bellevue Place traffic. Other transportation mitigation proposed for the project include:~~

See Transp. analysis in st. rept.

- Bellevue Place's active transportation demand management program would be expanded to incorporate the proposed uses; and,
- For the occasions when special event parking demand exceeds supply, options such as valet parking or off-site parking would be employed.

15. Public Services

- a. **Would the project result in an increased need for the public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe.**

The proposed project would increase the need for fire protection, police protection and health care based on the level of development on site. Fire and police protection would be provided by the City of Bellevue. It is anticipated that the City has adequate capacity to serve the project with fire and police protection services. ✓

- b. **Proposed measures to reduce or control direct impacts on public services, if any.**

- The proposed building would conform to current City of Bellevue Building Code requirements for fire prevention, including sprinklers thus, the potential demand on fire services would be reduced; and,
- The project would contribute to the tax base of the City of Bellevue to offset the increased need for public services. ✓

16. Utilities

- a. **Circle utilities currently available at the site: electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other.**

Sewer and Water:	City of Bellevue
Electricity and Natural Gas:	Puget Sound Energy
Telephone:	US West
Refuse:	As provided by local franchised/authorized firms(s) contracted through the City of Bellevue.

- b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity that might be needed.

Sewer

The proposed project would be served by the City of Bellevue municipal sewer system. An 8-inch sewer line currently exists in NE 10th Street and an 18-inch sewer line currently exists in 106th Avenue NE. Sanitary sewer discharge from the project would be routed to the existing 18-inch sewer line in 106th Avenue NE. This sewer line has adequate capacity to serve the project.

Water Supply

The proposed project would be served by the City of Bellevue municipal water system. An 8-inch water line currently exists in NE 10th Street. Water supply should be adequate to meet the proposed project demand and City Code requirements. Existing water lines and fire services in NE 10th Street would need to be relocated prior to construction of the project. Details on how municipal water would be supplied to the project would be provided during the building and construction permit process.

Solid Waste

The City of Bellevue would provide solid waste and recycling services to the proposed development.

Energy

Puget Sound Energy would provide electrical and gas service to the project.

Telephone

US West would provide telephone services to the project.

Signature

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature

Bradley R. Smith Selater Partners Architects

Date Submitted

29 Sept '08





City of Bellevue
Department of Planning and Community Development
Land Use Division Staff Report

Proposal Name: Bellevue Place Hyatt Expansion

Proposal Address: 10500 NE 8th Street

Proposal Description: Construct an expansion to the existing Bellevue Place Hyatt Hotel. The hotel expansion (approx. 340,000 gross square feet) will be a 19-story tower with 351 hotel rooms, an exhibition hall, one ballroom, meeting rooms, offices and retail. There will be five levels of underground parking with 516 parking stalls. Site improvements include landscaping and site utilities.

File Number: 06-123807-LD

Applicant: Kemper Development Company

Decisions Included: Combined Design Review and SEPA, Process II

Planner: Carol Saari, Associate Planner *CSaari*

State Environmental Policy Act Threshold Determination: **Determination of Non-Significance and Adoption of the 2006-2017 Transportation Facilities Plan Final Environmental Impact Statement (TFP EIS) issued November 30, 2006; and incorporation by reference of the Bellevue Place final EIS issued June 10, 1986.**

Carol V. Helland

Carol V. Helland
Environmental Coordinator

Director's Decision: **Approval with Conditions**

Carol V. Helland for

Matthew A. Terry, Director
Dept. of Planning & Community Development

Notice of Decision Date: 08/30/07
Appeal Deadline: 09/13/07

For information on how to appeal a proposal, visit the Permit Center at City Hall or call (425) 452-6880. Comments on State Environmental Policy Act (SEPA) Determinations can be made with or without appealing the proposal within the noted comment period for a SEPA Determination. Appeal of the Decision must be received in the City Clerk's Office by 5 PM on the date noted for appeal of the decision.

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ATTACHED:

- Exhibit A: Project Plans
- Exhibit B: Vicinity Map
- Exhibit C: Zoning Map
- Exhibit D: Comprehensive Plan Policies
- Exhibit E: Certificate of Concurrency and Concurrency Analysis Spread Sheet

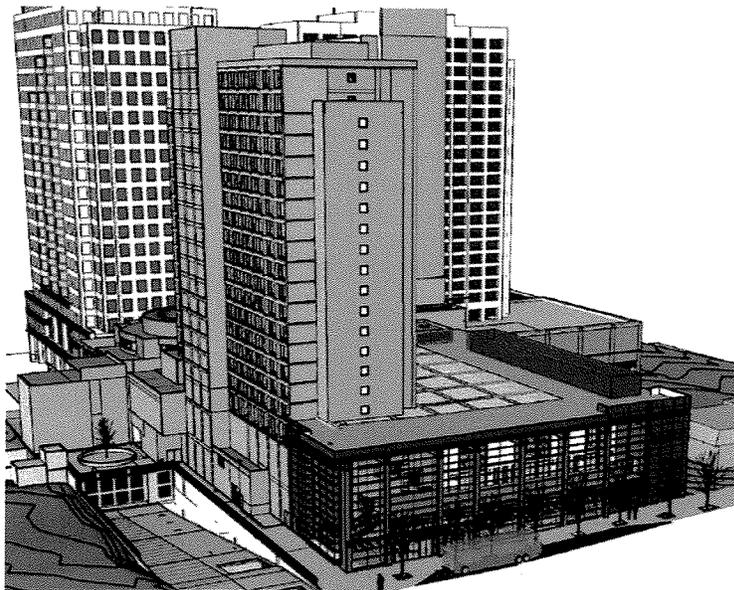
I. REQUEST/PROPOSAL DESCRIPTION

The applicant requests Design Review approval to expand the existing Bellevue Place Hyatt Hotel. The hotel expansion (338,570 gross square feet) will be a 19-story tower with 351 hotel rooms, an exhibition hall, one ballroom, meeting rooms, offices and retail. There will be five levels of underground parking with 516 parking stalls. Site improvements include landscaping and site utilities.¹

Refer to Exhibit A, Project Plans, for more information.

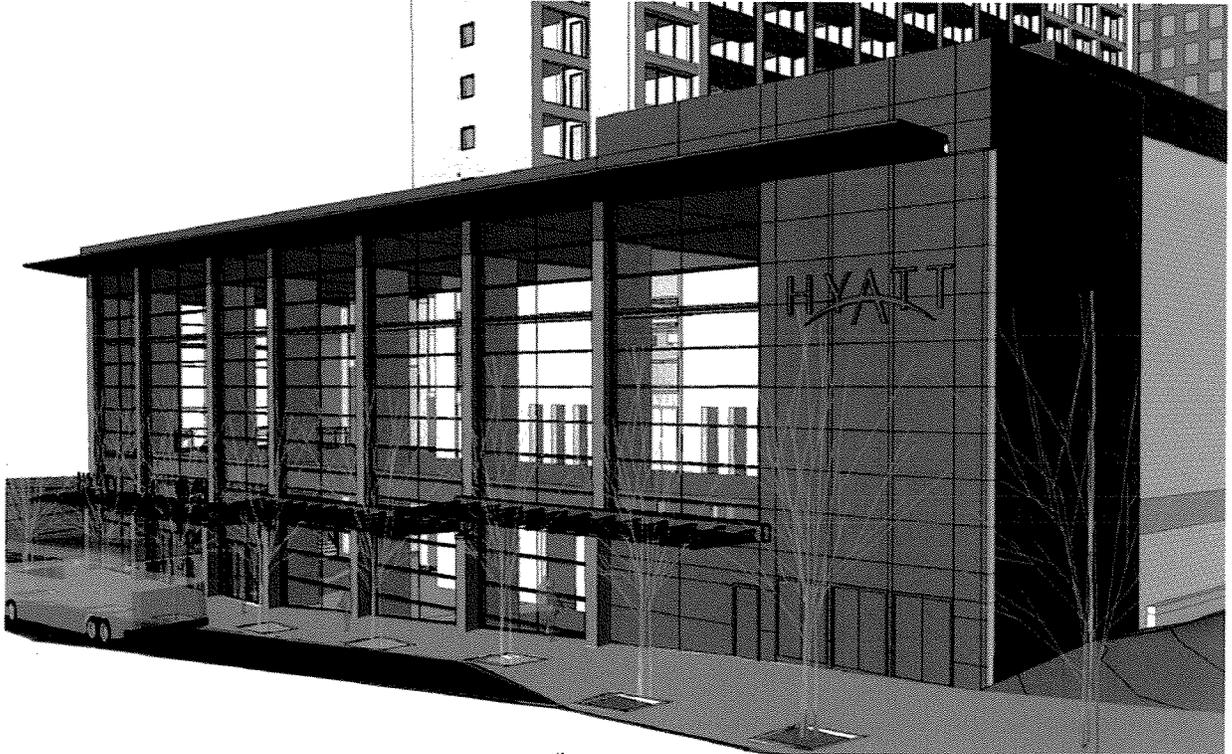
The Hyatt Hotel expansion is a part of the overall Bellevue Place development within the superblock between NE 8th Street & NE 10th Street and Bellevue Way NE & 106th Ave. NE. Upon completion of this project, the superblock complex will contain three towers: the original Hyatt Hotel at 24 stories, the existing Bank of America building at 21 stories and the new Hyatt Hotel expansion tower at 19 stories located on the north central portion of the super block.

The existing Hyatt Hotel is approximately 337,500 gross square feet with 1560 parking stalls (shared with other uses onsite). With the expansion, the Hyatt Hotel will be a total of approximately 676,070 gross square feet. The total parking count with the expansion (Wendy's site, existing parking and proposed parking) will be 2,076 parking stalls. The underground parking garages will connect as well as the existing and proposed hotel towers at the 1st and 2nd levels. When the Performing Arts Center (PACE) is constructed, there will be a connection from this hotel expansion to PACE on the 2nd level.



Perspective from NE 10th Street (northeast corner of site)

¹ The adjacent Wallace and Wheeler site and Wendy's parking lot are not a part of this Design Review approval. Permits were already issued for demolition and clearing and grading activities. The demolition of the Wallace and Wheeler building is complete. The clearing and grading is now underway for expansion of the parking lot for Wendy's.



Perspective from NE 10th Street (northwest corner)

A. Site Design

The site is a 1.64 acre parcel in downtown Bellevue. The site slopes from west (high) to east (low) with a slope of about 10% and elevation drop of about 20 feet. The site is currently being used as a staging area for construction of the Wendy's surface parking lot (approved under separate permits). Existing areas to remain are the vehicular ramp to the underground parking garage, the landscape buffer along the east and the plaza in the southeast corner of the site.

The proposed building faces onto NE 10th Street. The building covers most of the site, with the exception of areas dedicated to the driveway ramp, Wendy's (including parking), pedestrian areas and landscaping. Vehicular access for residents and guests will be from the driveway ramp at NE 10th Street. Vehicular access is also available from Bellevue Way and NE 8th Street at the existing driveway entrances to the Hyatt Hotel. There will be no new curb cuts for vehicular access to the underground parking garage.

The project includes standard roadway frontage improvements along NE 10th Street and Bellevue Way. NE 10th Street will have a new pull off bus load/unload area in front of the building entrance, new sidewalks, street trees and planting areas. Bellevue Way will have new street trees and plantings to match the plantings to the south at the existing Hyatt Hotel. All street trees (on NE 10th Street and Bellevue Way) shall be installed with structural soils. See condition #X.B.1. The Wendy's new surface parking lot will have

parking area landscaping and outside seating. The existing linear landscape buffer along the east perimeter will remain until the Performing Arts Center (PACE) is constructed (when the buffer is targeted for removal).

The existing plaza in the southeast corner of this site (with raised circle tree planter) is an area that has an unfriendly environment, run down and used as a smoking/cell phone area for building workers. The applicant proposes to retain this area for the new Hyatt expansion until PACE is constructed (when the plaza is targeted for demolition). If the PACE project is abandoned or delayed, from submitting the Design Review application, 5 years or more from the date of this subject approval, the applicant for the Hyatt expansion shall submit for review and approval a new plaza plan which includes warmer plaza materials (i.e. wood) and less concrete, more seating, tables, pedestrian-scale lighting, seasonal flowers, ornamental shrubs and trees at a pedestrian scale (not to exceed 15' at maturity). The intent of this condition is to create a more friendly pedestrian environment in this plaza corner as an outdoor green refuge area which could attract hotel guests and conference/ballroom guests. See condition #X.E.1.

Site design amenities include underground parking for the hotel and a mid-block connection through the building connecting to the existing Hyatt Hotel (providing a NE 10th Street to NE 8th Street connection). The mid-block connection must be signed and remain open during business hours. Mid-block connection signs shall be installed to be consistent with the mid-block connection plan, as established by the original Bellevue Place Design Review approval. Signs shall be consistent with the City of Bellevue Downtown Wayfinding document dated June 2005. See condition #X.D.1.

Related Site Design under Separate Permit Approval

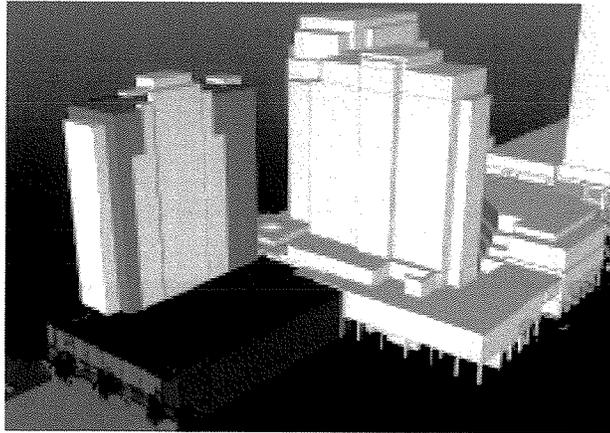
The "Wendy's site" has been redesigned so that Wendy's will remain and there will be a new surface parking lot extending to the south (in the location of the former Wallace and Wheeler building). This work has received prior permit approval. The existing Wallace and Wheeler building has already been demolished. The new Wendy's parking area will provide a longer queuing space for the existing drive through. The parking area will be landscaped to City requirements. Seating will be provided on the south side of the building for the public and restaurant users.

B. Building Design

Project Design Intent

The design of the tower is very similar to the existing tower. It is lower in height (by approximately 50 feet), but will use the same materials and colors as the existing tower. The tower will consist of a concrete frame with stucco end walls. This frame and end walls will be painted to match the currently two tone scheme of the existing tower with tan and terra cotta colors. The top of the tower will be tapered with stepbacks at the upper floors for stairs and mechanical screening. The applicant has worked with the City to provide consistency to the towers and to provide a more interesting building form.

The design of the base is very similar to the current porte-cochere and entry to the Hyatt. The façade facing NE 10th will be open to the ballroom pre-function spaces on both levels. The height of the podium and large glazing areas are very similar to the proposed PACE (Performing Arts Center Eastside) façade. The tower is setback from the street, so this lower façade will be the main architectural feature to interact with the street front. The large glazing areas will provide views for pedestrians and the public into the space. Columns will be brick to match the current podium / base of the existing Bellevue Place development.



