

CITY COUNCIL AGENDA MEMORANDUM

SUBJECT

Resolution No. 7435 approving the Pedestrian Bridge location and design plan and adopting findings that the proposal satisfies the public benefit test set forth in LUC 20.25A.130.C and the development standards of LUC 20.25A.130.D and is consistent with the Comprehensive Plan for the pedestrian bridge proposed between Lincoln Square and Bellevue Place.

FISCAL IMPACT

Fees associated with review of the location and design plan were billed to the applicant (Kemper Development Company) consistent with the cost recovery goals adopted by the City Council for the Development Services function. Therefore, no fiscal impacts are anticipated as a result of this action.

Prior to construction of the proposed pedestrian bridge, if the design is approved, the City and Kemper Development Company must enter into a legal agreement allowing installation of the bridge over the right of way. That agreement will be brought forward for Council consideration later in September. The fiscal impacts associated with approval of the right-of-way permit and execution of the legal agreement for location of the bridge over NE 8th Street will be discussed in more detail in the agenda materials provided with that agreement.

STAFF CONTACT

Carol Helland, PCD Land Use Director 452-2724
Patti Wilma, PCD Land Use Manager 452-2725

POLICY CONSIDERATION

Should the City Council approve the revised location and design plan for the pedestrian bridge across NE 8th Street between Bellevue Way NE and 106th Avenue NE, based on the staff recommendation and analysis of consistency with the City's Land Use Code and Comprehensive Plan contained within this memorandum?

BACKGROUND

In 2006, Kemper Development Company (KDC) recommenced construction of the Lincoln Square office tower. A significant addition to the project is a proposed pedestrian bridge to link the second floor of Bellevue Place to the second floor of Lincoln Square. The original bridge location and design plan was presented to City Council on June 19, 2006. Staff acknowledged that additional refinement was necessary in order for the design plan to comply with the terms of the Land Use Code.

The plan presented on June 19th has been revised based on staff comments to provide better wayfinding, a clearer sense of entry to the bridge at Bellevue Place and better connection to the sidewalk by using bridge design elements, special paving and lighting. Consistent with prior Council direction, staff has prepared a Resolution approving the revised location and design plan (see Attachments 1A-1E for the plans on which approval is based), and describing the public benefit of the proposed NE 8th Street Pedestrian Bridge.

Analysis of consistency of the proposed plan with the Land Use Code standards is included for Council consideration. Staff recommends Council approve the revised location and design plan because it demonstrates public benefit and compliance with the development standards of LUC 20.25A.130 C & D.

FINDINGS

I. LUC 20.25A.130.C. Public Benefit:

1. The bridge improves pedestrian mobility;

Jaywalking is a common occurrence from the motor court drive at Bellevue Place to Lincoln Square. The current distance between public crossings on NE 8th Street is approximately 600 feet. The proposed bridge will provide a convenient and safe opportunity for pedestrians to cross the street mid-block without competing with vehicles. Both second level and at-grade access to the bridge will be identified through signage and architectural treatment so that clear direction from the street level to the second levels of both developments is provided. Safe barrier-free crossing of NE 8th Street will be greatly enhanced by the bridge.

The clear message about access to the bridge provided by signage, lighting, and architectural details will encourage use of the bridge. The convenient route to the bridge will encourage pedestrian mobility from sidewalk to sidewalk as well as from the inside of the two adjacent buildings.

2. The bridge does not detract from street level activity;

The use of the existing pedestrian bridge between Bellevue Square and Lincoln Square is a good indicator of how much activity remains at the street level while a bridge is in use. Bellevue Way is animated and lively even when the existing bridge is well used. The proposed location and design of the NE 8th Street bridge will integrate pedestrian activity with the newly renovated streetscape and Bellevue Place and will provide one more opportunity to enter Lincoln Square. As with the Bellevue Way bridge, this bridge will itself be an attractor of street level activity as pedestrians access it from highly visible exterior entrances on both sides of the street.

The bridge will not detract from street level activity, rather it is likely to increase it as it becomes a landmark and focal point attracting people to the enhanced streetscape and midblock building entry points. Crossings at Bellevue Way and 106th Avenue NE will remain active and heavily used because they will serve key street level entries to other nearby buildings and businesses, as well as those at Lincoln Square and Bellevue Place.

Visibility of access is enhanced in the revised design. Convenient access to the bridge is critical to ensure street level activity and use of the bridge are optimized. It is also critical that uses with high pedestrian orientation be located at the street level adjacent to the bridge ends.

3. The bridge functions as part of the public realm.

The architectural character of the bridge has been designed in a language different from the buildings it connects. The result is that the bridge stands as an element of its own and not an extension of either one of the adjoining buildings. With the design of this bridge so strongly relating to the Bellevue Way bridge, the sense of them being public infrastructure, rather than extensions of the private realm, is heightened. There are design characteristics within the bridge that represent, but do not replicate, other public facilities within the downtown core of Bellevue, such as the Transit Center and Ashwood Library. This will enhance the architectural language of the public realm within the City.

II. LUC 20.25A.130. D. Development Standards.

Each proposed downtown pedestrian bridge must be developed in compliance with the following standards:

1. The bridge must be open from at least 6:00 a.m. to midnight, or during the hours of operation of adjacent uses, whichever is greater. Signs shall be posted in clear view stating that the pedestrian bridge is open to the public during these hours;

The pedestrian bridge will provide a key element of the pedestrian circulation system in this section of Downtown Bellevue. As such, the pedestrian bridge should be open and accessible as much as possible. Public accessibility will ensure that it functions as part of the public realm, does not detract from street level activity and improves pedestrian mobility. The approval of the bridge design should be conditioned as follows:

Prior to issuance of a Certificate of Occupancy, signs shall be installed at the interior and exterior ground and second level entrances at Lincoln Square and Bellevue Place. The signs shall reference the Bellevue City Code that requires the bridge be open for nonexclusive right of pedestrian use and access by the public during hours between 6AM and midnight, or during the hours of operation of adjacent uses, whichever is greater.

