

Bel-Red Subarea Design Guidelines

DRAFT
April 11, 2008

City of Bellevue

ATTACHMENT D

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Introduction

The Bel-Red Subarea Design Guidelines consist of a design review checklist and six guideline chapters:

- Character and site guidelines
- Pedestrian emphasis guidelines
- Architecture guidelines
- Lighting guidelines
- Sign guidelines

Each of the six chapters contains a series of distinct guidelines. Each individual guideline page provides the following detail:

- Intent: An initial concise statement of the objective of the guideline
- Guideline: Explanatory text describing the details of the guideline
- Recommended: Textual and photographic examples of recommended development consistent with the intent of the guideline
- Not Recommended: Textual and photographic examples of development that does not meet the intent of the guideline

Visual examples are included as models for design and review purposes. They are intended to provide a means to effectively judge a building or project relative to the design criteria; they are not intended to be specific examples to be replicated.

The Checklist

The accompanying design guidelines checklist outlines each individual guideline. This tool can be used to assess which guidelines apply, and whether the applicable guideline is being followed.

Why Are Design Guidelines Necessary?

Guidelines give developers and citizens an understanding of the city's expectations and provide consistent criteria by which to review proposed

projects. They ensure a degree of order, harmony and quality within the built environment.

Role of Design Guidelines

The Bel-Red Subarea Design Guidelines support and complement the community vision described in the Bel-Red Subarea Plan that is part of the city's adopted Comprehensive Plan. The Design Guidelines offer a flexible tool for quality and innovation. They do not prescribe specific design solutions or make rigid requirements. There are many ways to meet a particular guideline. The guidelines are a descriptive template for promoting and improving the urban character of the area without dictating or prescribing a specific style or theme.

Design Review Process

The Bel-Red Design Standards and Guidelines will be implemented through the Design Review process (LUC Sec. 20.30.F). This is a land use entitlement process that ensures a development proposal is consistent with the Comprehensive Plan, meets all applicable guidelines or criteria of the Land Use Code and is compatible with and responds to the intended character, appearance, quality of development and physical characteristics of the vicinity.

The review and approval process is made up of three phases.

1. Preapplication Conference: This is a one-time service that allows for early feedback and direction from Development Services staff on a development proposal. This occurs at the schematic design phase when access, site and building design are easily modified.

2. Predevelopment Services: This is an optional step that allows a developer to get on-going feedback and detailed direction from staff on proposal. The

purpose is to resolve issues early and assist in the preparation of a complete formal Design Review application and quality project.

3. Design Review: Design Review is a Land Use review process in which Development Services staff from every city permit authority, including Fire, Utilities, Transportation will review a project for compliance with applicable codes, standards, and guidelines. In most cases the SEPA Determination will be included in this review. This is an administrative decision made by the Director using Process II (LUC 20.35.200).

Character and Site Guidelines

Overview

The Character and Site Guidelines address the qualities that make the Bel-Red Subarea unique. They consider what makes an area a special, distinct "place," not simply a group of individual buildings and streets.

The Character and Site Guidelines are divided into the following elements:

- Integrate the natural environment
- Provide graceful transitions between land uses
- Promote architectural compatibility
- Establish and strengthen gateways
- Protect and enhance surface water resources
- Integrate art

Visual examples are included as models for design and review purposes. They are intended to provide a means to identify recommended character and site elements.

Integrate the Natural Environment

Intent

Reinforce linkages and orient buildings to the Bel-Red Subarea's natural and landscaped features.

Guideline

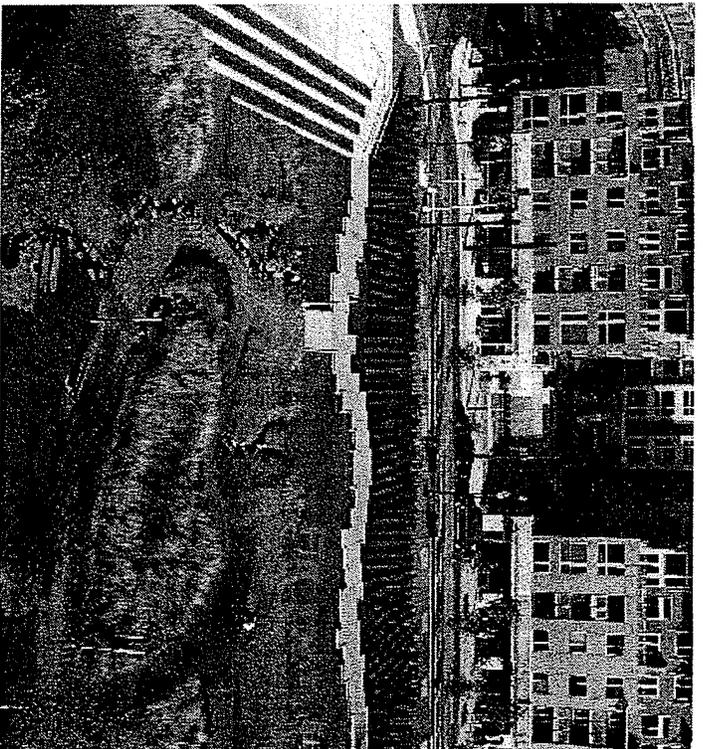
Site and building design should capitalize on significant elements of the natural environment, Highland Community Park and planned park and open space, riparian corridors and wetlands. Designs should incorporate open space amenities for residents, employees and visitors. Depending on the location, this may be accomplished through integration of the natural environment with new development or providing a smooth transition between the natural and built environments.

Recommended

- Active and passive gathering places and walkways oriented toward parks and open, natural spaces
- Clear and convenient public access to open space amenities
- Elements that engage the natural environment where the sight, sound and feel of nature can be directly experienced
- Buildings sited to take maximum advantage of adjacent public amenities
- Walkways and plazas paved with high-quality materials (such as brick or stone), and other architectural elements that use materials, colors and forms that are harmonious with the natural surroundings

Not Recommended

- Buildings that turn their back on open space amenities
- Islands of 'native' planting schemes within large, automobile oriented parking lots



Recommended



Not Recommended

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Promote Architectural Compatibility

Intent

New buildings should contribute to the quality and character of their architectural context.

Description

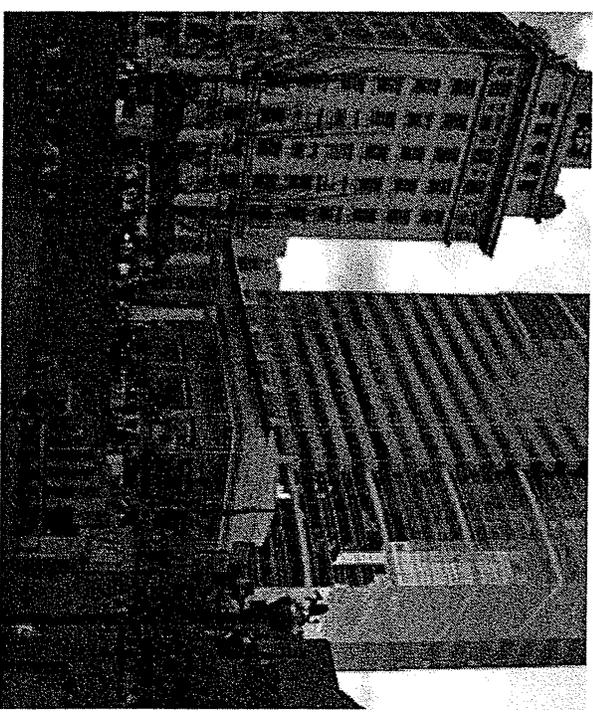
Buildings should “fit” with their architectural surroundings – relating to nearby buildings rather than calling attention to themselves through design excesses or novel variations. Architectural elements should enhance not detract from the areas overall character.

Recommended

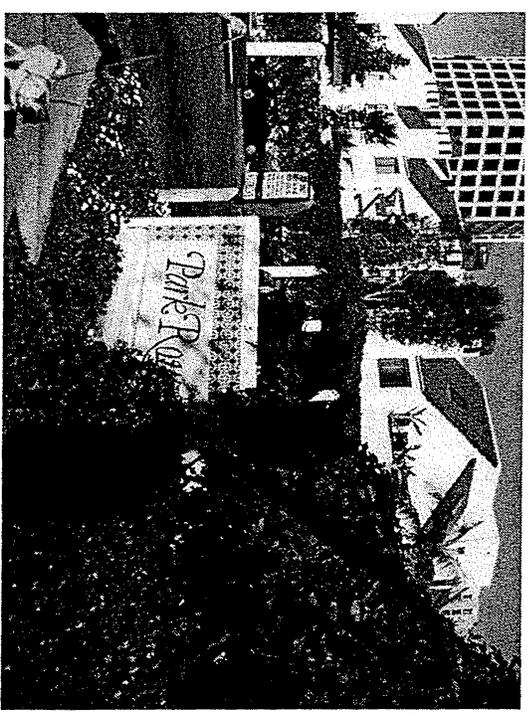
- Architectural elements used at a scale and level of detailing proportionate to the size of the building
- Forms, proportions, rhythms, materials, colors and architectural motifs that are suggested by and complement adjacent buildings

Not Recommended

- Out-of-scale, over-simplified, cartoon-like or other architectural elements applied without regard to size or use of the element
- Building elements that do not respect the scale, materials, proportions and heights of adjacent high-quality buildings



Recommended



Not Recommended

Establish and Strengthen Gateways

Intent

Use architectural and landscape elements to mark transitions and entrances.

Guideline

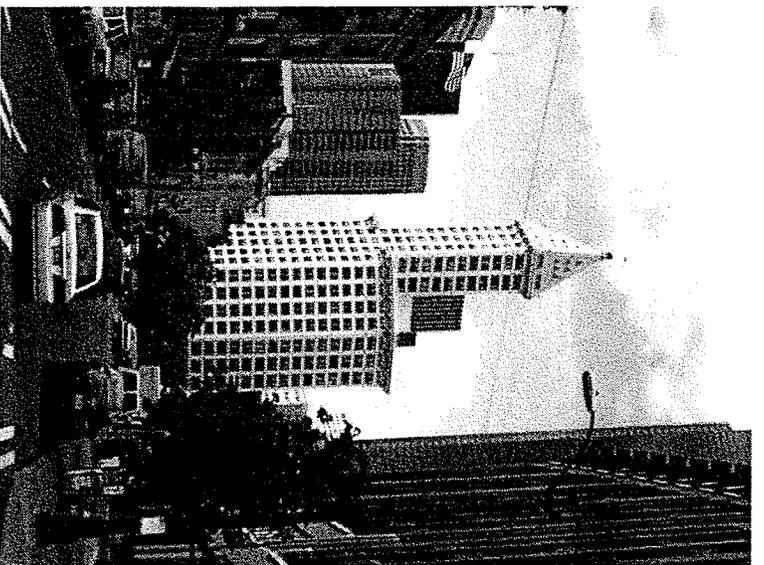
Entrances into and within the Bel-Red Subarea should be celebrated at many levels. Pedestrians, cyclists, transit passengers, and motorists should experience a sense of "entering" or moving into the area as well as entry into unique districts or neighborhoods in the subarea.

Recommended

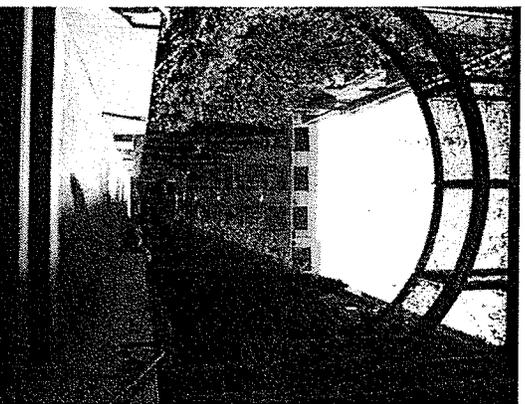
- New buildings designed to create gateways, using elements such as arches, arcades, pylons, columns, fountains and bridges
- Signage, landscaping, lighting, or sculptural and artistic elements used to identify a gateway
- Markers or inlaid art treatment in sidewalk paving to strengthen sense of entry into a particular district or neighborhood
- Design elements that indicate a change or separation in transportation modes (i.e., from auto to pedestrian areas, or into transit stations)

Not Recommended

- Gated, private, residential compounds



Recommended



Not Recommended

Protect and Enhance Surface Water Resources

Intent

Conserve water quality, natural hydrology and habitat, and preserve biodiversity through protection of water bodies and wetlands.

Guideline

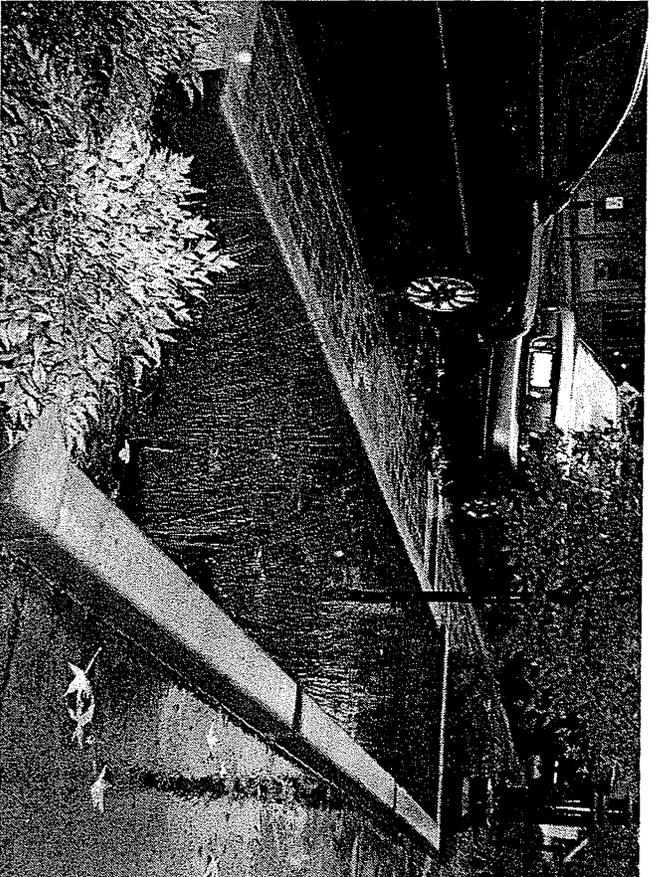
Natural water systems regulate water supply, provide biological habitat and may provide recreational opportunities. Undeveloped ecosystems absorb the precipitation and convey only a small portion of rainfall as surface runoff. New and Infill development should minimize disturbances to the on-site, adjacent, and regional natural water systems.

Recommended

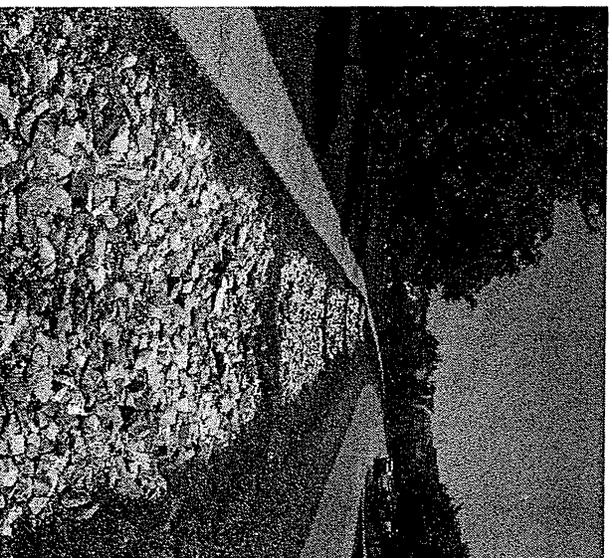
- Grading and plan layout that captures and slows runoff
- Pervious or semi-pervious surfaces that allow water to infiltrate soil
- On-site landscape-based water treatment methods that treat rainwater runoff from all surfaces, including parking lots, roofs and sidewalks

Not Recommended

- New and Infill development within close proximity of wetlands and/or streams
- Buried, piped or culverted stream channels
- Water quality enhancement projects that detract from the urban character of the area



Recommended



Not Recommended

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Integrate Art

Intent

Art in the Bel-Red Subarea should complement the character of a site, building or district as a whole. Art should be integrated into the design of the building or outdoor space.

Guideline

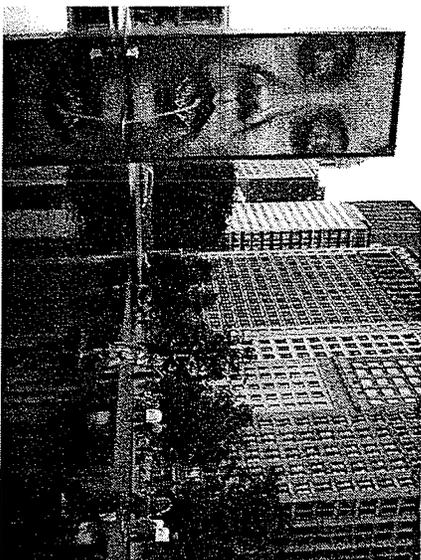
Large scale art in both public and private applications should bring focus to an outdoor space while small scale pieces should bring detail to the pedestrian realm surrounding a building or site. At any scale, art should not overwhelm outdoor spaces or render buildings mere backdrops.

Recommended

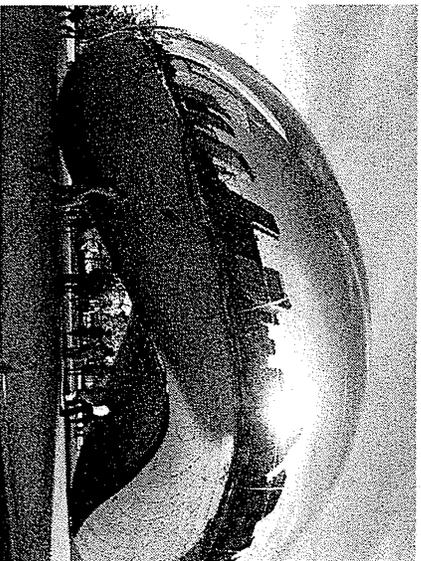
- Artwork designed for and integrated into the building or site
- Functional or interactive artwork
- Durable materials that are vandal-resistant and designed to age well

Not Recommended

- Amateur art projects
- Artwork used as advertising
- Display conditions that detract from the artwork



Recommended



Recommended



Not Recommended



Not Recommended

Pedestrian Emphasis Guidelines

Overview

These guidelines promote an environment where pedestrians are a priority. The highest consideration should be given to the ease and comfort of pedestrian movement and gathering places.

The pedestrian emphasis guidelines fall into the following elements:

- Define the pedestrian environment
- Enhance the pedestrian system
- Protect pedestrians from the elements
- Create a variety of successful outdoor spaces
- Provide places for stopping and viewing

Visual examples are included as models for design and review purposes. They are intended to provide a means to identify recommended character elements.

Define the Pedestrian Environment

Intent

A building should provide a continuous, visually rich pedestrian experience along its ground floor street front.

Guideline

The most important part of a building to a pedestrian is its ground floor – the lowest 15 feet of the facade, which a person experiences walking past or entering the building. This “pedestrian experience zone” should provide a sense of enclosure, and a continuous and comfortable street edge for the pedestrian. Ground floor building transparency should foster interaction between the public and private realms.