Aerial view from the northwest corner

Other Design Elements

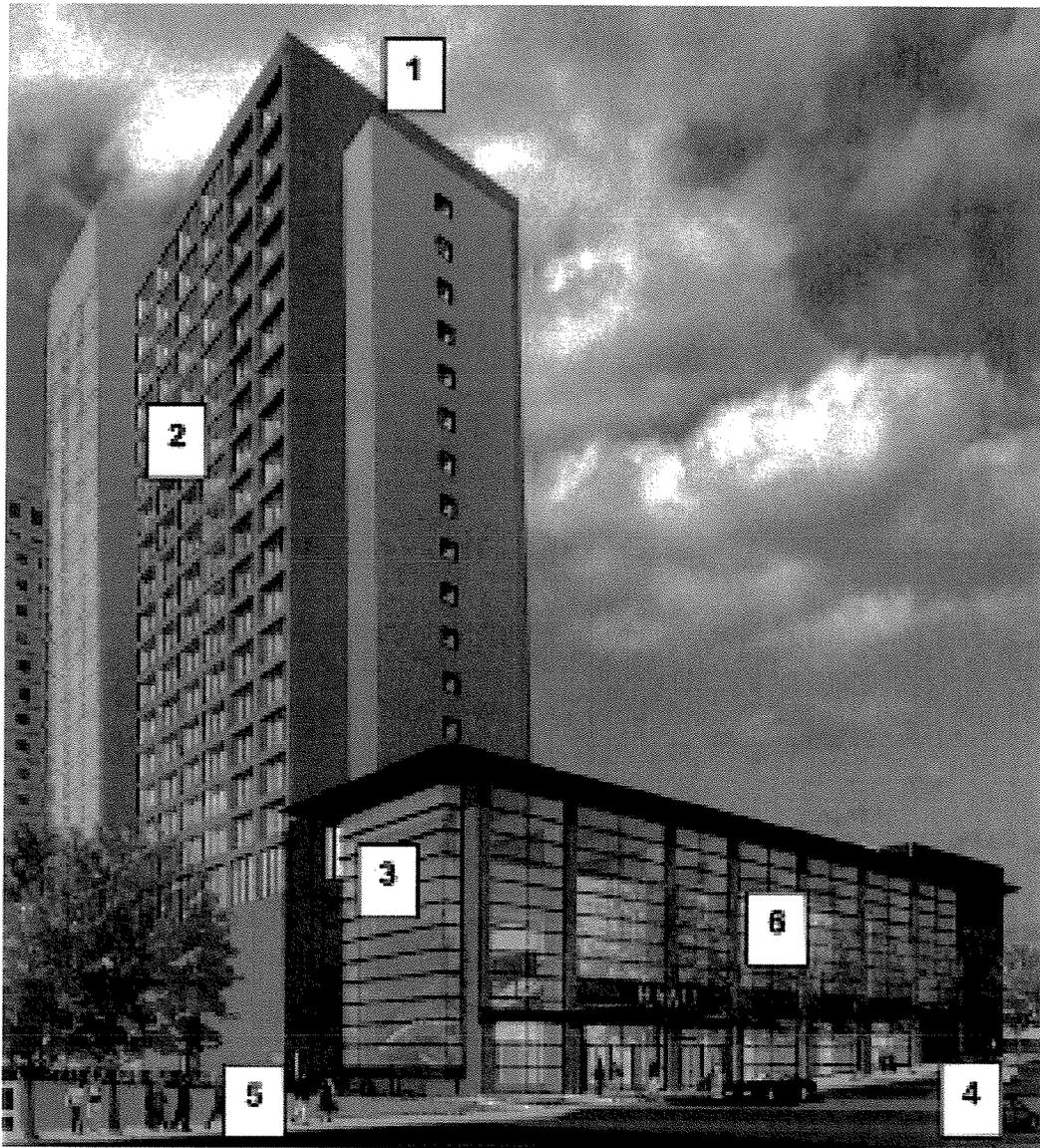
The garage will be accessed in the same locations as Bellevue Place: NE 10th Street, 106th Ave. NE, NE 8th Street and Bellevue Way NE. No new access locations are proposed, however the entrance to the garage at NE 10th will be widened as part of this project. The project will also provide a bus turn out lane at NE 10th so that buses will not stop traffic at NE 10th. The applicant has worked with the City on width of the bus lane and proposing different color materials to distinguish it from NE 10th while still providing the required sidewalk widths and a pleasant pedestrian sidewalk experience along NE 10th.

A continuous marquee, similar to those along the street frontages of Bellevue Place, is proposed on NE 10th for pedestrian protection. The applicant proposes marquees over the doorways. The final colors and material of the marquees shall be submitted for review and approval. See condition #X.C.1.

The applicant submitted a conceptual sign package which will be finalized during construction of the building. A separate sign permit approval is required. See condition #X.D.1.

Rooftop Equipment

All rooftop equipment will be screened as required by codes. The roof itself will have a pattern so as to provide a more interesting feature than a plain black roof when viewed from above. The applicant shall submit drawings of the roof pattern for review and approval. Rooftop equipment shall be screened from all sides and above. Rooftop equipment shall be architecturally incorporated into the tower design. See condition #X.C.2.



Rendering of project as viewed from NE 10th Street, looking Southwest

Legend:

- 1) Top of building tapered to mirror existing Hyatt top. Tapered top at stair towers, mechanical equipment and elevator shafts.
- 2) Two toned color scheme at concrete frame of tower to match existing colors and materials.
- 3) Height of podium has been coordinated with PACE for similar bulk and scale at the NE 10th facing elevations. Tower set back from podium to reduce bulk at the street level.
- 4) Continuous marquee at NE 10th and street trees for pleasant pedestrian experience. Views and glazing into the building. Widening of NE 10th for the bus load/unload area in front of the building.
- 5) Widening of existing ramp into the parking garage at NE 10th.
- 6) Exterior materials are clear glazing, metal panel roof extensions and brick columns. All of these materials are consistent with the existing entry into the hotel at the plaza entered into the existing complex at NE 8th and Bellevue Way.

Refer to Exhibit A, Project Plans, for more information.

II. LAND USE CONTEXT AND ZONING

Land Use Context

The site is located along the south side of NE 10th Street between Bellevue Way NE and 106th Ave. NE. Wendy's fast food restaurant is located to the west along Bellevue Way NE, an office building is located to the east (future PACE location), the existing Hyatt Hotel is to the south and auto sales/retail sales are located to the north.

The general vicinity, from NE 8th Street to NE 12th Street between Bellevue Way NE and 112th Ave. NE, is undergoing transition and new development. There are numerous new multi-family residential projects to the east of this location, with the closest being the Washington Square residential/office development along NE 10th Street and 106th Ave. NE.

See attached Vicinity Map, Exhibit B.

Zoning

The property is zoned Downtown – Mixed Use. It is not located within the Downtown Core, City Center or any Perimeter Districts. **See Exhibit C, Zoning Map.** There is no concomitant agreement attached to the property. The proposed hotel use is permitted outright in this zoning district. See Section III.A.1 below for more discussion about land uses.

III. CONSISTENCY WITH LAND USE CODE/ ZONING REQUIREMENTS

A. General Provisions of the Land Use Code

1. Use

The applicant proposes a hotel expansion which is allowed outright in the Downtown-MU zoning district. Within the hotel expansion, there will be a conference area/ballroom, one retail store along the mid-block connection to the existing Hyatt Hotel, and administrative offices for the hotel.

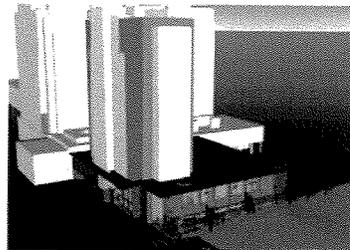
2. Dimensional Requirements

As conditioned, all applicable dimensional requirements of the Land Use Code would be met. Refer to the following tables for further information.

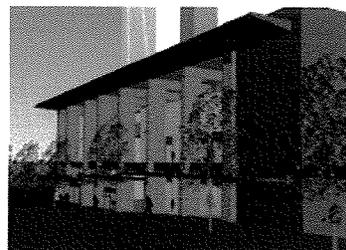
Dimensional Requirements

Item	Permitted/Required	Proposed	Comments/Conditions
Project Limit	No minimum	71,442 SF	
Building Height LUC 20.25A.020, 20.25A.020.B.4.a	Basic 150 FT and Maximum 200 FT plus 10% (20 FT) for interesting roof form and 15' for rooftop mechanical screening = 235 FT	227.5 FT	Meets code w/incentive for modulated tower form and integrated rooftop mechanical screening.
Floor Area Ratio (FAR) LUC 20.25A.020	Basic 2.0 and Maximum 5.0	4.74	Meets code.
Project Total Gross Square Feet (GSF) LUC 20.50.022	357,210 GSF (at 5.0 FAR)	338,570 GSF	Meets code. Excludes parking and mechanical areas from GSF.
Floor Area per Floor Above 40 Feet & 80' LUC 20.25A.020 Note (18)	Above 40 feet = 22,000 SF max. Above 100 feet = 20,000 SF max.	Above 40 feet = 12,289 SF max. Above 100 feet = 12,289 SF max. up to Level 18. 10,865 SF max. Levels 19 & 20	Meets code.
Setbacks			
Item	Permitted/ Required	Proposed	Comments/Conditions
Front(s) LUC 20.25A.020	0 FT	NE 10 th St = 0 FT podium, 78 FT tower	Meets code.
Side LUC 20.25A.020	0/20 FT	West Side = 0 FT podium, 93 FT tower East Side = 26 FT podium, 73 FT tower	Meets code.
Rear LUC 20.25A.020	0/20 FT	NA	Rear yard setback is not applicable since project abuts existing buildings.

Item	Permitted/ Required	Proposed	Comments/Conditions
Sidewalk Width LUC 20.25A.060.A	Bellevue Way = 12' wide sidewalk + 4' wide planter + 6" curb. NE 10 th = 8' wide sidewalk + 4' wide planter + 6" curb.	Bellevue Way = 12' wide sidewalk + 4' wide planter + 6" curb. NE 10 th = 8' wide sidewalk + 4' wide planter + 6" curb.	Meets code.
Street Tree/ Species & Caliper LUC 20.25A.060.B	Bellevue Way = 3" caliper Honey Locust. NE 10 th Street = 3" caliper Marshall Seedless Ash.	Bellevue Way = 3" caliper Honey Locust. NE 10 th Street = 3" caliper Marshall Seedless Ash.	Meets code.
Mid-Block Walkways LUC 20.25A.060.C	Required for a north-south connection.	NE 10 th Street to Wintergarden. Wintergarden to existing areas of project is already established as a mid-block walkway.	Meets code.



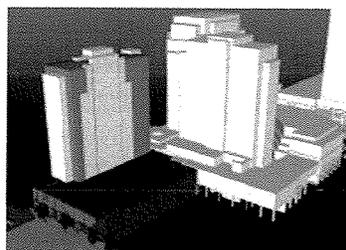
Aerial View from Northeast



Street Level View from Northwest



Street Level View from Northeast



Aerial View from Northwest

Parking and Loading

Item	Required		Proposed	Comments/Conditions
Parking – Downtown MU zone	Minimum and Maximum Permitted Ratios (stalls per guest room or 1000 NSF)	Required and Permitted Parking Stalls	Proposed Parking Stalls	LUC 20.25A.050,B,C (including 20% reduction for shared parking)
Hotel guest rooms	0.9-1.5 per guest room			
Retail	1.0-2.0			
Banquet/Conference	6.0-10.0			
Admin. Office for hotel	2.5-3.0			
Total Parking Stalls²		<u>392 min.</u> stalls <u>654 max.</u> stalls	516 stalls	Meets code.

Item	Required	Proposed	Comments/Conditions
Compact Stalls LUC 20.25A.050.F.2	50%	33%	Meets code.
Loading Area	One off-street 10'x55' space, or other as approved by the Director (refer to 20.20.590K.4.a-c)	Provided two bays in loading space at Level P2 (underground parking garage)	Meets code.

² See file for a breakdown of individual use parking requirements.

B. Special District Requirements

1. FAR Amenity System Requirements

Subject to LUC 20.25A.020C.2, the amount of bonus floor area in square feet generated by the amenities provided to meet the Basic Floor Area Ratio (FAR) requirement must at a minimum be equal to 20% of the Project Limit area times the Basic FAR permitted for a non-residential building in the land use district.

**The Basic FAR requirement in square feet =
 0.20 X the Project Limit area X the Basic Non-residential FAR.
 For this project, the Basic FAR calculation is:
 0.20 X 71,442 SF X 0.5 = 7,144 SF**

The amount of "Basic" FAR Amenity Earned (7,144 SF), calculated below, equals the minimum required 7,144 SF.

Bellevue Place was designed as a complex with the original Hyatt Hotel and existing Bank of America building, with future plans for the Hyatt Hotel expansion tower (under application now). As such, the basic and bonus amenity points within the Bellevue Place complex are shared between all buildings onsite. The applicant shall record with the King County Records and Elections the FAR points and amenities for the entire Bellevue Place project limit and transfer of points, as noted in Tables 1 and 2 below. See condition #X.D.3.

Table 1: Bonus Amenity Area Earned

Project Gross Floor Area (GFA)	338,570 SF
Basic Permitted Floor Area (Basic FAR X Project Limit)	142,884 SF (2.0 X 71,442 SF = 142,884 SF)
Additional Floor Area Requested (Project GFA – Basic Permitted Floor Area)	195,686 SF (338,570 SF- 142,884 SF= 195,686 SF)

<p>“Basic” FAR Amenity Required</p> <p>“Basic” FAR Amenity Earned</p>	<p>7,144 SF $(0.20 \times 71,442 \text{ SF} \times 0.5 = 7,144 \text{ SF})$</p> <p>3,940 SF + 3,204 transferred from Bellevue Place Phase One = 7,144 SF</p> <p>(Refer to Table 2 below)</p>
<p>Remaining (“Bonus”) FAR Amenity to Earn</p> <p>(Project GFA - Basic Permitted Floor Area – “Basic” FAR Amenity Earned)</p>	<p>188,542 SF</p> <p>$(338,570 \text{ GFA} - 142,884 \text{ SF} - 7,144 \text{ SF} = 188,542 \text{ SF})$</p>
<p>Total FAR Amenity Earned</p>	<p>195,686 SF (Refer to Table 2 below)</p>
<p>Excess FAR Amenity Earned</p> <p>(Total FAR Amenity Earned – Basic FAR Amenity Earned - “Bonus” FAR Amenity to Earn)</p>	<p>0 SF</p> <p>$(195,686 \text{ SF} - 7,144 \text{ SF} - 188,542 \text{ SF} = 0 \text{ SF})$</p>

Table 2: Bonus Amenities

Amenity		Units of Measure	Bonus Ratio	Bonus Floor Area Earned	Public Benefit/Condition
Basic	Marquee	SF	2:1	Level 1 at NE 10 th has 1970 SF of overhead protection	A permanent overhead marquee projecting from the elevation of a building, and designed to provide continuous overhead weather protection to the area underneath.
				Total = 1970 SF x 2 = 3,940 SF (points)	

	Transferred points from Bellevue Place Phase One			3,204 SF	Excess points from Bellevue Place Phase One ³
	Sub-Total Basic			7,144 SF (points)	
Non-Basic (Bonus)	Underground Parking	SF	0.5:1	Level P5 to P1 add 272,845 SF or parking below grade Total = 272,845 SF x 0.5 = 136,423 SF (points)	Provides parking beneath the structure instead of devoting a large area of the site to surface parking. By placing the parking below the structure, it presents a more urban character than a suburban surface parking lot.
	Transferred points from Bellevue Place Phase One			52,119 SF	Excess points from Bellevue Place Phase One ⁴
	Sub-Total Non-Basic (Bonus)			188,542 SF (points)	
	Combined Total Basic and Non-Basic (Bonus)			TOTAL of 195,686 (7,144 +188,542)	

As summarized in Table 1 above, the project is required to provide a minimum of 195,686 square feet of amenity. The project provides 195,686 SF; therefore amenity requirements are met and there is no excess in amenity points.

4. Design Review Criteria

Subject to LUC 20.25A.110, 20.25A.100 and 20.25A.115, the following design criteria must be met for Design Review approval.

LUC 20.25A.110 Design Review Criteria

- 3** Bellevue Place was designed as a complex with the original Hyatt Hotel and existing Bank of America building, with future plans for the Hyatt Hotel expansion tower (under application now). As such the amenity points within the Bellevue Place complex are shared between all buildings onsite. Bellevue Place Phase One had 78,588 SF excess “Basic” amenity points. With this project, Hyatt Hotel Phase Two, using 3,204 SF of the “Basic” amenity points, there will still be an **excess of 75,384 SF “Basic” amenity points with Bellevue Place Phase One project.**
- 4** Bellevue Place Phase One had 519,515 SF excess “Bonus” amenity points. With this project, Hyatt Phase Two, using 52,119 SF of the “Bonus” amenity points, there will still be an **excess of 467,396 SF “Bonus” amenity points with Bellevue Place Phase One project.**

A. Site Design Criteria

1. Vehicular Circulation and Parking

- a. *Provide efficient vehicular access to parking and service areas which is coordinated on a superblock basis.* Parking for the Hyatt Hotel expansion will be provided in an underground garage that will connect to the existing Bellevue Place underground parking garage. Access to the underground parking garage is via an existing vehicular ramp from NE 10th Street.
- b. *Coordinate the location of vehicular and pedestrian mid-block connections, considering opportunities for mid-block crossings.* There will be vehicular connections within the underground parking garage. Upon construction of this project and the Wendy's surface parking lot expansion, there will be 2,076 parking stalls which will serve the Bellevue Place multi-building complex and Wendy's restaurant. For pedestrians, the applicant proposes a mid-block connection within the building interior to connect to the Wintergarden at Bellevue Place (which connects to the entry plaza and NE 8th Street). Thus, the proposed connection will complete the pedestrian mid-block connection between NE 8th Street and NE 10th Street.
- c. *Provide for safe and pleasant movement within the Downtown for the pedestrian and bicyclist.* The mid-block connection provides pedestrian access from NE 10th Street to NE 8th Street. Signage is required for the public for the mid-block connector. See condition #X.D.2. Since the mid-block connection is an internal walkway, bicycles will not be allowed. However, bicycle opportunities are available on surrounding City streets and within designated bicycles routes (maps are available at City Hall).
- d. *Maximize the separation of vehicular traffic from pedestrian areas by means of level changes, space and distance, or landscaping.* Along NE 10th Street, the sidewalks are separated from the vehicular surfaces by a 6 inch curb and four foot wide planting area. The walking surface will be of a different material than the bus load/unload area. Street trees will also provide a feeling of separation between the sidewalk and driving areas.
- e. *Incorporate retail shopping space at ground level into parking structures whenever practical and appropriate.* Since the hotel expansion has an underground parking garage, this was not possible.
- f. *Minimize the location of parking adjacent to pedestrian connections.* The parking for the Hyatt Hotel expansion is within an underground parking garage and is not adjacent to pedestrian connections.
- g. *Limit the number of driveway openings and the number of access lanes in each opening.* The project will use the existing vehicular ramp to the parking garage. A new curb cut will be required for the bus load/unload area in front of the building entrance on NE 10th Street.