2. The bridge connects upper-level publicly accessible space to upper-level publicly accessible space and provides a graceful and proximate connection between the sidewalk and bridge level that is visible and accessible from the sidewalk. The vertical connection should occur within 50 feet of the sidewalk;

The bridge connects to each building so that entry is through easily identifiable portals which are differentiated in form from adjacent uses. Since the bridge is connecting two existing buildings the distance from the public sidewalk to the access point to the bridge

is somewhat predetermined. On the Lincoln Square side the access point is at the back of the sidewalk just east of the main building entry. On the Bellevue Place side the access point is in the office building and is 50 ft from the back of the sidewalk. See Attachments 1D and 1E. Extensive lighting, signage and architectural enhancements will ensure this access point is easily recognized from the sidewalk. The second level lobbies in both buildings are designed to attract users and clearly lead them to and from the bridge and down to the street. The two ends of the bridge offer individual solutions for public passage ways leading to larger public spaces: the Lincoln Square atrium and the Hyatt Hotel lobby/Wintergarden.

Existing garage mechanical equipment and public utilities in the sidewalk made it unfeasible to locate the Bellevue Place access on the west side of the drive.

3. Vertical circulation elements must be designed to indicate the bridge is a clear path for crossing the public right-of-way;

At the sidewalk level on both sides of the street, signs are planned at the entry points as at the sidewalk as needed to indicate the appropriate pathways to reach vertical access to the bridge. The Lincoln Square entry and stairway is immediately identifiable as a clear path to the bridge because it is glass enclosed at the back of the sidewalk. See Attachments 2A and 2B. Both access points are primarily glass-enclosed so visibility will be optimized.

At Bellevue Place a new soffit mimicking the under side of the bridge will be constructed in the arcade and extend over the public sidewalk. See Attachments 2A and 2B. This unique feature will attract pedestrians to the pathway and entry to the bridge. The vestibule for the stairway and the stairway leading to the bridge will be embedded into the office building.

4. Directional signage shall identify circulation routes for all users;

Signage will be provided indicating principal as well as alternate pathways to and from the bridge, making the use of it as convenient as possible. Standard graphics developed in the Downtown Wayfinding project will be used here as well as at the existing bridge and other midblock connections to inform users that these circulation routes are publicly accessible.

5. Structures connected by the bridge shall draw pedestrians back to the sidewalk with retail activities, as defined by the Building/Sidewalk Design Guidelines, at the ground level immediately adjacent to both sides of the pedestrian bridge;

Both Lincoln Square and Bellevue Place have a variety of retail activities including restaurants and retail businesses at grade adjacent to the bridge access points. All these businesses provide visual and physical access to the sidewalks, encouraging pedestrian activity. A condition of approval for locating bridge access here is that the adjacent commercial space which is currently vacant be leased to a business that qualifies as a retail activity as defined by the Building/Sidewalk Design Guidelines.

- 6. It is preferred that the bridge remain unenclosed on the sides, but allow enclosure or partial enclosure if the applicant demonstrates it is necessary for weather protection;**

The proposed bridge will be partially open similar to the existing bridge. A translucent roof and transparent weather screens/guardrails will make the pedestrian crossing both safe and comfortable.

- 7. Visual access shall be provided from the sidewalk and street into the bridge;**

The bridge is designed to be transparent and structurally light. The translucent roof and transparent sidewalls of the bridge will make visibility and connection between the bridge users and those at street level virtually obstruction-free.

- 8. Bridge may not diverge from a perpendicular angle to the right-of-way by more than 30 degrees;**

In order to allow the bridge to connect the existing stairway vestibule in the Bellevue Place office building with the new Atrium at Lincoln Square, the bridge is approximately 26 degrees from perpendicular to NE 8th Street which results in a length of approximately 177 ft.

- 9. The interior width of the bridge, measured from inside face to inside face shall be no less than 10 feet and no more than 14 feet;**

The proposed bridge has approximately the same face to face dimension as the existing bridge across Bellevue Way. The minimum width will be 12 ft and the maximum inside width will be 14 ft. Visually the bridge will appear somewhat wider due to the form of the roof and the overhang. However, this width will appear appropriate in scale given the length necessary to span NE 8th Street.

- 10. Bridge shall be located at the second building level, with a minimum clearance of 16 feet above the grade of the public right-of-way;**

The bridge will connect to both buildings at their respective second levels. The bridge deck elevation shall align with the second floor, which is approximately 22.5 feet above grade.

- 11. Impacts on view corridors, as described in LUC 20.25A.100 and 20.25A.110, shall be minimized;**

LUC Sec. 20.25A.100 indicates that views of the Olympic and Cascade Mountains, Lake Washington, the Seattle skyline and major public open spaces (MPOS) should be maintained. Views of these features are not impacted by the bridge location or design as depicted in Attachments 3A and 3B. No MPOS is located nearby.

LUC Sec. 20.25A.110 speaks to consideration of the negative impact of a structure on views to and from buildings and from streets, intersections, parks and plazas. The scale of the bridge in plan (as viewed from taller buildings) is small. All surfaces of the bridge will be treated as finished elevations because it can be viewed from all

directions. Views from streets and intersections are depicted in Attachments 3A and 3B. These views are enhanced by the addition of the bridge and no view of the Cascades or Olympics is obscured. No negative impact will result from the addition of the bridge across NE 8th Street.

12. Impacts on the function of City infrastructure, including but not limited to utilities, lighting, traffic signals, etc., shall be avoided or mitigated;

All impacts on City infrastructure have been considered and carefully planned around. As an example, the bridge is located at mid-block to avoid conflicts with any traffic signals and street lighting. Where unavoidable conflicts do occur (i.e., the bridge foundations), the bridge design has been altered to accommodate the infrastructure. There shall be no permanent impact on any existing infrastructure. The architect has worked with the Transportation and Utilities Departments to ensure that utilities are straddled (i.e. sewer line) and adequate clearance for maintenance is provided.

13. Lighting shall be consistent with public safety standards;

The bridge shall be illuminated 24 hours a day. The bridge deck shall always be illuminated for safe pedestrian use per the applicable codes, and the translucent roofing and the lightweight roof structure shall be up-lighted to provide a soft, dramatic, but functional glow for its general illumination.

As with the existing bridge, specific lighting including seasonal and special event lighting will be described in the initial building and electrical permit documents. Modifications to the approved lighting schemes will be subject to Design Review to ensure that impacts upon public facilities and streets are mitigated.

14. Signage on the exterior of the bridge, or on the interior of the bridge that is visible from a public sidewalk or street is not permitted;

Only signage for wayfinding, public accessibility and ADA accessibility will be permitted on the bridge or in the bridge such that it is visible from the public sidewalk.