Recommended

- Windows that are transparent or have displays at the street level
- Walls that create visual interest by using a variety of forms, colors and compatible cladding materials
- Facades that provide a rhythm by using bays, columns, pilasters or other articulation at the street level
- Signs and lighting at the ground level that complement the human scale

Not Recommended

- Blank, flat, nondescript walls that are not articulated by any visual interest or detail at the street level
- Uniform treatment of entire block face



Recommended



Not Recommended

Enhance the Pedestrian System

Intent

Establish the pedestrian as the priority, eliminating pedestrian barriers and ensuring that walking routes are convenient, direct and pleasant.

Guideline

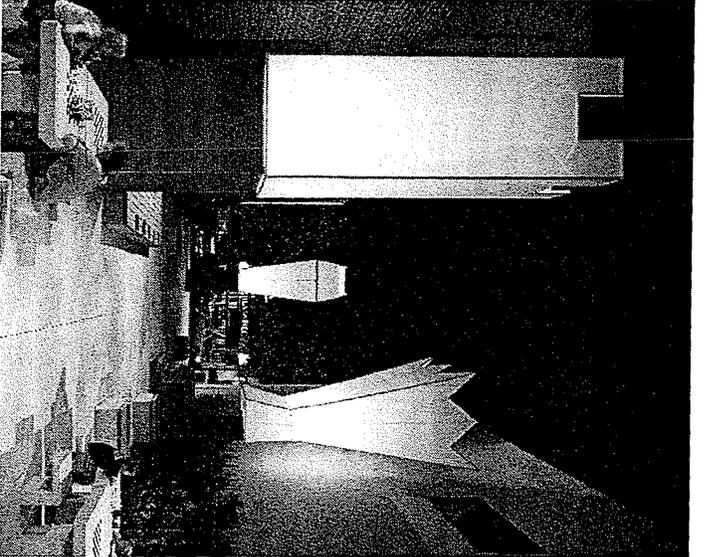
Pedestrian routes should be attractive, easy to use and encourage walking and activity. Sidewalks should be continuous, avoiding interruptions such as vehicle curbs or changes in direction or grade. The portion of the sidewalk dedicated to walking should be free of barriers such as utility poles, newspaper boxes, cafe tables and chairs, permanent planters, tree grates or other obstructions and clutter.

Recommended

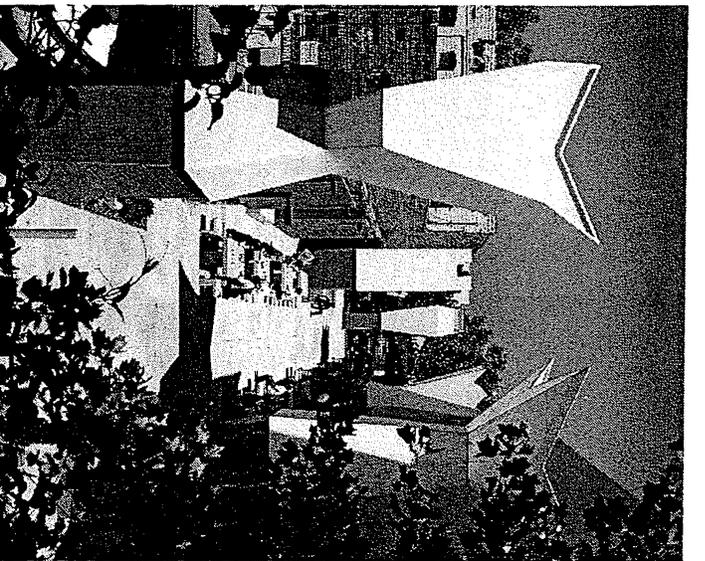
- Develop direct pedestrian routes
- Separate pedestrians from visual and other nuisances (e.g. trash dumpsters, loading docks, mechanical equipment, etc.)
- Maintain pedestrian access where rights-of-way have traditionally been located
- Parking lot walkways

Not Recommended

- Indirect or circuitous pedestrian routes
- Pedestrian-only streets
- Permanent or temporary pedestrian route obstructions
- Interrupted or discontinuous pedestrian routes



Recommended



Recommended



Not Recommended

Protect Pedestrians from the Elements

Intent

Provide pedestrians with protection from wind, sun, rain, sleet and snow.

Guideline

Awnings and canopies are encouraged along the ground floor of buildings to protect pedestrians from rain and snow and provide shade in summer.

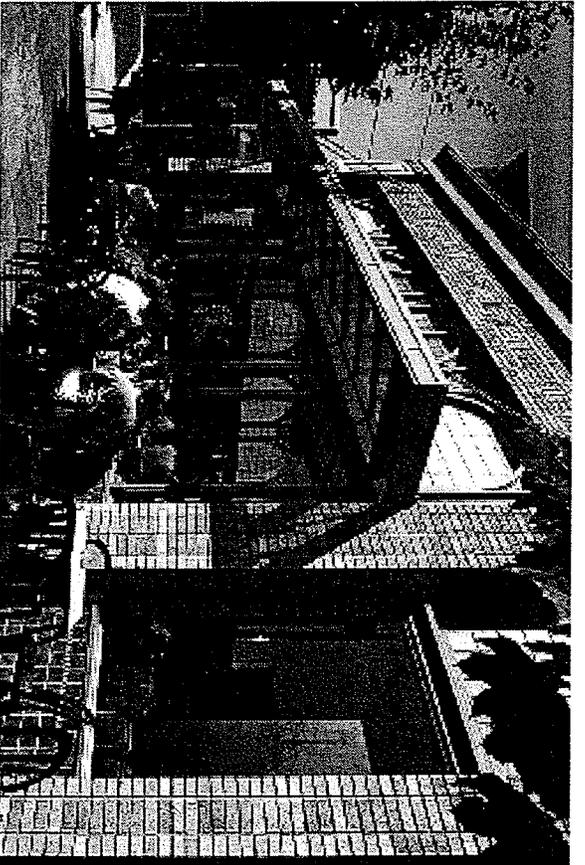
The design of awnings and canopies should be an integral component of the building facade. Awnings should be in proportion to the building and sidewalk, and not so large as to impact street trees, light fixtures or other street furniture.

Recommended

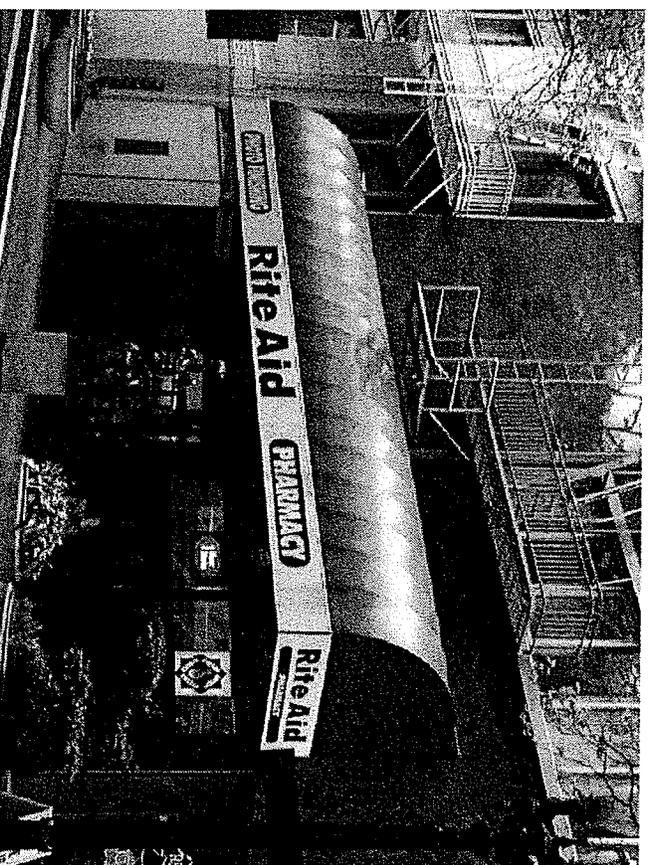
- Canvas fixed or retractable awnings
- Horizontal metal canopies, especially if transom or clerestory windows are above storefront glazing
- Glazed canopies
- Weather protection follows pattern of storefronts

Not Recommended

- Vinyl or other synthetic fabrics
- Backlit awnings
- Oversized advertising or tenant signs on awnings



Recommended



Not Recommended

Create a Variety of Successful Outdoor Spaces

Intent

Provide comfortable and inviting outdoor spaces for a variety of activities during all hours and seasons.

Guideline

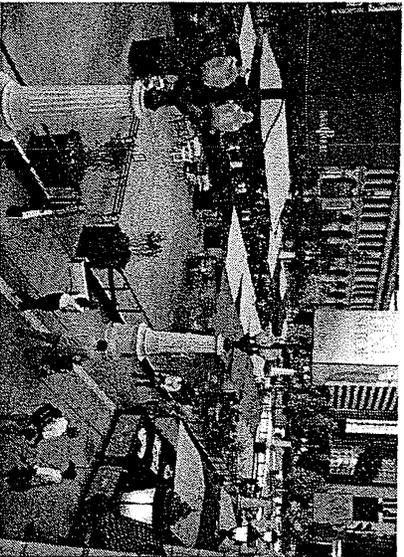
Outdoor gathering spaces should be inviting and maximize opportunities for use. They should be spatially well defined, inviting, secure, easy to maintain. They may be intimate and quiet or active and boisterous. All areas should work well for pedestrians and provide space for special events as well as passive activities.

Recommended

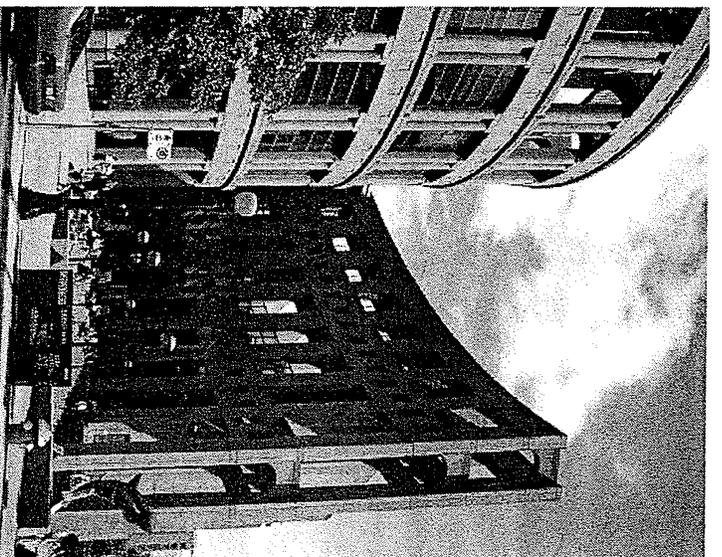
- Courtyards, squares and plazas with active adjacent ground floor uses
- Buildings surrounding green spaces to give the space visual definition and vitality generated by active ground floor uses
- Trees, shrubs and plants to help define walkways, create Recommended transitions from the park to the street and provide visual interest
- Structures, pavilions and seating areas that are easily accessible, and feel safe and secure during day and evening hours
- Greenways or pedestrian walkways and courtyards in residential or office development areas

Not Recommended

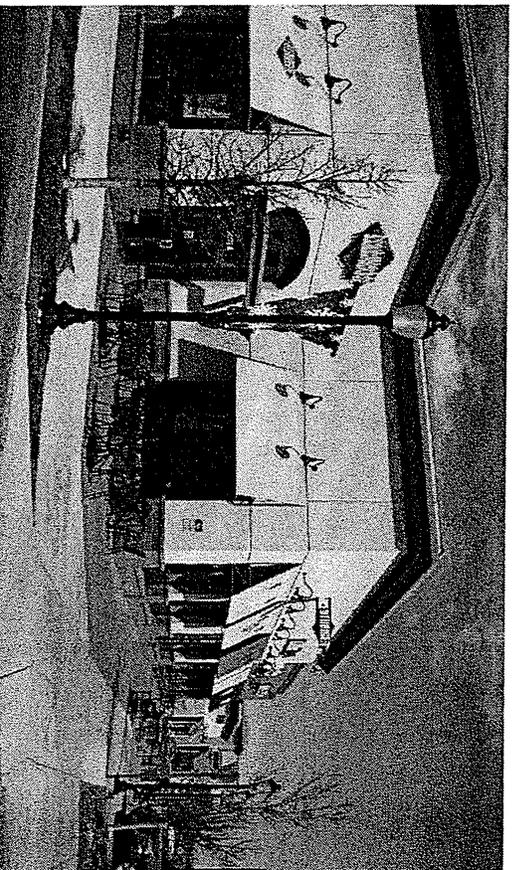
- Pocket parks, forecourts and plazas without active enclosing uses along retail streets
- Outdoor spaces separated from the street by visual barriers or change of grade
- "Leftover" green spaces, busy streets, parking lots or other adjacent inhospitable areas
- Sunken plazas at the edge of the street



Recommended



Recommended



Not Recommended

Provide Places for Stopping and Viewing

Intent

Provide comfortable and inviting places where people can stop to sit, rest and visit.

Guideline

People watching, socializing and eating are restful and pleasurable activities for the pedestrian; providing special places where they can do these activities increases the pedestrian's sense of enjoyment.

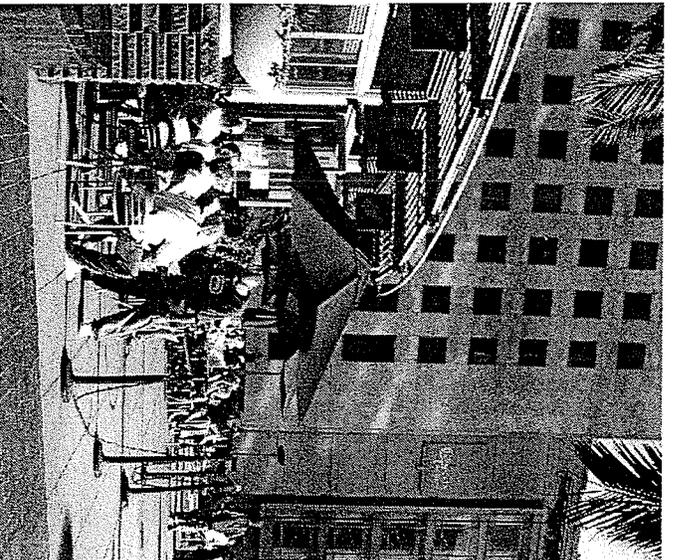
Seating and resting places can add vitality to the urban environment. People will use available seating in open, well-designed areas, not in secluded or awkward spots.

Recommended

- Formal (benches) and informal (e.g. wide steps, edges of landscaped planters and low walls) seating areas
- Higher degree of seating areas near active retail establishments (esp. outside eating and drinking establishments and near food vendors)
- Seating located directly off of pedestrian walkway
- Places for stopping and viewing adjacent to and within parks, squares, plazas and courtyards

Not Recommended

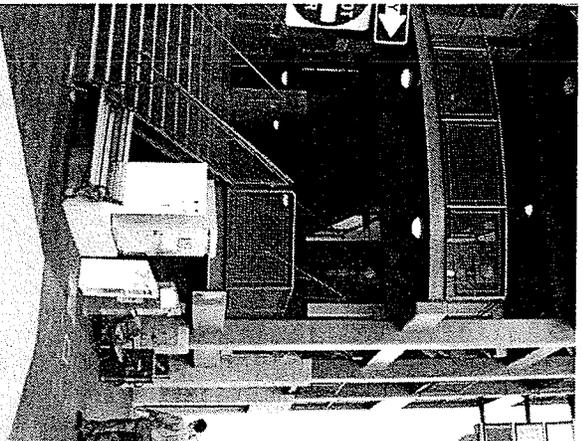
- Seating areas more than three feet above or below street grade
- Seating areas adjacent to loading, service bays or storage areas
- Seating areas that are hidden, secluded, dark or unsecured spaces behind or to the side of buildings
- Seating areas along busy or noisy streets



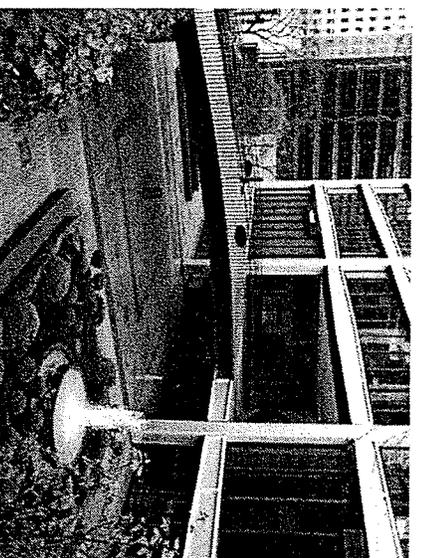
Recommended



Recommended



Not Recommended



Not Recommended

Architectural Guidelines

Overview

The architecture guidelines promote high quality development while reinforcing the area's sense of place and Northwest provenance by encouraging innovative design, construction techniques and materials that reflect the industrial roots of the area while emphasizing the emerging urban character of Bel-Red. The architecture guidelines fall into the following elements:

- Encourage high quality materials
- Provide interesting building massing
- Create attractive building silhouettes and rooflines
- Foster attractive rooftops
- Promote welcoming residential entries
- Promote visually interesting upper floor residential windows
- Design inviting retail and commercial entries
- Encourage retail corner entries
- Encourage inviting ground floor retail and commercial windows
- Build compatible parking structures

Visual examples are included as models for design and review purposes. They are intended to provide a means to identify recommended character elements.

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Encourage High Quality Materials

Intent

Create a sense of permanence in the Bel-Red Subarea through the use of high quality building materials.

Guideline

Quality wall materials can provide a sense of permanence and bring life and warmth to a neighborhood. Wall and building materials must enhance the street environment while maintaining compatibility with adjacent buildings. Articulation of wall materials should be bold, with materials that show depth, quality and durability. It should be apparent that the materials have substance and mass, and are not artificial, thin "stage sets" applied only to the building's surface.