- h. *Where appropriate, fit garages into the topography and make use of garage roofs for public open space, recreation or landscaped areas.* The garage is located underground with hotel expansion on top. There will be no new exposed garage roofs.
- i. *Maximize the use of underground parking.* All of the parking is provided in the underground parking garage which connects to the Bellevue Place underground parking garage.

2. Pedestrian Circulation and Amenities (see LUC 20.25A.060)

Pedestrians can circulate around the project, via the sidewalk along NE 10th Street and Bellevue Way. Pedestrians can also walk the mid-block connection to the existing uses within Bellevue Place. Street trees, landscaping, and marquees are used to provide a more pleasant pedestrian experience.

3. Wind and Sun

The proposed building, located along the north side of the existing Hyatt Hotel, will not be subject to some of the winds from the southwest as the existing Hyatt tower will help block winds. The proposed expansion will be open enough with the lower podium (entry structure) to still allow breezes in the summer. According to the wind study submitted by the applicant, it is expected that the pedestrian-level environment will be acceptable with regard to wind impact.

The building is located on the north side of the existing Hyatt Hotel. It will not block sun to the hotel rooms within the existing tower. Since the proposed tower is 19-stories in height (allowed by code), this height could potentially block afternoon sun to the Washington Square multi-family residential units to the east (under construction).

4. Open Space

The primary open space for the project is the southeast corner plaza (with the raised circle tree planter). As mentioned earlier in this report, this area has an unfriendly environment. The applicant proposes to retain this area for the new Hyatt expansion until PACE is constructed (when the plaza is targeted for demolition). If the PACE project is delayed more than 5 years, the applicant for the Hyatt expansion shall submit for a new plaza plan. See condition #X.E.1.

5. Light and Glare

Light and glare are controlled by utilizing low-intensity lighting fixtures on the exterior of the building and non-reflective glass, as well as the non-reflective stucco material.

B. Downtown Patterns and Context

1. Natural Setting and Topography

There is a elevation change of about 20 feet onsite (west uphill, east downhill) with an overall slope of 10%. The building is located on the lower portion of the site and will bring the floor elevation to the sidewalk elevation at NE 10th Street. Wendy's is located along the uphill side adjacent to Bellevue Way.

2. Landscape Design

The applicant will install specific street trees along NE 10th Street and Bellevue Way to be consistent with existing street tree conditions, the Land Use Code and the "Great Streets" project.⁵ Protection of existing trees to remain is required. Any damage to the existing street trees and/or the existing public irrigation system (along NE 10th Street) will be considered damage to public property. Damage or removal of existing street trees and/or the existing public irrigation system will require replacement and may result in penalties. Landscape plant types shall be native and adapted plant species to minimize irrigation water needs. In addition, the applicant shall submit appropriate assurance devices. See conditions #X.A.1, B.2, C.3, D.2.

3. Views

The hotel will provide territorial views to the north, west and east. Some views from the hotel to the east will be blocked by the Washington Square residential tower. And, some of the views from the Washington Square residential tower looking west will be blocked by this new Hyatt tower.

4. Building Height and Bulk

The scale and bulk of the project was designed to respond to the existing Bellevue Place Hyatt Hotel complex. As mentioned in Section I.B., conceptually, the project was designed to be consistent with the existing Hyatt Hotel tower. It is lower in height than the existing Hyatt Hotel per the zoning designations,⁶ but will use the same materials and colors. The top of the tower will be tapered with stepbacks at the upper floors for stairs and mechanical screening. The applicant has worked with the City to provide consistency to the towers and to provide a more interesting building form. The north tower elevation is an important gateway view to downtown Bellevue and will need additional scoring to provide a more "finished front" look. Additional scoring will provide articulation and interest to the north tower elevation. The applicant shall include such scoring in the building permit plans. See condition #X.C.1.

⁵ The City of Bellevue is undertaking the "Great Streets" project to improve the future appearance of streets in Downtown Bellevue. Key objectives of the "Great Streets" project are to improve conditions for pedestrians, introduce more and better landscaping, and enhance the aesthetic quality of street corridors. The Planning Principles document (April 2007) has been drafted to guide the project.

⁶ The original Hyatt Hotel is located in the Downtown O-2 Land Use district which allows a 250 foot building height.

The design of the base is very similar to the current porte-cochere and entry to the existing Hyatt Hotel. The height of the podium and large glazing areas are very similar to the proposed PACE façade (Design Review application not yet submitted). The tower is setback from the street, so this lower façade will be the main architectural feature to interact with the street front. Columns will be brick to match the current podium / base of the existing Bellevue Place development.

The building tower, at 19 stories, will serve as a gateway to the downtown core to the south. At this location, the hotel expansion is the first highrise to be constructed along Bellevue Way traveling south from NE 12th Street to NE 10th Street. It will provide a transition from the north (lower intensity commercial uses and multi-family residential areas) to the south (higher intensity office and commercial uses).

The proposal will also provide a transition from the commercial activities along Bellevue Way to the multi-family residential area to the east. This residential area to the east is an emerging neighborhood with numerous large multi-family projects under construction (i.e. Washington Square to the east) or under review for approval.

The project also provides a transition to the north from the intense Downtown Core to the lower intensity commercial/retail area to the north, which is subject to the Perimeter Design District (providing a further transition to the residential neighborhood to the north).

5. Transitions

Refer to discussion in item #4 above pertaining to the scale transition at adjacent properties.

6. Patterns of Activity

The building provides various opportunities for vital pedestrian-level activity at the ground level, including guests arriving to the hotel, guests attending the conference center and/or ballroom. There is also a retail store on the ground level which will provide pedestrian activity (although located within the building). There will be pedestrian activity for those who walk the mid-block connection from the new Hyatt tower to the existing Wintergarden/Hyatt Hotel.

7. Signage

A sign package was included in this application. Prior to issuance of any building permits, a sign package in compliance with the Sign Code must be submitted as a revision to this Design Review. See condition #X.D.1.

Design Guidelines – Building/Sidewalk Relationships LUC 20.25A.115

NE 10th Street is a designated as D right-of-way in the Design Guidelines – Building/Sidewalk Relationships (LUC 20.25A.115). The intent of the D right-of-way designation is to have low to moderate orientation to pedestrians. This shall be achieved by designing some relationship between exterior and interior activities with respect to visual access. Design attention should be given to sidewalk related activities and amenities.

The D rights-of-way design guidelines state that: 1) Street level edges of the entire project limit shall incorporate some amount of service and commercial activities; and 2) characteristics such as windows providing visual access, street walls, and differentiation at the ground level shall be incorporated into the project.

As discussed herein and illustrated on the attached project plans, the proposal complies with the intent of D rights-of-way, including its design guidelines by incorporating a prominent 3-story glazed entry/podium which will provide views for pedestrians and the public into the space. The project also includes marquees for weather protection and wide sidewalks and planter strips,

More specifically, the project includes the following at the sidewalk interface along NE 10th Street:

• Glazing Hotel/Conference Area	164 lineal feet
• <u>Marquees</u>	<u>163 lineal feet</u>
Total	327 lineal feet

The building façade is 211 lineal feet divided by 327 total lineal feet of street level edge features = 155% which exceeds the minimum requirement of 100%. The applicant shall record an agreement that 100% of the street level edges will remain pedestrian-oriented uses with pedestrian-oriented design elements, per LUC 20.25A.020.115. See condition #X.D.3.

IV. PUBLIC NOTICE AND COMMENT

Application Date: October 3, 2006
Notice of Application: January 4, 2007
Public Notice Sign: January 4, 2007
Minimum Comment Period: January 18, 2007

Although the minimum required public comment period ended on January 18, 2007, comments were accepted up to the date of this decision. There were no written comments. There were two phone calls inquiring about if Wendy's was a part of this project, which is not included. The inquirers were told how to become parties of record but declined to do so. There are no parties of record.

V. TECHNICAL REVIEW

A. Utility Department

Utility review has been on a conceptual basis only, consequently there are no implied approvals of the engineering specifications for the water, sewer and storm drainage components of the project. Engineering review of the water, sewer and storm drainage infrastructure will be performed under the Utility Developer Extension Agreements, and will coincide with the clearing and grading permit review. Final civil engineering may require changes to the site layout to accommodate the utilities. See conditions #X.A.2,3.

B. Clear and Grade Code

The plans and materials submitted for the project are sufficient for the Clearing and Grading section to approve the Design Review. Technical review will occur through the Clearing and Grading Permit.

C. Transportation Department

Holiday Construction & Traffic Restrictions

Construction activities such as hauling and lane closures will be allowed between November 15th and January 5th only between the hours of 10:00 p.m. and 6:00 a.m. due to holiday traffic. The dates and times of these restrictions are subject to change. The applicant shall contact the Transportation Department Right-of-Way Section to confirm the specifics of this restriction prior to applying for a Right-of-Way Use Permit, which is issued directly by the Transportation Department. See condition #X.A.4.

Use of the Right of Way

Applicants often request use of the right-of-way and of pedestrian easements for materials storage, construction trailers, hauling routes, fencing, barricades, loading and unloading and other temporary uses as well as for construction of utilities and street improvements. A Right-of-Way Use Permit for such activities must be acquired prior to issuance of any construction permit including demolition permit. This permit is issued directly by the Transportation Department. See condition #X.B.3.

Easements

There are existing utility easements contained on this site, which are affected by this development. Any negative impact that this development has on those easements must be mitigated or easements relinquished.

The applicant shall provide easements to the City for the location of street light facilities consisting of below-grade vaults between the building and sidewalk within the landscape area on the Bellevue Way, NE 10th and 106th NE frontages. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible.

The applicant shall provide easements to the City for the location of sidewalks and public and franchised utilities outside of the City right-of-way on Bellevue Way, NE 10th and 106th NE.

See conditions #X.C.4, C.5, C.6.

Street Frontage Improvements

In order to provide safe pedestrian and vehicular access in the vicinity of the site, and to provide infrastructure improvements in downtown area with a consistent and attractive appearance, the construction of street frontage improvements is required as a condition of development approval. The design of the improvements must conform to the

requirements of the Americans with Disabilities Act and the Transportation Development Code (BCC 14.60), and the provisions of the Transportation Department Design Manual.

A combined street tree and street light plan is required for review and approval prior to approval of engineering and landscape plans. The goal is to provide the optimum number of street trees while not compromising the light and safety provided by streetlights.

The Americans with Disabilities Act (ADA) requires that sidewalk cross slopes not exceed two percent. The sidewalk cross slope may be less than two percent only if the sidewalk has a longitudinal slope sufficient to provide adequate drainage. Bellevue's standard for curb height is six inches, except where curb ramps are needed. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm compliance. New curb and sidewalk shall be constructed in compliance with these requirements. Building elevations shall be consistent with the required curb and sidewalk elevations. Spot elevations must be included in the building plans in a manner that proves that building elevations are designed to correspond to the sidewalk elevations shown in the engineering plans, especially at entrances and other key points. Curb and sidewalk elevations will not be revised to fit the building, and city inspectors may require spot surveys during construction in order to confirm the required elevations.

ADA also requires provision of a consistent travel path for visually handicapped pedestrians. Potential tripping hazards are not allowed in the main pathway. Any planter boxes installed in the sidewalk to improve pedestrian sight distance at driveways must be designed to reduce the tripping potential and must not extend more than two feet into the public sidewalk. Traffic signal controller boxes and streetlight contactor cabinets must be located so as not to interfere with the main pedestrian path. Buildings shall be designed so that doors do not swing out into the pedestrian path. ADA-compliant curb ramps shall be installed where needed, consistent with standard drawings TE-12 or TE-13.

Electrical connections for lighting in tree wells or planter strips may be allowed, if installed in compliance with the electrical code and subjected to an electrical inspection. Irrigation devices and electrical components shall not create a tripping hazard in the sidewalk.

Any non-standard features or vegetation shall not create a sight obstruction within any required sight triangle, shall not create a tripping or slipping hazard in the sidewalk, and shall not create a raised fixed object in the street's clear zone. The materials and installation methods must meet typical construction requirements. Any non-standard features or vegetation shall not create a sight obstruction within any required sight triangle and shall not create a tripping or slipping hazard in the sidewalk.

No new building structure or garage shall be constructed under a street right of way or existing public sidewalk/utility easement. In some conditions (to be finalized during engineering and building plan review), new structure may be allowed under a new sidewalk/utility easement. No soil nailing is allowed under a street right-of-way or sidewalk/utility easement without an indemnification agreement that protects the city.

Any awning, marquee, balcony, etc. over a sidewalk or utility easement must be at least 16 feet above the sidewalk, or be removable (with an agreement regarding removal and replacement); and must have at least 3 feet horizontal clearance from any streetlight or traffic signal pole.

No fixed objects, including fire hydrants, trees, and streetlight poles, are allowed within ten feet of a driveway edge, defined as Point A in standard drawing Dev-6. Fixed objects are defined as anything with breakaway characteristics stronger than a 4-inch by 4-inch wooden post.

See conditions #X.C.7, D.4.

Right of Way Dedication

To incorporate street improvements which are reasonably necessary to mitigate the direct results of the development, the developer is required to dedicate property such that street surface to back of curb is accommodated within the public right-of-way. The proposed 12-foot wide bus pullout on NE 10th will not be dedicated to the City. See condition #X.C.8.

Site Access

Access to the proposed project will be mainly via the existing driveway on NE 10th with full vehicular turning movements. It should be noted that the Bellevue Place garage is also accessible from 106th NE and NE 8th. These driveways will maintain their existing operations. See condition #X.A.5.

Bus Pullout

A parallel parking pullout for private buses will be located adjacent to the City's NE 10th right-of-way where the applicant will be responsible for its operation and maintenance. The width of the pullout will be 12 feet and a centerline C-Curb on NE 10th will be required. The C-Curb will extend from Bellevue Way to the east end of the proposed pullout. In addition, the distance between the east side of the pullout and the existing Hyatt driveway will be sufficient to maintain vehicular and pedestrian sight distance. See condition #X.C.9.

Pavement Restoration

The City of Bellevue has established the Trench Restoration Program to provide developers with guidance as to the extent of resurfacing required when a street has been damaged by trenching or other activities. Under the Trench Restoration Program, every street in the City of Bellevue has been examined and placed in one of three categories based on the street's condition and the period of time since it has last been resurfaced. These three categories are, "No Street Cuts Permitted", "Overlay Required", and "Standard Trench Restoration". Each category has different trench restoration requirements associated with it. Damage to the street can be mitigated by placing an asphalt overlay well beyond the limits of the trench walls to produce a more durable surface without the unsightly piecemeal look that often comes with small strip patching. Currently, NE 10th is classified as "No Street Cuts Permitted" and Bellevue way and 106th NE is classified as "Standard Trench Restoration". See condition #X.D.5.

D. Fire Department

The site development plans for this application generally conform to Fire Code requirements. However, there are a number of conditions that must be met prior to issuance of the building permit. See condition #X.C.10.

VI. STATE ENVIRONMENTAL POLICY ACT

The environmental review indicates no probability of significant adverse environmental impacts occurring as a result of the proposal. Therefore, issuance of a Determination of Non-Significance (DNS) is the appropriate threshold determination under the State Environmental Policy Act (SEPA) requirements, with adoption of the 2006-2017 Transportation Facilities Plan Final Environmental Impact Statement (TFP EIS) issued November 30, 2006; and incorporation by reference of the Bellevue Place final EIS issued June 10, 1986. These documents are available in the Department of Planning and Community Development Records Room, Bellevue City Hall, 450 110th Ave NE.