15. Bridge must be architecturally distinct from the structures that it connects; and

The bridge is intended to be architecturally distinct from both Bellevue Place and Lincoln Square. The principal bridge materials (formed steel, glass, and polycarbonate) and form (suspended, curvilinear and graceful) are fundamentally dissimilar from surrounding development which is solid and predominately masonry, metal, and glass with no curved or inclined surfaces.

16. Bridge must exhibit exemplary artistic or architectural qualities.

The design of the proposed bridge strongly mimics the first bridge. It will appear more open because the translucent roof is flatter; less of it will be visible. As one is on the bridge, there will be a sense of connection with the street due to the use of glass sidewalls/guardrails and open sections between the top of the guardrail and the roof.

The bridge has been designed as a light and light-filled structure so as not to overshadow the street level.

Careful consideration has been given to its methods and materials. The materials are of high quality and long-lasting. As a permanent structure in Downtown, it must literally 'bridge' the passing architectural trends that will undoubtedly influence the designs of future bridges. Through the years, it must stand as a constant of beauty, quality, and refined engineering. And, it must appear strong and safe. In short, it must be thought of as elegant, from the day it is built and from that day forward. It must be serious but not solemn; fun but not silly.

Great care has been taken to ensure that the bridge is designed and built with these things in mind. The materials (chosen for their strength and durability) are of the highest quality. The engineering is advanced, appropriate, and thoughtful. Combined, the materials and methods are the products of leading-edge technology. It expresses those qualities in an exciting but still dignified manner. The bridge design is an exemplary marriage of art and engineering.

EFFECTIVE DATE

If approved, this Resolution becomes effective immediately.

OPTIONS

1. Resolution No. 7435 approving the Pedestrian Bridge location and design plan and adopting findings that the proposal satisfies the public benefit test set forth in LUC 20.25A.130.C and the development standards of LUC 20.25A.130.D and is consistent with the Comprehensive Plan for the bridge proposed between Lincoln Square and Bellevue Place.
2. Do not adopt Resolution 7435 and provide alternative direction to staff.

RECOMMENDATION

Adopt Resolution No. 7435 approving the Pedestrian Bridge location and design plan and findings that the proposal satisfies the public benefit test set forth in LUC 20.25A.130.C and the development standards of LUC 20.25A.130.D and is consistent with the Comprehensive Plan for the pedestrian bridge proposed between Lincoln Square and Bellevue Place.

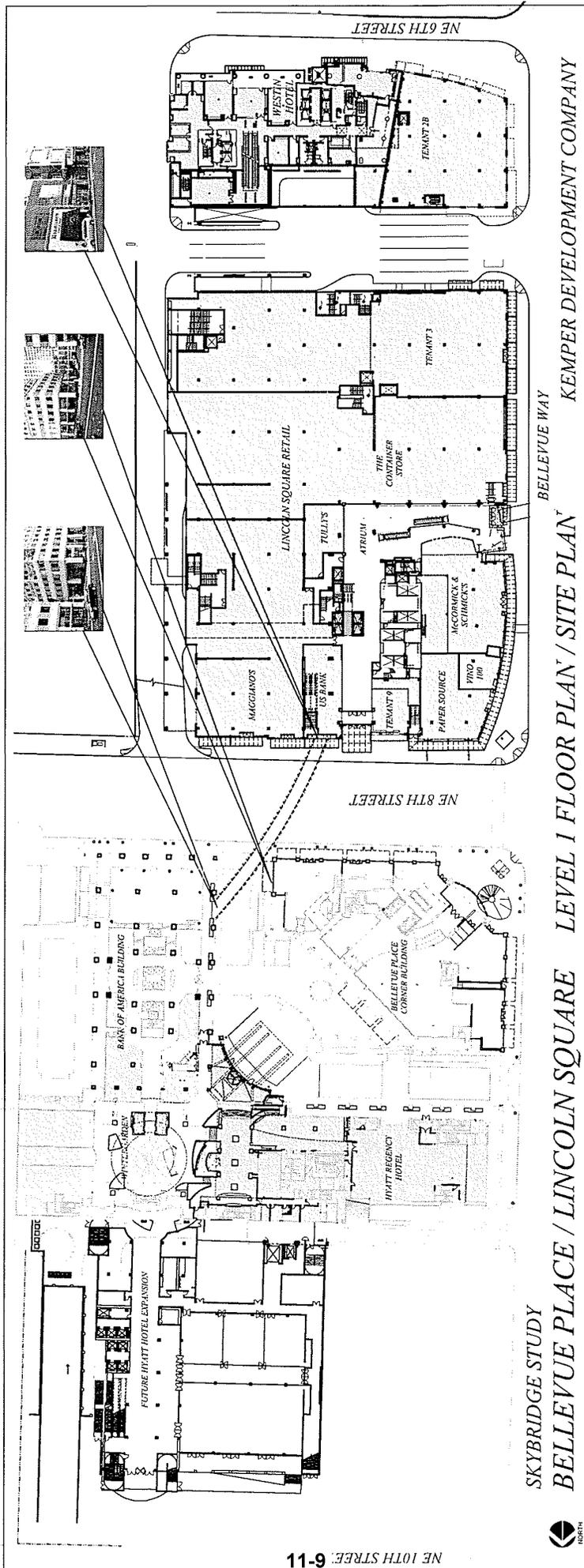
MOTION

Adopt Resolution No. 7435 approving the Pedestrian Bridge location and design plan and adopting findings that the proposal satisfies the public benefit test set forth in LUC 20.25A.130.C and the development standards of LUC 20.25A.130.D and is consistent with the Comprehensive Plan for the pedestrian bridge proposed between Lincoln Square and Bellevue Place.