Recommended

- Natural materials such as wood or masonry, or other high quality materials such as brick, finished concrete, stone, terracotta and cement stucco
- Natural or subdued building colors and limited use of bright accent trims
- Varied, yet compatible cladding materials
- Boldly articulated window and storefront trim

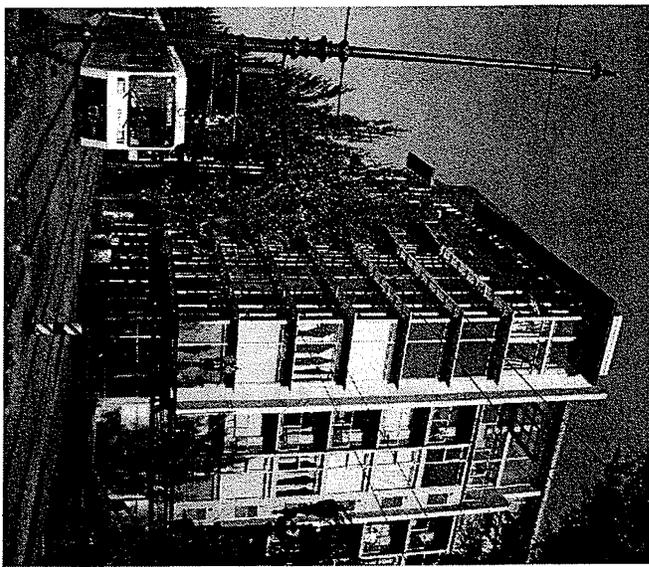
Not Recommended

Building materials such as:

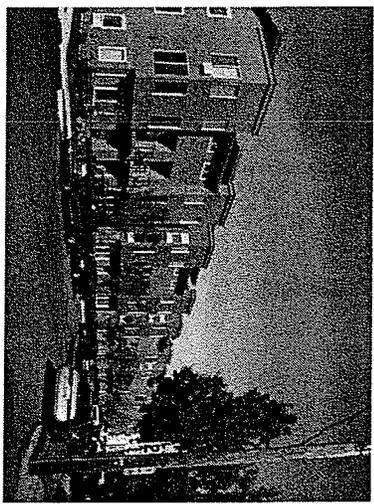
- Flagstone, simulated river rock or other similar veneer cladding
- Synthetic stucco (EIFS)
- Rough-sawn, simulated wood siding, wood veneer, clapboard or other types of residential siding
- Aluminum, plastic or vinyl siding
- Thin brick
- Cementitious lap or shingle siding
- Corrugated metal siding
- Concrete or cinder block

External wall treatments such as:

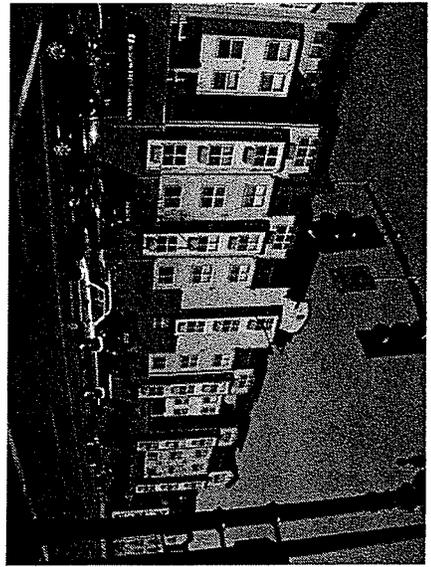
- Exposed metal storefront window systems
- Architectural foam detailing
- Applied simulated divided lite window systems



Recommended



Not Recommended



Not Recommended

119

Provide Interesting Building Massing

Intent

Use scale-defining devices to break up the longitudinal dimensions of buildings, creating a comfortable sense of enclosure by establishing an uninterrupted street edge.

Guideline

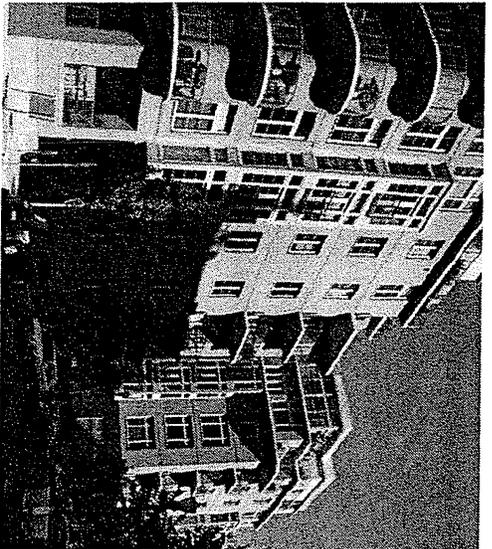
The length and breadth of a building should be friendly in scale and inviting to the pedestrian. Portions of a large building mass should be broken into smaller, appropriately scaled modules, with changes in plane indicated by bold projections and indentations. This allows an overly large building to appear as smaller, side-by-side buildings. Vertical and horizontal elements should be used to create a human scale and form a coherent pattern providing visual interest to the pedestrian.

Recommended

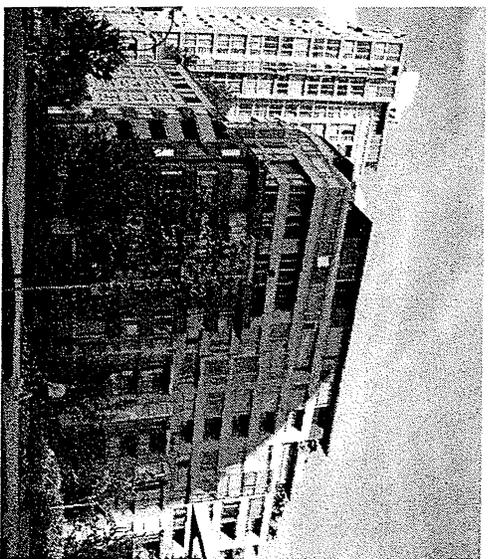
- Break down long expanses of building frontage both horizontally and vertically.
- A vertically articulated tripartite facade division – base, middle and top for buildings over three stories.
- Upper floors set back.
- Vertical articulation of windows, columns and bays.

Not Recommended

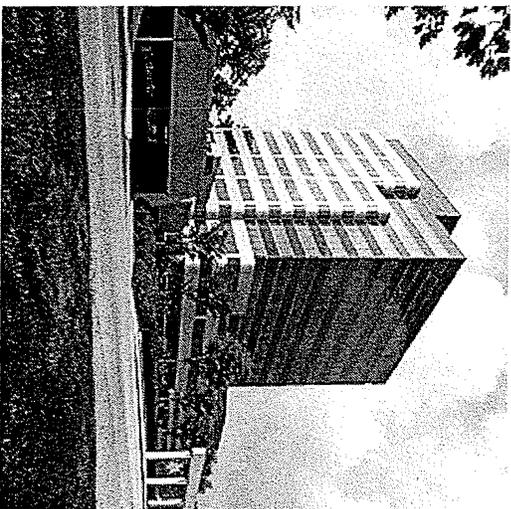
- Long unbroken volume along street facade.
- Smooth, undifferentiated facade.
- Suburban-styled horizontal orientation of building elements – walls, doors and windows.



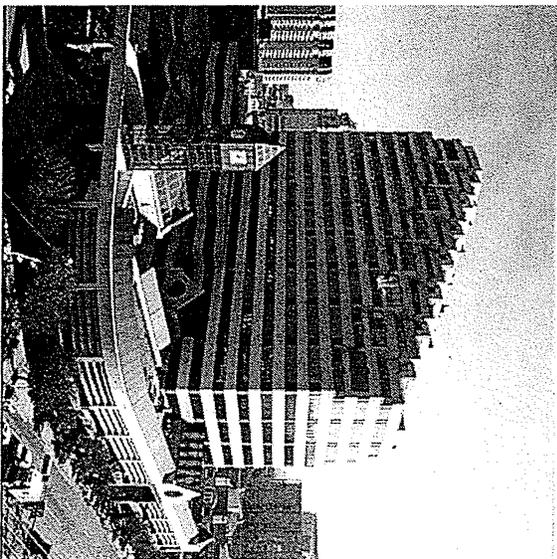
Recommended



Recommended



Not Recommended



Not Recommended

Create Attractive Building Silhouettes and Rooflines

Intent

Building rooflines should enliven the pedestrian experience and provide visual interest with details that create forms and shadows.

Guideline

A building's silhouette should be compatible with the intended character of the area and enhance the streetscape. In some cases, it may be appropriate to mark an entryway with a distinct form, such as a tower, to emphasize the significance of the building entry. Roof massing should be simple yet detailed and articulated. For example, flat roofs may be appropriate if they have a cornice designed with depth and detail expressing the top of the building wall. Dormers set into sloped roofs may be appropriate. These forms provide visual interest and bring additional living space, light and ventilation to upper floor and attic spaces.

Recommended

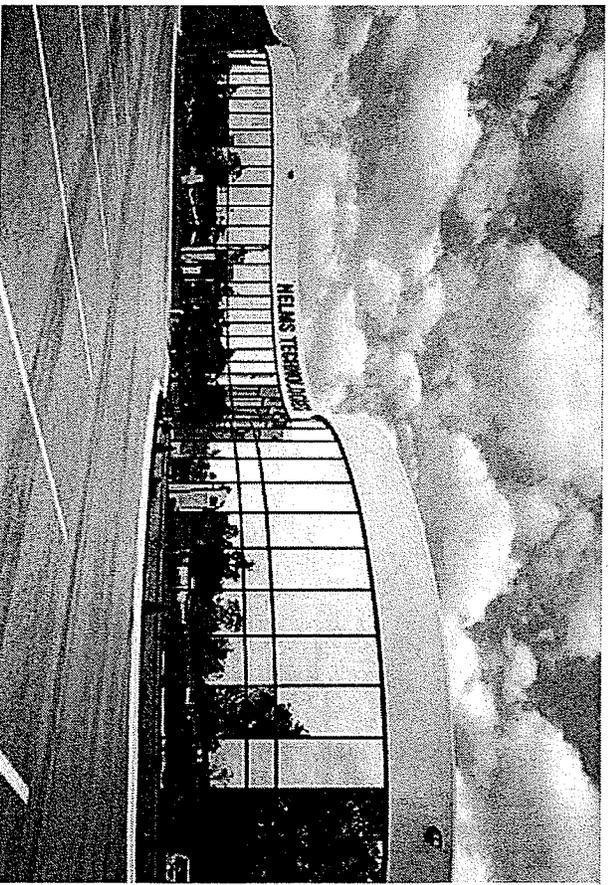
- Dormer windows
- Towers or similar vertical architectural expressions of important building functions such as entries
- Varied roof line heights
- Well-detailed cornices that have significant proportions (height and depth) and create visual interest and shadow lines

Not Recommended

- Unarticulated rooflines
- Poorly detailed decorative roof forms



Recommended



Not Recommended

Foster Attractive Rooftops

Intent

Integrate rooftop elements into the building design.

Guideline

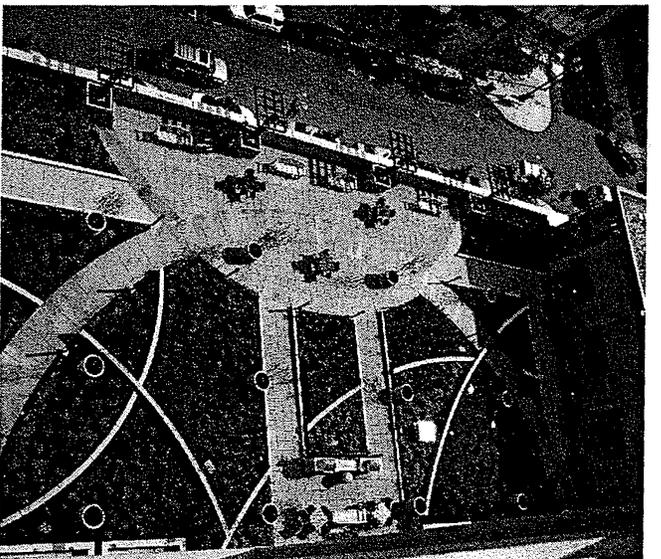
Roof shape, surface materials, colors, and penthouse functions should all be integrated into the overall building design. LUC 20.20.525 provides guidance for rooftop mechanical equipment.

Recommended

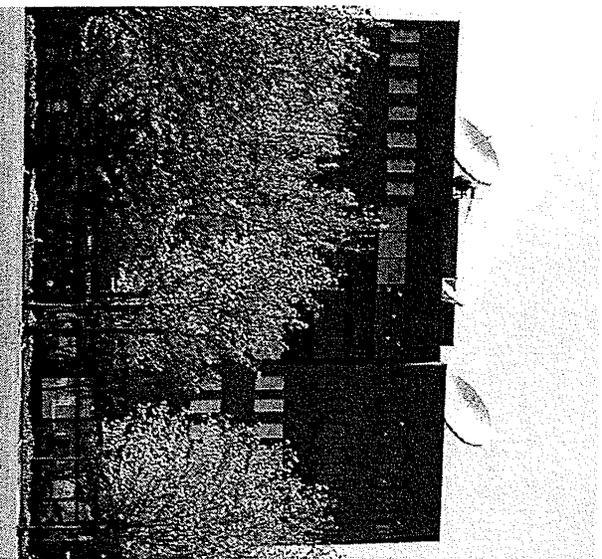
- Rooftop penthouse occupied residential or office spaces
- Rooftop terraces and gardens
- Green roofs that reduce stormwater runoff
- Consolidated and screened mechanical units

Not Recommended

- Exposed rooftop mechanical or electrical units
- Exposed telecommunications equipment, including satellite dishes, cell-phone towers or antennae



Recommended



Not Recommended

Promote Welcoming Residential Entries

Intent

Residential entries should provide a graceful transition between the public and private realms.

Guideline

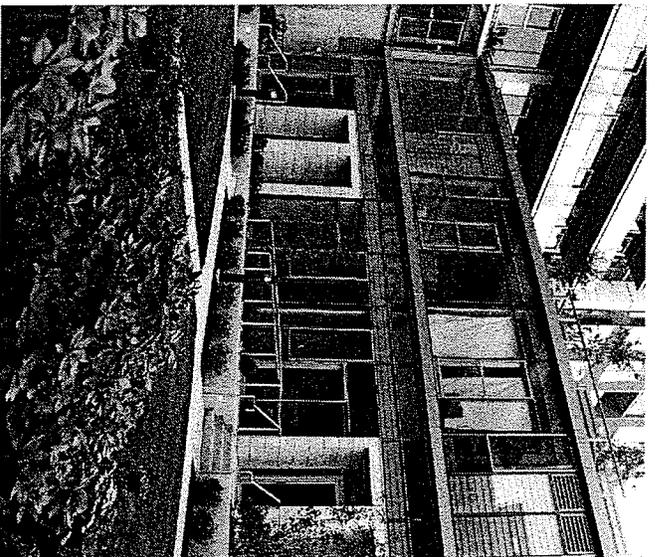
Residential entries should be substantial enough to suggest privacy yet welcoming to those who approach and enter. The overall character of the entry treatments will vary depending on street type. Entries on active edge streets will have a higher degree of transparency, orientation towards the street, and design detail than other portions of the subarea.

Recommended

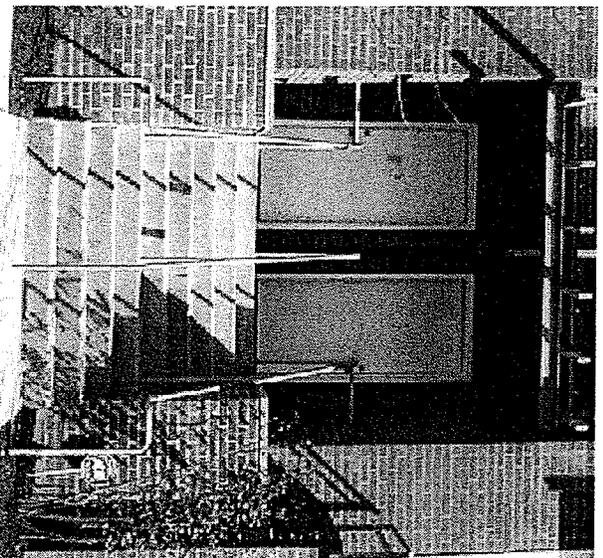
- Multi-panel painted doors
- Doors combined with transom windows or side lites
- Durable, high-quality metal door hardware
- Wood solid core doors
- Doors accessed from canopy-covered entries
- Moderate change of grade from sidewalk level to entry; more for townhouse-style housing than residential lobbies
- Lobby entries to multi-family buildings providing double or multiple doors

Not Recommended

- Doors raised more than three feet above sidewalk level for townhouse-style housing
- Unarticulated, flush doors or sliding glass doors
- Doors accessed directly from parking lots
- Door glazing with simulated divided lites
- Doors glazed with reflective or tinted glazing



Recommended



Not Recommended

Promote Visually Interesting Upper Floor Residential Windows

Intent

Upper floor residential windows should create an open and inviting atmosphere that add visual interest and enhances the experience of the building both inside and out.

Guideline

The windows of a residential building should be pleasing and coherent. Their size and detailing should be of a human scale with regular spacing and a rhythm of similarly shaped windows.

Recommended

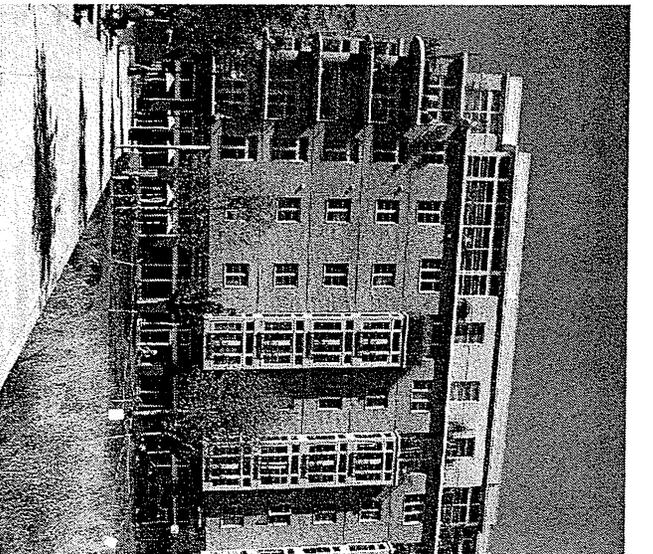
- Multiple lites or divisions
- Operable windows
- Trim around framed openings
- "Punched" windows recessed from building facade, not flush

Not Recommended

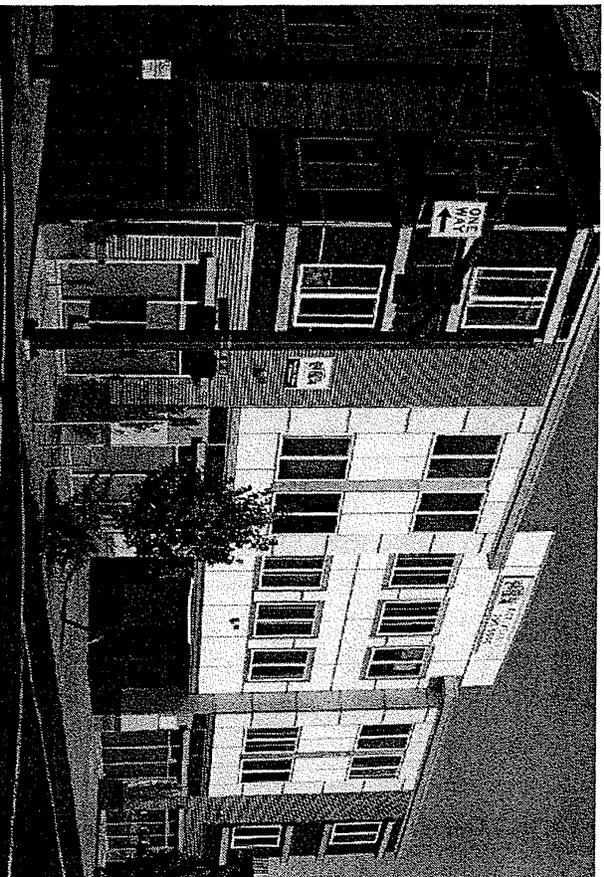
- "Strip" windows with more of a horizontal than vertical emphasis
- Windows flush with building facade
- New or replacement vinyl frames and sashes



Recommended



Recommended



Not Recommended

Design Inviting Retail and Commercial Entries

Intent

Design retail and commercial entries to create an open atmosphere that draws customers inside.