Adverse impacts which are less than significant are usually subject to City Codes or Standards which are intended to mitigate those impacts. Where such impacts and regulatory items correspond, further documentation is not necessary. For other adverse impacts which are less than significant, Bellevue City Code Sec. 22.02.140 provides substantive authority to mitigate impacts disclosed through the environmental review process.

TRANSPORTATION

Long Term Impacts and Mitigation

The City's current Transportation Facilities Plan (TFP) EIS evaluates the impacts of growth projected to occur within the City by 2017 on the roadway network assuming that all the transportation improvement projects proposed in the City's current TFP are in place.

The Transportation Facilities Plan EIS divides the City into several Mobility Management Areas (MMAs) for analysis purposes. Bellevue Place Hyatt Expansion lies within MMA # 3, which has a 2017 total growth projection of 850 hotel rooms, 2,320 multi-family dwelling units, 1,062,000 gross square feet of retail and 2,490,732 gross square feet of office. This development proposes 351 hotel rooms. Therefore, the volume of proposed development is within the assumptions of the Transportation Facilities Plan EIS.

It should be noted that the City of Bellevue's Downtown Implementation Plan (DIP) forecasts an additional five years beyond the TFP to 2020. Included with this additional five year forecast are additional transportation improvements, which are shown to further improve the area-wide level of service in Downtown.

Transportation improvements identified in the TFP and the DIP would adequately mitigate the future traffic levels anticipated for Bellevue Place Hyatt Expansion development.

The traffic impact fee paid by the developer of this proposal will be applied to projects identified in the current and upcoming TFP. Traffic impact fees are used by the City to fund street improvement projects to alleviate traffic congestion caused by the cumulative impacts of development throughout the City. Payment of the transportation impact fee, as required by BCC 22.16, contributes to the financing of transportation improvement projects in the current adopted Transportation Facilities Plan. Fee payment is required at the time of building permit issuance. See condition #X.C.11.

Mid-Range Impacts and Mitigation

Project impacts anticipated to occur in the next six years are assessed through a concurrency analysis. The Traffic Standards Code (BCC 14.10) requires that development proposals generating 30 or more p.m. peak hour trips must undergo a traffic impact analysis to determine if the concurrency requirements of the State Growth Management Act are maintained. For the purpose of the concurrency analysis, the City has been divided into 14 Mobility Management Areas.

This development will generate approximately 94 new p.m. peak hour trips. City staff distributed and then assigned project-generated trips to the street network using the City's EMME-2 travel forecasting model with the current Capital Investment Program network. In this project analysis, two system intersections received 20 or more p.m. peak hour trips. Using forecasted local traffic volumes with the expected project-generated traffic added, the area average levels of service were determined. For a baseline condition for comparison, the levels of service were also determined using forecasted traffic volumes without the site-generated traffic. Neither the maximum area-average levels of service nor the congestion allowances were exceeded as a result of traffic generated from this proposal. **See Exhibit E for Concurrency Certificate and Concurrency Analysis Spread Sheet.**

The rules of concurrency reservation are outlined in the Traffic Standards Code Director's Rules, updated May 23, 2001. This project complies with the Traffic Standards Code will receive a Certificate of Concurrency. **See Exhibit E for Concurrency Certificate and Concurrency Analysis Spread Sheet.**

The Certificate of Concurrency is reserved to this project as of February 9, 2007, when the building permit application (BB 07-105006) was deemed complete. The Certificate of Concurrency is issued together with this land use decision and will remain in effect for the life of the building permit application, pursuant to BCC 23.05.160.F. At issuance of building permit, the Certificate of Concurrency will be extended and remain in effect for one additional year (with the possibility of up to two one-year extensions) as provided for in BCC 23.05.160.E.

Short Term Operational Impacts and Mitigation

City staff directed the applicant's traffic consultant, TDA, Inc., to analyze the short term operational impacts of the proposal in order to recommend mitigation if necessary. These impacts included traffic operations conditions during the p.m. peak hours. Specific issues addressed in the Traffic Impact Analysis dated March 1, 2007, included LOS analysis surrounding intersections, site access and circulation, transit availability, and accident history analysis for the past three years. No adverse operational impacts are anticipated due to the proposed development.

WIND

There is the potential for street level wind to impact pedestrians. The applicant submitted a wind study titled, Wind Speed at Pedestrian Levels: An Assessment, by Magnusson Klemencic Associates dated May 16, 2003. This study states that it is expected the pedestrian-level environment around the proposal and adjoining towers will be acceptable and comparable to that of neighboring Bellevue sites. No mitigation is required.

NOISE

Noise will be generated from the construction phase of the development. The City of Bellevue Noise Ordinance, BCC 9.18 regulates hours of construction-related noise and the conditions under which they may be expanded. Residents located within the vicinity will be impacted by construction noise. Due to the close proximity of residences in the area, hours of construction are limited as noise mitigation. The best available noise abatement technology is required during construction. See conditions #X.A.6, A.7.

There is the potential that noise/garage exhaust could impact pedestrians along NE 10th Street. However, the garage exhaust vents are located within the vehicular ramp to the parking garage, 15 feet below the sidewalk elevation. These vents are about 75 feet from the sidewalk and 150 feet from the entrance to the hotel. At that distance, the noise/garage exhaust will unlikely affect pedestrians and guests.

VII. CHANGES TO PROPOSAL DUE TO CITY REVIEW

Building Design

- Rooftop mechanical equipment screening was revised in color to be consistent with the roof color. This was requested so rooftop equipment/screening would be less visible for guests looking down at the roof from above.
- No wireless equipment will be visible on the exterior of the building.
- Additional scoring on the north tower elevation will be provided for more articulation and interest to this elevation. See condition #X.C.1.

Site Design

- The Wendy's parking area was redesigned to provide a more organized queuing line. Seating was provided along the south side of the building. The 30% grade drop with plantings was redesigned to be at grade and eliminate the steep slope.
- The bus load/unload area along NE 10th Street was redesigned to provide adequate area for 2 buses to be parked at the same time.
- The water meter box in the sidewalk along NE 10th Street was relocated so the box would not be in the main walking path for pedestrians.
- Different surfaces (color/material) will be provided for the bus loading/unloading area, sidewalk in front of the building entrance, and NE 10th Street pavement. Some granite walking surface will be provided in front of the building entrance.
- A wider sidewalk area was provided at the northeast corner of the building for load/unload of luggage and for waiting guests. This area is outside of the main walking path of the sidewalk.
- The sidewalk in the northwest corner of the site was redesigned to provide a more gradual transition from the existing to proposed sidewalk to the building entrance.
- Street trees were revised to be consistent with Land Use Code requirements.
- The southeast plaza will be upgraded if PACE does not submit the Design Review application within 5 years.

VIII. DECISION CRITERIA

The Director may approve, or approve with modifications, an application for Design Review if:

1. The proposal is consistent with the Comprehensive Plan.

The Comprehensive Plan envisions this area of downtown to be of mixed land uses, including hotels, a performing arts center, residential buildings with ground floor retail uses and office uses. This proposal will further this vision of the area by completing the Hyatt Hotel with additional opportunities for the community by providing a conference area and ballroom.

A list of Comprehensive Plan policies that are applicable to, and provide support for, this project are attached in Exhibit D. A summary of how the project meets the Comprehensive Plan policies is below:

Policy S-DT-48. Provide for a sense of approach to Downtown at key entry points through the use of gateways and identity treatments that convey a sense of quality and permanence.

The building is designed to meet provide and transition and gateway into the City, as driving from the north going south on Bellevue Way (i.e. from NE 12th Street). The hotel tower has a lower height (19 stories) than the existing Hyatt Hotel (24 stories) and existing Bank of America building (21 stories). The project is the first highrise tower driving south to the Downtown Core. It presents a compatible treatment to the existing Bellevue Place complex while at the same time providing an urban cutting-edge entry to the conference center along NE 10th Street, with large expanses of glazing and several vertical brick columns. This will tie the lower podium floors together as one element, separate from the tower. This will provide a visual identity to the proposal. The materials, stucco, glass and brick, will convey a sense of quality and permanence. To provide more visual interest to the north tower elevation, additional scoring will be required. See condition #X.C.1.

Policy UD-72. Link the increased intensity of development with the increased pedestrian amenities, pedestrian-oriented building design, mid-block connections, public spaces, activities, openness, sunlight, and view preservation.

The project provides a mid-block connection between the existing and proposed Hyatt Hotel towers (between NE 8th Street and NE 10th Street). There will be ample public sidewalk width along NE 10th Street for pedestrians. Since the proposal is a hotel with conference meeting rooms and a ballroom, there are spaces that the public can utilize. The entry podium is pedestrian friendly with ample glazing and marquees for pedestrian weather-protection.

Policy UD-40. Ensure that sidewalks, walkways, and trails are furnished, where needed and appropriate, with lightning, seating, landscaping, street trees, trash receptacles, public art, bike racks, railings, handicap access, newspapers boxes etc. without interfering with pedestrian circulation.

As mentioned above, NE 10th Street will have ample sidewalk width for pedestrians. Street trees will be provided to help create a pedestrian friendly street environment. The pedestrian seating in the plaza at the southeast corner of the project will remain at this time. The entire building and site design will meet handicap access requirements.

2. The proposal complies with the applicable requirements of this Code.

As conditioned, the proposal complies with applicable requirements of the Land Use Code. Refer to Section III.A.2 of this report for further information. The applicant shall record a copy of all of the Conditions of Approval, Amenities Plan and FAR calculations with King County Recording. See condition #X.D.3.

3. The proposal addresses all applicable design guidelines or criteria of this Code in a manner which fulfills their purpose and intent.

The proposal meets the development standards and design guidelines of the Downtown MU Design district (LUC 20.25A.100). Refer to Section III.B.4.

Refer to Exhibit A, Project Plans, for more information about the project design.

4. The proposal is compatible with, and responds to, the existing or intended character, appearance, and quality of development and physical characteristics of the subject property and immediate vicinity.

The proposal is compatible with, and responds to, the intended character, appearance, and quality of development in the immediate vicinity which is a mix of low, mid and high rise development. It is just outside of the Downtown Core and as such serves as a gateway or transition from the lower intensity uses (north) to the Downtown Core (south). This proposal is an element of the overall superblock development of Bellevue Place by Kemper Development Company. As such, the colors and materials of the proposal will be consistent with the existing buildings within Bellevue Place development. The proposal, although different in colors and materials, will be compatible with the Washington Square multi-family development to the east (106th Ave. NE and NE 10th Street) and Lincoln Square to the south (Bellevue Way NE and NE 8th Street).

5. The proposal will be served by adequate public facilities including streets, fire protection, and utilities.

All required public services and facilities are available to the site. A condition of approval has been added that requires power, telephone, or other ground-mounted equipment not be located in above-ground cabinets in sidewalk areas. In addition, the applicant must contact Allied Waste to establish adequate sizing of recycling and solid waste collection area for this hotel expansion using current standards. See conditions #X.C.12, D.6.

IX. DECISION

After conducting the various administrative reviews associated with the proposal, including applicable Land Use consistency, SEPA and City Code & Standard compliance reviews, the Director of Planning and Community Development does hereby **APPROVE WITH CONDITIONS** the subject proposal.

X. CONDITIONS OF APPROVAL:

The following conditions are imposed on the applicant under the authority referenced:

A. GENERAL CONDITIONS: The following conditions apply to all phases of development.

1. DAMAGE TO STREET TREES AND/OR PUBLIC IRRIGATION SYSTEM

Protection of the existing trees to remain is required. Any damage to the existing street trees and/or the existing public irrigation system (along NE 10th Street) will be considered damage to public property. Damage or removal of the existing street trees and/or the existing public irrigation system will require replacement and may result in penalties. Any existing street trees damaged during construction shall be replaced by the applicant with the same size and type, and planted as specified by Parks Department tree planting standards.

AUTHORITY: S-DT-99, S-DT-110, LUC 20.25A.040
Reviewer: Carol Saari, 425-452-2731

2. PRELIMINARY UTILITY DESIGN

The Utilities Department approval of the Design Review application is based on the preliminary utility design. Final civil engineering of the utility design may require changes to the site layout to accommodate the utilities.

AUTHORITY: BCC Title 24.02, 24.04, 24.06
Reviewer: Nicole Mecum, 425-313-1078

3. DEVELOPER EXTENSION AGREEMENT

The water, sewer, and storm drainage systems shall be designed per the current City of Bellevue Utility Codes and Utility Engineering Standards. Utilities Department design review, plan approval, and field inspection is performed under the Developer Extension Agreement and Utilities Permit Processes.

AUTHORITY: BCC Title 24.02, 24.04, 24.06
Reviewer: Nicole Mecum, 425-313-1078

4. HOLIDAY CONSTRUCTION & TRAFFIC RESTRICTIONS

Construction activities such as hauling and lane closures between November 15th and January 5th will be allowed only between the hours of 10:00 pm and 6:00 am due to holiday traffic. The Transportation Department will be monitoring traffic and may modify this moratorium accordingly.

AUTHORITY: BCC 14.30.060
Reviewer: Jon Regalia, 425-452-4599

5. VEHICULAR ACCESS RESTRICTIONS

Access to this site from the proposed commercial driveways located on Bellevue Way will be restricted to right-turn-in and right-turn-out only.

AUTHORITY: BCC 14.60.150
Reviewer: Abdy Farid, 425-452-7698

6. CONSTRUCTION HOURS

Noise related to construction is allowed from 7:00 a.m. to 6:00 p.m. Monday through Friday and 9:00 a.m. to 6:00 p.m. on Saturday. Exceptions to the construction noise hours limitation contained in the Noise Control Code MAY be granted pursuant to 9.18.020C.1 when necessary to accommodate construction which cannot be undertaken during exempt hours. Prolonged exposure to noise created by extended hour construction activity is likely to have a significant impact on inhabitants of surrounding residential properties during the proposed timeline for construction. In order to minimize detriment on residential uses in the immediate vicinity of the project, the Contractor shall not rely on City issuance of a blanket exemption from the Noise Control Code during the construction period. Allowances for short term work outside of normal construction hours shall be limited and will be reviewed on a case by case basis to verify necessity and ensure appropriate noise mitigation is utilized to protect surrounding uses and properties. Written requests for exemption from the Noise Control Code must be submitted two weeks prior to the scheduled onset of extended hour construction activity. Such request shall include a noise analysis prepared by a noise consultant, including recommendations for achieving the noise limitations of the Noise Ordinance for new residential construction.

AUTHORITY: BCC 9.18.040
Reviewer: Carol Saari, 425-452-2731

7. USE OF BEST AVAILABLE NOISE ABATEMENT TECHNOLOGY

The use of best available noise abatement technology consistent with feasibility is required during construction to mitigate construction noise impacts to surrounding uses.

AUTHORITY: BCC 9.18.020F
Reviewer: Carol Saari, 425-452-2731

B. PRIOR TO CLEARING & GRADING PERMIT: The following conditions are imposed to ensure compliance with the relevant decision criteria and Code requirements and to mitigate adverse environmental impacts not addressed through applicable Code provisions. These conditions must be complied with on plans submitted with the Clearing & Grading or Demolition permit application:

1. STREET TREES

The street trees shall be installed with structural soils. Please contact Don McQuilliams, Parks Department at 425-452-7865.

AUTHORITY: LUC 20.20.520
Reviewer: Carol Saari, 425-452-2731

2. TREE PROTECTION

Include on the project plans tree preservation detail Drawing #35, Tree Protection, of the City of Bellevue Development Standards.

AUTHORITY: BCC 23.76 and Bellevue Development Standards
Reviewer: Carol Saari, 425-452-2731

3. RIGHT-OF-WAY USE PERMIT

Prior to issuance of any construction or clearing and grading permit, the applicant shall secure applicable right-of-way use permits from the City's Transportation Department, which may include:

- a) Designated truck hauling routes.
- b) Truck loading/unloading activities.
- c) Location of construction fences.
- d) Hours of construction and hauling.
- e) Requirements for leasing of right of way or pedestrian easements.
- f) Provisions for street sweeping, excavation and construction.
- g) Location of construction signing and pedestrian detour routes.
- h) All other construction activities as they affect the public street system.

In addition, the applicant shall submit for review and approval a plan for providing pedestrian access during construction of this project. Access shall be provided at all times during the construction process, except when specific construction activities such as shoring, foundation work, and construction of frontage improvements prevents access. General materials storage and contractor convenience are not reasons for preventing access.

The applicant will secure sufficient off-street parking for construction workers before the issuance of clearing and grading, building, foundation or demolition permits.