ATTACHMENTS

1A – 1E. Plans and Elevations

- 2A – 2B. North Entry/Bellevue Place
- 3A – 3B. Views East and West
- 4. Proposed Resolution No. 7435

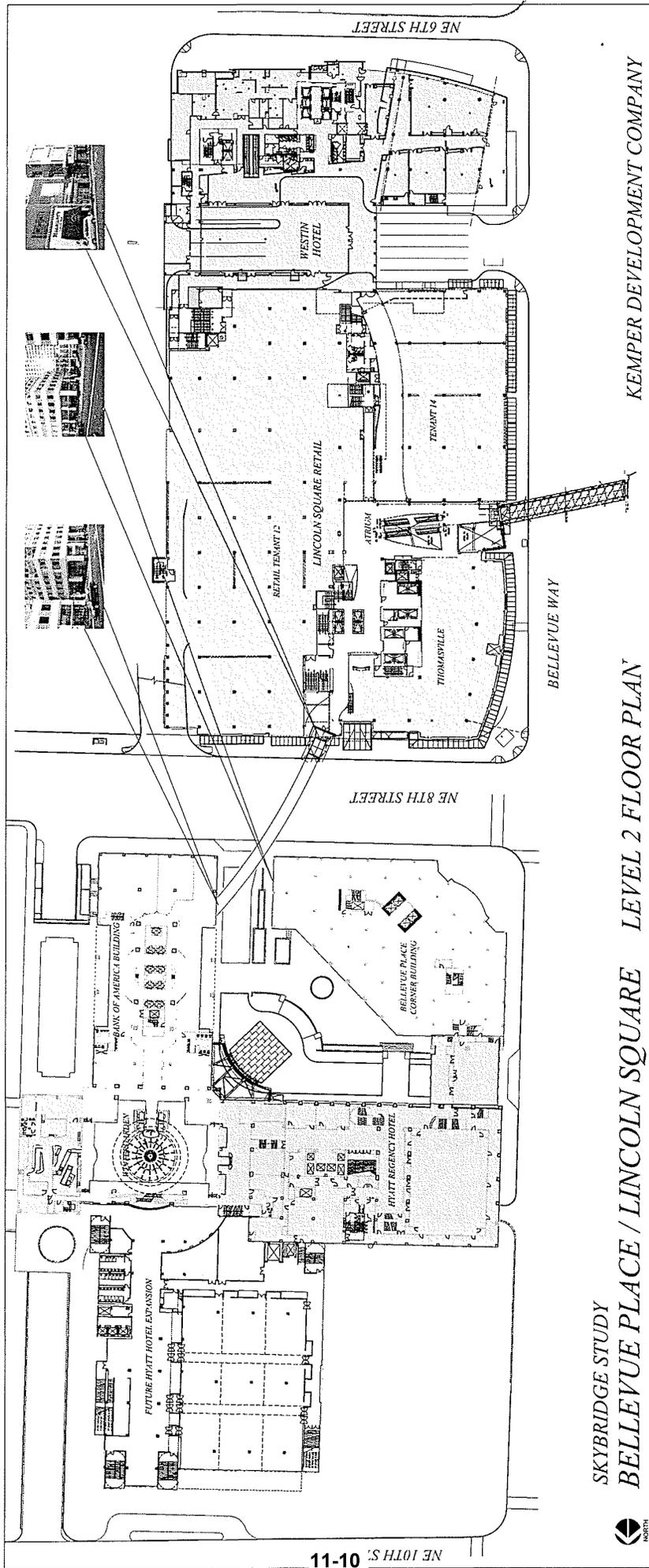


KEMPER DEVELOPMENT COMPANY

LEVEL 1 FLOOR PLAN / SITE PLAN

SKYBRIDGE STUDY

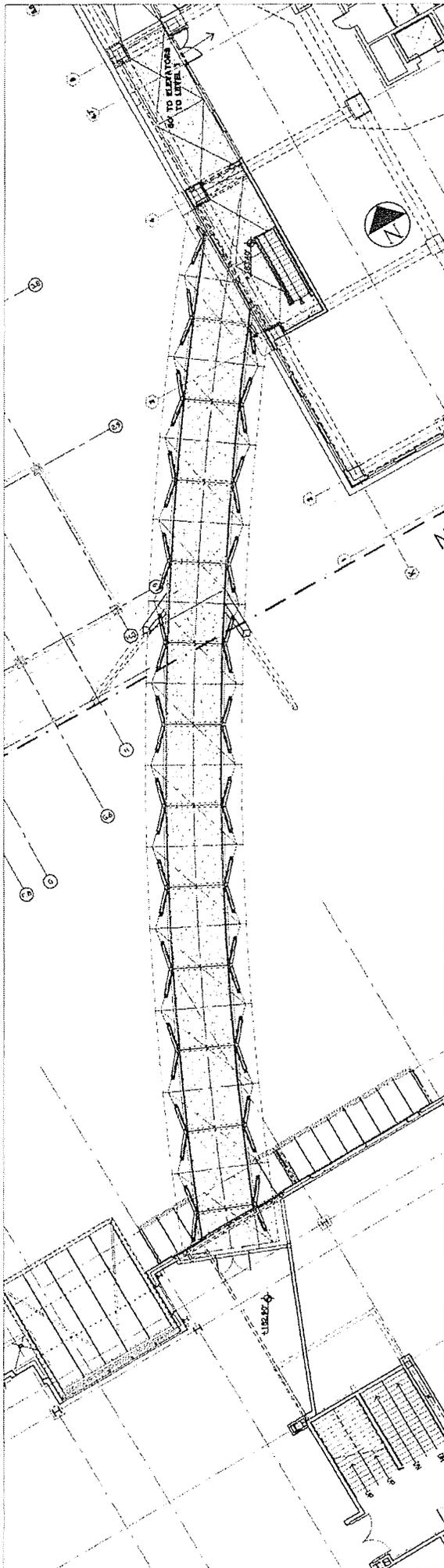