Guideline

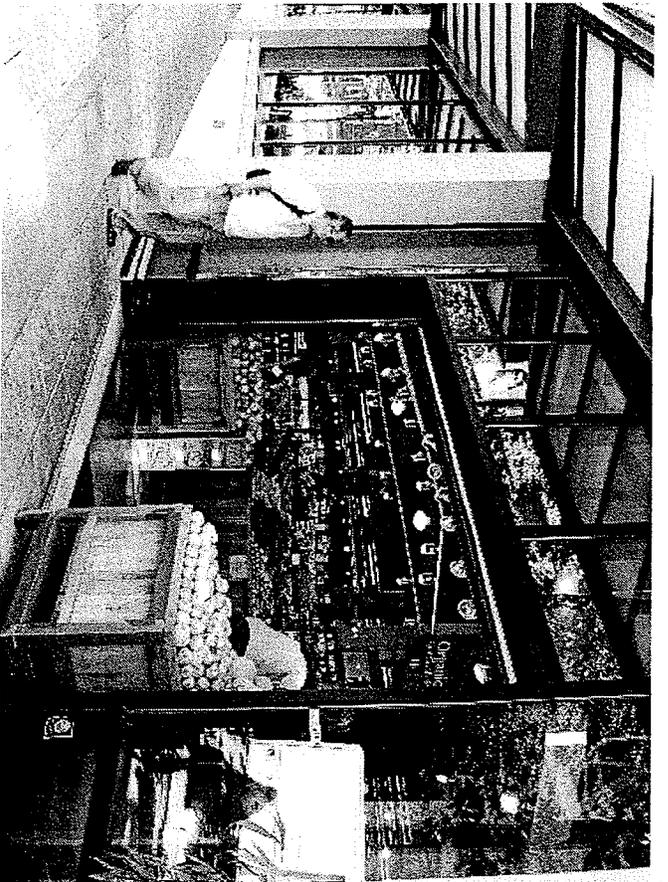
Primary entries to retail and commercial establishments should be transparent, allowing passersby to see the activity within the building and bring life and vitality to the street. Architectural detail should be used to help emphasize the building entry.

Recommended

- Doors with a minimum of 50% window area
- Building lighting that emphasizes entrances
- Transom, side lites or other window combinations
- Doors combined with special architectural detailing
- Double or multiple door entries
- Well-detailed or ornate door hardware
- Large cafe or restaurant doors that open the street to the interior by pivoting, sliding or rolling up overhead

Not Recommended

- Primary entry doors that are solid or windowless
- Primary entry doors raised more than three feet above sidewalk level
- Doors flush with the building facade
- Clear anodized aluminum frames
- Glazing with simulated divisions
- Reflective, opaque or tinted glazing
- Visual and physical obstructions near the entry



Recommended



Not Recommended

Encourage Retail Corner Entries

Intent

Use corner entries to reinforce intersections as important places for pedestrian interaction and activity.

Guideline

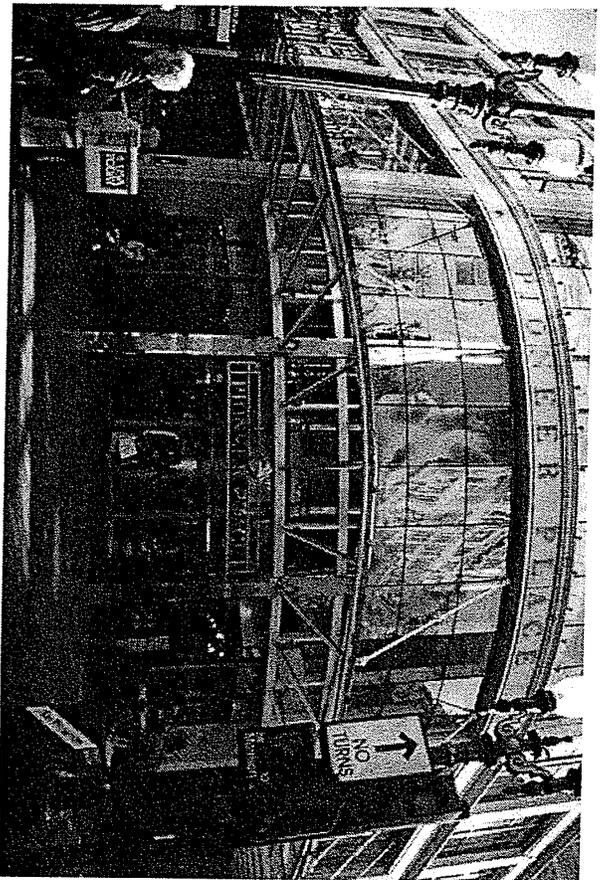
Locate entry doors on the corners of retail buildings wherever possible. Entries at 45 degree angles and free of visual obstructions are encouraged.

Recommended

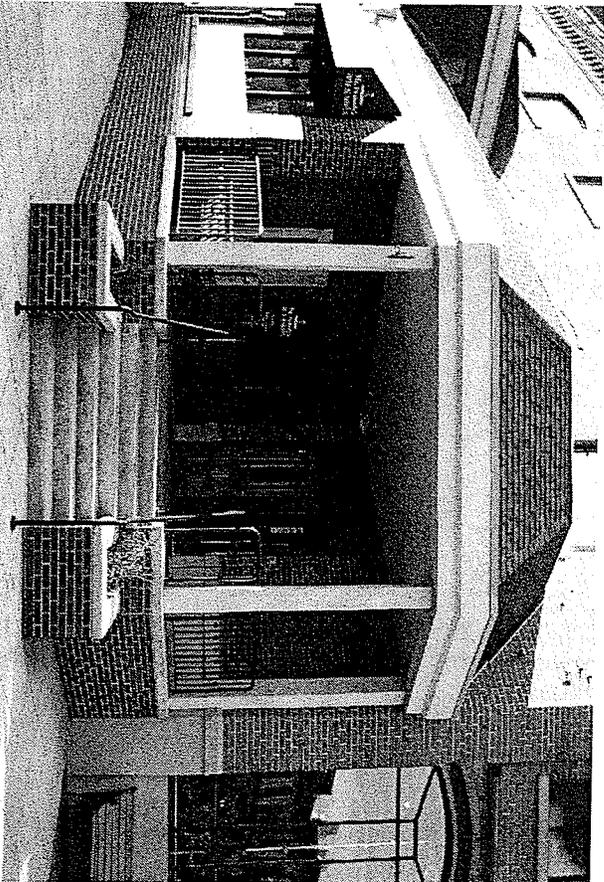
- Primary building entrance located at corner
- Weather protection, special paving, and building wall lighting, to emphasize corner entry
- Architectural detailing with materials, colors, and finishes that emphasize the corner entry
- Doors with large glass areas with adjacent windows

Not Recommended

- Visual and physical obstructions, especially utilities and columns



Recommended



Not Recommended

126

Encourage Inviting Ground Floor Retail and Commercial Windows

Intent

Use transparency to enhance visual interest and to draw people into retail and commercial uses.

Guideline

Retail and commercial uses should use unobstructed windows that add activity and variety at the street level, inviting pedestrians into retail and commercial uses and providing views both in and out.

Recommended

- Clear window glazing
- Operable windows that open by pivoting, sliding or shuttering for restaurants and cafes
- Painted wood, metal, and tile or stone clad panels below windows
- Transom windows

Not Recommended

- Clear or light bronze anodized aluminum windows
- White painted aluminum framed windows
- Residential-styled bays, multi-paned divided lites, half-round or other similar forms
- Tinted or reflective glazing
- Six-foot maximum between mullions
- Small scale windows



Recommended



Recommended



Not Recommended



Not Recommended

Build Compatible Parking Structures

Intent

Use design elements so enhance the compatibility of parking structures with the urban streetscape.

Guideline

Parking structures should be designed so that their streetscape interface has a consistent form, massing and use of materials with the vision for the area.

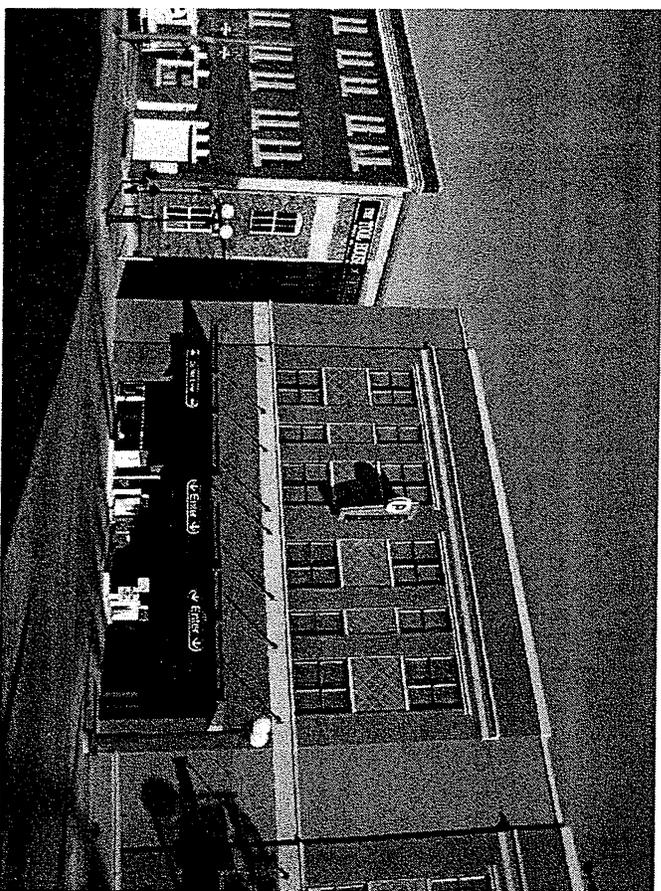
Any sidewalk facing parking garage frontages should appear be designed to appear like any other occupied buildings in the area. The horizontal garage form can be broken down by adding more wall surface and usable retail space, while retaining adequate garage ventilation.

Recommended

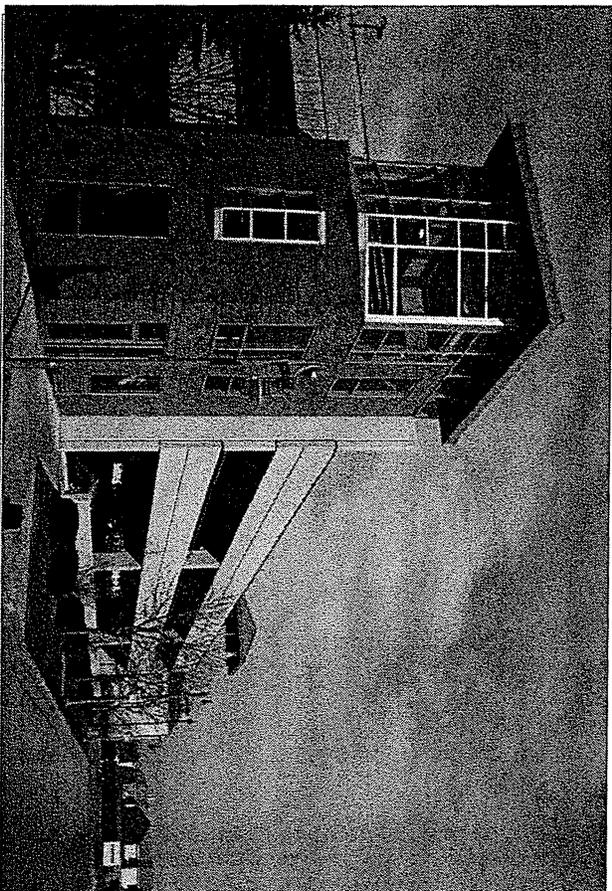
- Ground floor retail or other active uses
- Small openings that may be glazed to function as windows
- Stairways, elevators and parking entries and exits that occur at mid-block
- Single auto exit/entry control point to minimize driveways
- Vertical expression of building structure
- Cladding to disguise sloped floors from the outside view

Not Recommended

- Parked cars on the ground floor that are visible from adjacent sidewalks
- Exposed sloped floors visible from the street
- Stairways, elevators and parking entries and exits occurring at the building's corners, where retail is a more appropriate use
- Horizontal expression of building structure



Recommended



Not Recommended

Lighting Guidelines

Overview

The lighting of buildings and open spaces should provide security, and also contribute to the character and overall sense of and vitality of the area. The lighting guidelines include the following elements:

- Orient lighting towards sidewalks and public spaces
- Integrate building lighting

Visual examples are included as models for design and review purposes. They are intended to provide a means to identify recommended character elements.

129

Orient Lighting Toward Sidewalks and Public Spaces

Intent

Pedestrian-scaled lighting should be used to highlight sidewalks, street trees and other features, and harmonize with other visual elements in the subarea.

Guideline

Pedestrian-scaled lighting should be provided along pedestrian walkways and public open spaces. A single fixture type should be used throughout an area with slight variations allowed to identify smaller districts. Fixtures should be visually quiet as to not overpower or dominate the streetscape.

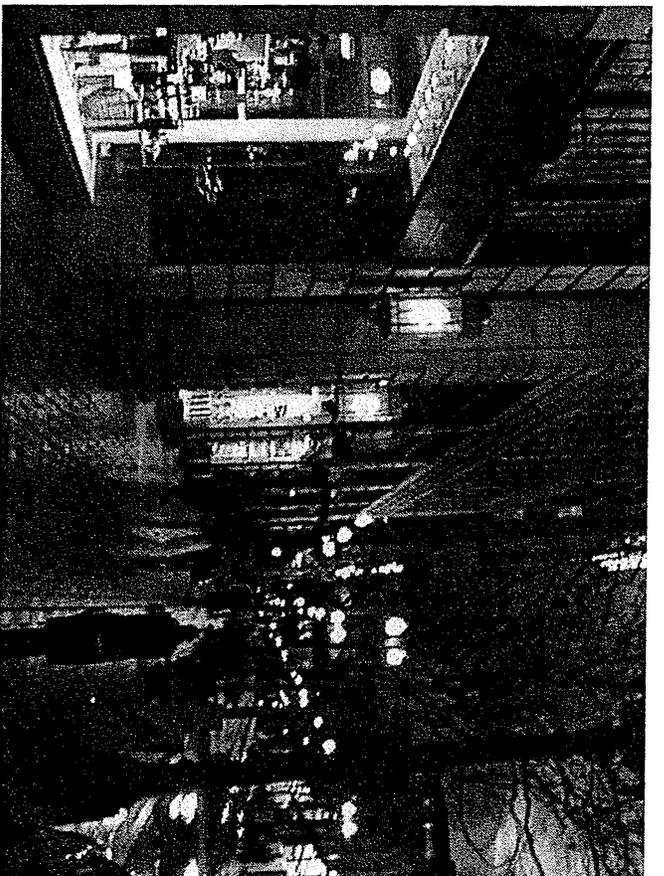
Lighting may also be used to highlight trees and similar features within public and private plazas, courtyards, walkways and other similar outdoor areas to create an inviting and safe ambiance.

Recommended

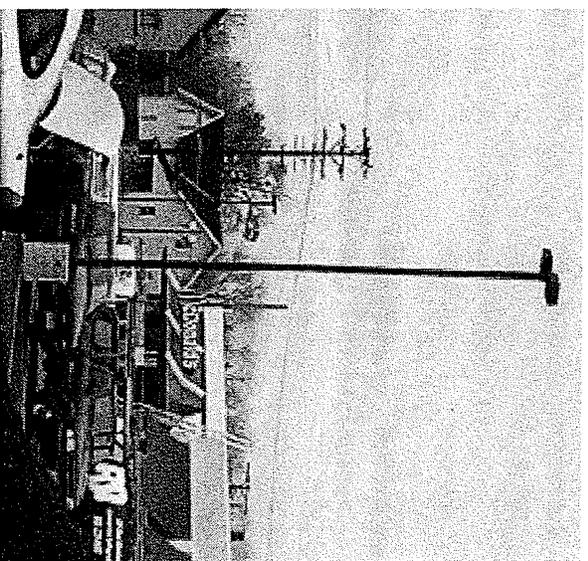
- Lighting of district design
- Pole standards black or dark green in color
- Dual-purpose standards (that accommodate pedestrian and vehicular fixtures)
- Standards accommodating banners and hanging flower pots
- Lighting to highlight landscape areas
- Fixtures concealed and integrated into the design of buildings or landscape walls and stairways
- Footlighting that illuminates walkways and stairs
- Energy efficient lighting
- Bollard lighting that is directed downward toward walking surfaces
- Festive lighting along signature streets on buildings and trees

Not Recommended

- Flashing or colored lights
- Exposed cords, outlets or other electrical devices that may provide safety hazards and are unsightly
- Exposed light source (bulbs should not be visible)
- Low pressure sodium lamps



Recommended



Not Recommended

Integrate Building Lighting

Intent

Architectural lighting that enhances and helps articulate building design, including illumination of cornices and entries, uplighting and other effects.

Guideline

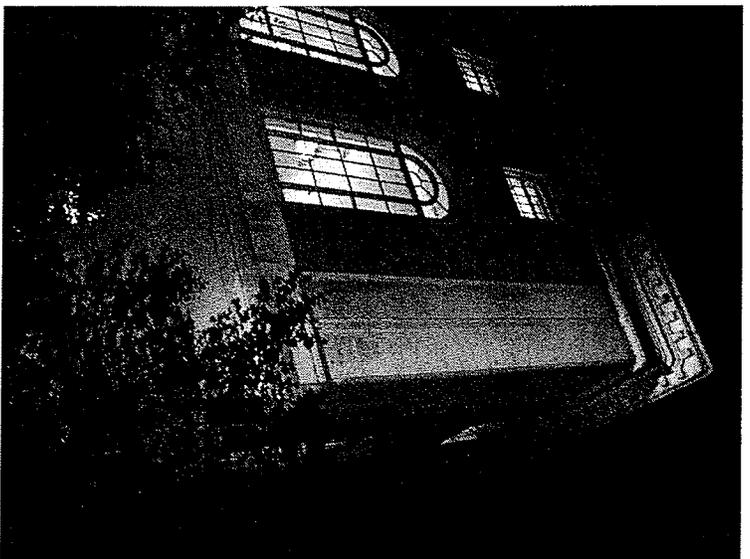
Exterior lighting of buildings should be an integral component of the facade composition. Lighting should be used to create effects of shadow, relief and outline that add visual interest and highlight aspects of the building. Lighting should not cast glare into residential units or onto adjacent lots or streets. In any way that decreases the safety of pedestrians and vehicles.

Recommended

- Wall-washing lighting fixtures
- Decorative wall sconce and similar architectural lighting fixtures
- Screened uplight fixtures on buildings or integrated with landscape
- Lighting that provides natural color

Not Recommended

- Flashing or colored lights
- Exposed light source (bulbs should not be visible)
- Fluorescent tube lighting
- Security spotlight
- Low pressure sodium lamps



Recommended



Not Recommended

Sign Guidelines

Overview

Signs may provide an address, identify a place of business, locate residential buildings or generally offer directions and information. Regardless of their function, signs should be architecturally compatible with and contribute to the character of the subarea. Signs should be good neighbors – they should not compete with each other or dominate the setting due to inconsistent height, size, shape, number, color, lighting or movement. Signs can contribute significantly to a positive retail and pedestrian environment, improve public safety perceptions and reinforce a sense of place.

The sign guidelines include the following elements:

- Consider size and placement of wall signs
- Orient hanging signs to pedestrians

Visual examples are included as models for design and review purposes. They are intended to provide a means to identify recommended character elements.

132

Consider Size and Placement of Wall Signs

Intent

Signs that are sized and placed so that they are compatible with a building's architectural design and contribute to the character of the subarea.

Guideline

Signs should not overwhelm the building or its special architectural features. Signs should not render the building a mere backdrop for advertising or building identification. Signs should be good neighbors – they should not compete with each other or dominate the setting due to inconsistent height, size, shape, number, color, lighting or movement.