AUTHORITY: BCC 11.70 & 14.30
Reviewer: Jon Regalia, 425-452-4599

C. PRIOR TO BUILDING PERMIT ISSUANCE: The following conditions are required by City Code. Unless specified otherwise below, these conditions must be complied with on plans submitted with the Building permit application:

1. MARQUEES AND NORTH TOWER ELEVATION SCORING

- a. The applicant shall submit for review and approval the final colors and material of the marquees.
- b. The applicant shall provide additional scoring on the north tower elevation to provide more articulation and interest to this elevation. The applicant shall include such scoring design in the plans for building permit issuance.

AUTHORITY: Comprehensive Plan Policies UD-1, UD-2, UD-5, UD-6, S-DT-48
Reviewer: Carol Saari, 425-452-2731

2. SCREENING FOR THE ROOFTOP MECHANICAL BOXES

The applicant shall submit drawings of the roof pattern for review and approval. The rooftop mechanical screening and equipment shall not exceed a height of 15-feet above the roof. The equipment shall be consolidated. Rooftop equipment shall be architecturally incorporated into the tower design. The screen shall be visually integrated with the form and color of the building, and the equipment shall be screened from all sides and from above. Screening from above shall be accomplished by a solid non-reflective roof, which may incorporate louvers, vents or similar penetrations to provide the necessary ventilation or exhaust the equipment being screened. The color of the screen shall match the color of the roof. Any rooftop structures to house mechanical equipment shall be the minimum size necessary and also painted to match the color of the roof.

AUTHORITY: LUC 20.20.525; Comprehensive Plan Policy UD-8
Reviewer: Carol Saari, 425-452-2731

3. LANDSCAPE PLANT TYPES

The landscape plan submitted with the building permit application shall be comprised of native and adapted plant species to minimize irrigation demands and reduce maintenance requirements.

AUTHORITY: LUC 20.20.520.I
Reviewer: Carol Saari, 425-452-2731

4. EXISTING EASEMENTS

There are existing utility easements contained on this site that are affected by this development. Any negative impact that this development has on those easements must be mitigated or easements relinquished.

AUTHORITY: BCC 14.60.100
Reviewer: Jon Regalia, 425-452-4599

5. EASEMENTS FOR SIGNAL CONTROL AND STREET LIGHT BOXES AND VAULTS

The applicant shall provide easements to the City for location of signal and street light facilities such as above-grade boxes and below-grade vaults between the building and sidewalk within the landscape area.

AUTHORITY: BCC 14.60.100
Reviewer: Abdy Farid, 425-452-7698

6. PEDESTRIAN AND UTILITIES EASEMENTS

The applicant shall provide easements such that sidewalks and utilities outside of the City right of way along the property frontage on Bellevue Way, NE 10th and 106th NE are located within a pedestrian and utilities easement area. The proposed bus pullout will provide utilities easement where as the meandering sidewalk behind it will provide sidewalk easement.

AUTHORITY: BCC 14.60.100
Reviewer: Abdy Farid, 425-452-7698

7. SITE (CIVIL ENGINEERING) PLANS

A street lighting plan and site (civil engineering) plan produced by a qualified engineer must be approved by the City prior to building permit approval. The design of all street frontage improvements must be in conformance with the requirements of the Americans with Disabilities Act and the Transportation Development Code, and the provisions of the Transportation Department Design Manual.

AUTHORITY: BCC 14.60; Transportation Department Design Manual
Reviewer: Abdy Farid, 425-452-7698

8. DEDICATION OF RIGHT OF WAY

The applicant shall dedicate right of way to the City along the existing property frontage on NE 10th such that the back of curb are located within the public right of way. The proposed bus pullout will not be dedicated to the City.

AUTHORITY: BCC 14.60.090
Reviewer: Abdy Farid, 425-452-7698

9. BUS PULLOUT AGREEMENT

The applicant will sign and record an agreement that acknowledges that the applicant is fully responsible for the operation and maintenance of the pullout. In addition, the agreement will provide for utilities easements under the area of the bus pullout.

AUTHORITY: BCC 14.60.090
Reviewer: Abdy Farid, 425-452-7698

10. FIRE DEPARTMENT CONDITIONS

- a. Provide automatic fire sprinklers throughout the building designed per NFPA 13.

AUTHORITY: International Fire Code (IFC) 903
Reviewer: Adrian Jones, 425-452-6032

- b. Provide 2 fire department connections at approved locations and within 50 feet of a fire hydrant.

AUTHORITY: International Fire Code (IFC) 903
Reviewer: Adrian Jones, 425-452-6032

- c. Provide 2 1/2 inch hose connections at intermediate stair locations such that 200 feet of hose reaches the most remote location throughout the building. (IFC 905)

AUTHORITY: International Fire Code (IFC) 905
Reviewer: Adrian Jones, 425-452-6032

- d. Provide 2 fire pumps. One taking suction from the city water system and one taking suction from the on site water tank.

AUTHORITY: International Fire Code (IFC) 903
Reviewer: Adrian Jones, 425-452-6032

- e. Provide an audible and visible fire alarm system throughout the building.

AUTHORITY: International Fire Code (IFC) 905 & BCC 5675
Reviewer: Adrian Jones, 425-452-6032

- f. Provide a voice/alarm communications system in the Group A occupancy area.

AUTHORITY: International Fire Code (IFC) 907
Reviewer: Adrian Jones, 425-452-6032

g. Provide a building radio coverage system throughout the building.

AUTHORITY: BCC 5675

Reviewer: Adrian Jones, 425-452-6032

h. Provide a smoke control system throughout the building.

AUTHORITY: BFDDS Chapter 11

Reviewer: Adrian Jones, 425-452-6032

i. Provide a fire control room with direct exterior access at the address of the building.

AUTHORITY: IFC 509

Reviewer: Adrian Jones, 425-452-6032

j. Provide standby and emergency power system for the life safety systems in the building.

AUTHORITY: IFC 604

Reviewer: Adrian Jones, 425-452-6032

k. Provide information on the fuel storage and filling locations for the emergency power systems.

AUTHORITY: IFC Chapter 34

Reviewer: Adrian Jones, 425-452-6032

l. Provide information on how this building will connect with existing and proposed buildings.

AUTHORITY: IFC 903

Reviewer: Adrian Jones, 425-452-6032

11. TRANSPORTATION IMPACT FEE

The Bellevue Place Hyatt Expansion development is subject to the Transportation Improvement Program requirement to pay a traffic impact fee. The amount of the fee will be determined at the time of the building permit issuance. The fee schedule in effect at the time of building permit issuance will apply.

AUTHORITY: BCC 22.16

Reviewer: Abdy Farid, 425-452-7698

12. ABOVE-GRADE MECHANICAL EQUIPMENT SCREENING

(a) Show the location of each piece of above-grade mechanical equipment, including communication equipment such as satellite dishes, and demonstrate that screening is provided so that these items are not visible from adjacent streets, public sidewalks, or the surrounding buildings. Above-grade equipment boxes shall not be located in

- the middle of the sidewalk and shall not block pedestrian access.
- (b) Power, telephone, or other equipment shall not be located in above ground cabinets in sidewalk areas. Such equipment shall be located in underground vaults or in a building or substantially screened per the approval of PCD.

AUTHORITY: LUC 20.20.650, 20.25A.110.B, 20.20.730
Reviewer: Carol Saari, 425-452-2731

D. PRIOR TO TEMPORARY CERTIFICATE OF OCCUPANCY ISSUANCE: The following conditions are required by City Code and supported by City Policy. The conditions shall be complied with prior to issuance of the Temporary Certificate of Occupancy (TCO):

1. PROJECT SIGNAGE AND SIGNAGE FOR MID-BLOCK CONNECTION

- a. A sign master plan package in compliance with the Sign Code shall be submitted to the Permit Center for review and approval as an application for a Sign Permit and revision to the Design Review.
- b. The public signs for the mid-block connection shall be installed. One sign shall be installed at the NE 10th Street entrance. Additional signs shall be installed (interior and exterior) within this project area to be consistent with the mid-block connection plan, as established by the original Bellevue Place Design Review Approval (ADR-85-56). Sign location shall be confirmed by a field inspection by the Land Use Planner. Signs shall be consistent with the City of Bellevue Downtown Wayfinding document dated June 2005. The mid-block connection shall remain open during business hours.

AUTHORITY: BCC 22B.025, LUC 20.25B.040.G, LUC 20.25A.060.C
Reviewer: Carol Saari, 425-452-2731

2. LANDSCAPE MAINTENANCE ASSURANCE DEVICE

The applicant shall file with the Department of Planning & Community Development a landscape maintenance assurance device for a one-year period in the form of an assignment of savings or letter of credit for 20% of the cost of labor and materials for all required landscaping.

AUTHORITY: LUC 20.40.490
Reviewer: Carol Saari, 425-452-2731

3. RECORD AN AGREEMENT TO PROVIDE PEDESTRIAN ORIENTED FRONTAGE USES AND DESIGN REVIEW CONDITIONS OF APPROVAL

The applicant shall record the following with the King County Office of Records and Elections:

- (a) The applicant shall record an agreement that 100% of the street level edge of NE 10th Street will remain pedestrian-oriented uses with pedestrian-oriented design elements, per LUC 20.25A.020.115.
- (b) The applicant shall record a document which contains all of the Design Review Conditions of Approval, Amenities Plan and FAR calculations. The document shall also include the FAR points and amenities for the entire Bellevue Place project limit and transfer of points for the Hyatt Hotel expansion, as noted in Tables 1 and 2 of this Design Review staff report.

AUTHORITY: LUC 20.25A.020.C, LUC 20.25A.020, .040, .050, .100, .115
Reviewer: Carol Saari, 425-452-2731

4. STREET FRONTAGE IMPROVEMENTS

All street frontage improvements and other required transportation elements, including pavement widening on NE 10th, and street light relocation, must be constructed by the applicant and accepted by the City Inspector. All existing street light and traffic signal apparatus affected by this development, including traffic controllers, pedestrian signal poles, traffic signal poles, and power sources, must be relocated as necessary. Transformers and utility vaults to serve the building shall be placed inside the building or below grade, to the extent feasible. Bonding or other types of assurance devices will not be accepted in lieu of construction. Specific requirements are detailed below.

- a) The applicant is responsible for full frontage improvements on Bellevue Way and NE 10th, including the construction of 12-foot wide bus pullout. The frontage improvements call for street widening, installation of curb and gutter, storm detention system, a new street lighting system, concrete sidewalks, street trees, pavement marking, installation of traffic signs and installation of C-Curb in front of the proposed bus pullout on NE 10th.
- b) The Americans with Disabilities Act (ADA) requires that sidewalk cross slopes shall not exceed two percent. The sidewalk cross slope may be less than two percent only if the sidewalk has a longitudinal slope sufficient to provide adequate drainage. Bellevue's standard for curb height is six inches, except where curb ramps are needed. The engineering plans must comply with these requirements, and must show adequate details, including spot elevations, to confirm compliance. New curb and sidewalk shall be constructed in compliance with these requirements. Building elevations shall be consistent with the required curb and sidewalk elevations. Curb and sidewalk elevations will not be revised to fit the building, and city inspectors may require spot surveys during construction in order to confirm the required elevations.
- c) Miscellaneous:
 - ♦ Driveway aprons for the proposed commercial driveways on the sites frontage must be constructed in accordance with Design Manual Standard Drawing DEV-6. C-curbs and appropriate signs will be installed since these driveways will

operate as right-in and right-out only.

- ♦ Landings on sloping approaches are not to exceed a 7% slope for a distance of 30 feet approaching the back edge of sidewalks. Driveway grades must be designed to prevent vehicles from bottoming out due to abrupt changes in grade.
- ♦ Vehicle and pedestrian sight distance must be provided per BCC 14.60.240 and 14.60.241. Sight distance triangles must be shown at all driveway locations and must consider all fixed objects and mature landscape vegetation. Vertical as well as horizontal line of sight must be considered when checking for sight distance.
- ♦ Any awning, marquee, balcony, etc. over a sidewalk or utility easement must be at least 16 feet above the sidewalk, or be removable (with an agreement regarding removal and replacement); and must have at least 3 feet horizontal clearance from any streetlight or traffic signal pole.

AUTHORITY: BCC 14.60.090, 100, 110, 120, 150, 181, 190, 200, 210, 240, 241;
Transportation Department Design Manual Sections 9, 12, 14, 19, 20; and
Transportation Department Design Manual Standard Drawings DEV-2, DEV-3, DEV-6,
DEV-10, TE-4, TE-5, TE-7, TE-10, TE-11, TE-12 and TE-21.
Reviewer: Abdy Farid, 425-452-7698

5. PAVEMENT RESTORATION

Pavement restoration associated with street frontage improvements or to repair damaged street surfaces shall be provided as follows:

- a. NE 10th: The street is designated as “No Cut” street. Should street cuts prove unavoidable or if the street surface is damaged in the construction process, the entire concrete panel or panels must be replaced
- b. Bellevue Way and 106th NE: The street is currently classified as “Standard Trench Restoration”, which requires trench restoration per the City’s Design Manual Drawing ROW-3.

AUTHORITY: BCC 14.60. 250; Design Manual Design Standard #21
Reviewer: Jon Regalia, 425-452-4599

6. SOLID WASTE/RECYCLING

The applicant shall provide a written document showing that Allied Waste (206-682-3037) has been contacted to establish adequate sizing of recycling and solid waste collection area for this hotel expansion using current standards. All rights of way and public easements shall not be occupied by trash receptacles, dumpsters, recycling bins or other such items.

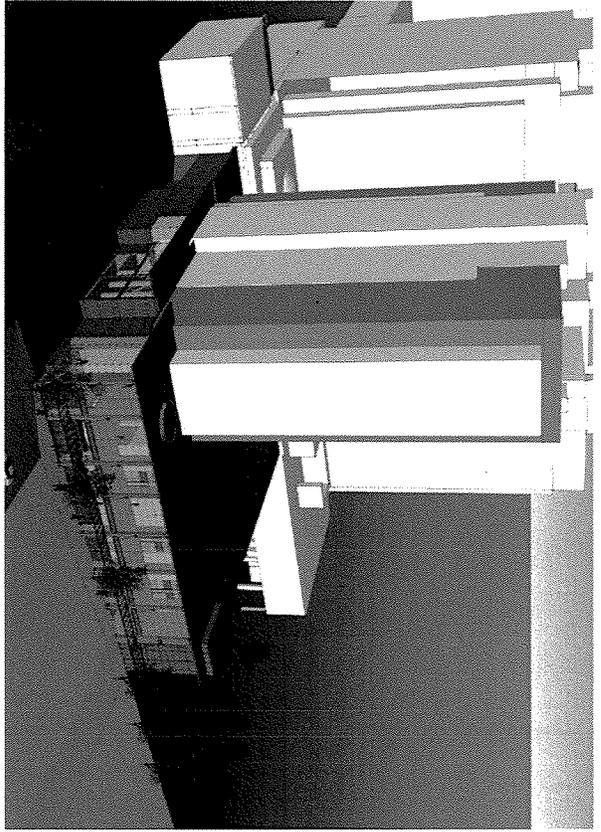
AUTHORITY: LUC 20.20.720
Reviewer: Carol Saari, 425-452-2731

E. OTHER

1. PLAZA AREA IN THE SOUTHEAST CORNER

If the PACE project is abandoned or delayed, from submitting the Design Review application, 5 years or more from the date of this subject approval, the applicant for the Hyatt expansion shall submit for review and approval a new plaza plan which includes warmer plaza materials (i.e. wood) and less concrete, more seating, tables, pedestrian-scale lighting, seasonal flowers, ornamental shrubs and trees at a pedestrian scale (not to exceed 15' at maturity).