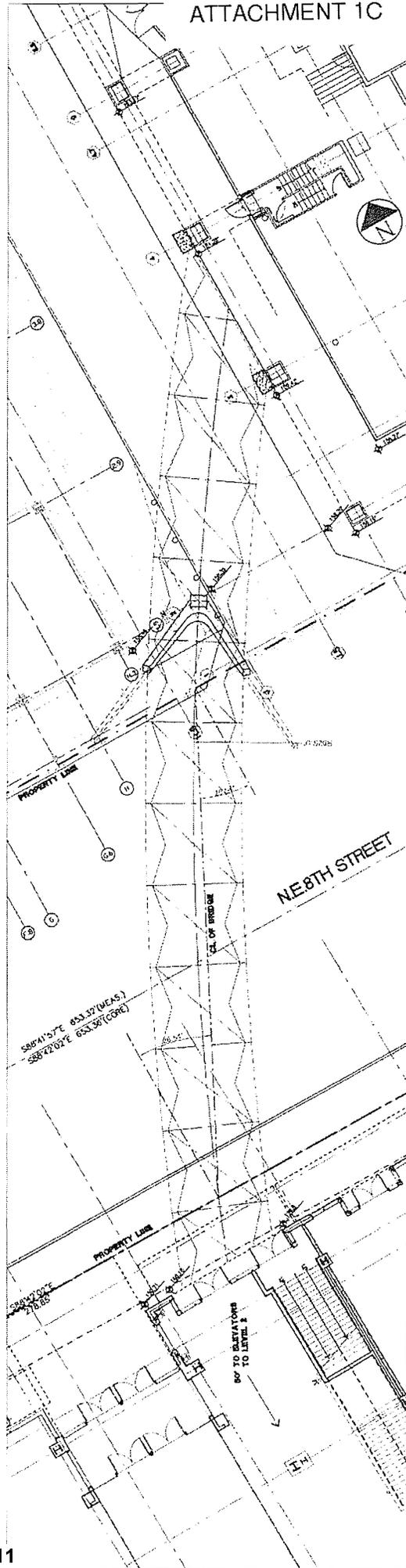
KEMPER DEVELOPMENT COMPANY

SKYBRIDGE STUDY
BELLEVUE PLACE / LINCOLN SQUARE LEVEL 2 FLOOR PLAN





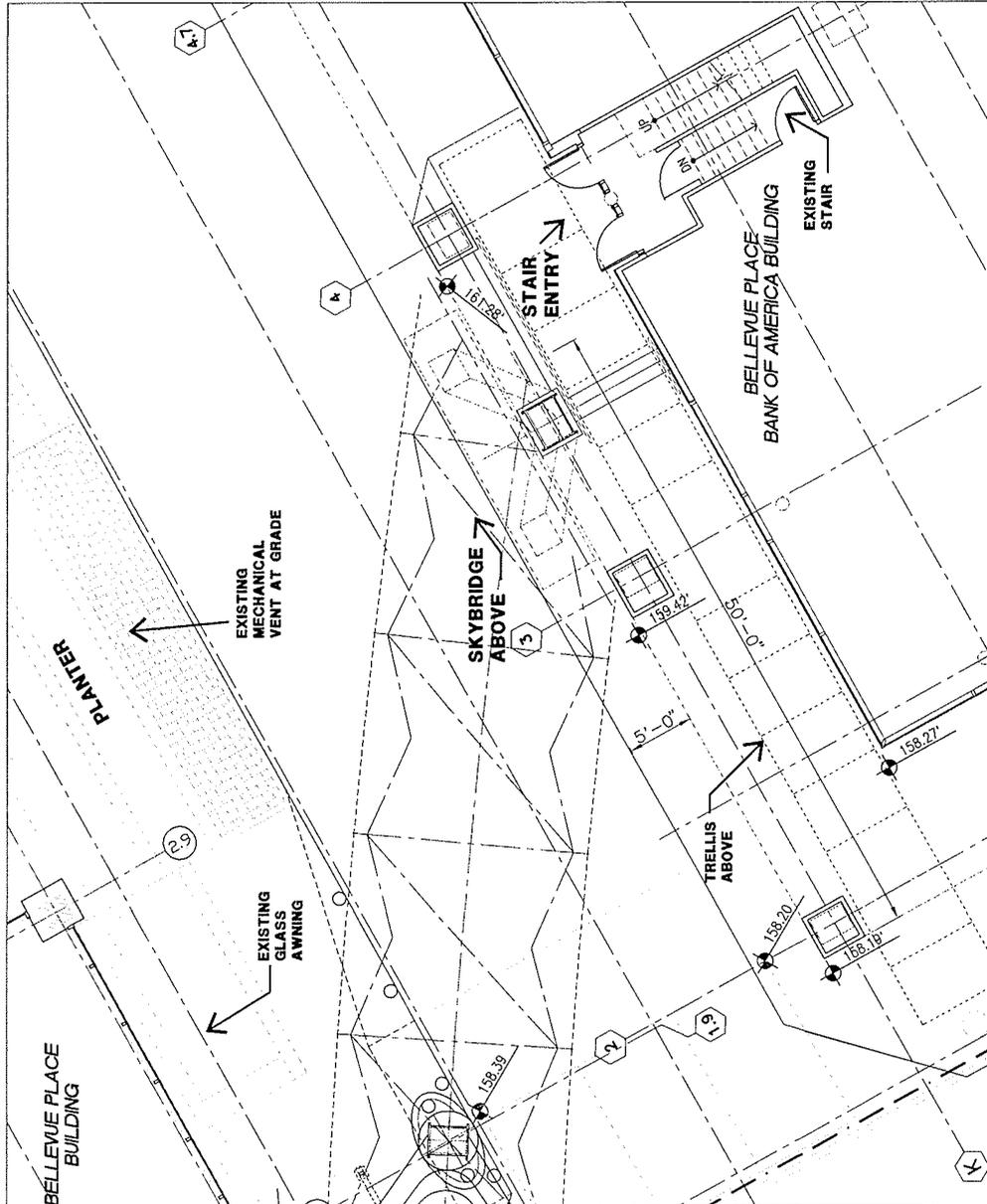
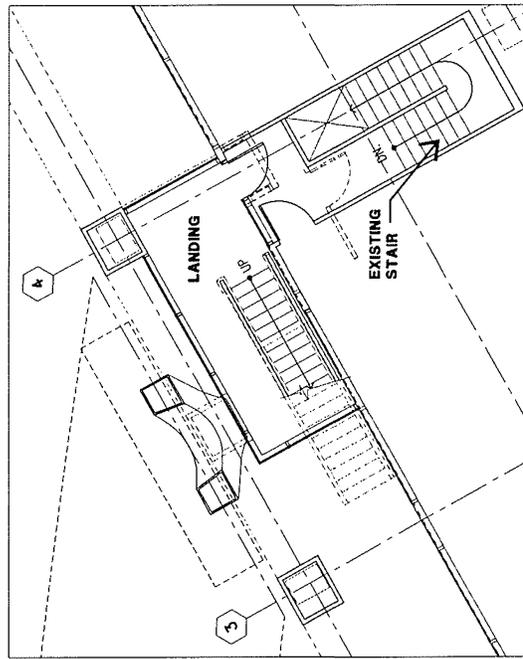
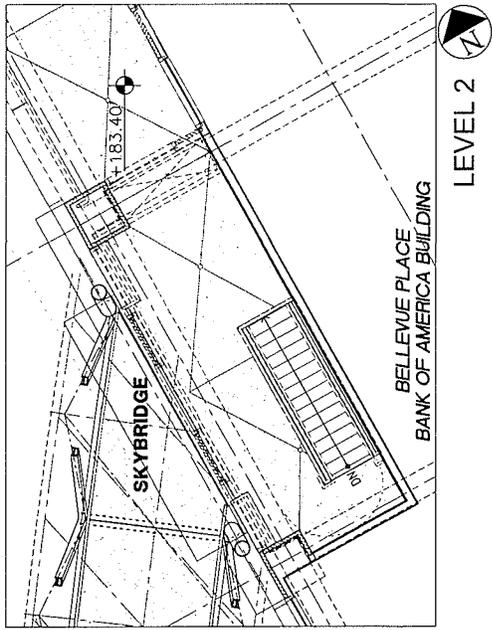
LEVEL 2 FLOOR PLAN