Recommended

- Signs incorporated into the building architecture as embossing, low relief casting or application to wall surfaces
- Signs constructed of individual, three-dimensional letters, as opposed to one single box with cutout flat letters
- Signs may be painted or made with applied metal lettering and graphics.
- Signs made of durable and long lasting materials
- Signs incorporating lighting as part of their design
- Signs located above storefronts, on columns or on walls flanking doorways

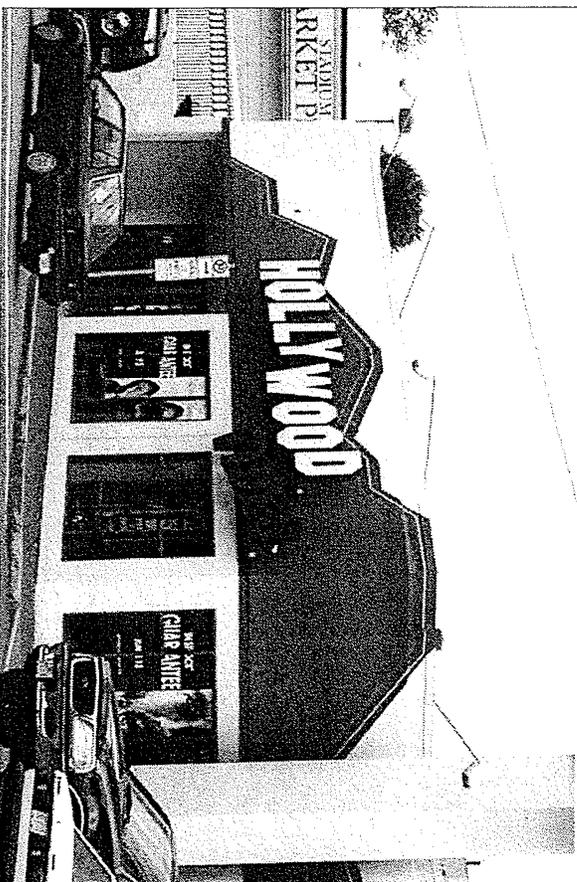
Not Recommended

The material, size and shape of signs that overwhelm, contrast greatly or adversely impact the architectural quality of the building

- Roof-mounted signs
- Cabinet or bow signs
- Backlit signs
- LED animated signs
- Video signs
- Painted window signs
- Electronic billboards



Recommended



Not Recommended

Orient Hanging Signs to Pedestrians

Intent

Hanging signs that are oriented to the pedestrian and highly visible from the sidewalk. Hanging signs can contribute significantly to a positive retail and pedestrian environment and reinforce a sense of place.

Guidelines

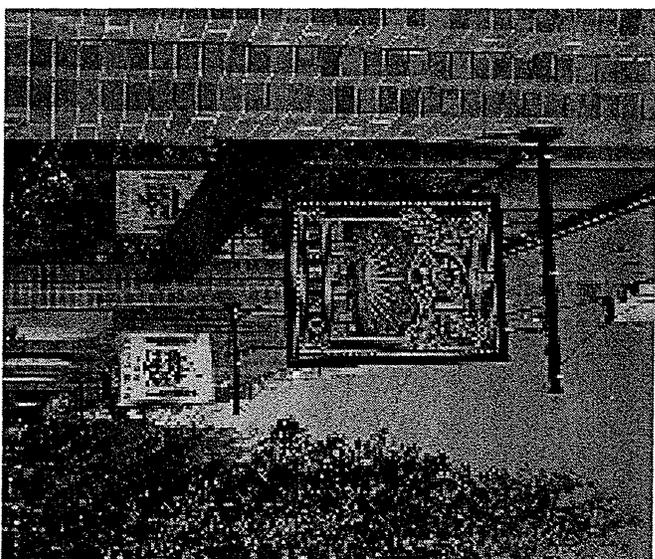
Signs should not overwhelm the streetscape. They should be compatible with and complement the building's architecture, including its awnings, canopies, lighting and street furniture.

Recommended

- Sign lighting that is integrated into the facade of the building
- Signs constructed of high-quality materials and finishes
- Signs attached to the building in a durable fashion

Not Recommended

- Signs interfering with sight lines, creating a safety hazard or obstructing views



Recommended

Appendix

Bel-Red Corridor Design Guidelines
DESIGN GUIDELINES CHECKLIST

Project and Applicant Name: _____

Zoning: _____ Other: _____

Building Use: _____ Submission Date: _____

DESIGN GUIDELINES

	Applies		Complies	
	Yes	No	Yes	No
1. Character and Site				
Integrate the Natural Environment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Promote Architectural Compatibility	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Establish and Strengthen Gateways	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Protect and Enhance Surface Water Resources	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Integrate Art	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Pedestrian Emphasis				
Define the Pedestrian Environment				
Enhance the Pedestrian System	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Protect Pedestrians from the Elements	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Create Successful Outdoor Spaces	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Provide Places for Stopping and Viewing	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Architecture				
Encourage High Quality Materials				
Provide Interesting Building Massing	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Create Attractive Building Silhouettes and Rooflines	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Foster Attractive Rooftops	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Promote Welcoming Residential Entries	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Promote Visually Interesting Upper Floor Windows	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Design Inviting Retail and Commercial Entries	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Encourage Retail Corner Entries	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Encourage Inviting Ground Floor Retail and Commercial Windows	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Build Compatible Parking Structures	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Lighting Guidelines				
Orient Lighting Toward Sidewalks				
Integrate Building Lighting	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Sign Guidelines				
Consider Size and Placement of Wall Signs				
Orient Hanging Signs to Pedestrians	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Bel-Red Subarea
Design standards

DRAFT April 18, 2008

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How to Use this Document

The Bel-Red Subarea Design Standards are a tool that will be used to help implement the vision and policy direction contained in the city's Comprehensive Plan for the Bel-Red area. The urban design goal articulated in the Bel-Red Subarea Plan is to achieve a design character that results in aesthetically pleasing, distinctive, and long-lasting places that evoke a strong sense of Bellevue and the Northwest, and a dynamic public realm that encourages social interaction.

These Design Standards are divided into two sections:

- Development Standards regulate physical orientation, uses and arrangement of buildings as well as site and circulation improvements
- Street Standards regulate the location and circulation of vehicular, transit, and non-motorized modes

Review Process

The Bel-Red Design Standards and Guidelines will be implemented through the Design Review process (LUC Sec. 20.30.F). This is a land use entitlement process that ensures a development proposal is consistent with the Comprehensive Plan, meets all applicable guidelines or criteria of the Land Use Code and is compatible with and responds to the intended character, appearance, quality of development and physical characteristics of the vicinity.

The review and approval process is made up of three phases:

1. **Preapplication Conference:** This is a one-time service that allows for early feedback and direction from Development Services staff on a development proposal. This occurs at the schematic design phase when access, site and building design are easily modified.

2. **Predevelopment Services:** This is an optional step that allows a developer to get on-going feedback and detailed direction from staff on proposal. The purpose is to resolve issues early and assist in the preparation of a complete formal Design Review application and qualify project.

3. **Design Review:** Design Review is a Land Use review process in which Development Services staff from every city permit authority, including Fire, Utilities, Transportation will review a project for compliance with applicable codes, standards, and guidelines. In most cases the SEPDA Determination will be included in this review. This is an administrative decision made by the Director using Process II (LUC 20.35.200).

Development Standards

Overview

The Development Standards establish regulatory requirements for new structures, development, or activities and remodels or expansions of public and private parcels within the Bel-Red Subarea. These standards support and strengthen existing and planned infrastructure while establishing a hierarchy of use and a highly identifiable subarea character.

The Development Standards are:

- Tools to aid in design and review of development projects
- Clear, objective and prescriptive requirements for essential aspects of development
- Tailored to the Bel-Red Subarea in order to enhance its unique potential and maximize development opportunities

The following pages identify regulatory requirements and design details for the Bel-Red Subarea, including:

- Maximum Allowable Floor Area Ratios
- Maximum Building Heights
- Bonus/Incentive Areas
- Required Ground Floor Uses
- Required Build-to Lines
- Required Active Edges

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Maximum Allowable Floor Area Ratios

Figure X below indicates the maximum allowable floor-area-ratios (FARs) in the Bel-Red Subarea. Higher allowable FARs occur in the nodes, taking advantage of transit-oriented development opportunities. Any additional FAR above the base FAR is only achievable through participation in the city's amenity incentive system (see 20.XX).

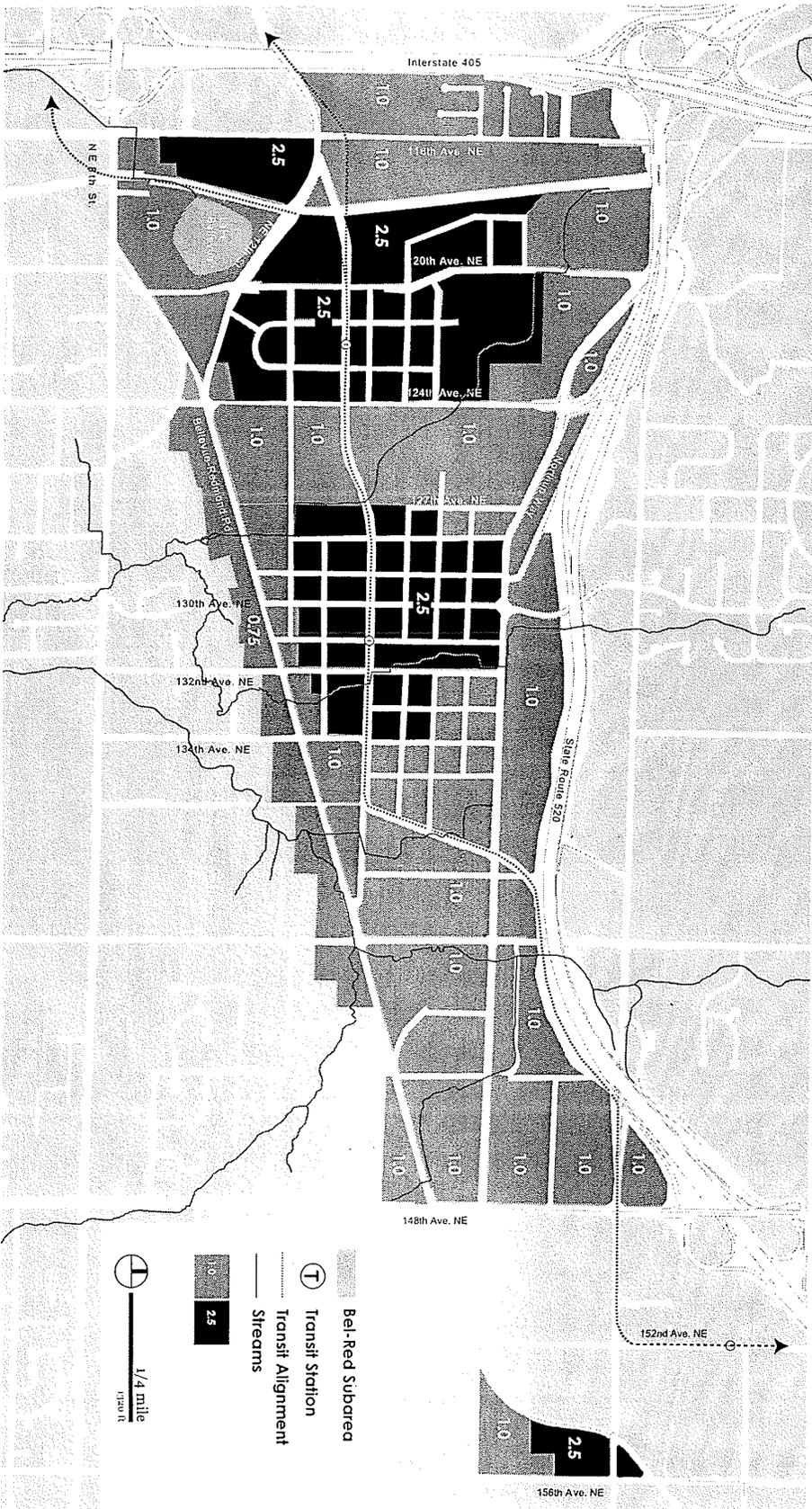


Figure X: Maximum Allowable Floor Area Ratios

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Maximum Building Heights

Figure X below indicates both the base and maximum allowable building heights in the Bel-Red Subarea. The highest allowable heights generally occur in the nodes and take advantage of transit-oriented development opportunities. Any height increase above the base height is only achievable through participation in the city's amenity incentive system (see XX).

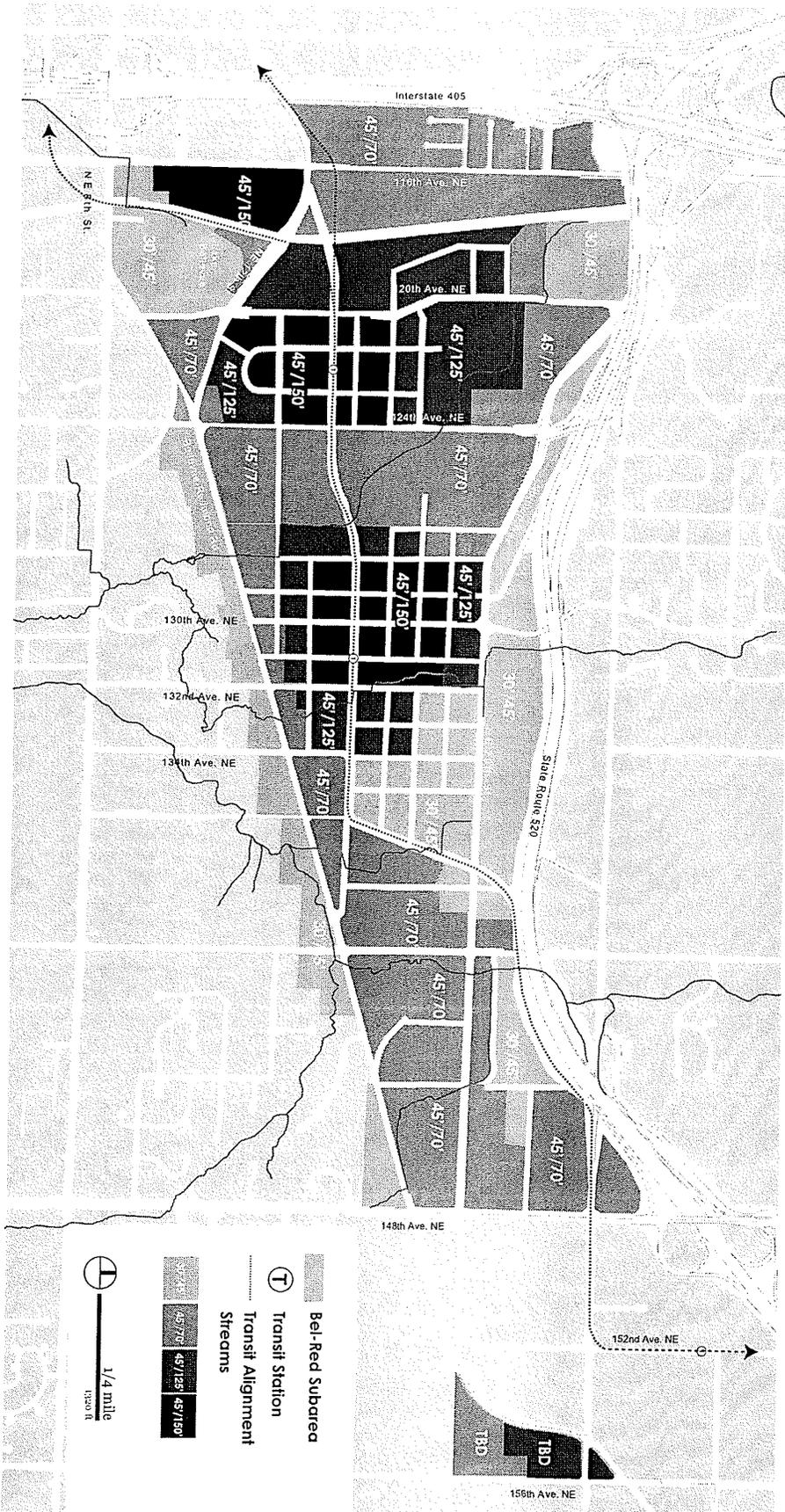


Figure X: Maximum Building Heights

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Bonus/Incentive Areas

The Bel-Red amenity incentive system is intended to facilitate the development of public amenities in exchange for additional FAR and height above base levels (see 20.XX). The amenity incentive system applies to the areas as shown in Figure X.

RESERVED

Figure X: Bonus/Incentive Areas

Required Ground Floor Uses

Ground floor retail and commercial uses are an essential component of active and vital streets as well as transit station areas. The ground floor uses figure below identifies the locations where a vibrant "8-hour" pedestrian environment is to be created—where neighborhood-serving uses are within an easy walk, bike or transit trip.

The ground floor uses framework applies only to building frontages identified in Figure X below. The street-edges facing the transit station areas along NE 15th/16th Street and buildings along the 130th Avenue pedestrian-oriented street require special attention to design and use to promote functionality and interest in the public realm. All other building ground floors within the subarea may include retail or commercial uses; however, they are not required.

Retail Ground Floor Uses

Pedestrian-oriented streets are most successful when a number of continuous retail storefronts are arranged edge-to-edge. Retail uses are defined as businesses that engage in the sale of merchandise. Primary permitted uses on the ground floor are limited to:

- Eating and drinking establishments
 - Merchandise sales
 - Personal services (i.e. cleaners/beauty)
- Interruptions may occur for residential lobbies. The ground floor requirement ensures that all other land uses are prohibited, including commercial uses such as banks and real estate offices.

Commercial Ground Floor Uses

Ground floor commercial uses are required for portions of NE 15th/16th Street that front future transit stations. Commercial uses are defined as businesses that engage in the sale of services. Along with the retail uses outlined above (eating and drinking establishments, merchandise sales, and personal services), permitted uses on the ground floor are limited to:

- Financial services, Real estate services, Insurance services
 - Lodging
- Commercial or retail uses on NE 15th/16th Street may be interrupted in the 122nd Avenue node by up to 50 percent of the ground floor frontage being the lobby entrance to office or residential buildings, and in the 130th Avenue node by up to 25 percent of the ground floor frontage being residential lobbies or live/work space.