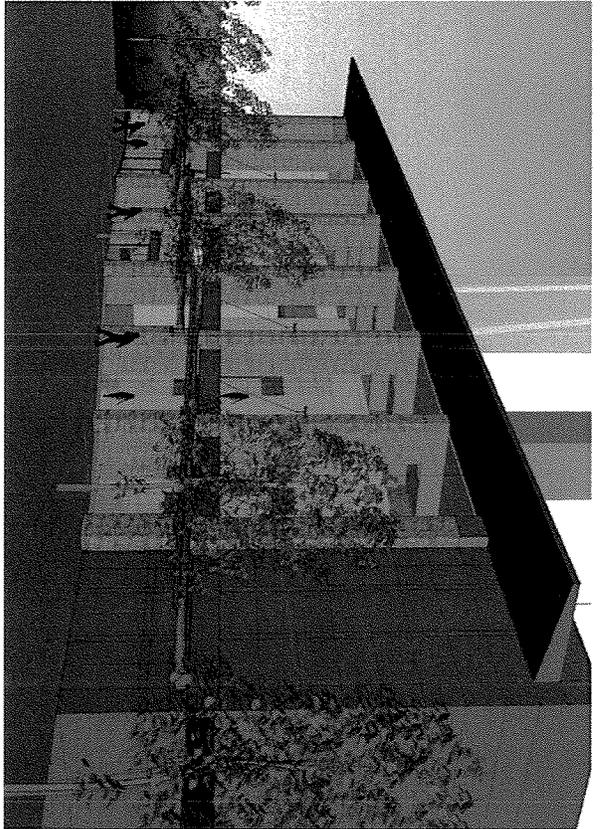
AUTHORITY: Comprehensive Plan Policies UD-5, UD-14, UD-15, UD-30
Reviewer: Carol Saari, 425-452-2731



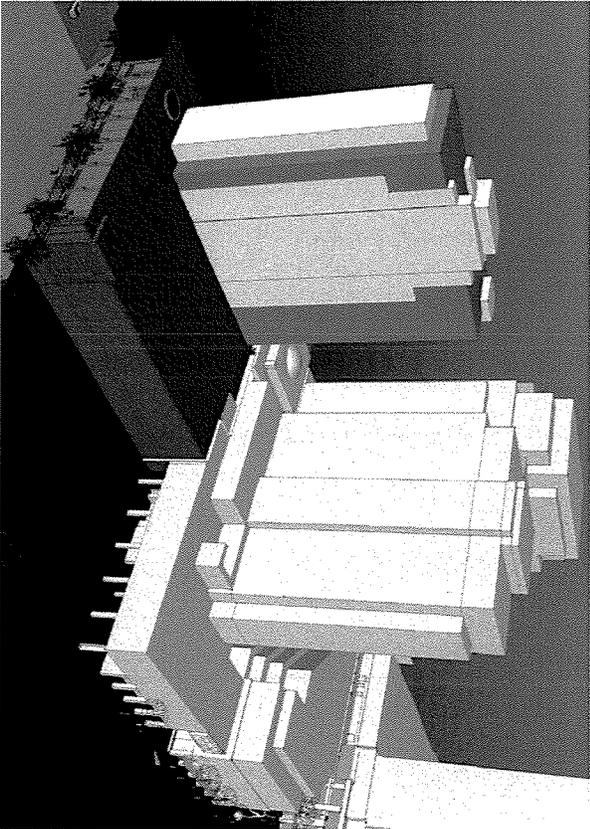
Aerial View from Northeast



Street Level View from Northeast



Street Level View from Northwest



Aerial View from Northwest

BELLEVUE PLACE HYATT EXPANSION
BELLEVUE WASHINGTON

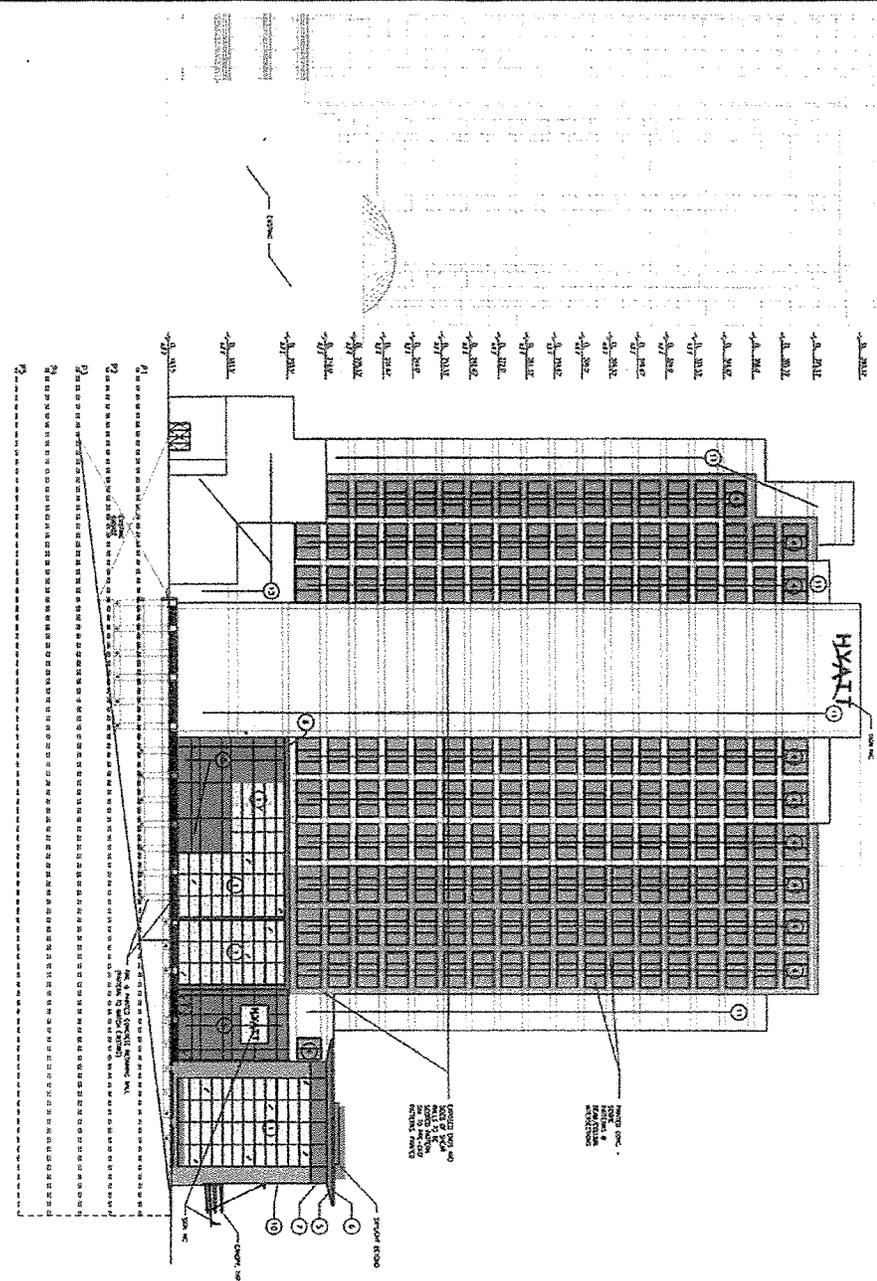
CITY OF BELLEVUE A.D.R.
 ISSUED: 02 OCTOBER 06



NOT FOR CONSTRUCTION

CITY OF BELLEVUE A.D.R.
 PERSPECTIVES

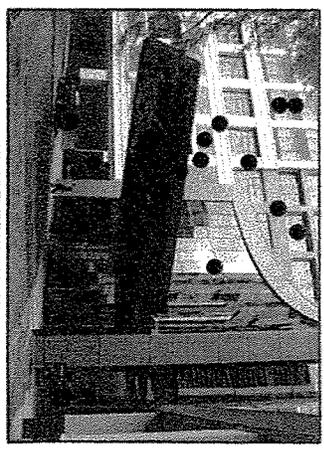
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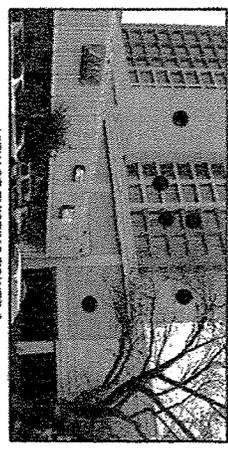
- 1. ALL UNITS TO MATCH EXISTING AT THIS
- 2. ALL EXISTING EXCEPT STYCO AND GUN TO BE REFINISHED
- 3. SOUND INSULATION BY OTHER CONTRACTOR. ALL EXISTING STAYS AND BEAMS EXPOSED AND PAINT FINISHED BY OTHERS

MATERIAL LEGEND

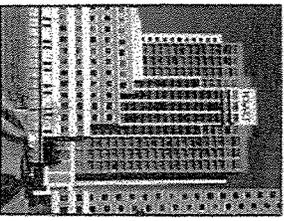
No.	SYMBOL	DESCRIPTION	FINISH	REMARKS
1	(Symbol)	CONCRETE	CONCRETE	AS-BUILT
2	(Symbol)	GLASS	GLASS	AS-BUILT
3	(Symbol)	STEEL	PAINTED	AS-BUILT
4	(Symbol)	WOOD	WOOD	AS-BUILT
5	(Symbol)	BRICK	BRICK	AS-BUILT
6	(Symbol)	STONE	STONE	AS-BUILT
7	(Symbol)	ROOFING	ROOFING	AS-BUILT
8	(Symbol)	MECHANICAL	MECHANICAL	AS-BUILT
9	(Symbol)	ELECTRICAL	ELECTRICAL	AS-BUILT
10	(Symbol)	PLASTER	PLASTER	AS-BUILT
11	(Symbol)	PAINT	PAINT	AS-BUILT
12	(Symbol)	GLASS	GLASS	AS-BUILT
13	(Symbol)	STEEL	PAINTED	AS-BUILT
14	(Symbol)	WOOD	WOOD	AS-BUILT
15	(Symbol)	BRICK	BRICK	AS-BUILT
16	(Symbol)	STONE	STONE	AS-BUILT
17	(Symbol)	ROOFING	ROOFING	AS-BUILT
18	(Symbol)	MECHANICAL	MECHANICAL	AS-BUILT
19	(Symbol)	ELECTRICAL	ELECTRICAL	AS-BUILT
20	(Symbol)	PLASTER	PLASTER	AS-BUILT
21	(Symbol)	PAINT	PAINT	AS-BUILT
22	(Symbol)	GLASS	GLASS	AS-BUILT
23	(Symbol)	STEEL	PAINTED	AS-BUILT
24	(Symbol)	WOOD	WOOD	AS-BUILT
25	(Symbol)	BRICK	BRICK	AS-BUILT
26	(Symbol)	STONE	STONE	AS-BUILT
27	(Symbol)	ROOFING	ROOFING	AS-BUILT
28	(Symbol)	MECHANICAL	MECHANICAL	AS-BUILT
29	(Symbol)	ELECTRICAL	ELECTRICAL	AS-BUILT
30	(Symbol)	PLASTER	PLASTER	AS-BUILT
31	(Symbol)	PAINT	PAINT	AS-BUILT
32	(Symbol)	GLASS	GLASS	AS-BUILT
33	(Symbol)	STEEL	PAINTED	AS-BUILT
34	(Symbol)	WOOD	WOOD	AS-BUILT
35	(Symbol)	BRICK	BRICK	AS-BUILT
36	(Symbol)	STONE	STONE	AS-BUILT
37	(Symbol)	ROOFING	ROOFING	AS-BUILT
38	(Symbol)	MECHANICAL	MECHANICAL	AS-BUILT
39	(Symbol)	ELECTRICAL	ELECTRICAL	AS-BUILT
40	(Symbol)	PLASTER	PLASTER	AS-BUILT
41	(Symbol)	PAINT	PAINT	AS-BUILT
42	(Symbol)	GLASS	GLASS	AS-BUILT
43	(Symbol)	STEEL	PAINTED	AS-BUILT
44	(Symbol)	WOOD	WOOD	AS-BUILT
45	(Symbol)	BRICK	BRICK	AS-BUILT
46	(Symbol)	STONE	STONE	AS-BUILT
47	(Symbol)	ROOFING	ROOFING	AS-BUILT
48	(Symbol)	MECHANICAL	MECHANICAL	AS-BUILT
49	(Symbol)	ELECTRICAL	ELECTRICAL	AS-BUILT
50	(Symbol)	PLASTER	PLASTER	AS-BUILT
51	(Symbol)	PAINT	PAINT	AS-BUILT
52	(Symbol)	GLASS	GLASS	AS-BUILT
53	(Symbol)	STEEL	PAINTED	AS-BUILT
54	(Symbol)	WOOD	WOOD	AS-BUILT
55	(Symbol)	BRICK	BRICK	AS-BUILT
56	(Symbol)	STONE	STONE	AS-BUILT
57	(Symbol)	ROOFING	ROOFING	AS-BUILT
58	(Symbol)	MECHANICAL	MECHANICAL	AS-BUILT
59	(Symbol)	ELECTRICAL	ELECTRICAL	AS-BUILT
60	(Symbol)	PLASTER	PLASTER	AS-BUILT
61	(Symbol)	PAINT	PAINT	AS-BUILT
62	(Symbol)	GLASS	GLASS	AS-BUILT
63	(Symbol)	STEEL	PAINTED	AS-BUILT
64	(Symbol)	WOOD	WOOD	AS-BUILT
65	(Symbol)	BRICK	BRICK	AS-BUILT
66	(Symbol)	STONE	STONE	AS-BUILT
67	(Symbol)	ROOFING	ROOFING	AS-BUILT
68	(Symbol)	MECHANICAL	MECHANICAL	AS-BUILT
69	(Symbol)	ELECTRICAL	ELECTRICAL	AS-BUILT
70	(Symbol)	PLASTER	PLASTER	AS-BUILT
71	(Symbol)	PAINT	PAINT	AS-BUILT
72	(Symbol)	GLASS	GLASS	AS-BUILT
73	(Symbol)	STEEL	PAINTED	AS-BUILT
74	(Symbol)	WOOD	WOOD	AS-BUILT
75	(Symbol)	BRICK	BRICK	AS-BUILT
76	(Symbol)	STONE	STONE	AS-BUILT
77	(Symbol)	ROOFING	ROOFING	AS-BUILT
78	(Symbol)	MECHANICAL	MECHANICAL	AS-BUILT
79	(Symbol)	ELECTRICAL	ELECTRICAL	AS-BUILT
80	(Symbol)	PLASTER	PLASTER	AS-BUILT
81	(Symbol)	PAINT	PAINT	AS-BUILT
82	(Symbol)	GLASS	GLASS	AS-BUILT
83	(Symbol)	STEEL	PAINTED	AS-BUILT
84	(Symbol)	WOOD	WOOD	AS-BUILT
85	(Symbol)	BRICK	BRICK	AS-BUILT
86	(Symbol)	STONE	STONE	AS-BUILT
87	(Symbol)	ROOFING	ROOFING	AS-BUILT
88	(Symbol)	MECHANICAL	MECHANICAL	AS-BUILT
89	(Symbol)	ELECTRICAL	ELECTRICAL	AS-BUILT
90	(Symbol)	PLASTER	PLASTER	AS-BUILT
91	(Symbol)	PAINT	PAINT	AS-BUILT
92	(Symbol)	GLASS	GLASS	AS-BUILT
93	(Symbol)	STEEL	PAINTED	AS-BUILT
94	(Symbol)	WOOD	WOOD	AS-BUILT
95	(Symbol)	BRICK	BRICK	AS-BUILT
96	(Symbol)	STONE	STONE	AS-BUILT
97	(Symbol)	ROOFING	ROOFING	AS-BUILT
98	(Symbol)	MECHANICAL	MECHANICAL	AS-BUILT
99	(Symbol)	ELECTRICAL	ELECTRICAL	AS-BUILT
100	(Symbol)	PLASTER	PLASTER	AS-BUILT



VIEW OF EXISTING TOWER & PODIUM (SOUTH FACADE AT PORTE COCHERE)



VIEW OF EXISTING TOWER & PODIUM (NORTH FACADE)