LEVEL 1 FLOOR PLAN
SURVEY- SPOT ELEVATIONS

NE 8TH STREET

S86°41'57" 653.37 (MEAS.)
S86°42'02" 653.50 (CORE)



INTERMEDIATE LEVEL

LEVEL 1

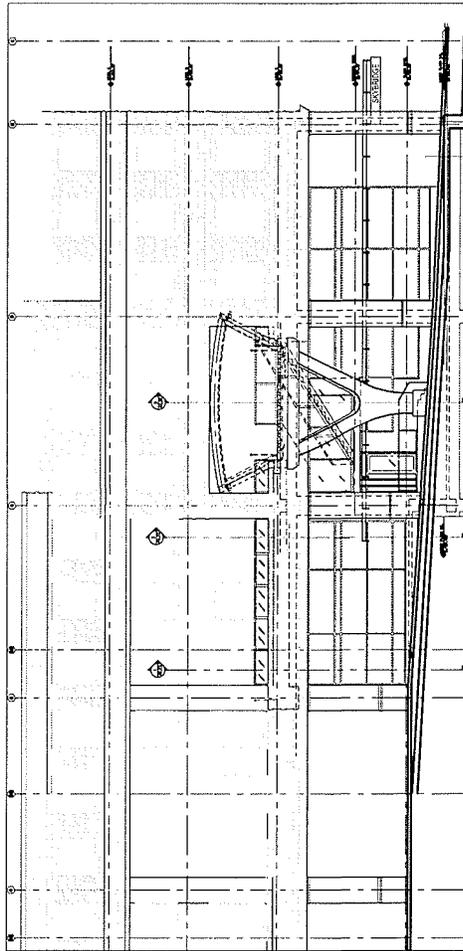
NE 8TH STREET SKYBRIDGE- NORTH ACCESS STAIR

Kemper
Development
Company

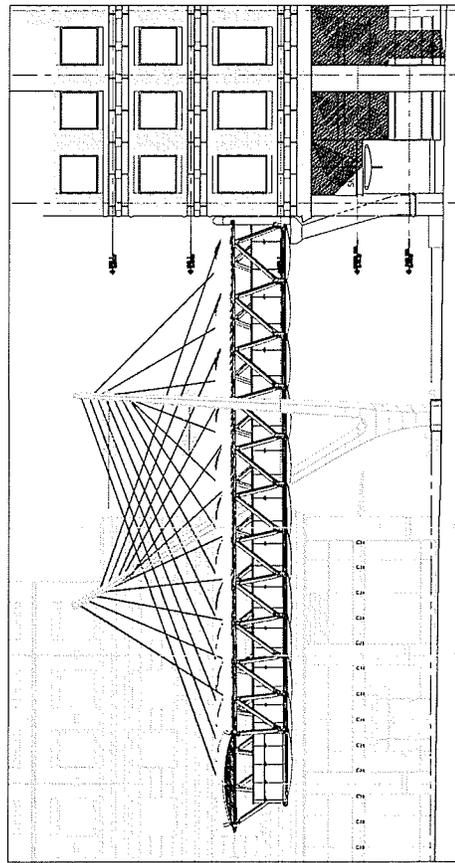


Lincoln Square
BELLEVUE, WASHINGTON
17 AUG 2006

SKYBRIDGE PARTNERS



BELLEVUE PLACE BANK OF AMERICA BLDG.-WEST ELEVATION



BELLEVUE PLACE BANK OF AMERICA BLDG.-SOUTH ELEVATION

NE 8TH STREET SKYBRIDGE- NORTH ACCESS STAIR



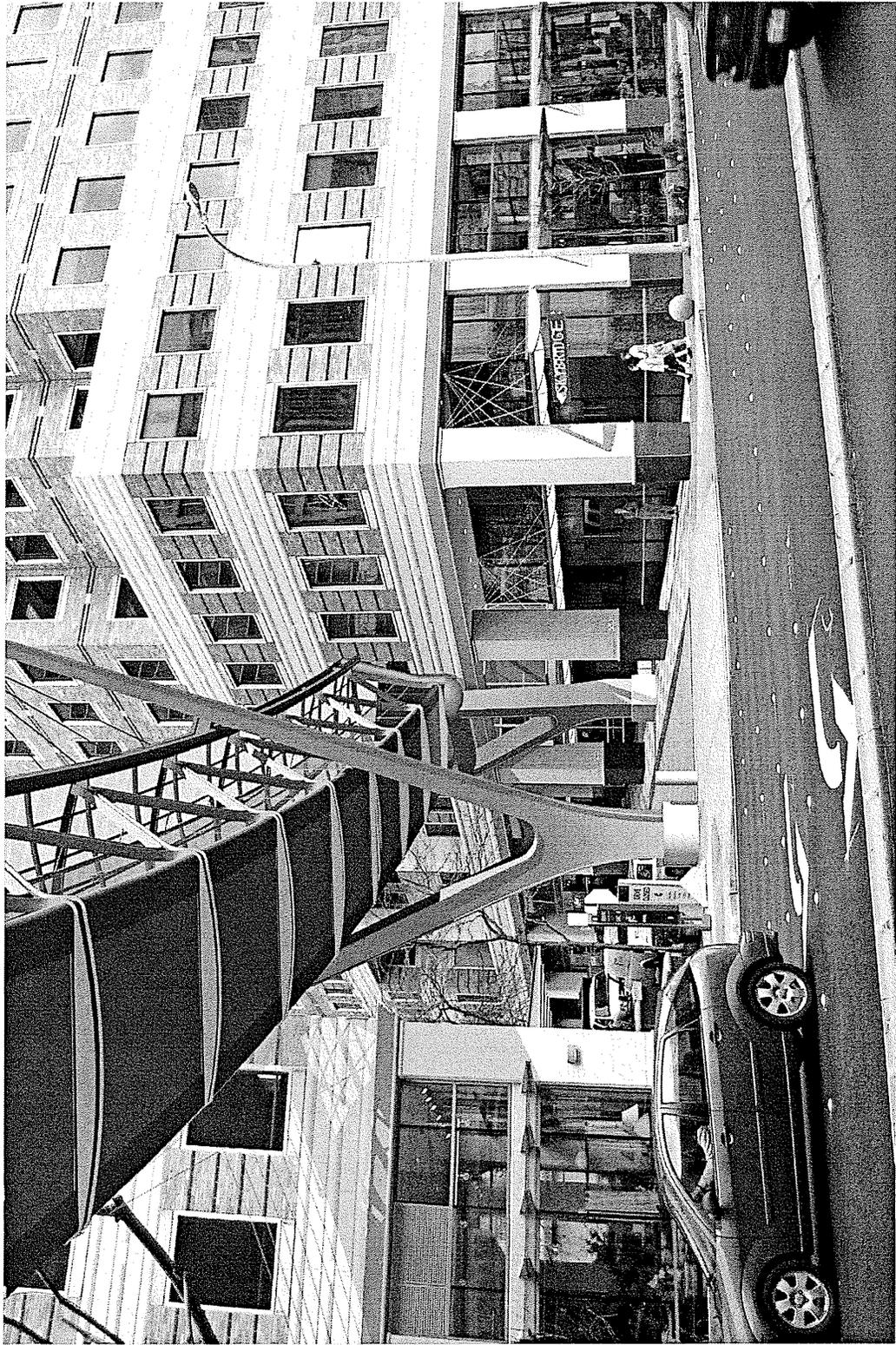
Kemper
Development
Company



Lincoln Square
 BELLEVUE, WASHINGTON
 03 AUG 2006

partners

skidmore
owings
& merrett
partners



NE 8TH STREET SKYBRIDGE- VIEW OF NORTH END FROM LINCOLN SQUARE

Lincoln Square

BELLEVUE, WASHINGTON