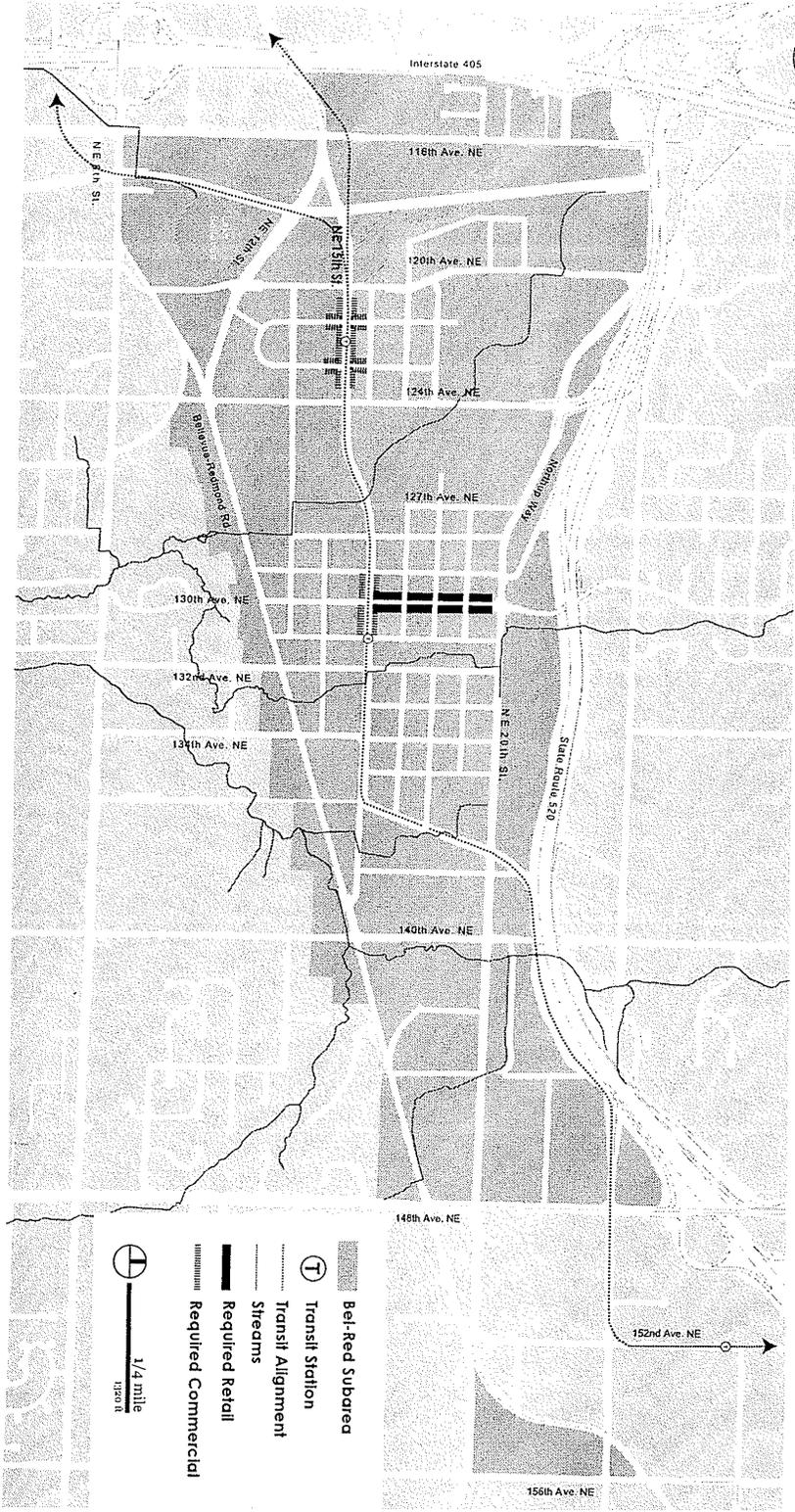


Figure X: Required Ground Floor Uses

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Required Build-to Lines

Build-to lines occur along select block faces to help establish a continuous "street wall", providing a sense of enclosure and visual interest for pedestrians. Figure X identifies locations where the ground-floor facades must be built to the back of sidewalk (exceptions are listed below). Other building frontages may have ground-floor facades built up to the property line, but are not priority areas.

The required build-to lines are identified in the same locations as required ground-floor retail or commercial uses as well as other streets in the core of the nodes where higher pedestrian activity is anticipated.

Build-to lines also contribute to the economic viability for retail and commercial uses by providing direct pedestrian access and visual exposure to potential drive-by customers.

The following criteria shall be met:

- Buildings shall be constructed along the entire block length as indicated in Figure X below
- Entrances to buildings may be recessed up to five feet maximum behind the build-to line
- Windows and walls may be recessed up to 18 inches if columns, pilasters, walls or other architectural

fenestration tie them together to the rest of the building frontage

- Interruption of the build-to line is allowable for access to public or private plazas, parks, or other publicly accessible spaces. Where interruptions occur, a maximum of 15 percent of total block frontage is allowable.

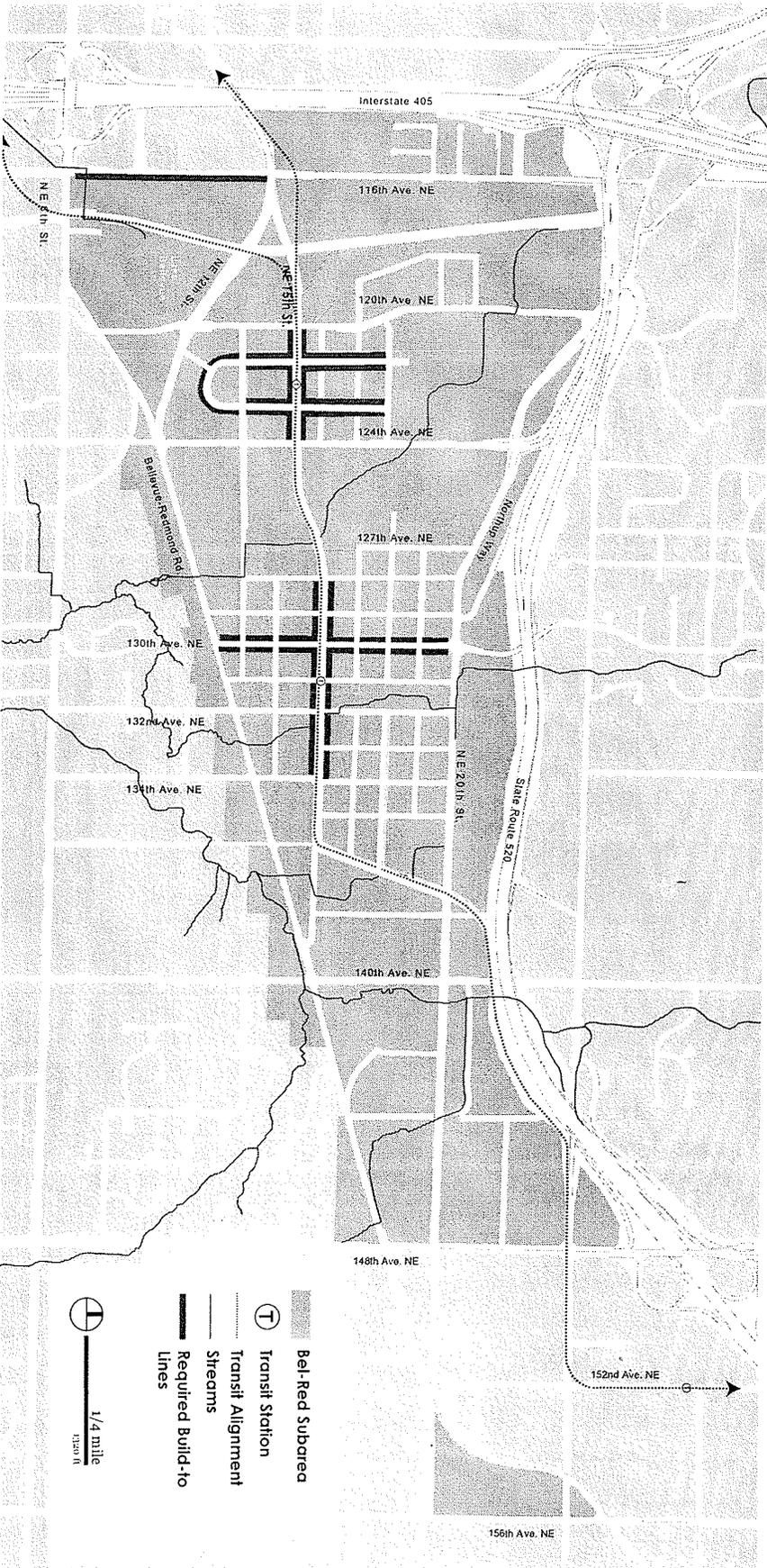


Figure X: Required Build-to Lines

Required Active Edges

Active edges are required along streets as shown in Figure X where a strong relationship is desired between pedestrians and adjacent activities. Other building frontages may comply with the active edge standards, but are not required. Active edges are characterized as ground floor building frontages with direct entries from the sidewalk and/or building frontages with a high degree of transparency and activating land use. This increased visual and physical interaction between the people inside and outside the buildings results in "eyes on the street" while creating a greater degree of safety and a vibrant public realm.

- The following criteria must be met for all ground floor retail and commercial uses on identified active edge streets:
 - A minimum of 70 percent transparent glass or screens along ground floor facades; frosted, tinted, reflective glass or other types of glass or window treatments that diminish transparency are prohibited
 - Primary, direct entrances to all ground floor uses shall be oriented to the public right-of-way. Doorways shall not be separated from adjacent public sidewalks by steps or ramps
 - Art walls, flower booths, newsstands or other activating

- land uses are permitted for a maximum of 10 percent of any block face
- The following criteria must be met for all other uses on identified active edge streets:
 - Primary entrances must be oriented toward the street or open space
 - Doorways may be separated from adjacent public sidewalks by a maximum of a 36" grade separation
 - Windows shall be provided along facades, but no minimum percentage of opening size or transparency is required

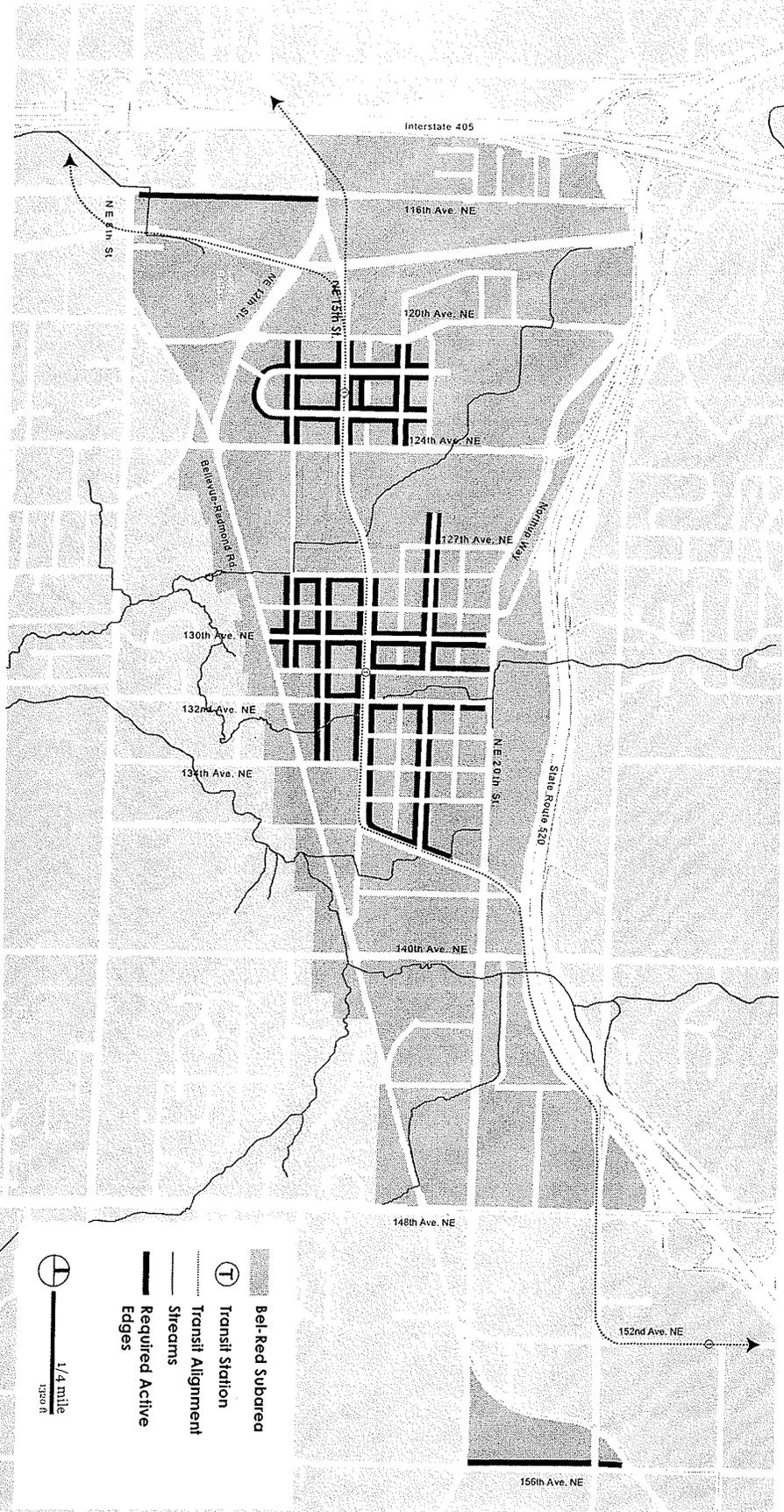


Figure X: Required Active Edges

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Street Standards

Overview

- The intent of the Street Standards is to establish a hierarchy of emphasis and design treatment for public areas within the Bel-Red Subarea. The standards will work in companion with approved standards in the City of Bellevue Design Manual for transportation projects, adding regulation only as necessary to create the desired hierarchy and subarea identity. The standards:
- Provide clear, objective, prescriptive requirements addressing only the most essential aspects of the public realm
 - Enhance the unique qualities of the Bel-Red Subarea
 - Will be implemented over time as existing uses redevelop or are improved
 - Ensure that a consistent, high-quality public realm is developed throughout the subarea
 - Provide certainty and predictability for the community and stakeholders that the public realm will be implemented as envisioned

Required New Local Streets

Figure X identifies a grid of local streets and/or public rights-of-way that improve mobility by increasing pedestrian accessibility, subarea roadway capacity and by dispersing traffic throughout the subarea. The street grid may be adjusted to respond to specific site conditions, property ownership and phasing considerations.

- The maximum perimeter distance of a block shall be 1200 feet when measured at the property line.
- All streets shall be accessible to the public at all times. Gates or any other means of restricting access are prohibited.

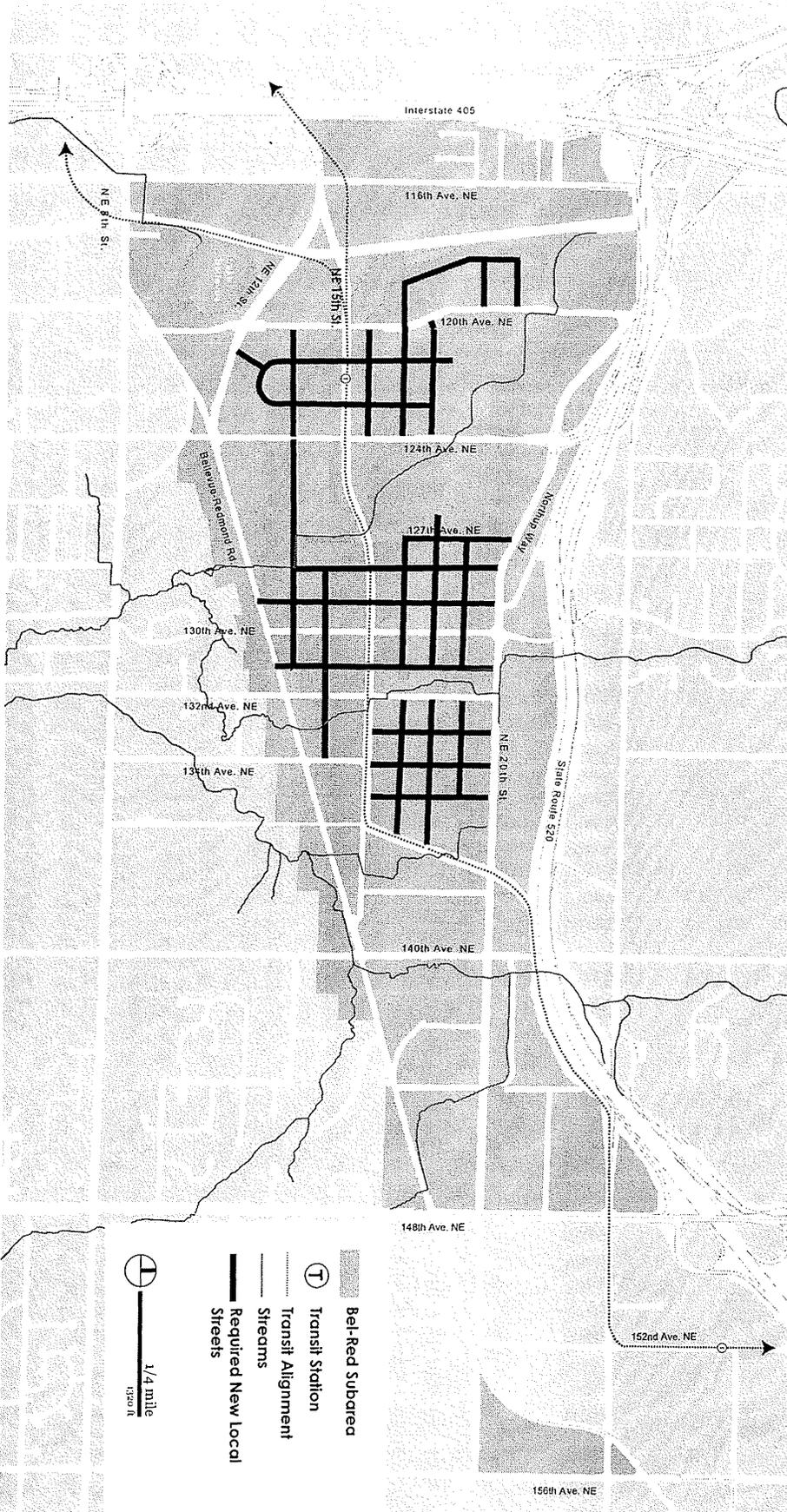


Figure X: Required New Local Streets

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Required On-Street Parking

Curbside, or on-street, parking can contribute to the pedestrian environment, be a great benefit to retail uses, and enhance elements of neighborhood character. Figure X below indicates the block faces where on-street parking is required. Required locations include portions of NE 15th/16th Street adjacent to retail and commercial uses, along the 130th Avenue pedestrian-oriented retail street, and for a number of the local grid streets with the transit nodes.

The design of parking on designated Green Streets must conform to the design details in XX section of this document. On-street parking is permitted, but not required on all other local streets.