VIEW OF EXISTING TOWER FROM SOUTH FOR REFERENCE

EAST ELEVATIONS 1

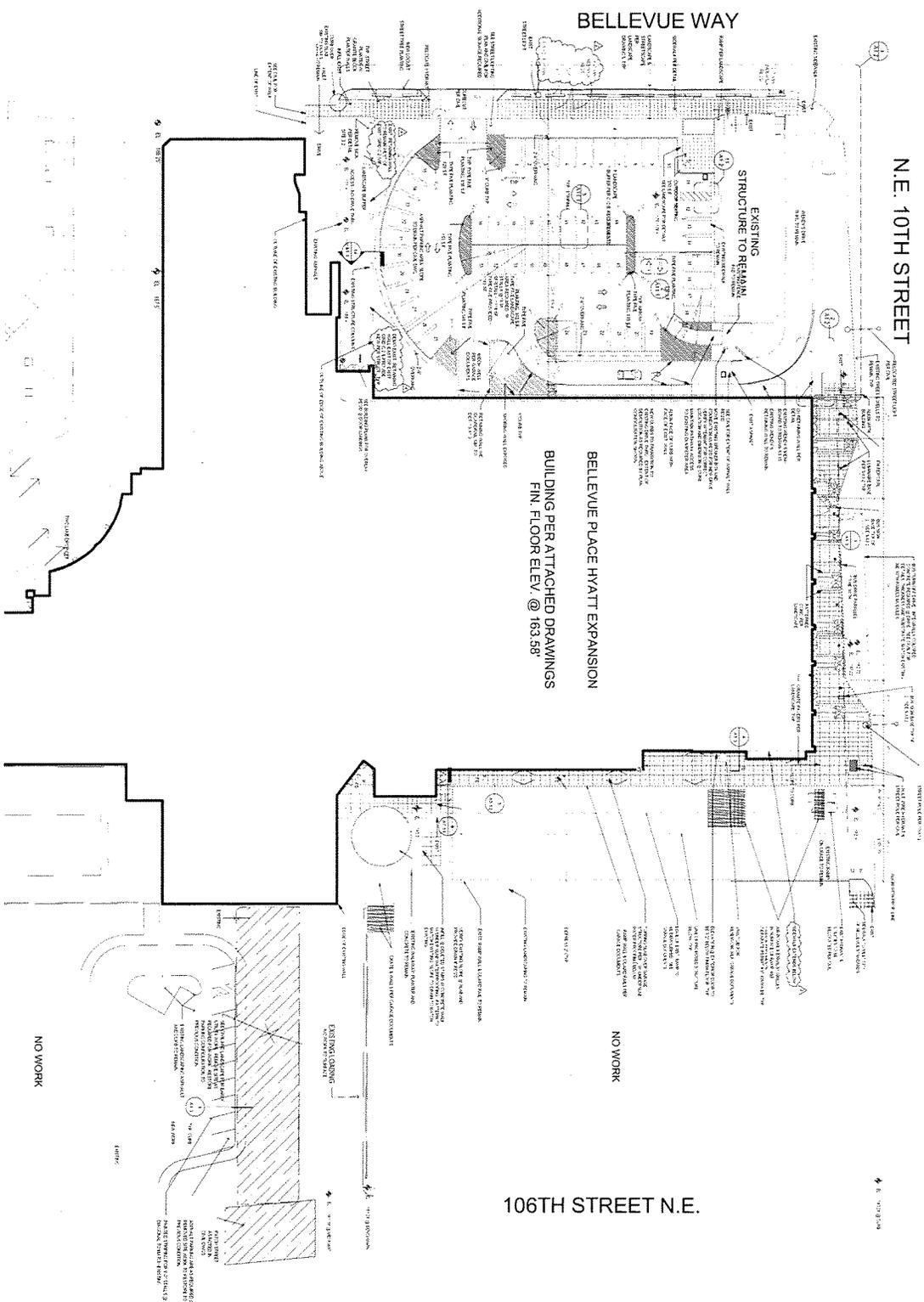
BELLEVUE PLACE HYATT EXPANSION
BELLEVUE WASHINGTON
CITY OF BELLEVUE A.D.R.
ISSUED: 02 OCTOBER 06



CITY OF BELLEVUE A.D.R.
EAST ELEVATION

0811.000 AA.1

NOT FOR CONSTRUCTION



Site Plan
SCALE
1" = 10'-0"

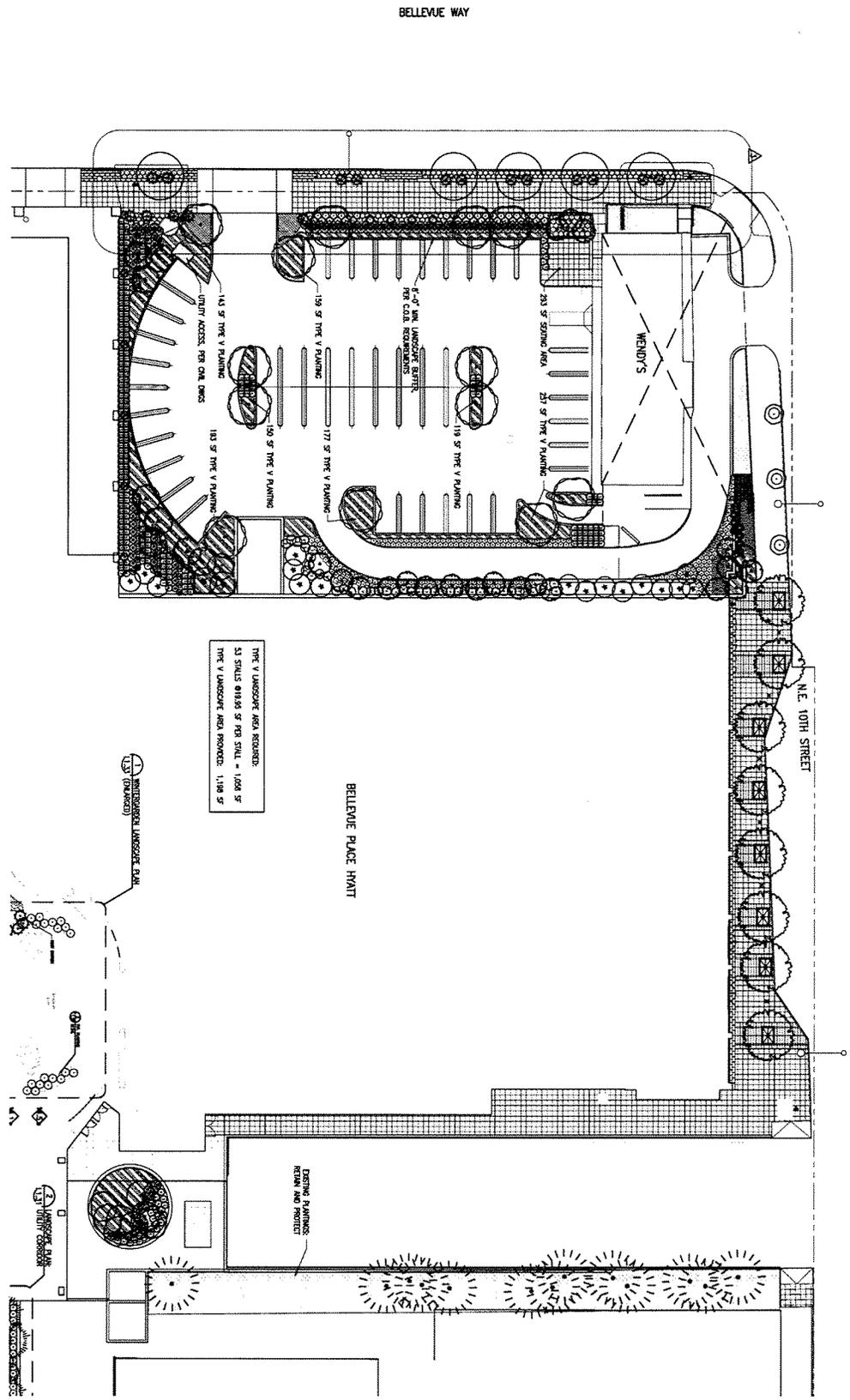
12/8/05 0611 0613
A1.1

Site Plan

Bellevue Place Hyatt Expansion
Bellevue, Washington
GMAX / CONSTRUCTION SET
June 25, 2007

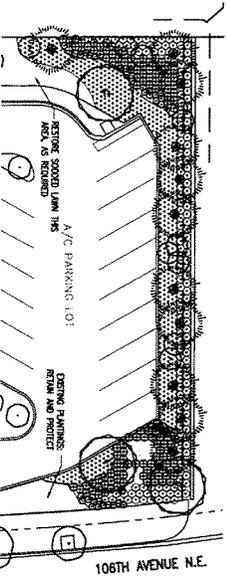


1 LANDSCAPE PLAN: BELLEVUE WY & NE 10TH ST FRONTAGES
 SCALE: 1/8" = 1'-0"



TYPE V LANDSCAPE AREA RECORDED:
 53 STUBS @ 18x8 SF PER STUB = 1,008 SF
 TYPE V LANDSCAPE AREA PROVIDED: 1,188 SF

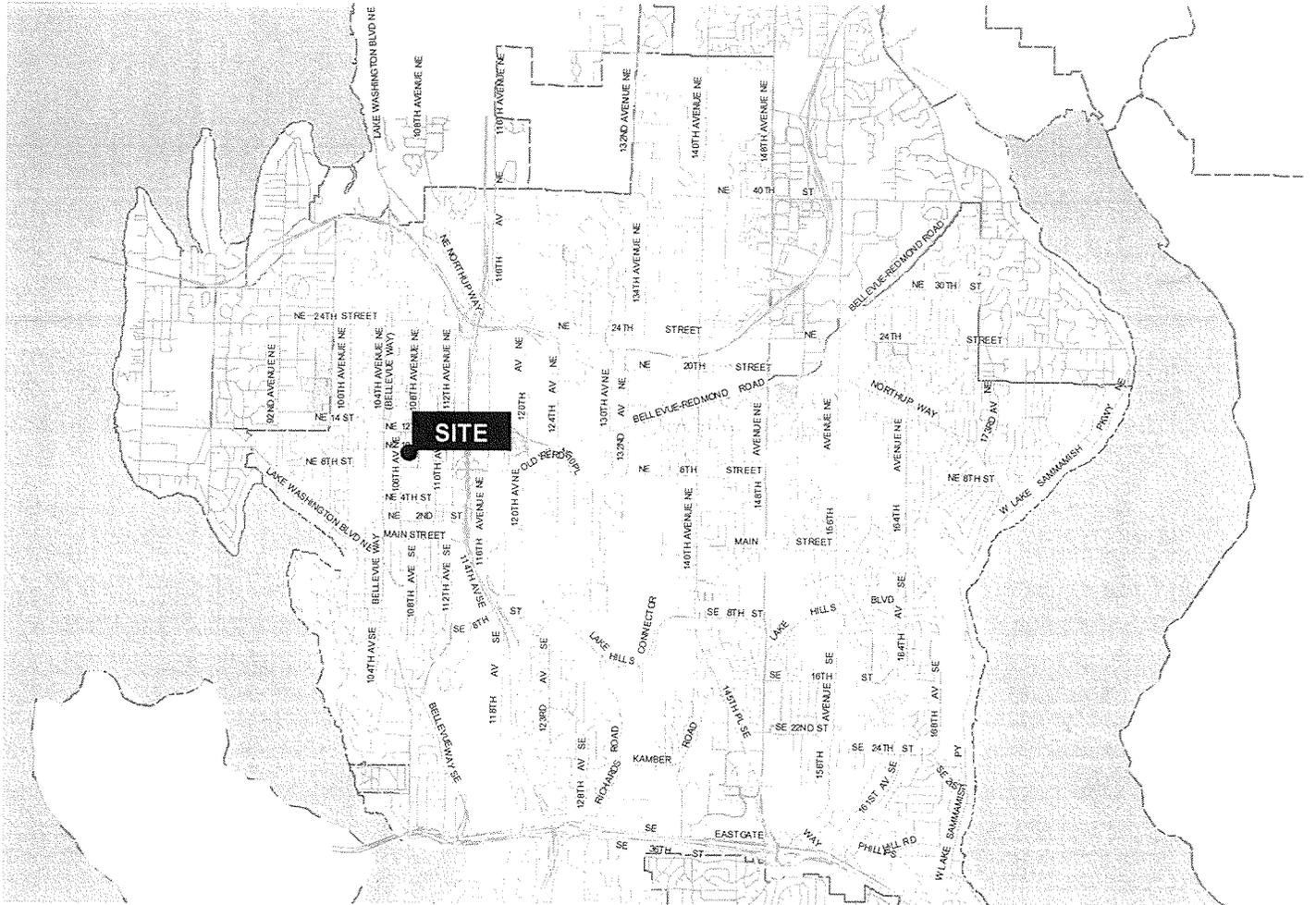
2 LANDSCAPE PLAN: UTILITY CORRIDOR @ 106TH AVE NE
 SCALE: 1/8" = 1'-0"



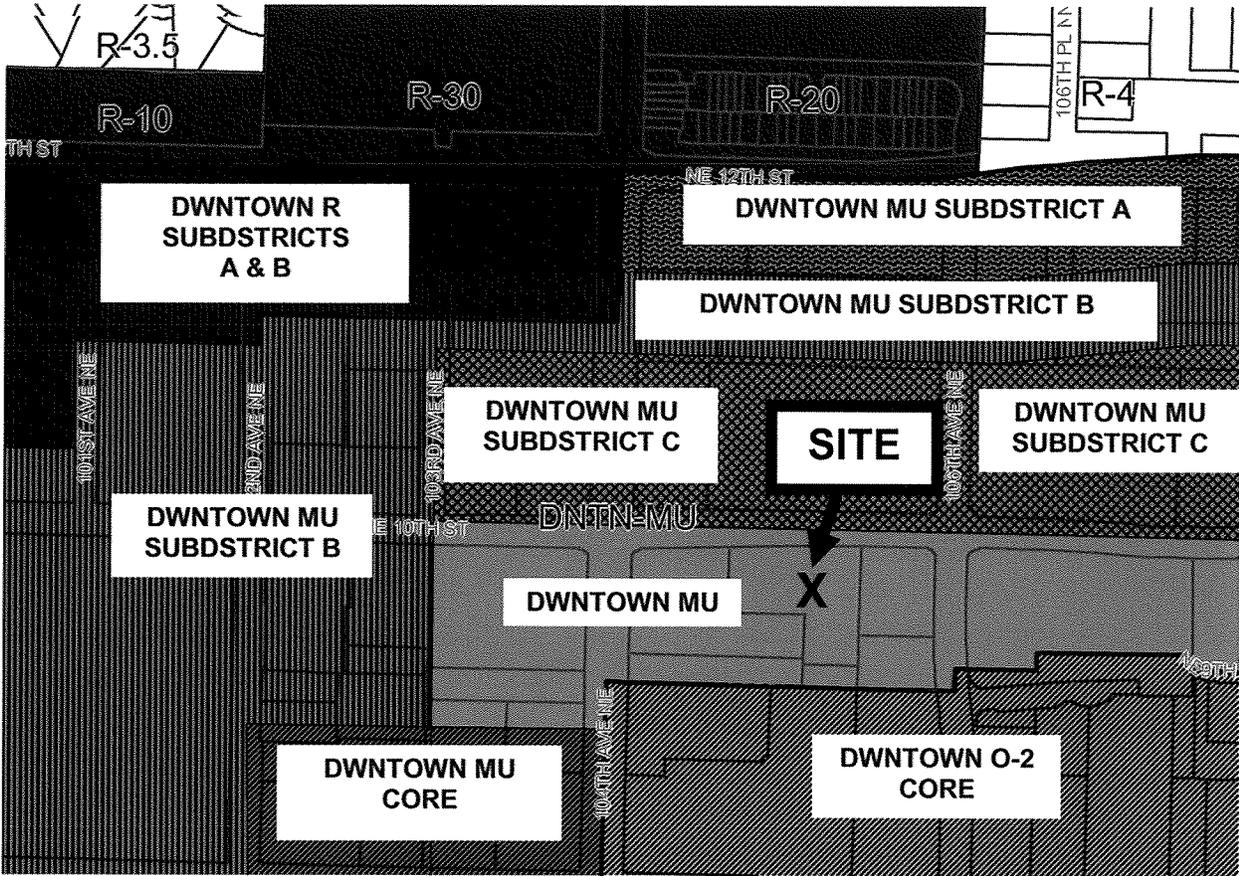
NOTE:
 SEE SHEET L1.32 FOR PLANTING SCHEDULE

DATE: 6/25/07 DRAWN BY: [Name] CHECKED BY: [Name] SCALE: L1.31	PROJECT: BELLEVUE PLACE HYATT EXPANSION SHEET: L1.31		BELLEVUE PLACE HYATT EXPANSION Bellevue, Washington GMAX / CONSTRUCTION SET June 25, 2007	

Vicinity Map



Zoning Map



Comprehensive Plan Policies

Site and Building Design Policies

Policy UD-1. Encourage high-quality, attractive architecturally appealing designs for major buildings in order to create distinctive visual reference points in the community.

Policy UD-2. Support designs for the built environment that are visually stimulating and thoughtful and which convey excellence in architecture and workmanship, and durability in building materials.

Policy UD-3. Encourage a variety of site and building designs which are compatible and consistent with surrounding development and that implement the policies of this plan.

Policy UD-4. Ensure that development relates, connects, and continues design quality and site functions from site to site.

Policy UD-5. Include accessible and attractive places for the general public, employees and visitors to wait, to be outdoors, or to socialize in more intensive commercial development.

Policy UD-6. Design buildings located on the edge of public spaces using materials, forms, details and other architectural elements that will enrich the appearance of the places and encourage people to use them.

Policy UD-8. Design rooftop mechanical screening so that it is integral with building architecture. Consider the visual effects of technical advances, such as satellite dishes, on building design.