17 AUG 2006







VIEW OF 8TH ST SKYBRIDGE LOOKING EAST FROM THE NW CORNER OF 8TH ST & BELLEVUE WAY

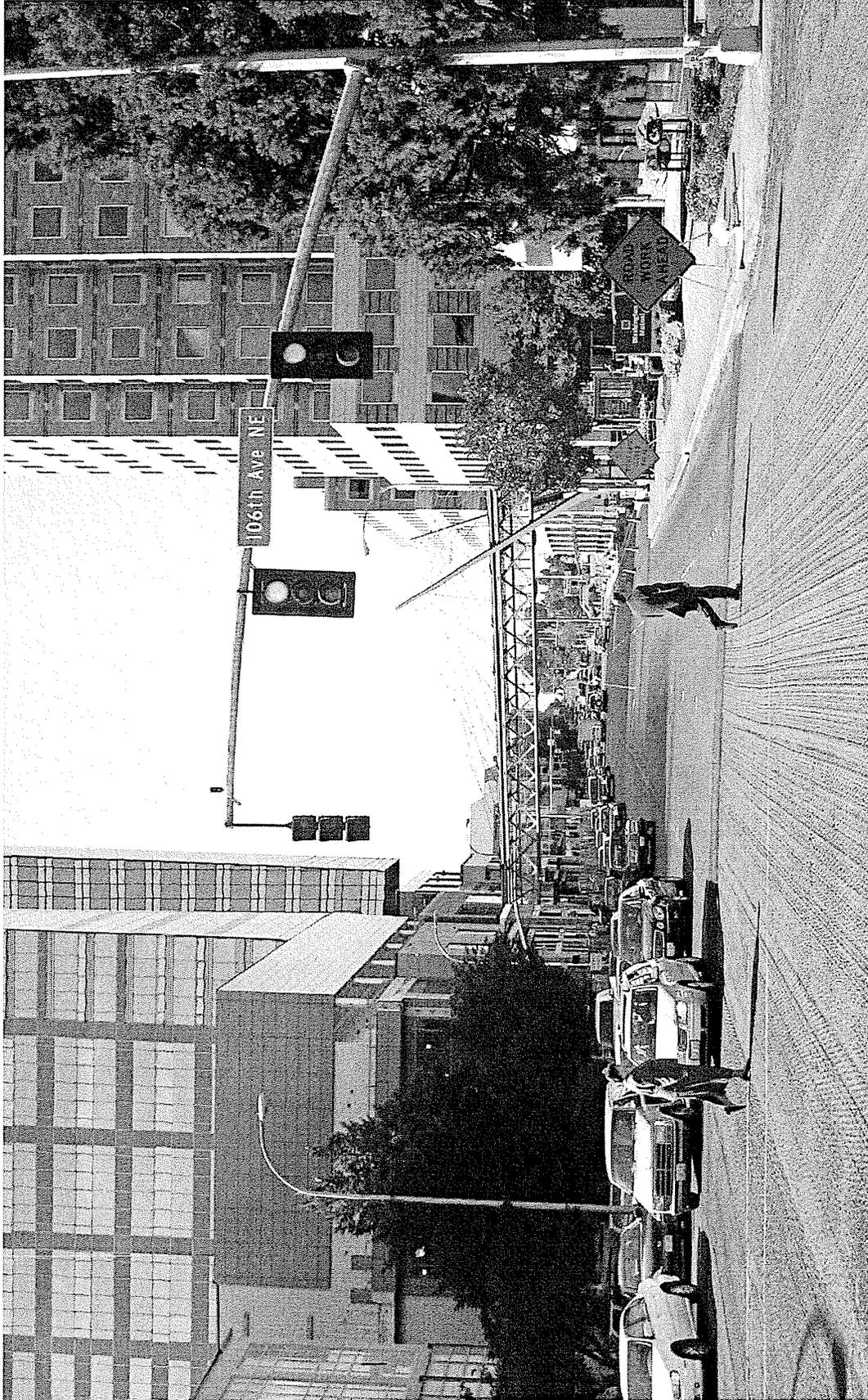
SKYBRIDGE PARTNERS

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Development
Company





VIEW OF 8TH ST SKYBRIDGE LOOKING WEST FROM 106TH AVE NE



Kemper
Development
Company



Lincoln Square

BELLEVUE, WASHINGTON

scd partners

CITY OF BELLEVUE, WASHINGTON

RESOLUTION NO. 7435

A RESOLUTION approving the Pedestrian Bridge location and design plan and adopting findings that the proposal satisfies the public benefit test set forth in LUC 20.25A.130.C and the development standards of LUC 20.25A.130.D and is consistent with the Comprehensive Plan for the pedestrian bridge proposed between Lincoln Square and Bellevue Place.