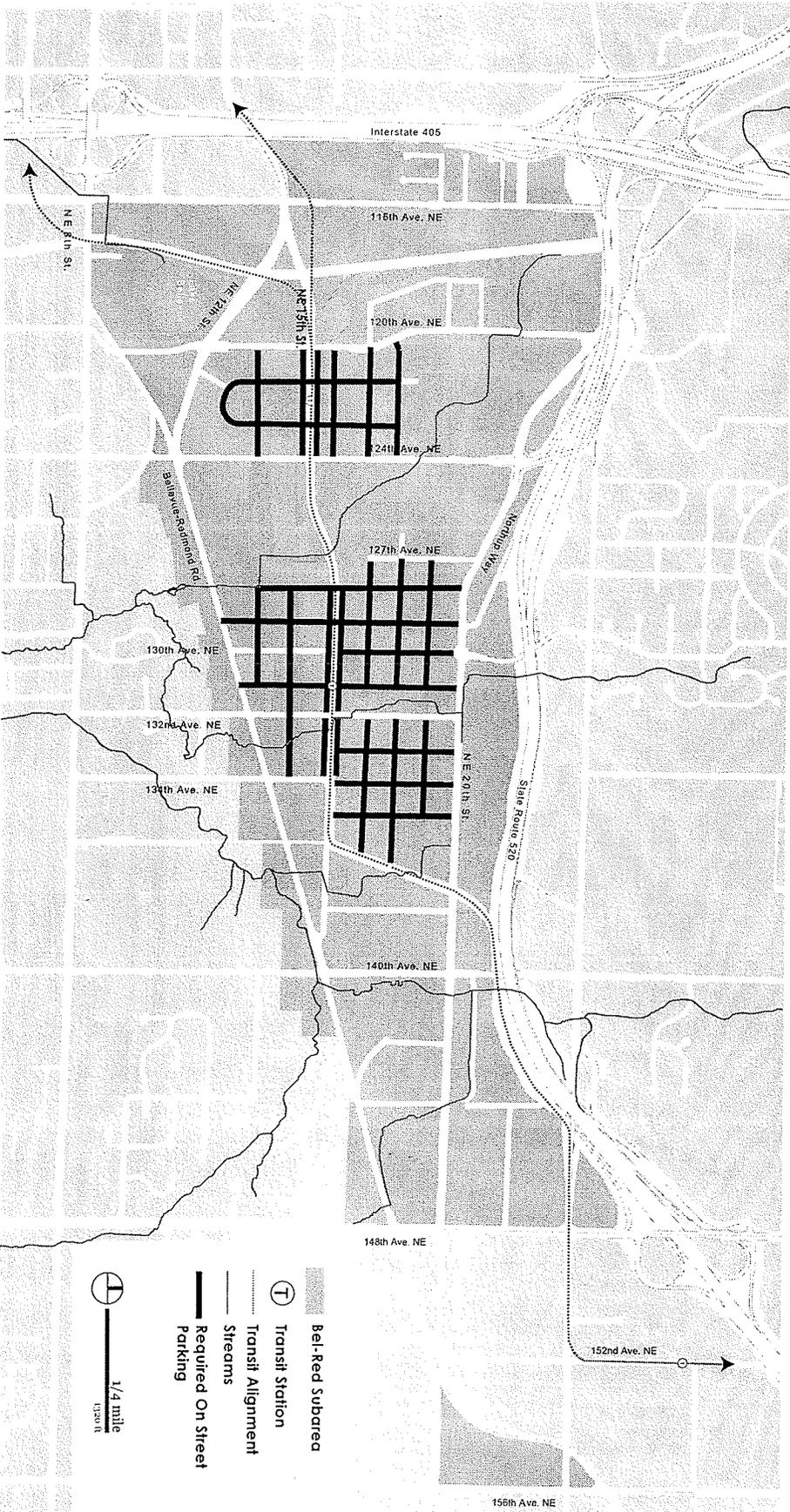


Figure X: Required On-Street Parking

Restricted Driveway Access

To avoid conflicts between transit, autos, bicycles and pedestrians and to reinforce active edge continuity along the commercial and retail frontages, the block faces along NE 15th/16th Street and the 130th Avenue pedestrian-oriented street as shown in Figure X are subject to driveway access limitations.

Where access limitations apply, the following standards must be met:

- Curb cut driveway access is prohibited. Access would occur via other arterials, local streets and alleys off of these designated corridors.
- Designated on-street loading and service spaces are prohibited with the exception of five minute (maximum) loading spaces for residential and office uses. These spaces shall be limited to a maximum of two spaces per block face.

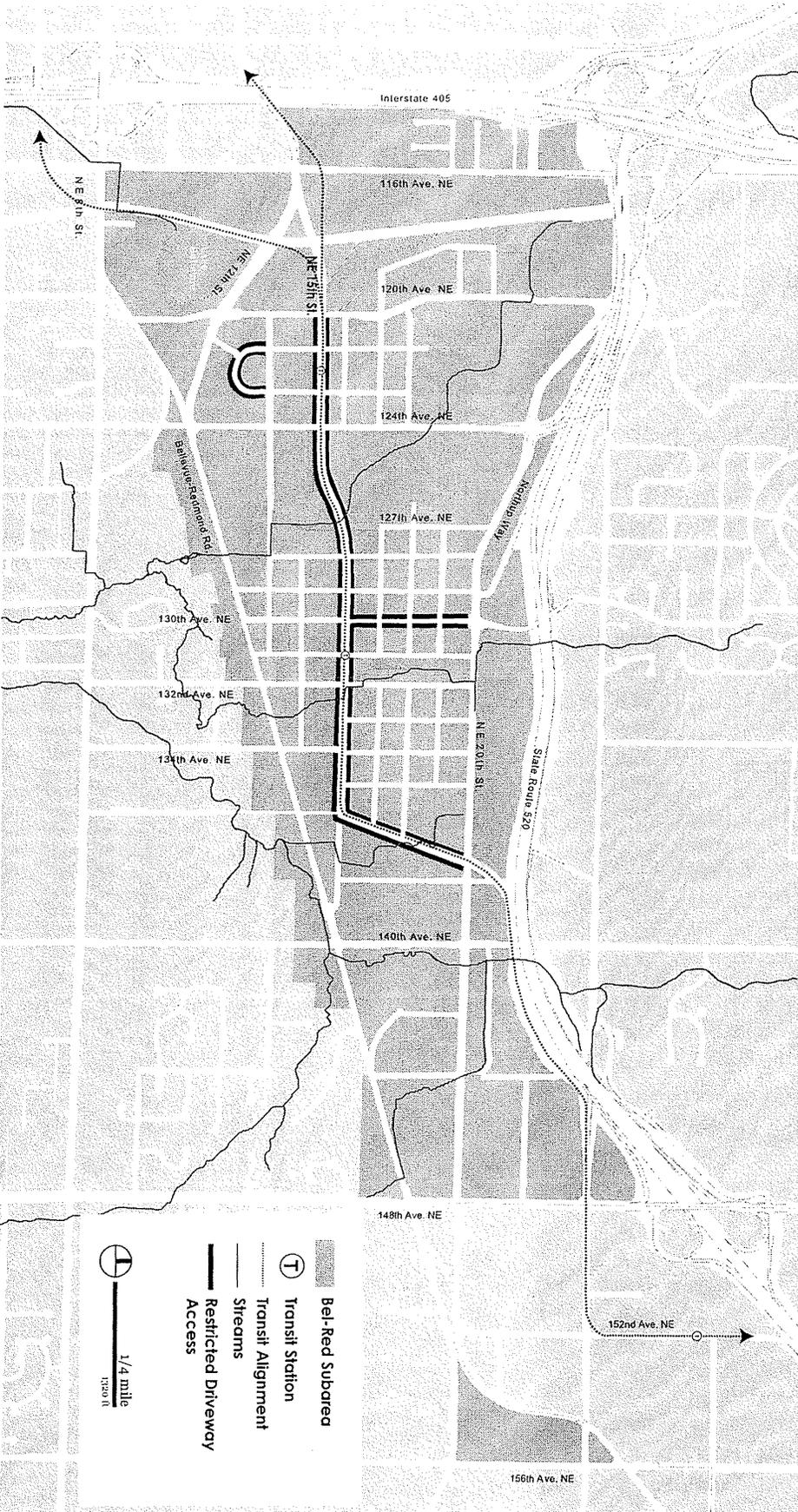


Figure X: Restricted Driveway Access

Required Ornamental Street Lighting

There shall be three lighting schemes used consistently on the streets identified in Figure X below.

- Pedestrian-scaled street lighting
- Neighborhood street lighting
- Ornamental roadway/transit trackway lighting

The intent of the required ornamental street lighting schemes is to:

- Reinforce the hierarchy of land uses and corridor character

- Ensure a safe environment is created for all modes of travel
- Reduce the potential amount of light pollution in the area
- Limit glare on adjacent uses
- Create a festive and attractive evening environment on signature retail and commercial streets.

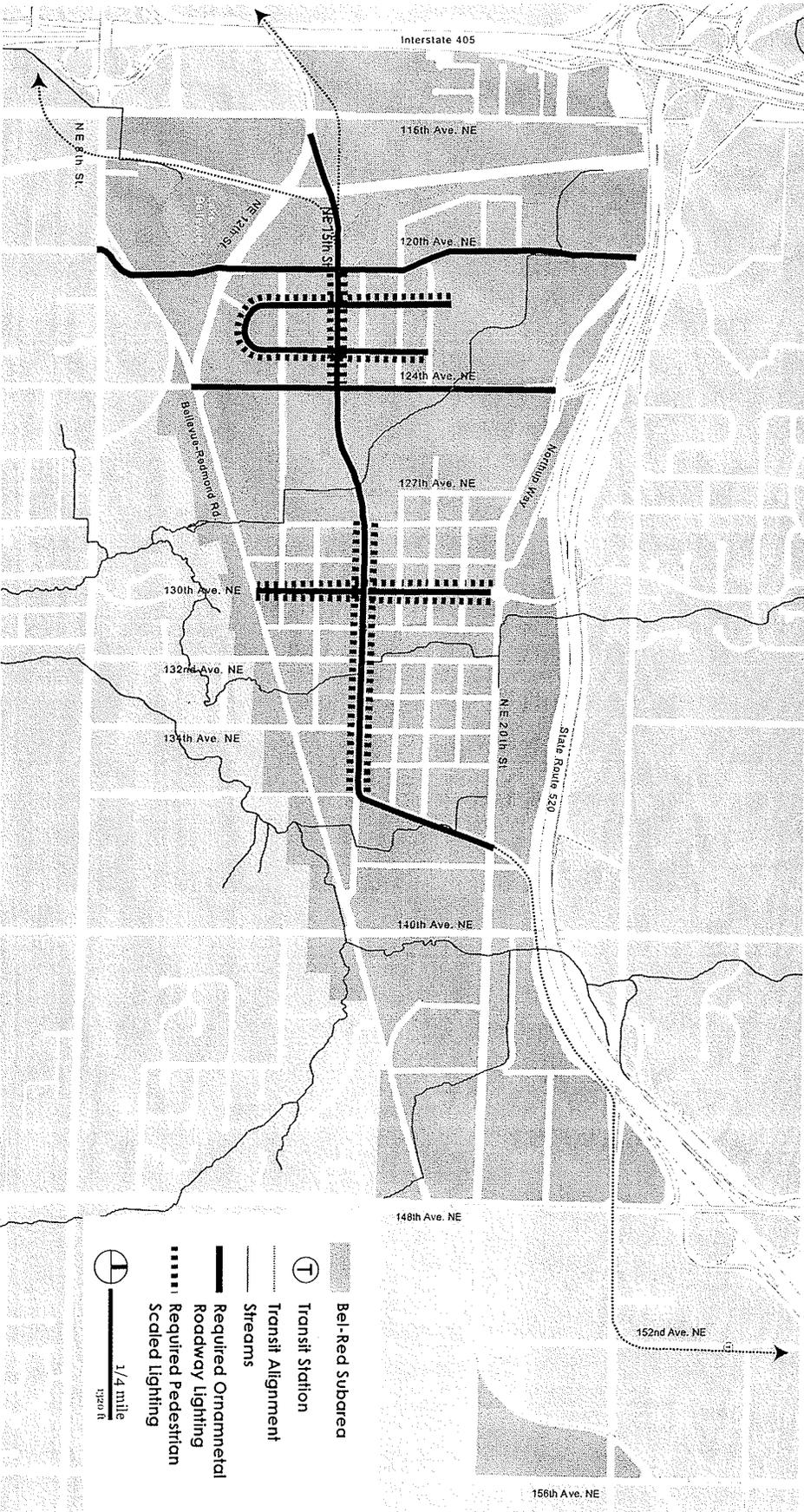


Figure X: Required Ornamental Street Lighting

Required Landscape Treatments

Four types of landscape treatments are required in the Bel-Red Subarea as shown in Figure X:

- NE 15th/16th Transit Boulevard landscaping
- Arterial landscaping
- Sustainable street landscaping
- Local street landscaping

The landscape palettes for these corridors have been created to:

- Establish a hierarchy of roadways

- Create a diverse, yet coherent, subarea-wide landscape
- Employ dramatic use of native materials wherever appropriate
- Enhance the pedestrian environment
- Selected plants include a variety of climate-appropriate, low-maintenance species that add to the beauty of the urban environment. The landscape treatments and associated plant palettes for NE 15th/16th Street are described in _____. Treatments for other arterials and local streets are described in Bellevue LUC 20.25._____.

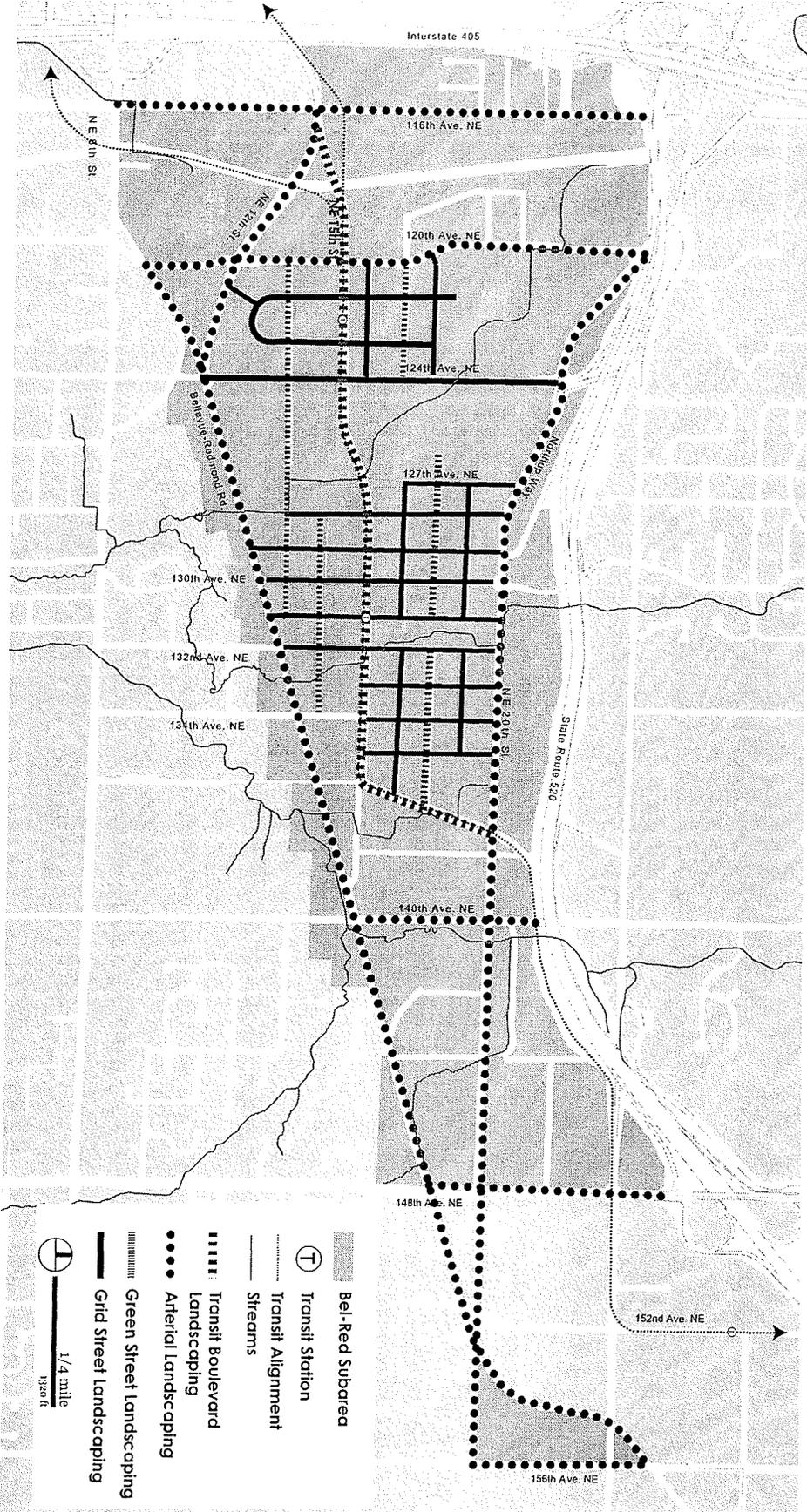


Figure X: Required Landscape Treatments

Required Street Furniture

Street furniture elements are required along streets as shown in Figure X. The street furniture elements support a consistent and uniform street character.

Required elements include:

- Seating: a minimum of three benches per block face
- Litter receptacles: two per block
- Bicycle racks: minimum of two per block face, with additional racks focused near transit stations and along

- 130th Avenue north of NE 15th/16th Street
- Safety bollards: at all intersections
- Newspaper dispensers: one per block face
- Drinking fountains: one per transit station platform

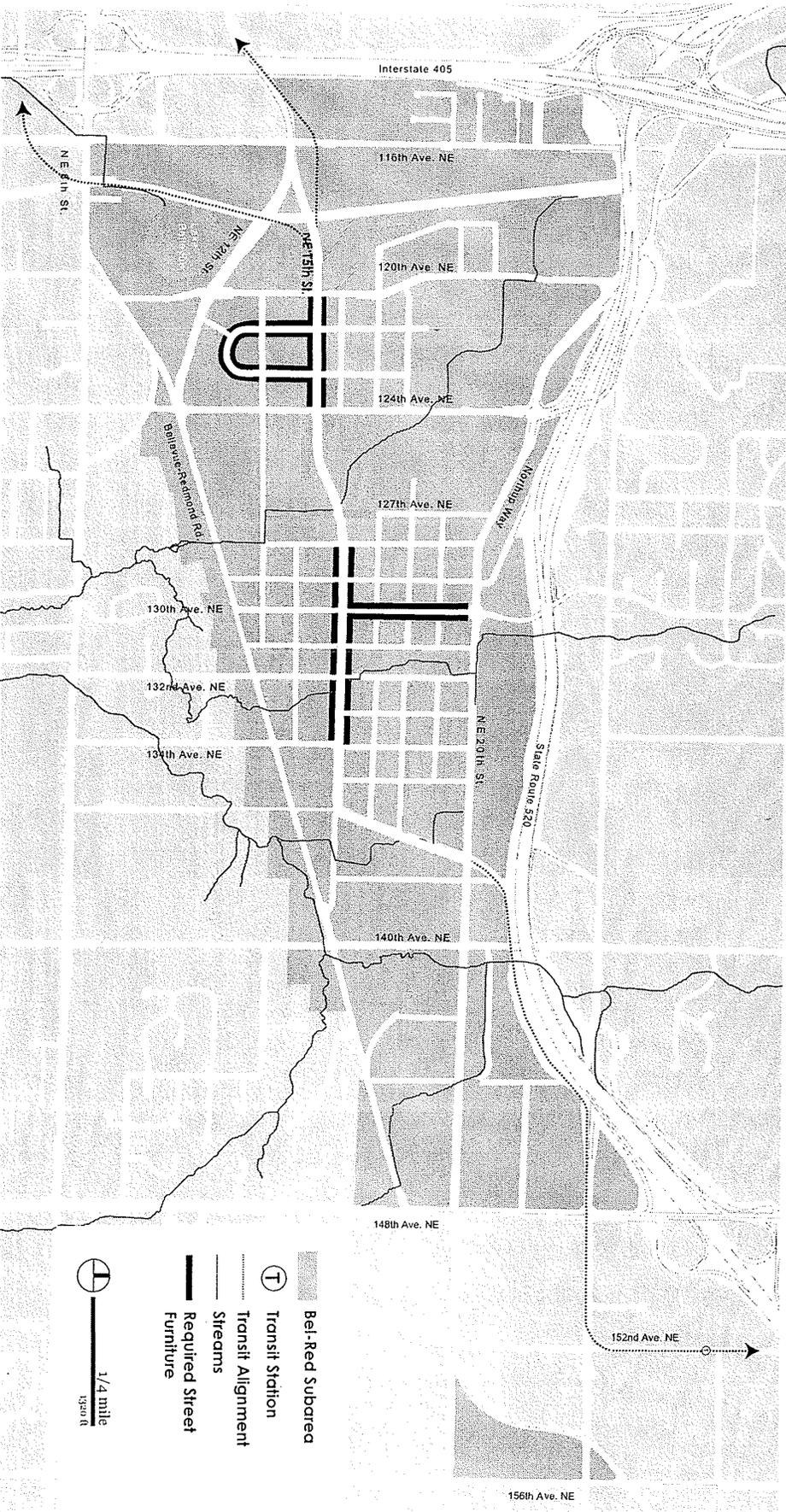


Figure X: Required Street Furniture

NE 15th/16th Transit Boulevard

The NE 16th Transit Boulevard is a key infrastructure investment that serves as the symbolic and functional thread that knits the subarea together. It connects the most intense areas of development and includes vehicular, high capacity transit, and non-motorized travel modes as well as incorporate significant urban open spaces, and environmentally sensitive design features.