POLICY S-DT-36. Utilize development standards for building bulk, heights, setbacks, landscaping requirements, setbacks, floor area ratios, open space requirements, and development incentives.

POLICY S-DT-37. Link building intensity to design guidelines relating to building appearance, amenities, pedestrian orientation and connections, impact on adjacent properties, and maintenance of view corridors. These guidelines will seek to enhance the appearance, image, and design character of the Downtown.

Policy UD-45. Establish attractive gateways at all key entry points into the city and into smaller districts and communities within the city.

Discussion: Study the concept of gateways to recommend various design treatments and locations at key entry points into the city and its neighborhoods. A gateway can be dramatic and obvious, sometimes including non-commercial signs, art, structures, and unique lighting in commercial areas. It can also be quiet and more subtle, using signs, a change in plant material or paving surface in residential areas.

Policy S-DT-48. Provide for a sense of approach to Downtown at key entry points through the use of gateways and identity treatments that convey a sense of quality and permanence.

Policy S-DT-56. Differentiate Downtown Districts through streetscape improvements such as wayfinding elements, gateways, mid-block pedestrian crossings, public art, landscaping and street trees, lighting, and street furniture.

The design of the building is intended to give a sense of excellence and durability of materials. The tower is very similar to the existing Hyatt Hotel tower. It is lower in height (by approximately 50 feet), but will use the same materials and colors as the existing tower. The tower will consist of a concrete frame with stucco end

walls. This frame and end walls will be painted to match the currently two tone scheme of the existing tower with tan and terra cotta colors. The top of the tower will be tapered with stepbacks at the upper floors for stairs and mechanical screening. Additional scoring will be provided along the north tower elevation to provide a “finished look” to this gateway building into downtown Bellevue. See condition #X.C.1.

The rooftop equipment and screening will be integral with the building architecture. The lower roof will have a pattern to provide a more interesting feature than a plain black roof when viewed from above. The upper rooftop equipment will be screened from all sides and is architecturally integrated with the tower design. All rooftop screening for mechanical equipment will be the minimum necessary. See condition #X.C.2.

The mid-block connection will complete the pedestrian connection from NE 8th Street to NE 10th Street. Public access signs will be visible to pedestrians. See condition #X.D.1.

Downtown Policies

Policy UD-67. Enhance the appearance, image, and design character of the downtown to be an inspiring place to live, shop, play, and work.

Policy UD-68. Encourage rooflines which create interesting and distinctive forms against the sky within downtown.

Policy UD-71. Permit high intensity residential development subject to design criteria which assures a livable urban environment.

Policy UD-72. Link the increased intensity of development with the increased pedestrian amenities, pedestrian-oriented building design, mid-block connections, public spaces, activities, openness, sunlight, and view preservation.

Policy UD-73. Create a pedestrian environment with a sense of activity and protection.

As described above, the project will provide a design character which will enhance the downtown and create a sense of gateway to downtown Bellevue. The project provides a mid-block connection to complete the pedestrian connection between NE 8th Street and NE 10th Street. The rooftops will have mechanical equipment which will be screened appropriately. See condition #X.C.2. There will be pedestrian activity at the street level with the use of the hotel for those arriving for overnight lodging, for conferences or for ballroom activities.

Vegetation and Landscaping Policies

Policy UD-14. Encourage seasonal color plantings in public and semi-public areas.

Policy UD-15. Use landscaping design that is urban in character in more urban settings.

Site and landscaping design objectives focus on integrating the project into the downtown context through the use of plant materials appropriate to the region and consistent with project environs. Landscaping is proposed along all three the NE 10th Street right-of-way, including street trees, shrubs and ground cover.

Open Space Policies

Policy UD-21. Encourage major development to integrate public and semi-public open spaces.

The project will retain the existing plaza in the southeast corner of the site.

Public Places and Connections Policies

Policy UD-29. Provide a system of public spaces of various sizes and types throughout the community.

Policy UD-30. Ensure public spaces give access to sunlight, a sense of security, seating, landscaping, accessibility, and connections to surrounding uses and activities.

Policy UD-32. Ensure access to sunlight in major public spaces by avoiding building shadows during periods of the year and times of day when outdoor activity is most prevalent.

The mid-block connection will be interior and open during business hours. As such, it will provide a sense of security for pedestrians using the connection. The Bellevue Place multi-building complex provides public open space within the Wintergarden and at the entrance to the existing Hyatt Hotel.

Sidewalks, Walkways, and Trails Policies

Policy UD-38. Ensure continuous and ample sidewalks along principal, minor and collector arterials which are integrated with abutting land uses.

Policy UD-39. Include clear and ample walkways from street sidewalks and parking areas to building entrances and within and between developments as a part of the site design.

Policy UD-40. Ensure that sidewalks, walkways, and trails are furnished, where needed and appropriate, with lightning, seating, landscaping, street trees, trash receptacles, public art, bike racks, railings, handicap access, newspapers boxes etc. without interfering with pedestrian circulation.

The mid-block connection will provide access for the public from NE 8th Street to NE 10th Street via an interior connection. This mid-block connection will be signed for public visibility. See condition #X.D.1. An ample sidewalk width of 12' will be provided along NE 10th Street (8' wide plus 4' wide planter).

Street Corridors Policies

Policy UD-43. Provide clear and identifiable circulation systems into and through Bellevue's large commercial blocks to improve pedestrian's activity.

Policy UD-45. Establish attractive gateways at all key points into the city and into smaller districts and communities within the city.

The vehicular access to the Hyatt Hotel expansion building will be from NE 10th Street, however, guests and visitors may use any of the access points to the Bellevue Place complex (underground parking is connected).

Land Use Compatibility Policies

Policy LU-7. Support inclusion of residential uses in commercial districts where compatibility can be demonstrated.

Policy LU-9. Maintain compatible use and design with the surrounding built environment when considering new development or redevelopment within an already developed areas.

Policy LU-13. Reduce the regional consumption of undeveloped land by facilitating redevelopment of existing developed land when appropriate.

The Downtown Mixed-Use zoning designation allows for a dense variety of commercial and residential land uses. This project incorporates hotel, conference, ballroom, hotel administrative office and retail.

CERTIFICATE OF CONCURRENCY

BELLEVUE PLACE HYATT EXPANSION

This certificate documents the Transportation Department Director's decision that the development project at 10500 NE 8th Street (Design Review File No. 06-123807 LD) complies with the requirements of the Traffic Standards Code (BCC 14.10). This decision reserves 94 p.m. peak hour trips to that project, subject to Process II appeal of either the concurrency determination or the Design Review decision. A building permit application (File No. 07-105006 BB) was filed for the project on February 6, 2007 and deemed complete on February 9, 2007. This concurrency reservation will remain in effect for the life of the building permit application (BCC 23.05.090.H). Upon issuance of the building permit, concurrency is reserved for one year; the applicant may request up to two one-year extensions (BCC 23.05.100.E).



Director, Transportation Department

8/30/07

Date

Certificate No. 43

Intersection LOS by UFOSNET using HCM 2000 Operation Method

*** >2012 Background vs. Bellevue Place Expansiton FMPK 2-Hr Avg LOS Comparison

Area 1: North Bellevue

INT	ADDRESS	MP6_R8(2)	DEVREV
69	Bellevue Way NE - NE 24th Street	0.639 B	0.638 B
74	Bellevue Way NE - Northup Way NE	0.627 B	0.623 B
78	108th Ave NE - Northup Way NE	0.765 C	0.769 C
93	Lk Washington B - NE 1st/NE 10th	0.285 A	0.285 A
Area wide average ->		0.579 A	0.579 A

Area 2: Bridle Trails

INT	ADDRESS	MP6_R8(2)	DEVREV
118	Northup Way - NE 24th Street	0.557 A	0.552 A
123	140th Ave NE - NE 40th Street	-----	-----
Area wide average ->		0.557 A	0.552 A

Area 3: Downtown

INT	ADDRESS	MP6_R8(2)	DEVREV
3	100th Ave NE - NE 8th Street	0.538 A	0.537 A
5	Bellevue Way NE - NE 12th Street	0.725 C	0.722 C
7	Bellevue Way NE - NE 8th Street	0.610 B	0.610 B
8	Bellevue Way NE - NE 4th Street	0.779 C	0.774 C
9	Bellevue Way - Main Street	0.778 C	0.798 C
20	108th Ave NE - NE 12th Street	0.539 A	0.545 A
21	108th Ave NE - NE 8th Street	0.748 C	0.746 C
22	108th Ave NE - NE 4th Street	0.594 A	0.571 A
24	108th Ave - Main Street	0.457 A	0.462 A
25	112th Ave NE - NE 12th Street	0.723 C	0.724 C
26	112th Ave NE - NE 8th Street	1.127 F	1.150 F
36	112th Ave - Main Street	0.692 B	0.703 C
72	112th Ave NE - NE 4th Street	0.685 B	0.673 B
Area wide average ->		0.692 B	0.693 B

Area 4: Bel-Red/Northup

INT	ADDRESS	MP6_R8(2)	DEVREV
29	116th Ave NE - NE 12th Street	0.684 B	0.685 B
30	116th Ave NE - NE 8th Street	0.813 D+	0.836 D+
32	120th Ave NE - NE 12th Street	0.557 A	0.542 A
34	124th Ave NE - Bellevue-Redmon	0.637 B	0.636 B
35	124th Ave NE - NE 8th Street	0.714 C	0.715 C
37	130th Ave NE - Bellevue-Redmon	0.586 A	0.590 A
68	130th Ave NE - NE 20th Street	0.592 A	0.593 A
73	116th Ave - Main Street	0.646 B	0.649 B
88	124th Ave NE - Northup Way NE	0.506 A	0.504 A
114	116th Ave NE - Northup Way NE	0.717 C	0.715 C
116	115th Place NE - Northup Way	0.658 B	0.641 B
117	120th Ave NE - NE 20th Street	0.532 A	0.526 A
131	116th Ave SE - SE 1st Street	0.691 B	0.689 B
139	116th Ave NE - NE 4th Street	0.541 A	0.572 A
233	120th Ave NE - NE 8th Street	0.743 C	0.737 C

Area wide average -> 0.641 B 0.642 B

Area 5: Crossroads

INT	ADDRESS	MP6_R8(2)	DEVREV
58	Bellevue-Redmon - NE 20th Street	0.523 A	0.519 A
62	156th Ave NE - Northup Way	0.788 C	0.792 C
63	156th Ave NE - NE 8th Street	0.642 B	0.647 B

Area wide average -> 0.651 B 0.652 B

Area 6: North-East Bellevue

INT	ADDRESS	MP6_R8(2)	DEVREV
75	164th Ave NE - NE 24th Street	0.616 B	0.615 B
76	164th Ave NE - Northup Way	0.539 A	0.539 A
87	164th Ave NE - NE 8th Street	0.739 C	0.742 C
111	Northup Way - NE 8th Street	-----	-----

Area wide average -> 0.631 B 0.632 B

Area 7: South Bellevue

INT	ADDRESS	MP6_R8(2)	DEVREV
14	112th Ave SE - Bellevue Way SE	0.789 C	0.789 C
89	112th Ave SE - SE 8th Street	0.596 A	0.587 A
102	118th Ave SE - SE 8th Street	0.731 C	0.707 C
219	I-405 NB Ramps - SE 8th Street	0.560 A	0.558 A
226	I-405 SB Ramps - SE 8th Street	0.393 A	0.396 A

Area wide average -> 0.614 B 0.607 B

Area 8: Richards Valley

INT	ADDRESS	MP6_R8(2)	DEVREV
43	140th Ave SE - SE 8th Street	0.564 A	0.583 A
44	145th Place SE - Lake Hills Blvd	0.592 A	0.598 A
45	145th Place SE - SE 16th Street	0.684 B	0.687 B
71	Lk Hills Connec - SE 8th St/7th	0.931 E+	0.937 E+
82	Richards Rd - Kamber Rd	0.649 B	0.651 B
85	Richards Rd - SE 32nd Street	0.504 A	0.511 A
134	Richards Rd - Lk Hills Connec	0.448 A	0.456 A
280	139th Ave SE - Kamber Road	0.302 A	0.333 A

Area wide average -> 0.584 A 0.594 A

Area 9: East Bellevue

INT	ADDRESS	MP6_R8(2)	DEVREV
41	140th Ave NE - NE 8th Street	0.757 C	0.748 C
42	140th Ave - Main Street	0.554 A	0.555 A
49	148th Ave NE - NE 8th Street	0.864 D-	0.852 D-
50	148th Ave - Main Street	0.806 D+	0.805 D+
51	148th Ave SE - Lake Hills Blvd	0.847 D+	0.845 D+
52	148th Ave SE - SE 16th Street	0.737 C	0.722 C
55	148th Ave SE - SE 24th Street	0.763 C	0.758 C
65	148th Ave SE - SE 8th Street	0.669 B	0.661 B
83	156th Ave - Main Street	0.652 B	0.666 B

Area wide average -> 0.739 C 0.735 C

Area 10: Eastgate

INT	ADDRESS	MP6_R8(2)	DEVREV
56	148th Ave SE - SE 27th Street	0.537 A	0.541 A
86	156th Ave SE - SE Eastgate Way	0.629 B	0.631 B
92	161st Ave SE - SE Eastgate Way	0.432 A	0.400 A
101	150th Ave SE - SE Eastgate Way	0.840 D+	0.836 D+
171	142nd Ave SE - SE 36th Street	0.729 C	0.719 C
174	150th Ave SE - SE 38th Street	0.662 B	0.662 B
227	150th Ave SE - I-90 EB Off-Ram	0.780 C	0.776 C
272	139th Ave SE - SE Eastgate Way	0.655 B	0.655 B
Area wide average ->		0.658 B	0.653 B

Area 11: Newcastle

INT	ADDRESS	MP6_R8(2)	DEVREV
98	Coal Creek Park - Forest Drive	0.597 A	0.610 B
133	150th Ave SE - SE Newport Way	0.520 A	0.514 A
228	Lakemont Blvd (- SE Newport Way	0.767 C	0.774 C
229	Lakemont Blvd - Forest Drive	-----	-----
242	164th Ave SE - Lakemont Blvd	-----	-----
257	164th Ave SE - SE Newport Way	-----	-----
Area wide average ->		0.628 B	0.632 B

Area 12: Overlake

INT	ADDRESS	MP6_R8(2)	DEVREV
39	140th Ave NE - NE 20th Street	0.756 C	0.755 C
40	140th Ave NE - Bellevue-Redmon	0.723 C	0.720 C
47	148th Ave NE - NE 20th Street	0.846 D+	0.846 D+
48	148th Ave NE - Bellevue-Redmon	0.806 D+	0.807 D+
59	Bellevue-Redmon - NE 24th Street	0.603 B	0.589 A
60	156th Ave NE - Bellevue-Redmon	0.646 B	0.643 B
61	156th Ave NE - NE 24th Street	0.689 B	0.682 B
64	140th Ave NE - NE 24th Street	0.622 B	0.618 B
79	148th Ave NE - NE 40th Street	0.579 A	0.574 A
81	148th Ave NE - NE 24th Street	0.967 E-	0.968 E-
138	Bellevue-Redmon - NE 40th Street	0.617 B	0.600 A
188	148th Ave NE - NE 29th Place	0.879 D-	0.880 D-
189	NE 29th Place - NE 24th Street	0.429 A	0.431 A
239	156th Ave NE - NE 40th Street	0.591 A	0.600 A
249	148th Ave NE - NE 51st Street	0.750 C	0.744 C
250	SR-520 SB Ramps - NE 51st Street	0.253 A	0.259 A
251	SR-520 NB Ramps - NE 51st Street	0.443 A	0.459 A
255	156th Ave NE - NE 51st Street	0.512 A	0.494 A
264	156th Ave NE - NE 31st Street	0.526 A	0.524 A
Area wide average ->		0.644 B	0.642 B

Area 13: Factoria

INT	ADDRESS	MP6_R8(2)	DEVREV
105	Richards Rd - SE Eastgate Way	0.695 B	0.704 C
202	128th Ave SE/Ne - SE Newport Way	0.555 A	0.556 A
203	SE Newport Way - Coal Creek Pkwy	0.605 B	0.613 B
204	128th Ave SE - SE 36th Street	0.806 D+	0.800 C
220	I-405 NB Ramps - Coal Creek Park	0.629 B	0.624 B
221	I-405 SB Ramps - Coal Creek Park	0.859 D-	0.852 D-
222	128th Ave SE - SE 38th Place	0.952 E-	0.957 E-
284	124th Ave SE - Coal Creek Park	0.810 D+	0.815 D+

Area wide average -> 0.739 c 0.740 c