WHEREAS, Comprehensive Plan Policy S-DT-80 provides "Pedestrian Crossings may be appropriate over the public right-of-way on Bellevue Way between NE 4th Street and NE 8th Street, and over NE 4th and NE 8th Streets between Bellevue Way and 110th Avenue NE, provided that there is a clear demonstration of public benefit and design criteria are fully met." and;

WHEREAS, Land Use Code (LUC) Section 20.25A130.A allows pedestrian bridges at or near the mid-block in certain locations including on NE 8th Street between Bellevue Way and 110th Avenue NE, if certain conditions are met; and,

WHEREAS, the City Council finds that the proposed pedestrian bridge across NE 8th between Lincoln Square and Bellevue Place, as shown in the location and design plan included in agenda materials dated September 5, 2006 (the Pedestrian Bridge) provides a public benefit, meets the development standards in LUC 20.25A.130.D and is consistent with the Comprehensive Plan; and

WHEREAS, the City of Bellevue has complied with the State Environmental Policy Act (SEPA), RCW 43.21C, and the City Environmental Procedures Code, Bellevue City Code (BCC) Chapter 22.02; now therefore,

THE CITY COUNCIL OF THE CITY OF BELLEVUE, WASHINGTON, DOES RESOLVE AS FOLLOWS:

Section 1. The City Council makes the following findings based on the information presented to it, including the location and design plan and analysis contained in the agenda materials dated September 5, 2006:

a. The Pedestrian Bridge improves pedestrian mobility. The presence of the Pedestrian Bridge will greatly enhance a convenient, safe, and barrier- and vehicle-free mid-block crossing of NE 8th Street.

b. The Pedestrian Bridge does not detract from street level activity, but is likely to increase it as it will be a landmark and focal point attracting people to the enhanced streetscape and mid-block building entry points. The proposed location and design of the Pedestrian Bridge integrates pedestrian mobility across the bridge

with the current streetscape at the street level. Vertical circulation adjacent to the bridge provides points of connection between the bridge and the street level.

c. The Pedestrian Bridge functions as part of the public realm. At the street level the architectural character of the bridge has a different language than both of the adjacent developments so that the bridge is an element of its own and not an extension of either one of the adjoining buildings. The tree-like supports on the north side of the bridge relate the public street to the pedestrian bridge and the bridge has a clearly unique identity to distinguish it from Bellevue Place.

Section 2. Based on the findings set forth in Section 1 above, the City Council finds that the Pedestrian Bridge satisfies the public benefit test set forth in LUC 20.25A.130.C.

Section 3. The City Council further finds that the Pedestrian Bridge is consistent with the Comprehensive Plan and meets the development standards of LUC 20.25A.130.D with the following conditions:

a. Prior to issuance of a Certificate of Occupancy for the Pedestrian Bridge, signs shall be installed at/on the exterior of the Lincoln Square and Bellevue Place buildings and at the second level entrances of each development indicating the bridge is open for nonexclusive right of pedestrian use and access by the public between 6AM and midnight, or during the hours of operation of adjacent uses, whichever is greater.

b. Prior to issuance of any building permit for the Pedestrian Bridge, the owners of Lincoln Square and Bellevue Place shall execute and record an agreement, in a form acceptable to the City, containing the terms and conditions set forth in LUC 20.25A.130.E.

c. Prior to issuance of a Certificate of Occupancy for the Pedestrian Bridge, wayfinding signs shall be installed at/on the exterior of the Lincoln Square and Bellevue Place buildings and at the second level entrances of each development identifying the barrier-free pathways and publicly accessible spaces in order to ensure that the pedestrian connections are proximate and easily identifiable.

d. Bridge enclosures shall be limited to the minimum amount necessary for weather protection. The building permit for the Pedestrian Bridge shall demonstrate enclosures consistent with the approved Pedestrian Bridge location and design plan and this condition. Proposals to add to or modify the bridge enclosure may only be approved through the Design Review process, LUC Part 20.30F, now or as hereafter amended, based on demonstration that additional weather protection is required.

e. Bridge lighting shall be consistent with public safety standards. The building permit and any electrical permits for the Pedestrian Bridge shall demonstrate lighting, including seasonal and special event lighting, consistent with the approved Pedestrian Bridge location and design plan and this condition. Proposals to add to or modify the approved lighting, including seasonal and special event lighting, may only be approved through the Design Review process, LUC Part 20.30F, now or as hereafter amended, based on demonstration that impacts upon public facilities and streets are mitigated.

f. Uses in the spaces at grade in Bellevue Place and Lincoln Square adjacent to the Pedestrian Bridge shall be limited to "retail activity", as defined in the City's Design Guidelines – Building/Sidewalk Relationships, now or as hereafter amended.

g. Signage on the exterior of the Pedestrian Bridge, or on the interior of the bridge that is visible from a public sidewalk or street is prohibited.

h. No construction may commence on any part of the Pedestrian Bridge prior to obtaining all necessary permits and agreements for installation of the Pedestrian Bridge over and/or in City right-of-way.

Section 4. The City Council hereby approves, with the conditions set forth in Section 3, the Pedestrian Bridge location and design plan.

Section 5. This resolution shall take effect immediately.

Passed by the City Council this _____ day of _____, 2006,
and signed in authentication of its passage this _____ day of _____,
2006.

(SEAL)

Grant Degginger, Mayor

Attest:

Myrna L. Basich, City Clerk