NE 15th/16th varies in character from intimate and active within the nodes, to open and passive along future parks and enhanced stream corridors.

The specific design treatments for NE 15th/16th are

- prescribed for three distinct areas, or segments:
 - NE 15th segment between 120th Avenue and 124th Avenue, including a future light rail station, and potential interim bus-rapid transit
 - NE 16th segment between 127th Avenue and 134th Avenue, including a future light rail station, and potential interim bus-rapid transit
 - Three connecting segments which serve as links between station areas in some instances and to less intense land uses in other cases

Design details, including roadway cross-sections and usage will be developed for each segment.

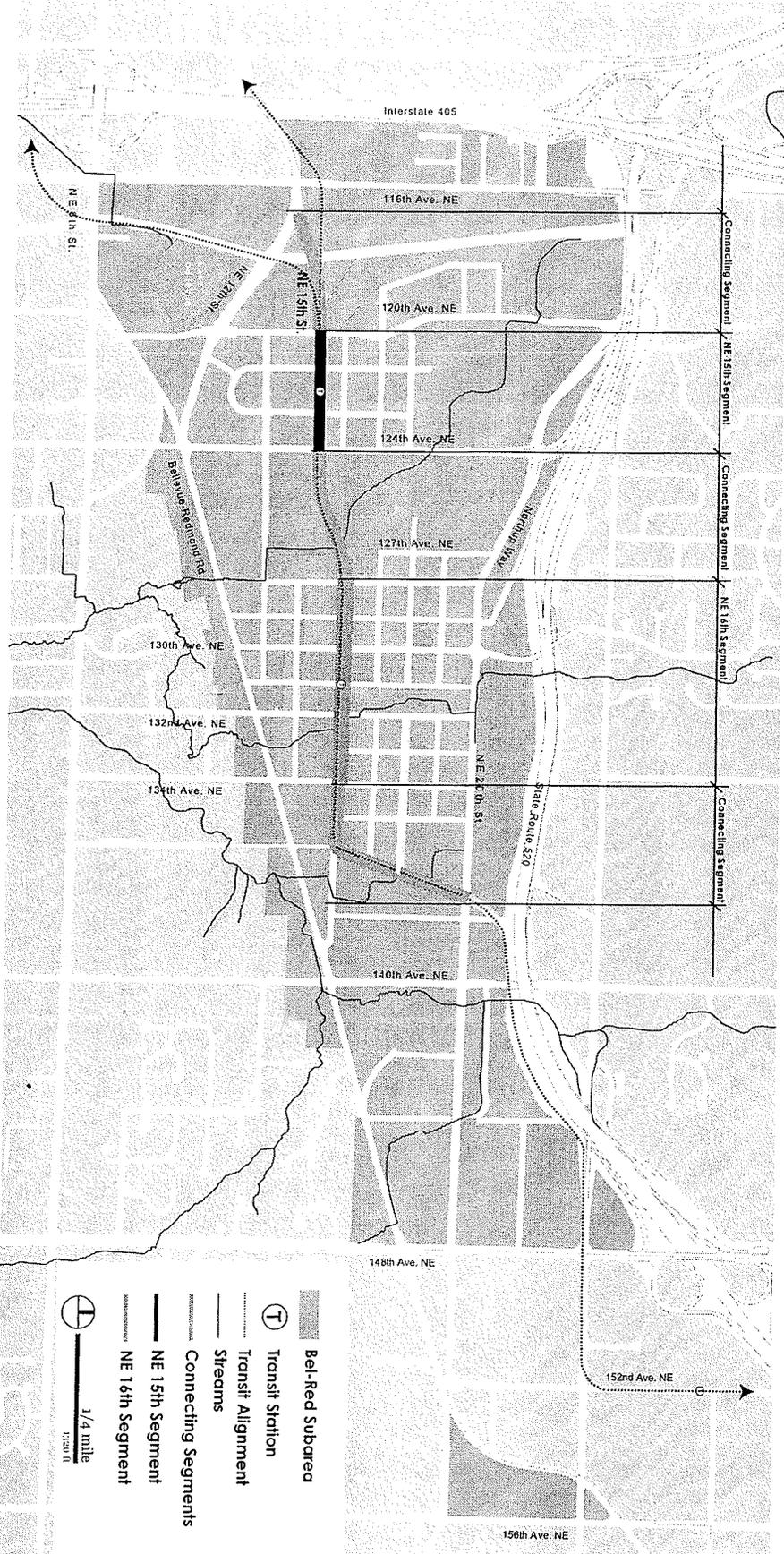


Figure X: NE 15th/16th Transit Boulevard Segments

130th Avenue Pedestrian-Oriented Retail Street

The portion of 130th Avenue north of NE 16th as shown in Figure X is envisioned as a centralized neighborhood-serving retail street. This is a central location within the subarea, with a high potential for redevelopment (especially residential), and adjacent to a proposed transit station area.

The design of this portion of 130th Avenue will help make the pedestrian experience a priority and foster a vital retail environment by using:

- Uniform design features to create a common thread along the street

- High quality materials to support diverse pedestrian and retail activities
- Sidewalk widths that create an ample, yet intimate pedestrian through zone as well as a furniture zone for cafe seating and benches

The following are requirements for 130th Avenue NE north of NE 16th Street:

- Comfortable pedestrian environment to support a diverse and dynamic range of pedestrian, retail and commercial uses

- New or widened (14'-6") sidewalks
- High quality paving such as granite accent pavers
- Continuous landscaping, including canopy street trees and landscaped tree wells and curb extensions
- Pedestrian-scale elements such as lighting, landscaping, furniture, signage and appropriate weather protection

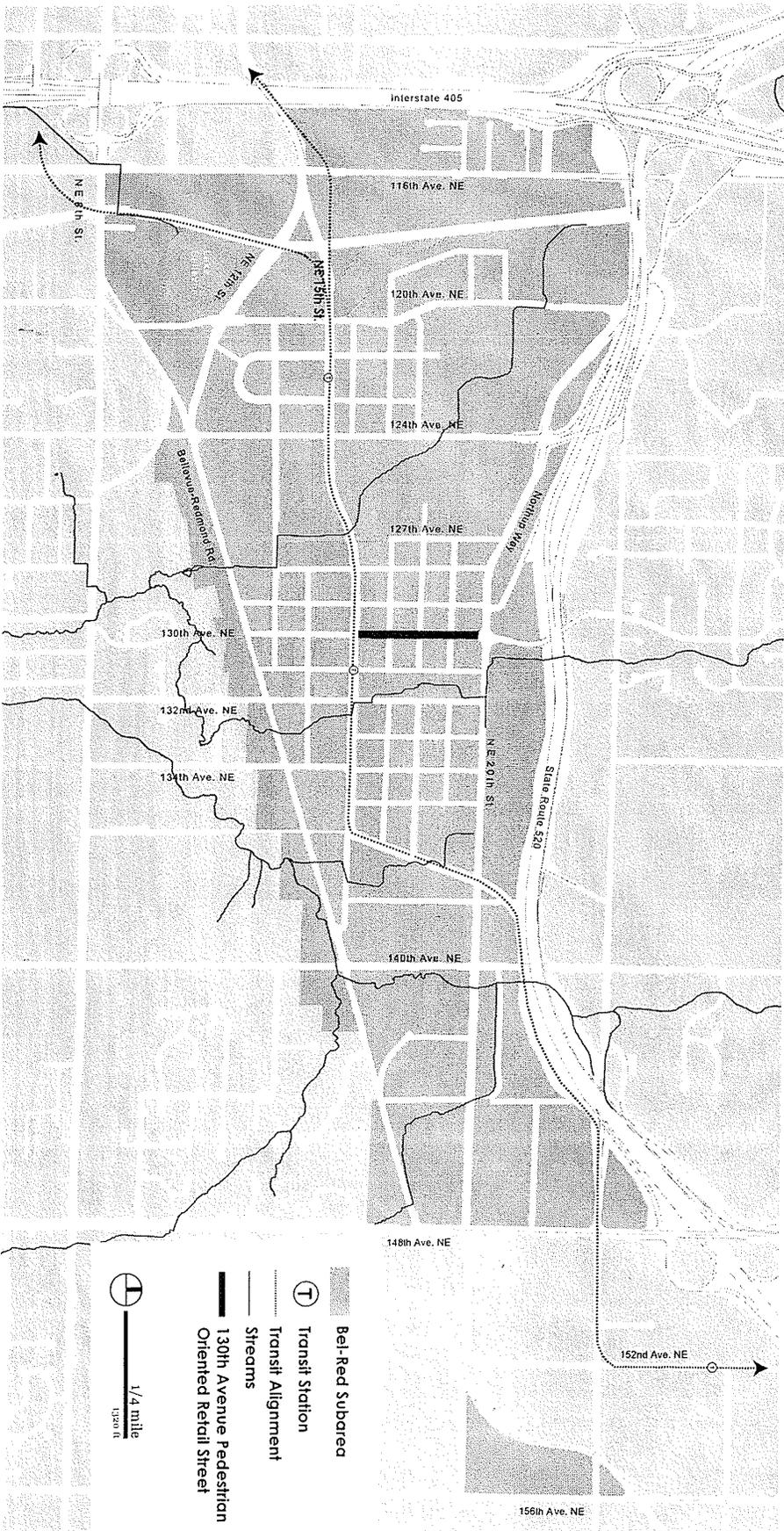


Figure X: 130th Avenue Pedestrian-Oriented Retail Street

Green Streets

- Green Streets utilize natural drainage systems, where appropriate, to improve and reduce the amount of stormwater runoff at its source. Green Streets will:
 - Reduce the quantity of polluted stormwater that enters local streams
 - Reduce impervious surfaces, allowing stormwater to infiltrate and recharge ground water supplies
 - Be aesthetically pleasing and provide an attractive pedestrian experience
- Increase urban greenspace
- Green Streets shall include a series of landscaped stormwater planters, where appropriate, designed to capture and infiltrate run-off while maintaining strong pedestrian circulation, on-street parking, and vehicle access as well as improving the streetscape with attractive green space.

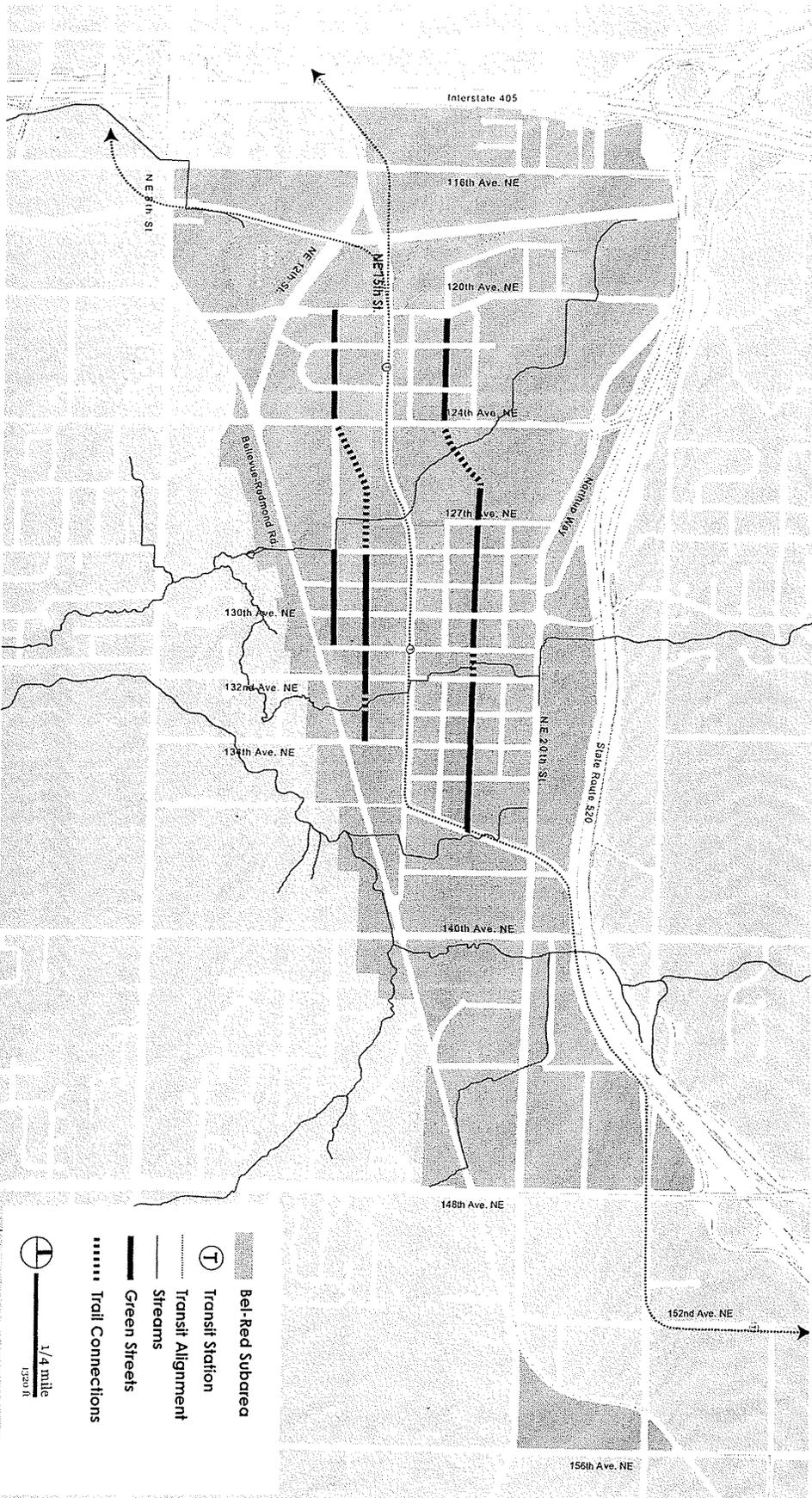


Figure X: Plan of Typical Green Street

Local Grid Streets

Local grid streets are intended to complement adjacent land uses and accommodate multiple transportation modes, pedestrians, bicycles and vehicles without compromising safety or function.

These local streets are generally designed to accommodate low vehicle traffic volumes, and function as places for public interaction and environmental enhancement more than simple transportation corridors. The typical cross section for a local grid street is 62 feet.

Typical street design shall include the following minimum elements:

- 13-foot sidewalks or a combination of eight-foot sidewalks, one-foot curbs and four-foot landscaped areas with trees, turf and ground cover between the sidewalks and the curbs
- Two travel lanes
- Eight-foot curbside parallel parking lanes on both sides of the roadway
- Landscaped curb extensions at each street corner
- Scored Concrete Paved Crosswalks at major intersections
- 8-foot through pedestrian sidewalk zone
- 5-foot furniture and landscaping sidewalk zone
- Canopy trees planted on both sides of the street at regular intervals (30' maximum)

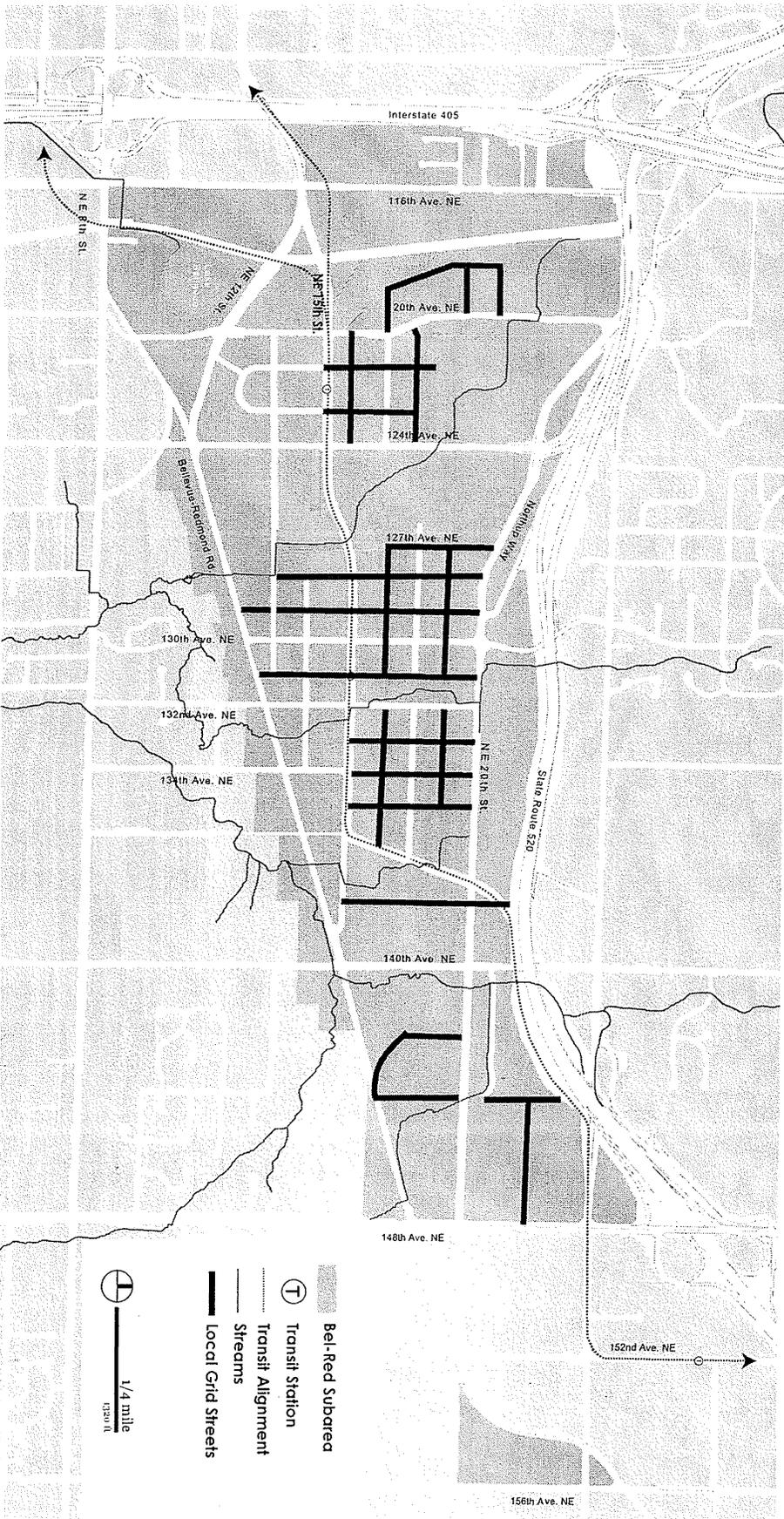


Figure X: Local Grid Streets